CONSTRUCTION PLANS - ISSUED JANUARY 12, 2018

RECONSTRUCT T-HANGAR TAXIWAY PAVEMENT WITH PCC AND CORRECT DRAINAGE ISSUES ADJACENT TO THE SOUTH END OF THE MAIN RAMP

ST. LOUIS REGIONAL AIRPORT AUTHORITY ST. LOUIS REGIONAL AIRPORT (ALN) EAST ALTON, MADISON COUNTY, ILLINOIS

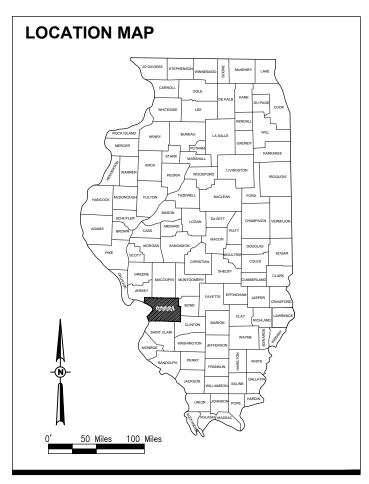
IDA PROJECT NO.: ALN-4587

SBG PROJECT NO.: 3-17-SBGP-139

SCOPE OF WORK:

THIS PROJECT CONSISTS OF RECONSTRUCTING THE T-HANGAR TAXIWAY PAVEMENT NEAR THE SOUTHEAST AREA OF THE MAIN RAMP. THIS PROJECT WILL INCLUDE BITUMINOUS PAVEMENT REMOVAL, CONSTRUCTION OF NEW PCC PAVEMENT, AND DRAINAGE IMPROVEMENTS.

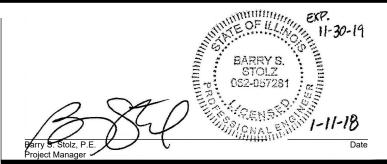
VICINITY MAP NOTA LIEFT Beltrees Beltrees 10 10 2 Godfrey Alton ORAL 11 3 Alton ORAL 11 4 Alton ORAL 11 4 Alton ORAL IIII Alton ORAL IIII ORAL ORAL IIII ORAL IIII ORAL ORAL IIII ORAL ORAL ORAL ORAL ORAL IIII ORAL ORA



NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	Ву





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David C. Miller
Director of Avietion

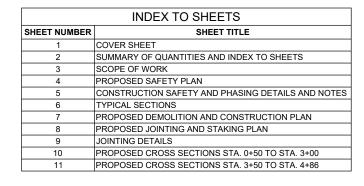
	SUMMARY OF QUANT	ITIES		
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150520	MOBILIZATION	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152480	SHOULDER ADJUSTMENT	S.Y.	64	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	1,211	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	2,820	
AR401911	REMOVE & REPLACE BIT. SURFACE	S.Y.	80	
AR501506	6" PCC PAVEMENT	S.Y.	2,819	
AR501530	PCC TEST BATCH	EA.	1	
AR701006	6" PVC STORM SEWER	L.F.	53	
AR701012	12" PVC STORM SEWER	L.F.	76	
AR701015	15" PVC STORM SEWER	L.F.	20	
AR701900	REMOVE PIPE	L.F.	316	
AR751001	TRENCH DRAIN	L.F.	271	
AR751900	REMOVE INLET	EA.	3	
AR751912	REMOVE SPECIAL STRUCTURE	EA.	1	
AR752850	SPECIAL STRUCTURE	EA.	1	

GENERAL NOTES:

QUANTITIES
PAYMENT WILL BE MADE UNDER THE ITEM NUMBERS, DESCRIPTIONS AND UNITS NOTED IN THE ABOVE TABLE IN ACCORDANCE WITH THE BASIS OF PAYMENT FOR EACH RESPECTIVE WORK ITEM COMPLETED AND ACCEPTED BY THE ENGINEER.

CERITIFIED PAYROLLS
THE RESIDENT ENGINEER/TECHNICIAN CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATIONS
MATERIALS TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL. ALL MATERIALS TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. USE OF MATERIALS WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.





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Illinois Licensed Professional Service Corporation #184-001084



ST. LOUIS REGIONAL AIRPORT AUTHORITY

ST. LOUIS REGIONAL AIRPORT 8 TERMINAL DRIVE EAST ALTON, ILLINOIS 62024



RECONSTRUCT T-HANGAR TAXIWAY PAVEMENT WITH PCC AND CORRECT DRAINAGE ISSUES ADJACENT TO THE SOUTH END OF THE MAIN RAMP

IDA No: ALN-4587

Contract No. SR091

NO.	DATE	DES	CRIPT	ION
INO.	DATE	DES	DWN	REV
ISSUE:	JANUAF	RY 12,	2018	
PROJEC	T NO: 1	7A001	4	

CAD FILE: G-002-FLP.DWG DESIGN BY: JRH 11/29/2017

DRAWN BY: JRH 11/29/2017 REVIEWED BY: BSS 01/10/18

SHEET TITLE

SUMMARY OF QUANTITIES AND INDEX TO SHEETS

<u>UTILITY NOTE</u>

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES.

PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF ITS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND THE RESIDENT ENGINEER/TECHNICIAN SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES.
LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE
COORDINATED WITH AND LOCATED BY THE FAA. ALSO CONTACT AIRPORT MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY______MADISON
CITY______BETHALTO
TOWNSHIP____ALTON - WOODRIVER
SECTION NO.___13 & 18
ADDRESS_____ST. LOUIS REGIONAL AIRPO

ST. LOUIS REGIONAL AIRPORT 8 TERMINAL DRIVE EAST ALTON, ILLINOIS 62024



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TWY A

TWY B1

RECONSTRUCT T-HANGAR

AND CORRECT DRAINAGE

TAXIWAY PAVEMENT WITH PCC

CONTROL POINT DATA							
NO.	DESCRIPTION	LATITUDE	LONGITUDE	ELEV.			
Α	NGS PID: DE9194	N038* 53' 07.85"	W090° 03' 29.40"	538.56			
В	NGS PID: DE9195	N038° 52' 58.25"	W090° 03' 26.69"	534.78			
С	TP CROSS	N038° 53' 25.76"	W090' 03' 13.25"	540.76			
D	TP IRON ROD	N038* 53' 18.52"	W090° 03' 18.04"	539.62			

SURVEY NOTES

- . ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE WEST ZONE NAD-83 (2007).
- 2. REFER TO IDOT STANDARD SPECIFICATIONS 50-06, CONSTRUCTION LAYOUT STAKES, FOR ADDITIONAL INFORMATION.

<u>LEGEND</u>

EXISTING IMPROVEMENTS

EXISTING BUILDINGS

PROPOSED IMPROVEMENTS

GENERAL NOTES

TROSE PPPTTIPO

HALF SIZE SCALE: 1"= 1000

FULL SIZE SCALE: 1"= 500'

4-3-11-11 [2-3-12-6]

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BUCHTA ROAD (TOWNSHIP)

- THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS AND THEIR APPROXIMATE LOCATIONS AND LIMITS, FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. THIS SHEET SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE FOLLOWING PLAN SHEETS FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
- THIS PROJECT CONSISTS OF RECONSTRUCTING THE T-HANGAR TAXIWAY PAVEMENT NEAR THE SOUTHEAST AREA OF THE MAIN RAMP. THIS PROJECT WILL INCLUDE PAVEMENT REMOVAL AND PLACEMENT AND DRAINAGE IMPROVEMENTS.
- THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN.
- . THE CONTRACTOR IS NOT PERMITTED TO USE THE AIRPORT ENTRANCE DRIVE AND AUTO PARKING LOT FOR MATERIAL AND EQUIPMENT HAULING OR STORAGE. THE CONSTRUCTION ENTRANCE AS SHOWN ON THE PROPOSED SAFETY AND PHASING PLAN WILL BE THE PRIMARY ACCESS TO THE PROPOSED CONSTRUCTION SITE. ALL HAULING OF MATERIALS AND EQUIPMENT SHALL BE RESTRICTED TO THE DESIGNATED CONSTRUCTION ENTRANCE.
- 6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT. ANY DAMAGE TO FAA CABLES, ACCESS ROADS OR TO FAA FACILITIES DURING CONSTRUCTION WILL REQUIRE THE CONTRACTOR TO REPLACE THE DAMAGED CABLES, ACCESS ROAD OR FAA FACILITIES TO FAA REQUIREMENTS AT THE CONTRACTOR'S EXPENSE. SPLICING OF CABLES IS NOT AN ACCEPTABLE FORM OF REPAIR.
- NO EQUIPMENT SHALL BE PERMITTED TO CROSS OR USE ANY EXISTING PAVEMENT OUTSIDE THE CONSTRUCTION LIMITS, GENERAL PROJECT AREA OR HAUL ROUTE.
- . CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- . UNLESS OTHERWISE NOTED, ALL DISTURBED AREAS OUTSIDE OF THE PROPOSED CONSTRUCTION LIMITS SHALL BE GRADED, SEEDED AND/OR HYDROMULCH SEEDED AT NO ADDITIONAL COST TO THE CONTRACT.
- 10. ALL WASTE MATERIAL SHALL BE HAULED FROM THE AIRPORT AND PROPERLY DISPOSED OF UNLESS OTHERWISE SPECIFIED HEREIN.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
- THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- THE CONTRACTOR SHALL COORDINATE WITH THE RESIDENT ENGINEER/TECHNICIAN SO THEY MAY DEVELOP ONE SET OF REDLINED AS—BUILT DRAWINGS AT THE COMPLETION OF THE PROJECT.
- 14. CONTRACTOR SHALL NOTE THAT ALL AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO BEGINNING ANY WORK. ALL AREAS WHICH HAVE BEEN FARMED AND OR DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION, AND HAVE BEEN DISTURBED BY CONSTRUCTION ACTIVITY, SHALL BE CHISEL PLOWED (36" MAX.) OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER.)
- 15. CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS TO BE REPAIRED, GRADED, AND SEEDED AND MULCHED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF THE HAUL POLITE.
- 16. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER/TECHNICIAN IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS
- 17. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER PERSONS FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- 18. THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE FOR ALL AREAS AFFECTED BY HIS WORK.



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ST. LOUIS REGIONAL AIRPORT AUTHORITY

ST. LOUIS REGIONAL AIRPORT 8 TERMINAL DRIVE EAST ALTON, ILLINOIS 62024



RECONSTRUCT
T-HANGAR TAXIWAY
PAVEMENT WITH PCC
AND CORRECT DRAINAGE
ISSUES ADJACENT TO
THE SOUTH END OF THE
MAIN RAMP

IDA No: ALN-4587

Contract No. SR091

NO.	DATE	DESCRIPTION			
110.	DATE	DES	DWN	REV	
ISSUE: JANUARY 12, 2018					
PROJECT NO: 17A0014					
CAD FILE: G-003-SOW.DWG					

SHEET TITLE

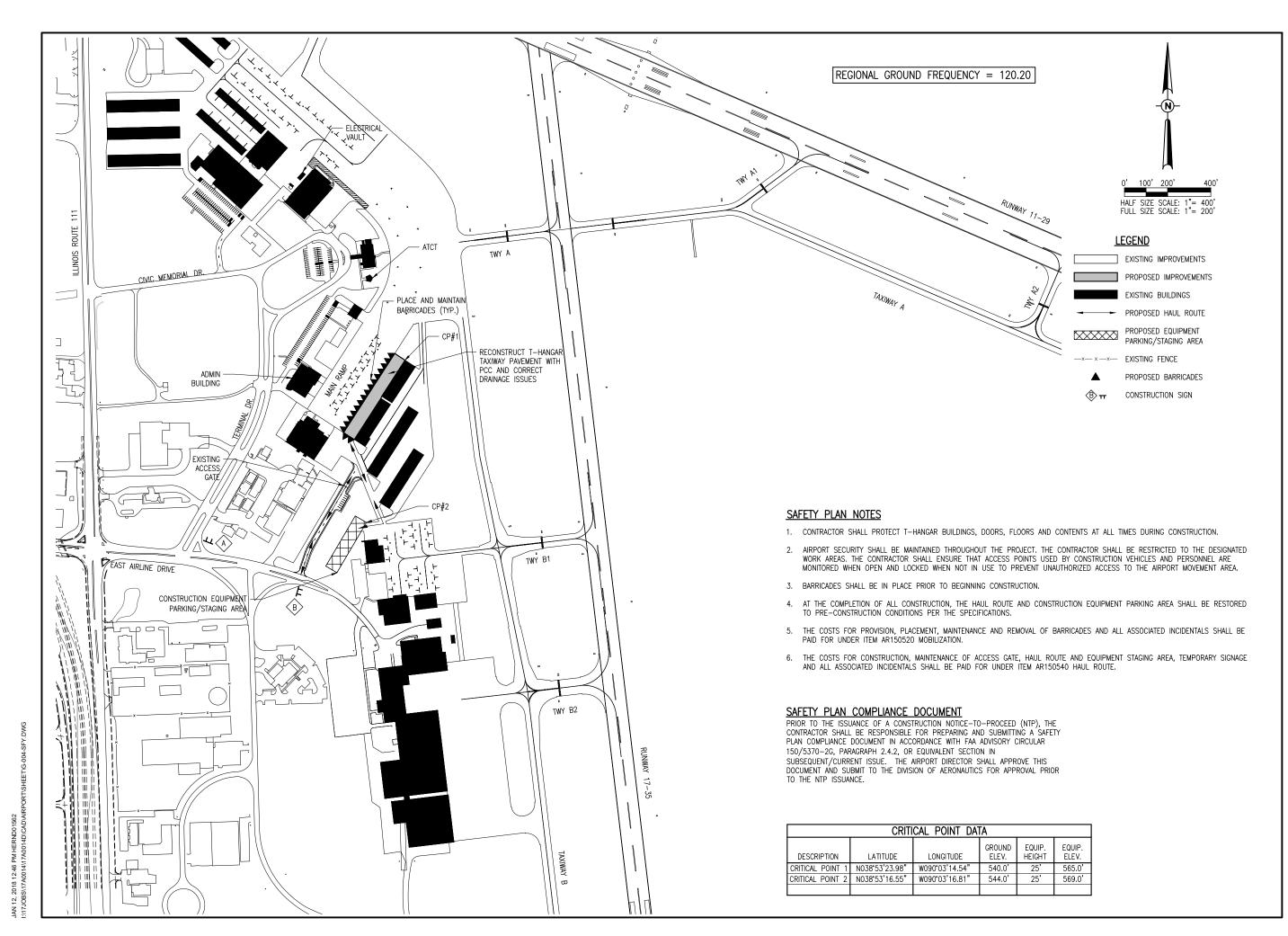
SCOPE OF WORK

DESIGN BY: JRH 11/30/2017 DRAWN BY: JRH 11/30/2017

REVIEWED BY: BSS 01/10/18

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RECONSTRUCT T-HANGAR TAXIWAY PAVEMENT WITH PCC AND CORRECT DRAINAGE ISSUES ADJACENT TO THE SOUTH END OF THE MAIN RAMP

IDA No: ALN-4587

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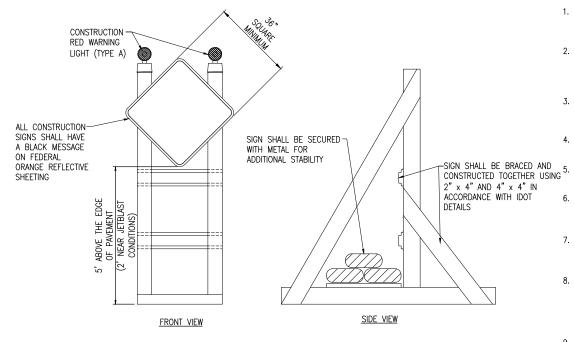
NO.	DATE	DESCRIPTION				
INO.	DATE	DES	DWN	REV		
ISSUE:	JANUAF	RY 12,	2018			
PROJEC	CT NO: 1	7A001	4			
CAD FILE: G-004-SFY.DWG						
DESIGN BY: JRH 12/04/2017						
DRAWN BY: JRH 12/04/2017						

PROPOSED SAFETY

REVIEWED BY: BSS 01/10/18

SHEET TITLE

PLAN



SIGNAGE NOTES

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- 2. UNLESS OTHERWISE SPECIFIED, CONSTRUCTION SIGNS SHALL BE MOUNTED ON PORTABLE OR NON-PORTABLE SUPPORTS. A PORTABLE SUPPORT IS DEFINED AS A TYPICAL SIGN STANDARD AS SHOWN ON THIS SHEET. OR A SMALL LIGHT WEIGHT TRAILER, A NON-PORTABLE SUPPORT IS DEFINED AS DRIVEN METAL OR WOOD POST. ALL SIGNS, REGARDLESS OF THE TYPE OF SUPPORTS USED, SHALL BE MOUNTED SUCH THAT THE MESSAGE ON THE SIGN IS LEVEL IN THE HORIZONTAL PLANE AFTER PLACEMENT. THE COST OF CONSTRUCTION WARNING LIGHTS SHALL BE INCLUDED IN THE COST OF THE CONSTRUCTION
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY ARE TO BE USED IN A LOW INTENSITY FLASHING MODE (TYPE A)
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING SIGNS SHALL BE INCLUDED IN ITEM AR150540 HAUL ROUTE.

CONSTRUCTION **CONSTRUCTION TRAFFIC** TRAFFIC $\langle A \rangle$ $\langle B \rangle$ 48" x 48 48" y 48"

BARRICADE NOTES

- 1. ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- 2. BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT IN 10' INCREMENTS. BARRICADES ARE TO BE SET BACK 93' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS.
- 3. CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE. LENS DIRECTED. ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- 4. THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR BEFORE SUNSET AND 1/2 HOUR AFTER SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION
- 5. BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- 6. THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING BARRICADES SHALL BE INCLUDED IN THE COST OF THE OTHER CONTRACT ITEMS.

SAFETY NOTES

- FOLLOWING ARE THE CONSTRUCTION SAFFTY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET AND THIS
- 2. ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- 3. THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFFTY AND PHASING PLAN SHFFTS
- 4. NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED IN THE EQUIPMENT PARKING/STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- 7. NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 93' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY AIRPORT OPERATIONS AREA WILL BE PERMITTED UNLESS PROPERLY MARKED, OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.
- 10. OPEN TRENCHES EXCAVATIONS AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR
- 11. NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT. HOWEVER OTHER EQUIPMENT TALLER THAN 25' MAY BE PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE
- 12. NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS. ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT
- 13. SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEPT, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- 14. EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE
- 15. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- 16. CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT
- 17. CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- 18. THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE
- 19. CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- 20. CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- 21. CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE
- 22. THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- 23. NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 7 DAYS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED BY THE AIRPORT MANAGER TO THE APPROPRIATE FLIGHT SERVICE STATION.
- 24. UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.



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AIRPORT AUTHORITY

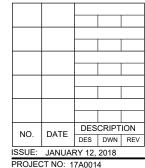
ST. LOUIS REGIONAL AIRPORT 8 TERMINAL DRIVE EAST ALTON, ILLINOIS 62024



RECONSTRUCT T-HANGAR TAXIWAY PAVEMENT WITH PCC AND CORRECT DRAINAGE ISSUES ADJACENT TO THE SOUTH END OF THE MAIN RAMP

IDA No: ALN-4587

Contract No. SR091



CAD FILE: G-501-SFY.DWG

DESIGN BY: JRH 12/07/2017 DRAWN BY: JRH 12/07/2017

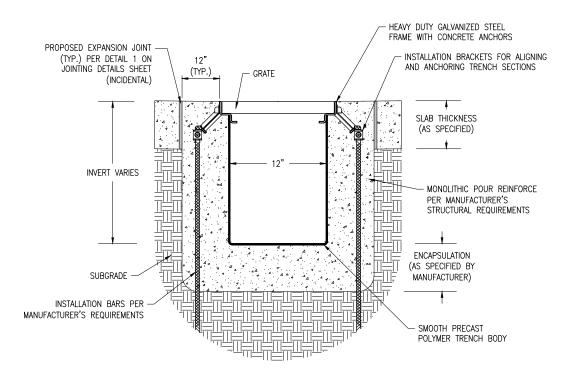
REVIEWED BY: BSS 01/10/18

SHEET TITLE

CONSTRUCTION SAFETY AND PHASING DETAILS AND NOTES

TYPICAL SECTION - PROPOSED T-HANGAR TAXIWAY NOT TO SCALE

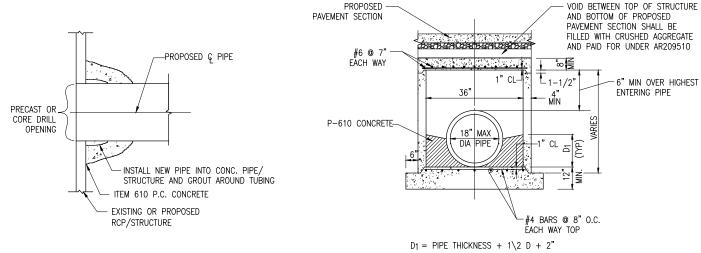
NOTE
CONTRACTOR MAY ELECT TO ADJUST CONSTRUCTION LIMITS TO 1 FOOT OUTSIDE OF THE HANGAR DOORS TO FACILITATE CONSTRUCTION WITH



TRENCH DRAIN TYPICAL SECTION A-A

NOT TO SCALE

- STANDARD SLOPE IS 1.0% UNLESS OTHERWISE SPECIFIED
- REINFORCE ACCORDING TO MANUFACTURER'S STRUCTURAL REQUIREMENTS
- 3. TRENCH DRAIN MUST BE 1 BELOW FINISHED CONCRETE GRADE



SPECIAL JUNCTION STRUCTURE

NOT TO SCALE

COST OF ALL WORK ASSOCIATED WITH THIS DETAIL SHALL BE INCLUDED IN COST OF PAY ITEM FOR

SHALL MEET ALL REQUIREMENTS IN IDOT APPROVED

STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE

DESIGN AS SET FORTH IN THE CURRENT IDOT

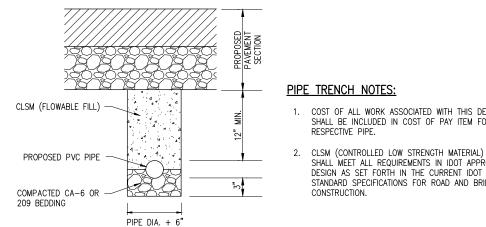
RESPECTIVE PIPE.

CONSTRUCTION.

CONCRETE COLLAR AND GROUT CONNECTION

NOT TO SCALE

NOTE: CONNECTION IS INCIDENTAL TO PIPE.



PIPE TRENCH DETAIL

NOT TO SCALE

OUTLET PIPE CONNECTION TO -TRENCH DRAIN PER MANUFACTURER'S REQUIREMENTS 12" TRENCH DRAIN AND -GRATE (AR751001) 12" I.D. PVC 12" I.D. PVC PROPOSED OUTLET PIPE OUTLET PIPE SPECIAL JUNCTION STRUCTURE ITEM 610 P.C. CONCRETE TYP. TRENCH (INCIDENTAL TO TRENCH DRAIN) 12" (ALL SIDES) DRAIN 15" I.D. OUTLET

PLAN VIEW NOT TO SCALE

TRENCH DRAIN NOTES:

- 1. FOR TRENCH DRAIN LOCATION AND INVERT ELEVATIONS, SEE CONSTRUCTION PLAN.
- TRENCH DRAIN ACCESSORIES, TO INCLUDE BUT NOT LIMITED TO, CHANNEL FRAMES, COUPLERS, AND ADAPTERS ARE THE RESPONSIBILITY OF THE CONTRACTOR AND INCIDENTAL TO TRENCH DRAIN.



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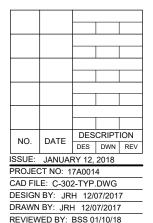
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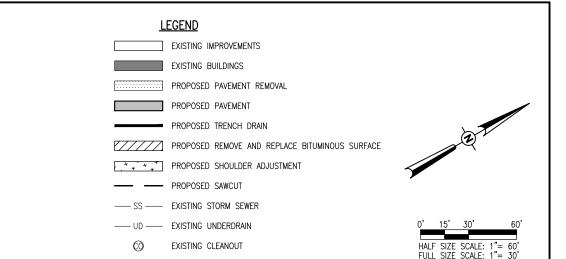
TYPICAL SECTIONS

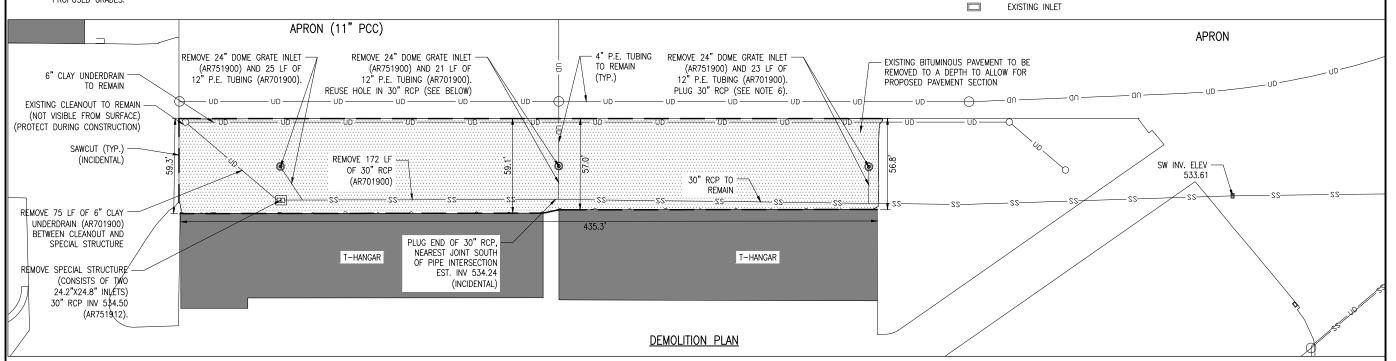
SHEET TITLE

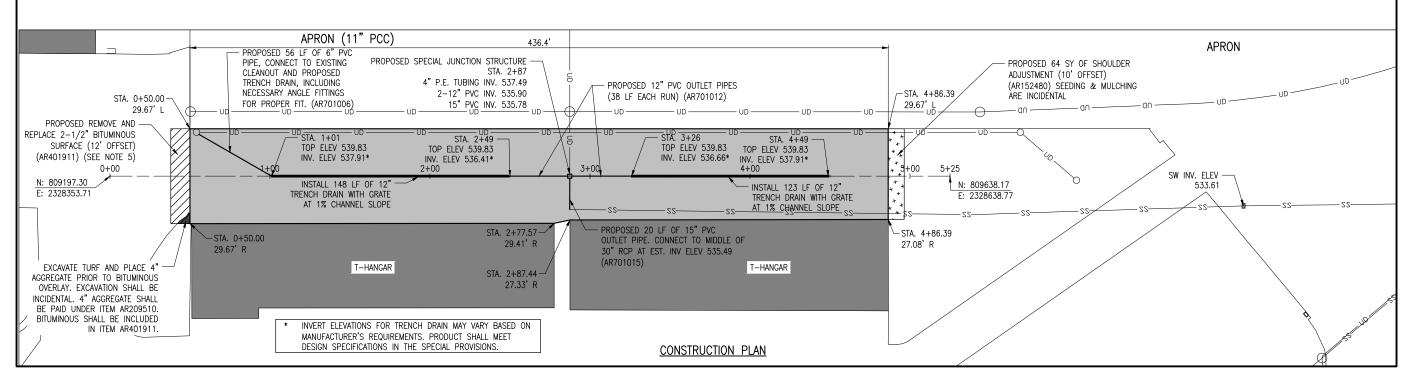
NOTES

- EXISTING T-HANGAR TAXIWAY PAVEMENT SECTION IS APPROX. 2-3/8"
 ASPHALT ON 10" CRUSHED AGGREGATE. DEPTH OF PAVEMENT REMOVAL
 SHALL ACCOMMODATE PROPOSED PAVEMENT DEPTH/ELEVATIONS.
- PROPOSED T-HANGAR TAXIWAY PAVEMENT SECTION IS 6" PCC ON 4" CRUSHED AGGREGATE.
- 3. CONTRACTOR SHALL PROTECT T-HANGAR BUILDINGS, DOORS, FLOORS AND CONTENTS AT ALL TIMES DURING CONSTRUCTION.
- CONTRACTOR MAY ELECT TO ADJUST CONSTRUCTION LIMITS TO 1 FOOT OUTSIDE OF THE HANGAR DOORS TO FACILITATE CONSTRUCTION AT NO ADDITIONAL COST TO THE CONTRACT.
- 5. DURING THE BITUMINOUS SURFACE REMOVAL AND REPLACEMENT WORK, THE CONTRACTOR SHALL PROTECT THE THE PROPOSED PCC PAVEMENT AREA AND EXISTING PAVEMENT TO REMAIN. THE INTENT OF THIS WORK ITEM IS TO REMOVE A SUFFICIENT DEPTH OF PAVEMENT IN ORDER TO PAVE A 2-1/2" DEPTH OF BITUMINOUS SURFACE TO PROPOSED GRADES. IF FOR ANY REASON THE CONTRACTOR REMOVES A GREATER DEPTH, NO ADDITIONAL COMPENSATION WILL BE ALLOWED TO PAVE BACK TO THE PROPOSED GRADES

- THE COST OF PLUGGING HOLE IN 30" RCP FROM 12" P.E. TUBING REMOVAL WILL BE INCIDENTAL TO ITEM AR701900 - REMOVE PIPE.
- VOIDS LEFT BY REMOVAL OF STRUCTURES AND PIPES WILL BE BACKFILLED TO A DEPTH TO ALLOW FOR PROPOSED PAVEMENT SECTION USING CRUSHED AGGREGATE TO BE PAID UNDER ITEM AR209510.
- 8. EXISTING UNDERDRAINS TO BE ENCOUNTERED WITHIN THE PROJECT AREA MAY CONTAIN SOME AMOUNT OF WATER. THIS HAS NOT BEEN ABLE TO BE VERIFIED AND IS THEREFORE UNKNOWN. THE CONTRACTOR IS EXPECTED TO ANTICIPATE THIS CONDITION AND VERIFY DURING CONSTRUCTION. METHODS OF ALLEVIATING AND CONTROLLING WATER WITHIN THE PROJECT AREA SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE INCLUDED WITHIN THEIR BID AS INCIDENTAL TO OTHER ITEMS.









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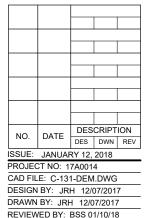
ST. LOUIS REGIONAL AIRPORT 8 TERMINAL DRIVE EAST ALTON, ILLINOIS 62024



RECONSTRUCT T-HANGAR TAXIWAY PAVEMENT WITH PCC AND CORRECT DRAINAGE ISSUES ADJACENT TO THE SOUTH END OF THE MAIN RAMP

IDA No: ALN-4587

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SHEET TITLE

PROPOSED
DEMOLITION AND
CONSTRUCTION
PLAN

JOINTING NOTES

- 1. ALL JOINT EDGES SHALL BE SAWCUT TO PRODUCE THE 1/4" CHAMFER.
- 2. ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED.
- 3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO INSTALLATION.
- 4. DOWEL BARS FOR 6 IN. THICK PAVEMENT SHALL BE 3/4 IN. DIAMETER, 18 IN. LONG AT 12 IN. SPACING.
- 5. DOWELS IN TRANSVERSE CONTRACTION AND LONGITUDINAL CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.
- 6. ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE ±1/8" FOR CONSTRUCTION JOINTS AND ±1/4" FOR CONTRACTION JOINTS.
- 7. THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED DOWELS IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE DIVISION OF AERONAUTICS PRIOR TO USE.
- 8. THE COST OF ALL DOWEL BARS, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE 501 PCC PAVEMENT.
- 9. WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL PROTECT THE TRANSVERSE JOINTS.
- 10. JOINT SEALANT SHALL BE AS SPECIFIED IN THE STANDARD SPECIFICATIONS ITEM 605.
- 11. CURING COMPOUND SHALL BE AS SPECIFIED IN THE STANDARD SPECIFICATIONS ITEM 501-2.9 AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE RESIDENT ENGINEER/TECHNICIAN.

T-HANGAR

12. ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.

LEGEND EXISTING IMPROVEMENTS EXISTING BUILDINGS PROPOSED PCC PAVEMENT **─A** ISOLATION JOINT (TYPE A) ---C-- DOWELED CONTRACTION JOINT (TYPE C) ---E-- DOWELED CONSTRUCTION JOINT (TYPE E) DUMMY CONTRACTION JOINT (TYPE D) PROPOSED REINFORCED PANEL \$541.00 PROPOSED PAVEMENT SPOT ELEVATION

CONTRACTOR MAY PROPOSE ALTERNATIVE PAVING/JOINTING PLAN FOR ENGINEER'S REVIEW AND APPROVAL FOLLOWING AWARD.

T-HANGAR



BARRY S. STOLZ 062-057281 RECONSTRUCT

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St.Louis Regional

ST. LOUIS REGIONAL AIRPORT 8 TERMINAL DRIVE

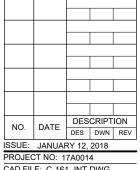
EAST ALTON, ILLINOIS 62024

AIRPORT AUTHORITY

T-HANGAR TAXIWAY PAVEMENT WITH PCC AND CORRECT DRAINAGE ISSUES ADJACENT TO THE SOUTH END OF THE MAIN RAMP

IDA No: ALN-4587

Contract No. SR091

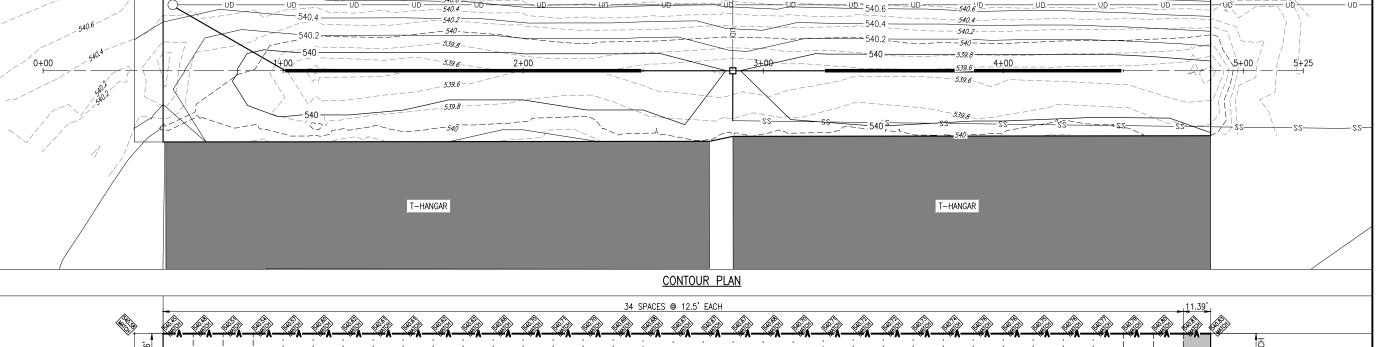


CAD FILE: C-161-JNT.DWG

DESIGN BY: JRH 12/11/2017 DRAWN BY: JRH 12/11/2017 REVIEWED BY: BSS 01/10/18

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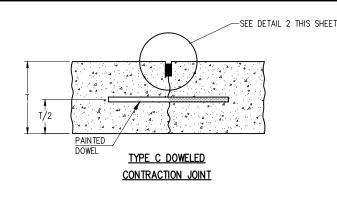
PROPOSED JOINTING AND STAKING PLAN

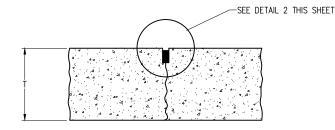


JOINTING/STAKING PLAN

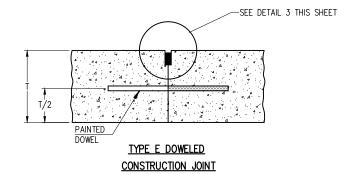
PROPOSED REMOVE

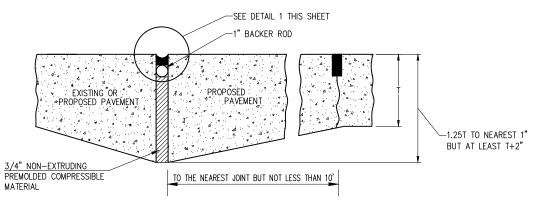
AND REPLACE BITUMINOUS SURFACE



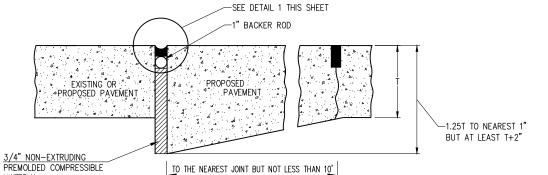


TYPE D DUMMY
CONTRACTION JOINT





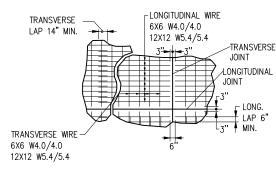
TYPE—A THICKENED EDGE ISOLATION JOINT



TYPE-A MODIFIED

ISOLATION JOINT

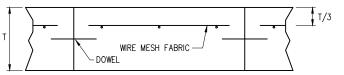
MATERIAL



REINFORCEMENT SHEET WIRE FABRIC DETAIL

NOT TO SCALE

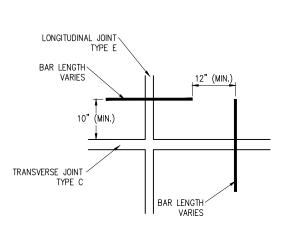
DIFFERENT WIRE MESH SIZES/CONFIGURATIONS OR SIZES ARE PERMITTED IF THE MINIMUM CROSS SECTIONAL AREA EQUALS 0.5 OR GREATER.



PAVEMENT REINFORCING DETAIL NOT TO SCALE

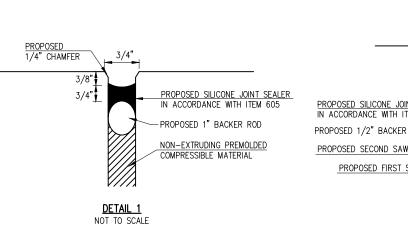
REINFORCING NOTES:

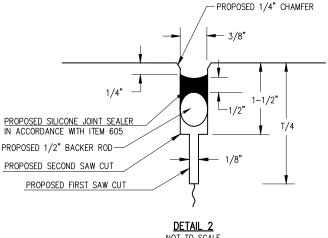
- . END LAPS SHALL BE A MINIMUM OF 12", BUT NOT LESS THAN 30 TIMES THE DIAMETER OF THE LONGITUDINAL WIRE OR BAR.
- 2. SIDE LAPS SHALL BE A MINIMUM OF 6", BUT NOT LESS THAN 20 TIMES THE DIAMETER OF TRANSVERSE WIRE OR BAR.
- 3. END AND SIDE CLEARANCES SHALL BE A MAXIMUM OF 6" AND A MINIMUM OF 2".
- 4. LONGITUDINAL MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 12" ADAPT
- TRANSVERSE MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 24" APART.
- 6. REINFORCING SHALL CONSIST OF WELDED STEEL WIRE FABRIC CONFORMING TO THE REQUIREMENTS OF ASTM A 185.
- 7. PAVEMENT REINFORCING SHALL BE INCIDENTAL TO ITEM 501 PCC PAVEMENT.



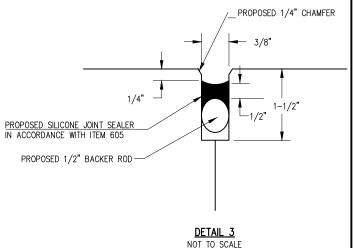
POSITION OF DOWELS AT EDGE OF JOINT TYPE C OR E

DOWEL PLAN VIEW





JOINT SEALING DETAILS



JOINTING DETAILS

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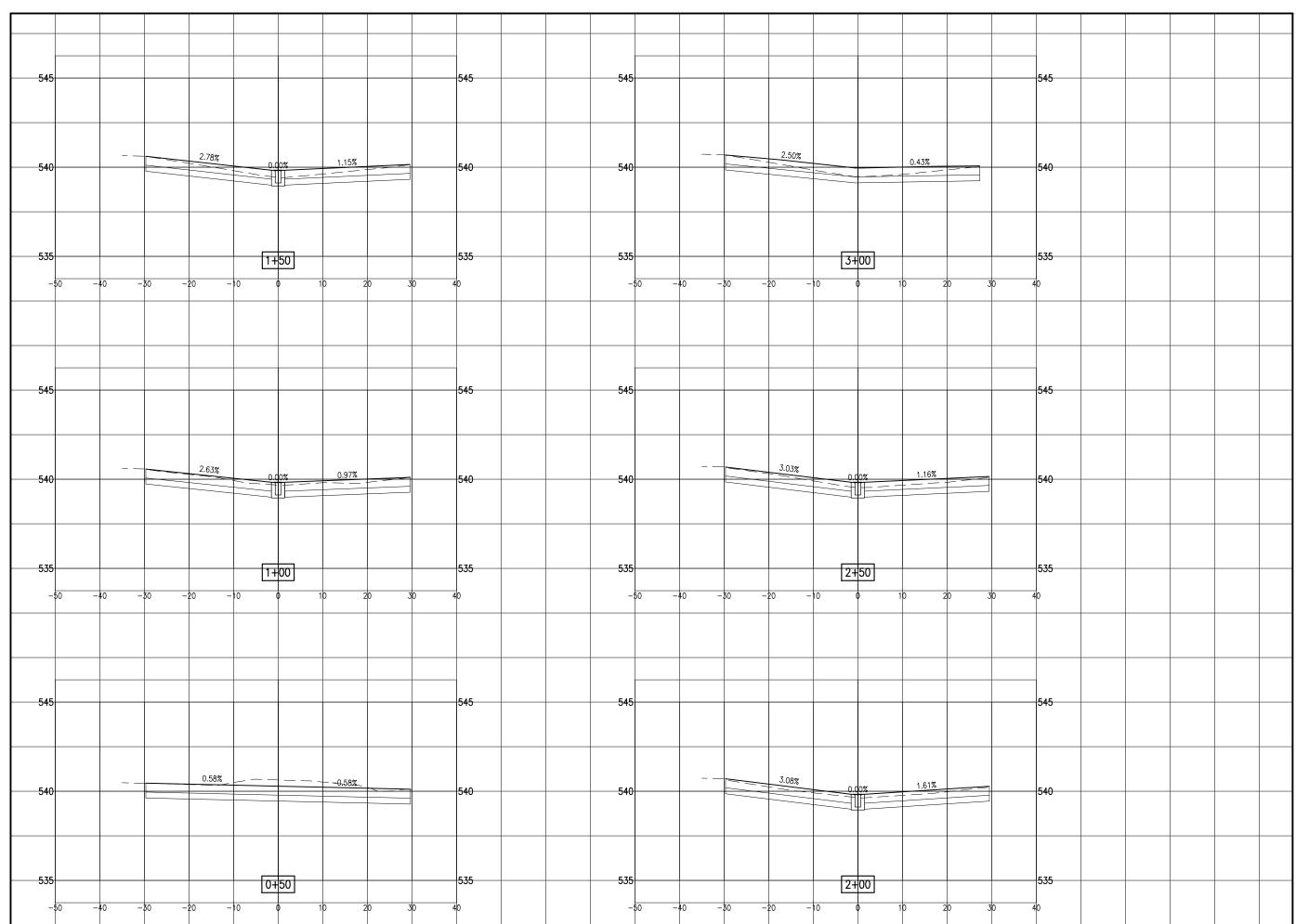
ST. LOUIS REGIONAL AIRPORT 8 TERMINAL DRIVE EAST ALTON, ILLINOIS 62024



RECONSTRUCT T-HANGAR TAXIWAY PAVEMENT WITH PCC AND CORRECT DRAINAGE ISSUES ADJACENT TO THE SOUTH END OF THE MAIN RAMP

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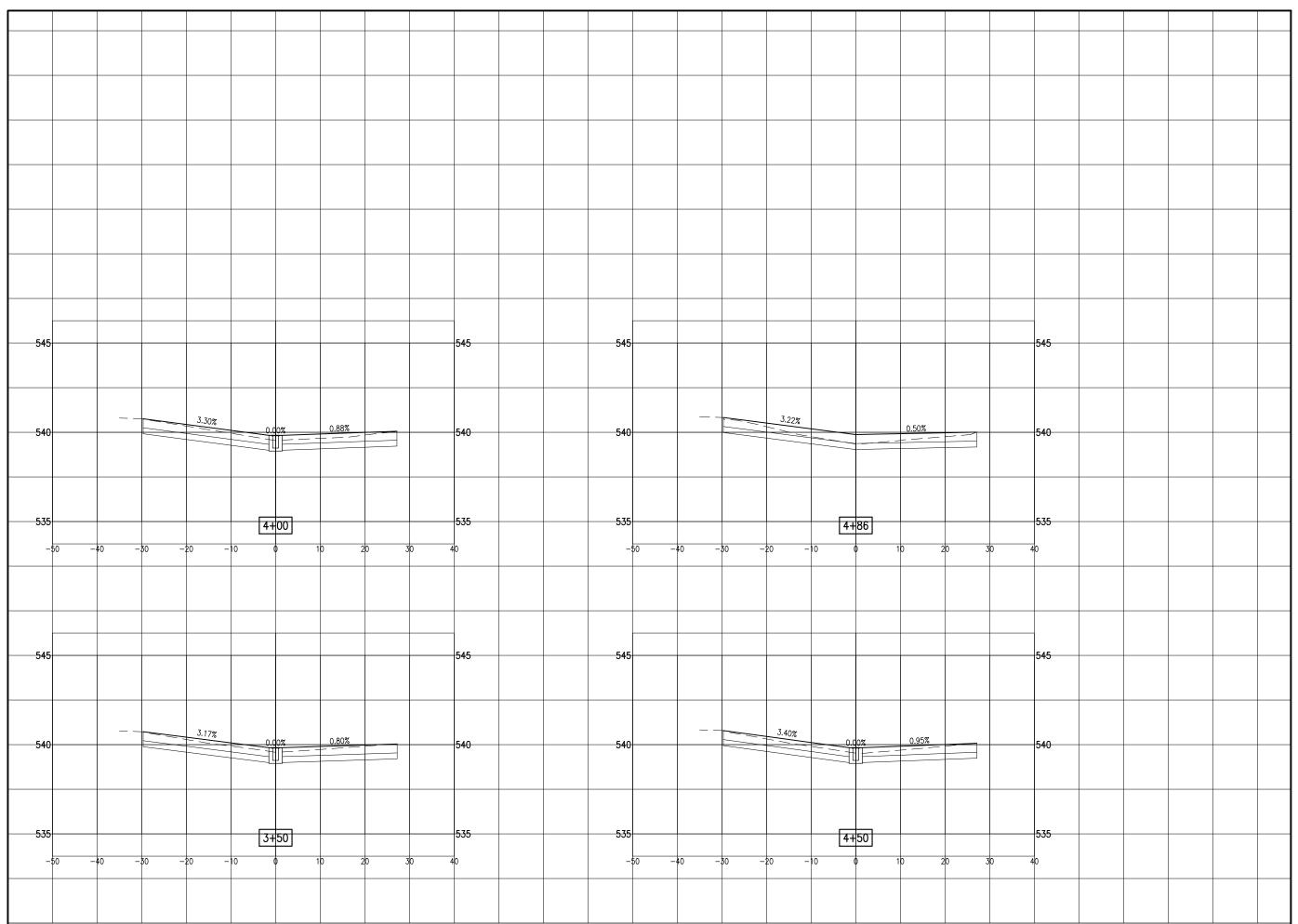
Contract No. SR091

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	ISSUE: JANUARY 12, 2018				
Ì	PROJECT NO: 17A0014				

CAD FILE: C-301-XS.DWG
DESIGN BY: JRH 12/20/2017
DRAWN BY: JRH 12/20/2017
REVIEWED BY: BSS 01/10/18

SHEET TITLE

PROPOSED CROSS SECTIONS STA. 0+50 TO STA. 3+00





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DESIGN BY: JRH 12/20				20/201	7

REVIEWED BY: BSS 01/10/18

DRAWN BY: JRH 12/20/2017

SHEET TITLE

PROPOSED CROSS SECTIONS STA. 3+50 TO STA. 4+86