

INDEX OF SHEETS

- COVER SHEET, STANDARDS, & SUMMARY OF QUANTITIES
- EXISTING & PROPOSED TYPICAL CROSS SECTIONS
- EXISTING & PROPOSED TYPICAL CROSS SECTIONS
- MISCELLANEOUS DETAILS
- INTERSECTION DETAILS
- SCHEDULE OF QUANTITIES

03-10-2023 LETTING ITEM 135

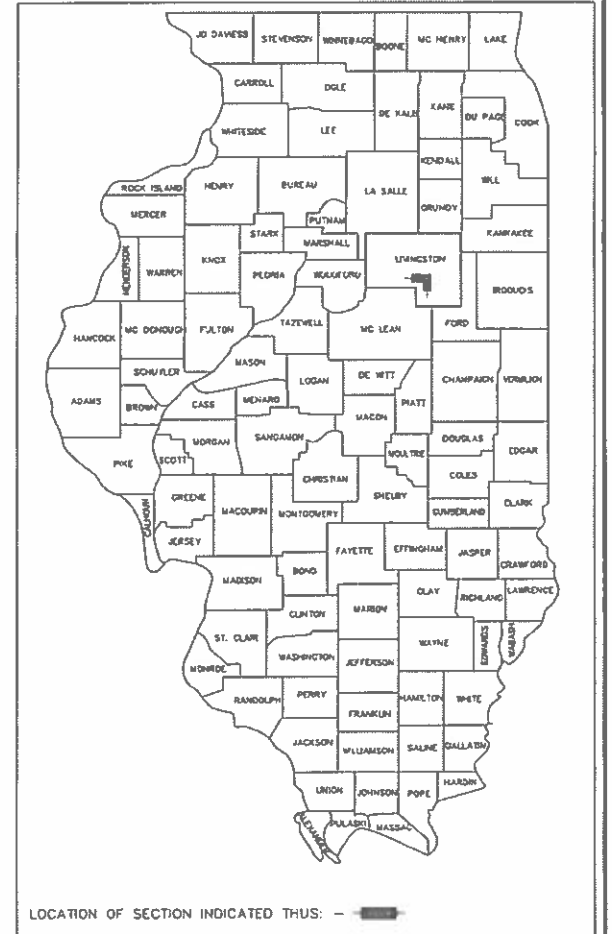
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
SURFACE TRANSPORTATION PROGRAM

| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|--------------------|--------------|-----------|
| 346 & 1362 | 20-00135-04-RS | LIVINGSTON | 6 | 1 |
| PROJECT NO. KEK8(123) | | CONTRACT NO. 87803 | | |

HIGHWAY STANDARDS

- 000001-08 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 420001-10 PAVEMENT JOINTS
- 420101-07 24' JOINTED PCC PAVEMENT
- 420701-03 PAVEMENT WELDED WIRE REINFORCEMENT
- 701301-04 LANE CLOSURE 2L, 2W SHORT TIME OPERATIONS
- 701306-04 LANE CLOSURE 2L, 2W SLOW MOVING OPERATIONS DAY ONLY ≥ 45MPH
- 701311-03 LANE CLOSURE 2L, 2W MOVING OPERATIONS DAY ONLY
- 701901-08 TRAFFIC CONTROL DEVICES
- 780001-05 TYPICAL PAVEMENT MARKINGS
- BLR 21-9 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL HIGHWAYS
- BLR 22-7 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL HIGHWAYS (TWO-LANE TWO WAY RURAL TRAFFIC) (ROAD CLOSED TO THRU TRAFFIC)

F.A.S. ROUTE 346 & 1362
CH 6, 2150E RD
CH 8, 1200N RD
SECTION 20-00135-04-RS
PROJECT KEK8(123)
LIVINGSTON COUNTY
C-93-015-23



| SUMMARY OF QUANTITIES | | | | CONST. CODE 0005 |
|-----------------------|--|-------|------------------|------------------|
| ITEM NO. | ITEM | UNIT | TOTAL QUANTITIES | |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 3,912 | |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 78,632 | |
| 40600370 | LONGITUDINAL JOINT SEALANT | FOOT | 15,429 | |
| 40602955 | HOT-MIX ASPHALT BINDER COURSE, IL-9.5FG, N50 | TON | 7,751 | |
| 40604000 | HOT-MIX ASPHALT SURFACE COURSE, IL-9.5FG, MIX 'C', N50 | TON | 8,391 | |
| 40600050 | INCIDENTAL HOT-MIX ASPHALT SURFACING | TON | 248 | |
| 42000050 | WELDED WIRE REINFORCEMENT | SQ YD | 124 | |
| 42000564 | PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED) | SQ YD | 124 | |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 49 | |
| 44000153 | HOT-MIX ASPHALT SURFACE REMOVAL, 1" | SQ YD | 9,610 | |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B | TON | 5,471 | |
| 58100200 | WATERPROOFING MEMBRANE SYSTEM | SQ YD | 97 | |
| 67100100 | MOBILIZATION | L SUM | 1 | |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 19,604 | |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | SQ FT | 1,588 | |
| 78001110 | PAINT PAVEMENT MARKING - LINE 4" | FOOT | 111,143 | |
| X4401198 | HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | SQ YD | 1,091 | |
| X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1 | |
| Z0055300 | RUMBLE STRIP | EACH | 4 | |

* SEE SPECIAL PROVISIONS
Δ SPECIALTY ITEMS

THIS PORTION OF THE PROJECT CONSISTS OF HMA SURFACE REMOVAL, HMA BINDER COURSE, IL-9.5FG, HMA SURFACE COURSE, IL-9.5FG, MIX 'C', N50 AND AGGREGATE SHOULDERS FROM THE INTERSECTION OF 1600E (CH 10) AND 1200N (CH 8) AND EXTENDS EASTERLY TO 2150E (CH 6) THEN EXTENDS SOUTHERLY TO CHERRY ST. IN FAIRBURY.

TRAFFIC DATA

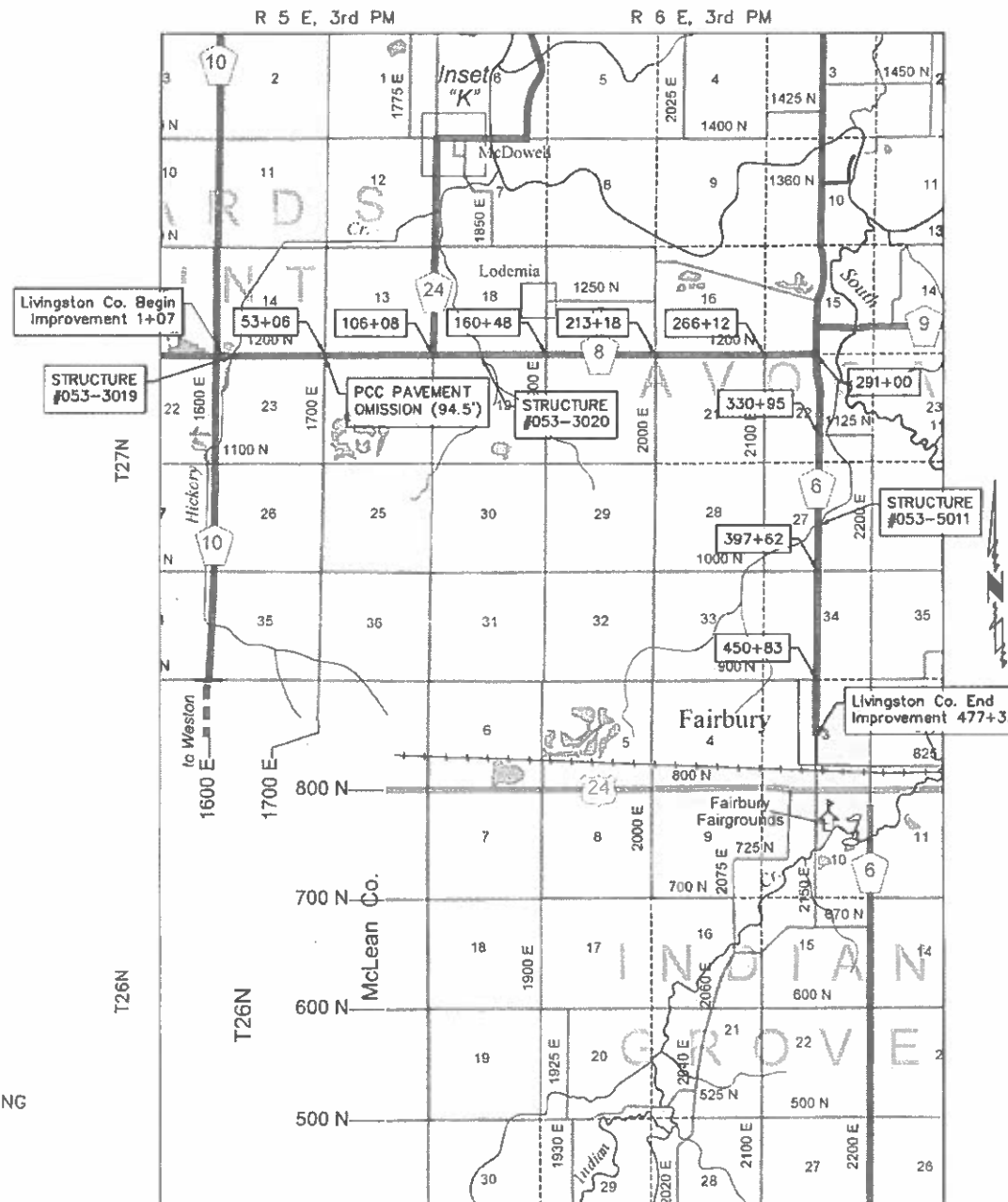
C.H. 8 - COLLECTOR
POSTED SPEED = 55 MPH-RURAL
DESIGN SPEED = 50 MPH-RURAL
DESIGN ADT= SEE SHEET 2
CLASS III ROADS, 80,000#

C.H. 6 - COLLECTOR
POSTED SPEED = 55 MPH-RURAL STA. 291+00 TO 437+13
40 MPH-URBAN STA. 437+13 TO 450+33
35 MPH-URBAN STA. 450+33 TO 457+69
25 MPH-URBAN STA. 457+69 TO 477+33

DESIGN SPEED = 50 MPH-RURAL
DESIGN ADT= SEE SHEET 3
CLASS III ROADS, 80,000#

UTILITIES

CONTACT J.U.L.I.E. 811 or
800-892-0123 BEFORE DIGGING



LIVINGSTON COUNTY GROSS LENGTH = 47,626' = 9.02 MILES
LIVINGSTON COUNTY NET LENGTH = 47,531.5' = 9.00 MILES



THESE PLANS WERE MADE BY ME OR BY A MEMBER OF MY STAFF WORKING UNDER MY PERSONAL SUPERVISION.

DATE: 12/23/2022
TRENT RUESTMAN, P.E.
NO. 062.068070
EXPIRATION DATE 11/30/23

"THE ACCEPTANCE OF THIS PROJECT IS BASED ON THE MINIMUM DESIGN CRITERIA UNDER JR GUIDELINES."

APPROVED: 12/23 20 22
Clay Metcalf
LIVINGSTON COUNTY OFFICIAL

PASSED: JANUARY 5, 20 23
District Three Local Roads & Streets Engineer

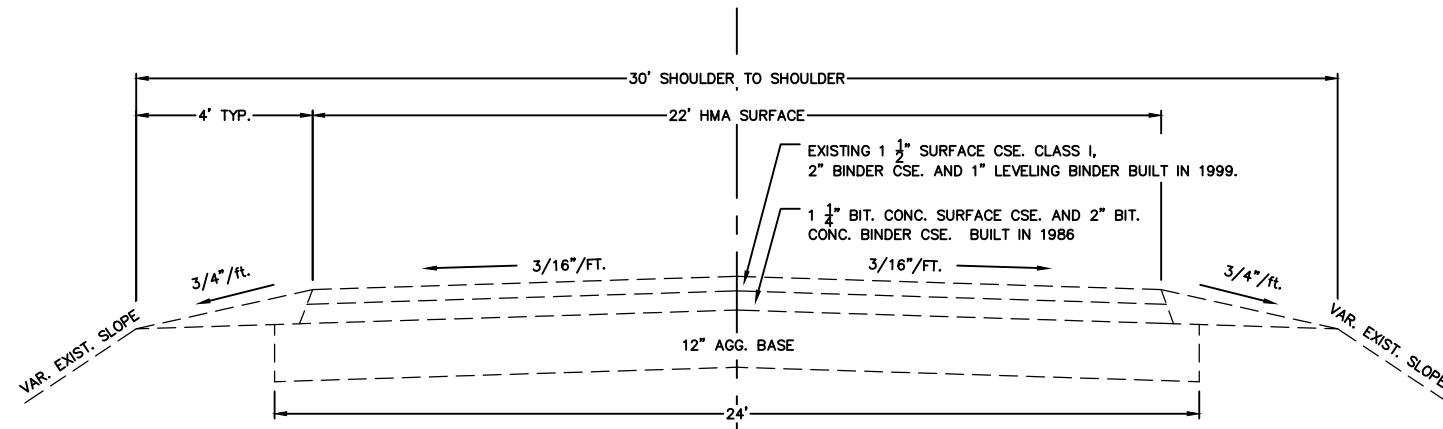
RELEASING FOR BID BASED ON LIMITED REVIEW: JANUARY 5, 20 23
Region Two Engineer

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

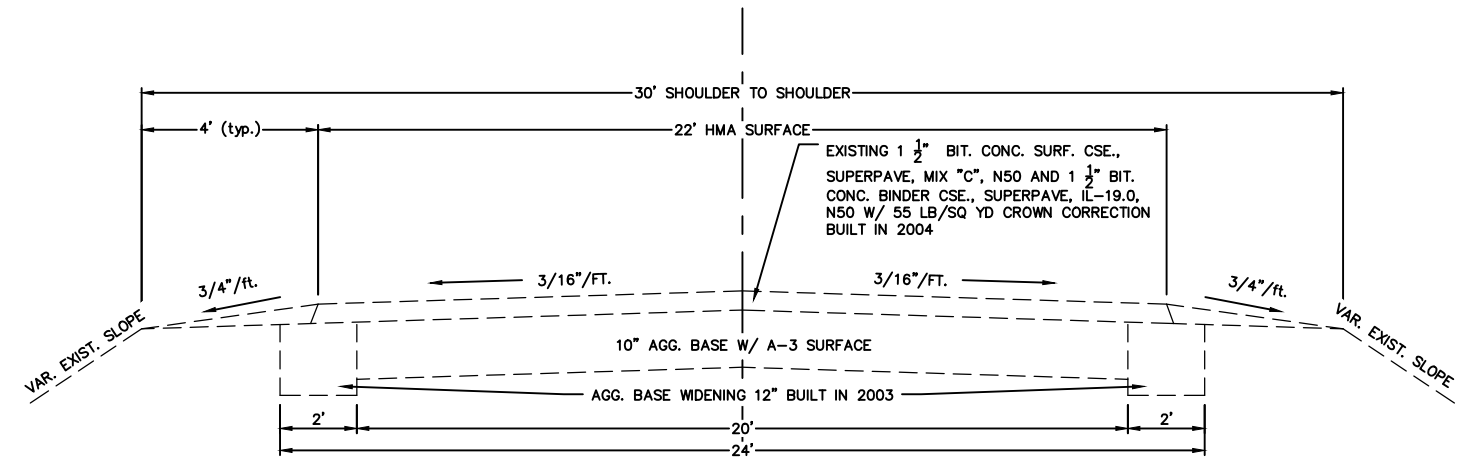
CONTRACT NO. 87803

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

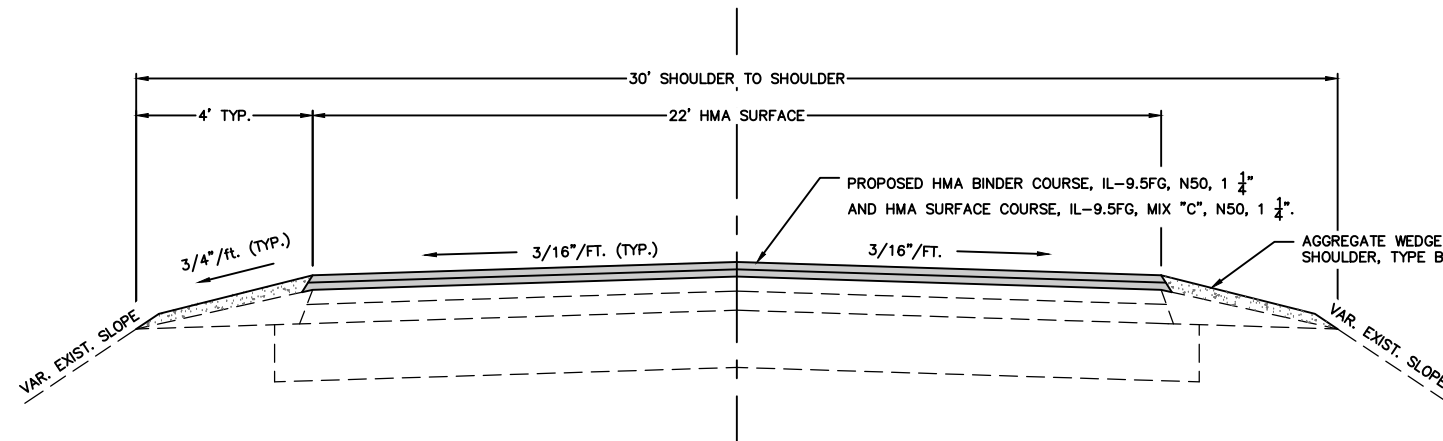
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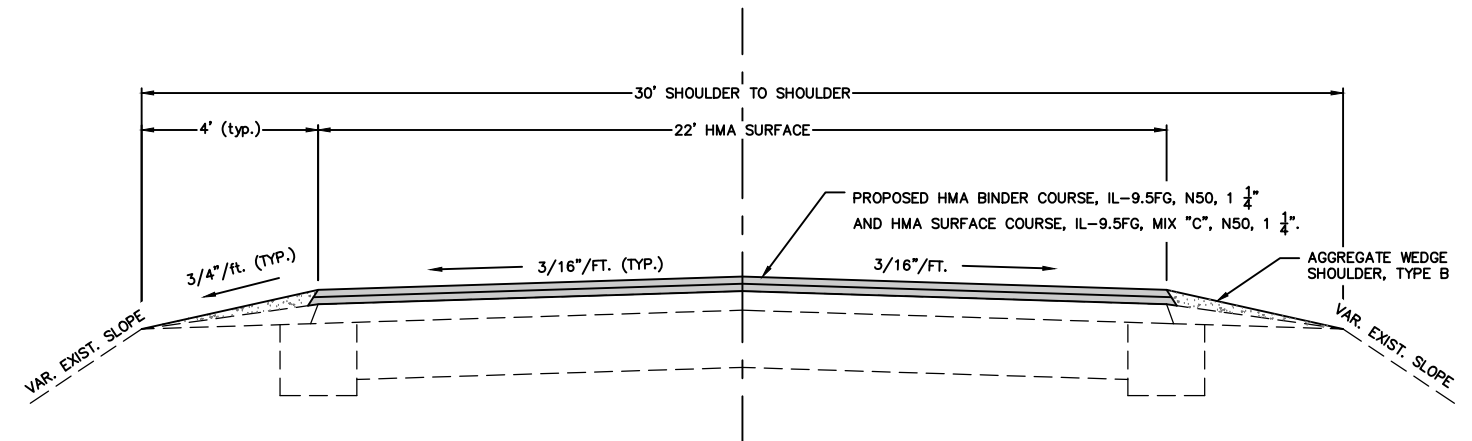
EXISTING TYPICAL CROSS SECTION (NTS)
STA. 1+07 TO STA. 106+08



EXISTING TYPICAL CROSS SECTION (NTS)
STA. 106+08 TO STA. 290+50



PROPOSED TYPICAL CROSS SECTION (NTS)
STA. 1+07 TO STA. 106+08



PROPOSED TYPICAL CROSS SECTION (NTS)
STA. 106+08 TO STA. 290+50

Sec. 20-00135-04-RS: Test Road (CH 8)
LIVINGSTON COUNTY STA. 1+07 TO STA. 106+08

| | | | |
|--|------------|----------|------|
| STRUCTURAL DESIGN DATA 80,000# | | | |
| CLASS III ROAD DESIGN PERIOD 20 YEARS | | | |
| CURRENT A.D.T. | 1050 | YEAR: | 2043 |
| STRUCTURAL DESIGN TRAFFIC: | | | |
| PERCENT OF DESIGN TRAFFIC IN DESIGN LANE: | | | |
| P.C. 81% | S.U. 11% | M.U. 8% | |
| P.C. 1012.5 | S.U. 137.5 | M.U. 100 | |
| MINIMUM SOIL SUPPORT: IBR= 3.5 | | | |
| TRAFFIC FACTOR (T.R.)= 0.536 | | | |
| STRUCTURAL NUMBER (DI)= 3.70 | | | |
| PAVEMENT STRUCTURE MATERIALS: | | | |
| SURFACE: PROPOSED HMA PAVEMENT 2.50 @ 0.40 a1= 1.00 | | | |
| BASE: EXISTING HMA SURFACE 1999 4.50 @ 0.30 a2= 1.35 | | | |
| BASE: EXISTING HMA SURFACE 1986 3.25 @ 0.23 a3= 0.75 | | | |
| SUBBASE: EXISTING AGGREGATE BASE COURSE 12 @ 0.08 a4= 0.86 | | | |
| PROPOSED DI TOTAL = 4.06 | | | |

NOTE: EXISTING HMA PAVEMENT THICKNESS & AGG. BASE THICKNESS BASED ON EXISTING PLANS

| | |
|----------------------|-----------|
| Curve #1 Data | |
| P.I. Sta. | 103+19.91 |
| Δ | 3°40'00" |
| R(existing) | 7813' |
| T | 250.08' |
| L | 500.00' |
| E | 4.00' |
| e | 2.50% |
| D | 0°44'00" |
| T.R. | 33' |
| S.E. Run | 55' |
| P.C. Sta. | 100+69.83 |
| P.T. Sta. | 105+69.83 |

| | |
|----------------------|-----------|
| Curve #2 Data | |
| P.I. Sta. | 109+19.91 |
| Δ | 3°40'00" |
| R(existing) | 7813' |
| T | 250.08' |
| L | 500.00' |
| E | 4.00' |
| e | 2.50% |
| D | 0°44'00" |
| T.R. | 33' |
| S.E. Run | 55' |
| P.C. Sta. | 106+69.83 |
| P.T. Sta. | 111+69.83 |

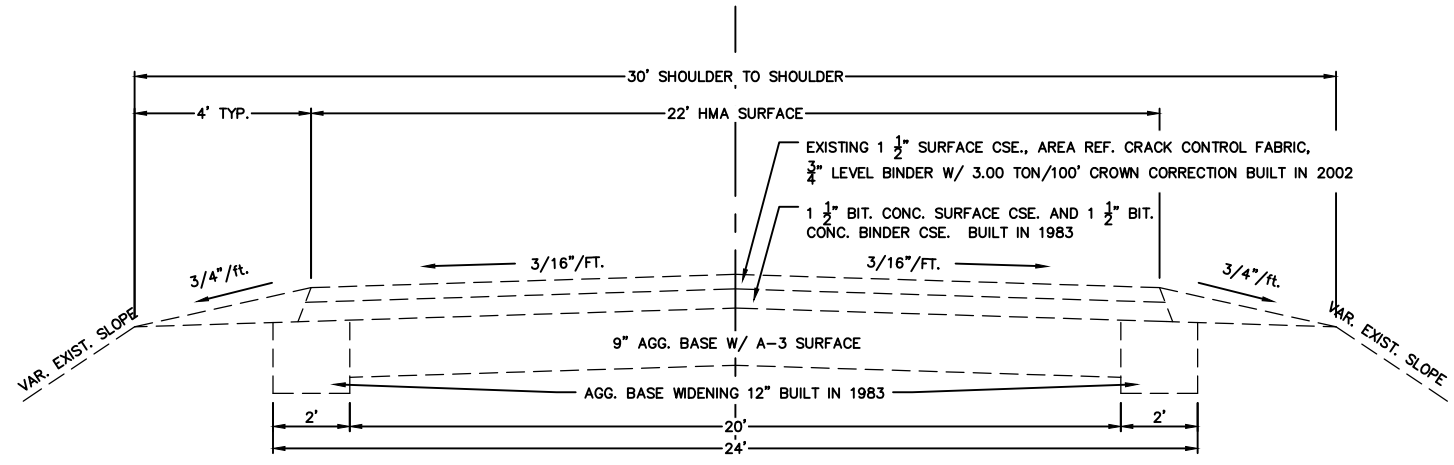
Sec. 20-00135-04-RS: Test Road (CH 8)
LIVINGSTON COUNTY STA. 106+08 TO STA. 290+50

| | | | |
|--|-----------|-----------|------|
| STRUCTURAL DESIGN DATA 80,000# | | | |
| CLASS III ROAD DESIGN PERIOD 20 YEARS | | | |
| CURRENT A.D.T. | 950 | YEAR: | 2043 |
| STRUCTURAL DESIGN TRAFFIC: | | | |
| PERCENT OF DESIGN TRAFFIC IN DESIGN LANE: | | | |
| P.C. 88% | S.U. 7% | M.U. 5% | |
| P.C. 1012 | S.U. 80.5 | M.U. 57.5 | |
| MINIMUM SOIL SUPPORT: IBR= 3.5 | | | |
| TRAFFIC FACTOR (T.R.)= 0.310 | | | |
| STRUCTURAL NUMBER (DI)= 3.35 | | | |
| PAVEMENT STRUCTURE MATERIALS: | | | |
| SURFACE: PROPOSED HMA PAVEMENT 2.50 @ 0.40 a1= 1.00 | | | |
| BASE: EXISTING HMA SURFACE 2004 3.00 @ 0.30 a2= 0.90 | | | |
| BASE: EXISTING BITUMINOUS A-3 3 @ 0.11 a3= 0.33 | | | |
| SUBBASE: EXISTING AGGREGATE BASE COURSE 10 @ 0.08 a4= 0.80 | | | |
| PROPOSED DI TOTAL = 3.03 | | | |

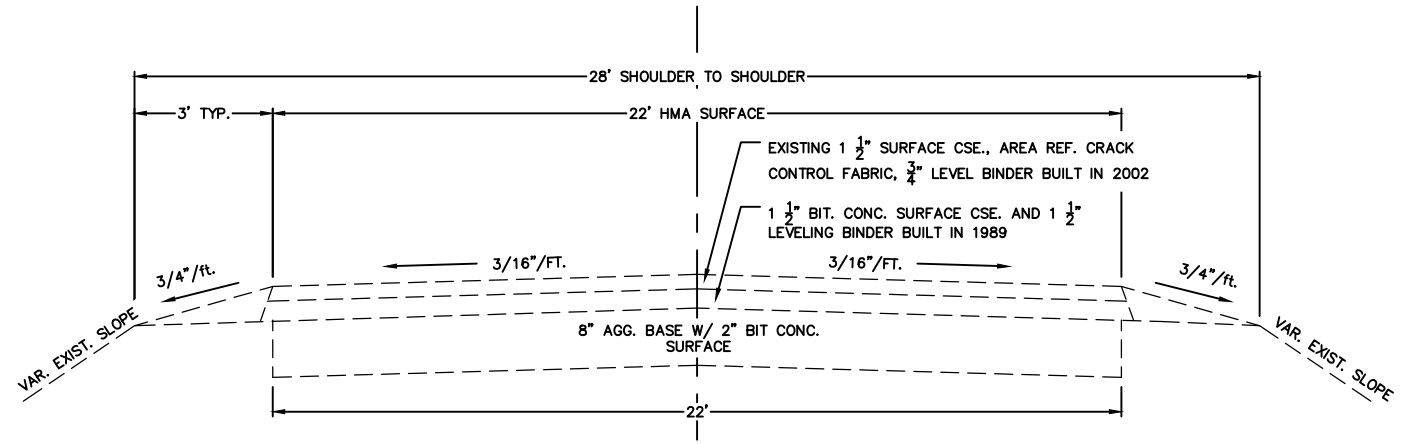
EXISTING PAVEMENT CONDITION DOES NOT WARRANT ADDITIONAL THICKNESS THAN PROPOSED.
NOTE: EXISTING HMA PAVEMENT THICKNESS & AGG. BASE THICKNESS BASED ON EXISTING PLANS

| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|--------------------|--------------|-----------|
| 346 & 1362 | 20-00135-04-RS | LIVINGSTON | 6 | 2 |
| PROJECT NO. KEK8(123) | | CONTRACT NO. 87803 | | |

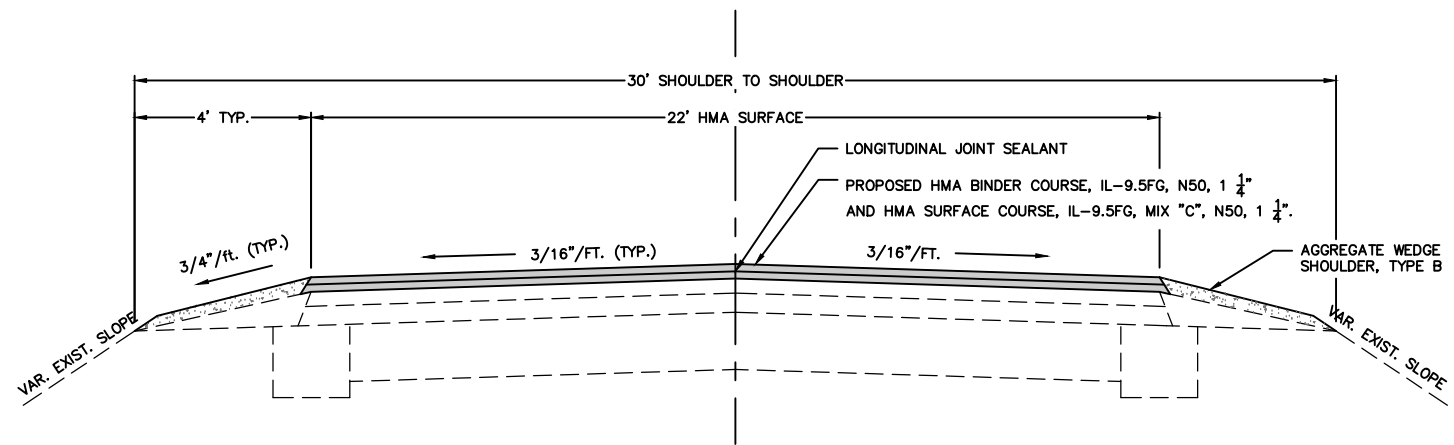
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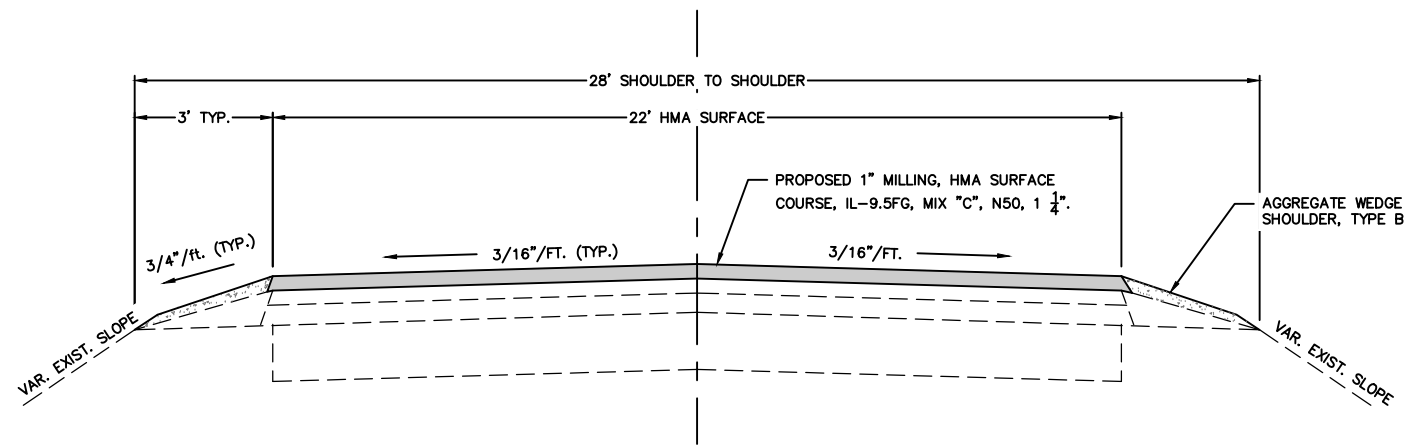
EXISTING TYPICAL CROSS SECTION (NTS)
STA. 291+00 TO STA. 450+83



EXISTING TYPICAL CROSS SECTION (NTS)
STA. 450+83 TO STA. 477+33



PROPOSED TYPICAL CROSS SECTION (NTS)
STA. 291+00 TO STA. 450+83



PROPOSED TYPICAL CROSS SECTION (NTS)
STA. 450+83 TO STA. 477+33
NOTE: SEE END OF IMPROVEMENT DETAIL ON SHEET 4 FOR 1" MILLING TRANSITION

Sec. 20-00135-04-RS: Fairbury North (CH 9)
LIVINGSTON COUNTY STA. 291+00 TO STA. 450+83

| | | | |
|---|-------------|----------|------|
| STRUCTURAL DESIGN DATA 80,000# | | | |
| CLASS III ROAD DESIGN PERIOD 15 YEARS | | | |
| CURRENT A.D.T. | 1750 | YEAR: | 2038 |
| STRUCTURAL DESIGN TRAFFIC: | | | |
| PERCENT OF DESIGN TRAFFIC IN DESIGN LANE: | | | |
| P.C. 95% | S.U. 3% | M.U. 2% | |
| P.C. 1900 | S.U. 60 | M.U. 40 | |
| MINIMUM SOIL SUPPORT: IBR= 3.5 | | | |
| TRAFFIC FACTOR (T.R.)= 0.166 | | | |
| STRUCTURAL NUMBER (D _i)= 3.09 | | | |
| PAVEMENT STRUCTURE MATERIALS: | | | |
| SURFACE: PROPOSED HMA PAVEMENT | 2.50 @ 0.40 | a1= 1.00 | |
| BASE: EXISTING HMA SURFACE 2002 | 2.25 @ 0.30 | a2= 0.68 | |
| BASE: EXISTING HMA SURFACE 1983 | 3.00 @ 0.23 | a2= 0.69 | |
| BASE: EXISTING BITUMINOUS A-3 | 1 @ 0.11 | a3= 0.11 | |
| SUBBASE: EXISTING AGGREGATE BASE COURSE | 9 @ 0.08 | a4= 0.72 | |
| PROPOSED D _i TOTAL = 3.20 | | | |
| NOTE: EXISTING HMA PAVEMENT THICKNESS & AGG. BASE THICKNESS BASED ON EXISTING PLANS | | | |

| | |
|----------------------|-----------|
| Curve #3 Data | |
| P.I. Sta. | 299+87.77 |
| Δ | 16°30'00" |
| R(existing) | 1724' |
| T | 250' |
| L | 496.55' |
| E | 18.03' |
| e | 5.75% |
| D | 3'19"23" |
| T.R. | 33' |
| S.E. Run | 126.50' |
| P.C. Sta. | 297+41.22 |
| P.T. Sta. | 302+37.77 |

| | |
|----------------------|-----------|
| Curve #4 Data | |
| P.I. Sta. | 306+84.28 |
| Δ | 16°00'00" |
| R(existing) | 1778' |
| T | 250' |
| L | 496.51' |
| E | 17.48' |
| e | 5.50% |
| D | 03'13"21" |
| T.R. | 33' |
| S.E. Run | 121' |
| P.C. Sta. | 304+37.77 |
| P.T. Sta. | 309+34.28 |

| | |
|----------------------|-----------|
| Curve #5 Data | |
| P.I. Sta. | 449+83 |
| Δ | 06°48'40" |
| R(existing) | 2521' |
| T | 150' |
| L | 299.65' |
| E | 4.46' |
| e | 3.00% |
| D | 02'16"23" |
| T.R. | 33' |
| S.E. Run | 66' |
| P.C. Sta. | 448+33.35 |
| P.T. Sta. | 451+33.00 |

Sec. 20-00135-04-RS: Fairbury North (CH 6)
LIVINGSTON COUNTY STA. 450+83 TO STA. 477+33

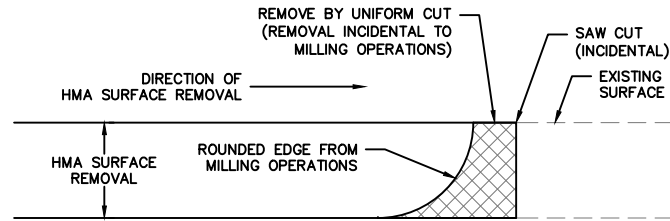
| | | | |
|---|-------------|----------|------|
| STRUCTURAL DESIGN DATA 80,000# | | | |
| CLASS III ROAD DESIGN PERIOD 15 YEARS | | | |
| CURRENT A.D.T. | 1900 | YEAR: | 2038 |
| STRUCTURAL DESIGN TRAFFIC: | | | |
| PERCENT OF DESIGN TRAFFIC IN DESIGN LANE: | | | |
| P.C. 97% | S.U. 2% | M.U. 1% | |
| P.C. 2134 | S.U. 44 | M.U. 22 | |
| MINIMUM SOIL SUPPORT: IBR= 3.5 | | | |
| TRAFFIC FACTOR (T.R.)= 0.102 | | | |
| STRUCTURAL NUMBER (D _i)= 2.83 | | | |
| PAVEMENT STRUCTURE MATERIALS: | | | |
| SURFACE: PROPOSED HMA PAVEMENT | 1.25 @ 0.40 | a1= 0.50 | |
| BASE: EXISTING HMA SURFACE 2002 | 1.25 @ 0.30 | a2= 0.38 | |
| BASE: EXISTING HMA SURFACE 1989 | 3.00 @ 0.23 | a2= 0.69 | |
| BASE: EXISTING HMA SURFACE | 2 @ 0.23 | a3= 0.46 | |
| SUBBASE: EXISTING AGGREGATE BASE COURSE | 8 @ 0.08 | a4= 0.64 | |
| PROPOSED D _i TOTAL = 2.67 | | | |
| EXISTING PROFILE CANNOT BE INCREASED DUE TO EXISTING ENTRANCES AND UTILITIES | | | |
| NOTE: EXISTING HMA PAVEMENT THICKNESS & AGG. BASE THICKNESS BASED ON EXISTING PLANS | | | |

| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|--------------------|--------------|-----------|
| 346 & 1362 | 20-00135-04-RS | LIVINGSTON | 6 | 3 |
| PROJECT NO. KEK8(123) | | CONTRACT NO. 87803 | | |

NOT PLOTTED TO SCALE

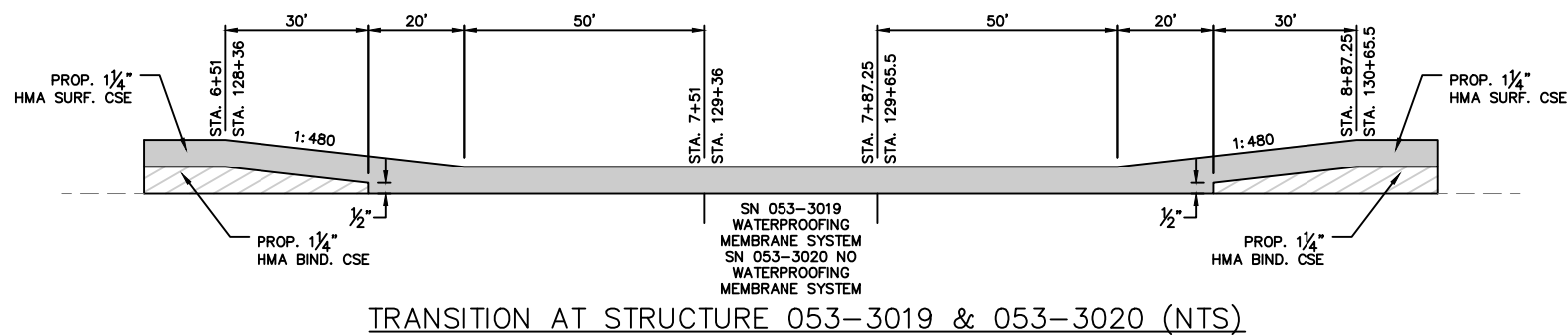
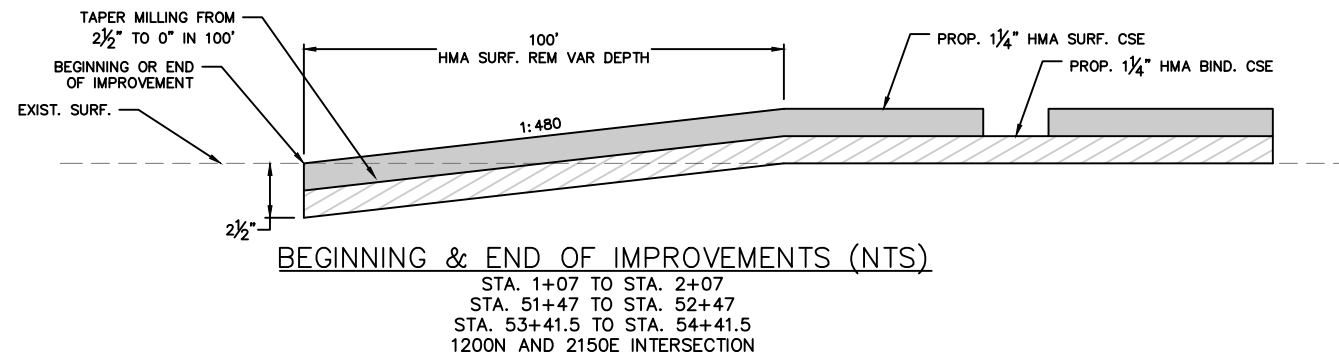
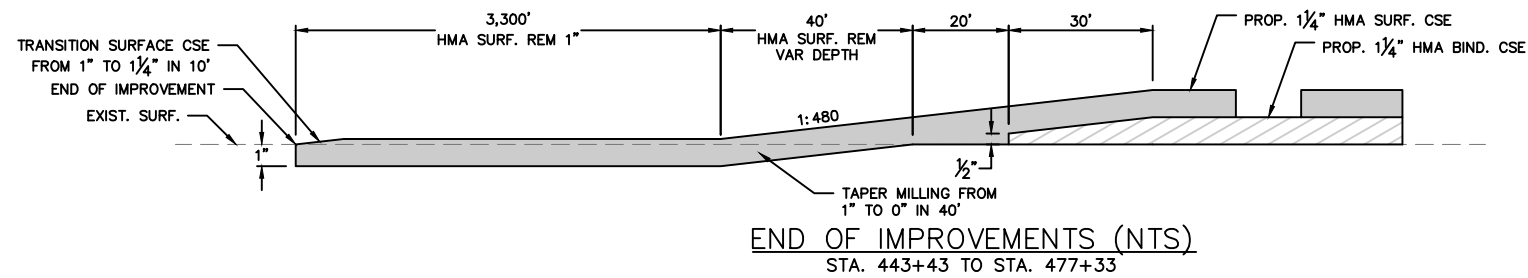
GENERAL NOTES:

1. ALL AREAS WHERE A 4' WING OUT AT PRIVATE ENTRANCES AND SIDE ROADS SHALL BE CONSTRUCTED WITH THE EXTENDABLE SCREED WIDENER AND PAID FROM HOT-MIX ASPHALT BINDER AND HOT-MIX ASPHALT SURFACE COURSE PAY ITEMS.
2. ALL PRIVATE ENTRANCES, COMMERCIAL ENTRANCES, SIDE ROADS, AND SOME RURAL MAILBOX TURNOUTS ARE TO BE CONSTRUCTED AND PAID FROM INCIDENTAL HOT-MIX ASPHALT SURFACING AND SHALL BE LAID OUT BY THE ENGINEER.
3. AGGREGATE SHOULDERS SHALL BE PLACED ALONG ALL SIDEROAD RETURN RADII AND SHALL BE PLACED ALONG AND BEHIND ALL FIELD ENTRANCES, MAILBOX TURNOUTS, AND ALL PRIVATE AND COMMERCIAL ENTRANCES UNLESS SPECIFIED BY THE ENGINEER.



NOTE: WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAW CUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE USE OF THIS DETAIL.

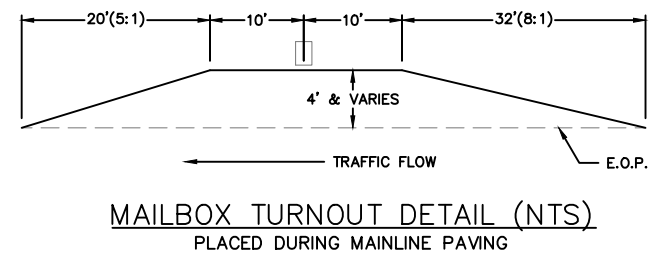
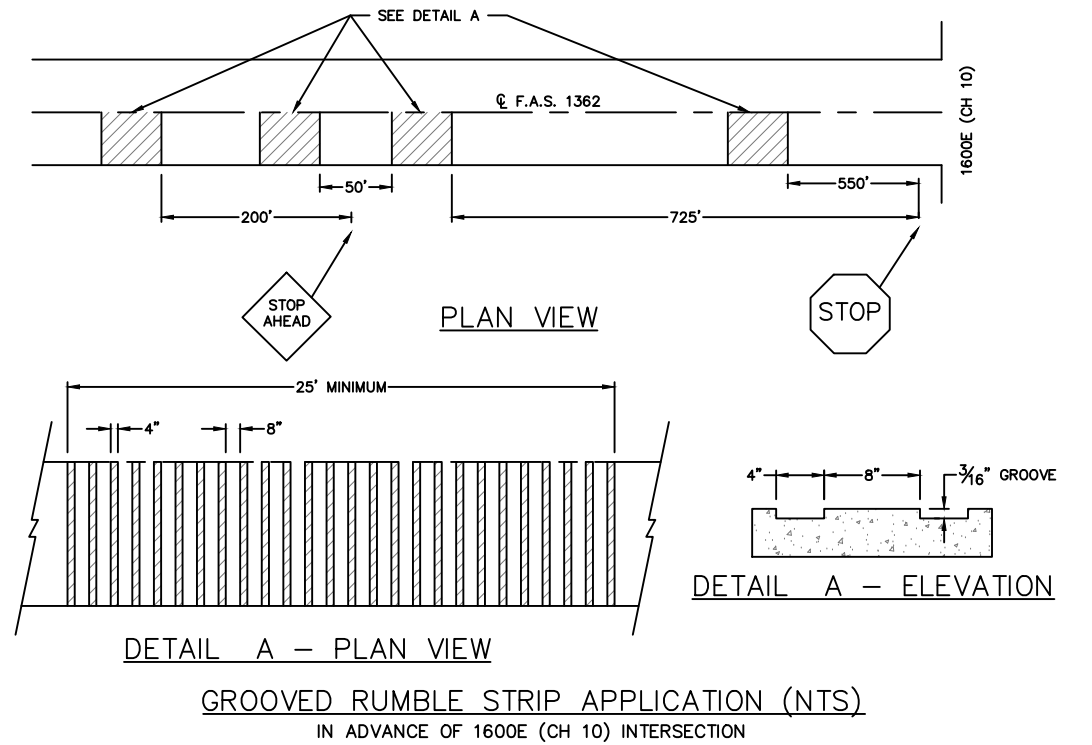
HMA DETAIL AT BUTT JOINTS



| LOCATION | SCHEDULE OF PAINT PAVEMENT MARK 4" | | | | |
|--------------------|------------------------------------|-----------|----------|---------|---------|
| | YELLOW | | | WHITE | |
| | NO PASSING ZONE | SKIP DASH | EDGELINE | | |
| STATION TO STATION | NBL | SBL | Q | NBL/WBL | SBL/EBL |
| 0+ to 290+85 | | | 7,270 | 29,085 | 29,085 |
| 289+44 to 299+81 | | 1,037 | 260 | 1,037 | 1,037 |
| 299+81 to 306+65 | 684 | 684 | * | 684 | 684 |
| 306+65 to 318+55 | 1,190 | | 300 | 1,190 | 1,190 |
| 318+55 to 477+33 | | | 3,970 | 15,878 | 15,878 |
| TOTALS | 1,874 | 1,721 | 11,800 | 47,874 | 47,874 |
| TOTAL PAY QUANTITY | 111,143 | | FOOT | | |

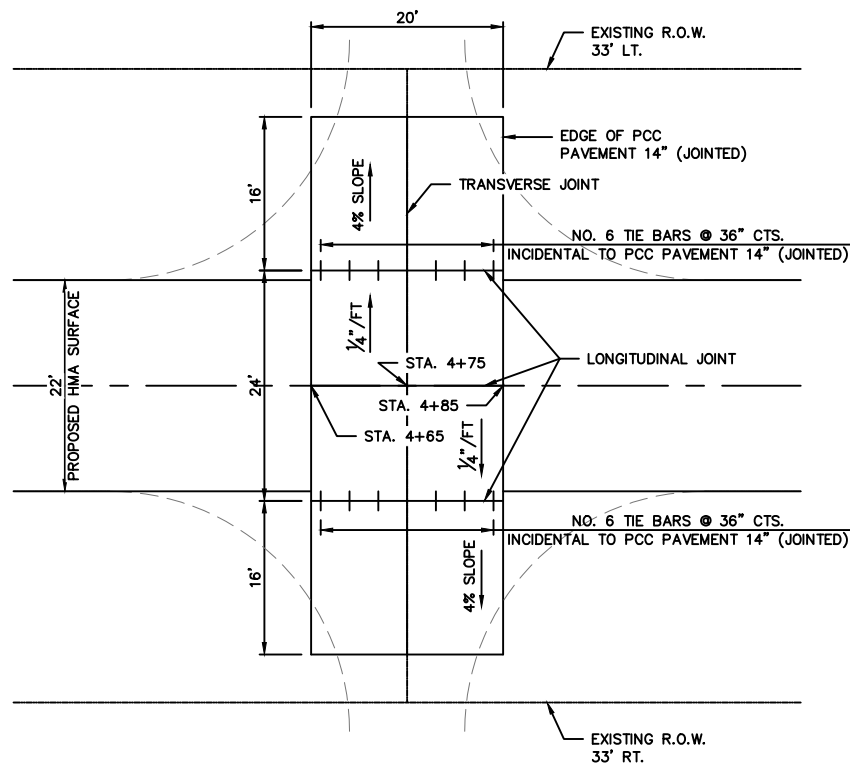
BITUMINOUS MIX TABLE

| Mixture Use(s) | HMA Binder | HMA Surface | HMA Incidental |
|----------------------------|---------------------|---------------------|--------------------------|
| Location(s) | ENTIRE PROJECT | ENTIRE PROJECT | ENTIRE PROJECT |
| PG Grade | PG64-22 | PG64-22 | PG64-22 |
| Design Air Voids | 4.0% @ N50 | 4.0% @ N50 | 4.0% @ N50 |
| Mixture Composition | IL 9.5FG | IL 9.5FG | IL 9.5FG |
| Friction Aggregate | N/A | Mixture C | Mixture C |
| Density Test Method | LR 1030-2 | LR 1030-2 | Satisfaction of Engineer |
| Mixture Weight | 112 # / s.y. / inch | 112 # / s.y. / inch | 112 # / s.y. / inch |
| Quality Management Program | QC/QA | QC/QA | QC/QA |
| Sublot Size | N/A | N/A | N/A |
| Material Transfer Device | NO | NO | NO |

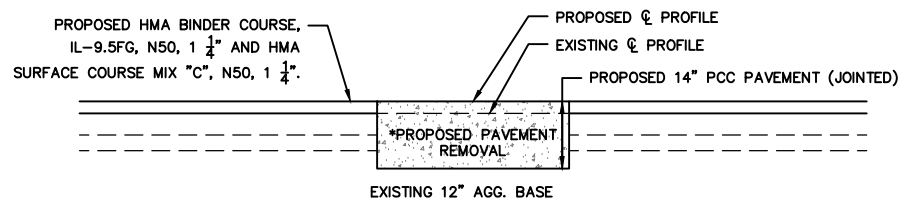


| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|--------------------|--------------|-----------|
| 346 & 1362 | 20-00135-04-RS | LIVINGSTON | 6 | 4 |
| PROJECT NO. KEK8(123) | | CONTRACT NO. 87803 | | |

NOT PLOTTED TO SCALE



PLAN VIEW (NTS)
SEE STANDARDS 420001-10, 420101-07 & 420701-03 FOR REINF. NOT SHOWN.



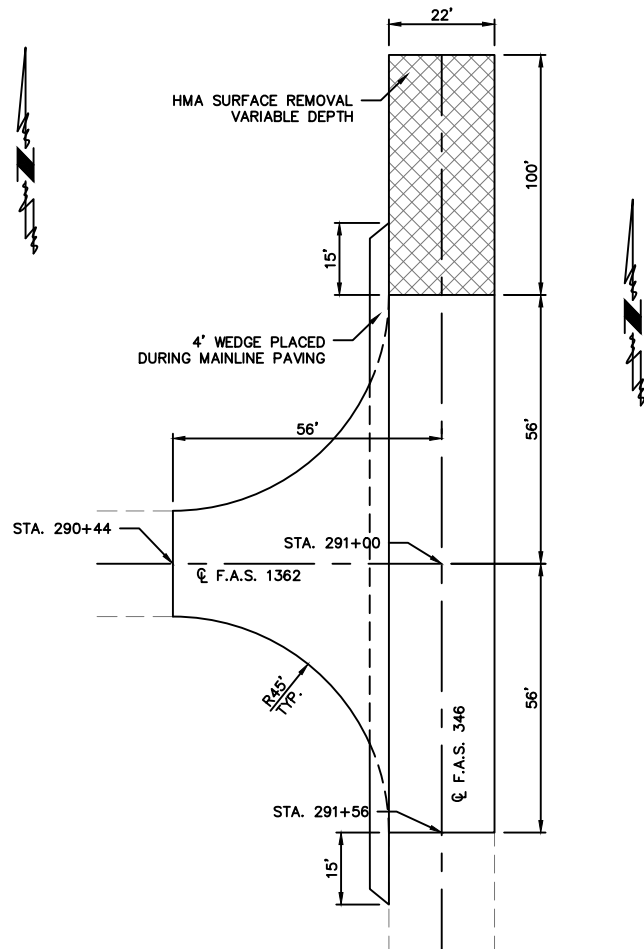
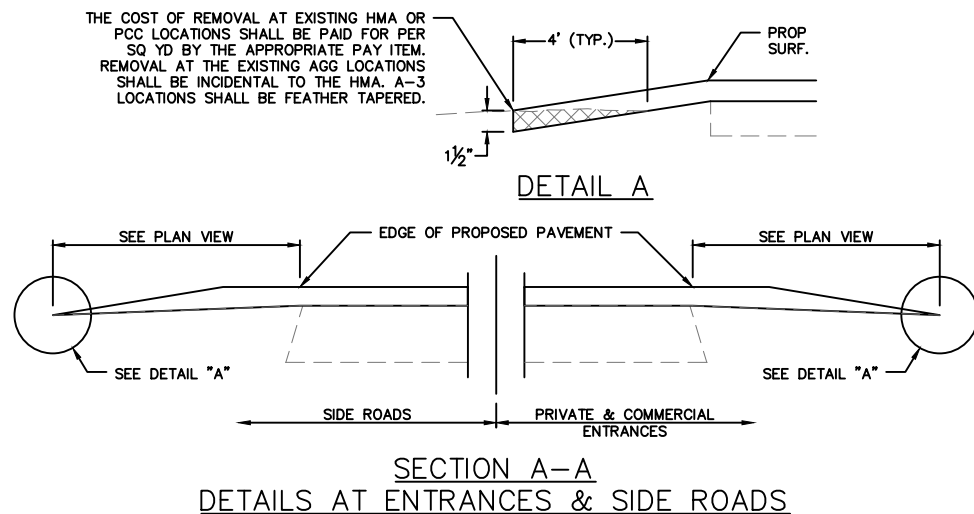
ELEVATION VIEW (NTS)

*PAVEMENT REMOVAL WILL BE PAID FOR BY THE SQ YD OF HMA BEING REMOVED. AGGREGATE SHOULDERS AND ENTRANCE REMOVAL WILL BE CONSIDERED INCIDENTAL TO THE PAVEMENT REMOVAL

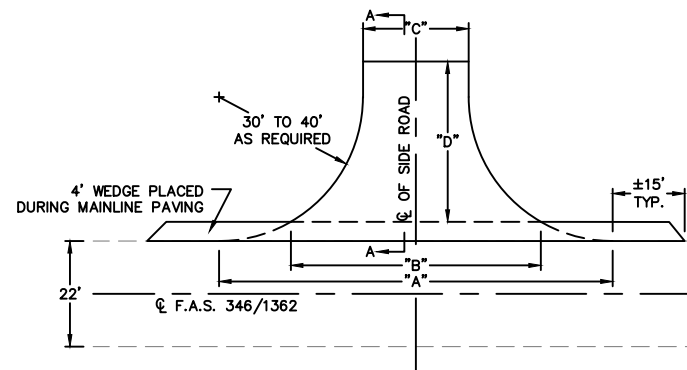
COMMERCIAL CROSSING DETAIL

| DESCRIPTION | QUANTITY |
|----------------------------|-----------|
| STA. 4+65 TO STA. 4+85 | |
| PAVEMENT REMOVAL | 49 SQ YD |
| PCC PAVEMENT 14" (JOINTED) | 124 SQ YD |
| WELDED WIRE REINFORCEMENT | 124 SQ YD |

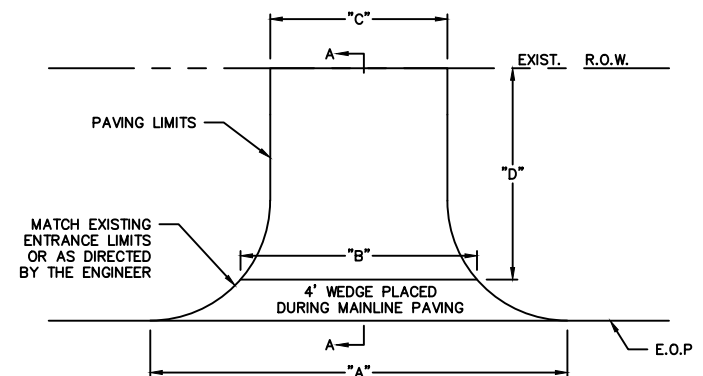
THE COST OF REMOVAL AT EXISTING HMA OR PCC LOCATIONS SHALL BE PAID FOR PER SQ YD BY THE APPROPRIATE PAY ITEM. REMOVAL AT THE EXISTING AGG LOCATIONS SHALL BE INCIDENTAL TO THE HMA. A-3 LOCATIONS SHALL BE FEATHER TAPERED.



1200N & 2150E INTERSECTION (NTS)



TYPICAL SIDE ROAD INTERSECTION (NTS)
SEE TABLE FOR LOCATION AND QUANTITIES



P.E. / C.E. DETAILS (NTS)
SEE TABLE FOR LOCATION AND QUANTITIES

| LOCATION | A(Ft.) | B(Ft.) | C(Ft.) | D(Ft.) | HMA SURF REM 1" (S.Y.) | BIT. MAT. (TACK COAT) (LB) | BINDER COURSE | SURFACE COURSE | INCID. BIT. |
|-----------------|--------|--------|--------|--------|------------------------|----------------------------|---------------|----------------|-------------|
| 1800E (R) | 74 | 64 | 23 | 32 | - | 100 | 3 | 3 | 22 |
| 1800E (L) | 78 | 61 | 21 | 34 | - | 101 | 3 | 3 | 22 |
| 1900E (R) | 62 | 49 | 17 | 21 | - | 61 | 3 | 3 | 11 |
| 1900E (L) | 72 | 61 | 20 | 26 | - | 82 | 3 | 3 | 16 |
| 2000E (R) | 66 | 55 | 20 | 25 | - | 74 | 3 | 3 | 15 |
| 2000E (L) | 51 | 41 | 18 | 23 | - | 57 | 2 | 2 | 11 |
| 2100E (R) | 55 | 45 | 18 | 24 | - | 62 | 3 | 3 | 12 |
| 1125N (L) | 59 | 50 | 21 | 15 | - | 52 | 3 | 3 | 8 |
| 1000N (R) | 69 | 55 | 23 | 25 | - | 77 | 3 | 3 | 15 |
| 1000N (L) | 71 | 59 | 23 | 27 | - | 84 | 3 | 3 | 17 |
| 900N (R) | 340 | 340 | - | - | 151 | 68 | - | 11 | - |
| CARTER DR (R) | 57 | 57 | - | - | 25 | 11 | - | 2 | - |
| COLUMBIA DR (L) | 138 | 138 | - | - | 61 | 28 | - | 4 | - |
| WILLOW ST (L) | 63 | 63 | - | - | 28 | 13 | - | 2 | - |
| CHERRY ST (L) | 58 | 58 | - | - | 26 | 12 | - | 2 | - |
| TOTALS | | | | | 291 | 882 | 29 | 50 | 149 |

| TYPE | STATION | A(Ft.) | B(Ft.) | C(Ft.) | D(Ft.) | HMA SURF REM 1" (S.Y.) | BIT. MAT. (TACK COAT) (LB) | BIT. MAT. (PRIME COAT) (LB) | BINDER COURSE | SURFACE COURSE | INCID. BIT. |
|---------------------------|------------|--------|--------|--------|--------|------------------------|----------------------------|-----------------------------|---------------|----------------|-------------|
| PE | 77+25(R) | 54 | 39 | 15 | 18 | - | 12 | 168 | 1 | 1 | 8 |
| PE | 190+25(R) | 58 | 45 | 15 | 18 | - | 14 | 187 | 2 | 2 | 9 |
| CE | 212+54(L) | 35 | 30 | 22 | 18 | - | 12 | 150 | 1 | 1 | 8 |
| PE | 226+22(L)* | 35 | 28 | 12 | 18 | - | 6 | 118 | 2 | 2 | 4 |
| PE | 229+00(R)* | 55 | 37 | 14 | 18 | - | 8 | 148 | 2 | 2 | 5 |
| MBTO (L)** | - | 98 | 20 | 6 | - | - | 9 | - | - | - | 6 |
| PE | 296+00(L) | 65 | 53 | 25 | 18 | - | 18 | 244 | 2 | 2 | 11 |
| PE | 307+64(L)* | 61 | 54 | 14 | 18 | - | 21 | 212 | - | - | 14 |
| PE | 404+00(R)* | 75 | 69 | 31 | 18 | - | 15 | 233 | 2 | 2 | 10 |
| PE | 439+00(L) | 60 | 50 | 22 | 18 | - | 16 | 217 | 2 | 2 | 11 |
| PE | 440+06(L) | 29 | 22 | 15 | 18 | - | 8 | 109 | 1 | 1 | 6 |
| PE | 442+36(L) | 38 | 29 | 15 | 18 | - | 10 | 133 | 1 | 1 | 7 |
| PE | 444+50(L) | 68 | 68 | - | - | 30 | 14 | - | - | - | - |
| PE | 444+92(L) | 189 | 137 | - | - | 72 | 33 | - | - | - | 5 |
| PE | 445+55(L) | 27 | 27 | - | - | 12 | 5 | - | - | - | 1 |
| PE | 448+00(L) | 56 | 56 | - | - | 25 | 11 | - | - | - | 2 |
| PE | 449+27(R) | 37 | 37 | - | - | 16 | 7 | - | - | - | 1 |
| PE | 450+62(L) | - | - | - | - | - | - | - | - | - | 0 |
| Cherry to willow St(R) | 218 | 218 | - | - | 97 | 44 | - | - | - | - | 7 |
| Cherry to willow St(L) | 55 | 55 | - | - | 24 | 11 | - | - | - | - | 2 |
| willow St to Carter Dr(R) | 232 | 232 | - | - | 103 | 46 | - | - | - | - | 7 |
| willow St to Carter Dr(L) | 208 | 208 | - | - | 92 | 42 | - | - | - | - | 6 |
| Columbia Dr to 900N (R) | 831 | 831 | - | - | 369 | 166 | - | - | - | - | 26 |
| Columbia Dr to 900N (L) | 652 | 652 | - | - | 290 | 130 | - | - | - | - | 20 |
| MBTO | 2 Total | 72 | 20 | - | - | - | 56 | - | 3 | 3 | - |
| TOTALS | | | | | | 1,130 | 714 | 1,919 | 19 | 96 | 99 |

* DIMENSIONS ARE TO THE CL OF MAILBOX. QUANTITIES INCLUDE THE MBTO.
** MBTO GREATER THAN 4' FROM EOP. APPROACH LENGTH IS 8:1 AND DEPARTURE LENGTH IS 5:1

| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|--------------------|--------------|-----------|
| 346 & 1362 | 20-00135-04-RS | LIVINGSTON | 6 | 5 |
| PROJECT NO. KEK8(123) | | CONTRACT NO. 87803 | | |

SCHEDULE OF RESURFACING QUANTITIES

| LOCATION (QUANTITY) | LENGTH | Paving Width | HMA SURF REMOVAL VAR DEPTH (SQ YD) | SHORT-TERM PAVT MARKING (FOOT) | SHORT-TERM PAVT MARKING REMOVAL (SQ FT) | PAINT PVT MARKING LINE, 4" (FOOT) | BIT MATLS TACK CT (POUND) | BIT MATLS PR CT (POUND) | LONG. JOINT SEALANT (FOOT) | WATERPRF MEMBRANE SYSTEM (SQ YD) | HMA SURF REMOVAL 1.00" DEPTH (SQ YD) | HMA BINDER CSE IL-9.5FG N50 (TON) | HMA SURF CSE IL-9.5FG "C" N50 (TON) | AGG WDG E SHLDR TYPE B (TON) |
|--|---------------|--------------|------------------------------------|--------------------------------|---|-----------------------------------|---------------------------|-------------------------|----------------------------|----------------------------------|--------------------------------------|-----------------------------------|-------------------------------------|------------------------------|
| MAINLINE | | | | <i>(4 Apps.)</i> | <i>(1 App.)</i> | | | | | | | | | |
| 1+07. to 2+07. (TAPER) | 100.00 | 22 | 248 | 40 | 3 | | 165 | - | - | - | - | 17 | 17 | 6 |
| 2+07. to 4+65. | 258.00 | 22 | - | 104 | 9 | | 426 | - | - | - | - | 45 | 44 | 33 |
| 4+65. to 4+85. PCC PAVEMENT | 20.00 | 56 | - | - | - | | - | - | - | - | - | - | - | - |
| 4+85. to 6+51. | 166.00 | 22 | - | 68 | 6 | | 274 | - | - | - | - | 29 | 28 | 21 |
| 6+51. to 6+81. (TAPER) | 30.00 | 22 | - | 12 | 1 | | 49 | - | - | - | - | 4 | 5 | 3 |
| 6+81. to 7+01. (TAPER) | 20.00 | 22 | - | 8 | 1 | | 22 | - | - | - | - | - | 4 | 2 |
| 7+01. to 7+51. | 50.00 | 22 | - | 20 | 2 | | 55 | - | - | - | - | - | 9 | 6 |
| 7+51. to 7+87.25 (SN 053-3019) | 36.25 | 24 | - | 16 | 1 | | 44 | - | - | 97 | - | - | 7 | - |
| 7+87.25 to 8+37.25 | 50.00 | 22 | - | 20 | 2 | | 55 | - | - | - | - | - | 9 | 6 |
| 8+37.25 to 8+57.25 (TAPER) | 20.00 | 22 | - | 8 | 1 | | 22 | - | - | - | - | - | 4 | 2 |
| 8+57.25 to 8+87.25 (TAPER) | 30.00 | 22 | - | 12 | 1 | | 49 | - | - | - | - | 4 | 5 | 3 |
| 8+87.25 TO 51+47. | 4259.75 | 22 | - | 1704 | 142 | | 7,029 | - | - | - | - | 740 | 729 | 539 |
| 51+47. to 52+47. (TAPER) | 100.00 | 22 | 248 | 40 | 3 | | 165 | - | - | - | - | 17 | 17 | 6 |
| PCC PAVEMENT OMISSION | 94.5 | 22 | - | - | - | | 156 | - | - | - | - | 16 | 16 | - |
| 53+41.5 to 54+41.5 (TAPER) | 100.00 | 22 | 248 | 40 | 3 | | 165 | - | - | - | - | 17 | 17 | 6 |
| 54+41.5 to 100+70. | 4628.50 | 22 | - | 1852 | 154 | | 7,637 | - | - | - | - | 804 | 792 | 586 |
| 100+70. to 105+70. (EXTRA WIDTH 4' RT) | 500.00 | 26 | - | 200 | 17 | | 875 | 500 | - | - | - | 102 | 101 | 32 |
| 105+70. to 106+70. | 100.00 | 22 | - | 40 | 3 | | 165 | - | - | - | - | 17 | 17 | 13 |
| 106+70. to 111+70. (EXTRA WIDTH 4' LT) | 500.00 | 26 | - | 200 | 17 | | 875 | 500 | - | - | - | 102 | 101 | 32 |
| 111+70. to 128+36. | 1666.00 | 22 | - | 668 | 56 | | 2,749 | - | - | - | - | 289 | 285 | 211 |
| 128+36. to 128+66. (TAPER) | 30.00 | 22 | - | 12 | 1 | | 49 | - | - | - | - | 4 | 5 | 3 |
| 128+66. to 128+86. (TAPER) | 20.00 | 22 | - | 8 | 1 | | 22 | - | - | - | - | - | 4 | 2 |
| 128+86. to 129+36. | 50.00 | 22 | - | 20 | 2 | | 55 | - | - | - | - | - | 9 | 6 |
| 129+36. to 129+65.5 (SN 053-3020) | 29.50 | 24 | - | - | - | | 36 | - | - | - | - | - | 6 | - |
| 129+65.5 to 130+15.5 | 50.00 | 22 | - | 20 | 2 | | 55 | - | - | - | - | - | 9 | 6 |
| 130+15.5 to 130+35.5 (TAPER) | 20.00 | 22 | - | 8 | 1 | | 22 | - | - | - | - | - | 4 | 2 |
| 130+35.5 to 130+65.5 (TAPER) | 30.00 | 22 | - | 12 | 1 | | 49 | - | - | - | - | 4 | 5 | 3 |
| 130+65.5 to 290+44. | 15978.50 | 22 | - | 6392 | 533 | | 26,365 | - | - | - | - | 2,776 | 2,734 | 2022 |
| SEE INTERSECTION DETAIL ON SHEET 5 | 112.00 | - | 248 | 44 | 4 | | 504 | - | 212 | - | - | 52 | 52 | 14 |
| 291+56. to 297+41. | 585.00 | 22 | - | 236 | 20 | | 965 | - | 585 | - | - | 102 | 100 | 74 |
| 297+41. to 302+38. (EXTRA WIDTH 4' LT) | 497.00 | 26 | - | 200 | 17 | | 870 | 497 | 497 | - | - | 102 | 101 | 31 |
| 302+38. to 304+38. | 200.00 | 22 | - | 80 | 7 | | 330 | - | 200 | - | - | 35 | 34 | 25 |
| 304+38. to 309+34. (EXTRA WIDTH 4' RT) | 496.00 | 26 | - | 200 | 17 | | 868 | 496 | 496 | - | - | 102 | 100 | 31 |
| 309+34. to 443+43. | 13409.00 | 22 | - | 5364 | 447 | | 22,125 | - | 13409 | - | - | 2,329 | 2,294 | 1697 |
| 443+43. to 443+73. (TAPER) | 30.00 | 22 | - | 12 | 1 | | 49 | - | 30 | - | - | 4 | 5 | 3 |
| 443+73. to 443+93. (TAPER) | 20.00 | 22 | - | 8 | 1 | | 22 | - | - | - | - | - | 4 | 2 |
| 443+93. to 444+33. (TAPER) | 40.00 | 22 | 99 | 16 | 1 | | 44 | - | - | - | - | - | 7 | 1 |
| 444+33. to 477+33. | 3300.00 | 22 | - | 1320 | 110 | | 3,630 | - | - | - | 8189 | - | 565 | 42 |
| TOTAL JOB LENGTH OR MAINLINE QUANTITY | 47626 | - | 1091 | 19004 | 1588 | 111143 | 77036 | 1993 | 15429 | 97 | 8189 | 7713 | 8245 | 5471 |
| MISCELLANEOUS | | | | | | | | | | | | | | |
| PRIVATE & COMMERCIAL ENTRANCES (17) | - | - | - | - | - | - | 658 | 1919 | - | - | 1130 | 16 | 93 | - |
| SIDEROAD INTERSECTIONS (15) | - | - | - | - | - | - | 882 | - | - | - | 291 | 29 | 50 | - |
| M.B.T.O.'s (3) | - | - | - | - | - | - | 56 | - | - | - | - | 3 | 3 | - |
| TOTALS OF MAINLINE & MISC. | 47,626 | - | 1,091 | 19,004 | 1,588 | 111,143 | 78,632 | 3,912 | 15,429 | 97 | 9,610 | 7,761 | 8,391 | 5,471 |

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THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:
HOT-MIX ASPHALT 112 LB/SQ YD/INCH
AGGREGATE WEDGE SHOULDER, TYPE B 2.05 TONS/CU YD

| BITUMINOUS MATERIALS APPLICATION RATES | |
|--|----------------|
| SURFACE TYPE | RESIDUAL RATE |
| AGGREGATE BASE (PRIME COAT) | 0.250 LB/SQ FT |
| MILLED HMA OR PCC (TACK COAT) | 0.050 LB/SQ FT |
| EXISTING PAVEMENT (TACK COAT) | 0.050 LB/SQ FT |
| TACK COAT (BETWEEN LIFTS) | 0.025 LB/SQ FT |

| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|--------------------|--------------|-----------|
| 346 & 1362 | 20-00135-04-RS | LIVINGSTON | 6 | 6 |
| PROJECT NO. KEK8(123) | | CONTRACT NO. 87803 | | |