03A IDOT LETTING MARCH 10, 2023

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS

PROJECT INFORMATION

CONTRACTOR: RESIDENT ENGINEER: ASSISTANT RESIDENT ENGINEER: ORIGINAL CONTRACT AMOUNT: FINAL CONSTRUCTION COST: IDOT LETTING DATE: IDOT AWARD DATE: NOTICE TO PROCEED: START OF CONSTRUCTION: SUBSTANTIAL COMPLETION:

LOCAL AGENCY CONTACT INFORMATION VILLAGE OF WHEELING - 847.459.2600 CITY OF PROSPECT HEIGHTS - 847.398.6070

ENGINEER'S PROJECT PERMIT LOG NPDES # N/A FAA AIRSPACE # CCDD LPC-663 DATED CONTRACTOR TO SUPPLY MWRDGC PERMIT # 03-246 & RL 09-063 N/A VILLAGE APP FOR CONSTRUCTION PERMIT # VILLAGE FLOODPLAIN PERMIT # CONTRACTOR'S REGISTRATION WITH VILLAGE VILLAGE SITE ALTERATION PERMIT # N/A CITY APPLICATION FOR PERMIT # N/A CITY SITE GRADING PERMIT # N/A CONTRACTOR'S REGISTRATION WITH CITY N/A

CONSTRUCTION PLANS FOR CHICAGO EXECUTIVE AIRPORT

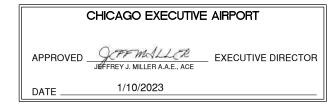
REHABILITATE ACCESS ROAD AT NW QUADRANT HANGARS

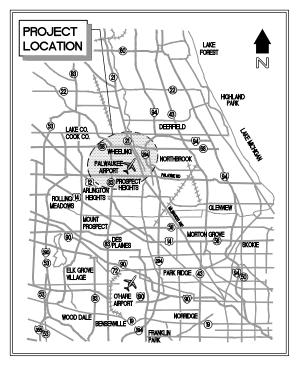
ILLINOIS PROJECT: PWK-4794

JANUARY 30, 2023

CHICAGO EXECUTIVE AIRPORT TOWNSHIP: 42 NORTH WHEELING TOWNSHIP RANGE: 11 EAST (SECTION: 13) COOK COUINTY

	CONSULTING ENG License No. 062-069	INEERS
	SUBMITTED BY	D Kyle Realizedy
	DATE	d. kyle peabody, p.e. 1/30/2023
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LOCATION MAP

SITE PLAN

PALATINE

ROAD

PROJECT

LOCATIONS

HINTZ RD

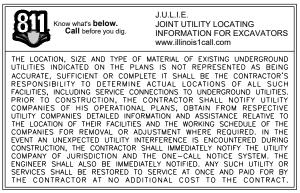
2102030-00\NWAR - COVER SHEET.DWG

PA064 TOTAL SHEETS = 9



D Kyle Pealody

LICENSE EXPIRATION DATE: 11/30/2023 DATE SIGNED: 01/30/2023



CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

STORM WATER POLLUTION PREVENTION NOTES:

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR'S COST. AND AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING A MILL AND OVERLAY ON THE ACCESS ROAD AT NW QUANDRANT HANGARS AT CHICAGO EXECUTIVE AIRPORT. THE PROJECT INCLUDES PAVEMENT REHABILITATION AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

- THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES:
- 1. INSTALL TEMPORARY EROSION CONTROL MEASURES.
- 2. PAVEMENT MILLING AND REMOVAL
- 3. CONSTRUCT NEW OVERLAYS.

4. REMOVAL AND DISPOSAL OF TEMPORARY EROSION CONTROL MEASURES.

AREA OF CONSTRUCTION SITE

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THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE

THE CONSTRUCTION SITE DRAINS INTO STORM SEWER SYSTEMS THAT OUTLET INTO THE DES PLAINES RIVER.

SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
- 4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
- SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
- 6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND FSTABILISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

SOIL EROSION AND SEDIMENT CONTROL NOTES:

- ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
- NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
- . CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
- TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
- 6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
- SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY, THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, COOK COUNTY, CHICAGO EXECUTIVE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
- 9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
- 10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
- 11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
- 12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
- 13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS, SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
- 14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
- 15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
- 16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

MUNICIPALITIES GENERAL NOTES

- . THE CHICAGO EXECUTIVE AIRPORT IS A JOINT OWNERSHIP BY BOTH THE VILLAGE OF WHEELING AND CITY OF PROSPECT HEIGHTS. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH VILLAGE AND CITY CODES, ORDINANCES AND STANDARDS AS APPLICABLE.
- ALL CONTRACTORS AND SUBCONTRACTORS SHALL BE REGISTERED WITH THE VILLAGE AND CITY PRIOR TO THE NOTICE TO PROCEED. ALL REGISTRATION FEES SHALL BE INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL WORK WITH THE AIRPORT AND ENGINEER TO SECURE THE REQUIRED VILLAGE AND CITY LOCAL CONSTRUCTION PERMITS PRIOR TO THE NOTICE TO PROCEED.
- 4. THE CONTRACTOR SHALL COORDINATE WITH THE VILLAGE AND CITY AT THE WEEKLY PROGRESS MEETINGS AND SHALL NOTIFY THE CITY OF PROSPECT HEIGHTS (847.398.6700) AND THE VILLAGE OF WHEELING (847.459.2600) A MINIMUM OF 48 HOURS PRIOR TO ANY REQUIRED VILLAGE(ITY INSPECTIONS.
- 5. ALL STORM SEWERS AND SANITARY SEWERS ON THE AIRPORT SITE ARE OWNED, OPERATED AND MAINTAINED BY THE CHICAGO EXECUTIVE AIRPORT UNLESS LABELED OTHERWISE.

STAGING AND PROTECTION NOTES

- . THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO
 ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUBE REMOVING ANY AND ALL STORM WATER POLLUTION
 PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
- 3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.
- 4. ALL EQUIPMENT FUELING AND GREASING SHALL BE COMPLETED AT THE STAGING AREA. SPILLS SHALL BE IMMEDIATELY CONTAINED AND THE AREA CLEANED AT NO ADDITIONAL COST THE CONTRACT.

INDEX TO SHEETS

SHEET NUMBER	SHEET TITLE
1	COVER SHEET
2	INDEX TO SHEETS /
3	SITE PLAN AND PRO
4	SEQUENCE OF CON
5	SEQUENCE OF CON
6	EXISTING CONDITIO
7	PROPOSED IMPROV
8	TYPICAL SECTIONS
9	EROSION CONTROL

SUMMARY OF QUANTITIES BASE BID				
AR150520	MOBILIZATION	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	70	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	195	
AR156520	INLET PROTECTION	EACH	9	
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	65	
AR208910	REMOVE & REPLACE AGGREGATE BASE	SQ YD	195	
AR401610	BITUMINOUS SURFACE COURSE	TON	135	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	1050	
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	490	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	75	5
AR403610	BITUMINOUS BASE COURSE	TON	70	
AR602510	BITUMINOUS PRIME COAT	GALLON	145	
AR603510	BITUMINOUS TACK COAT	GALLON	240	

ADDITIVE ALTERNATE NO. 1

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AS156520	INLET PROTECTION	EACH	3	
AS401610	BITUMINOUS SURFACE COURSE	TON	75	
AS401650	BITUMINOUS PAVEMENT MILLING	SQ YD	710	
AS401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	12	
AS603510	BITUMINOUS TACK COAT	GALLON	115	

DITIVE ALTERNATE NO. 2

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AT156520	INLET PROTECTION	EACH	4	
AT401610	BITUMINOUS SURFACE COURSE	TON	70	
AT401650	BITUMINOUS PAVEMENT MILLING	SQ YD	575	
AT603510	BITUMINOUS TACK COAT	GALLON	90	

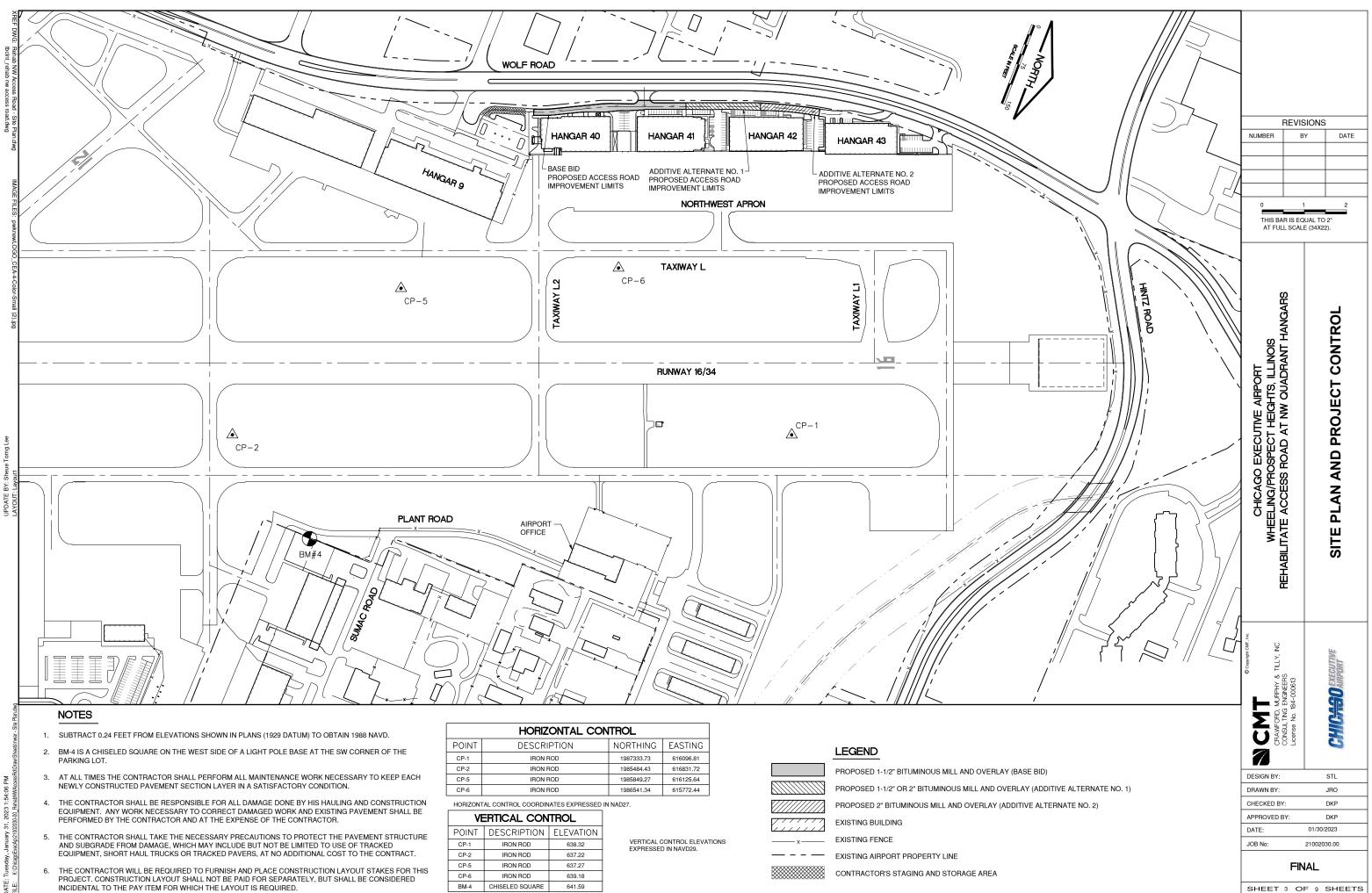
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/ SUMMARY OF QUANTITIES / GENERAL NOTES

- NSTRUCTION PLAN
- NSTRUCTION GENERAL NOTES AND DETAILS
- DNS / PROPOSED REMOVALS
- VEMENTS PLAN

PLAN

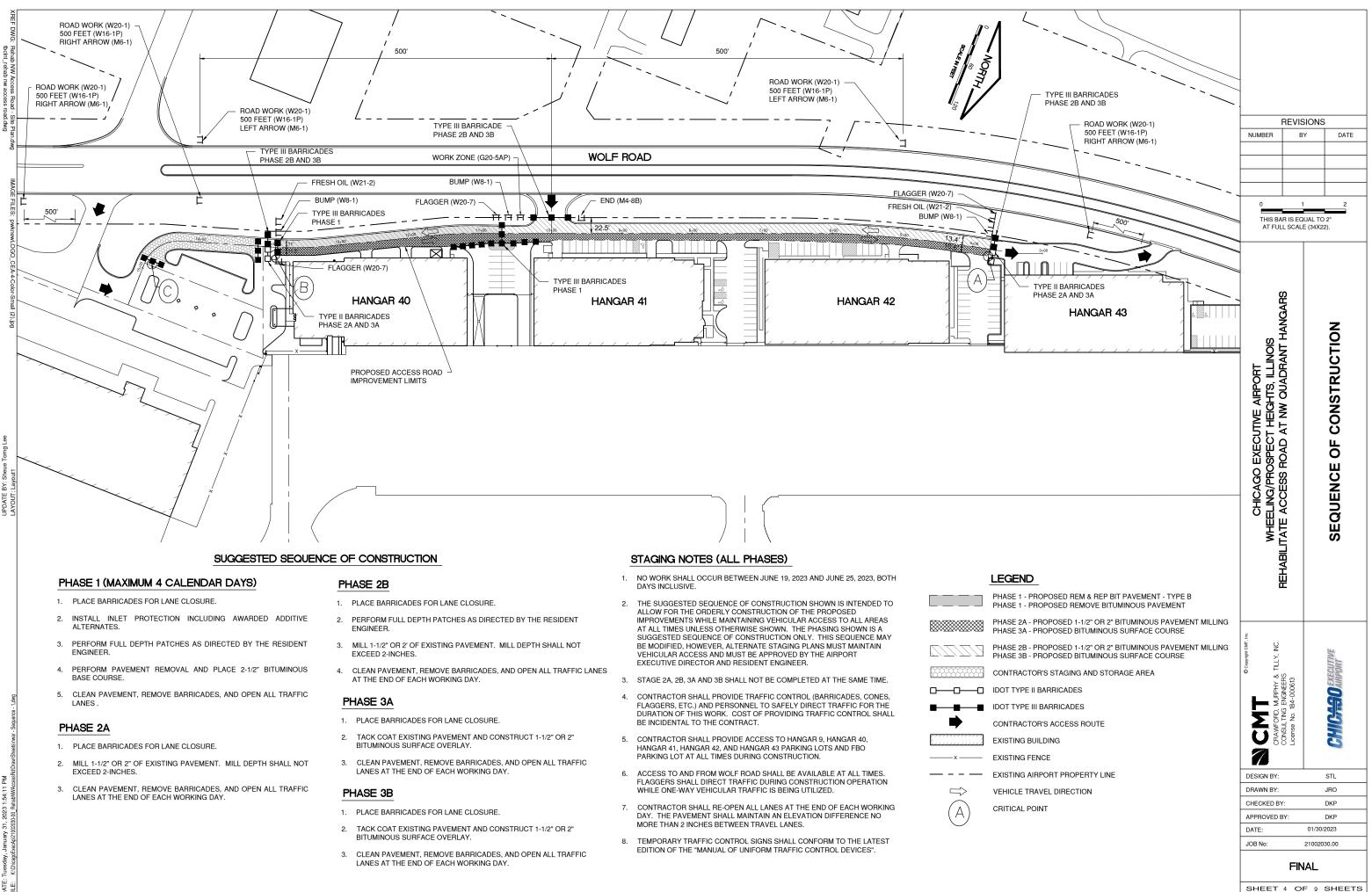
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CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS DELIADII ITATE ACCESS DATA AT ANY OLIADAANT UAMICABS			INDEX TO SHEETS/SUMMARY OF QUANTITIES/ GENERAL NOTES
CCAMPERT © Convertence of the Convertence of the Convertence of Converting of Converti	License No. 184-000613		CHICEO EXECUTIVE AIRPORT
DESIGN BY: DRAWN BY: CHECKED BY APPROVED B DATE:		01/30/2	STL JRO DKP DKP 2023
JOB No:	FIN 2 0		30.00 SHEETS

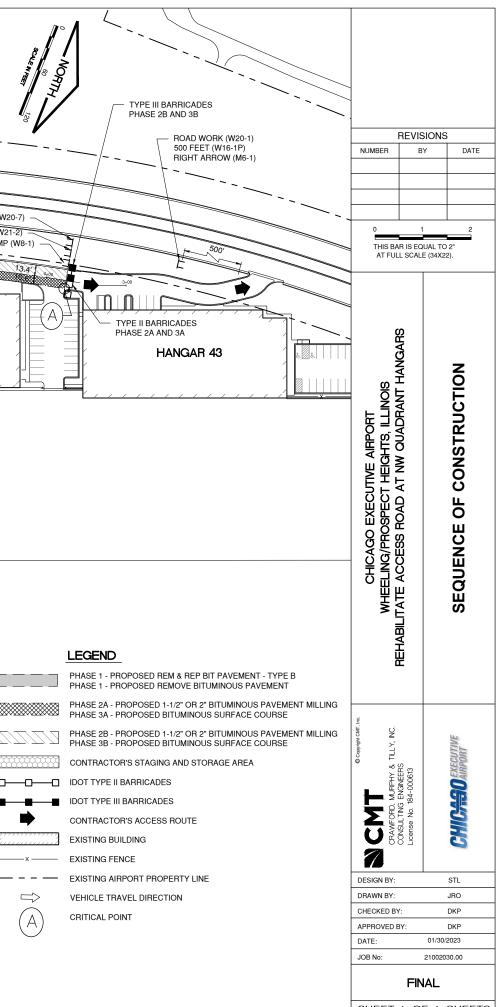


- INCIDENTAL TO THE PAY ITEM FOR WHICH THE LAYOUT IS REQUIRED.

HORIZONTAL CONTROL					
POINT	DESCRIPTION	NORTHING	EASTING		
CP-1	IRON ROD	1987333.73	616096.81		
CP-2	IRON ROD	1985484.43	616831.72		
CP-5	IRON ROD	1985849.27	616125.64		
CP-6	IRON ROD	1986541.34	615772.44		

POINT	DESCRIPTION	ELEVATION
CP-1	IRON ROD	638.32
CP-2	IRON ROD	637.22
CP-5	IRON ROD	637.27
CP-6	IRON ROD	639.18
BM-4	CHISELED SQUARE	641.59





GENERAL NOTES

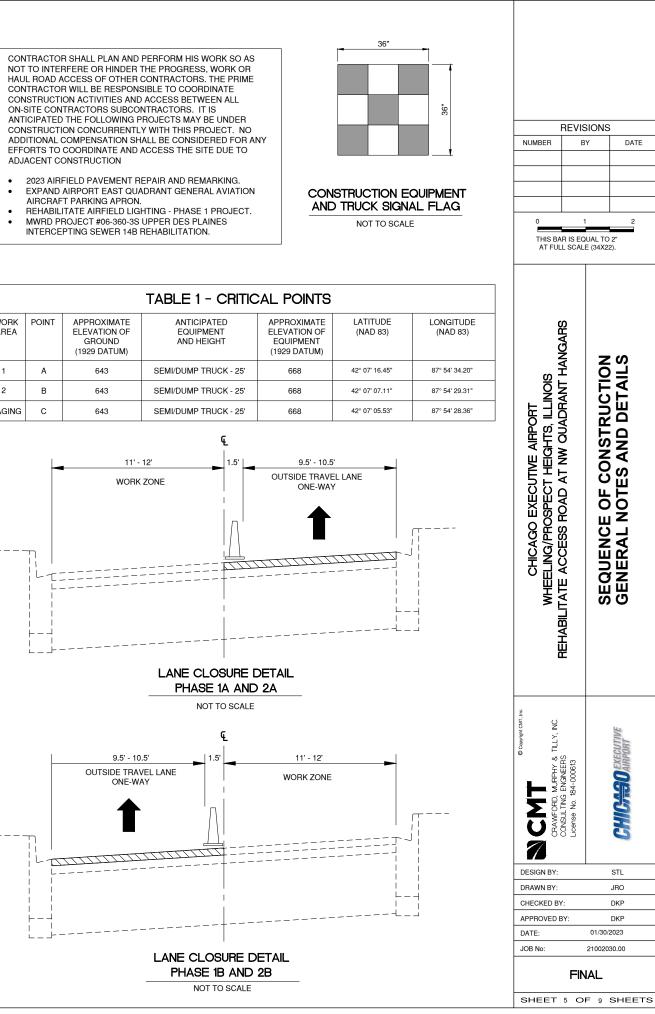
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING VEHICLE ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT EXECUTIVE DIRECTOR AND RESIDENT ENGINEER.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370 (LATEST EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL 3 STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS. NO EXCEPTIONS FOR SLOW MOVING EQUIPMENT SHALL BE ALLOWED
- THE AIRPORT EXECUTIVE DIRECTOR SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING PHASING AND SEQUENCE AS IT RELATES TO PEDESTRIAN AND VEHICULAR SAFETY
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT EXECUTIVE DIRECTOR.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE TENANT PARKING LOTS AND DRIVEWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER AND AIRPORT.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT
- MATERIALS REMOVED FROM THE PROJECT WILL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE
- 10. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, CONSTRUCTION FENCE, SIGNING, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES SHALL HAVE FLASHING YELLOW LIGHT(S) AND CONFORM TO IDOT STANDARD 701901-08, TYPE II AND TYPE III, SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT
- 11. THE CONTRACTOR SHALL CONTACT THE AIRPORT EXECUTIVE DIRECTOR THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- 12. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT EXECUTIVE DIRECTOR AND THE RESIDENT ENGINEER IMMEDIATELY
- 13 DUBING ADVERSE WEATHER THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- 14. CHICAGO EXECUTIVE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- 15. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING DBAINAGE, GBADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES
- 16. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- 17. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO PREVIOUS EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- 18. COORDINATION MEETINGS THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA. AND OTHER APPROPRIATE OFFICIALS, MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

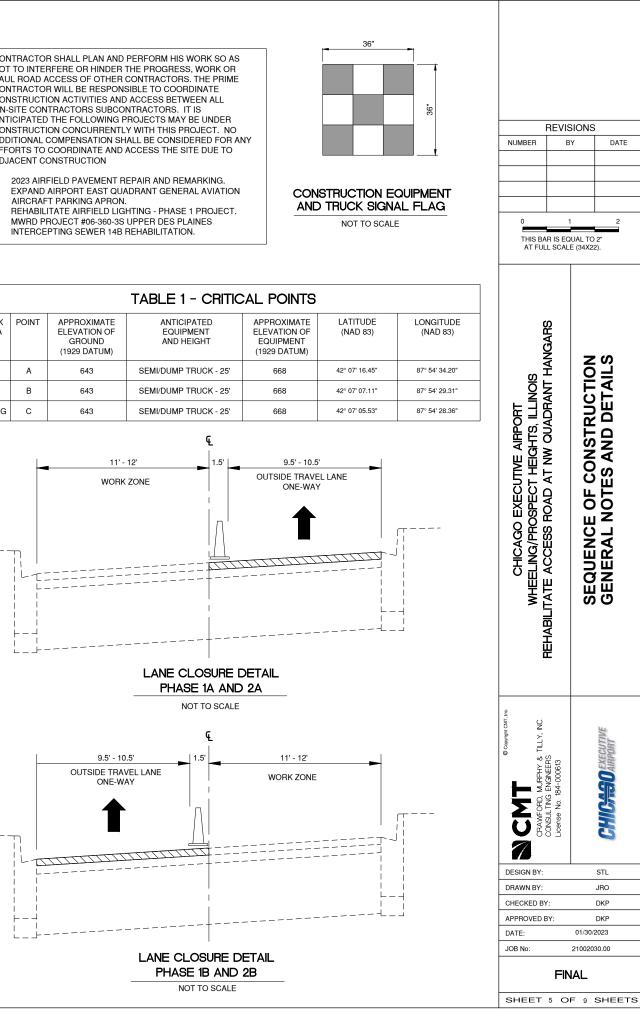
- 19. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES
- 20. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS. FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL. UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 21. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT EXECUTIVE DIRECTOR
- 22. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS OR A WORKING BEACON LIGHT ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL
- 23. PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT. THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DETAILS HOW THE CONTRACTOR WILL COMPLY WITH THE CSPP. ALSO, IT WILL NOT BE POSSIBLE TO DETERMINE ALL SAFETY PLAN DETAILS (FOR EXAMPLE SPECIFIC HAZARD EQUIPMENT AND LIGHTING, CONTRACTOR'S POINTS OF CONTACT, CONSTRUCTION EQUIPMENT HEIGHTS) DURING THE DEVELOPMENT OF THE CSPP. THE SUCCESSFUL CONTRACTOR MUST DEFINE SUCH DETAILS BY PREPARING AND SPCD THAT THE AIRPORT OPERATOR REVIEWS FOR APPROVAL PRIOR TO ISSUANCE OF A NOTICE-TO-PROCEED. THE SPCD IS A SUBSET OF THE CSPP, SIMILAR TO HOW A SHOP DRAWING REVIEW IS A SUBSET TO THE TECHNICAL SPECIFICATIONS

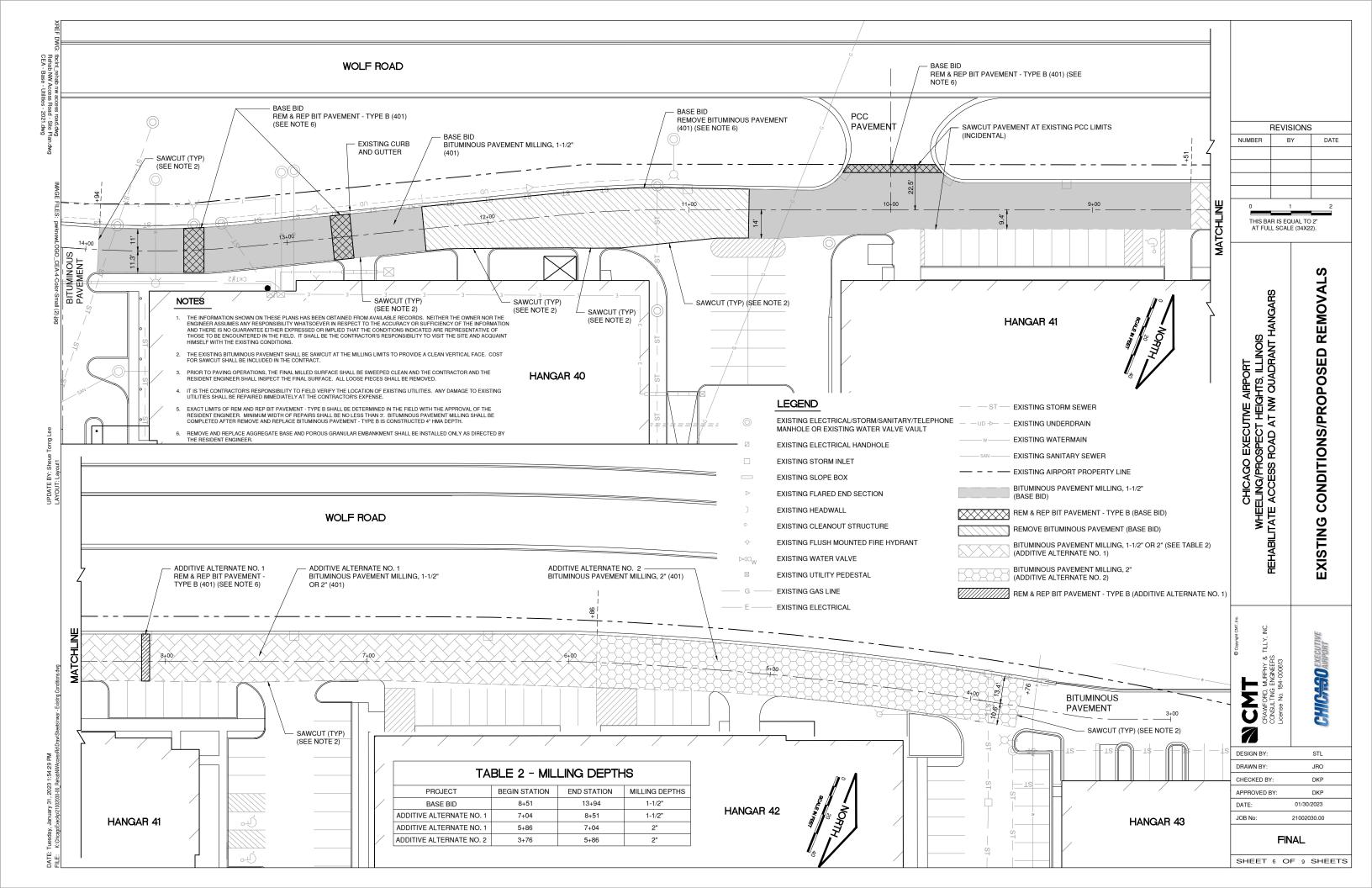
CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER EFFORTS TO COORDINATE AND ACCESS THE SITE DUE TO ADJACENT CONSTRUCTION

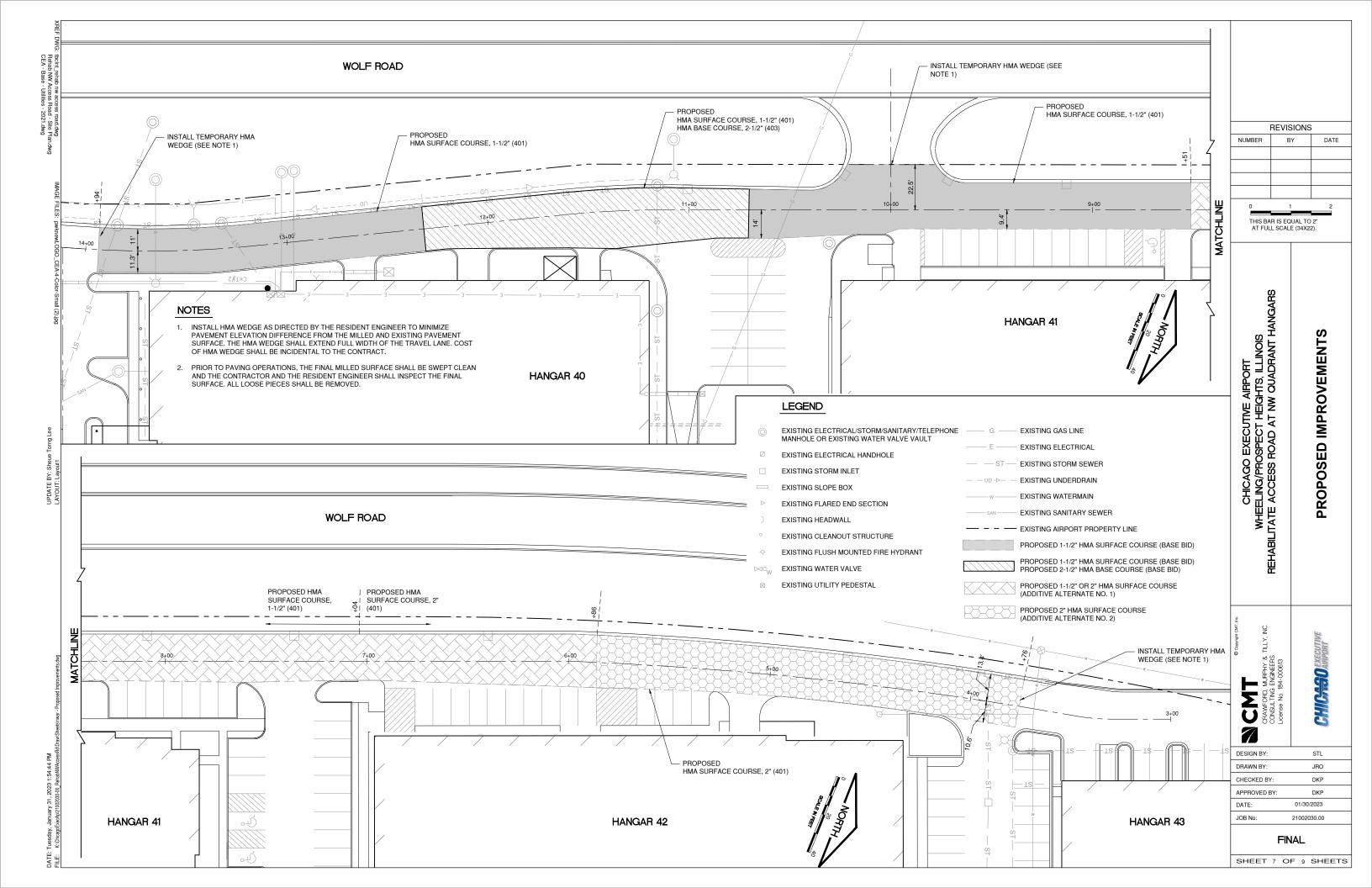
- AIRCRAFT PARKING APRON.
- MWRD PROJECT #06-360-3S UPPER DES PLAINES INTERCEPTING SEWER 14B REHABILITATION.

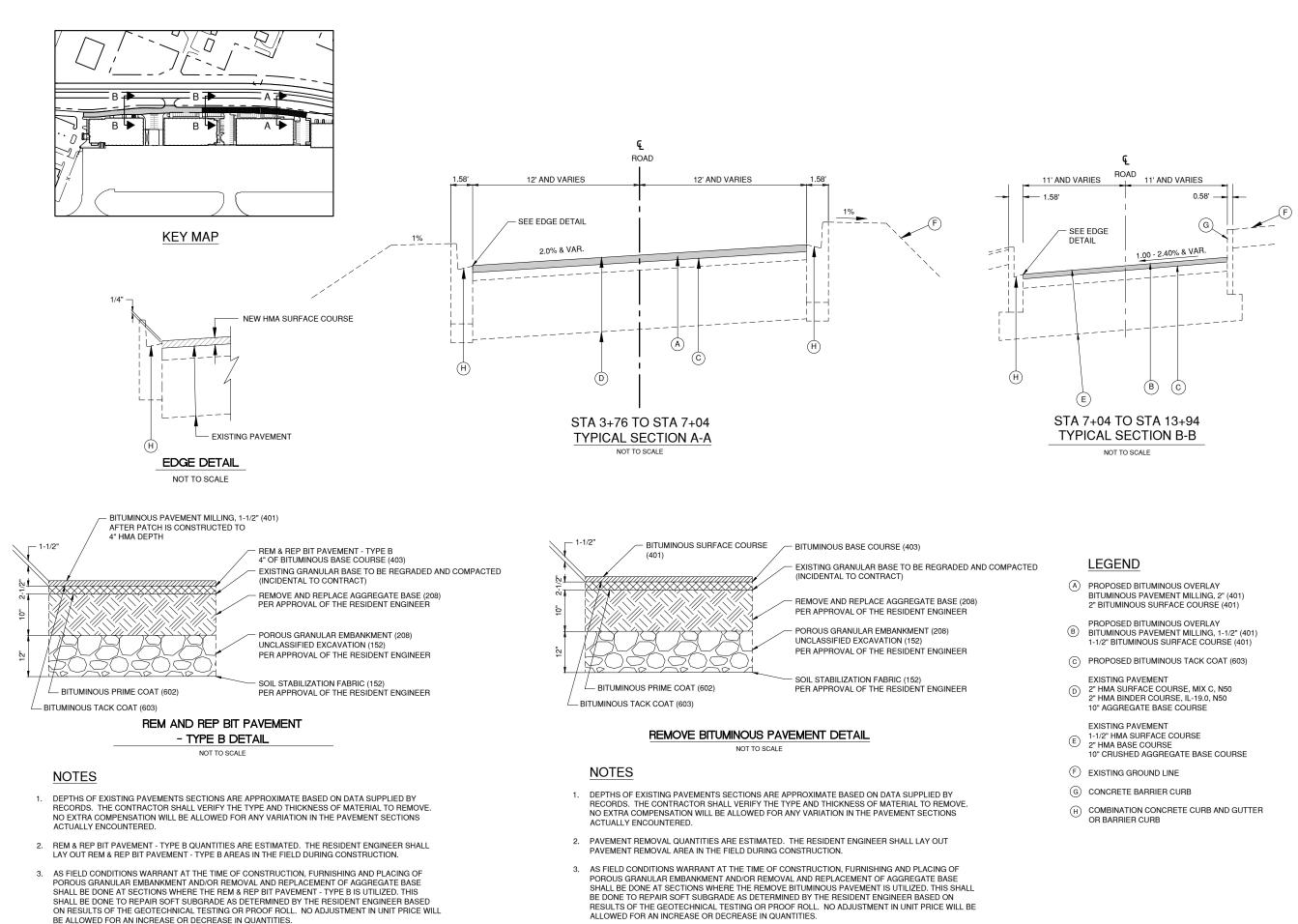
			TABLE 1 - CRITIC
WORK AREA	POINT	APPROXIMATE ELEVATION OF GROUND (1929 DATUM)	ANTICIPATED EQUIPMENT AND HEIGHT
1	А	643	SEMI/DUMP TRUCK - 25'
2	В	643	SEMI/DUMP TRUCK - 25'
STAGING	С	643	SEMI/DUMP TRUCK - 25'











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