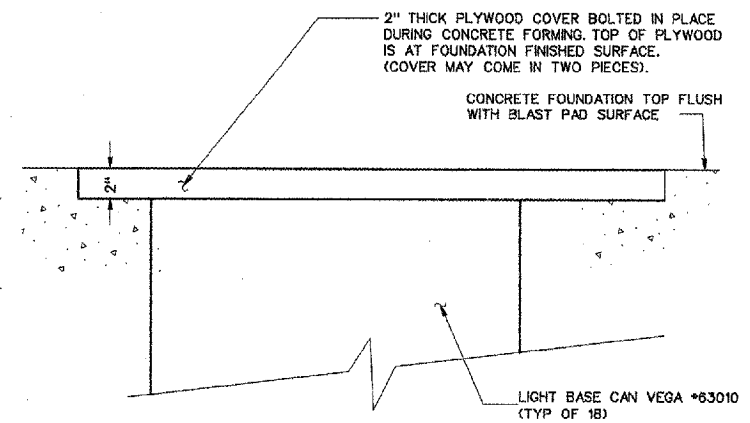


THRESHOLD BAR-ELEVATION VIEW
 1/2 OF BAR IS SHOWN. BAR IS SYMMETRICAL ABOUT RUNWAY CENTERLINE



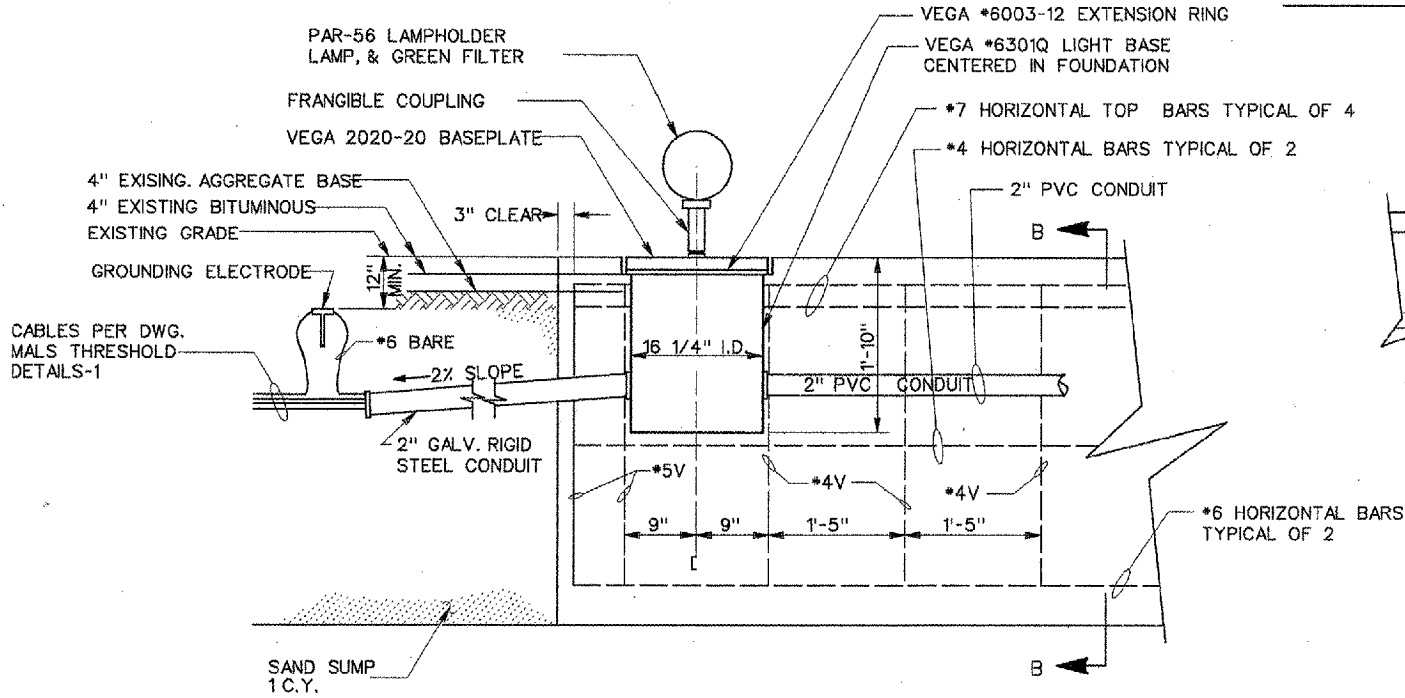
DETAIL "A"
 CONCRETE-FORMING DETAIL-FOUNDATION RECESS

NOTES:

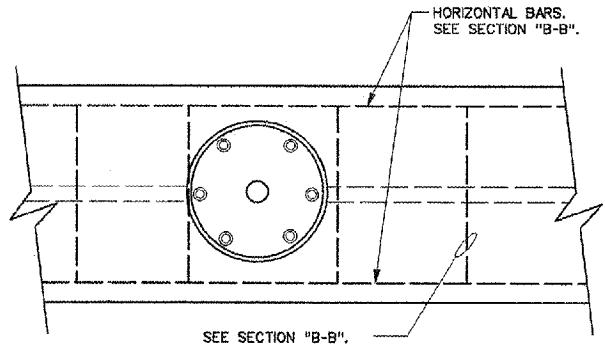
1. FOUNDATION. ALL PAVEMENT CUTS SHALL BE STRAIGHT-LINE SAW CUTS EXACTLY OUTLINING THE THRESHOLD BAR CONCRETE FOUNDATION. AFTER EXCAVATING, HAND-SHAPE THE EXCAVATION SIDES IN SMOOTH VERTICAL PLANE EXTENDING DOWN FROM THE SAW-CUT OUTLINE. THE LIGHT BAR FOUNDATION CONCRETE SHALL BE FORMED AGAINST THESE PREPARED EARTH SIDES. ALL OVERDIG SHALL BE REPLACED WITH CONCRETE. THE FOUNDATION TOP SHALL BE FINISHED FLUSH WITH THE EXISTING ADJACENT ASPHALT BLAST PAD SURFACE.
2. STEEL PLATES. SEGMENTS OF THE LIGHT BAR FOUNDATION EXCAVATION WHICH MUST BE LEFT OPEN OVERNIGHT, SHALL BE COVERED WITH 2" THICK STEEL PLATES. THESE PLATES SHALL BE LEFT IN PLACE UNTIL WORK RESUMES ON THE COVERED PORTION OF THE FOUNDATION. IN PLACING AND REMOVING THE STEEL PLATES, THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION NOT TO DAMAGE THE SAW-CUT EDGES OF THE BLAST PAD ASPHALT AROUND THE PERIMETER OF THE FOUNDATION, OR TO CAVE IN THE SIDES OF THE EXCAVATION. WHEN THE FOUNDATION CONCRETE IS PLACED IN ITS ENTIRETY, THE STEEL PLATES SHALL BE REMOVED FROM THE CONSTRUCTION SAFETY AREA.
3. LIGHT BASES (CANS). THE MALS THRESHOLD BAR LIGHT BASES SHALL HAVE THE FOLLOWING SPECIAL FEATURES:
 - A. ALL THE LIGHT BASES HAVE 2" FLEX CONNEX GROMMETS INSTEAD OF 2" THREADED HUBS.
 - B. ALL LIGHT BASES (FOR FRANGIBLE PAR-56 LIGHTS) 16 1/4"-I.D. COME WITH A 17 3/4"-DIA, 2"-THICK PLYWOOD COVER.
4. LIGHT BASE INSTALLATION. VERIFY THAT THE PLYWOOD COVER SUPPLIED WITH EACH LIGHT BASE IS 2" THICK, AND HAS THE CORRECT DIAMETER (SEE 3B ABOVE.) VERIFY THAT THE PLYWOOD COVER RIM IS COATED WITH PARAFFIN FOR EASE OF REMOVAL FROM SET CONCRETE. SEE DETAIL "A". CUT AND PLACE THE 2" PVC CONDUIT SUCH THAT THE CONDUIT END PROJECTS 1" TO 2" INTO THE LIGHT BASE BEYOND THE GROMMET. ALL CONDUIT ENDS MUST BE DEBURRED DURING PLACEMENT AND SET OF THE THRESHOLD BAR FOUNDATION SEGMENT, THE LIGHT BASE/PLYWOOD COVER COMBINATION MUST BE HELD RIGIDLY IN PLACE WITH A JIG, SUCH THAT:
 - A. THE LIGHT BASE IS VERTICAL.
 - B. THE LIGHT BASE IS ORIENTED SUCH THAT TWO DIAMETRICALLY OPPOSITE FLANGE BOLTS ARE CENTERED ON THE LIGHT BAR CENTERLINE, PERPENDICULAR TO RUNWAY CENTERLINE, +/- 1/2 DEGREE, AND SO VERIFIED WITH A THEODOLITE OR TRANSIT.
 - C. THE TOP OF THE LIGHT BASE IS 2" BELOW THE ELEVATION OF THE PAVEMENT SURFACE ADJACENT THE POINT ON THE LIGHT BASE RIM FARTHEST FROM THE RUNWAY CENTERLINE. (DUE TO RUNWAY CROWN, THE PAVEMENT SURFACE ADJACENT THE POINT ON THE LIGHT BASE RIM FARTHEST FROM THE RUNWAY CENTERLINE WILL BE LOWER THAN THE PAVEMENT SURFACE AT ANY OTHER POINT AROUND THE LIGHT BASE RIM.)
 - D. THE LIGHT BASE CANNOT FLOAT UP THROUGH THE CONCRETE BEFORE IT SETS.
5. THE LIGHT BASE EXTENSION TOP FLANGE IS HORIZONTAL WITHIN A TOLERANCE OF 1/16" VERTICAL PER 1 FOOT HORIZONTAL. SEE NOTE 6.
6. TO ACHIEVE CONSTRUCTION WITHIN TOLERANCES IS RECOMMENDED THAT THE ASSEMBLY OF LIGHT BASE, CONDUITS AND COUPLINGS BE HELD IN PLACE WITH A JIG DURING PLACEMENT OF CONCRETE. IT IS NOT ACCEPTABLE TO CORRECT ANY OUT OF TOLERANCE DIMENSIONS BY TILTING OR BENDING EMT LIGHTS MASTS.

REVISIONS		
NUMBER	BY	DATE

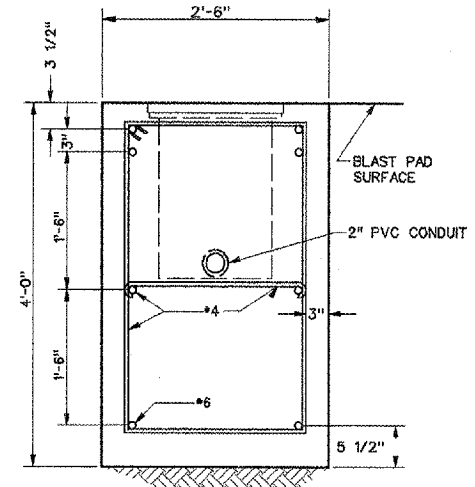
0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



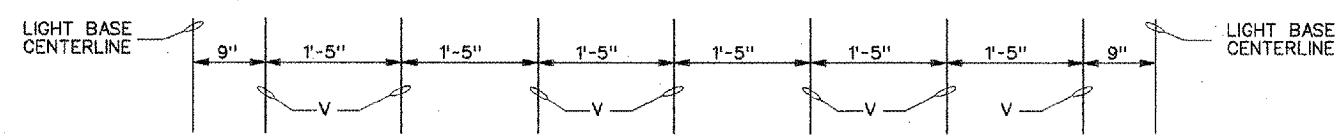
ELEVATION "A"
 END OF LIGHT BAR FOUNDATION
 (TYPICAL BOTH ENDS. THE SOUTH END IS ILLUSTRATED ABOVE, WITH OBSERVER FACING WEST.)
 NOT TO SCALE



PLAN



SECTION "B-B"
 NOTE:
 *4 MIDDLE TIES SHALL BE PLACED NEXT TO VERTICAL STIRRUPS.



DETAIL "V"
 NO SCALE

NOTE:
 LIGHT-TO-LIGHT SPACING OF *4 STIRRUPS ARE AS SHOWN ABOVE THROUGHOUT THE FOUNDATION.

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS

MALS THRESHOLD BAR FOUNDATION

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
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DESIGN BY:	TM
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	12/25/2004
JOB No:	030850300

01/18/2005 K:\Bloomington\ap\0308503\draw\sheets\laf-gl-d-5618-6-1-s1.dgn