

BITUMINOUS PAVEMENT MILLING

THE AREA DESIGNATED AS [diagonal hatching] ON THIS SHEET WILL BE CUT OR TRIMMED AS SHOWN ON THE CROSS-SECTIONS. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE DEPTH OF THE MILLING WILL BE 1-1/4" ±1/8". THE MINIMUM LENGTH OF TRAVELING SKI USED FOR MILLING OPERATIONS IS 40 FEET.

THE EXISTING PAVEMENT WILL BE SAWED AS SHOWN ON THIS SHEET, IF A TRUE VERTICAL EDGE CAN NOT BE ACHIEVED BY MILLING. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL WILL BE DISPOSED OF AT THE WASTE/FILL AREA SHOWN ON THE SAFETY PLAN.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT OR EQUIPMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER. ALL PAVEMENT MILLING WILL BE COMPLETED IN ACCORDANCE WITH THE SPECIAL PROVISIONS, INCLUDING THE DISPOSAL AND USE OF THE WASTE MILLINGS.

THE BITUMINOUS PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM:
AR401650 "BITUMINOUS PAVEMENT MILLING" 110,014 SQ. YDS.

CLEANING AND SEALING BITUMINOUS CRACKS

THE RESIDENT ENGINEER WILL DETERMINE WHICH CRACKS ARE LARGE ENOUGH TO WARRANT CLEANING AND SEALING FOLLOWING THE BITUMINOUS PAVEMENT MILLING OPERATIONS.

FOLLOWING A VISUAL SURVEY OF THE EXISTING RUNWAY SURFACE A PROPOSED PLAN QUANTITY OF 10,000 LINEAR FEET OF CRACK CLEANING AND SEALING WAS ESTABLISHED. THE EXACT AMOUNT OF CRACKS TO BE CLEANED AND SEALED WILL BE THE NUMBER OF LINEAR FEET OF CRACKS MARKED BY THE RESIDENT ENGINEER ONCE THE MILLED SURFACE IS EXPOSED AND THE EXTENT OF THE CRACKS CAN BE ACCURATELY ASSESSED.

ALL CRACKS DESIGNATED BY THE RESIDENT ENGINEER FOR CLEANING AND SEALING WILL BE DONE SO AS DESCRIBED IN THE RECURRING SPECIAL PROVISIONS, DATED JULY 1, 2004.

THIS ITEM WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAR FOOT OF CLEANING AND SEALING CRACKS IN THE MILLED PAVEMENT, COMPLETE; WHICH PRICE AND PAYMENT SHALL CONSTITUTE FULL COMPENSATION FOR ALL ROUTING, CLEANING, PREPARATION AND DISPOSAL OF ALL LOOSE MATERIALS; AND FOR ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM.

PAYMENT WILL BE MADE UNDER:

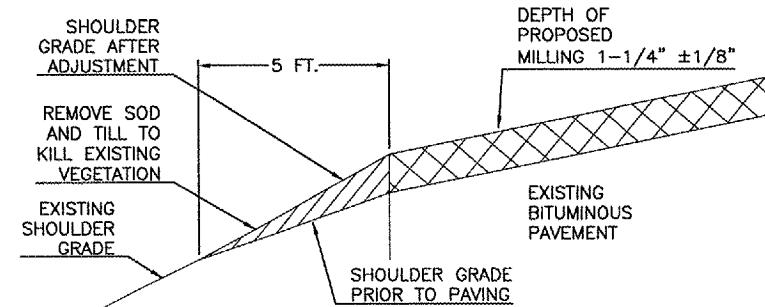
ITEM AR201661 CLEAN & SEAL BITUMINOUS CRACKS - PER LINEAR FOOT.

THRESHOLD LIGHT REMOVAL

THE CONTRACTOR MAY TEMPORARILY REMOVE THE BASE MOUNTED THRESHOLD LIGHTS SHOWN FOR ACCESS TO THE RUNWAY ENDS BY MILLING AND PAVING EQUIPMENT.

STANDARD L-867 BASE COVER PLATES (1/2" OR PLATE STEEL (3/4" MIN.) WILL BE REQUIRED AS TEMPORARY COVER TO PROTECT EACH CAN AND ITS CONTENTS FROM BEING DAMAGED BY WEATHER OR HEAVY EQUIPMENT. EACH LIGHT MUST BE DISCONNECTED FROM THE SECONDARY LEAD AND STORED IN AN AREA WHERE IT WILL NOT BE DAMAGED. THE CONNECTOR UNPLUGGED FROM THE LIGHT WILL BE WRAPPED AND TAPED INSIDE THE CAN TO PREVENT MOISTURE FROM BEING INTRODUCED.

THE REMOVAL, STORAGE, AND REPLACEMENT OF THE LIGHTS WHEN THE TASK IS COMPLETE, AS WELL AS ANY DAMAGE SUSTAINED, IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR, WILL BE CONSIDERED INCIDENTAL TO THE PROJECT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



RUNWAY END SHOULDER DETAIL
"NOT TO SCALE"

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.J.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

SHOULDER ADJUSTMENT NOTES

THE AMOUNT OF SHOULDER PROPOSED FOR ADJUSTMENT IS THE AMOUNT NECESSARY TO PROVIDE A FLUSH TRANSITION FROM THE PROPOSED PAVEMENT SURFACE AT THE RUNWAY END TO A DISTANCE 5 FT OFF OF EACH RUNWAY END, IN ORDER TO PREVENT THE PAVING EQUIPMENT FROM INTRODUCING SOD AND SOIL INTO THE BITUMINOUS MIX AS THE PAVER BEGINS AND ENDS A PAVING LANE.

SHOULDER ADJUSTMENTS WILL BE CONSTRUCTED IN ACCORDANCE TO THE SPECIAL PROVISIONS. NO COMPACTION CONTROL TESTS WILL BE REQUIRED.

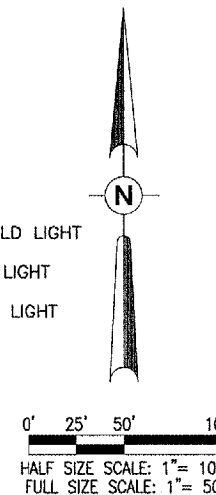
THE QUANTITY OF SHOULDER ADJUSTMENT SHOWN IN THE SUMMARY OF QUANTITIES IS BASED ON WIDTH OF THE RUNWAY AND DISTANCE 5 FEET OFF EACH RUNWAY END.

SEEDING AND MULCHING OF THE AREA OF SHOULDER ADJUSTMENT WILL BE CONSIDERED INCIDENTAL TO THE ITEM AR152480-SHOULDER ADJUSTMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

SHOULDER ADJUSTMENT WILL BE PAID FOR AT THE MEASURED NUMBER OF SQ. YDS. OF GRADED SHOULDER COMPLETED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

LEGEND

- [diagonal hatching] EXISTING PAVEMENT
- [diagonal hatching] PROPOSED IMPROVEMENTS
- E- EXISTING ELECTRICAL CABLE
- [square with dot] EXISTING BASE MOUNTED THRESHOLD LIGHT
- [square with dot] EXISTING BASE MOUNTED RUNWAY LIGHT
- [circle with dot] EXISTING STAKE MOUNTED TAXIWAY LIGHT
- [triangle] EXISTING REILS
- [square] EXISTING VASI
- [rectangle] EXISTING TAXI GUIDANCE SIGN
- [line with dashes] EXISTING ELECTRICAL DUCT



BY	
REVISION	
DATE	

HANSON
MT. VERNON, ILLINOIS
A.I.P. PROJ.: 3-17-0074-B1
MWN-3428

Project No. 819-05RWD_0803	File Name R-111PRP.dgn	Scale 1" = 50'	Date 11/30/04
LAYOUT	R.A.W.	11/30/04	
DRAWN	B.A.K.	11/30/04	
REVIEWED	C.A.H.	11/30/04	

HANSON
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REHABILITATE RUNWAY 5-23
PROPOSED PAVEMENT PREPARATION PLAN
STA. 5+00 TO STA. 19+00

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4 of 27 sheets