

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	(1-X-B)I-1	IROQUOIS	14	1
		ILLINOIS	CONTRACT NO. 66A28	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP 332 (IL 1)
SECTION (1-X-B)I-1

**BRIDGE REPAIRS
IROQUOIS COUNTY**

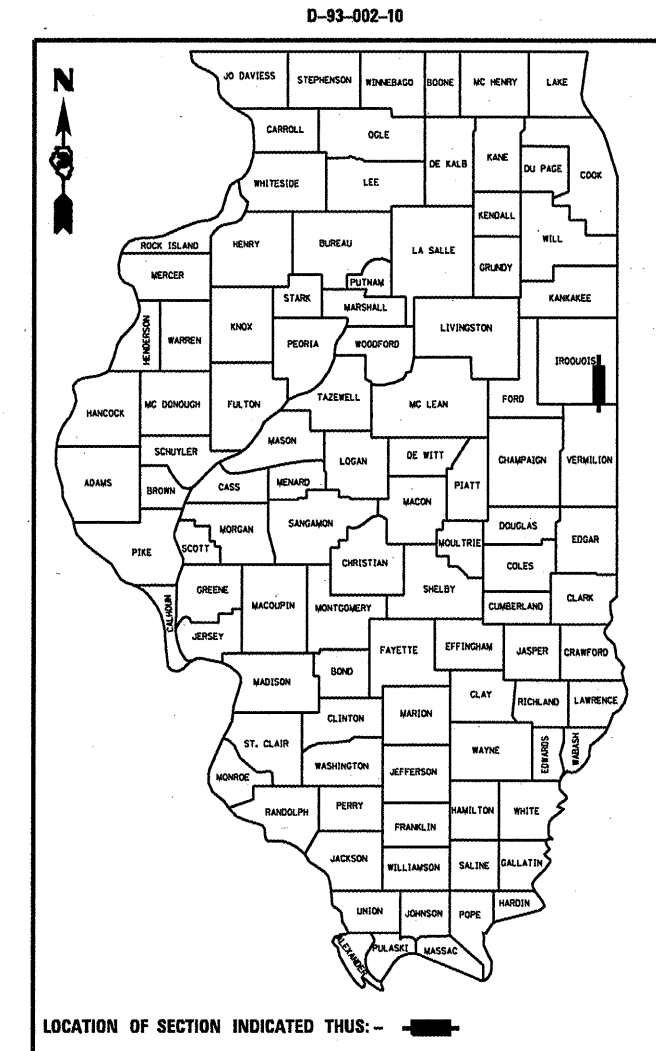
C-93-113-10

INDEX OF SHEETS

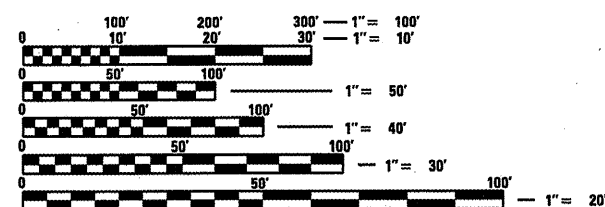
1. COVER SHEET
2. GENERAL NOTES
3. SUMMARY OR QUANTITIES
4. TYPICAL SECTION
5. STAGING DETAILS
6. TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
7. WIDTH RESTRICTION SIGNING
- 8 - 14. STRUCTURE REPAIR DETAILS

STANDARDS

- | | |
|-----------|-------------------------------------------------------------------------------|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 001001-02 | AREAS OF REINFORCEMENT BARS |
| 001006 | DECIMAL OF AN INCH AND OF A FOOT |
| 482001-02 | HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT |
| 635011-02 | REFLECTOR MARKER AND MOUNTING DETAILS |
| 701001-02 | OFF-ROAD OPERATIONS 2L, 2W, MORE THAN 15' (4.5 m) AWAY |
| 701006-03 | OFF-ROAD OPERATIONS 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 701011-02 | OFF-ROAD MOVING OPERATIONS 2L, 2W, DAY ONLY |
| 701101-02 | OFF-ROAD OPERATIONS MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 701106-02 | OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY |
| 701201-04 | LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS \geq 45 MPH |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701321-11 | LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER |
| 701901-01 | TRAFFIC CONTROL DEVICES |
| 704001-06 | TEMPORARY CONCRETE BARRIER |



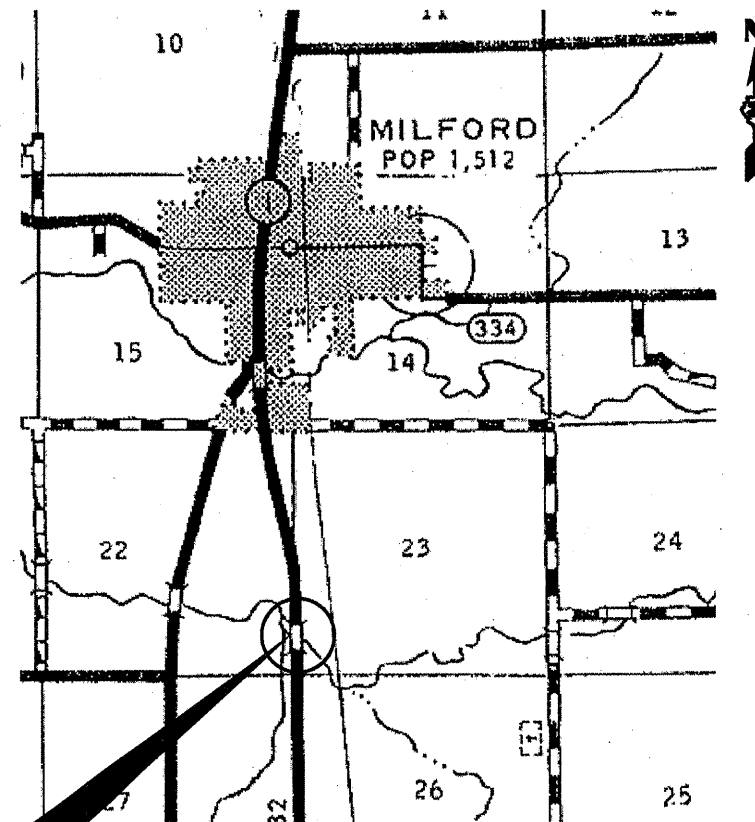
**RURAL
OTHER PRINCIPAL ARTERIAL**
2009 ADT = 2300
P.V. = 82.6%
S.U. = 6.5%
M.U. = 10.9%



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT LOCATION
SN 038-0200
CARRYING FAP 332 (IL 1)
OVER A TRIBUTARY OF SPRING CREEK
0.8 MILES SOUTH OF MILFORD



POINT LOCATION

PROJECT ENGINEER: JOE KANNEL, P.E.
UNIT CHIEF: RON WOODSHANK
TOWNSHIP: MILFORD
CONTRACT NO. 66A28

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED DECEMBER 21 20 10
George F. Rosen
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Feb 4 20 11
Scott E. Stitt P.E.
acting ENGINEER OF DESIGN AND ENVIRONMENT

Feb 4 20 11
Christine M. Leadler
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY FURNISHED BASED UPON THE UNIT BID PRICE FOR THE WORK.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE "JULIE" NUMBER IS 1-800-892-0123. A MINIMUM OF FORTY-EIGHT (48) HOURS ADVANCE NOTICE IS REQUIRED.

THE COST OF ANY SAW CUTS MADE TO COMPLETE THE WORK AS DESCRIBED IN PLAN DETAILS, UNLESS OTHERWISE NOTED SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED WITH THE VARIOUS PAY REMOVAL PAY ITEMS INVOLVED.

THE EXISTING AGGREGATE SHOULDERS ARE LOW. IN THE EVENT THAT CONSTRUCTION OPERATIONS RESULT IN A DROP OFF GREATER THAN 3" BETWEEN THE EDGE OF PAVEMENT OR EDGE OF SHOULDER WITHIN 3' OF THE EDGE OF PAVEMENT AND THE EARTH OR AGGREGATE SHOULDERS, THEN THE DROP OFF SHALL BE PROTECTED ACCORDING TO ARTICLE 701.04(B)(1) OF THE STANDARD SPECIFICATIONS. THIS WORK WILL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES FOR TRAFFIC CONTROL.

THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.

ON EXISTING PAVEMENT WHICH MAY BE SUPERELEVATED, THE NEW HMA PAVEMENT SHALL BE BUILT WITH THE SAME SUPERELEVATION UNLESS NEW SUPERELEVATION RATES ARE GIVEN ON THE PLANS.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

HMA RESURFACING	112	LBS / SQ YD
-----------------	-----	-------------

HMA MIXTURE TABLE

	HMA WIDENING
PG GRADE	PG64-22
DESIGN AIR VOIDS	4.0% N50
MIXTURE COMPOSITION	IL 19.0
FRICTION AGGREGATE	
DENSITY CONTROL METHOD	CORES

• MATERIAL SHALL BE COMPACTED TO 93.0 - 70.4 PERCENT OF MAXIMUM THEORETICAL DENSITY, EXCEPT THAT WHEN PLACED AS FIRST LIFT ON AN UNIMPROVED SUBGRADE THE MINIMUM PERCENT COMPACTION SHALL BE 92.0 PERCENT. THE MAXIMUM THEORETICAL DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE AS SPECIFIED IN THE QC.QA SPECIFICATION.

•• WHEN RAP EXCEEDS 20 PERCENT, THE VIRGIN ASPHALT BINDER SHALL BE REDUCED BY ONE GRADE (I.E. 25 PERCENT RAP WOULD REQUIRE A VIRGIN ASPHALT BINDER GRADE OF PG64-22 TO REDUCE TO A PG58-22).

DATE: 12-21-10

PREPARED BY: Dave Brownlee CSR
DISTRICT STUDIES & PLANS ENGINEER

EXAMINED BY: Herbert Young (WRP)
DISTRICT CONSTRUCTION ENGINEER

Ray J. Phillips
DISTRICT MATERIALS ENGINEER

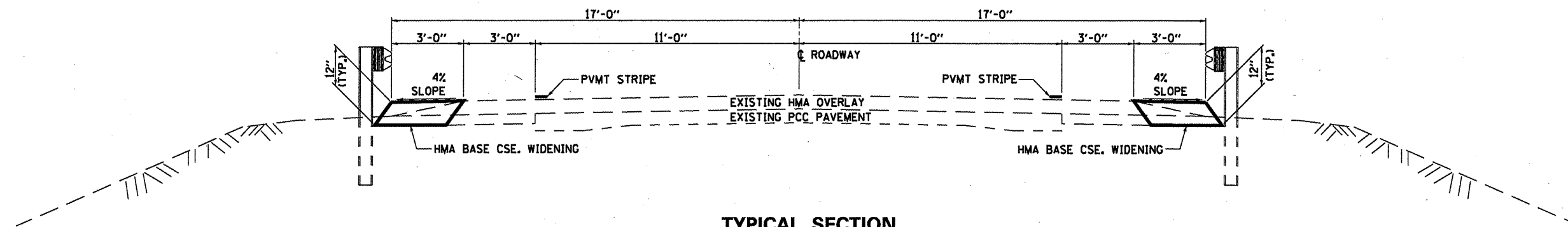
Bruce A. Weeber
DISTRICT OPERATIONS ENGINEER

FILE NAME #	USER NAME # potelyj	DESIGNED - RON WOODSHANK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
os:\pv\work\pwid\potelyj\0143185\CMAIN	1-D366A28-details.dgn	DRAWN - RON WOODSHANK	REVISED -			332	(1-X-BI-1)	IROQUOIS	14	2	
	PLOT SCALE = 100.0059' / IN.	CHECKED -	REVISED -			CONTRACT NO. 66A28		ILLINOIS FED. AID PROJECT			
	PLOT DATE = Dec 21, 2010 - 08:59:38 AM	DATE -	REVISED -			SCALE: _____	SHEET NO. 1 OF 1 SHEETS	STA. _____ TO STA. _____			

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				STATE FUNDS	
				100% STATE	
				STRUCTURE	
				0014	
				RURAL	
20200500	EARTH EXCAVATION (WIDENING)	CU YD	134		134
35600724	HOT-MIX ASPHALT BASE COURSE WIDENING, 12"	SO YD	396		396
50102400	CONCRETE REMOVAL	CU YD	39		39
50300255	CONCRETE SUPERSTRUCTURE	CU YD	39		39
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	7880		7880
50800515	BAR SPLICERS	EACH	46		46
52000110	PREFORMED JOINT STRIP SEAL	FOOT	116		116
58700300	CONCRETE SEALER	SO FT	7305		7305
67100100	MOBILIZATION	L SUM	1		1
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1		1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1		1
70400100	TEMPORARY CONCRETE BARRIER	FOOT	350		350
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	350		350
* 78001110	PAINT PAVEMENT MARKING - LINE 4 "	FOOT	802		802
* 78001130	PAINT PAVEMENT MARKING - LINE 6 "	FOOT	190		190
78300100	PAVEMENT MARKING REMOVAL	SO FT	645		645
X0322194	POLYMER MODIFIED PORTLAND CEMENT MORTAR	SO FT	6		6
X7200201	WIDTH RESTRICTION SIGNING	L SUM	1		1
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2		2
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2		2
Z0064600	SELECTIVE CLEARING	ACRE	0.4		0.4

* SPECIALITY ITEMS

Rev.

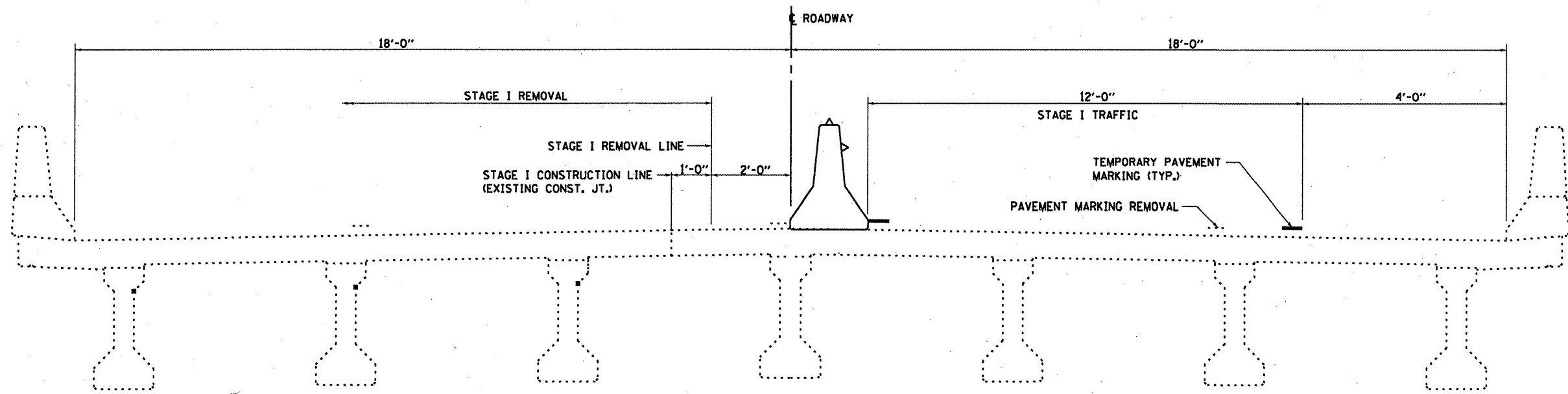


TYPICAL SECTION

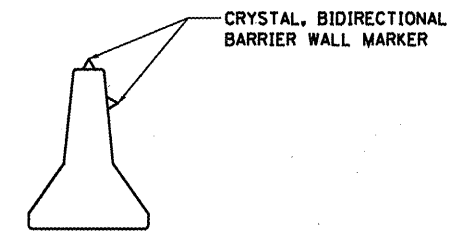
- STA. 63+67 TO STA. 66+34 RT.
- STA. 63+67 TO STA. 66+69 LT.
- STA. 68+03 TO STA. 71+28 RT.
- STA. 68+38 TO STA. 71+28 LT.

• AS MEASURED ALONG EDGE OF PAVEMENT

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ai\pwork\pwork\woodshankr1\08143165\MAIN1-0368A28-details.dgn	DRAWN - RON WOODSHANK	REVISED -	332			(1-X-B)-1	IROQUOIS	14	4	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 66A28							
PLOT DATE = Dec 21, 2010 - 11:02:33 AM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
				SCALE: _____	SHEET NO. 1 OF 1 SHEETS	STA. _____ TO STA. _____				

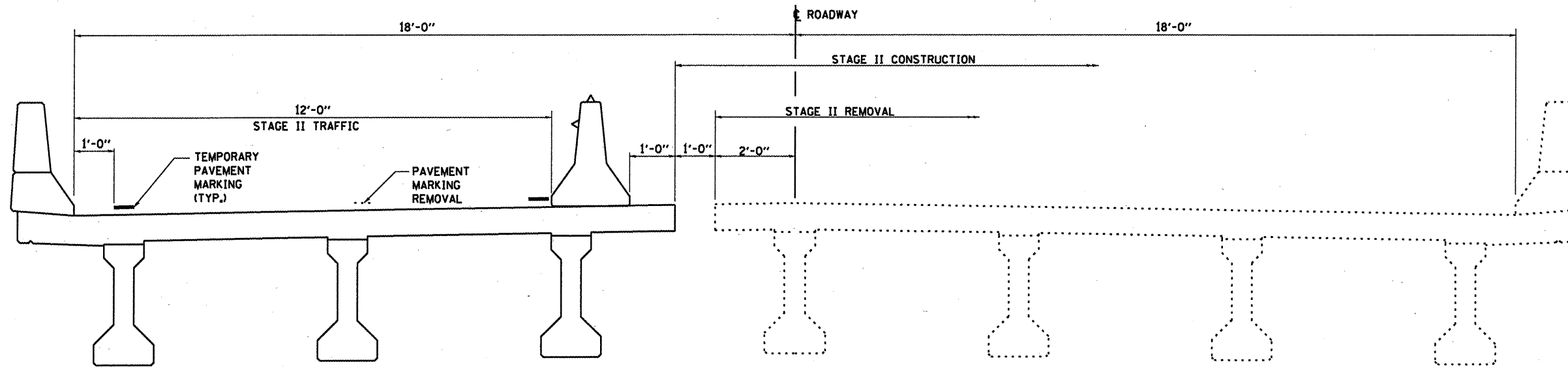


**CROSS SECTION
STAGE I**
LOOKING SOUTH



TEMPORARY CONCRETE BARRIER

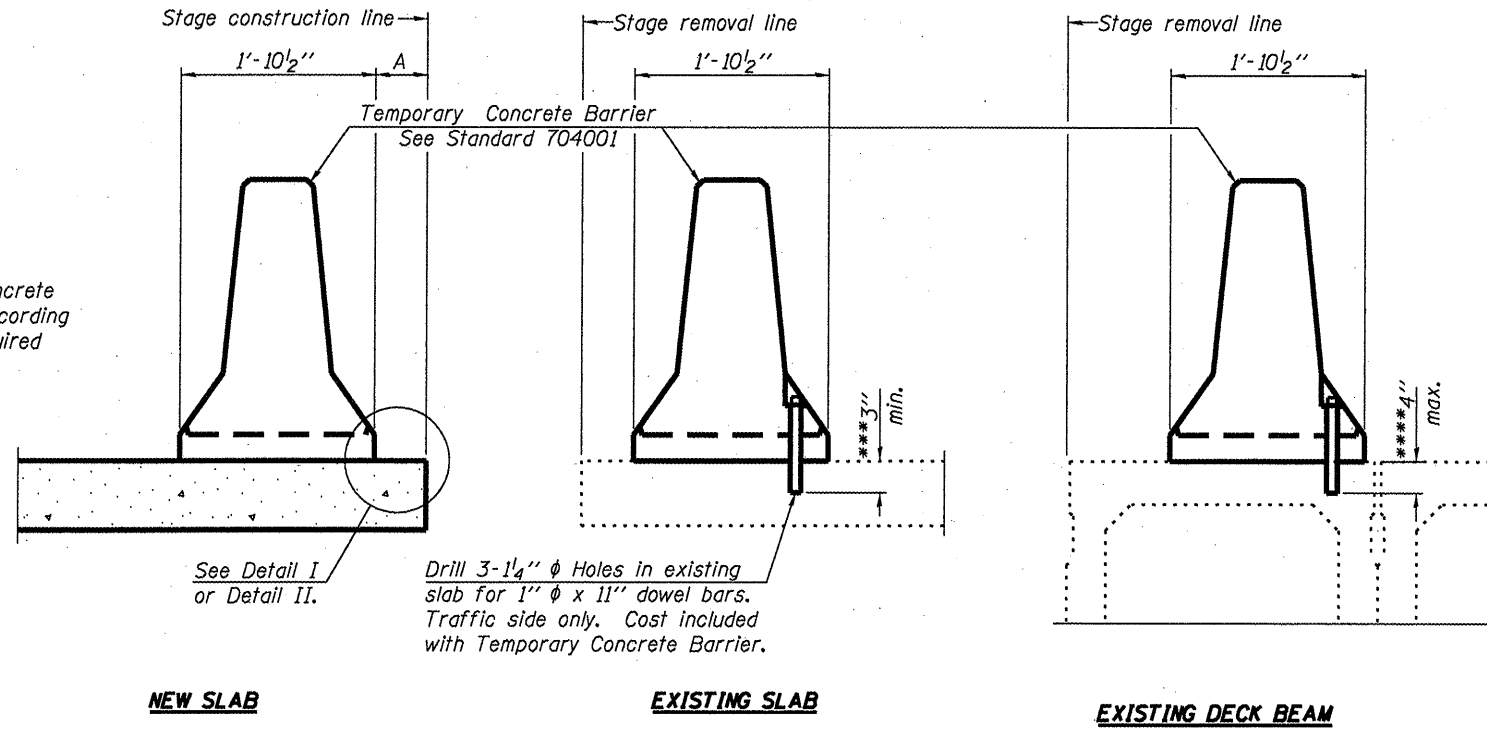
NOTE: WIDTH RESTRICTION SIGNING REQUIRED FOR STAGE II CONSTRUCTION



**CROSS SECTION
STAGE II**
LOOKING SOUTH

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cd:\pw_work\psidot\woodshankr1\d0143168\	MAIN11-0366A28-details.dgn	DRAWN - RON WOODSHANK	REVISED -			332	(1-X-BN-1)	IROQUOIS	14	5	
PLOT SCALE = 1/8" = 1'-0"		CHECKED -	REVISED -			SCALE: _____ SHEET NO. 1 OF 1 SHEETS STA. _____ TO STA. _____					
PLOT DATE = Dec 21, 2010 - 11:02:41 AM		DATE -	REVISED -			ILLINOIS FED. AID PROJECT CONTRACT NO. 66A28					

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

NOTES

Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel \bar{P} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

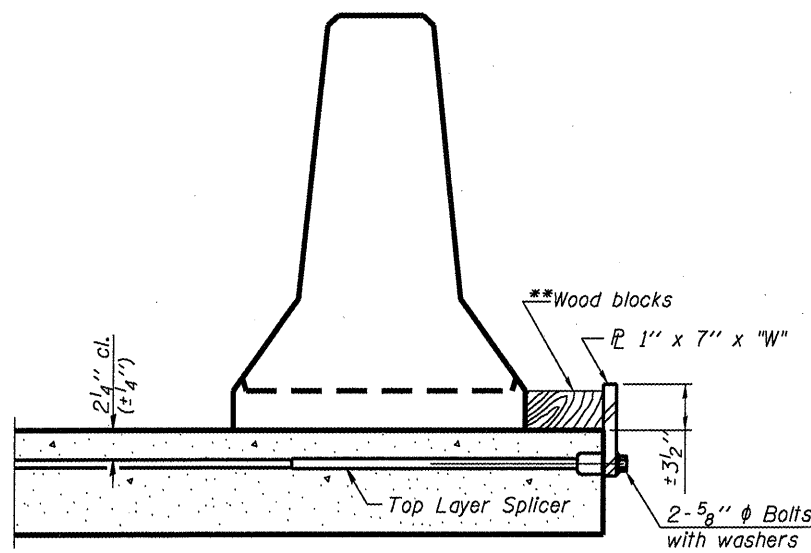
Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel \bar{P} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

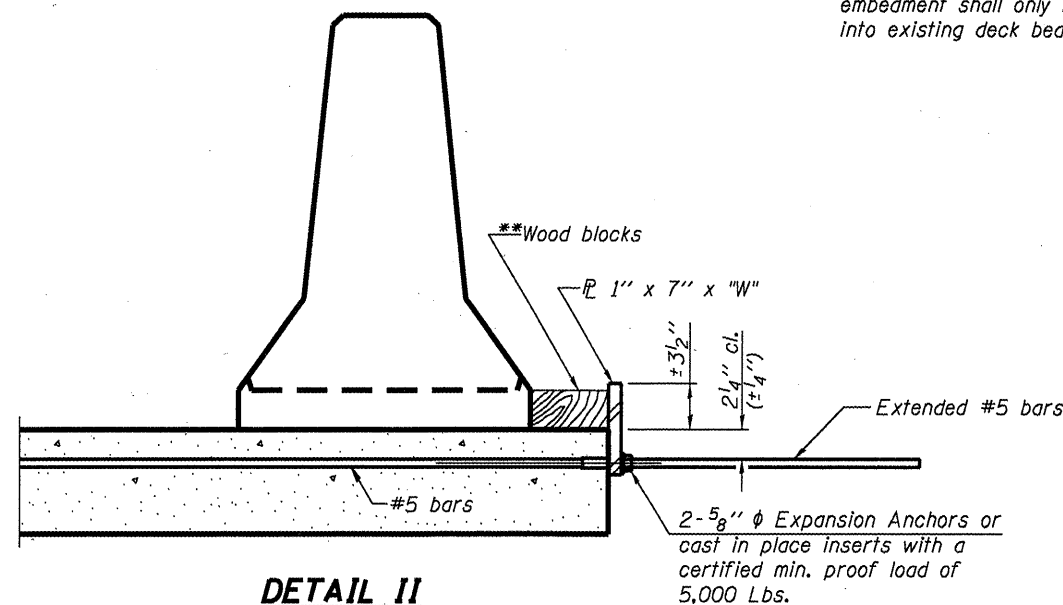
SECTIONS THRU SLAB OR DECK BEAM

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

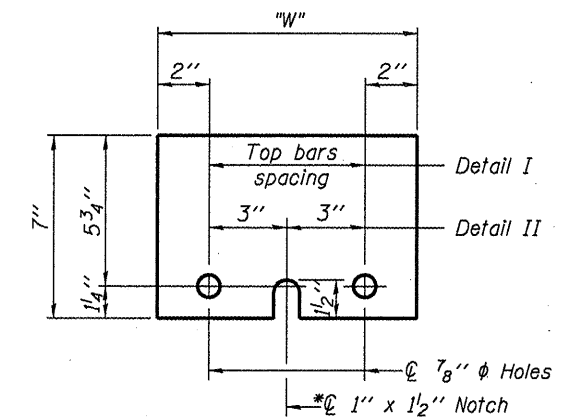
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER P 1" x 7" x "W"

* Required only with Detail II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

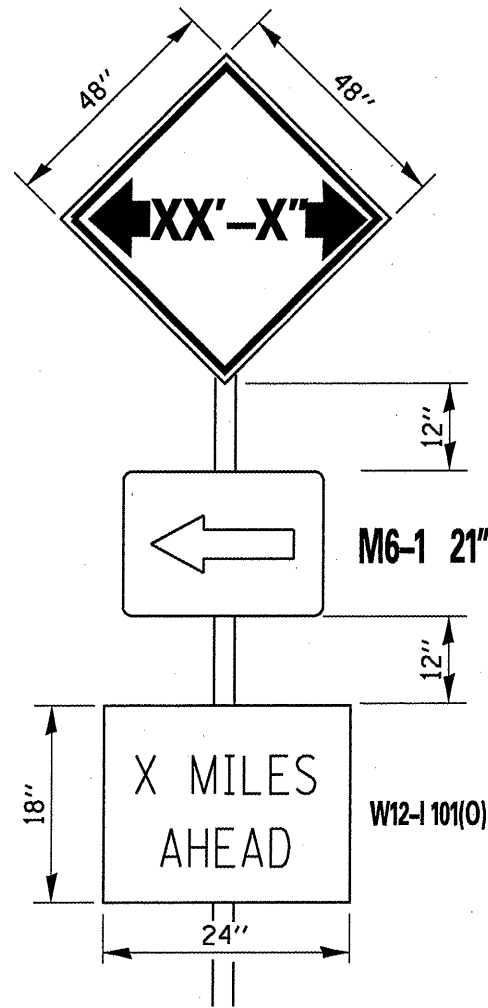
"W" = Top bars spacing + 4"

R-27

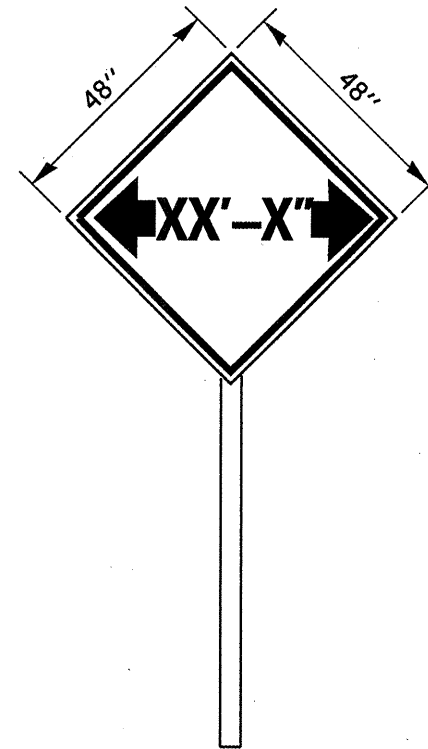
7-1-10

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION	F.A.P. RTE. 332	SECTION (I-X-B1)-1	COUNTY IROQUOIS	TOTAL SHEETS 14	SHEET NO. 6		
CONTRACT NO. 66A28	MAIN1-D386A28-detail1.dgn	DRAWN - RON WOODSHANK	REVISED -			SCALE: _____	SHEET NO. 1 OF 1 SHEETS	STA. _____	TO STA. _____	ILLINOIS FED. AID PROJECT		
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -									
	PLOT DATE = Dec 21, 2010 - 11:02:52 AM	DATE -	REVISED -									

WIDTH RESTRICTION SIGNING TABLE				
NO. OF SIGNS	TYPE OF SIGN	SIGN DESIGNATION	LOCATION	WIDTH RESTRICTION & DISTANCE
1	WIDTH RESTRICTION W12-1-101-(O) M6-1	(A)	ON US RTE. 24 NE CORNER INTERSECTION WITH IL RTE. 1	10'-6" 1.1 MILES
1	WIDTH RESTRICTION W12-1-101-(O) M6-1	(B)	ON US RTE. 24 SW CORNER INTERSECTION WITH IL RTE. 1	10'-6" 1.6 MILE
1	WIDTH RESTRICTION W12-1-101-(O) M6-1	(C)	WITHIN TRAFFIC CONTROL AND PROTECTION STANDARD 701321	10'-6" 1.6 MILE
1	WIDTH RESTRICTION	(D)	WITHIN TRAFFIC CONTROL AND PROTECTION STANDARD 701321	10'-6" 4.4 MILE
1	WIDTH RESTRICTION	(E)	ON IL RTE. 9 NE CORNER INTERSECTION WITH IL RTE. 1	10'-6" 4.4 MILE
1	WIDTH RESTRICTION W12-1-101-(O) M6-1	(C)	ON IL RTE. 1 NE CORNER INTERSECTION WITH IL RTE. 9	10'-6" 1.6 MILE
1	WIDTH RESTRICTION W12-1-101-(O) M6-1	(D)	ON IL RTE. 9 SW CORNER INTERSECTION WITH IL RTE. 1	10'-6" 4.4 MILE
1	WIDTH RESTRICTION W12-1-101-(O) M6-1	(E)	ON US RTE. 24 NE CORNER INTERSECTION WITH IL RTE. 1	10'-6" 4.4 MILE

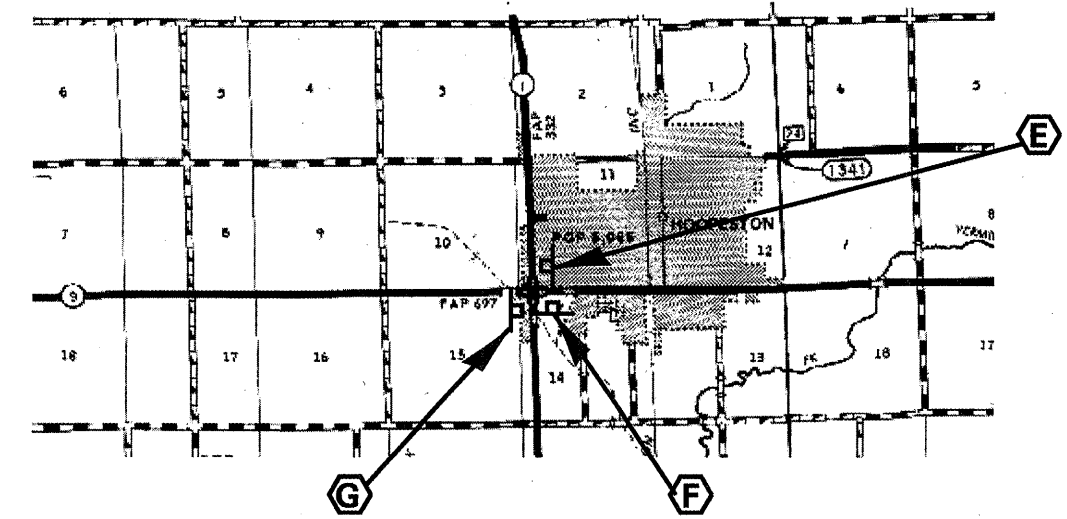
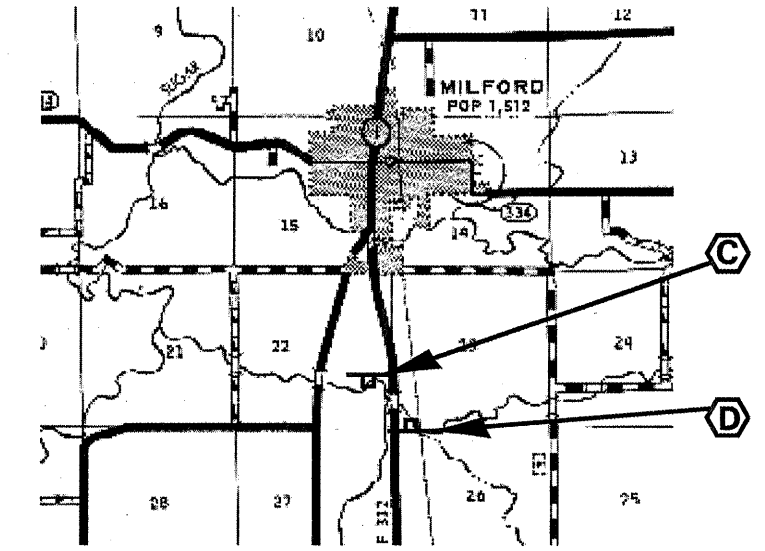
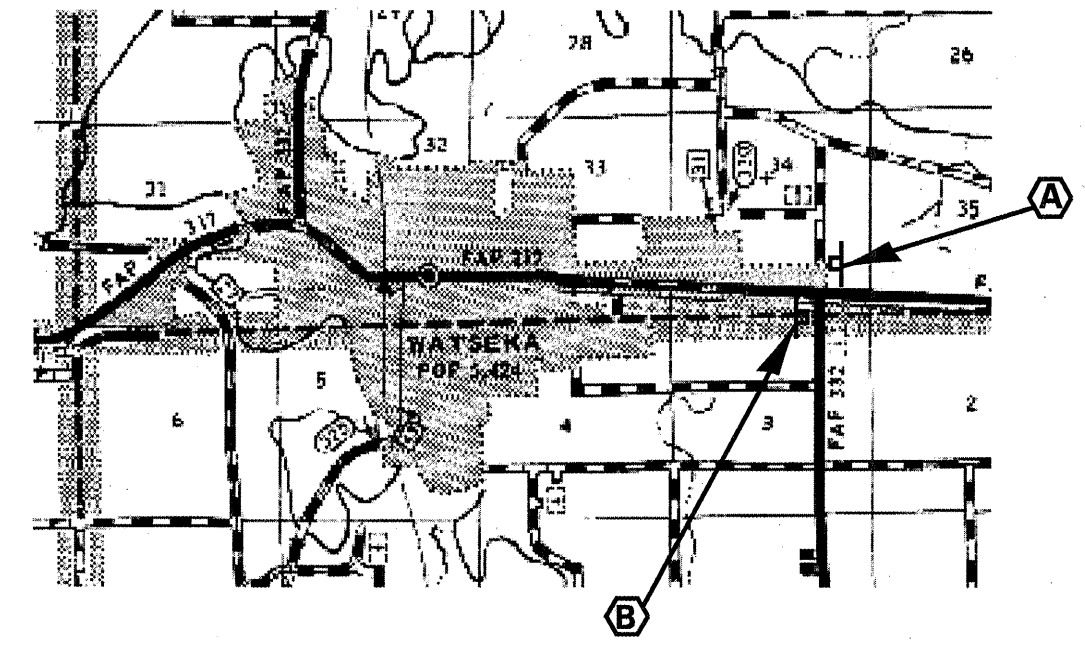


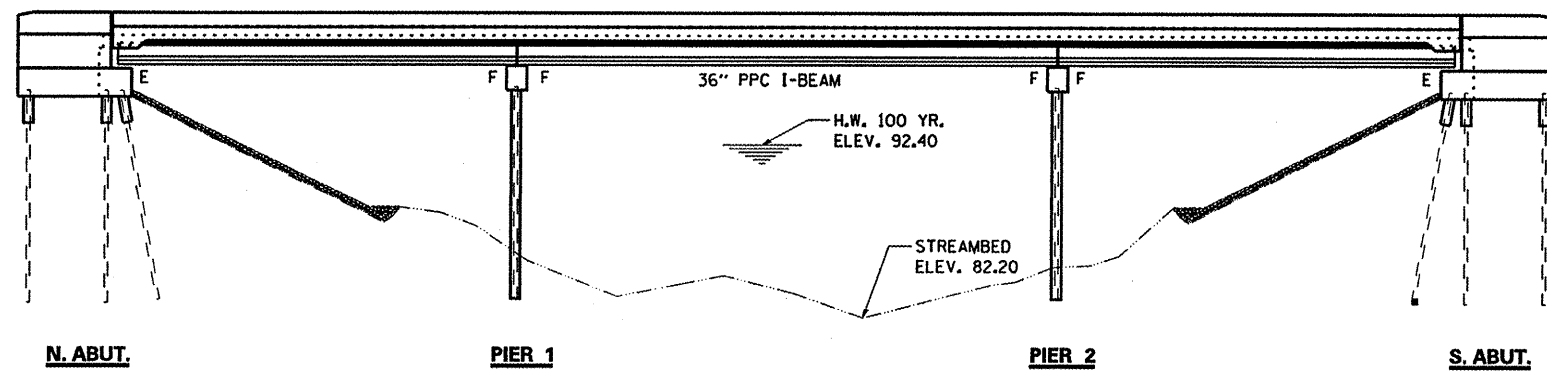
12" D BLACK LETTERING
WITH ORANGE BACKGROUND



NOTES

WIDTH RESTRICTION SIGNING SHALL BE REQUIRED FOR "STAGE II" TRAFFIC ONLY.
 SIGNS ARE TO BE POST MOUNTED AS SHOWN ELSEWHERE IN THE PLANS.
 THE ENGINEER WILL NOTIFY DISTRICT 3 BUREAU OF OPERATIONS 14 CALENDAR DAYS PRIOR TO INSTALLING ANY TRAFFIC CONTROL DEVICES THAT WILL RESTRICT THE PAVEMENT WIDTH. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE ENGINEER TO MEET THIS REQUIREMENT.





ELEVATION

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY FURNISHED BASED UPON THE UNIT BID PRICE FOR THE WORK.

THE DECK SURFACE SHALL HAVE ITS FINAL FINISH TINED ACCORDING TO ARTICLE 420.09(e)(1) OF THE STANDARD SPECIFICATIONS.

REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 705 GR. 60.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

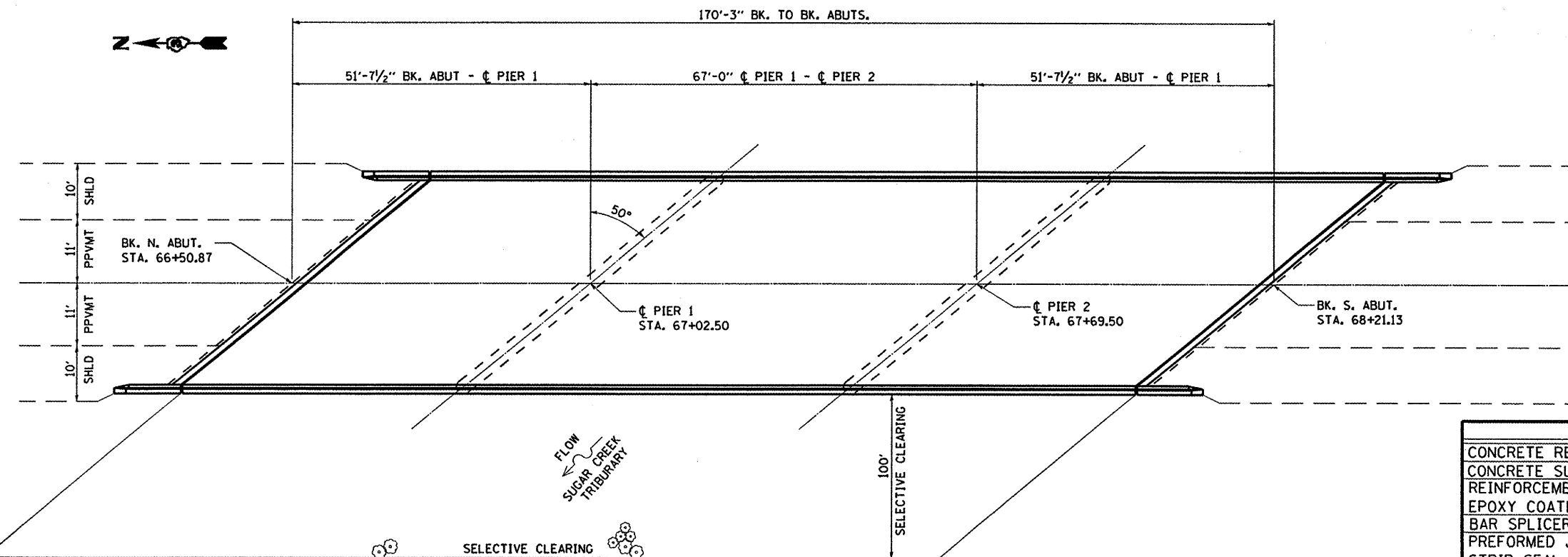
JOINT OPENING SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50 DEGREES F.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

THE CONTRACTOR WILL BE REQUIRED TO MARK ON THE TOP OF THE CONCRETE DECK THE LOCATIONS OF THE TOP FLANGE OF ALL CONCRETE BEAMS PRIOR TO ANY REMOVAL OF THE CONCRETE DECK. SAW CUTTING DIRECTLY OVER TOP OF BEAM FLANGES IS NOT PERMITTED.



David Carl Puze 2/7/11
Expires 11/30/2012



PLAN
SN 038-0200

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	N ABUT	S ABUT	TOTAL
CONCRETE REMOVAL	CU YD		19.5	19.5	39
CONCRETE SUPERSTRUCTURE	CU YD		19.5	19.5	39
REINFORCEMENT BARS	POUND		3940	3940	7880
EPOXY COATED					
BAR SPLICERS	EACH		23	23	46
PREFORMED JOINT	FOOT		58	58	116
STRIP SEAL					
CONCRETE SEALER	SQ FT	7305			7305
POLYMER MODIFIED PORTLAND CEMENT MORTAR	SQ FT	6			6

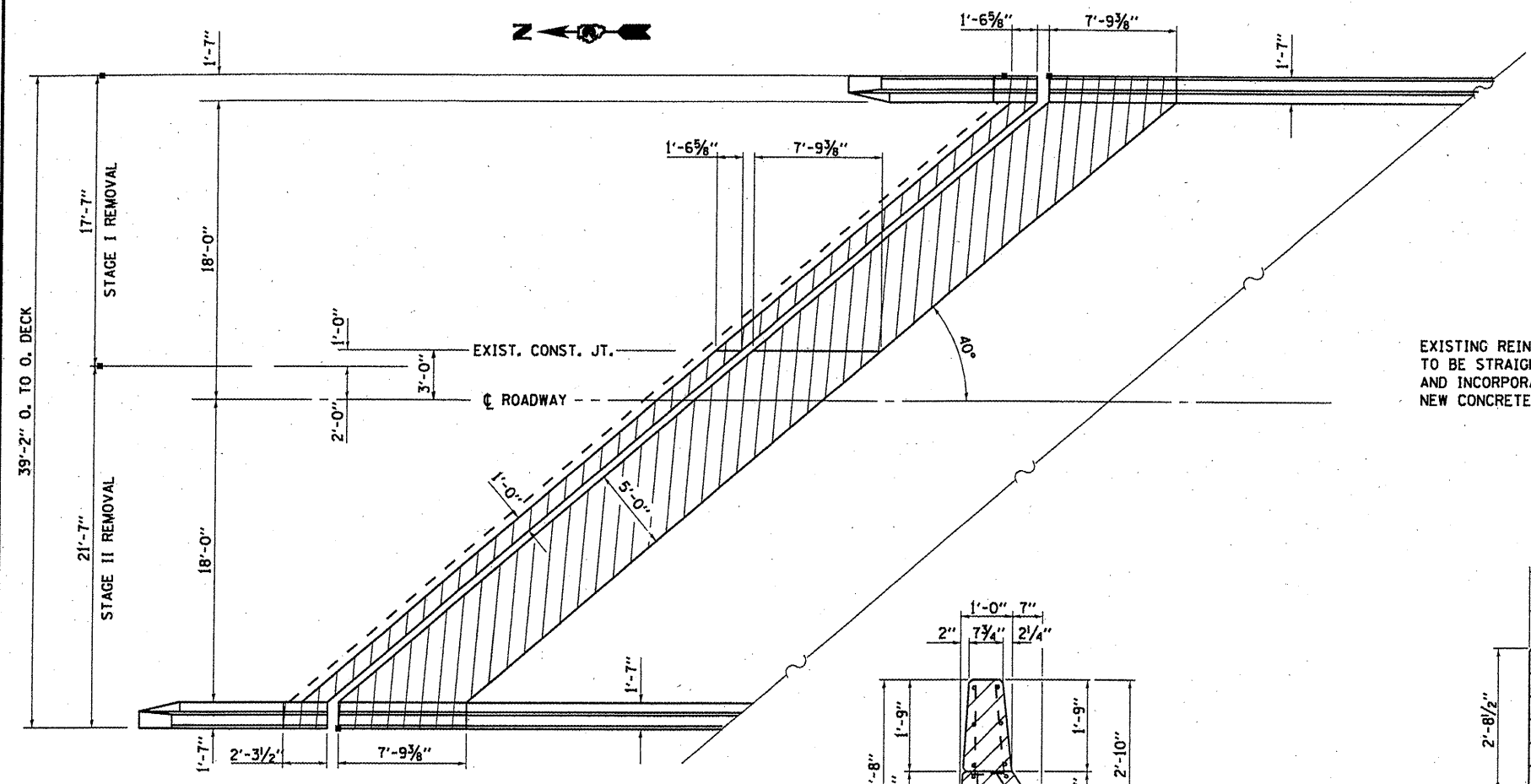
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PLOT SCALE = 100.0000 / IN.		CHECKED -	REVISED -
PLOT DATE = Feb 07, 2011 - 09:29:55 AM		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN AND ELEVATION
FOR S.N. 038-0200

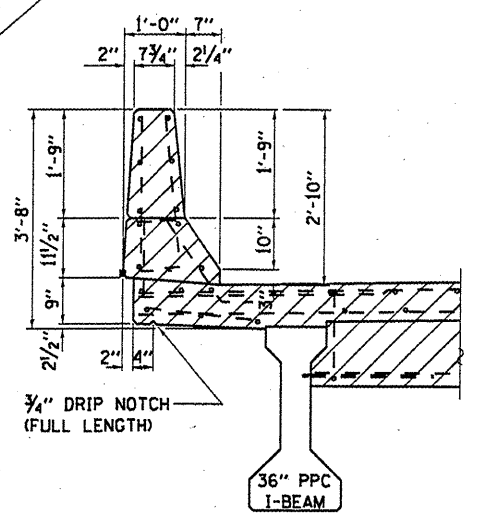
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	(1-X-B)-1	IROQUOIS	14	8
CONTRACT NO. 66A28			ILLINOIS FED. AID PROJECT	

SCALE: SHEET NO. 1 OF 7 SHEETS STA. TO STA.

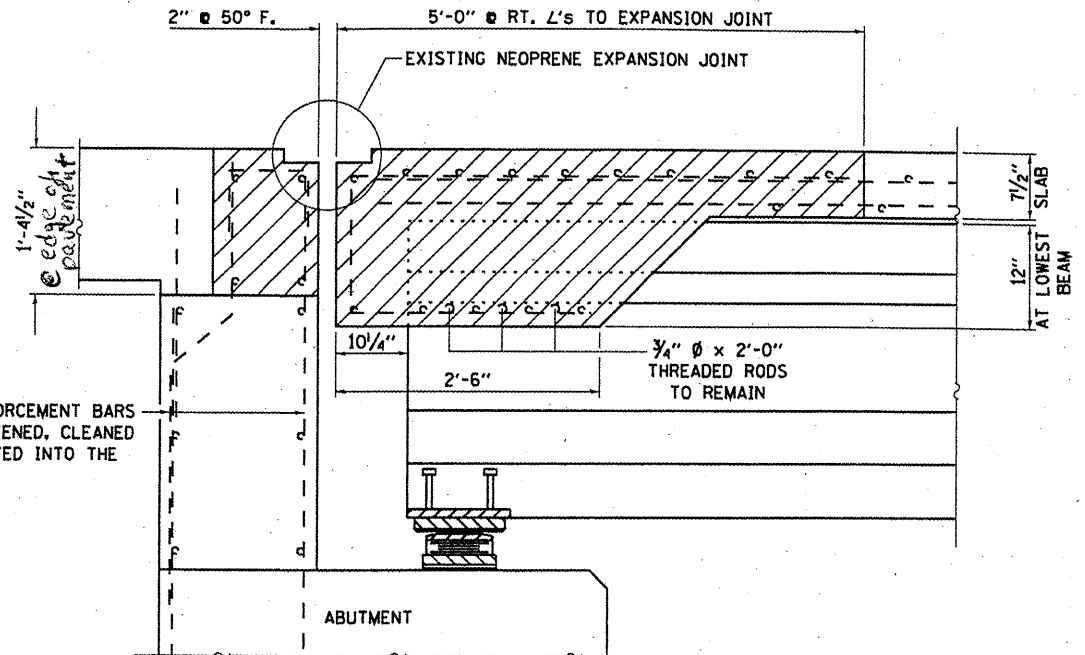


PARTIAL PLAN
N. ABUT. SHOWN
S. ABUT. SIMILAR

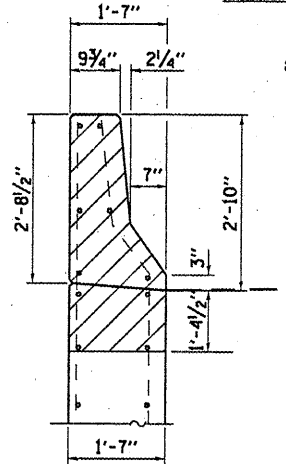
MIN. BAR LAP
#4 - 1'-9"
#5 - 2'-2"
#8 - 4'-6"



SECTION AT PARAPET



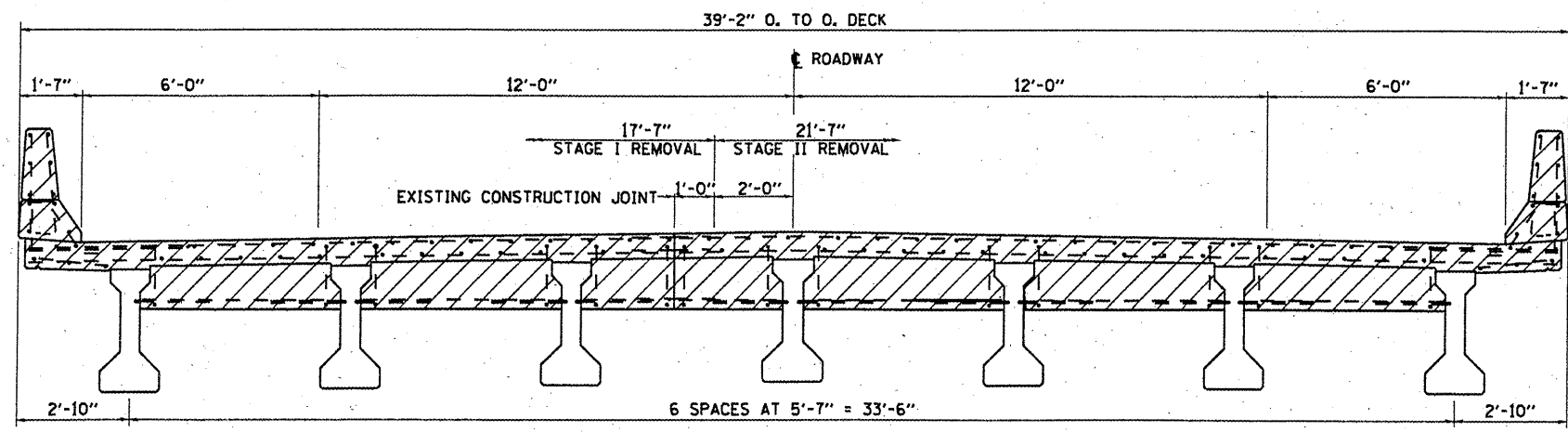
ABUTMENT SECTION



SECTION AT WINGWALL

NOTES

- * HATCHED AREA REPRESENTS CONCRETE REMOVAL LIMITS
- EXISTING 3/4" Ø x 2'-0" THREADED RODS SHALL BE CLEANED AND INCORPORATED INTO THE NEW CONCRETE.
- COST FOR REINFORCEMENT REMOVAL AND EXPANSION JOINT REMOVAL SHALL BE INCLUDED IN THE COST OF "CONCRETE REMOVAL".
- * CONTRACTOR MUST EXERCISE EXTREME CARE DURING CONCRETE REMOVAL SO AS NOT TO DAMAGE THE PPC I BEAMS. ANY DAMAGE TO THE BEAMS IS TO BE REPAIRED AT THE CONTRACTOR'S EXPENSE.



CROSS SECTION AT ABUTMENTS
LOOKING SOUTH
N. ABUTMENT SHOWN - S. ABUTMENT SIMILAR

TOTAL BILL OF MATERIAL

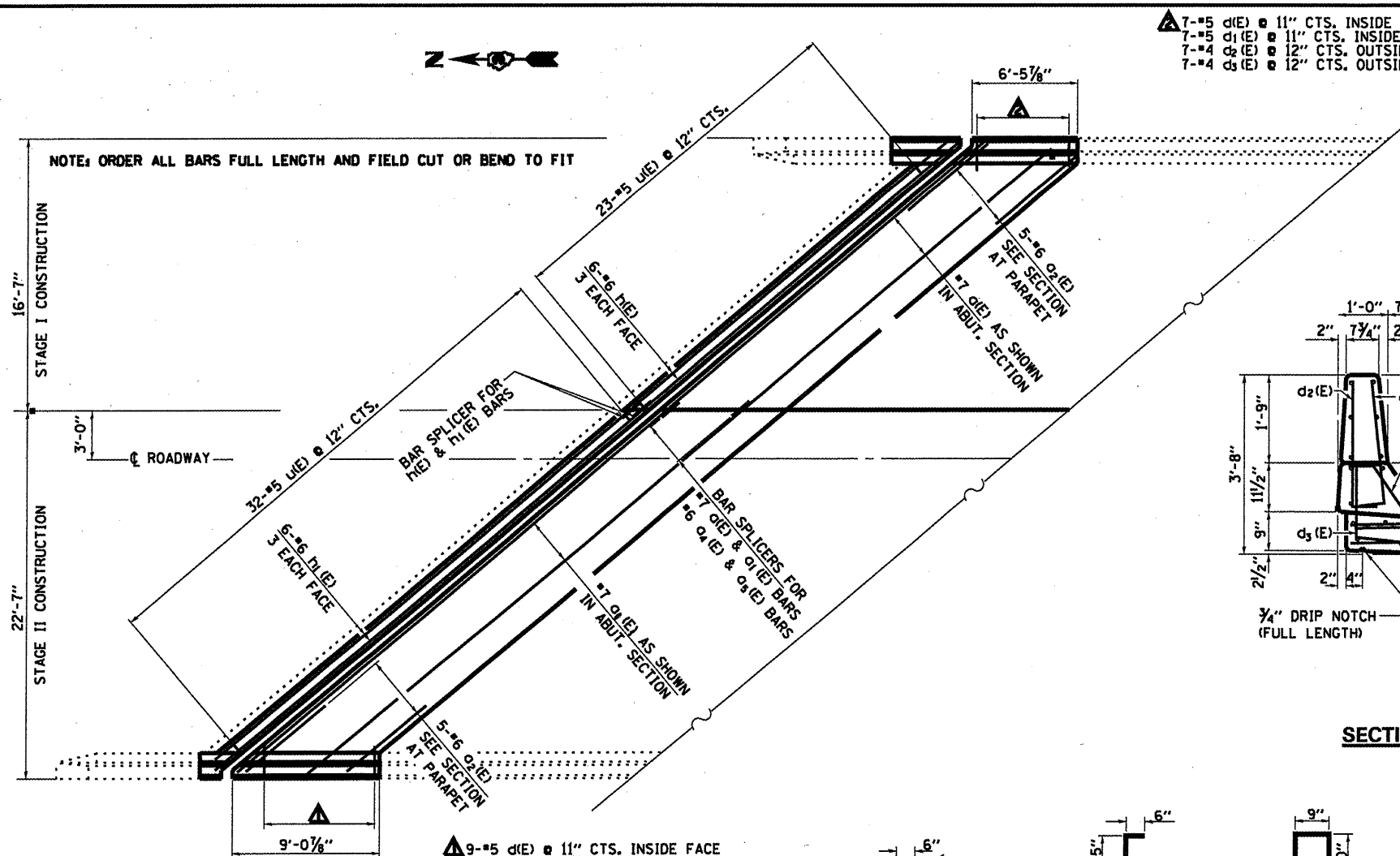
ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	CU YD	39

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PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = Dec 21, 2010 - 11:03:32 AM		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING STRUCTURE DETAILS
SCALE: _____ SHEET NO. 2 OF 7 SHEETS STA. _____ TO STA. _____

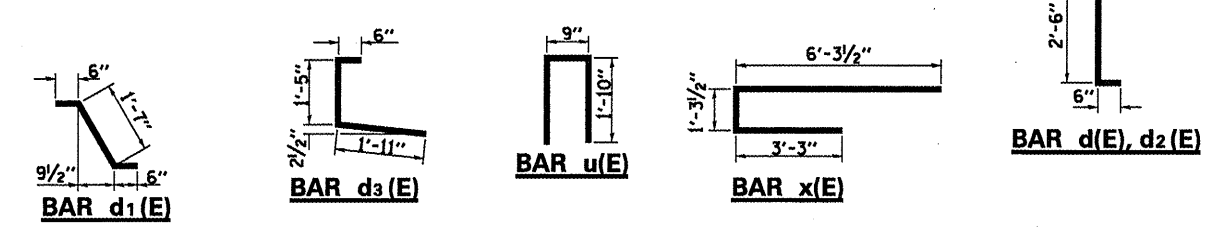
F.A.P. RTE. 332	SECTION (I-X-BI)-1	COUNTY IROQUOIS	TOTAL SHEETS 14	SHEET NO. 9
CONTRACT NO. 66A28				ILLINOIS FED. AID PROJECT



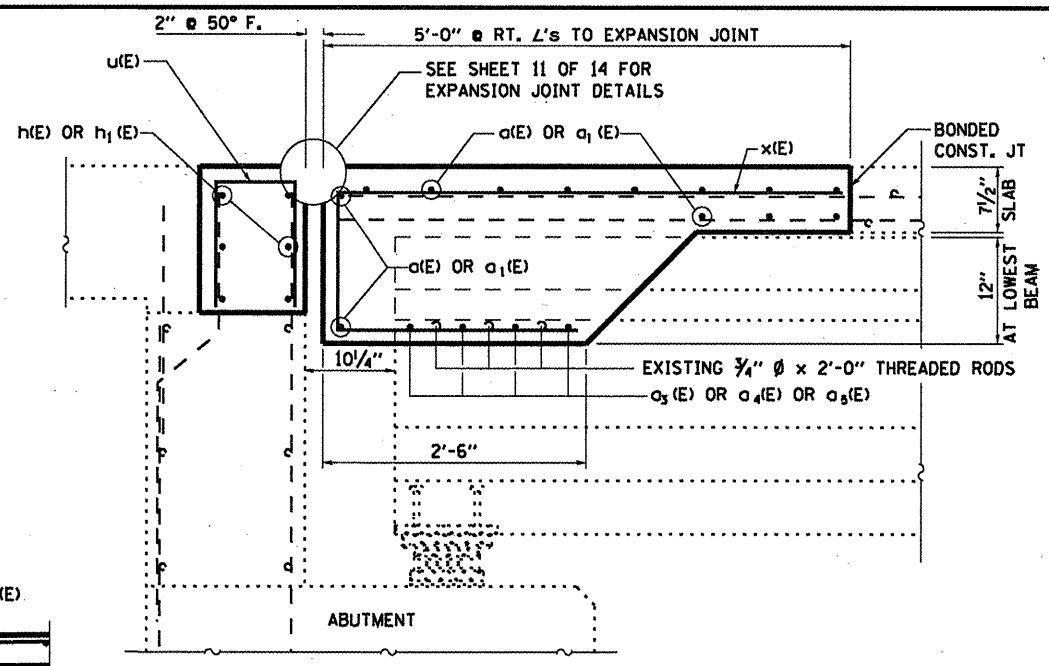
PARTIAL PLAN
N. ABUT. SHOWN - S. ABUT. SIMILAR

7-#5 d(E) @ 11" CTS. INSIDE FACE
7-#5 d1(E) @ 11" CTS. INSIDE FACE
7-#4 d3(E) @ 12" CTS. OUTSIDE FACE
7-#4 d5(E) @ 12" CTS. OUTSIDE FACE

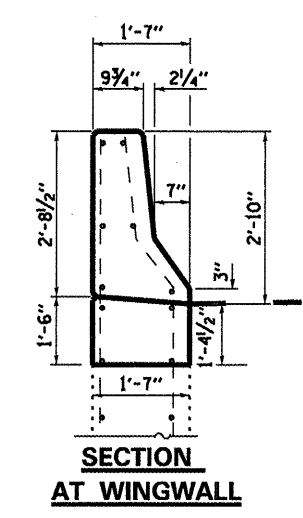
9-#5 d(E) @ 11" CTS. INSIDE FACE
9-#5 d1(E) @ 11" CTS. INSIDE FACE
9-#4 d3(E) @ 12" CTS. OUTSIDE FACE
9-#4 d5(E) @ 12" CTS. OUTSIDE FACE



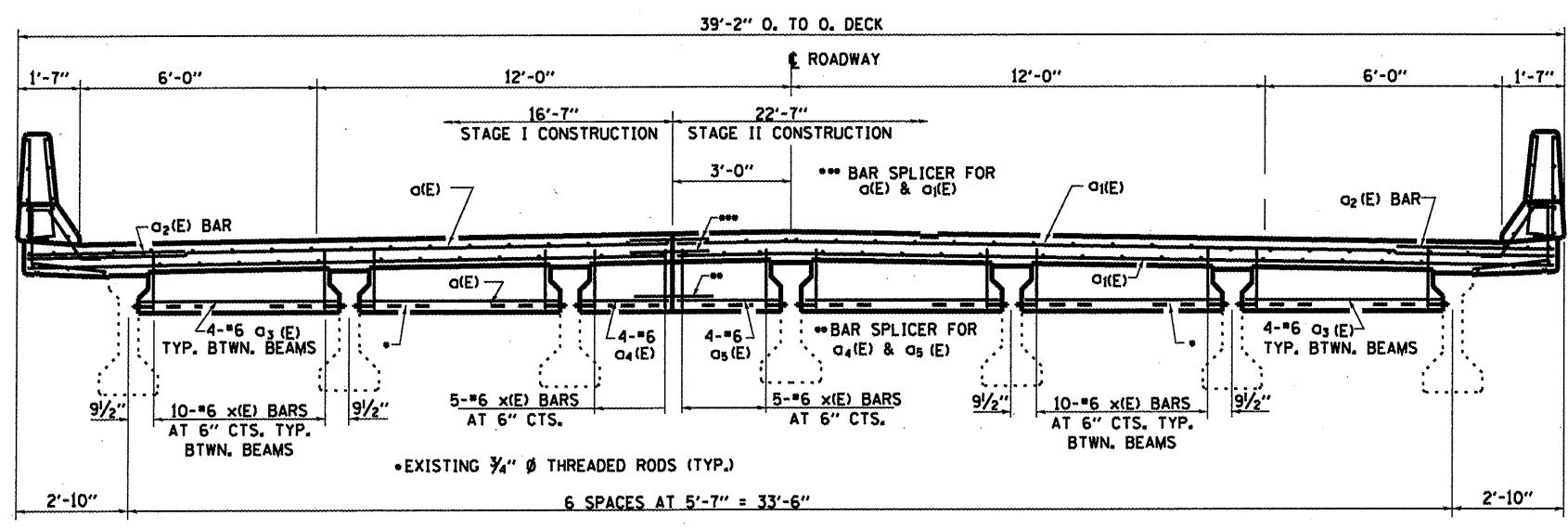
SECTION AT PARAPET



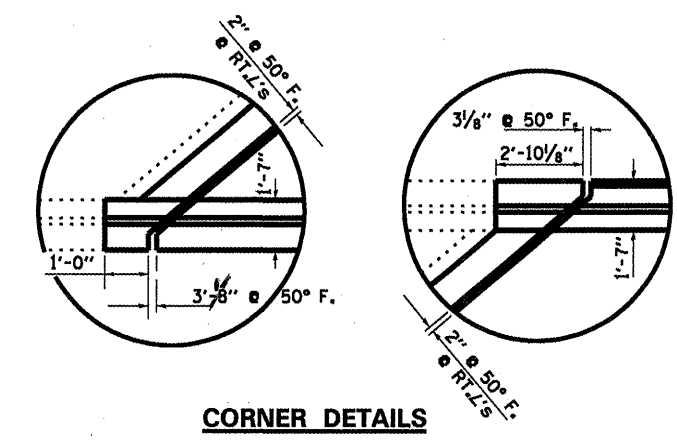
ABUTMENT SECTION



SECTION AT WINGWALL



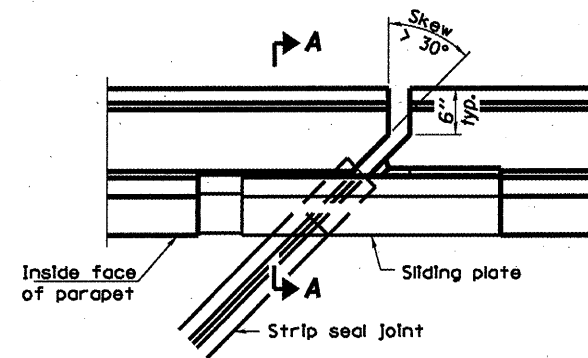
CROSS SECTION AT ABUTMENTS
LOOKING SOUTH
N. ABUTMENT SHOWN - S. ABUTMENT SIMILAR



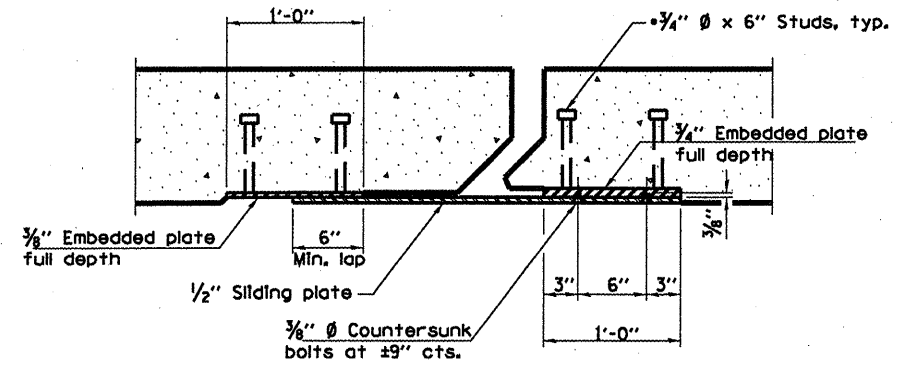
CORNER DETAILS

BILL OF MATERIAL

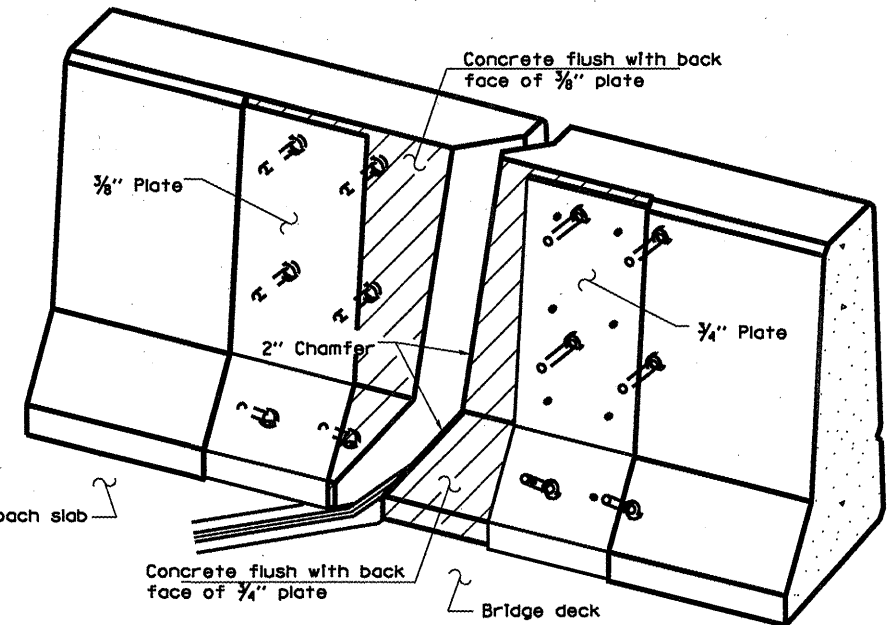
Bar	No.	Size	Length	Shape
a(E)	26	#7	24'-1"	—
a1(E)	26	#7	33'-5"	—
a2(E)	20	#6	6'-3"	—
a3(E)	40	#6	7'-6"	—
a4(E)	8	#6	2'-10"	—
a5(E)	8	#6	3'-6"	—
d(E)	32	#5	3'-0"	—
d1(E)	32	#5	2'-7"	—
d2(E)	32	#4	3'-0"	—
d3(E)	32	#4	3'-10"	—
h(E)	12	#6	25'-4"	—
h1(E)	12	#6	33'-2"	—
u(E)	110	#5	4'-5"	—
x(E)	120	#6	10'-10"	—
Reinforcement Bars, Epoxy Coated			Pound	7880
Concrete Superstructure			Cu. Yds.	39



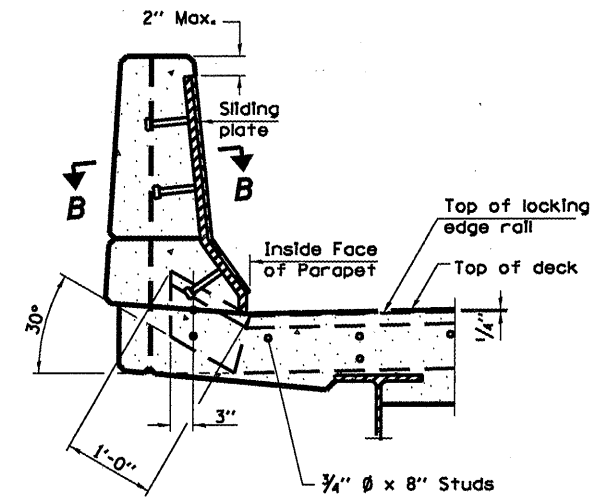
PLAN
(For skews > 30°)
Showing point block



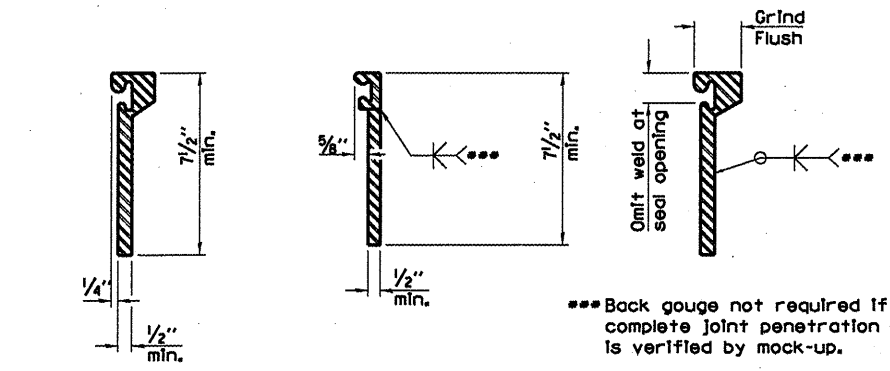
SECTION B-B



TRIMETRIC VIEW
(Showing back plates only)



SECTION A-A

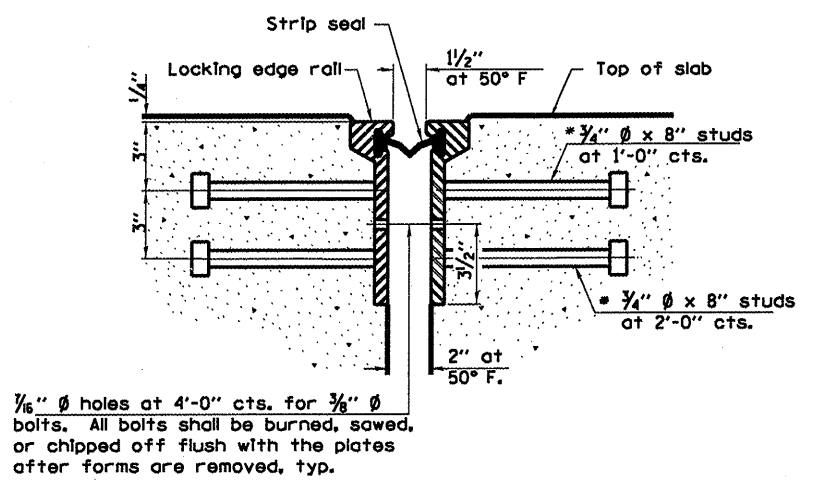


ROLLED EXTRUDED RAIL **WELDED RAIL**

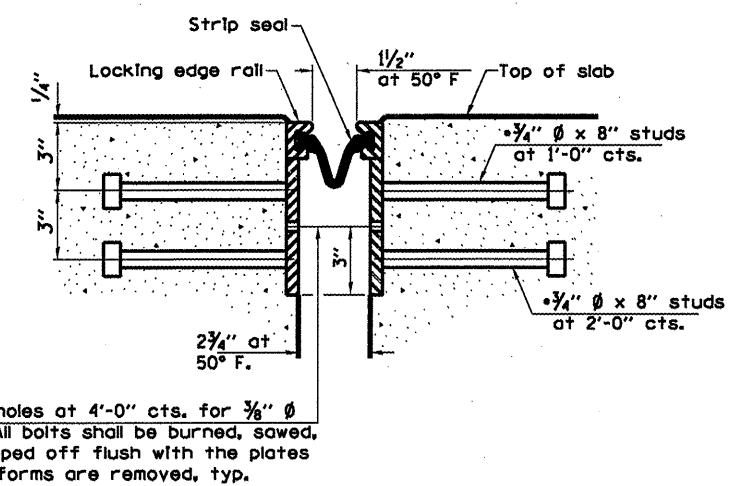
LOCKING EDGE RAIL SPLICE
The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
The manufacturer's recommended installation methods shall be followed.
The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments at stage lines shall be 1/16", sealed with a suitable sealant.
Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.



SECTION THRU ROLLED RAIL JOINT



SECTION THRU WELDED RAIL JOINT

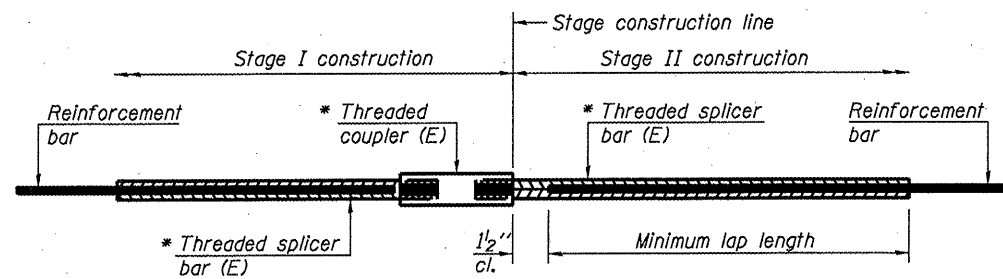
1/16" diameter holes at 4'-0" cts. for 3/8" diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

1/16" diameter holes at 4'-0" cts. for 3/8" diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
PREFORMED JOINT STRIP SEAL	FOOT.	116



STANDARD BAR SPLICER ASSEMBLY

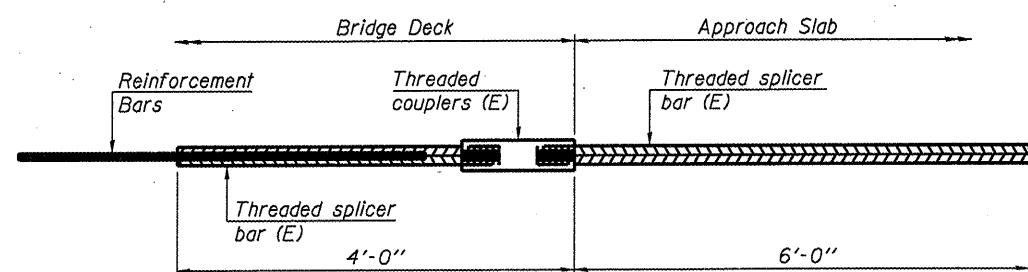
Minimum Lap Lengths					
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

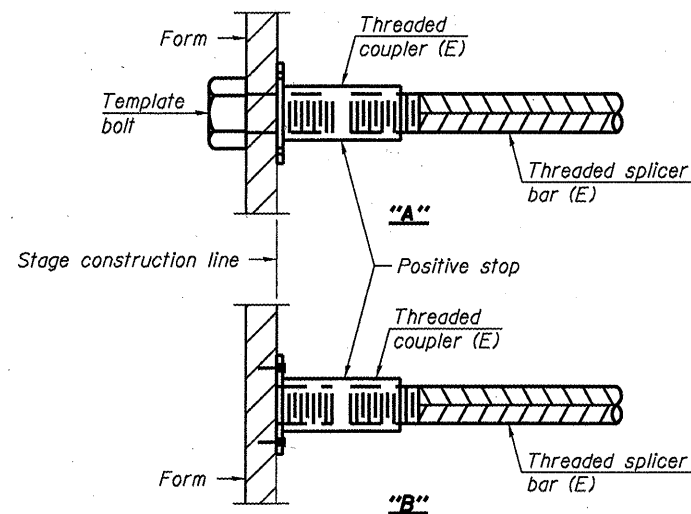
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
NORTH END- DECK	#7	13	3
	#6	4	3
NORTH END-BLOCKOUT	#6	6	3
SOUTH END-DECK	#7	13	3
	#6	4	3
SOUTH END DAM	#6	6	3
TOTALS		46	



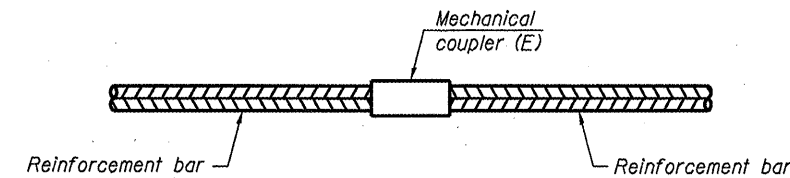
BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



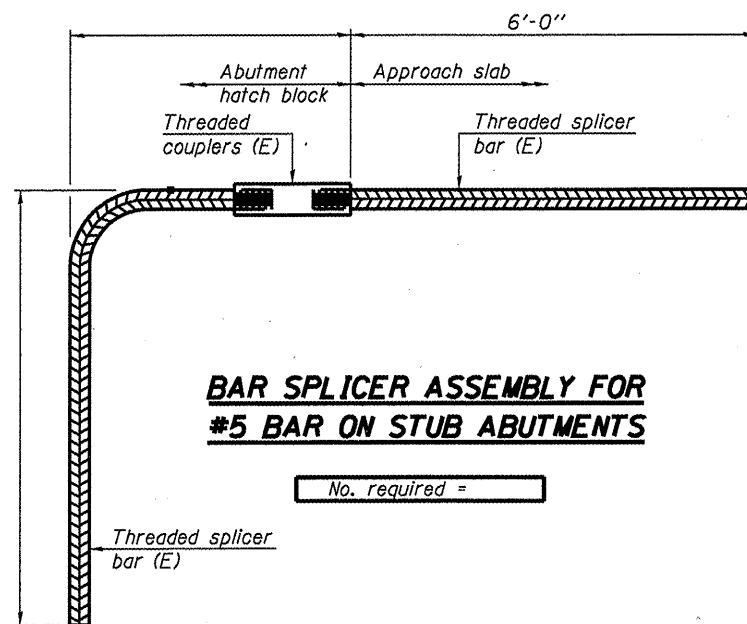
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

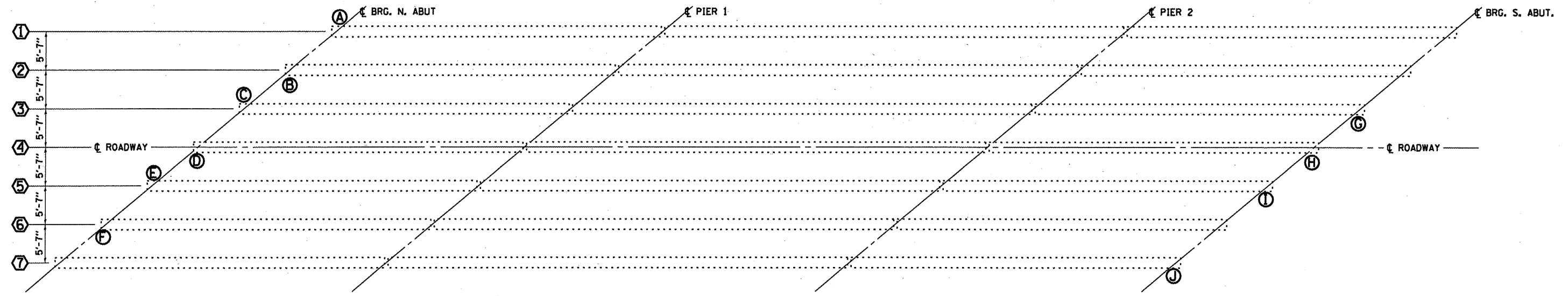


BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

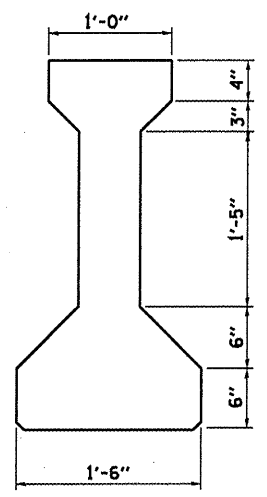
No. required =

NOTES

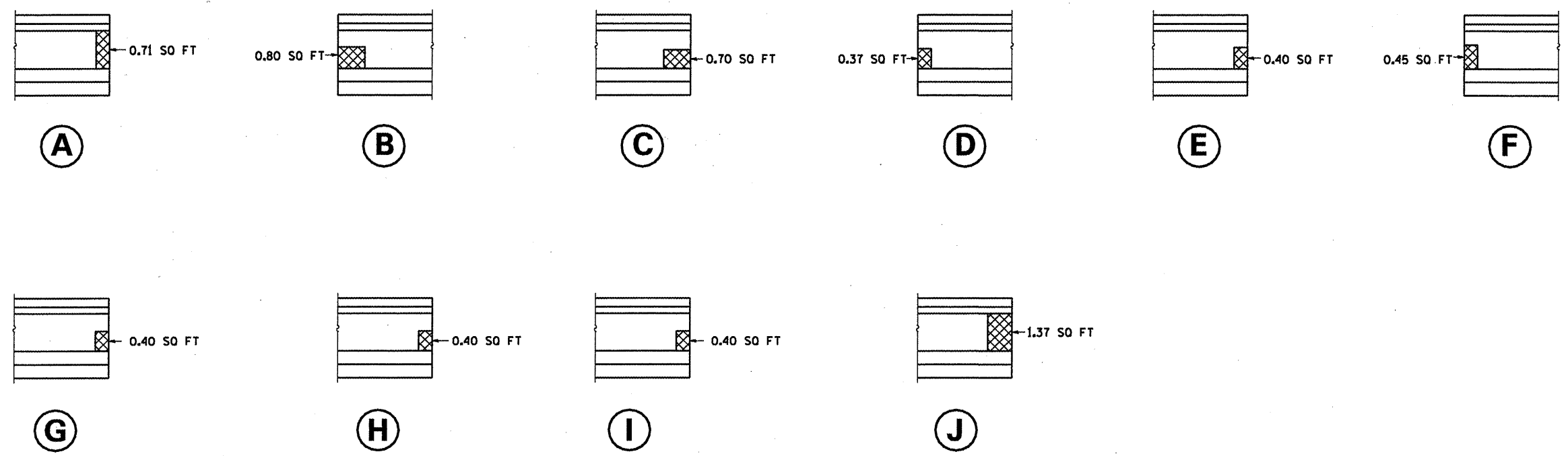
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See special provision for Mechanical Splicers.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.



FRAMING PLAN

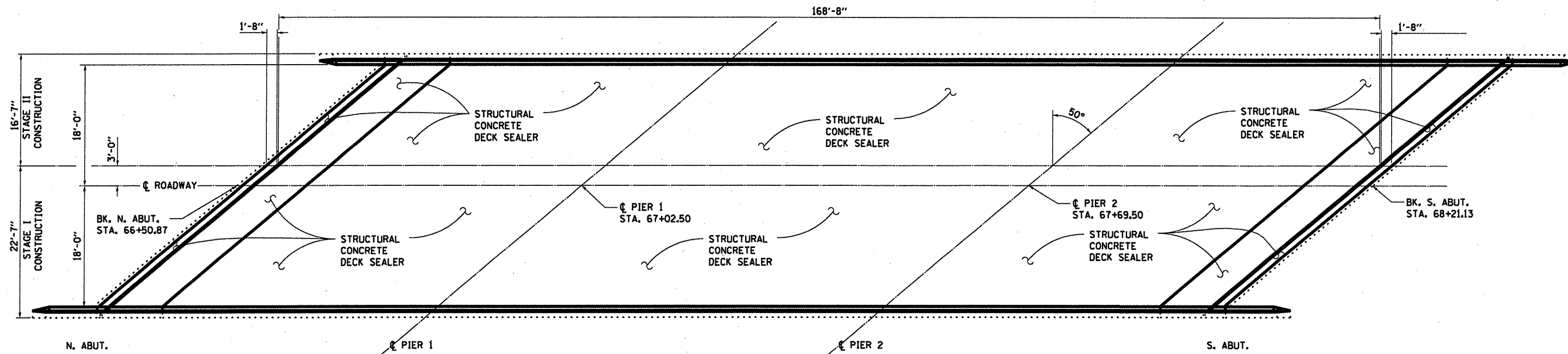


I-BEAM SECTION

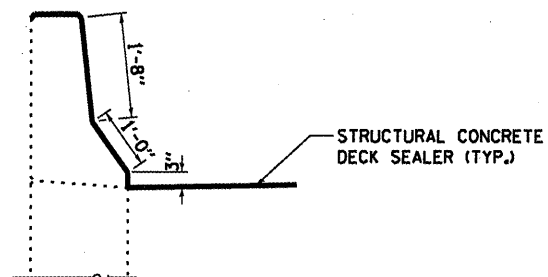


TOTAL BILL OF MATERIAL

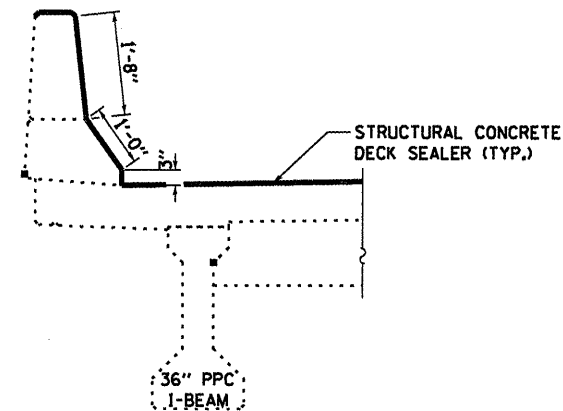
ITEM	UNIT	QUANTITY
POLYMER MODIFIED PORTLAND CEMENT MORTAR	SQ. FT.	6



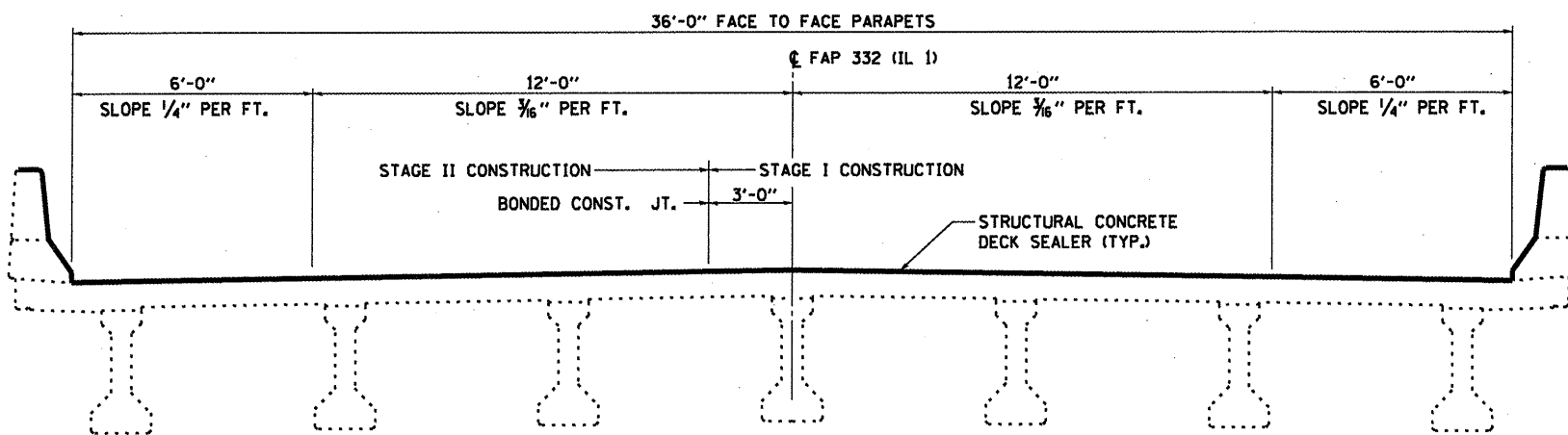
EXISTING PAVEMENT MARKINGS SHALL BE TEMPORARILY COVERED PRIOR TO APPLICATION OF THE DECK SURFACE TREATMENT, TO PREVENT THE MATERIAL FROM BEING APPLIED TO THE MARKINGS. THE TEMPORARY COVERING SHALL BE REMOVED AFTER APPLICATION OF THE DECK SURFACE TREATMENT AND PRIOR TO OPENING TO TRAFFIC. COST INCLUDED WITH CONCRETE SEALER.



SECTION AT WINGWALL



SECTION AT PARAPET



**CROSS SECTION
LOOKING SOUTH**

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CONCRETE SEALER	SQ. FT.	7305

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c:\pvc\work\p1\dot\woodshankr1\d0143185

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MAIN1-0366A28-deta1.s.dgn
PLOT SCALE = 100.0000' / IN.
PLOT DATE = Dec 21, 2010 - 11:04:28 AM

DESIGNED - RON WOODSHANK
DRAWN - RON WOODSHANK
CHECKED -
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONCRETE SEALER DETAILS

SCALE: SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	(1-X-B1)-1	IROQUOIS	14	14

CONTRACT NO. 66A28
ILLINOIS FED. AID PROJECT