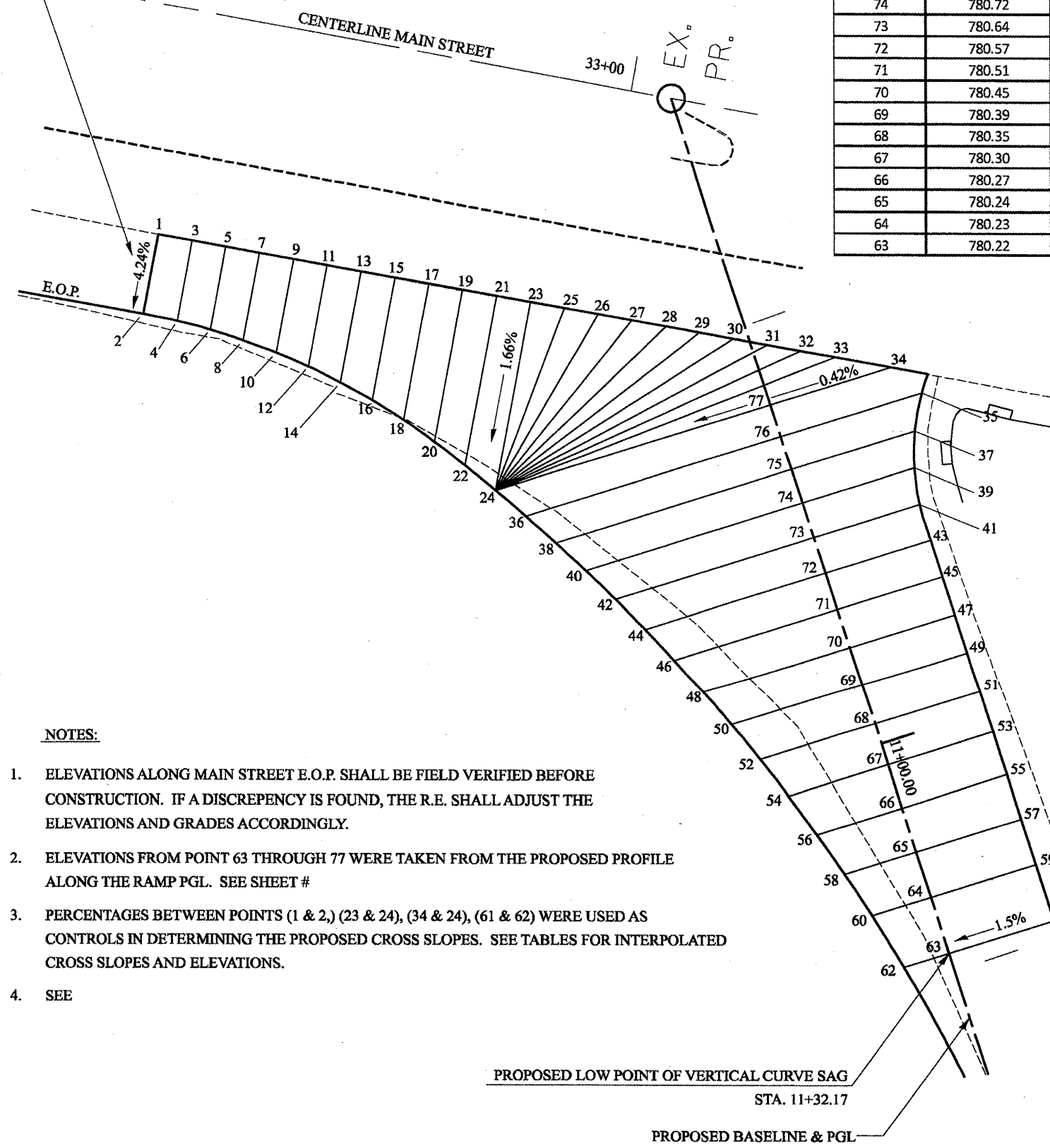


THE GRADE BETWEEN POINTS 1 & 2 MATCHES EXISTING



| POINT NO. | RAMP PGL ELEVATIONS |
|-----------|---------------------|
| 77 | 780.94 |
| 76 | 780.87 |
| 75 | 780.79 |
| 74 | 780.72 |
| 73 | 780.64 |
| 72 | 780.57 |
| 71 | 780.51 |
| 70 | 780.45 |
| 69 | 780.39 |
| 68 | 780.35 |
| 67 | 780.30 |
| 66 | 780.27 |
| 65 | 780.24 |
| 64 | 780.23 |
| 63 | 780.22 |

| EXISTING MAIN STREET STATIONING | POINT NO. | ELEVATION | OFFSET* (FEET) | % GRADE BETWEEN POINTS | EXISTING MAIN STREET STATIONING | POINT NO. | ELEVATION | OFFSET* (FEET) |
|---------------------------------|-----------|-----------|----------------|------------------------|---------------------------------|-----------|-----------|----------------|
| 32+36.08 | 1 | 781.50 | 33 RT | 4.24 | 32+36.08 | 2 | 780.99 | 44.96 RT |
| 32+41.08 | 3 | 781.48 | 33 RT | 4.00 | 32+41.08 | 4 | 780.99 | 45.12 RT |
| 32+46.08 | 5 | 781.46 | 33 RT | 3.77 | 32+46.08 | 6 | 780.98 | 45.52 RT |
| 32+51.08 | 7 | 781.43 | 33 RT | 3.54 | 32+51.08 | 8 | 780.96 | 46.18 RT |
| 32+56.08 | 9 | 781.41 | 33 RT | 3.31 | 32+56.08 | 10 | 780.94 | 47.10 RT |
| 32+61.08 | 11 | 781.38 | 33 RT | 3.07 | 32+61.08 | 12 | 780.92 | 48.29 RT |
| 32+66.08 | 13 | 781.35 | 33 RT | 2.84 | 32+66.08 | 14 | 780.88 | 49.75 RT |
| 32+71.08 | 15 | 781.33 | 33 RT | 2.60 | 32+71.08 | 16 | 780.85 | 51.50 RT |
| 32+76.08 | 17 | 781.30 | 33 RT | 2.37 | 32+76.08 | 18 | 780.81 | 53.56 RT |
| 32+81.08 | 19 | 781.28 | 33 RT | 2.13 | 32+81.08 | 20 | 780.79 | 55.95 RT |
| 32+86.08 | 21 | 781.26 | 33 RT | 1.90 | 32+86.08 | 22 | 780.77 | 58.61 RT |
| 32+91.08 | 23 | 781.24 | 33 RT | 1.66 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 32+96.08 | 25 | 781.22 | 33 RT | 1.56 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+01.08 | 26 | 781.20 | 33 RT | 1.43 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+06.08 | 27 | 781.18 | 33 RT | 1.27 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+11.08 | 28 | 781.15 | 33 RT | 1.11 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+16.08 | 29 | 781.13 | 33 RT | 0.96 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+21.08 | 30 | 781.10 | 33 RT | 0.82 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+26.08 | 31 | 781.08 | 33 RT | 0.70 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+31.08 | 32 | 781.06 | 33 RT | 0.60 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+36.08 | 33 | 781.05 | 33 RT | 0.53 | 32+91.08 | 24 | 780.77 | 61.42 RT |
| 33+44.40 | 34 | 781.02 | 33 RT | 0.42 | 32+91.08 | 24 | 780.77 | 61.42 RT |

*PERPENDICULAR TO THE EXISTING MAIN STREET BASELINE

NOTES:

- ELEVATIONS ALONG MAIN STREET E.O.P. SHALL BE FIELD VERIFIED BEFORE CONSTRUCTION. IF A DISCREPANCY IS FOUND, THE R.E. SHALL ADJUST THE ELEVATIONS AND GRADES ACCORDINGLY.
- ELEVATIONS FROM POINT 63 THROUGH 77 WERE TAKEN FROM THE PROPOSED PROFILE ALONG THE RAMP PGL. SEE SHEET #
- PERCENTAGES BETWEEN POINTS (1 & 2,) (23 & 24), (34 & 24), (61 & 62) WERE USED AS CONTROLS IN DETERMINING THE PROPOSED CROSS SLOPES. SEE TABLES FOR INTERPOLATED CROSS SLOPES AND ELEVATIONS.
- SEE

| PROPOSED RAMP STATIONING | POINT NO. | OFFSET* (FEET) | ELEVATION | % GRADE BETWEEN POINTS | POINT NO. | OFFSET* (FEET) | ELEVATION |
|--------------------------|-----------|----------------|-----------|------------------------|-----------|----------------|-----------|
| 10+52.19 | 35 | 21.47 LT | 780.97 | 0.48 | 36 | 38.93 RT | 780.68 |
| 10+57.27 | 37 | 18.80 LT | 780.90 | 0.55 | 38 | 35.97 RT | 780.59 |
| 10+62.48 | 39 | 17.05 LT | 780.82 | 0.61 | 40 | 33.08 RT | 780.51 |
| 10+67.82 | 41 | 16 LT | 780.75 | 0.68 | 42 | 30.27 RT | 780.44 |
| 10+73.32 | 43 | 16 LT | 780.69 | 0.75 | 44 | 27.53 RT | 780.37 |
| 10+78.96 | 45 | 16 LT | 780.64 | 0.82 | 46 | 24.87 RT | 780.30 |
| 10+84.86 | 47 | 16 LT | 780.59 | 0.90 | 48 | 22.26 RT | 780.25 |
| 10+90.74 | 49 | 16 LT | 780.55 | 0.97 | 50 | 19.82 RT | 780.20 |
| 10+96.89 | 51 | 16 LT | 780.51 | 1.05 | 52 | 17.43 RT | 780.16 |
| 11+03.24 | 53 | 16 LT | 780.48 | 1.13 | 54 | 15.15 RT | 780.12 |
| 11+09.78 | 55 | 16 LT | 780.47 | 1.22 | 56 | 12.97 RT | 780.11 |
| 11+16.55 | 57 | 16 LT | 780.45 | 1.30 | 58 | 10.91 RT | 780.10 |
| 11+23.55 | 59 | 16 LT | 780.45 | 1.39 | 60 | 8.98 RT | 780.10 |
| 11+32.17 | 61 | 16 LT | 780.46 | 1.50 | 62 | 6.87 RT | 780.12 |

*PERPENDICULAR TO THE PROPOSED RAMP BASELINE

PROPOSED LOW POINT OF VERTICAL CURVE SAG
STA. 11+32.17
PROPOSED BASELINE & PGL