

PROPOSED HIGHWAY PLANS

F.A.U. ROUTE 3542: OGDEN AVE.

KEDZIE AVE. TO TAYLOR ST.

SECTION NO.: 2020-156-RS & SW

PROJECT NO.: STP-KSUN(567)

STANDARD OVERLAY, ADA IMPROVEMENTS

COOK COUNTY

C-91-374-20

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 62M44	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

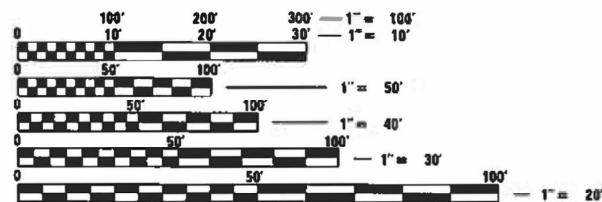
PROJECT IS LOCATED IN THE CITY OF CHICAGO

TRAFFIC DATA:

2018 ADT - 18600 TO 19800 VPD
SPEED LIMIT - 30 MPH



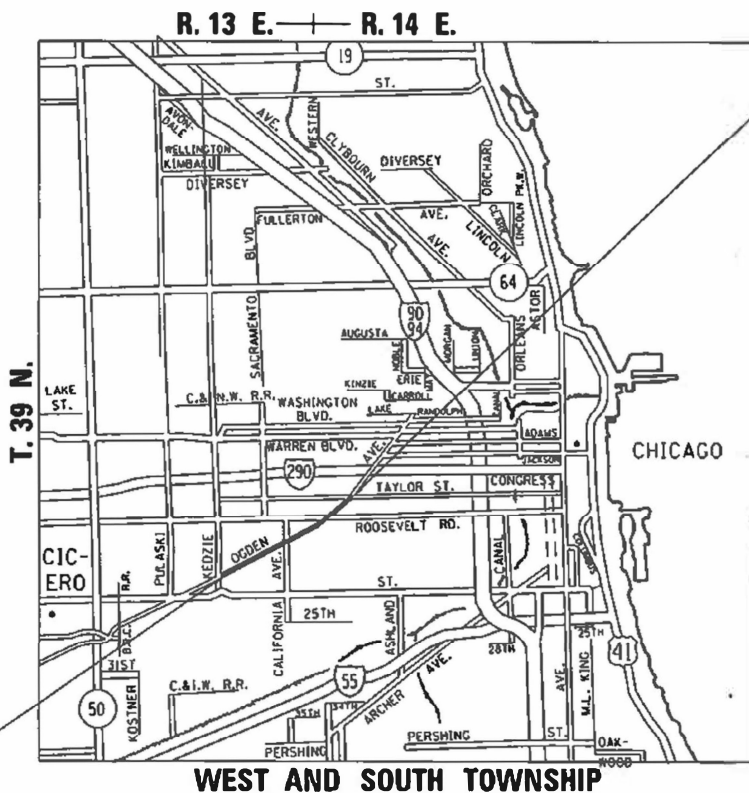
PROJECT ENDS
STA. 100 + 32



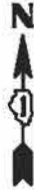
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
(312) 744-7000

PROJECT BEGINS
STA. 20 + 69



WEST AND SOUTH TOWNSHIP



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED DECEMBER 20 21
Jose Rios
JOSE RIOS, REGIONAL ENGINEER

February 4 20 22
Stephen M. Sullivan
ENGINEER OF DESIGN AND ENVIRONMENT

February 4 20 22
Stephen M. Sullivan
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432

PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62M44

GROSS AND NET LENGTH OF PROJECT - 7963 FEET - 1.51 MILES

INDEX OF SHEETS

SHEET NO.	DESCRIPTION:
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
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5-10	EXISTING AND PROPOSED TYPICAL SECTIONS
11-14	PROPOSED ROADWAY AND PAVEMENT MARKING PLANS
15-16	DETECTOR LOOP LOCATION DETAILS
17	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-08)
18	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
19	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
20	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
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22	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
23	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TC-14)
24	SHORT-TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
25	ARTERIAL ROAD INFORMATION SIGNING (TC-22)
26-28	CITY OF CHICAGO - TYPICAL PAVEMENT MARKINGS (TC-24)
29	DRIVEWAY ENTRANCE SIGNING (TC-26)
30	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STATE STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-06	DEPRESSED CORNER FOR SIDEWALKS
424031-02	MEDIAN PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS, TYPE 1
604006-05	FRAME AND GRATE, TYPE 3
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	PAVEMENT PATCHES, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, 2L, 2W, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS LESS THAN OR EQUAL TO 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NON-TRANSVERSABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS FOR RAISED REFLECTIVE PAVEMENT MARKERS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES:

- BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).
- 10 FEET (3 METERS) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE CITY OF CHICAGO.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT IN PART OF THIS CONTRACT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/ h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/ h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM (1:3).
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR FOR ARTERIALS AT KALPANA.KANNAN-HOUSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-24)
- THE RESIDENT ENGINEER SHALL CONTACT MR. EMAD ALHUSSEINI, AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST TWO WEEKS PRIOR TO BEGINNING LANDSCAPE AND FORESTRY WORK FOR LAYOUT.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT-TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- THE PCC BUS PADS SHALL NOT BE MILLED OR OVERLAID
- THE ADA RAMPS SHALL BE INSTALLED PER CITY OF CHICAGO STANDARDS

GENERAL NOTES - CITY OF CHICAGO:

- ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF SEWERS STANDARDS
- PERMITS FROM THE DEPARTMENT OF SEWERS ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY OR COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR RESURFACING WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF SEWERS PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION.
- BENCH MONUMENT LOCATIONS WITHIN THE LIMITS OF IMPROVEMENT CAN BE OBTAINED FROM THE DEPARTMENT OF SEWERS AT 333 SOUTH STATE STREET, SUITE 410, CHICAGO, IL 60604. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF THE REPLACEMENT OF ANY BENCH MONUMENT IF DAMAGED OR DESTROYED DURING CONSTRUCTION.
- ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND LIDS ON SEWER STRUCTURES SHALL BE REPLACED WITH THE NEW DEPARTMENT OF SEWER STANDARD FRAME AND LIDS.
- CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED, OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION OF THIS ROADWAY WITHOUT THE WRITTEN PERMISSION FROM THE CITY OF CHICAGO.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS

FILE NAME =	USER NAME = steedpe	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES FAU 3542 (OGDEN AVE.)—KEDZIE AVE. TO TAYLOR ST.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -			CONTRACT NO. 62M44					
PLOT DATE = 12/17/2021						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005	100% STATE 0005			
20200100	EARTH EXCAVATION	CU YD	7	7				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	644	644				
25200110	SODDING, SALT TOLERANT	SO YD	644	644				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	55230	55230				
40600370	LONGITUDINAL JOINT SEALANT	FOOT	18645	18645				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	123	123				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	795	795				
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	3376	3376				
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	6873	6873				
42001300	PROTECTIVE COAT	SO YD	761	761				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	85	85				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	81821	81821				
44000600	SIDEWALK REMOVAL	SO FT	85	85				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005	100% STATE 0005			
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SO FT	2435	2435				
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	654	654				
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	409	409				
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	573	573				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	7	7				
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	7	7				
66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1				
*66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1				
*66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1				
*66901006	REGULATED SUBSTANCES MONITORING	CAL DA	15	15				
67100100	MOBILIZATION	L SUM	1	1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				

* SPECIALTY ITEMS
 NON PARTICIPATING ITEMS

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	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES	
FAU 3542 (OGDEN AVE.)—KEDZIE AVE. TO TAYLOR ST.	
SCALE: NONE	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 3542	SECTION 2020-156-RS & SW	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62M44	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005	100% STATE 0005			
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3498	3498				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	583	583				
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SO FT	713	713				
70300221	TEMPORARY PAVEMENT MARKING - LINE 4" - PAINT	FOOT	18948	18948				
70300241	TEMPORARY PAVEMENT MARKING - LINE 6" - PAINT	FOOT	2970	2970				
70300261	TEMPORARY PAVEMENT MARKING - LINE 12" - PAINT	FOOT	4756	4756				
70300281	TEMPORARY PAVEMENT MARKING - LINE 24" - PAINT	FOOT	1057	1057				
70306120	TEMPORARY PAVEMENT MARKING - LINE 4" TYPE III TAPE	FOOT	1749	1749				
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	713	713				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005	100% STATE 0005			
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	18948	18948				
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2970	2970				
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	4756	4756				
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1057	1057				
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	396	396				
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1				
X4240800	DETECTABLE WARNINGS (SPECIAL)	SO FT	43	43				
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	88	88				
X4400501	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	32	32				
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3863	3863				
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	7	7				
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
Z0076600	TRAINEES	HOURS	500	500				* SPECIALTY ITEMS
Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500				<input type="checkbox"/> NON PARTICIPATING ITEMS

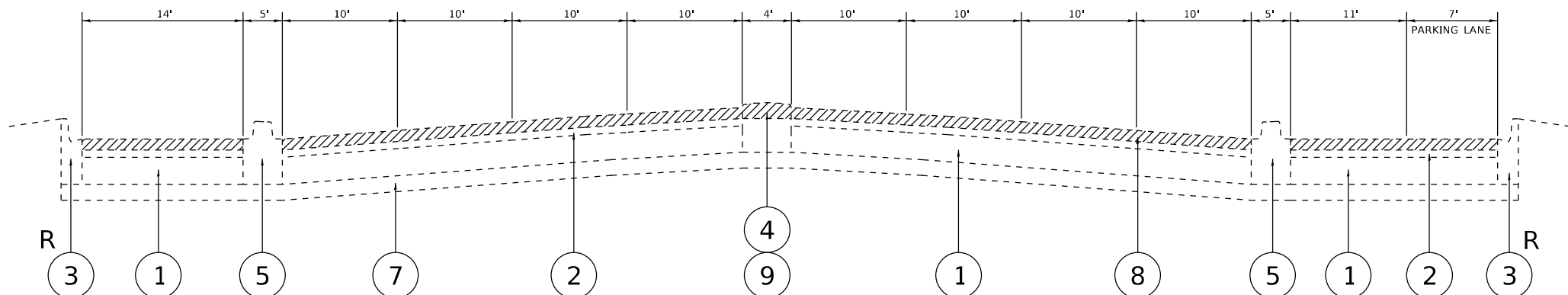
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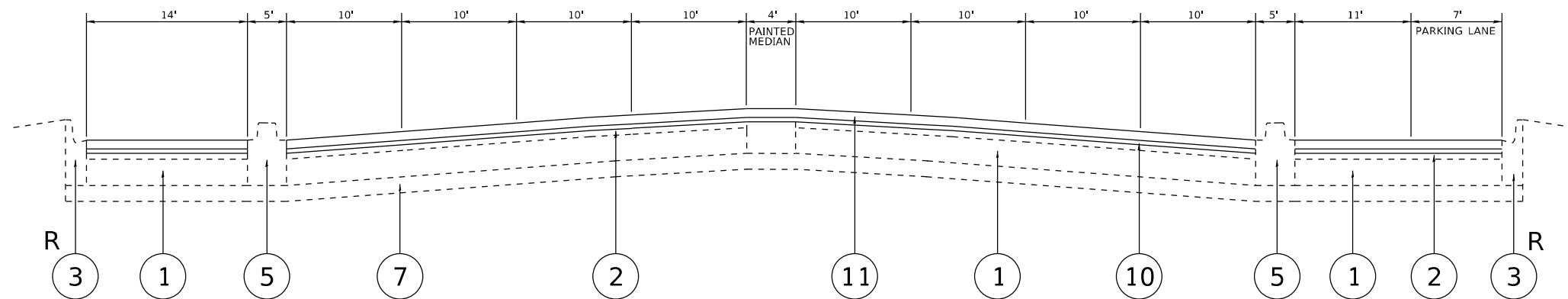
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
FAU 3542 (OGDEN AVE.)—KEDZIE AVE. TO TAYLOR ST.			
SCALE: NONE	SHEET	OF SHEETS	STA. TO STA.

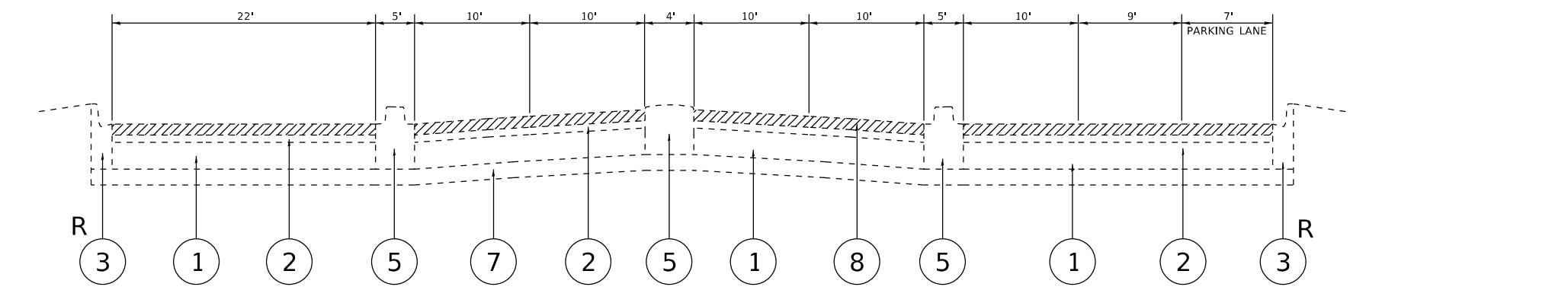
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CONTRACT NO. 62M44			ILLINOIS FED. AID PROJECT	



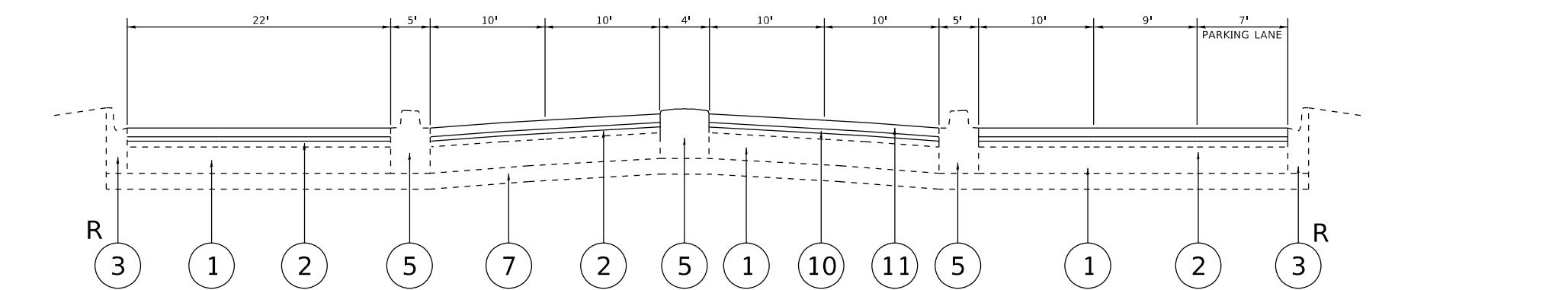
EXISTING TYPICAL SECTION
STA. 20+69 TO 27+97



PROPOSED TYPICAL SECTION
STA. 20+69 TO 27+97



EXISTING TYPICAL SECTION
STA. 27+97 TO 29+29



PROPOSED TYPICAL SECTION
STA. 27+97 TO 29+29

LEGEND:

- ① EXISTING P.C.C. PAVEMENT [9"]
- ② EXISTING HOT-MIX ASPHALT SURFACE [4" (AFTER MILLING)]
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING CORRUGATED MEDIAN
- ⑤ EXISTING BARRIER MEDIAN
- ⑥ EXISTING LANDSCAPE MEDIAN
- ⑦ EXISTING STABILIZED SUB-BASE
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑨ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 1/2"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
PATCHING		
CLASS "D" PATCHES (HMA BINDER, IL-19MM)	4% @ 70 GYR.	QC / QA
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1 1/2"	4% @ 70 GYR.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	QCP
QMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (QCP); QUALITY CONTROL/ QUALITY ASSURANCE (QC/ QA); PAY FOR PERFORMANCE (PFP)		

NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 LBS./SQ. YD./ IN.

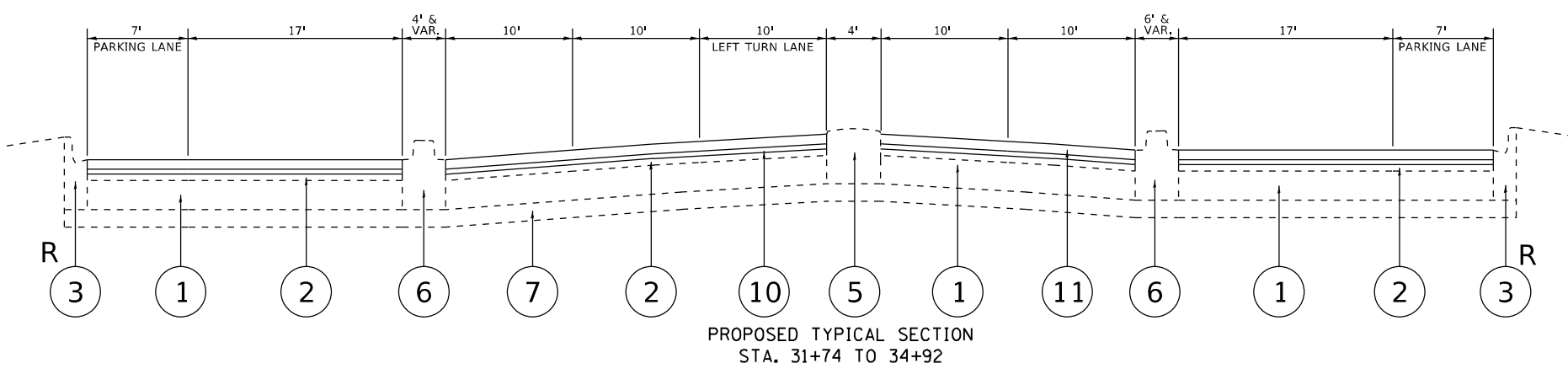
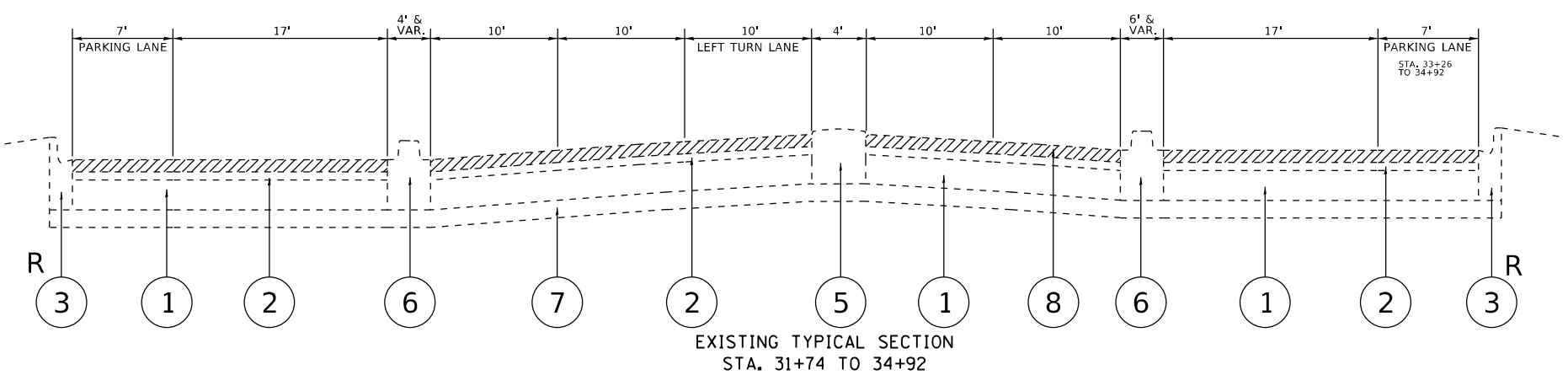
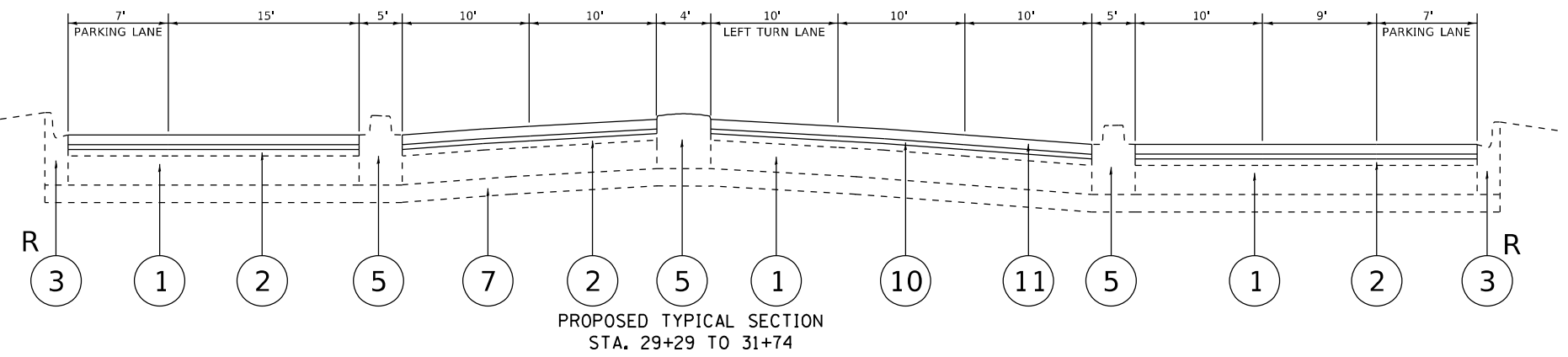
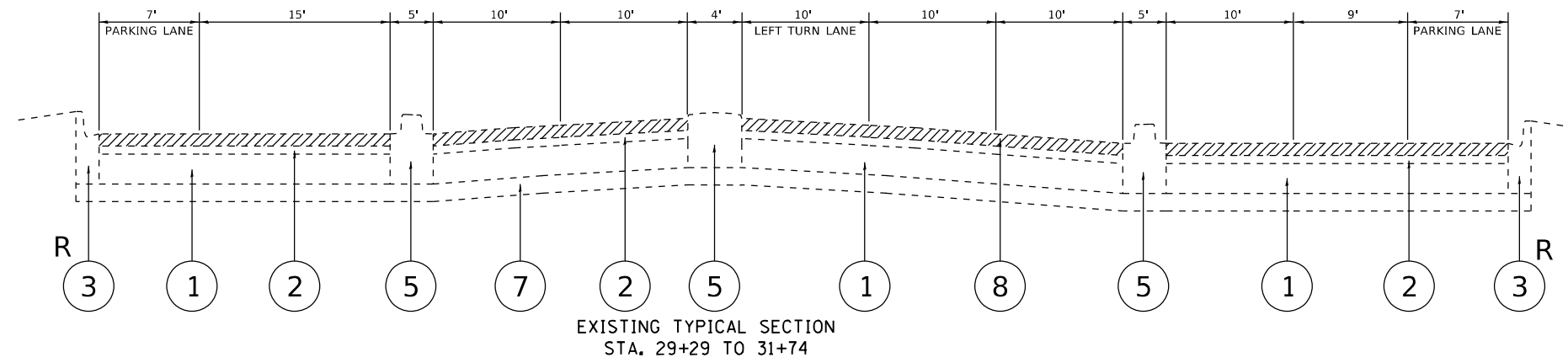
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

THE MILLING SHALL BE DONE PRIOR TO PATCHING

THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 4.75, N50.

LEGEND:

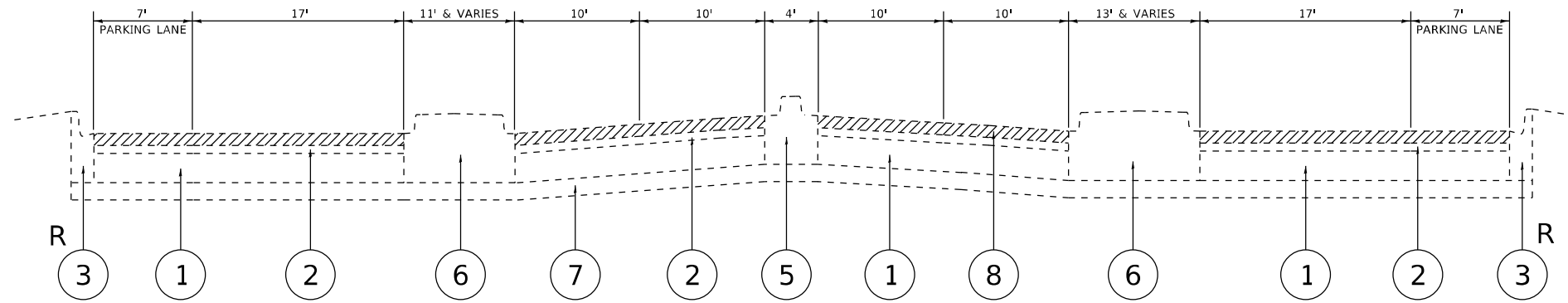
- ① EXISTING P.C.C. PAVEMENT [9"
- ② EXISTING HOT-MIX ASPHALT SURFACE [4" (AFTER MILLING)
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING CORRUGATED MEDIAN
- ⑤ EXISTING BARRIER MEDIAN
- ⑥ EXISTING LANDSCAPE MEDIAN
- ⑦ EXISTING STABILIZED SUB-BASE
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2¼"
- ⑨ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, ¾"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1½"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



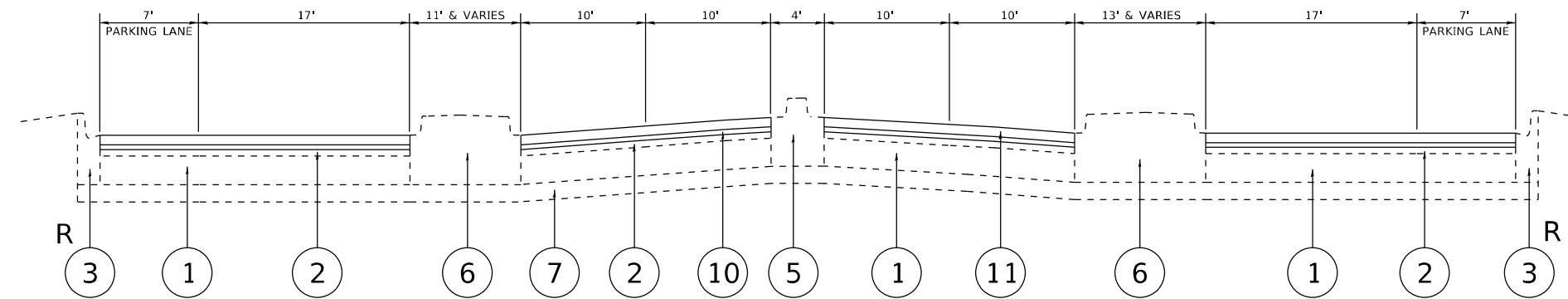
FILE NAME =	USER NAME = steedpo	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS FAU 3542 (OGDEN AVE.)—KEDZIE AVE. TO TAYLOR ST.	F.A.U. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\idot-pw\bentley.com\PROJECTS\DOT\DISTRICT 1\PROJECTS\DI57520\CD\DRAWING\DI57520-sht-pln.dgn	DOT OFFICES\District 1\Projects\DI57520\CD\DRAWING\DI57520-sht-pln.dgn	CHECKED -	REVISED -			3542	2020-156-RS & SW	COOK	30	6
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	REVISED -			CONTRACT NO. 62M44				
PLOT DATE = 12/17/2021	DATE -	REVISED -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT	

LEGEND:

- ① EXISTING P.C.C. PAVEMENT [9"
- ② EXISTING HOT-MIX ASPHALT SURFACE [4" (AFTER MILLING)
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING CORRUGATED MEDIAN
- ⑤ EXISTING BARRIER MEDIAN
- ⑥ EXISTING LANDSCAPE MEDIAN
- ⑦ EXISTING STABILIZED SUB-BASE
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2¼"
- ⑨ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, ¾"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1½"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



EXISTING TYPICAL SECTION
 STA. 34+92 TO 45+85
 STA. 52+28 TO 55+00
 STA. 58+98 TO 62+47
 STA. 66+32 TO 76+25



PROPOSED TYPICAL SECTION
 STA. 34+92 TO 45+85
 STA. 52+28 TO 55+00
 STA. 58+98 TO 62+47
 STA. 66+32 TO 76+25

FILE NAME =	USER NAME = steedpo	DESIGNED -	REVISED -
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PLOT DATE = 12/17/2021	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

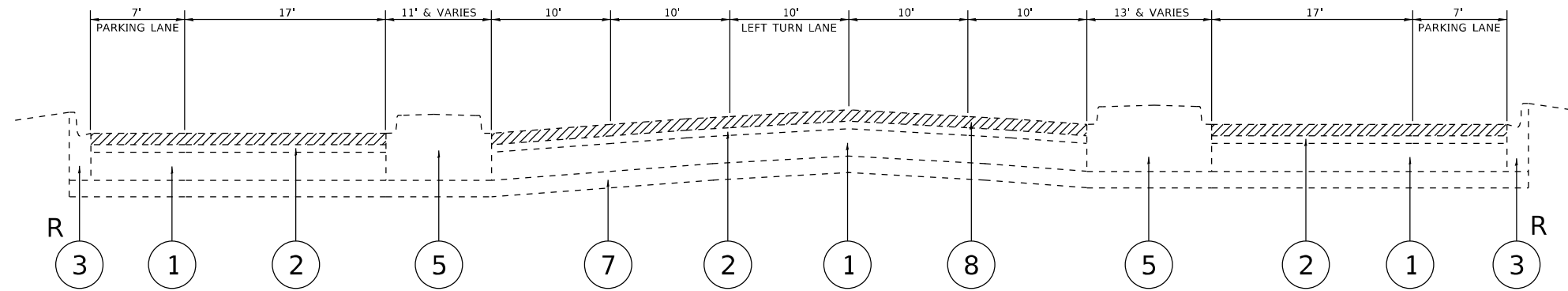
**EXISTING AND PROPOSED TYPICAL SECTIONS
 FAU 3542 (OGDEN AVE.)—KEDZIE AVE. TO TAYLOR ST.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

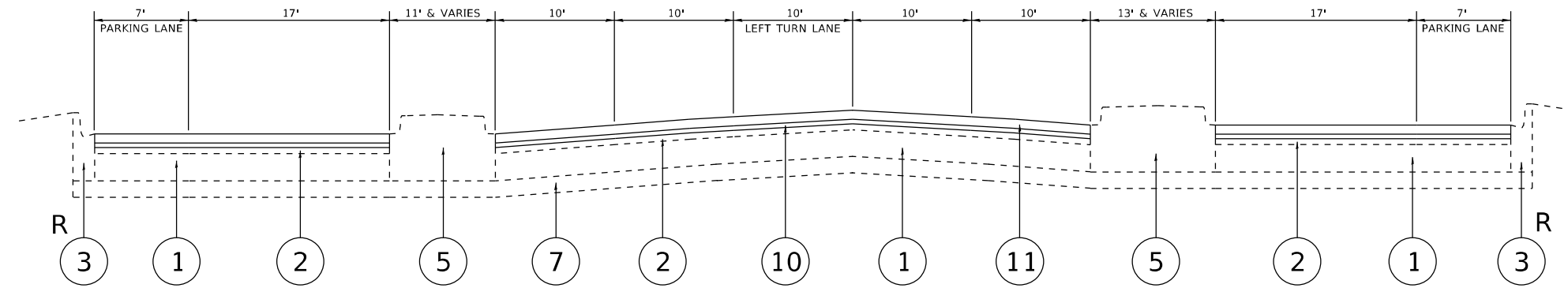
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3542	2020-156-RS & SW	COOK	30	7
CONTRACT NO. 62M44				
ILLINOIS FED. AID PROJECT				

LEGEND:

- ① EXISTING P.C.C. PAVEMENT [9"
- ② EXISTING HOT-MIX ASPHALT SURFACE [4" (AFTER MILLING)
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING CORRUGATED MEDIAN
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- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2¼"
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- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, ¾"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1½"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



EXISTING TYPICAL SECTION
 STA. 45+85 TO 52+28
 STA. 55+00 TO 58+98
 STA. 62+47 TO 66+32
 STA. 76+25 TO 82+64



PROPOSED TYPICAL SECTION
 STA. 45+85 TO 52+28
 STA. 55+00 TO 58+98
 STA. 62+47 TO 66+32
 STA. 76+25 TO 82+64

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PLOT DATE = 12/17/2021	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

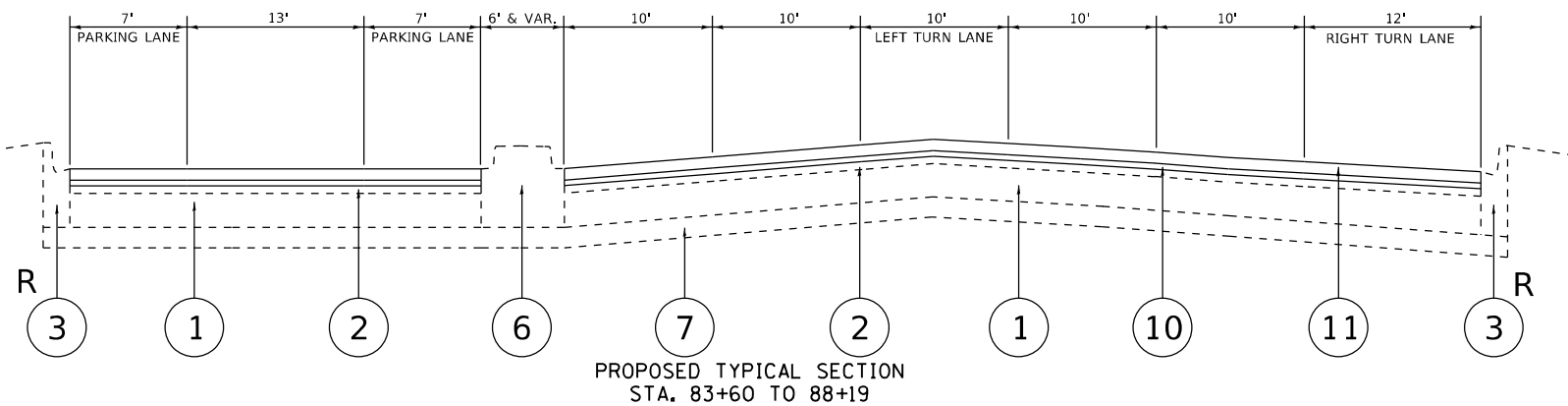
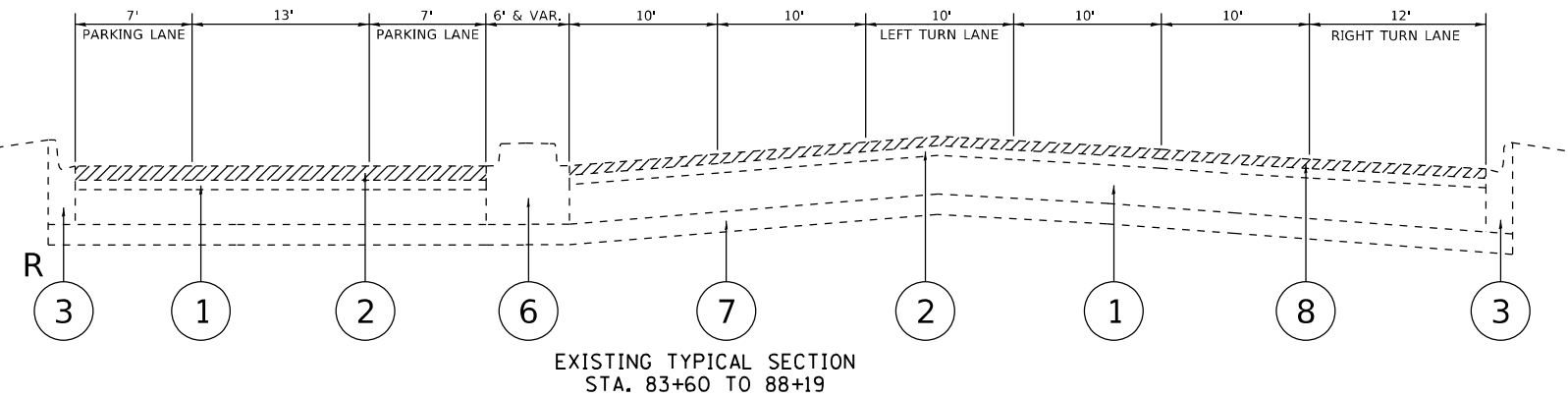
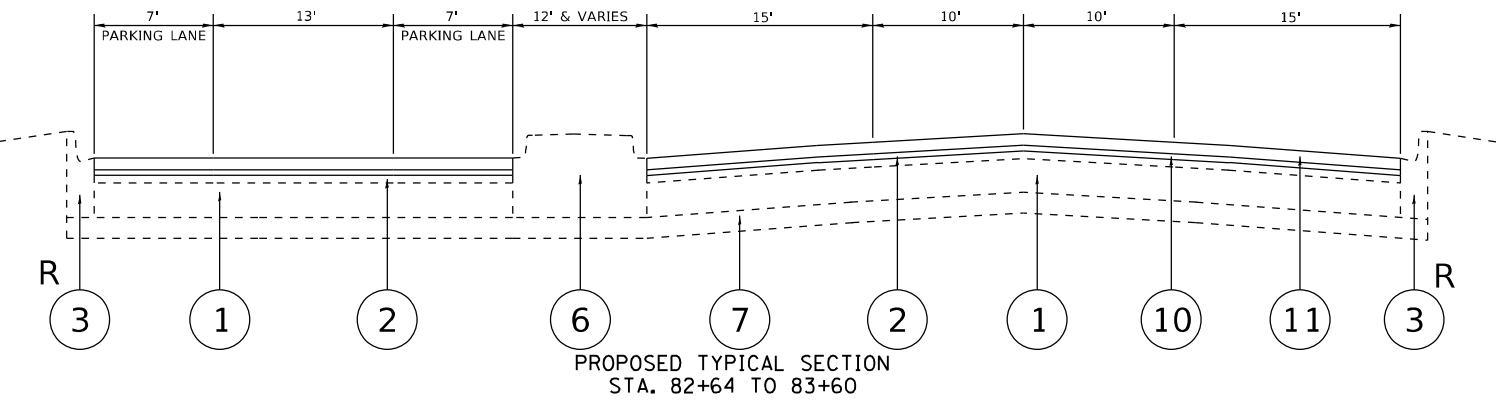
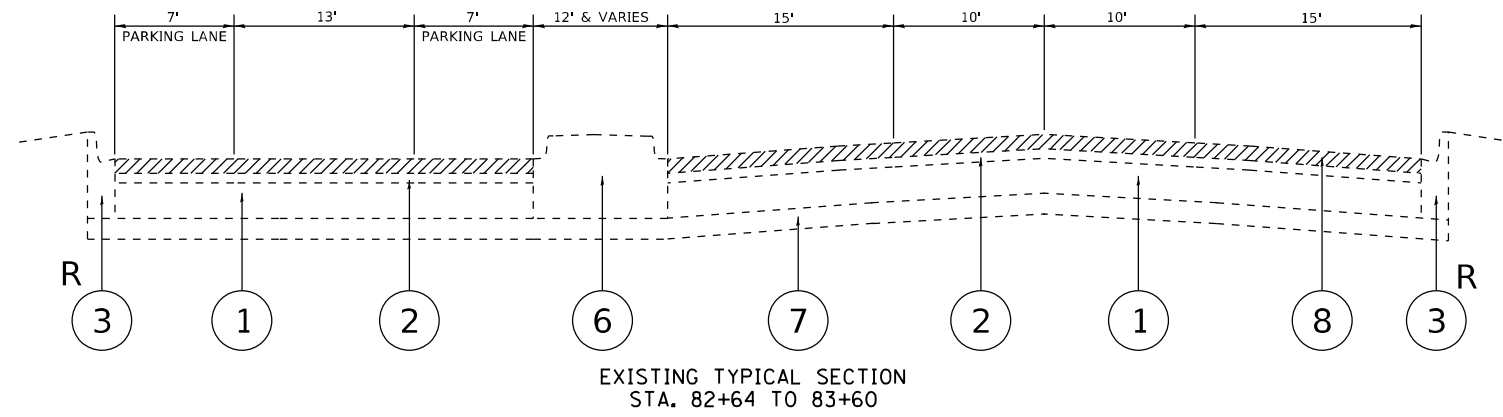
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 FAU 3542 (OGDEN AVE.)—KEDZIE AVE. TO TAYLOR ST.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

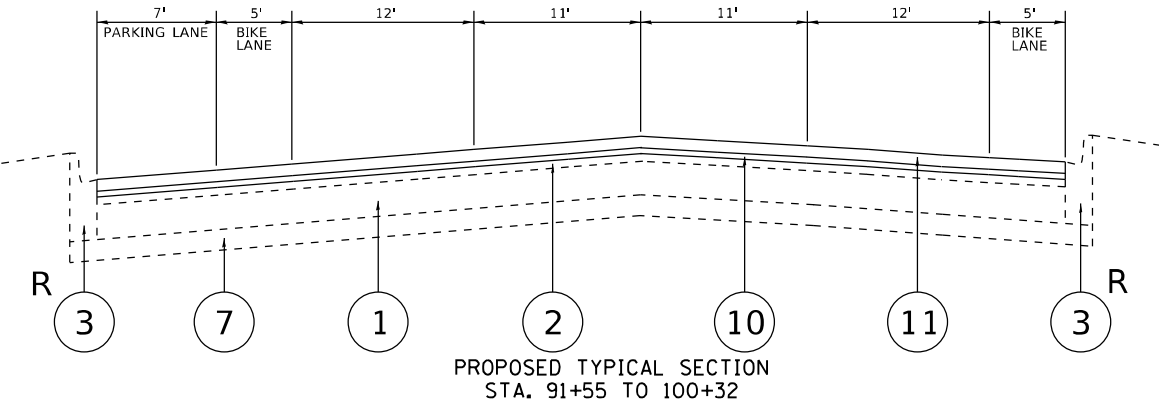
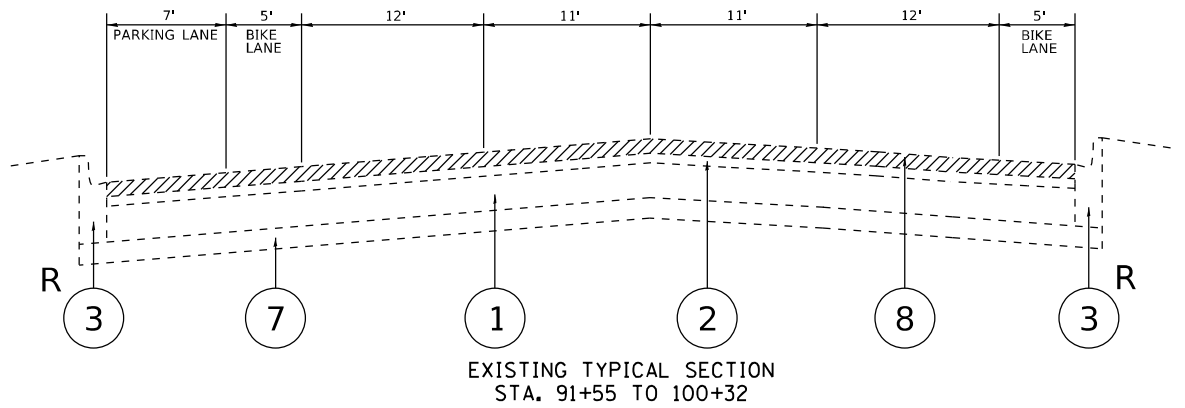
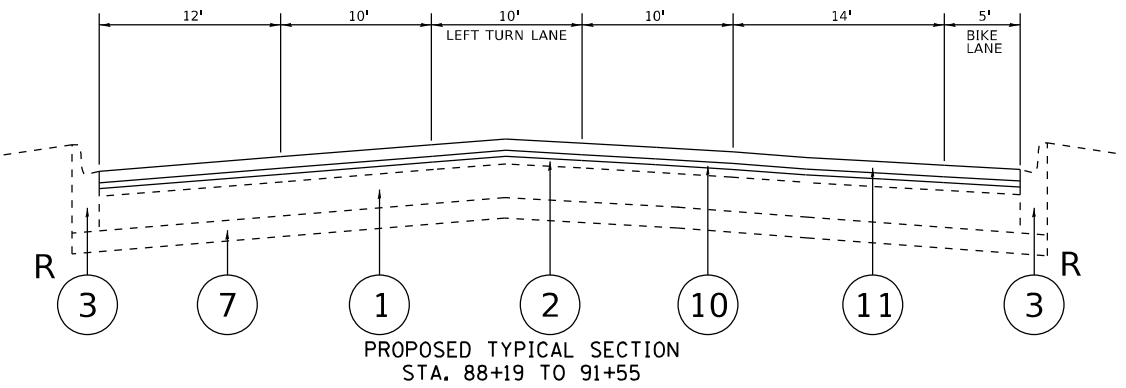
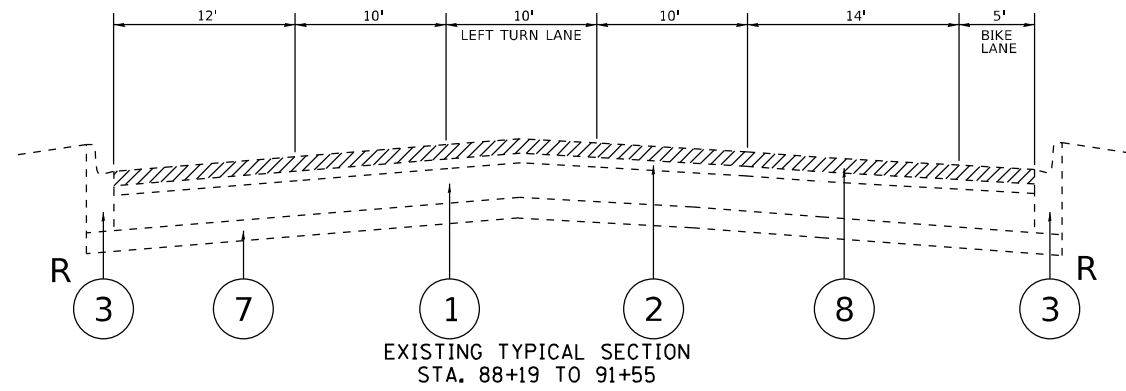
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	8
CONTRACT NO. 62M44				
ILLINOIS FED. AID PROJECT				

LEGEND:

- ① EXISTING P.C.C. PAVEMENT [9"]
- ② EXISTING HOT-MIX ASPHALT SURFACE [4" (AFTER MILLING)]
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING CORRUGATED MEDIAN
- ⑤ EXISTING BARRIER MEDIAN
- ⑥ EXISTING LANDSCAPE MEDIAN
- ⑦ EXISTING STABILIZED SUB-BASE
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑨ PROPOSED MEDIAN REMOVAL PARTIAL DEPTH
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 1/2"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



FILE NAME =	USER NAME = steedpo	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS FAU 3542 (OGDEN AVE.)—KEDZIE AVE. TO TAYLOR ST.	F.A.U. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = 12/17/2021		DATE -	REVISIONS -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT



LEGEND:

- ① EXISTING P.C.C. PAVEMENT | 9"
- ② EXISTING HOT-MIX ASPHALT SURFACE | 4" (AFTER MILLING)
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING CORRUGATED MEDIAN
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- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

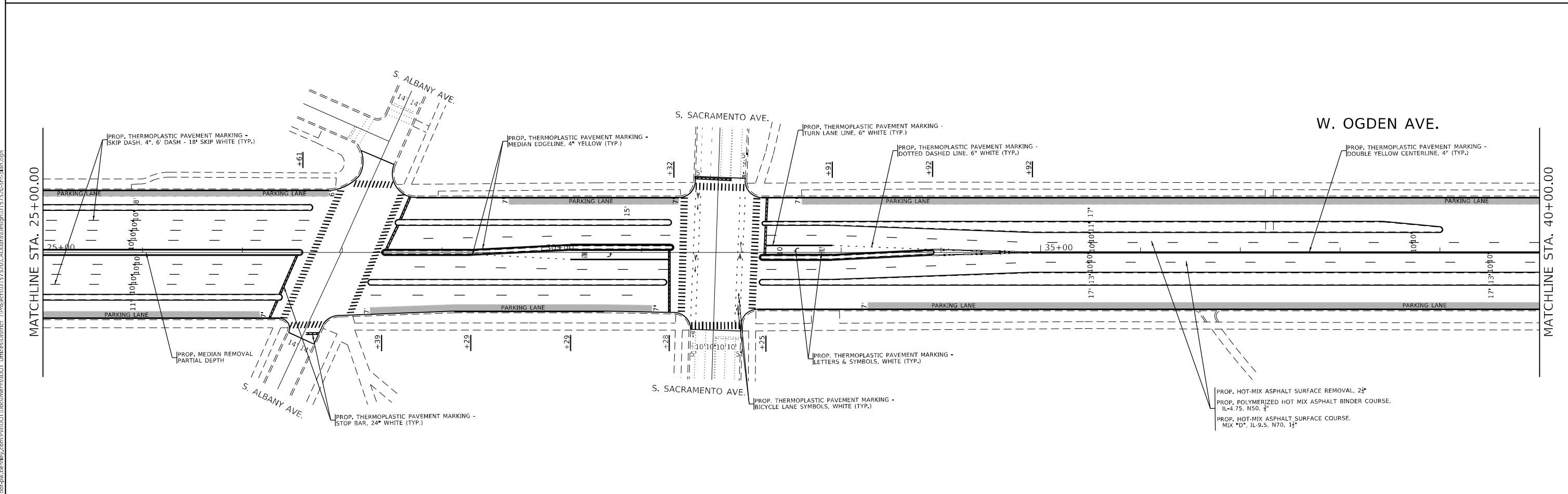
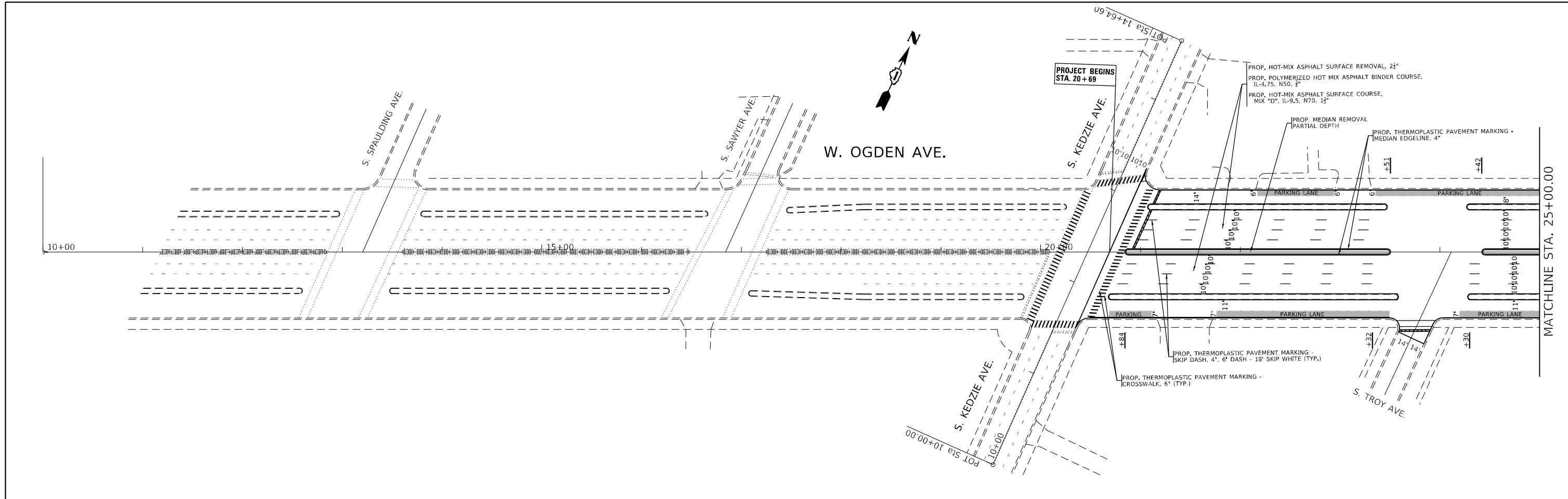
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PLOT SCALE = 100.0000' / in.			
PLOT DATE = 12/17/2021			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS
FAU 3542 (OGDEN AVE.)—KEDZIE AVE. TO TAYLOR ST.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	10
CONTRACT NO. 62M44				
ILLINOIS FED. AID PROJECT				



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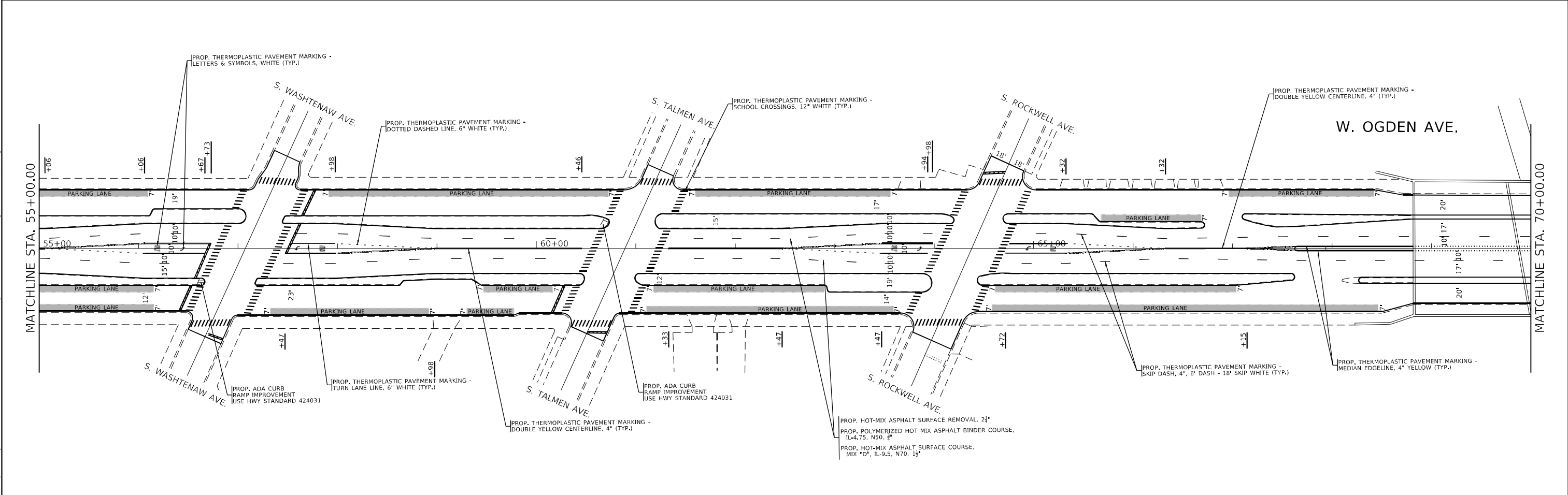
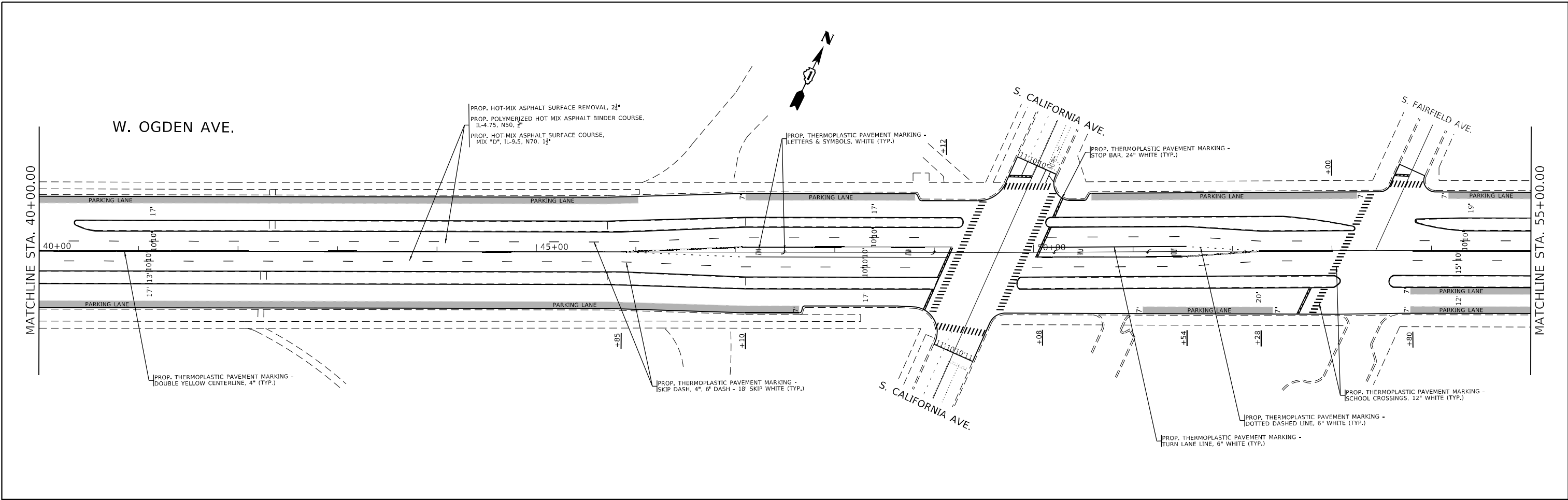
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PLOT DATE = 12/17/2021	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN
FAU 3542 / OGDEN AVE.—KEDZIE AVE. TO TAYLOR ST.

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-R5&SW	COOK	30	11
CONTRACT NO. 62M44				
ILLINOIS FED. AID PROJECT				



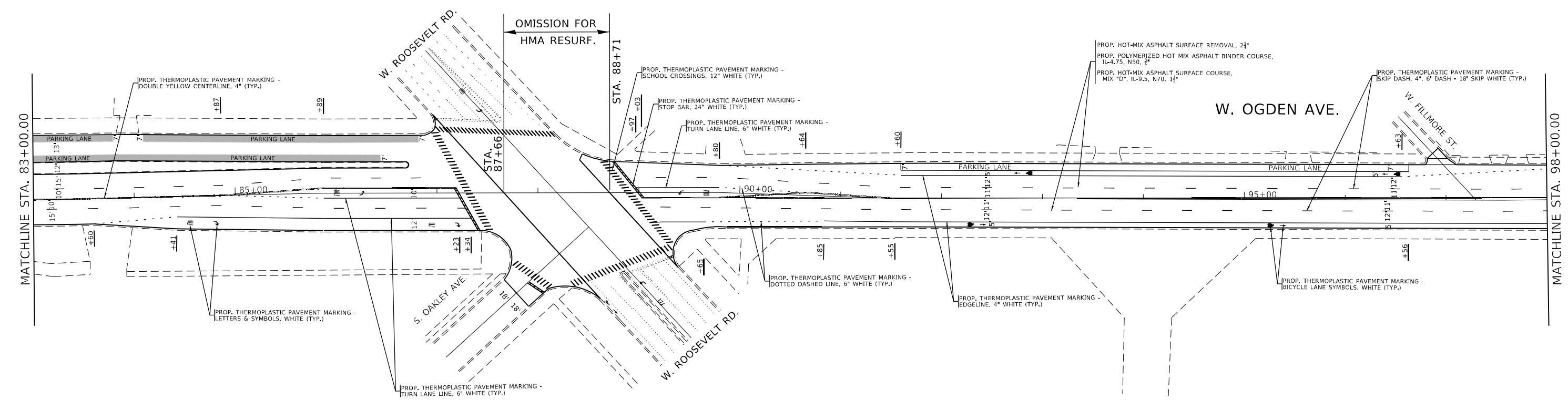
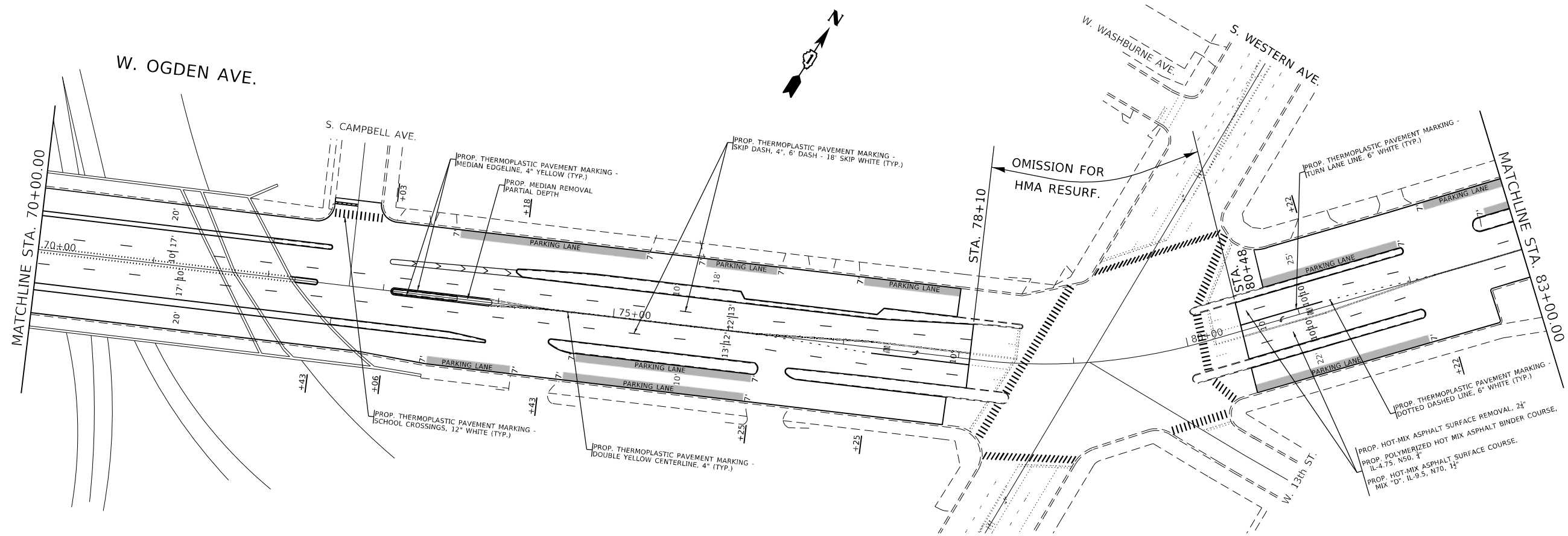
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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/17/2021	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN
FAU 3542 / OGDEN AVE.—KEDZIE AVE. TO TAYLOR ST.
 SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-R5&SW	COOK	30	12
CONTRACT NO. 62M44				
ILLINOIS FED. AID PROJECT				



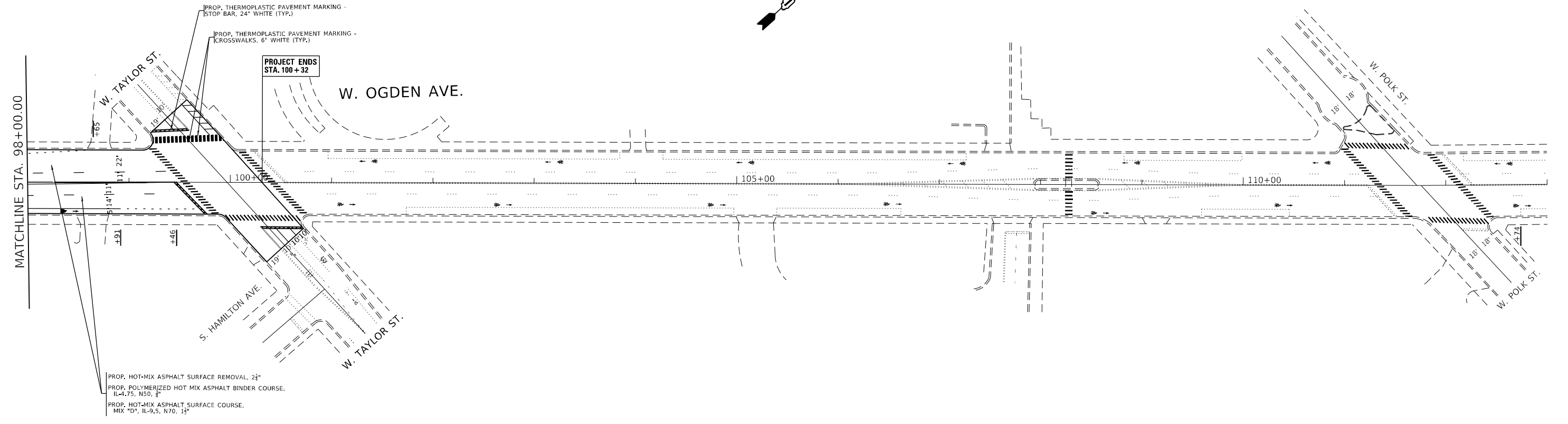
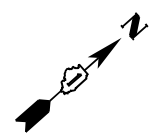
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PLOT DATE = 12/17/2021	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN
FAU 3542 / OGDEN AVE.—KEDZIE AVE. TO TAYLOR ST.
 SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-R5&SW	COOK	30	13
CONTRACT NO. 62M44				
ILLINOIS FED. AID PROJECT				



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PLOT DATE = 12/17/2021	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
 FAU 3542 / OGDEN AVE.—KEDZIE AVE. TO TAYLOR ST.**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

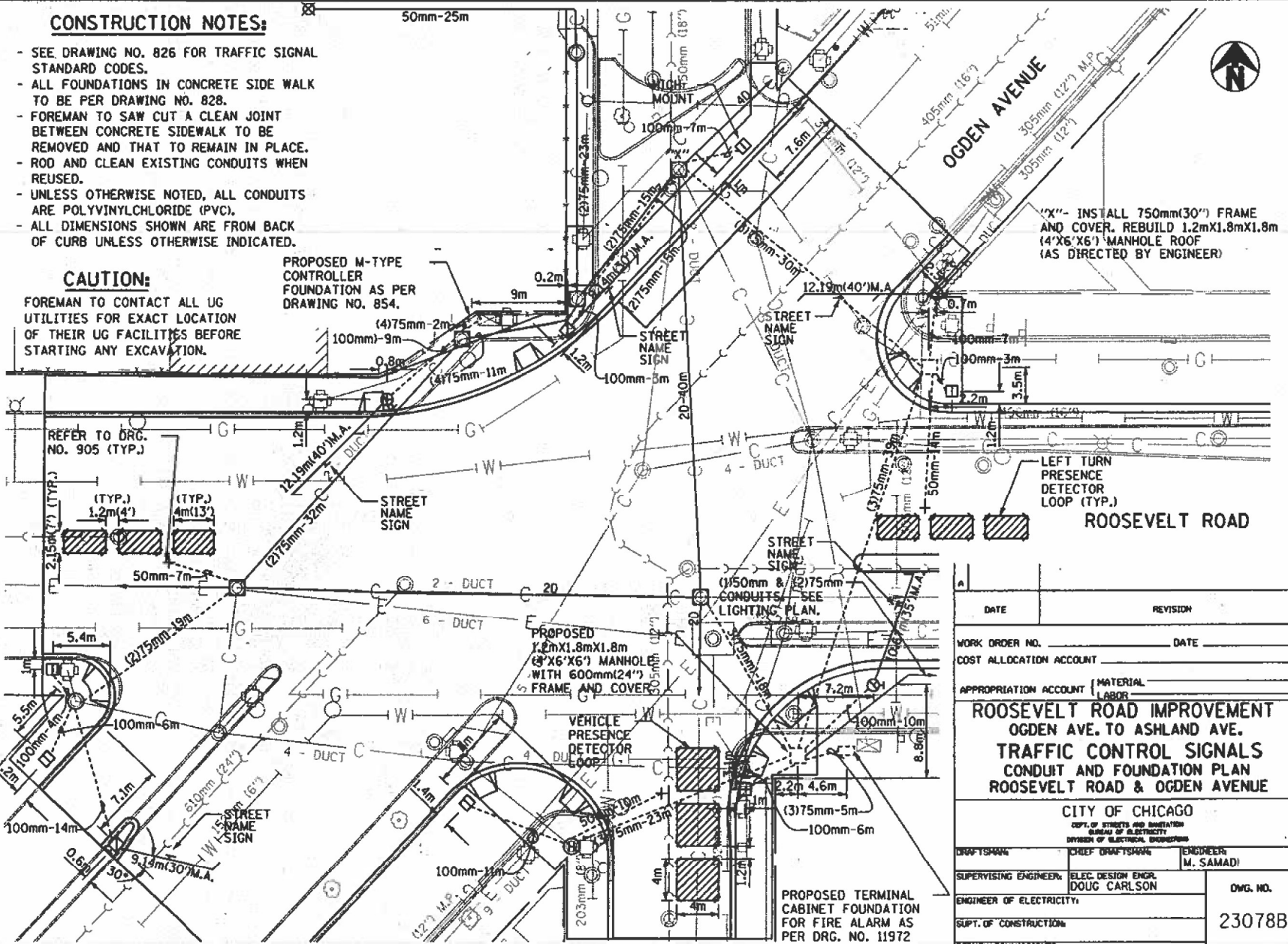
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3542	2020-156-R5&SW	COOK	30	14
CONTRACT NO. 62M44				
ILLINOIS FED. AID PROJECT				

CONSTRUCTION NOTES:

- SEE DRAWING NO. 826 FOR TRAFFIC SIGNAL STANDARD CODES.
- ALL FOUNDATIONS IN CONCRETE SIDE WALK TO BE PER DRAWING NO. 828.
- FOREMAN TO SAW CUT A CLEAN JOINT BETWEEN CONCRETE SIDEWALK TO BE REMOVED AND THAT TO REMAIN IN PLACE.
- ROD AND CLEAN EXISTING CONDUITS WHEN REUSED.
- UNLESS OTHERWISE NOTED, ALL CONDUITS ARE POLYVINYLCHLORIDE (PVC).
- ALL DIMENSIONS SHOWN ARE FROM BACK OF CURB UNLESS OTHERWISE INDICATED.

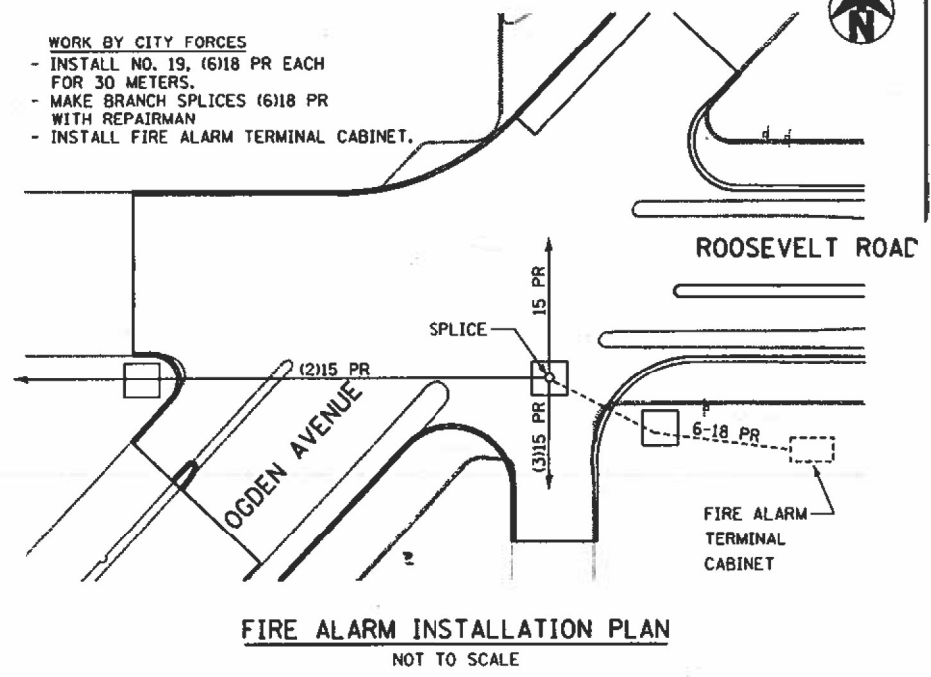
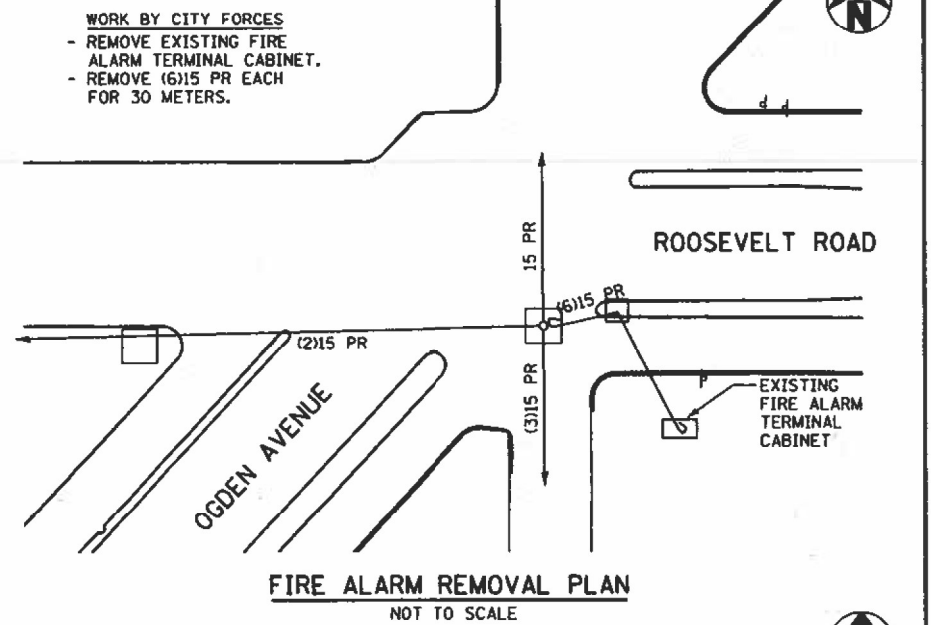
CAUTION:

FOREMAN TO CONTACT ALL UG UTILITIES FOR EXACT LOCATION OF THEIR UG FACILITIES BEFORE STARTING ANY EXCAVATION.



DATE	REVISION
WORK ORDER NO.	DATE
COST ALLOCATION ACCOUNT	
APPROPRIATION ACCOUNT (MATERIAL LABOR)	
ROOSEVELT ROAD IMPROVEMENT OGDEN AVE. TO ASHLAND AVE. TRAFFIC CONTROL SIGNALS CONDUIT AND FOUNDATION PLAN ROOSEVELT ROAD & OGDEN AVENUE	
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL PROGRAMS	
DRAFTSMAN	CHIEF DRAFTSMAN
ENGINEER (M. SAMADI)	
SUPERVISING ENGINEER	ELEC. DESIGN ENGR.
DOUG CARLSON	
ENGINEER OF ELECTRICITY	DWG. NO.
23078B	
SUPT. OF CONSTRUCTION	DEPUTY COMMISSIONER
SIZE	SCALE
DATE	

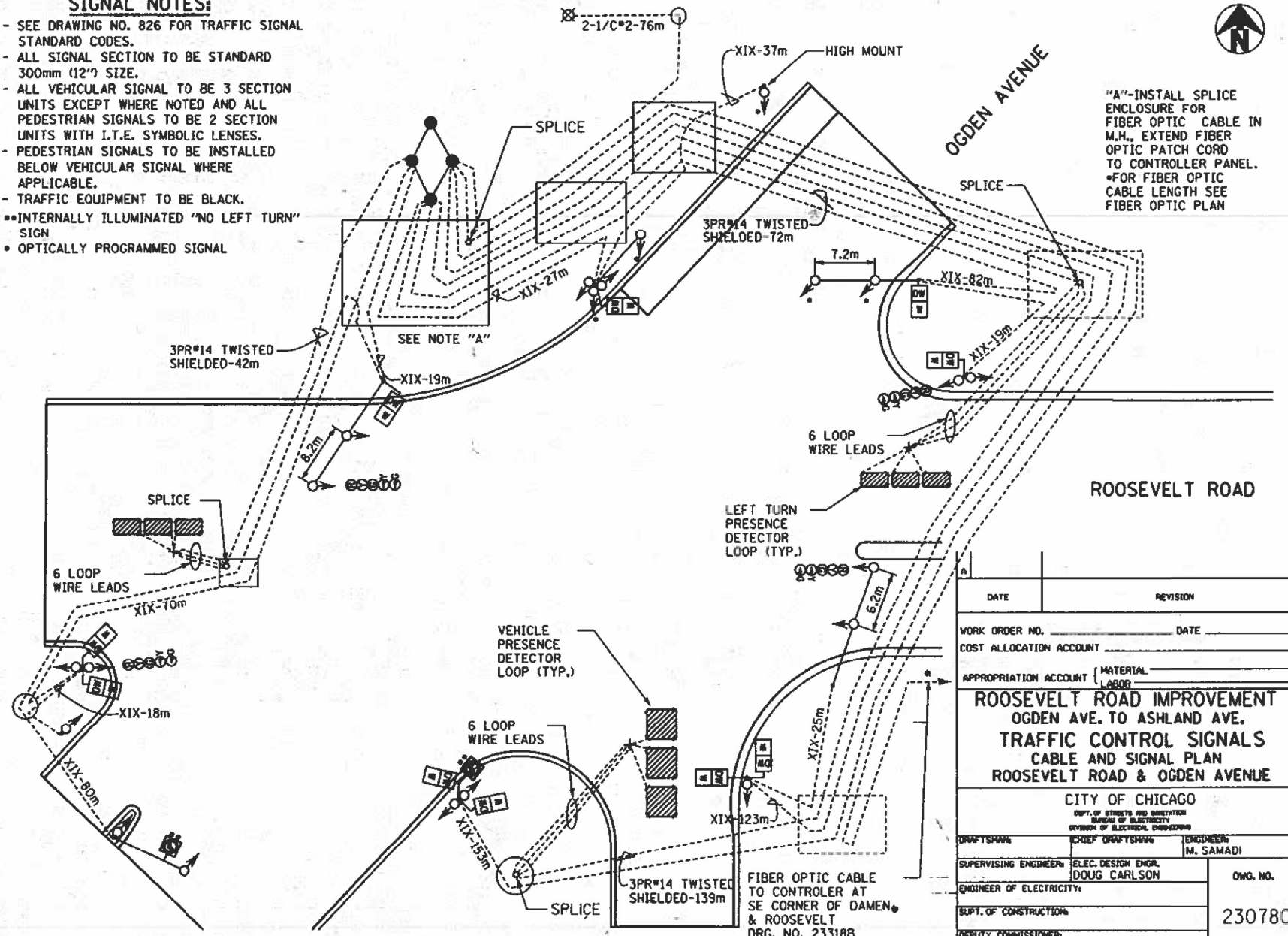
FEDERAL AID ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAP 347	98-08227-00-R5	COOK	30	15
FHWA REGION NO. 5		ILLINOIS	PROJECT	



ROOSEVELT ROAD IMPROVEMENT OGDEN AVE. TO ASHLAND AVE. TRAFFIC CONTROL SIGNALS CONDUIT AND FOUNDATION PLAN ROOSEVELT ROAD & OGDEN AVENUE TYLIN INTERNATIONAL-BASCOR	CITY OF CHICAGO DEPARTMENT OF TRANSPORT/ BUREAU OF HIGHWAYS DRAWN: AMB CHECKED: DAJ APPROVED: DATE: 03-24-2000 SCALE: AS NOTED CONTRACT NO.:
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SIGNAL NOTES:

- SEE DRAWING NO. 826 FOR TRAFFIC SIGNAL STANDARD CODES.
- ALL SIGNAL SECTION TO BE STANDARD 300mm (12") SIZE.
- ALL VEHICULAR SIGNAL TO BE 3 SECTION UNITS EXCEPT WHERE NOTED AND ALL PEDESTRIAN SIGNALS TO BE 2 SECTION UNITS WITH I.T.E. SYMBOLIC LENSES.
- PEDESTRIAN SIGNALS TO BE INSTALLED BELOW VEHICULAR SIGNAL WHERE APPLICABLE.
- TRAFFIC EQUIPMENT TO BE BLACK.
- INTERNALLY ILLUMINATED "NO LEFT TURN" SIGN
- OPTICALLY PROGRAMMED SIGNAL



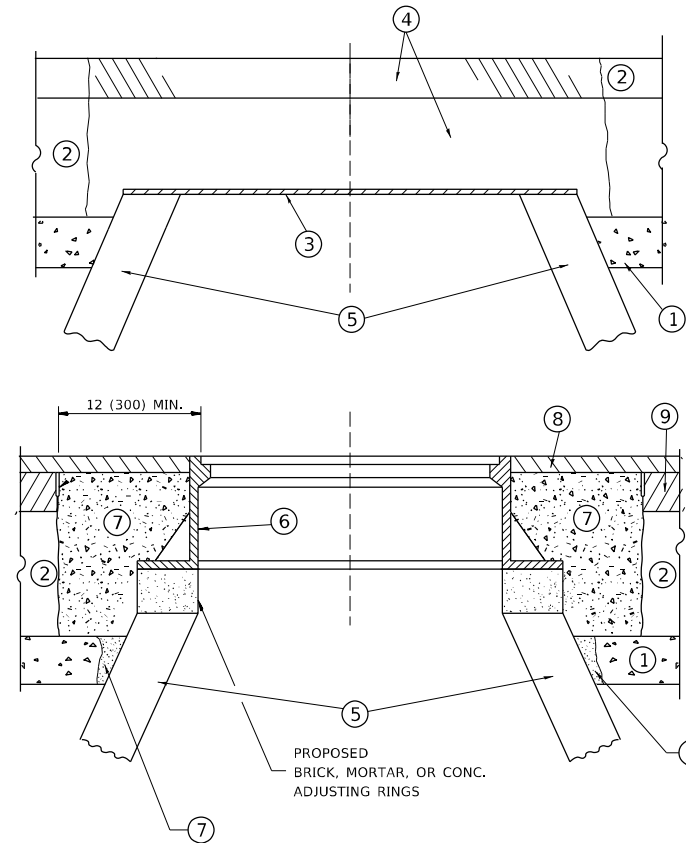
CABLE AND SIGNAL PLAN
NOT TO SCALE

FEDERAL AID ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAP 347	98-80227-00-RS	COOK	30	16
FHWA REGION NO. 5		ILLINOIS	PROJECT	

DATE	REVISION
WORK ORDER NO.	DATE
COST ALLOCATION ACCOUNT	
APPROPRIATION ACCOUNT	MATERIAL
	LABOR
ROOSEVELT ROAD IMPROVEMENT	
OGDEN AVE. TO ASHLAND AVE.	
TRAFFIC CONTROL SIGNALS	
CABLE AND SIGNAL PLAN	
ROOSEVELT ROAD & OGDEN AVENUE	
CITY OF CHICAGO	
DEPT. OF STREETS AND SANITATION	
BUREAU OF ELECTRICITY	
DIVISION OF ELECTRICAL ENGINEERING	
DRAFTSMAN	ENGINEER
CHIEF DRAFTSMAN	M. SAMADI
SUPERVISING ENGINEER	ELEC. DESIGN ENGR.
DOUG CARLSON	
ENGINEER OF ELECTRICITY	OWG. NO.
	23078C
SUPT. OF CONSTRUCTION	
DEPUTY COMMISSIONER	
SIZE:	SCALE:
	DATE:

SHEET 3 OF 3

ROOSEVELT ROAD IMPROVEMENT	CITY OF CHICAGO
OGDEN AVE. TO ASHLAND AVE.	DEPARTMENT OF TRANSPORT
TRAFFIC CONTROL SIGNALS	BUREAU OF HIGHWAYS
CABLE AND SIGNAL PLAN	DRAWN: AMB
	CHECKED: DAJ
	APPROVED:
ROOSEVELT ROAD & OGDEN AVENUE	DATE: 03-24-2000
TYLIN INTERNATIONAL-BASCOR	SCALE: NONE
	CONTRACT NO.:
	PROJECT NO.:



NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1 *CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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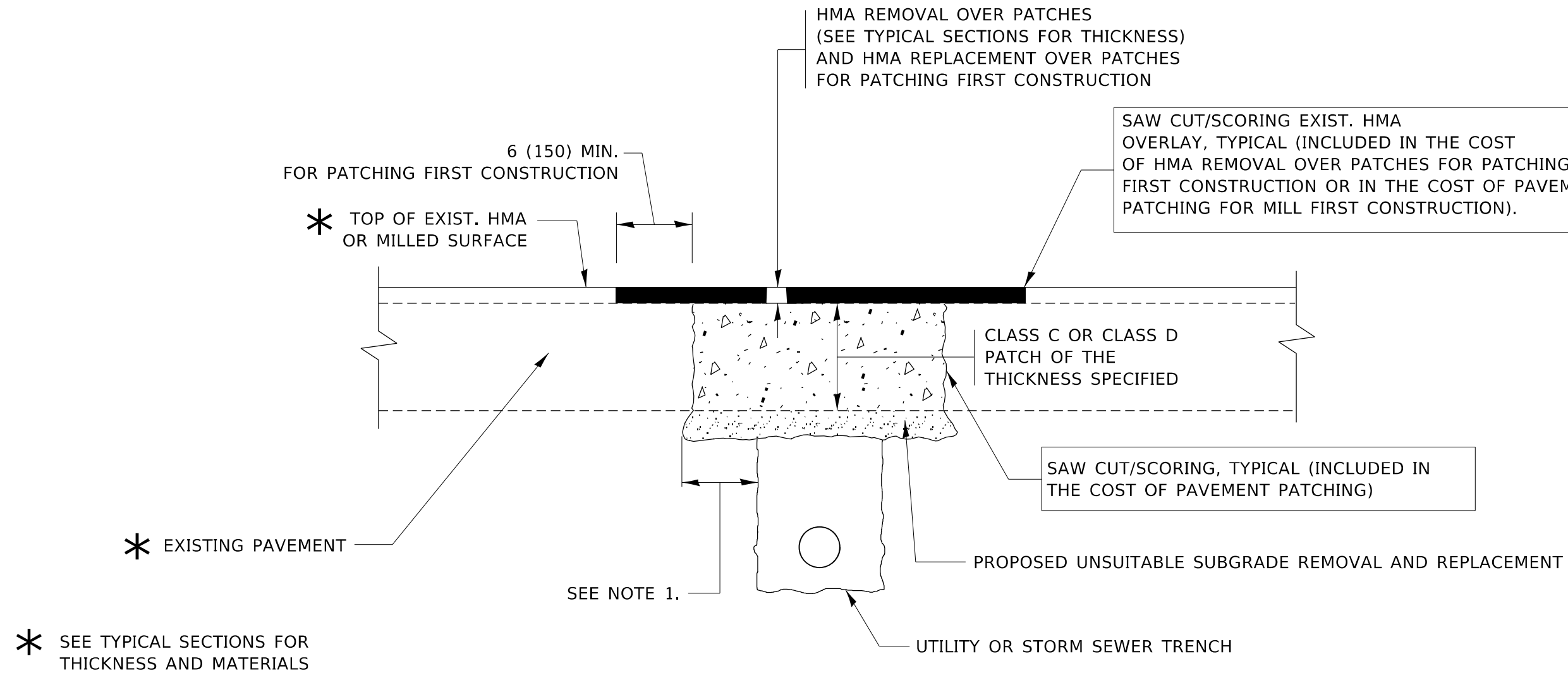
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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 12/15/2021	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-R5 & SW	COOK	30	17
BD600-03 (BD-8)		CONTRACT NO. 62M44		
ILLINOIS FED. AID PROJECT				



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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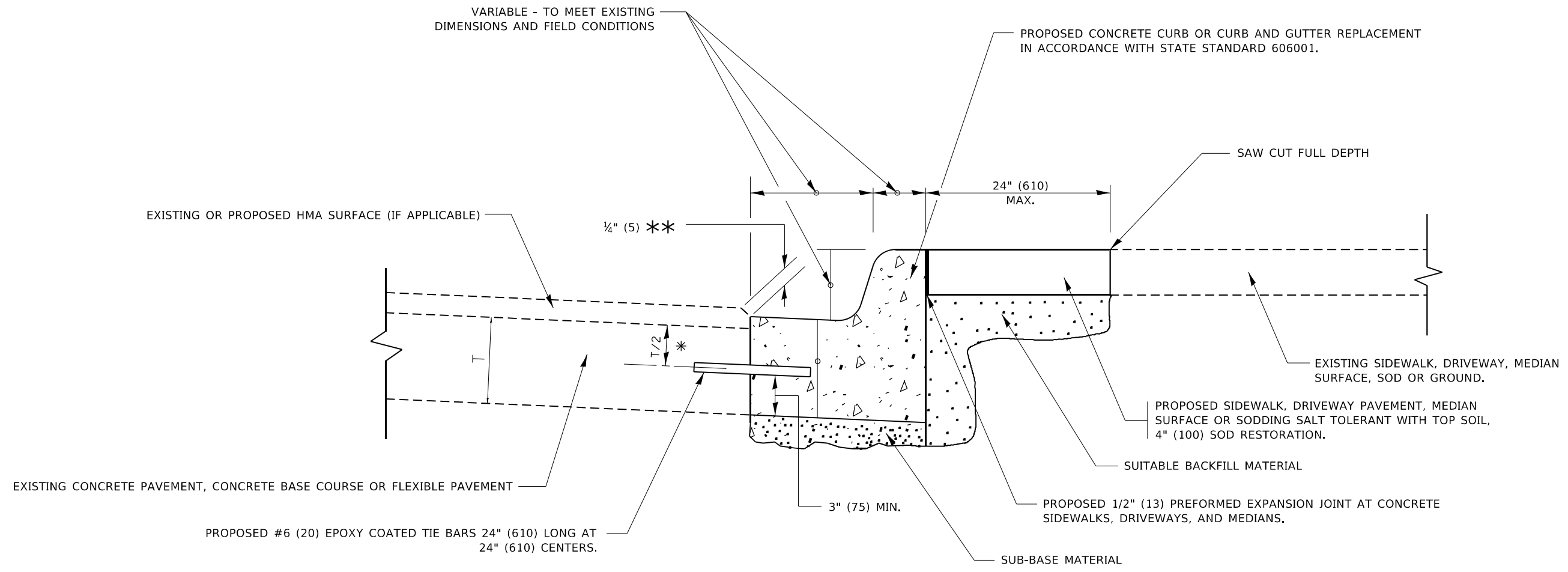
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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - R. BORO 09-04-07
PLOT DATE = 12/15/2021	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-R5 & SW	COOK	30	18
BD400-04 (BD-22)		CONTRACT NO. 62M44		
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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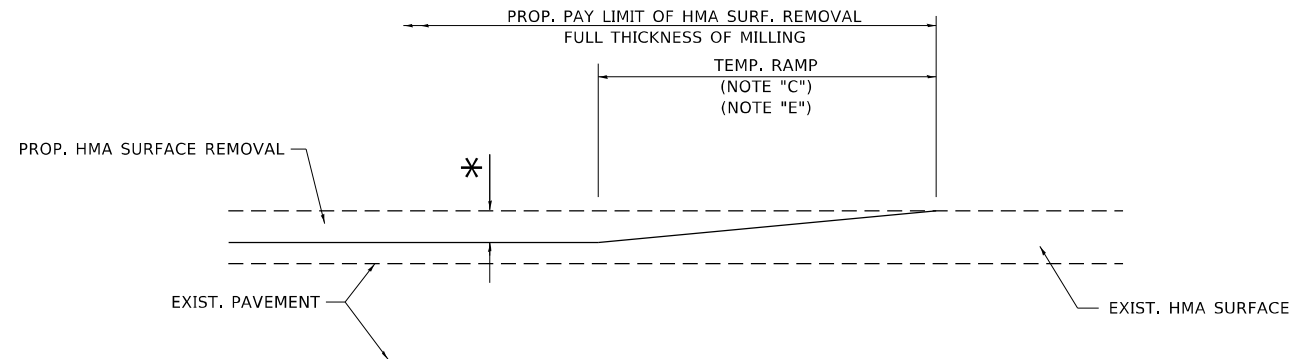
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PLOT DATE = 12/15/2021	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

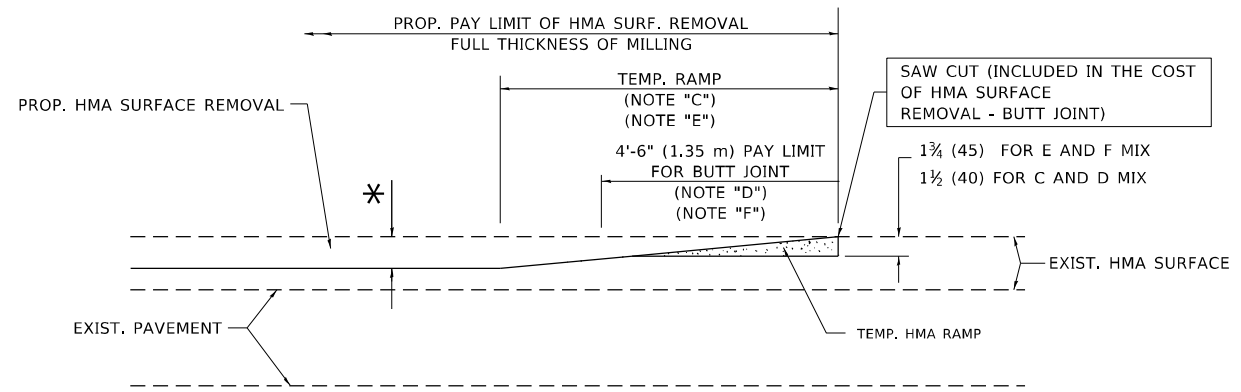
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3542	2020-156-R5 & SW	COOK	30	19
BD600-06 (BD-24)			CONTRACT NO. 62M44	
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

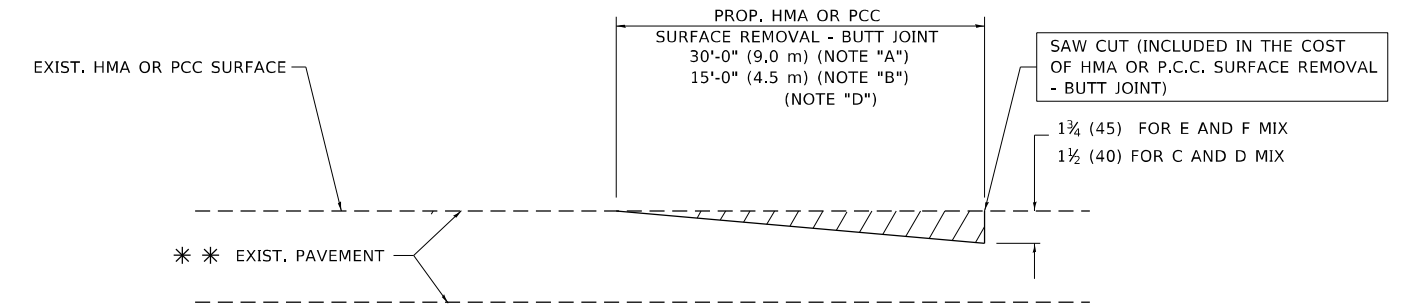


HMA CONSTRUCTED TEMPORARY RAMP

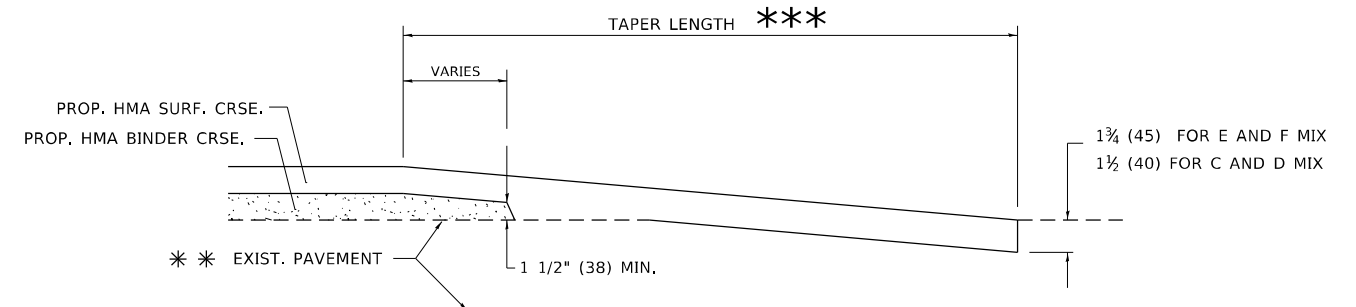
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

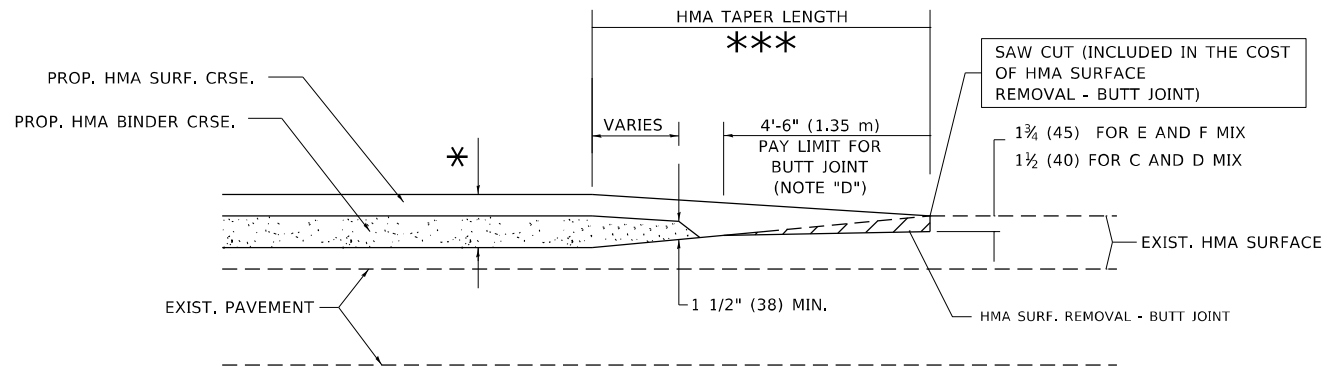
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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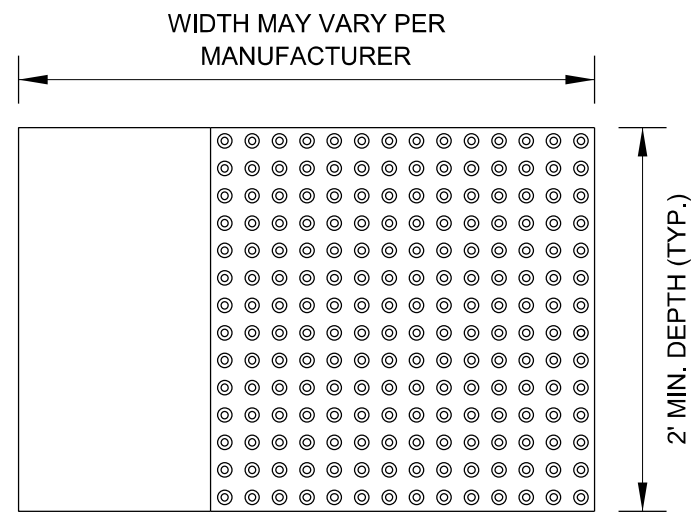
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PLOT DATE = 12/15/2021	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
		REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

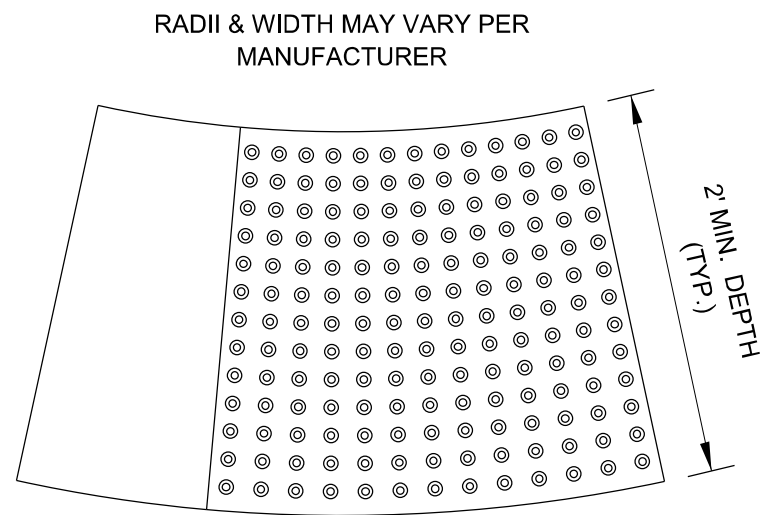
BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	20
BD400-05 BD32		CONTRACT NO. 62M44		
ILLINOIS FED. AID PROJECT				

STRAIGHT DETECTABLE WARNING UNITS



RADIAL DETECTABLE WARNING UNITS

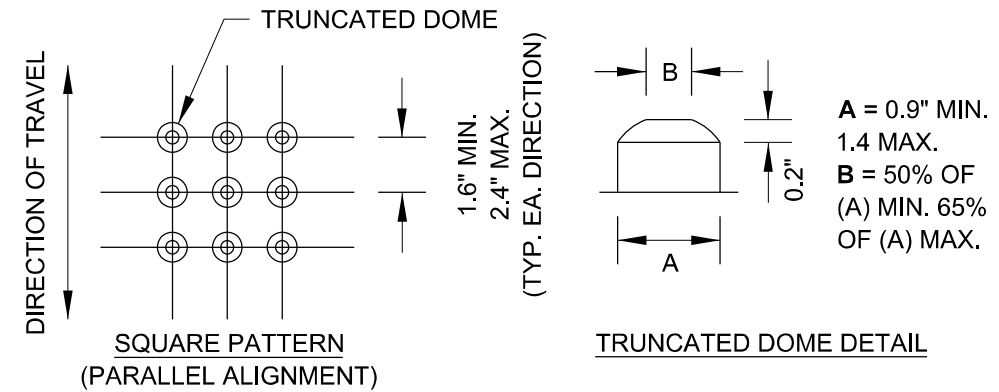


DETECTABLE WARNING UNIT SIZES

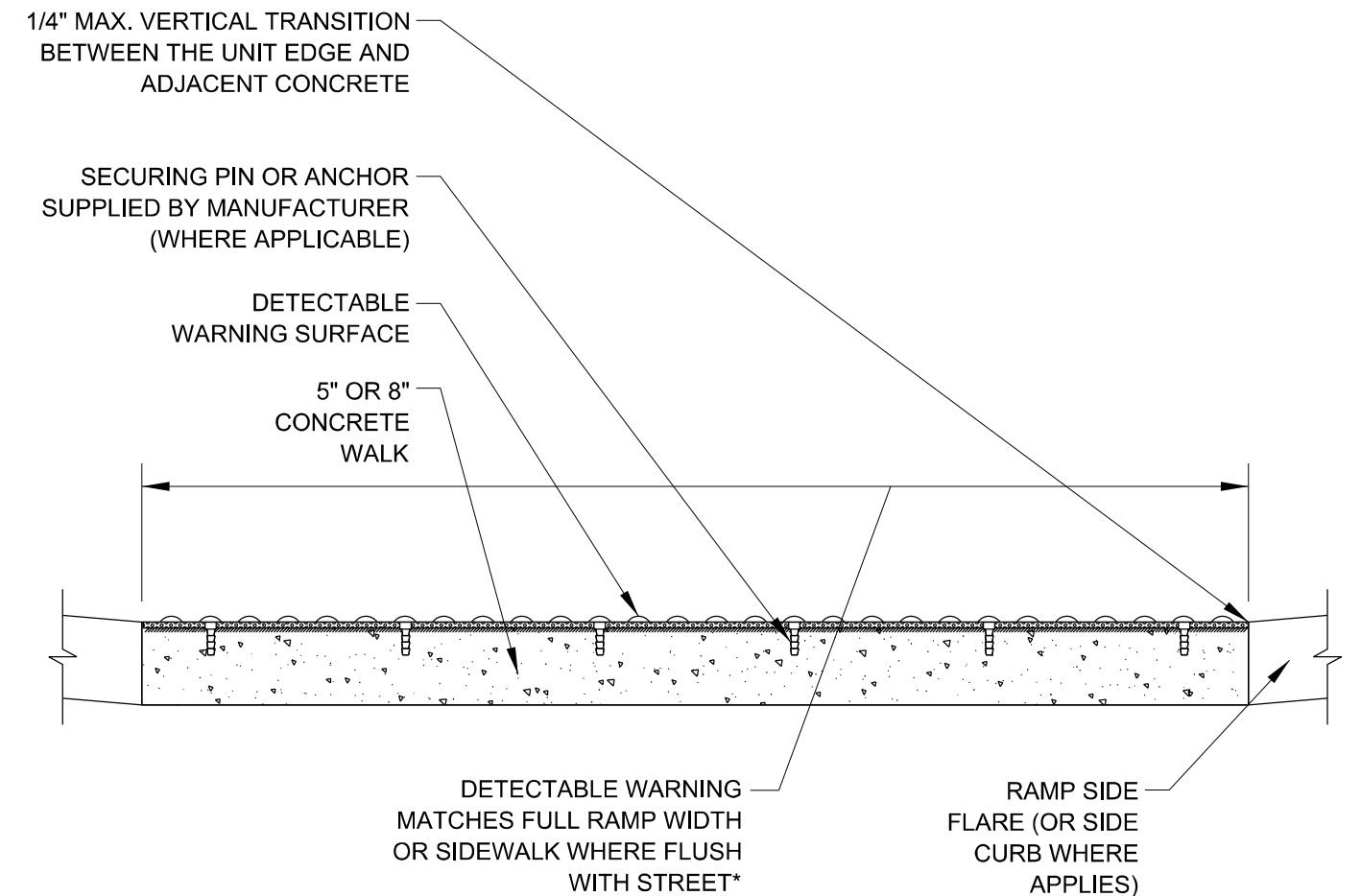
- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



UNIT PATTERN & DOME DETAIL



*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

DETECTABLE WARNING UNIT SECTION

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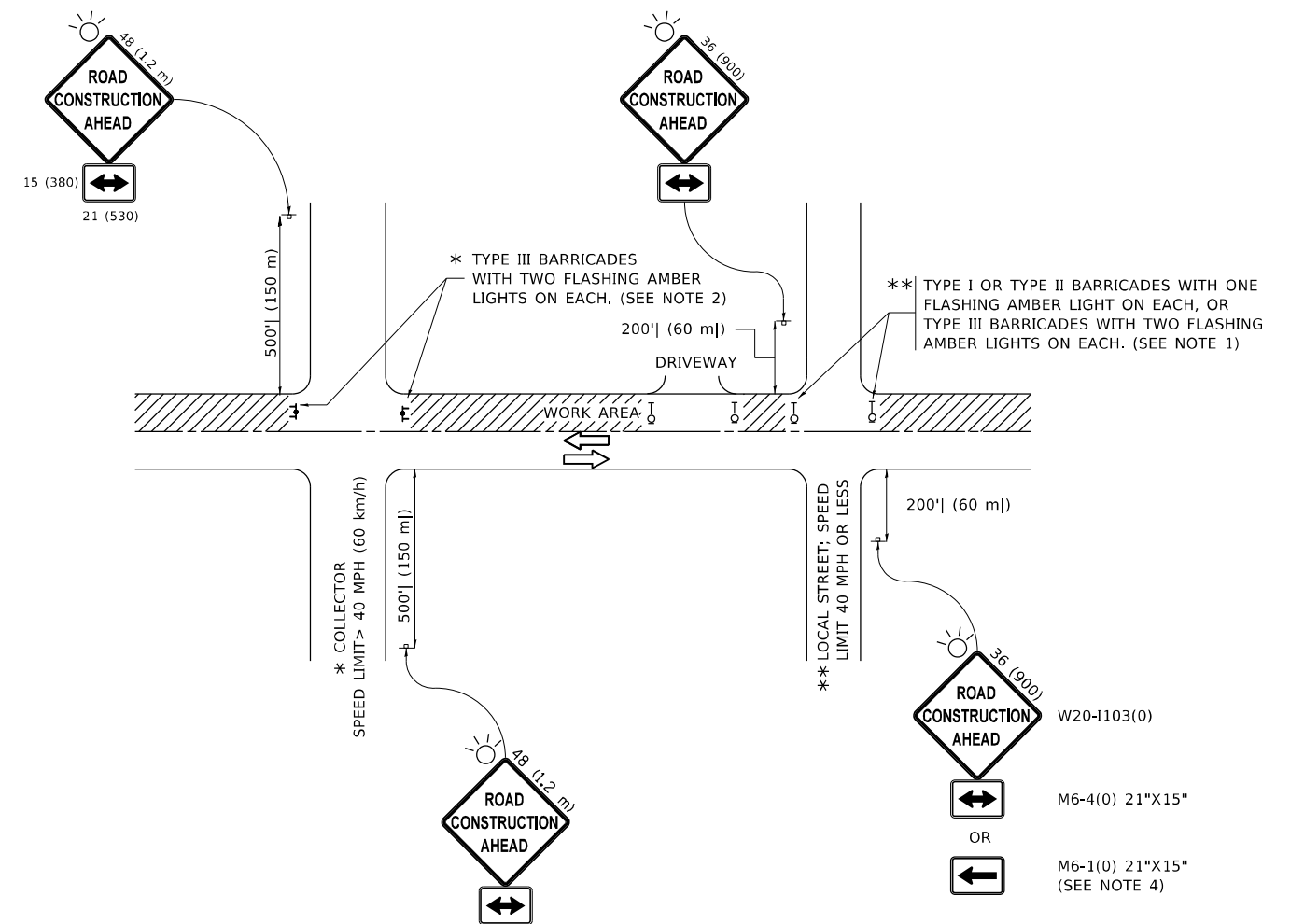
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PLOT DATE = 12/15/2021	DATE - 06-20-2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
DETECTABLE WARNINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-R5 & SW	COOK	30	21
BD 58		CONTRACT NO. 62M44		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 12/15/2021	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	22
TC-10			CONTRACT NO. 62M44	
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

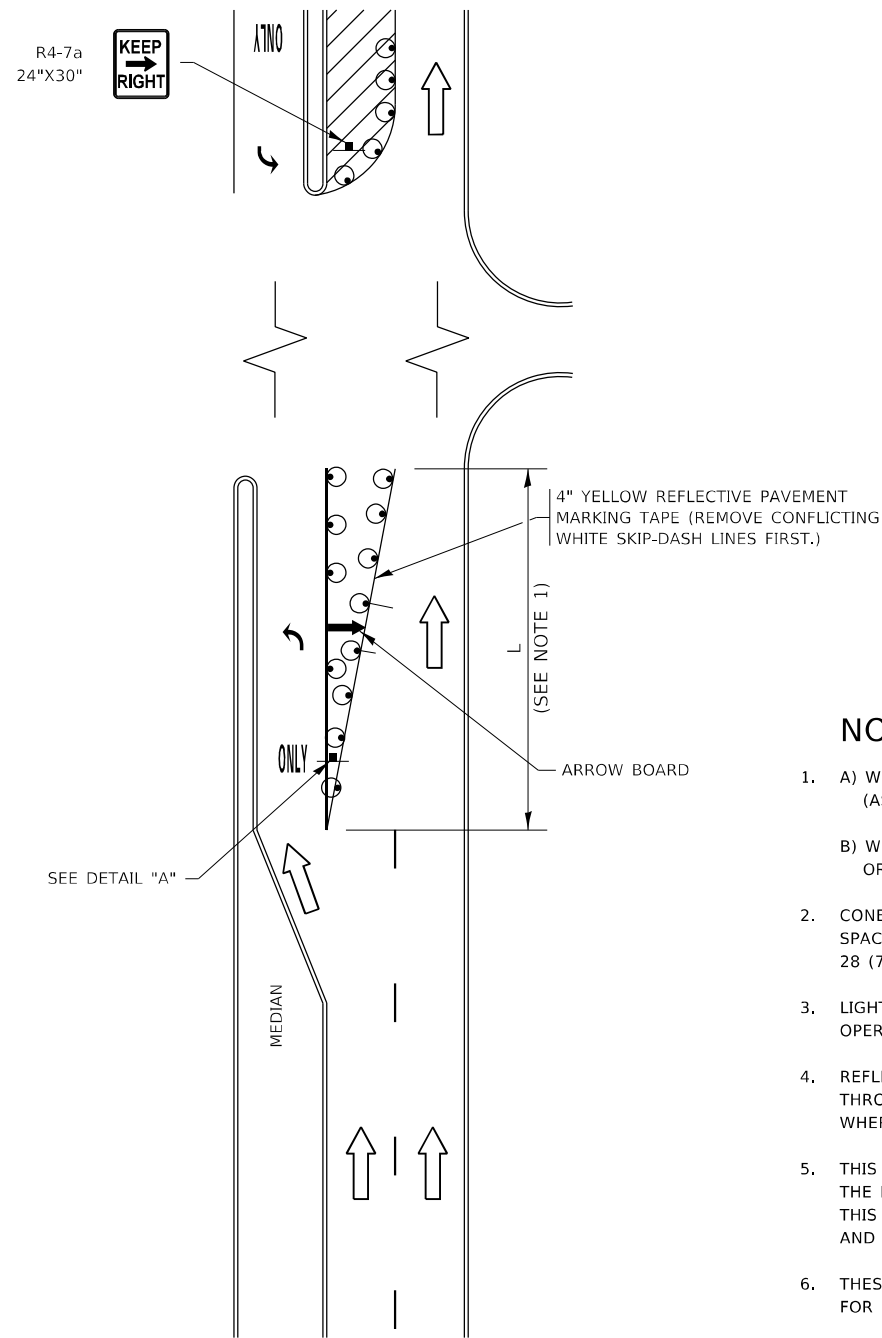


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

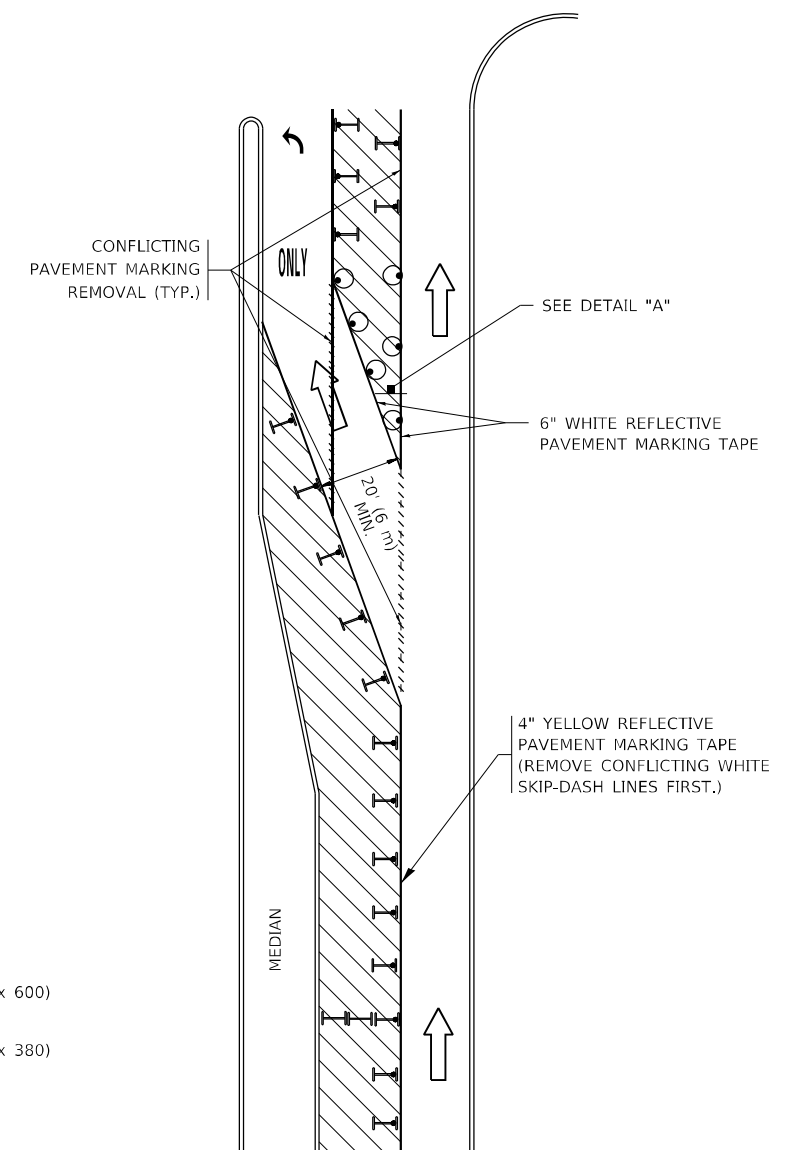
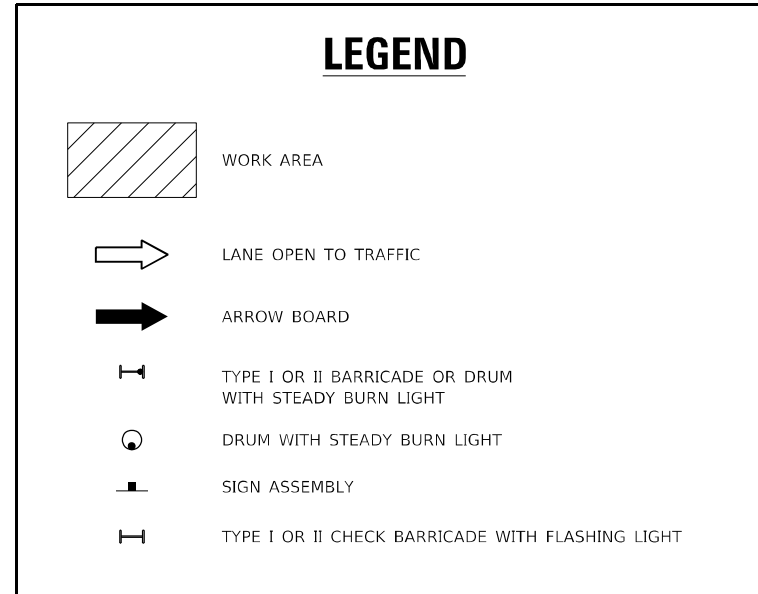
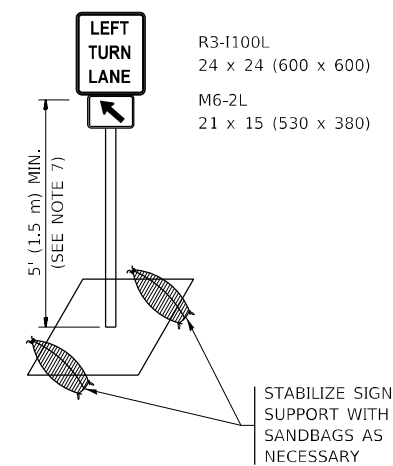


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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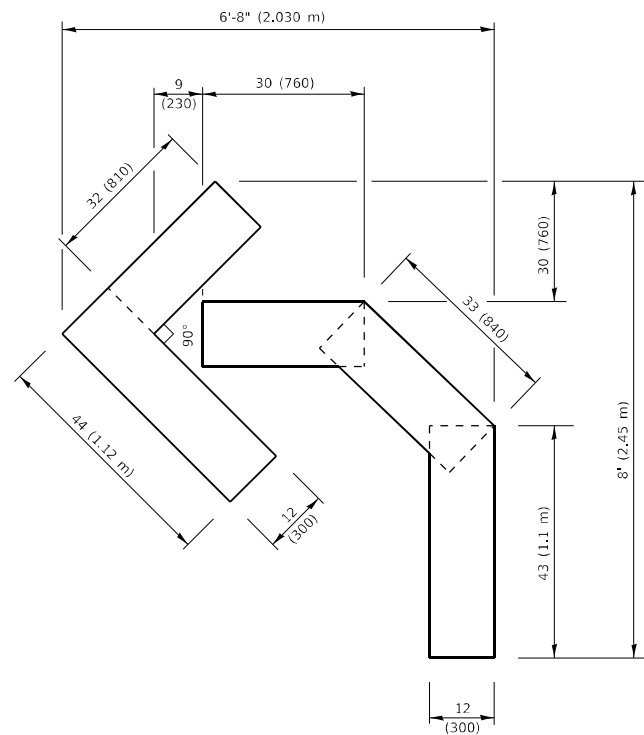
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PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 12/15/2021	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

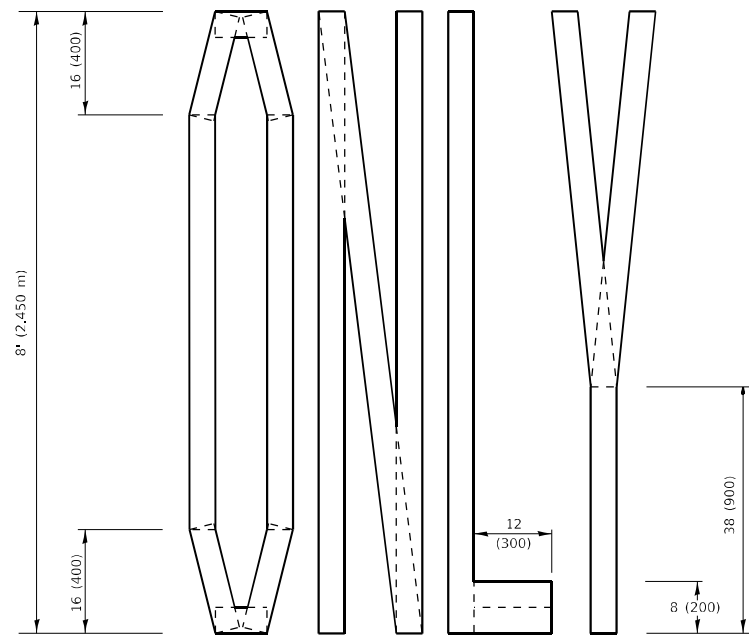
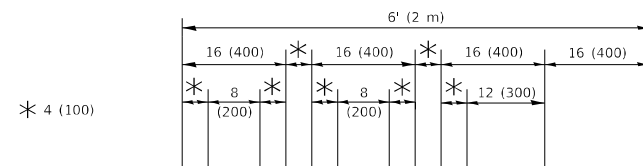
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-R5 & SW	COOK	30	23
TC-14		CONTRACT NO. 62M44		
ILLINOIS FED. AID PROJECT				



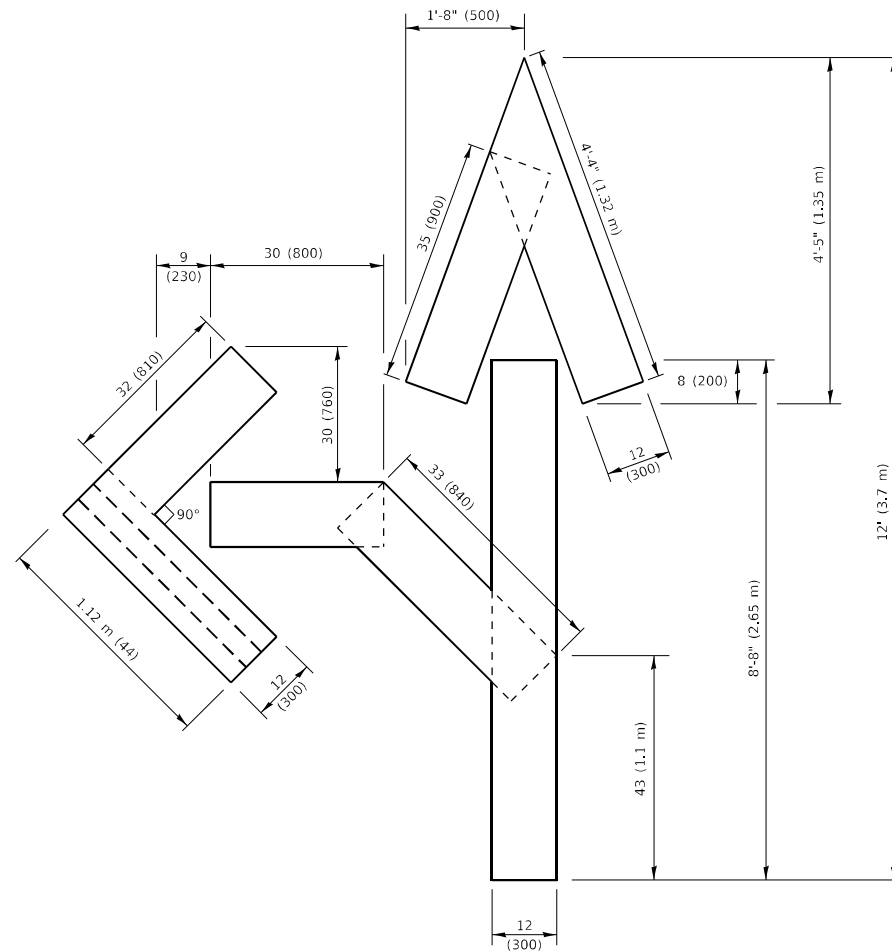
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

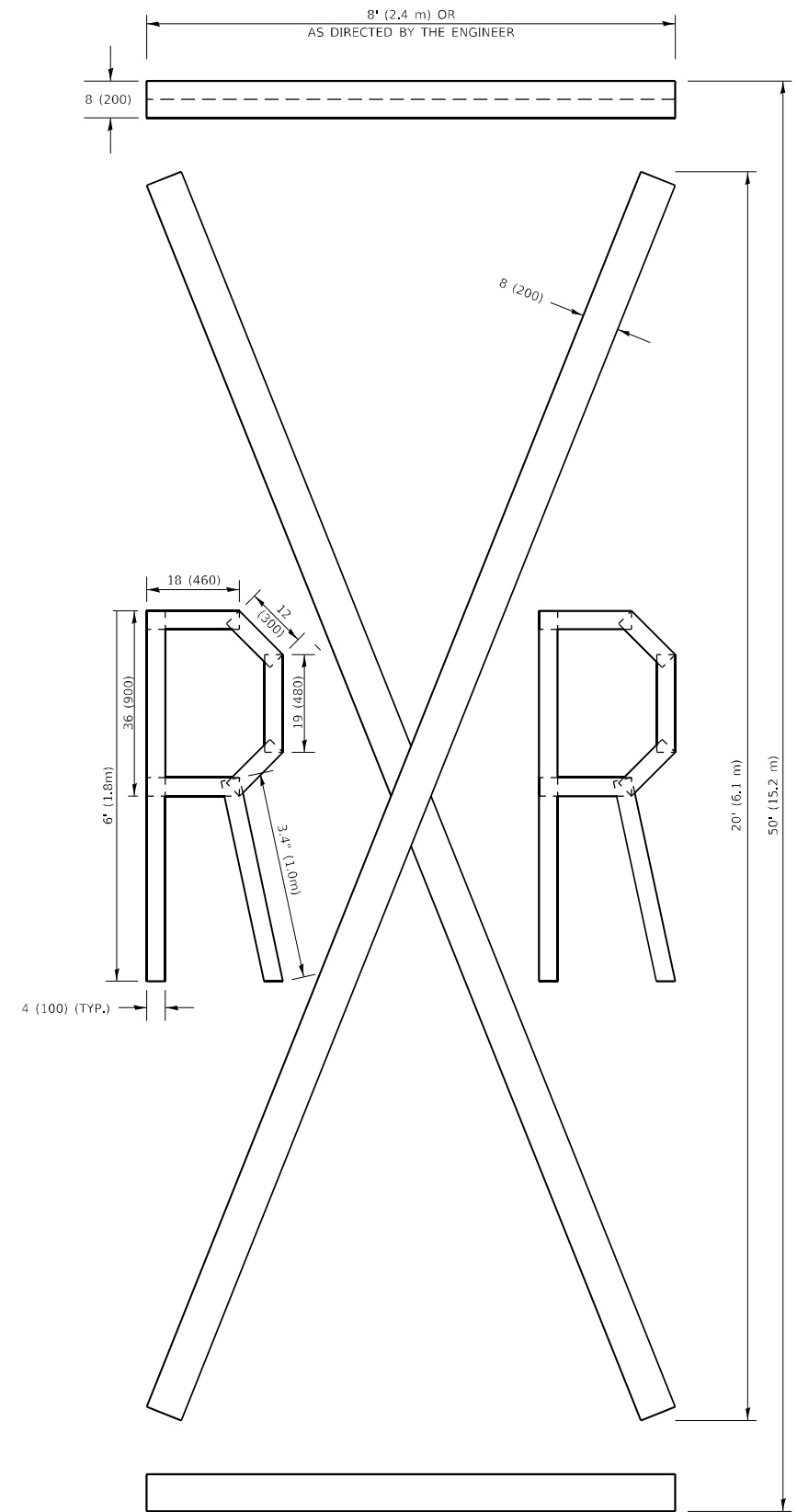


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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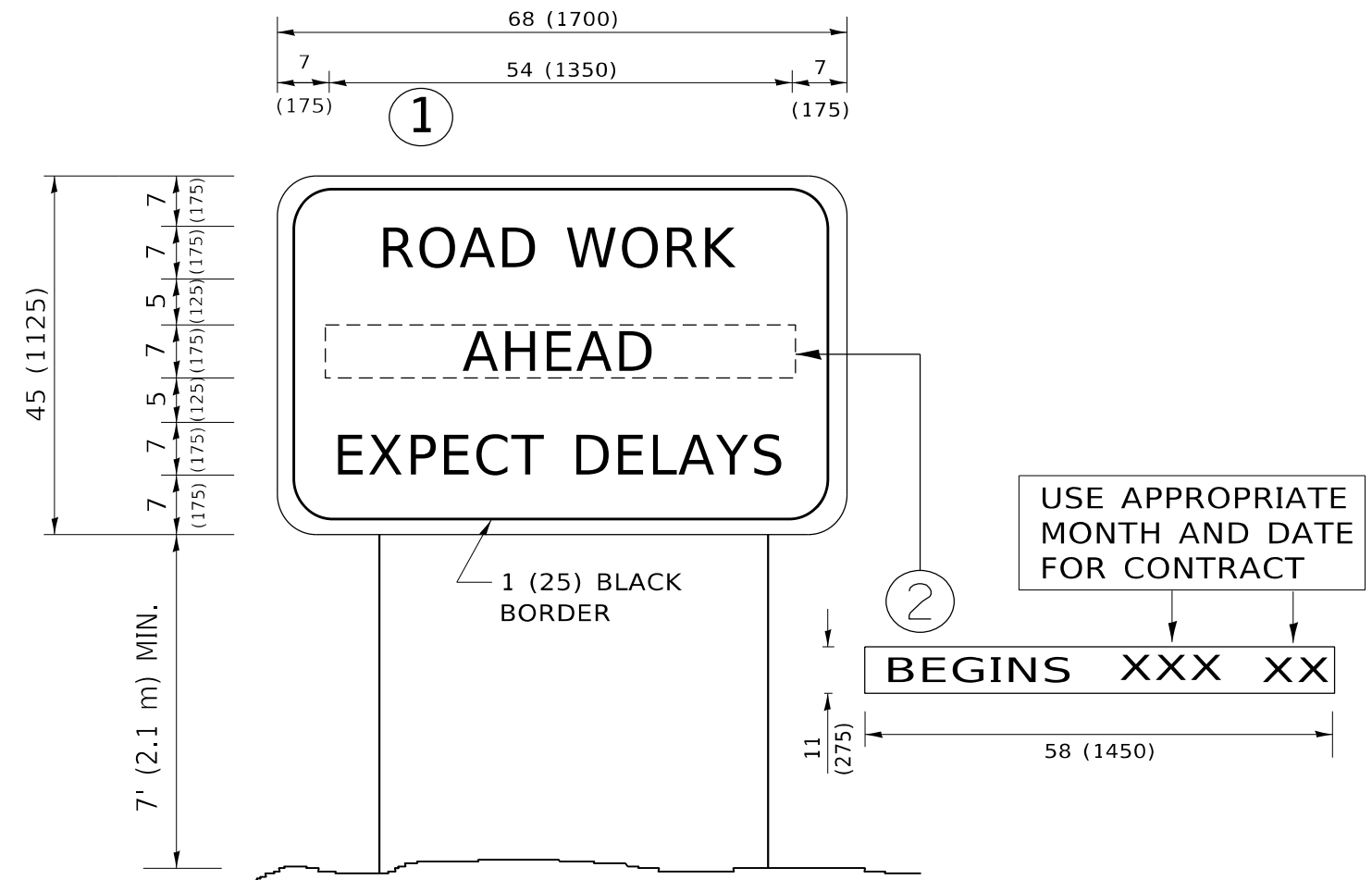
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PLOT DATE = 12/15/2021	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	24
TC-16			CONTRACT NO. 62M44	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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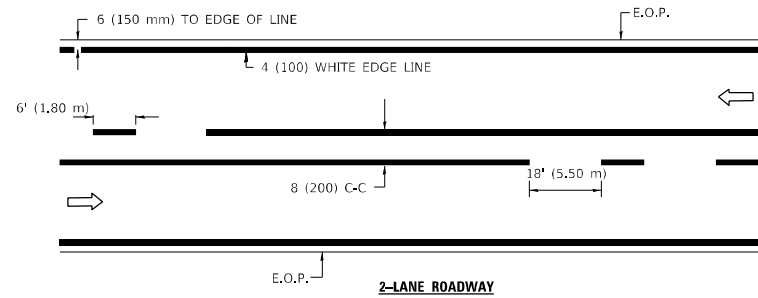
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

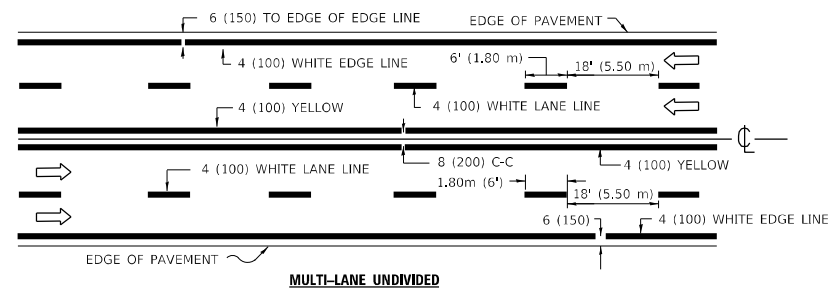
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

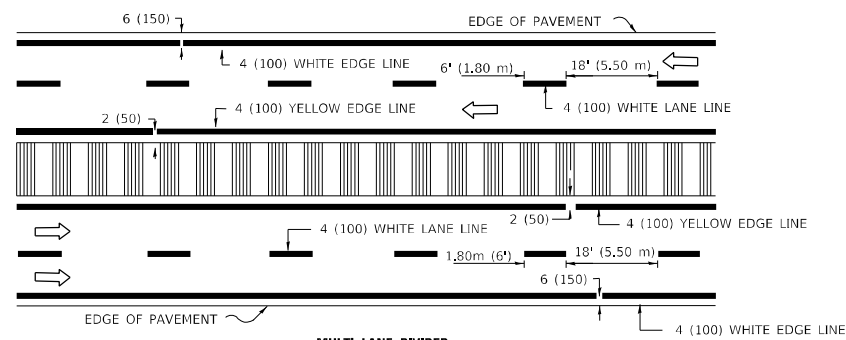
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3542	2020-156-RS & SW	COOK	30	25
TC-22			CONTRACT NO. 62M44	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



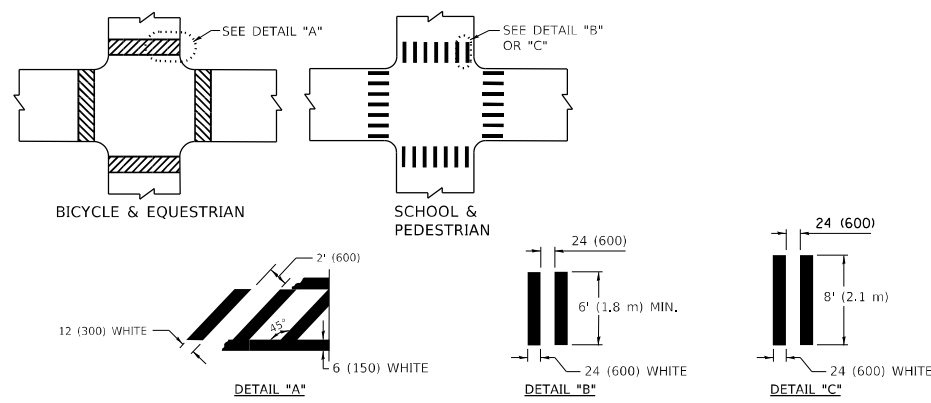
MULTI-LANE UNDIVIDED



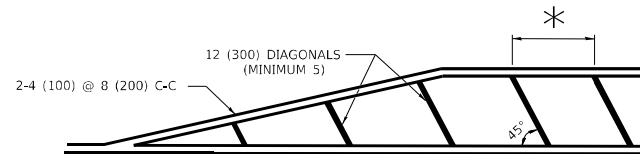
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



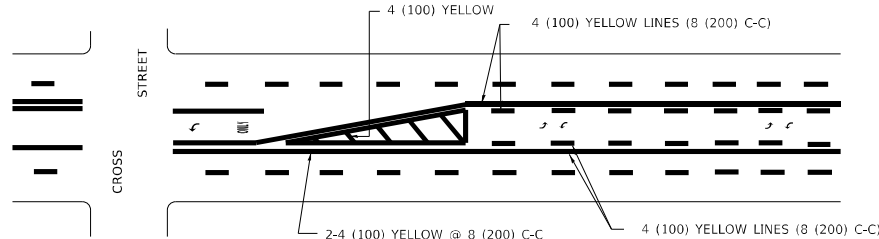
TYPICAL CROSSWALK MARKING



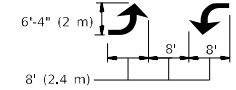
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

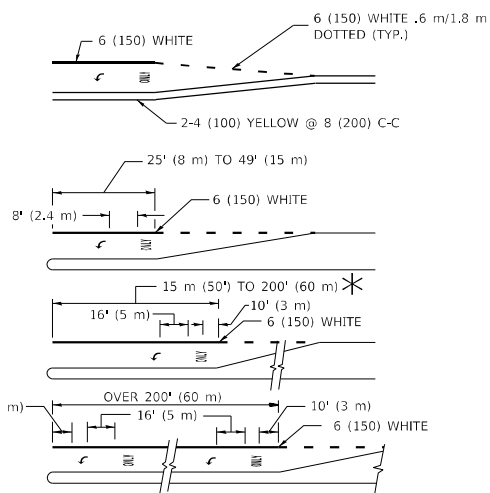


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

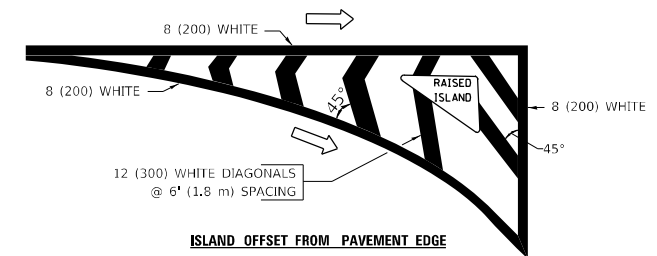


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

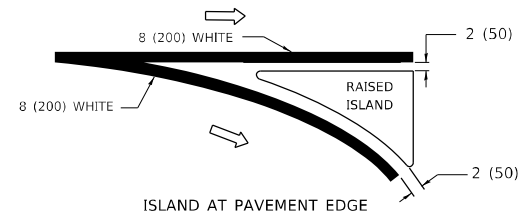
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONAL (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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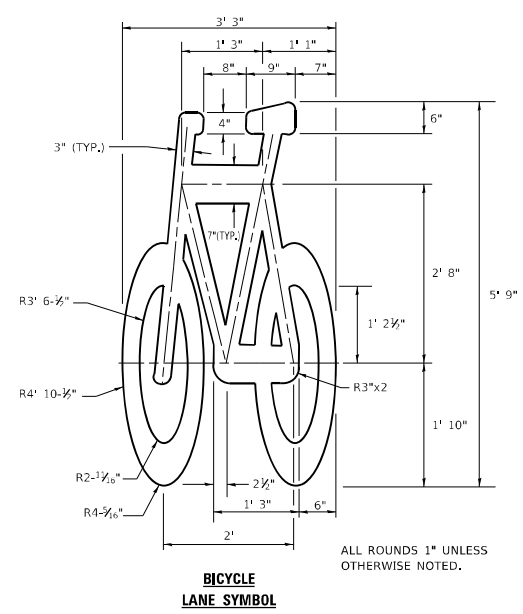
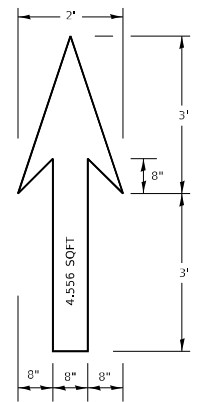
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

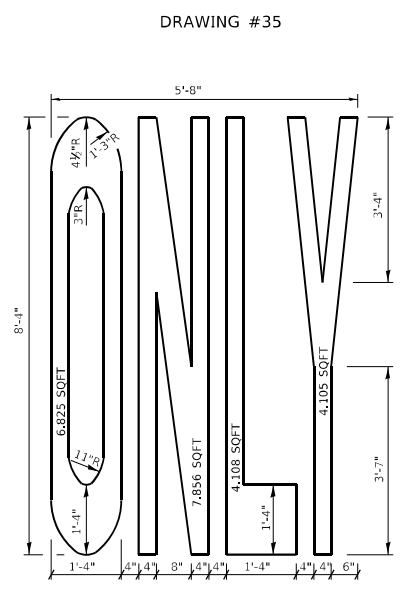
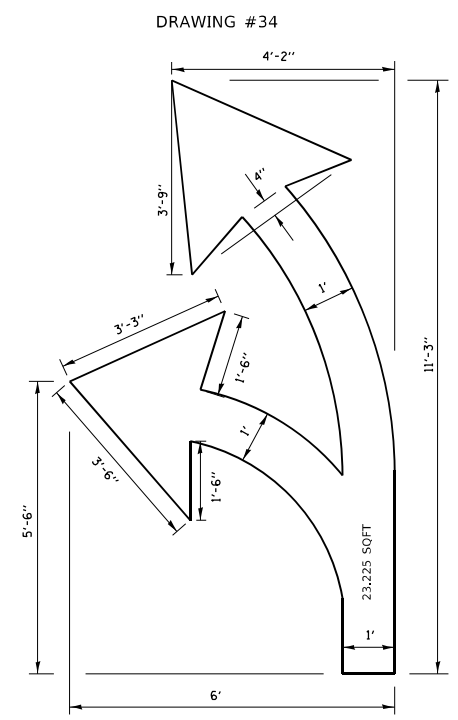
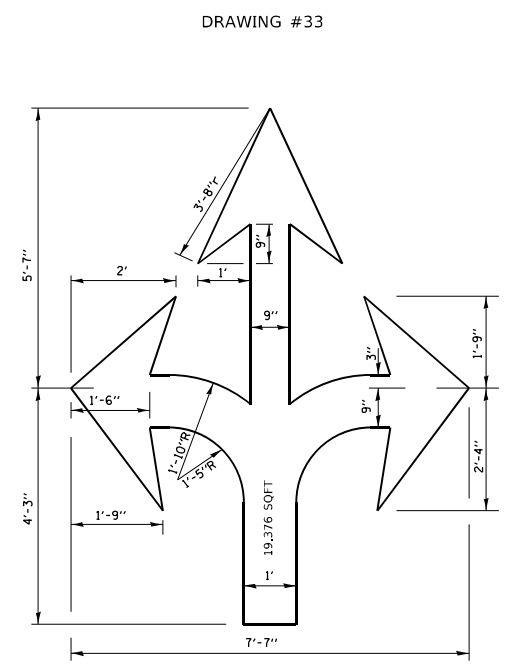
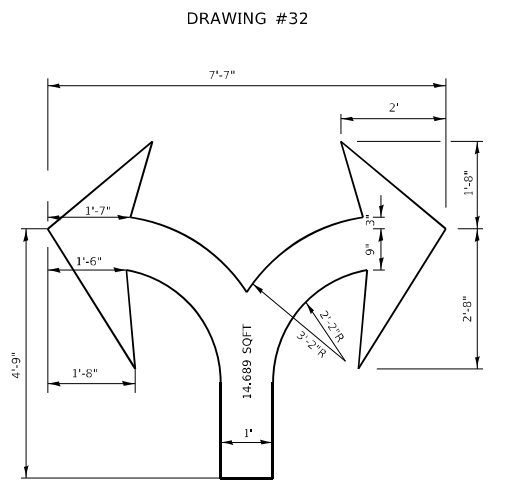
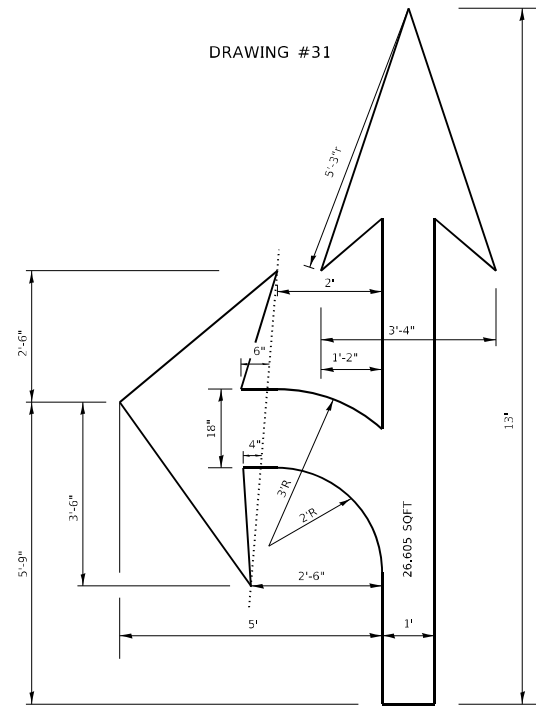
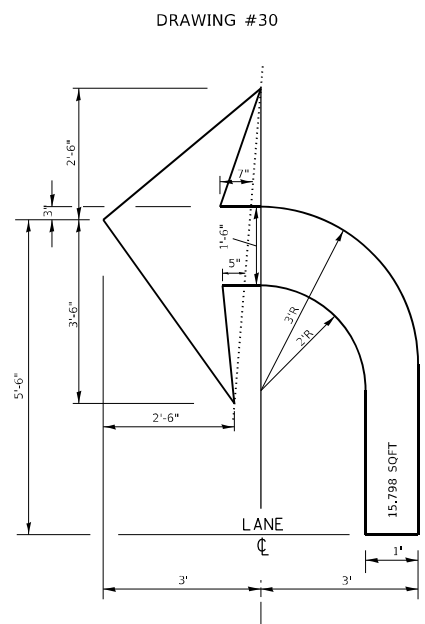
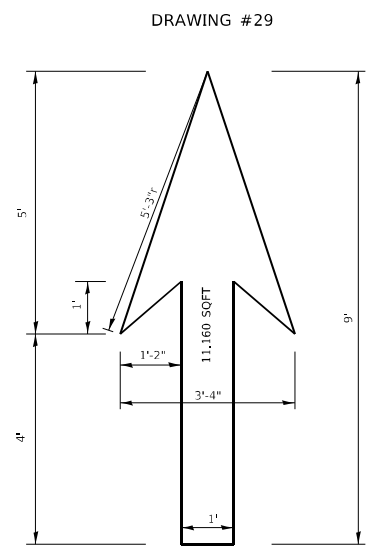
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	26
TC-24		CONTRACT NO. 62M44		
ILLINOIS		FED. AID PROJECT		



- NOTE:
- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 - THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

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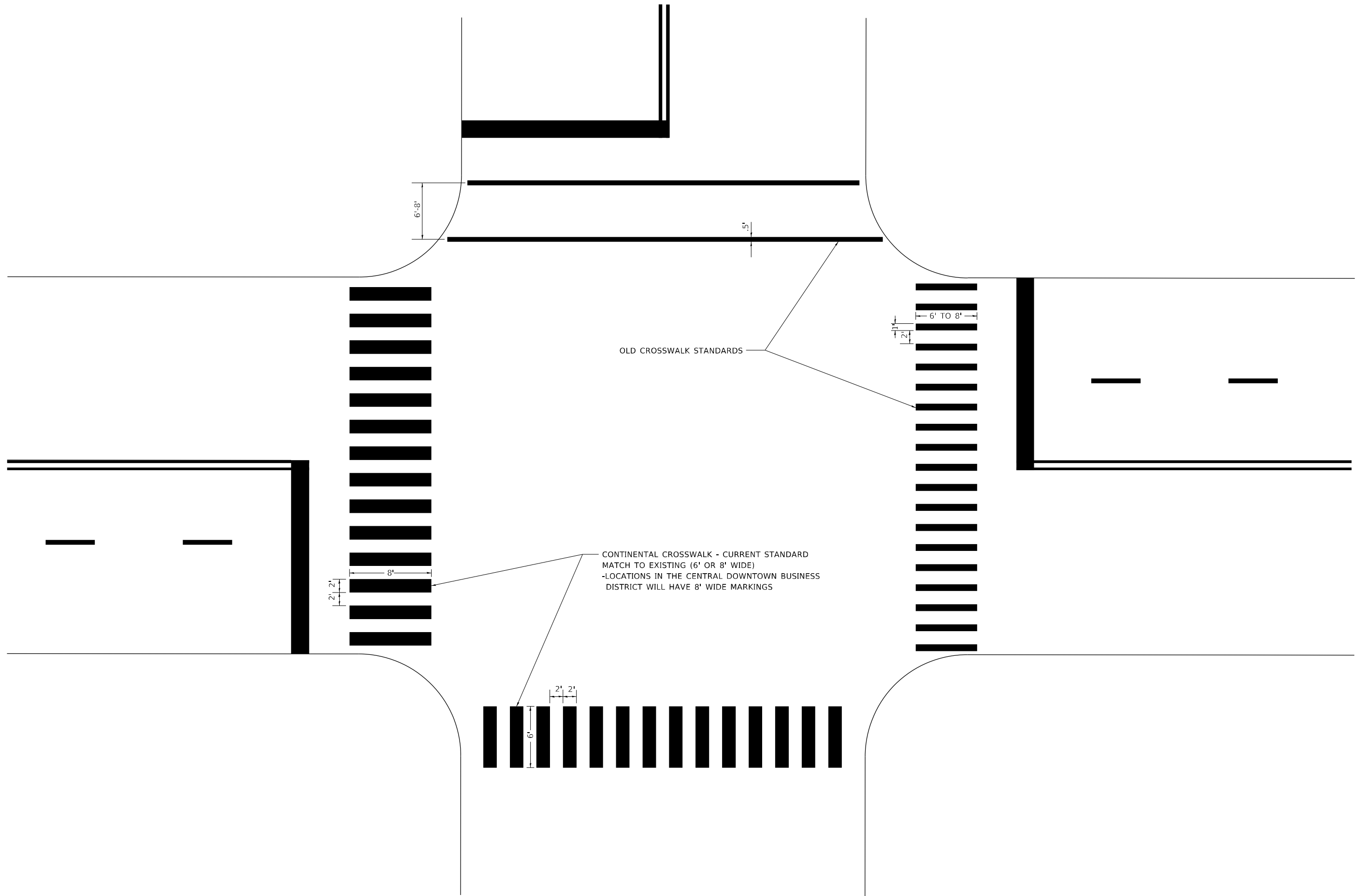
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	27
TC-24		CONTRACT NO. 62M44		
ILLINOIS FED. AID PROJECT				

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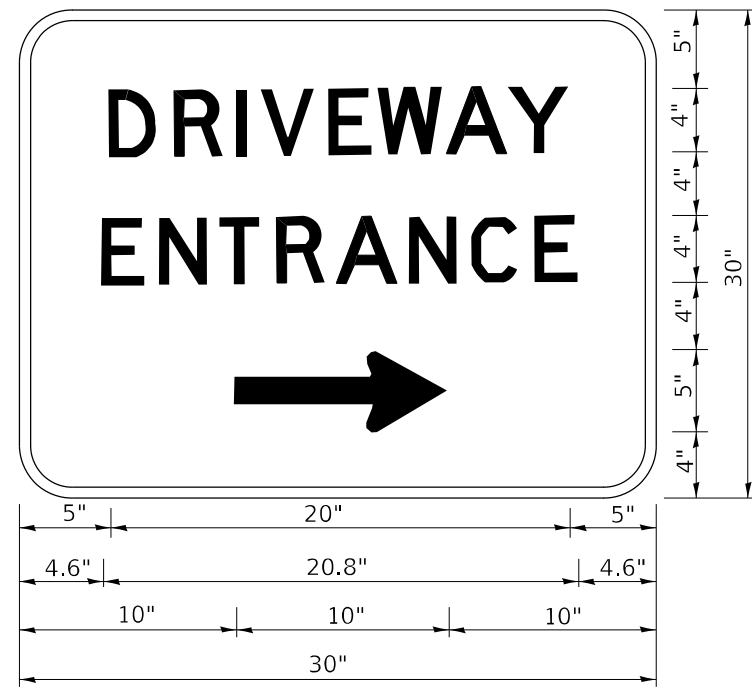
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	28
TC-24			CONTRACT NO. 62M44	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

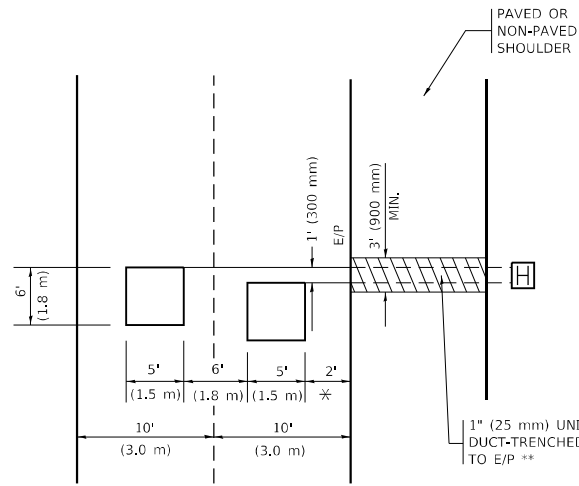
DRIVEWAY ENTRANCE SIGNING

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	29
TC-26			CONTRACT NO. 62M44	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

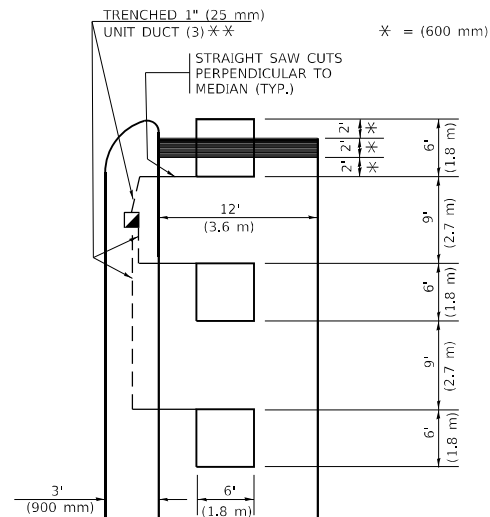


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.

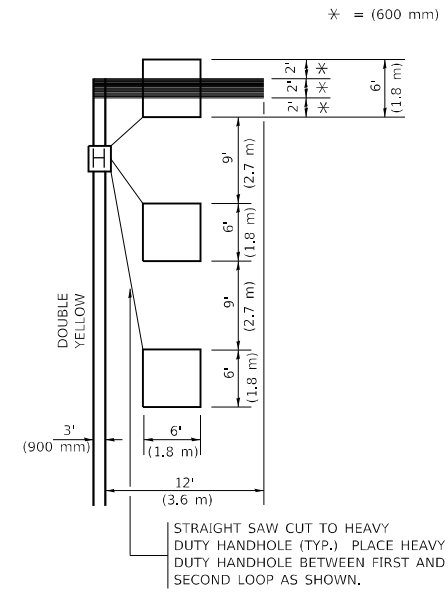


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



* = (600 mm)

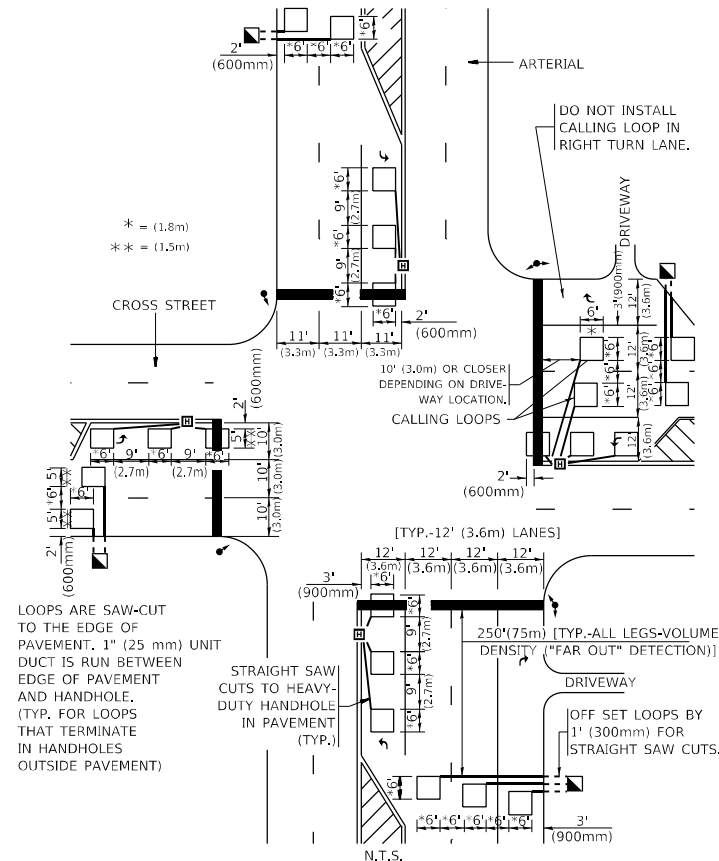
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

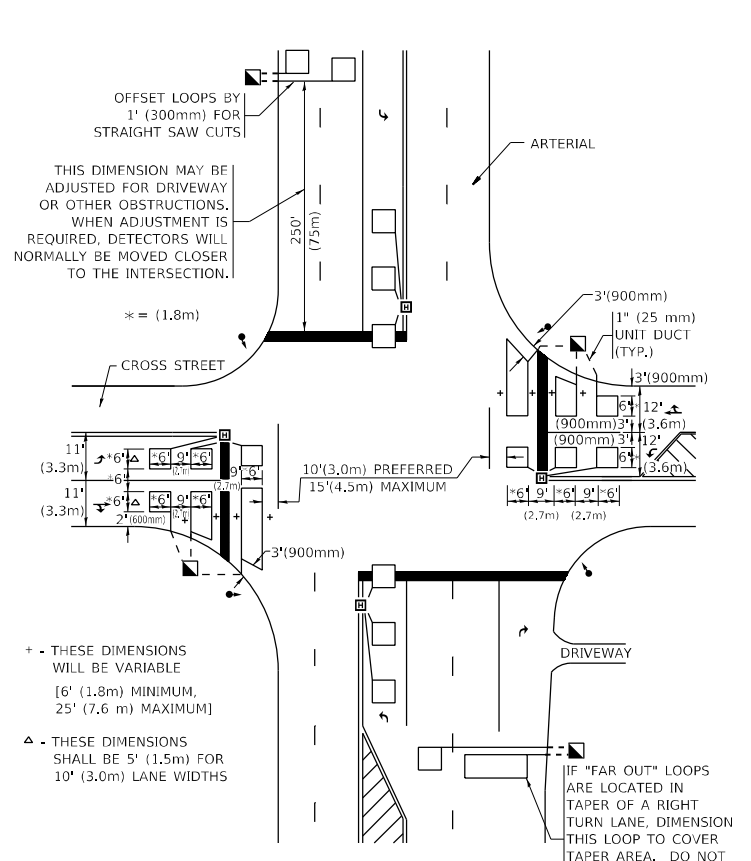
**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)**



* = (1.8m)
** = (1.5m)

**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



* = (1.8m)

+ - THESE DIMENSIONS
WILL BE VARIABLE
[6' (1.8m) MINIMUM,
25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS
SHALL BE 5' (1.5m) FOR
10' (3.0m) LANE WIDTHS

**DETAIL 2
N.T.S.**

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLOT DATE = 12/15/2021	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT 1 - DETECTOR LOOP INSTALLATION			
DETAILS FOR ROADWAY RESURFACING			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3542	2020-156-RS & SW	COOK	30	30
TS-07		CONTRACT NO. 62M44		
ILLINOIS		FED. AID PROJECT		