

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	1
		ILLINOIS	CONTRACT NO. 62N45	

FOR INDEX OF SHEETS AND HIGHWAY STANDARDS, SEE SHEET NO. 2

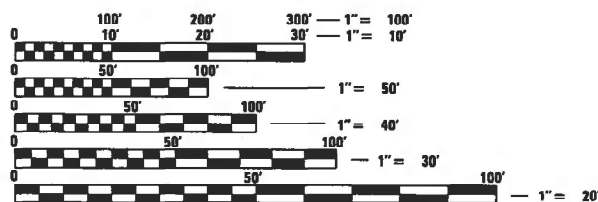
THIS PROJECT IS LOCATED IN THE CITY OF WOODSTOCK

**PROPOSED
HIGHWAY PLANS**
FAP ROUTE 305: US 14
HARTLAND ROAD TO IL ROUTE 47
SECTION: 2021-022-RS
PROJECT: NHPP-N8JF(892)
SMART OVERLAY
MCHENRY COUNTY

C-91-118-21

TRAFFIC DATA

2019 ADT = 14,500 VPD
POSTED SPEED LIMIT = 55 MPH

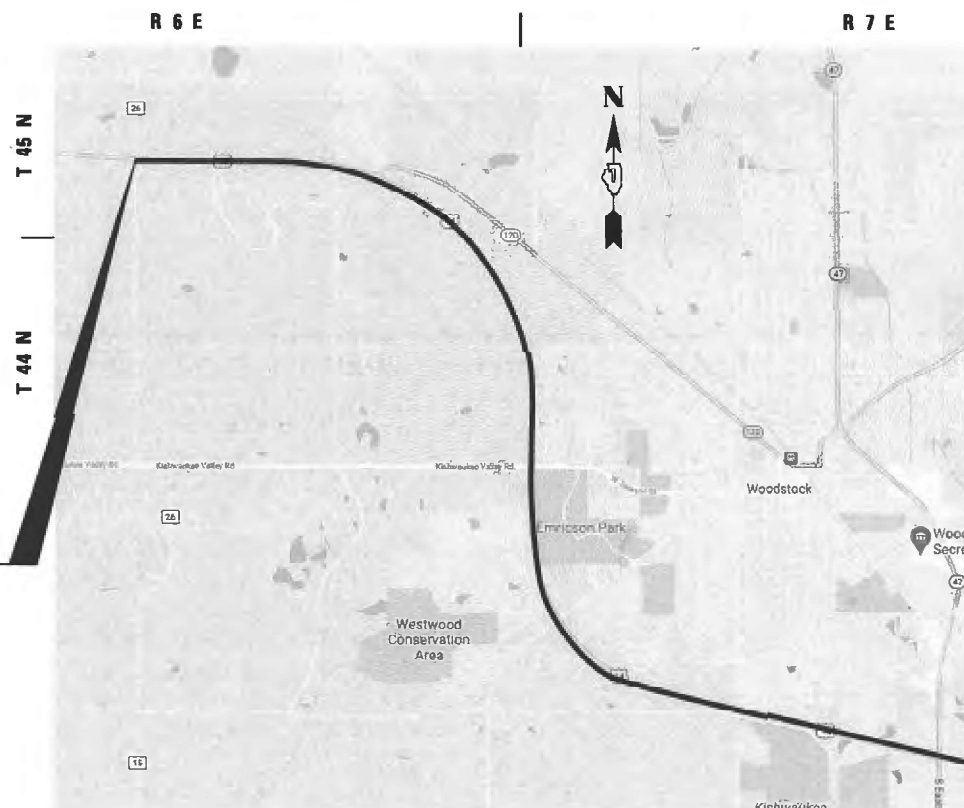


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: LUKASZ POCIECHA (847) 705-4255
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4211

CONTRACT NO. 62N45



PROJECT BEGINS
STA 19+14

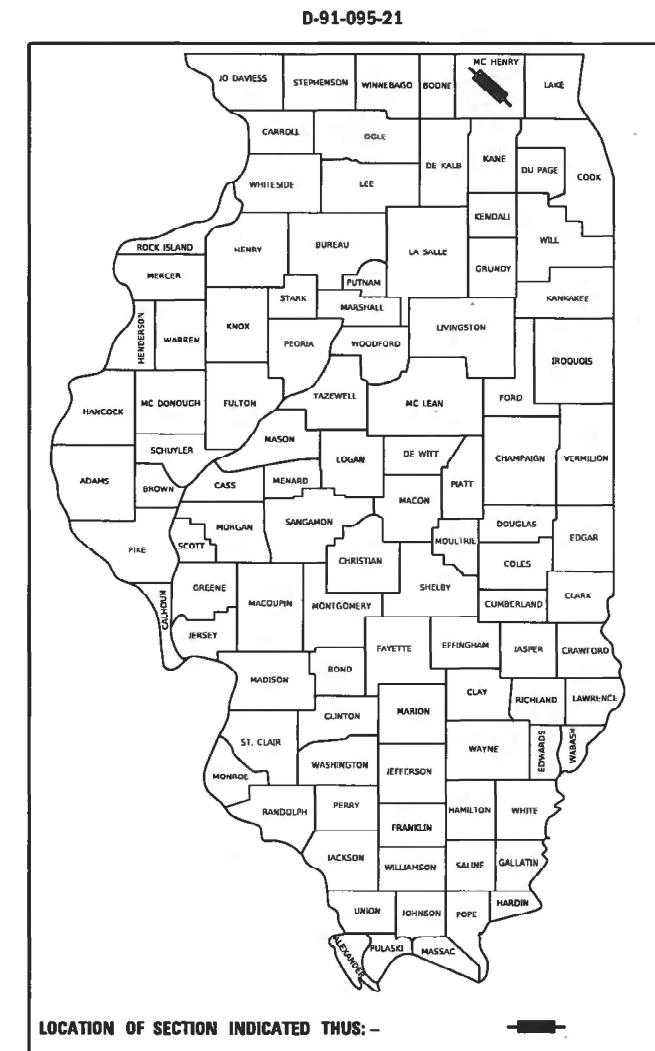
PROJECT ENDS
STA 331+05

HARTLAND TOWNSHIP

GROSS LENGTH = 31,191 FT. = 5.91 MILE
NET LENGTH = 31,191 FT. = 5.91 MILE



Alex Lane
ALEXANDER CARL LANE, P.E.
IL LIC. NO. 062-063261
EXP: 11/30/2023
DATE: 12/02/2021



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED December 9, 2021

Alex Lane
REGIONAL ENGINEER

February 4, 2022

Stephen M. Lavin
ENGINEER OF DESIGN AND ENVIRONMENT

February 4, 2022

Stephen M. Lavin
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

INFRASTRUCTURE
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CONTACT: ALEXANDER LANE (312) 477-0620

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-6	TYPICAL SECTIONS
7-17	ROADWAY AND PAVEMENT MARKING PLANS
18-19	DRYWELL
20-22	DETECTOR LOOP REPLACEMENT PLAN
23	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
24	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
25	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
26	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
27	RUMBLE STRIPS FOR CENTERLINE, NON-FREEWAY (BD-55)
28	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
29	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
30	TYPICAL PAVEMENT MARKINGS (TC-13)
31	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
32	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
33	ARTERIAL ROAD INFORMATION SIGN (TC-22)
34	DRIVEWAY ENTRANCE SIGNING (TC-26)
35	DETECTOR LOOP SHOULDER DETAILS (TS-07)

STATE STANDARDS

STANDARD NO.	DRAWING NAME
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
642001-03	SHOULDER RUMBLE STRIPS, 16 IN.
642006-01	SHOULDER RUMBLE STRIPS, 8 IN.
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS ONLY, FOR SPEED ≥ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS DAY ONLY
701336-07	LANE CLOSURE, 2L, 2W, WORK AREA IN SERIES, FOR SPEED ≥ 45MPH
701421-08	LANE CLOSURE, 2L, 2W, MULTILANE, DAY OPERATIONS ONLY, FOR SPEED ≥ 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, 2L, 2W, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEED ≥ 45 MPH TO 55 MPH
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-08	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF WOODSTOCK AND MCHENRY COUNTY DIVISION OF TRANSPORTATION.
- FRAME AND GRATE ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV, GOV, A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURE SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D AND CLASS B PATCHING.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIANS.
- CATCH BASINS, MANHOLES, INLETS, DRAINAGE STRUCTURES AND VALVE VAULTS ADJUSTMENT AND/OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS, UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING, IN STAGE 2, AFTER PAVEMENT MILLING, THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE, THE LANE SHALL BE FULLY OPEN TO TRAFFIC AT THE END OF EACH WORK SHIFT.
- WHEN MILLED PAVEMENT OPENS TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER, AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- ANY PAVEMENT MARKINGS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STRESS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE III TAPE SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
- ALL EXISTING R.O.W SHOWN IS APPROXIMATE AND MAY NEED TO BE VERIFIED IN THE FIELD. ANY R.O.W. CONFLICTS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER.
- WHEN CONSTRUCTION OPERATIONS ON TWO-LANE ROADS OPEN TO TRAFFIC RESULT IN THE REMOVAL OR COVERING OF ANY PAVEMENT STRIPING INDICATING PASSING RESTRICTIONS, "NO PASSING ZONES NOT STRIPED NEXT _____ MILES" SIGNS SHALL BE USED. THE CONTRACTOR SHALL PLACE THE SIGNS AT THE BEGINNING OF THE UNSTRIPED AREA, JUST BEYOND EACH MAJOR INTERSECTION WITHIN THE UNSTRIPED AREA, AND AT SUCH OTHER LOCATIONS AS THE ENGINEER MAY DIRECT TO ENSURE A MINIMUM SPACING OF FIVE MILES. THE SIGNS SHALL BE PLACED JUST PRIOR TO REMOVAL OR COVERING OF THE STRIPE AND SHALL REMAIN IN PLACE UNTIL FULL NO PASSING ZONE STRIPING HAS BEEN RESTORED. THIS WORK WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO EXTRA COMPENSATION WILL BE ALLOWED.
- ALL MILLED SURFACES SHALL BE AT A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.

MODEL Path: \\... Infrastructure Engineering, Inc. File Name: P:\2023\2023-04-09-400 IDOT Virobus Phase 2 (ITB) 106 Item: 16\Work Order: 13102\CAD\DWG\Sheets\162\164-SHT-GENNOTES-01.dgn

INFRASTRUCTURE ENGINEERING <small>INCORPORATED</small> 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9594 www.infrastructure-eng.com	USER NAME = ALane PLOT SCALE = 1200.0000 ' / ft. PLOT DATE = 1/5/2022	DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 10/15/2021	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES US-14 (HARTLAND RD TO IL-47)	F.A.P. RTE. = 305 SECTION = 2021-022-R5 COUNTY = MCHENRY CONTRACT NO. 62N45	TOTAL SHEETS = 35 SHEET NO. = 2
	SCALE: 1"=50' STA. TO STA.	ILLINOIS FED. AID PROJECT					

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	
				80% FEDERAL 20% STATE	100% STATE
				0005	
20200100	EARTH EXCAVATION	CU YD	103	103	
21101630	TOPSOIL FURNISH AND PLACE, 8"	SQ YD	17	17	
25000210	SEEDING, CLASS 2A	ACRE	0.1	0.1	
25000312	SEEDING, CLASS 4A	ACRE	0.1	0.1	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	73,179	73,179	
40600370	LONGITUDINAL JOINT SEALANT	FOOT	48,362	48,362	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	244	244	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	896	896	
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	1,764	1,764	
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	13,661	13,661	
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	162,620	162,620	
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	10,500	10,500	
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	500	500	
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	5,500	5,500	
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	3,000	3,000	
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	1,500	1,500	
52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	962		962
60100080	FRENCH DRAINS	CU YD	18		18
60100085	GEOTECHNICAL FABRIC FOR FRENCH DRAINS	SQ YD	82		82
60108208	PIPE UNDERDRAINS, TYPE 2, 8"	FOOT	67	67	
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2	
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	62,864	62,864	
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	103	103	

* = SPECIALTY ITEM

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	
				80% FEDERAL 20% STATE	100% STATE
				0005	
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2	
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1	
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1	
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	2	2	
67100100	MOBILIZATION	L SUM	1	1	
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1	
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	56	56	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	27,024	27,024	
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	1,417	1,417	
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	124,084	124,084	
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	3,335	3,335	
70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	3,867	3,867	
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	3,199	3,199	
70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	633	633	
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	13,512	13,512	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1,417	1,417	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	124,084	124,084	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,335	3,335	
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	3,867	3,867	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	3,199	3,199	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	633	633	

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INFRASTRUCTURE ENGINEERING <small>INCORPORATE</small> 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9560 F 312.425.9564 www.infrastructure-eng.com	USER NAME = ALane	DESIGNED - MLD	REVISED -
	PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
	PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
		DATE - 10/15/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
US-14 (HARTLAND RD TO IL-47)**

SCALE: 1"=50'

STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				0005	
				80% FEDERAL 20% STATE	100% STATE
	URBAN				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1,404	1,404	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1,404	1,404	
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	6,158	6,158	
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2,913	2,913	
X0326898	CENTER LINE - RUMBLE STRIP - 16"	FOOT	20,868	20,868	
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	1,038		1,038
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2	2	
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12	
X7800815	HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH	FOOT	25,990	25,990	
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	6		6
Z0019500	DRYWELL	EACH	2		2
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	312	312	
Ø Z0076600	TRAINEES	HOURS	500	500	
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500	

* = SPECIALTY ITEM

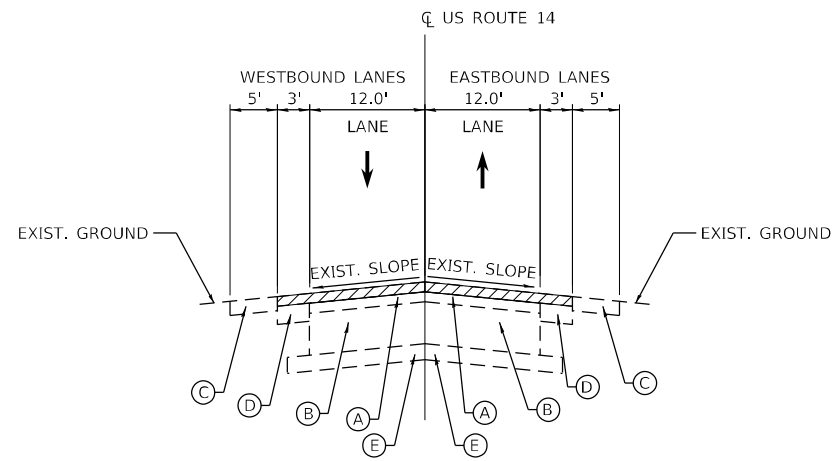
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
US-14 (HARTLAND RD TO IL-47)

SCALE: 1" = 50' STA. TO STA.

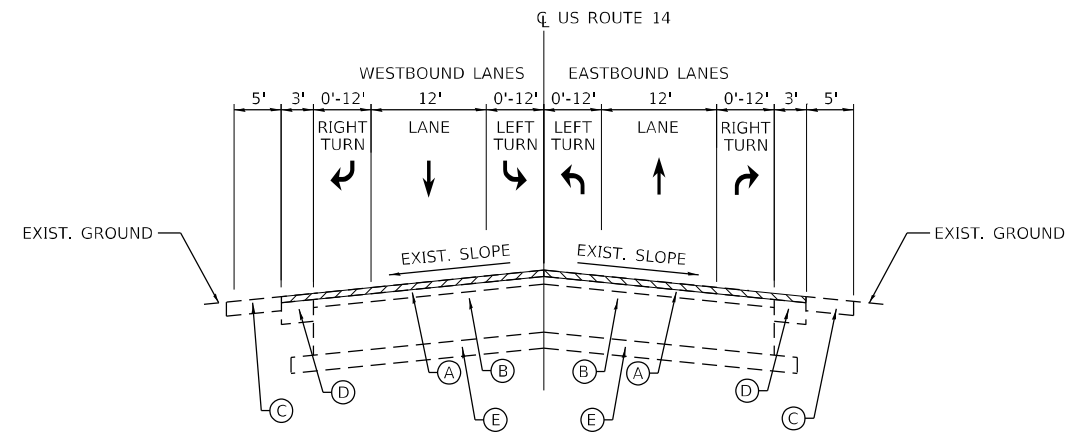
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	4
ILLINOIS				FED. AID PROJECT
CONTRACT NO. 62N45				

MODEL: D:\soft\... INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker | Suite 2650 | Chicago, IL 60606 P 312.425.9560 | F 312.425.9564 | www.infrastructure-eng.com



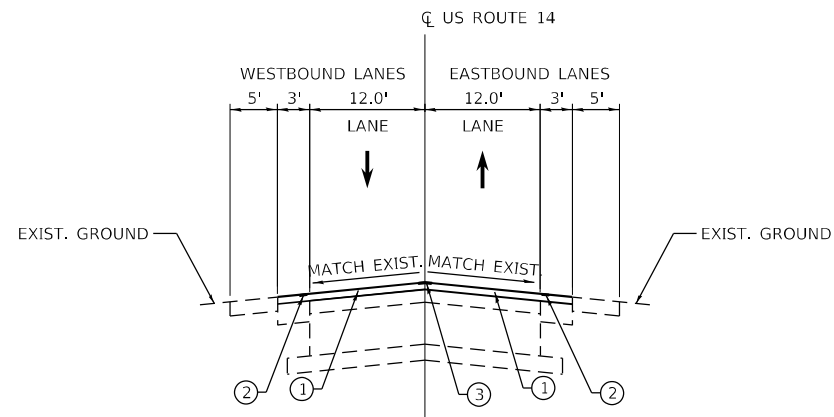
EXISTING TYPICAL CROSS SECTION

STA. 19+14 TO STA. 58+00
 STA. 100+68 TO STA. 166+67
 STA. 188+87 TO STA. 264+11
 STA. 285+95 TO STA. 315+79



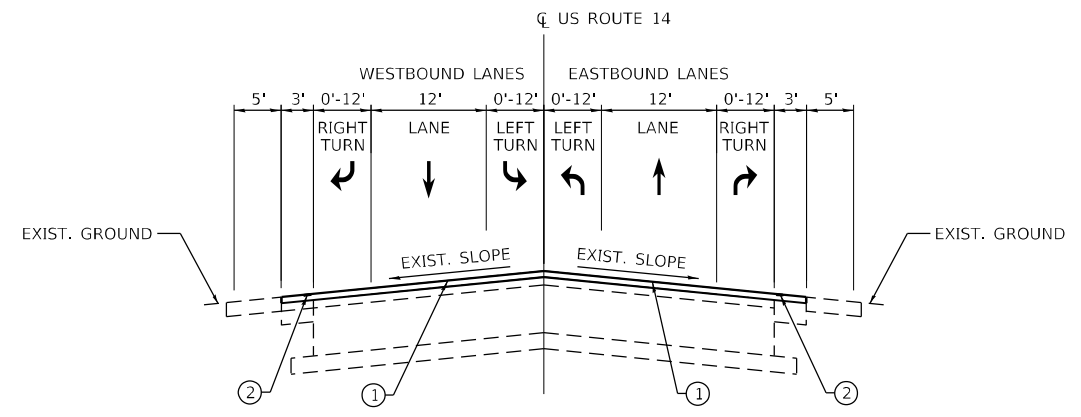
EXISTING TYPICAL CROSS SECTION

STA. 166+67 TO STA. 188+87
 STA. 264+11 TO STA. 285+95



PROPOSED TYPICAL CROSS SECTION

STA. 19+14 TO STA. 58+00
 STA. 100+68 TO STA. 166+67
 STA. 188+87 TO STA. 264+11
 STA. 285+95 TO STA. 315+79



PROPOSED TYPICAL CROSS SECTION

STA. 166+67 TO STA. 188+87
 STA. 264+11 TO STA. 285+95

LEGEND

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT, 3"±
- (B) EXISTING P.C.C. BASE COURSE, 10"±
- (C) EXISTING AGGREGATE SHOULDER, 6"±
- (D) EXISTING PAVED SHOULDER, 8"±
- (E) EXISTING SUB-BASE GRANULAR MATERIAL, TYP B, 4"±
- (F) EXISTING CONCRETE PAVEMENT, 9-3/4"±
- (G) EXISTING MEDIAN
- ① HOT-MIX ASPHALT SURFACE COURSE MIX "D", IL-9.5, N70, 1-1/2"
- ② SHOULDER RUMBLE STRIPS, 8"
- ③ CENTERLINE RUMBLE STRIPS, 16"
- 1-1/2" HMA SURFACE REMOVAL

NOTES:

1. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE.
2. THE CONTRACTOR SHALL PATCH BEFORE MILLING.

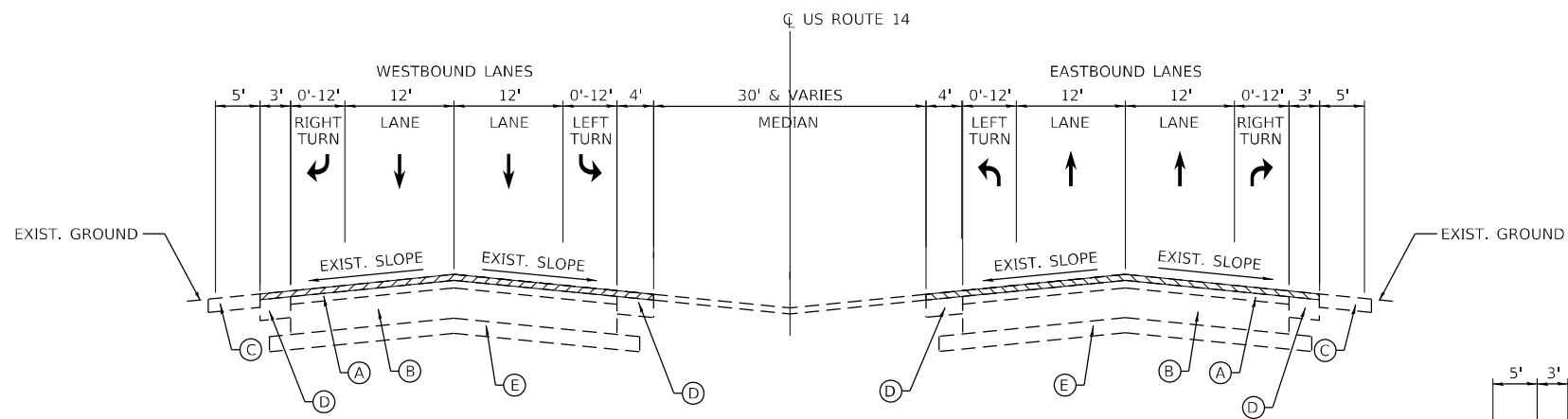
HOT-MIX ASPHALT MIXTURE REQUIRMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE MIX "D", IL-9.5, N70, 1-1/2"	4% @ 70 GYR	PFP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

MIXTURE REQUIREMENT NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

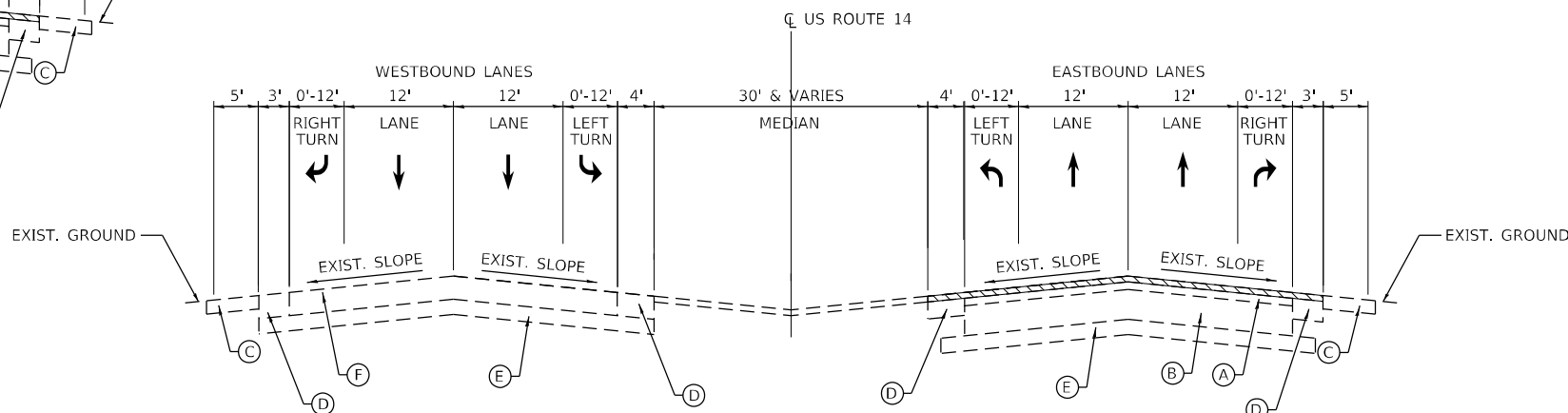
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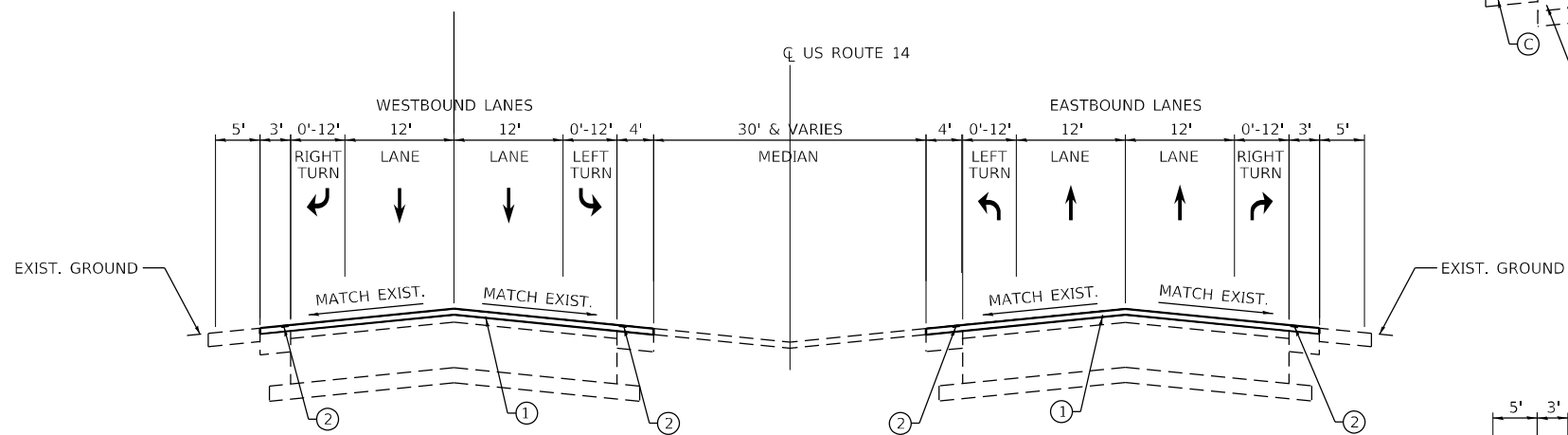
EXISTING TYPICAL CROSS SECTION

STA. 315+79 TO STA. 331+05



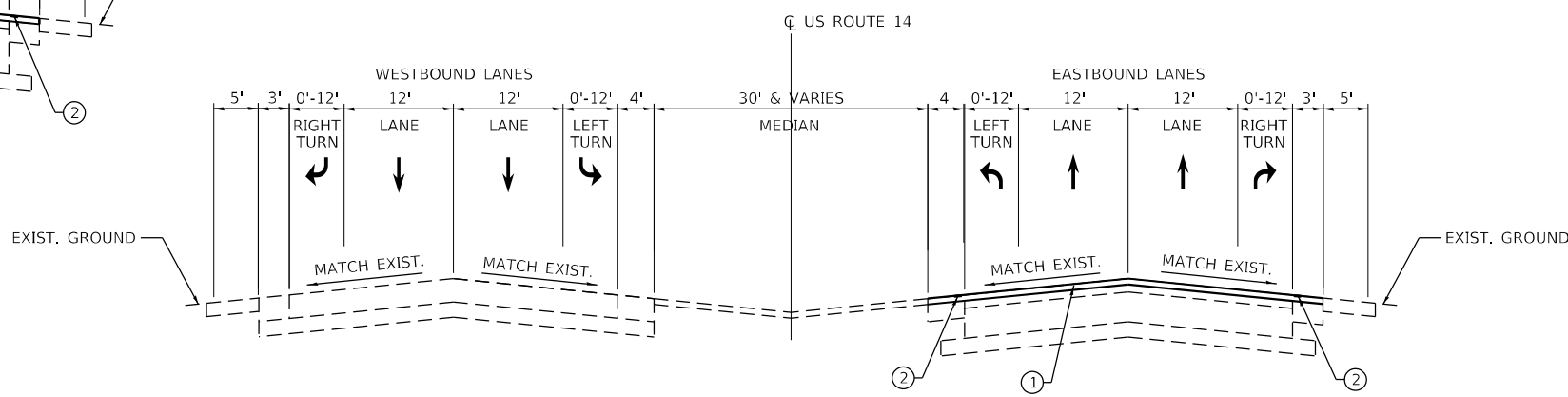
EXISTING TYPICAL CROSS SECTION

STA. 58+00 TO STA. 100+68



PROPOSED TYPICAL CROSS SECTION

STA. 315+79 TO STA. 331+05



PROPOSED TYPICAL CROSS SECTION

STA. 58+00 TO STA. 100+68

LEGEND:

1-1/2" HMA SURFACE REMOVAL

NOTES:

1. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE.
2. THE CONTRACTOR SHALL PATCH BEFORE MILLING.

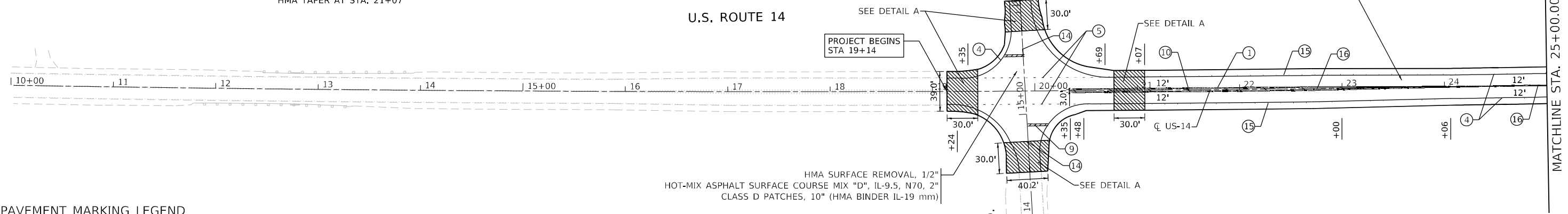
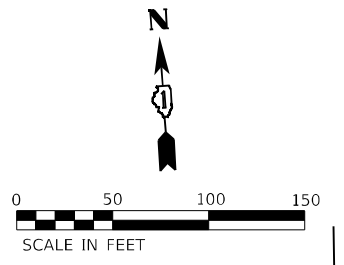
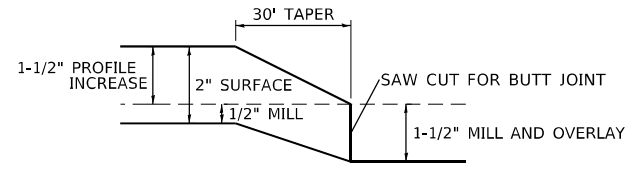
LEGEND

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT, 3"±
- (B) EXISTING P.C.C. BASE COURSE, 10"±
- (C) EXISTING AGGREGATE SHOULDER, 6"±
- (D) EXISTING PAVED SHOULDER, 8"±
- (E) EXISTING SUB-BASE GRANULAR MATERIAL, TYP B, 4"±
- (F) EXISTING CONCRETE PAVEMENT, 9-3/4"±
- (G) EXISTING MEDIAN
- (1) HOT-MIX ASPHALT SURFACE COURSE MIX "D", IL-9.5, N70, 1-1/2"
- (2) SHOULDER RUMBLE STRIPS, 8"
- (3) CENTERLINE RUMBLE STRIPS, 16"

USER NAME = ALane	DESIGNED - MLD	REVISED -
DRAWN - MLD	REVISED -	
PLOT SCALE = 1200.0000' / ft.	CHECKED - ACL	REVISED -
PLOT DATE = 12/13/2021	DATE - 10/15/2021	REVISED -

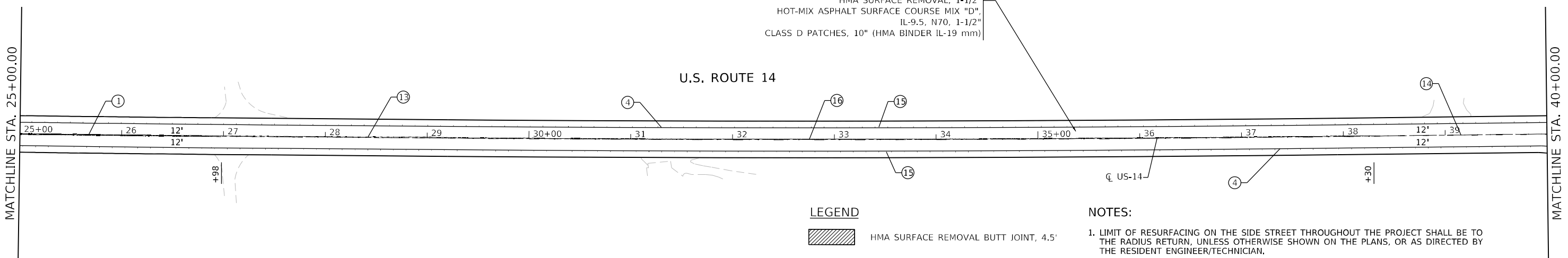
TYPICAL SECTIONS US-14 (HARTLAND RD TO IL-47)	
SCALE: 1"=50'	STA. TO STA.

F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHEMRY	TOTAL SHEETS 35	SHEET NO. 6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
- ④ LINE 4", SOLID WHITE - EDGE LINE (TYP)
- ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)
- ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP)
- ⑦ LETTERS & SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP)
- ⑧ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)
- ⑨ LINE 24", SOLID, WHITE - STOP LINES (TYP)
- ⑩ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑪ LINE 12", SOLID, WHITE - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
- ⑫ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
- ⑬ LINE 4", SOLID/SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - PASSING LANE (TYP)
- ⑭ LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP)
- ⑮ SHOULDER RUMBLE STRIPS, 8"
- ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH



LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

1. LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
2. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
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4. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

MODEL Path: I:\Work\Projects\2020\4094-00\DOT\Unbound Phase 2\ITRBL 106 Item: 161Work Order: 131021CADD_Sheets\162N45-SHT-PLAN-01.dwg

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PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

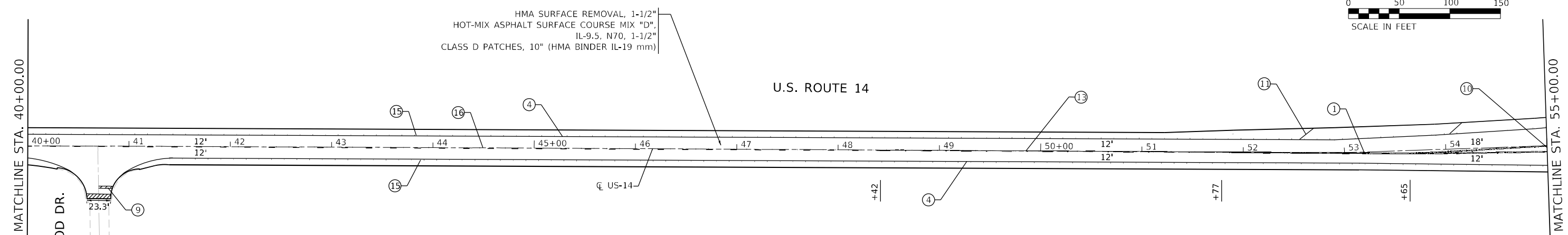
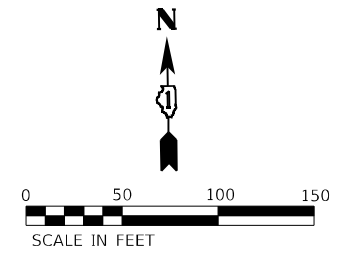
**ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)**

SCALE: 1"=50'

STA. TO STA.

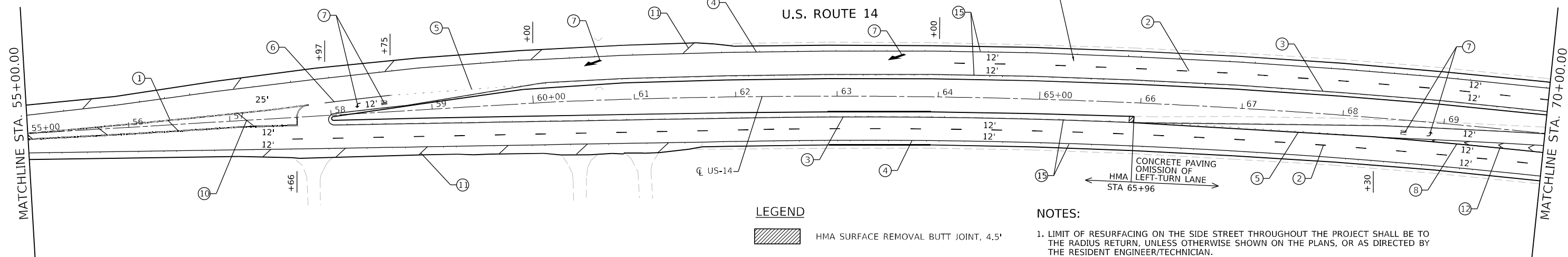
F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 7
			CONTRACT NO. 62N45	

ILLINOIS FED. AID PROJECT



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
- ④ LINE 4", SOLID WHITE - EDGE LINE (TYP)
- ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)
- ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP)
- ⑦ LETTERS & SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP)
- ⑧ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)
- ⑨ LINE 24", SOLID, WHITE - STOP LINES (TYP)
- ⑩ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑪ LINE 12", SOLID, WHITE - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
- ⑫ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
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- ⑭ LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP)
- ⑮ SHOULDER RUMBLE STRIPS, 8"
- ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH



LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

1. LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
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MODEL Path: \\... \Infrastructure\Phase 2\ITB\196 Item: 16\Work Order: 131\DCM\CADD_Sheets\62N45-SHT-PLAN-02.dwg

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PLOT SCALE = 1/200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/29/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

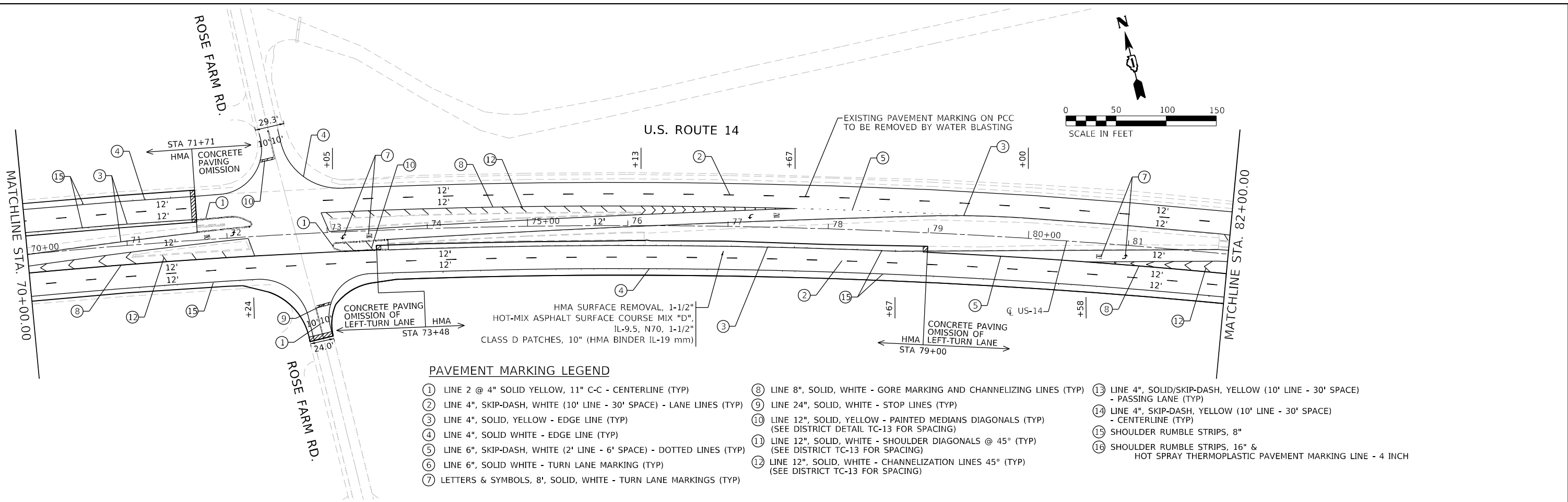
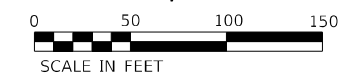
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)**

SCALE: 1"=50'

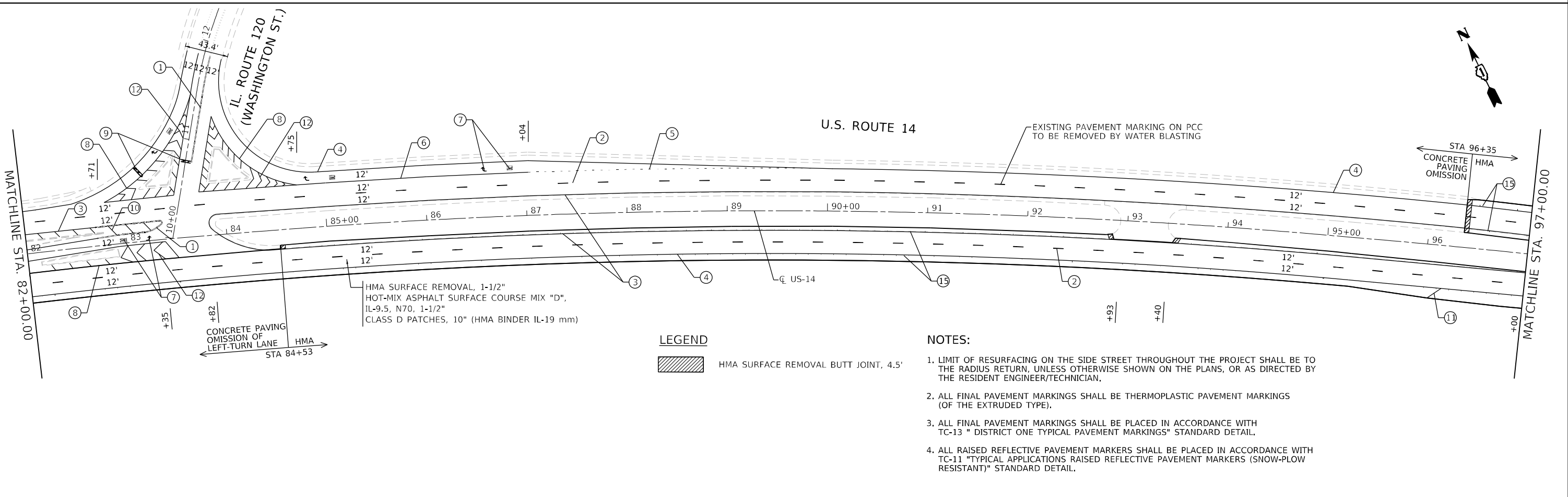
STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	8
CONTRACT NO. 62N45				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
- ④ LINE 4", SOLID WHITE - EDGE LINE (TYP)
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- ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP)
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- ⑧ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)
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- ⑮ SHOULDER RUMBLE STRIPS, 8"
- ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH



LEGEND

HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

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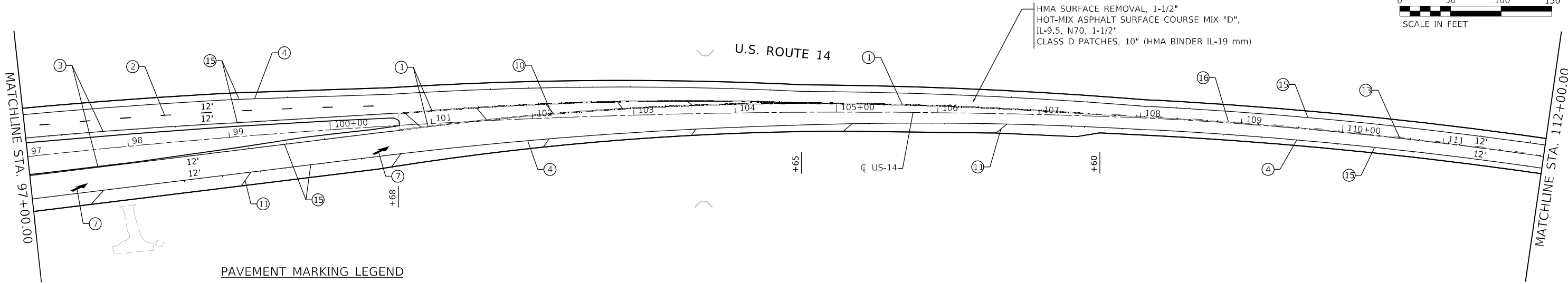
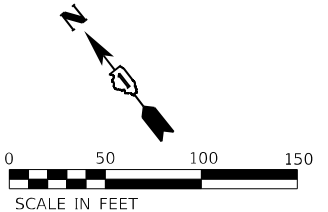
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PLOT DATE = 12/29/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)**

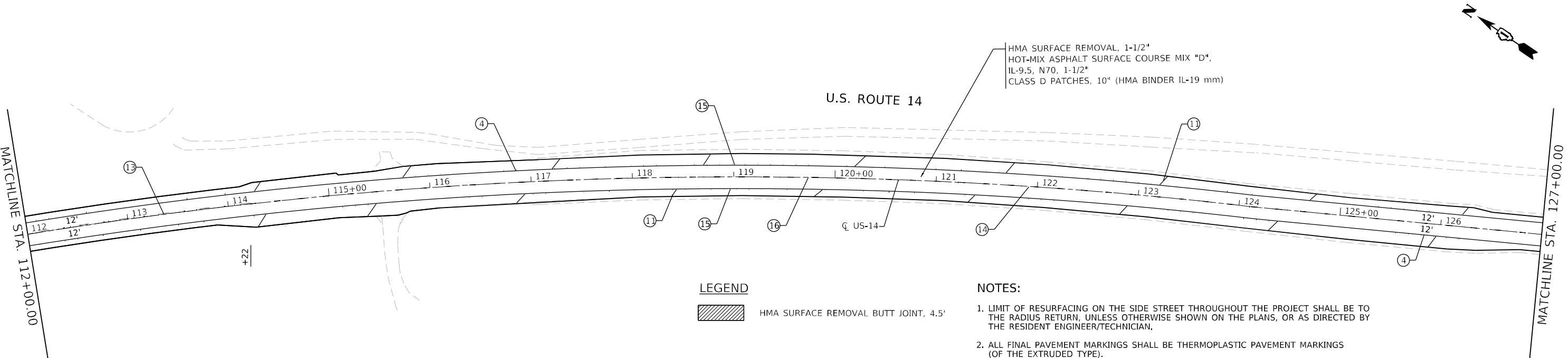
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F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
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- ⑮ SHOULDER RUMBLE STRIPS, 8"
- ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH



LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

1. LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
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PLOT SCALE = 1/200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
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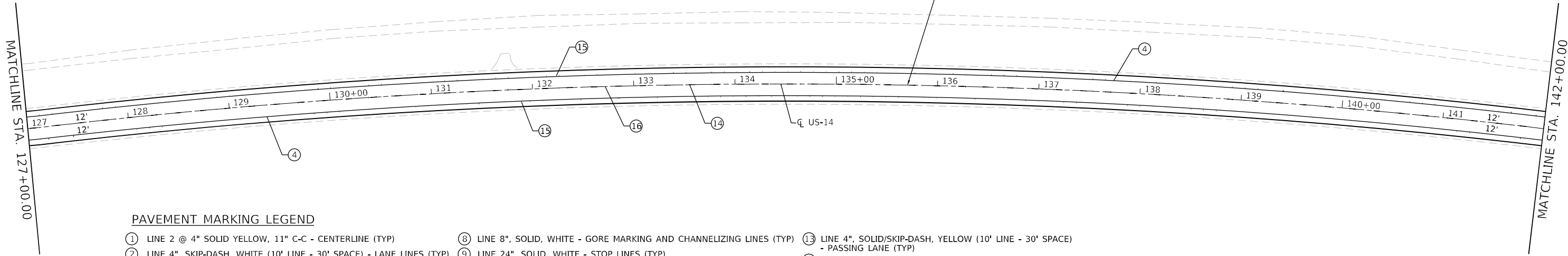
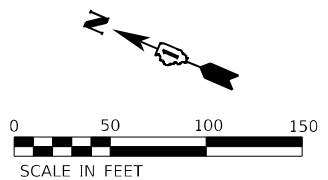
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)**

SCALE: 1"=50'

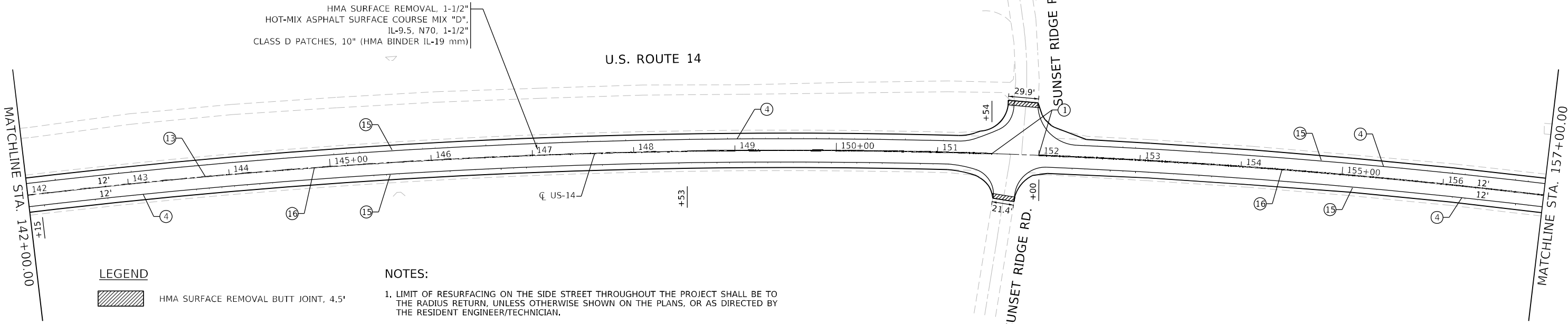
STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	10
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	



PAVEMENT MARKING LEGEND

- | | | |
|---|---|--|
| ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP) | ⑧ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP) | ⑬ LINE 4", SOLID/SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - PASSING LANE (TYP) |
| ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP) | ⑨ LINE 24", SOLID, WHITE - STOP LINES (TYP) | ⑭ LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP) |
| ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP) | ⑩ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING) | ⑮ SHOULDER RUMBLE STRIPS, 8" |
| ④ LINE 4", SOLID WHITE - EDGE LINE (TYP) | ⑪ LINE 12", SOLID, WHITE - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING) | ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH |
| ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP) | ⑫ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING) | |
| ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP) | | |
| ⑦ LETTERS & SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP) | | |



LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

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MODEL Path: \\... \Infrastructure\Phase 2\ITP\106 Item: 16\Work Order: 1310\GIS\CADD_Sheets\62N45-SHT-PLAN-05.dwg
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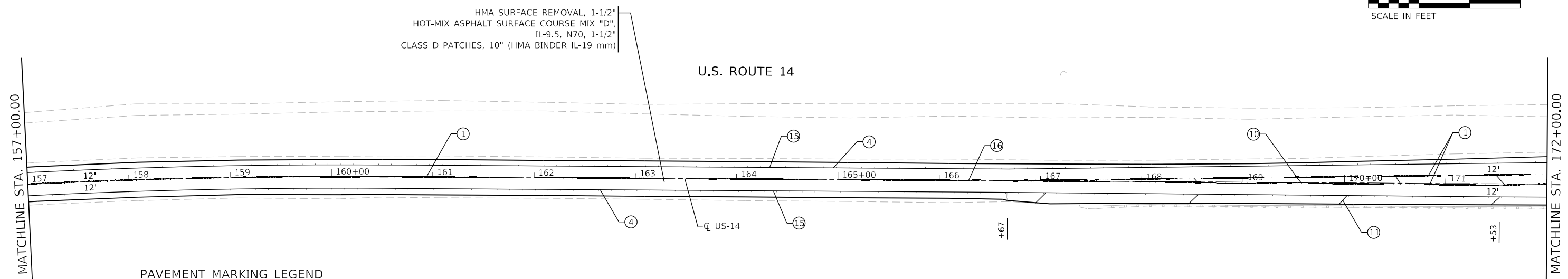
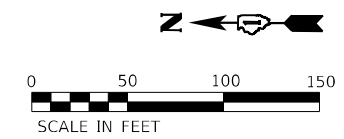
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PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

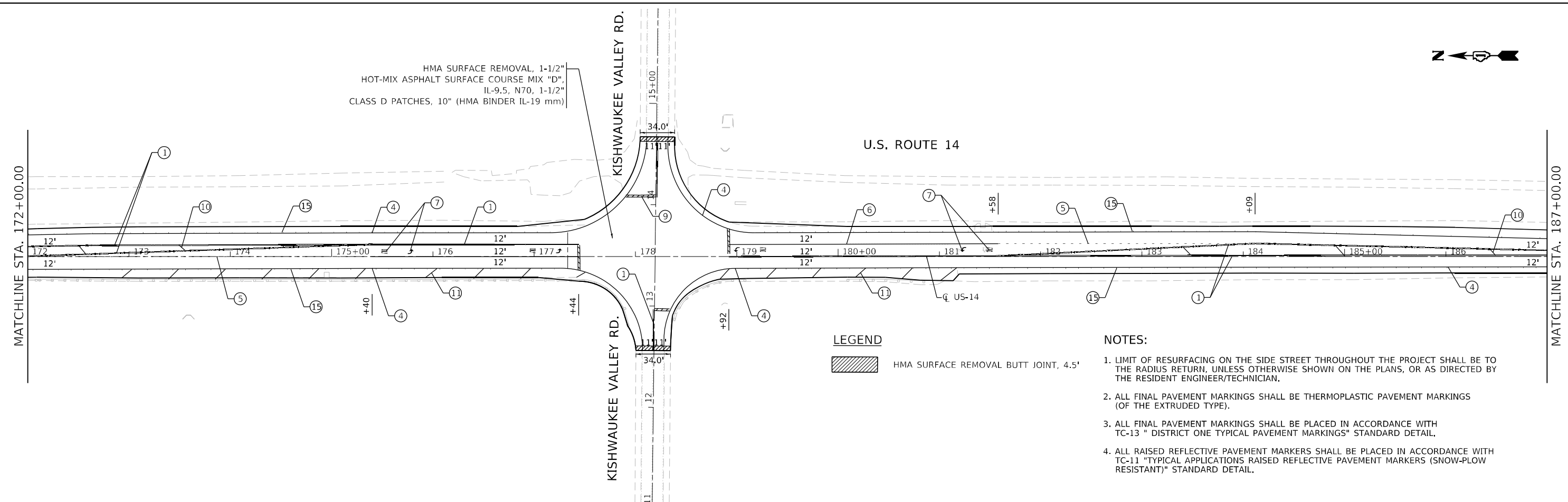
ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)
SCALE: 1"=50' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	11
CONTRACT NO. 62N45			ILLINOIS FED. AID PROJECT	



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
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- ⑮ SHOULDER RUMBLE STRIPS, 8"
- ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH



LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

1. LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
2. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
3. ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
4. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

MODEL Path: \\... \Infrastructure\Phase 2 (RTE 106 Item 16)\Work Order 131\DCM\CADD_Sheets\62N45-SHT-PLAN-06.dgn

INFRASTRUCTURE ENGINEERING INCORPORATED
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

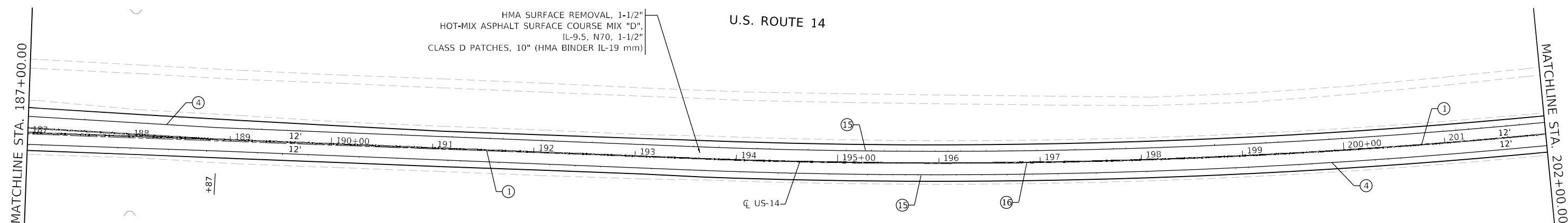
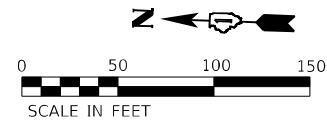
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PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)**

SCALE: 1"=50' STA. TO STA.

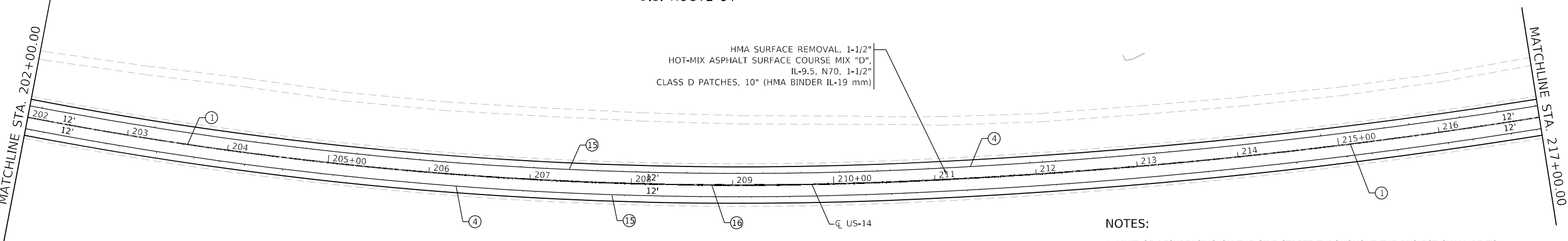
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	12
CONTRACT NO. 62N45			ILLINOIS FED. AID PROJECT	



PAVEMENT MARKING LEGEND

- | | | |
|---|---|--|
| ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP) | ⑧ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP) | ⑬ LINE 4", SOLID/SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - PASSING LANE (TYP) |
| ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP) | ⑨ LINE 24", SOLID, WHITE - STOP LINES (TYP) | ⑭ LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP) |
| ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP) | ⑩ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING) | ⑮ SHOULDER RUMBLE STRIPS, 8" |
| ④ LINE 4", SOLID WHITE - EDGE LINE (TYP) | ⑪ LINE 12", SOLID, WHITE - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING) | ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH |
| ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP) | ⑫ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING) | |
| ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP) | | |
| ⑦ LETTERS & SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP) | | |

U.S. ROUTE 14



NOTES:

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LEGEND

HMA SURFACE REMOVAL BUTT JOINT, 4.5'

MODEL: P:\d\h\h\... FILE NAME: P:\d\h\h\... ORDER: 131021\CA00... SHEETS: D162\N45-SHT-PLAN-07.dwg

INFRASTRUCTURE ENGINEERING INCORPORATED
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.425.9598 | F 312.425.9599 | www.infrastructure-eng.com

USER NAME = ALane	DESIGNED - MLD	REVISED -
PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

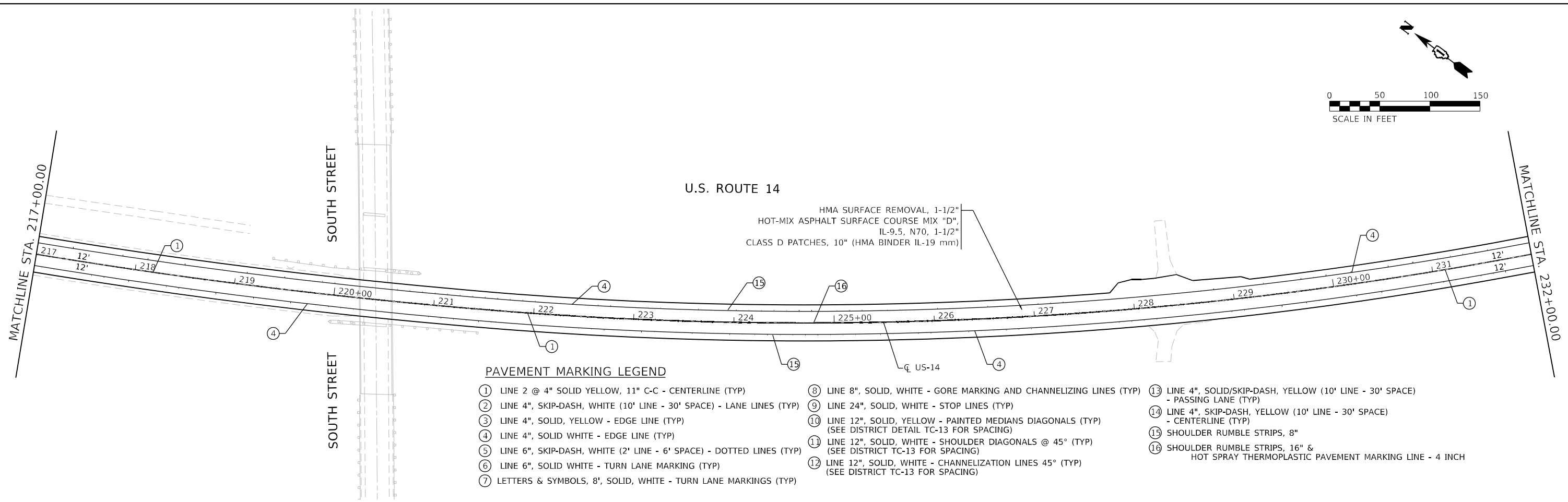
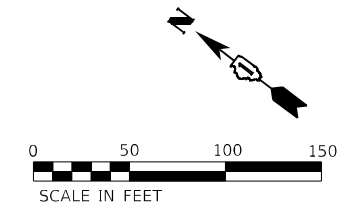
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)**

SCALE: 1"=50'

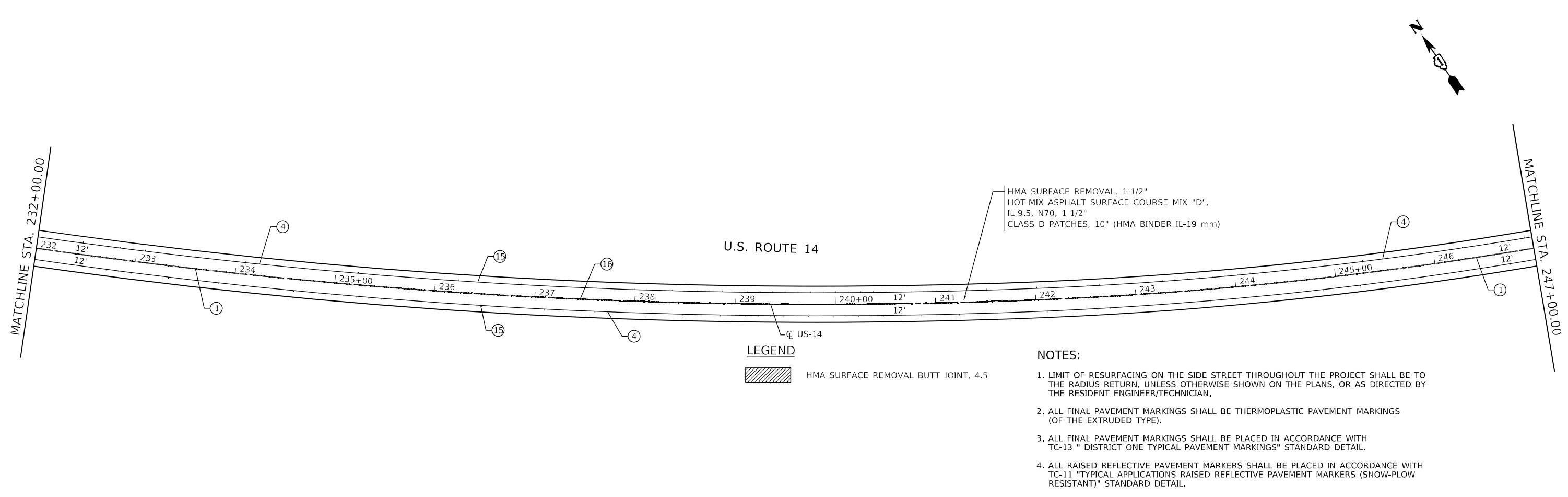
STA. TO STA.

F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 13
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	



PAVEMENT MARKING LEGEND

- | | | |
|---|---|--|
| ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP) | ⑧ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP) | ⑬ LINE 4", SOLID/SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - PASSING LANE (TYP) |
| ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP) | ⑨ LINE 24", SOLID, WHITE - STOP LINES (TYP) | ⑭ LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP) |
| ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP) | ⑩ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING) | ⑮ SHOULDER RUMBLE STRIPS, 8" |
| ④ LINE 4", SOLID WHITE - EDGE LINE (TYP) | ⑪ LINE 12", SOLID, WHITE - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING) | ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH |
| ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP) | ⑫ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING) | |
| ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP) | ⑬ LINE 2" @ 4" SOLID WHITE - TURN LANE MARKINGS (TYP) | |



LEGEND

HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

1. LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
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MODEL: P:\d\p\11 FILE NAME: P:\d\p\11\DOT_Vis\Phase 2 \ITB1_106 Item: 16\Work Order: 13102\CAD\Drawings\Sheet\PLAN\08.dgn

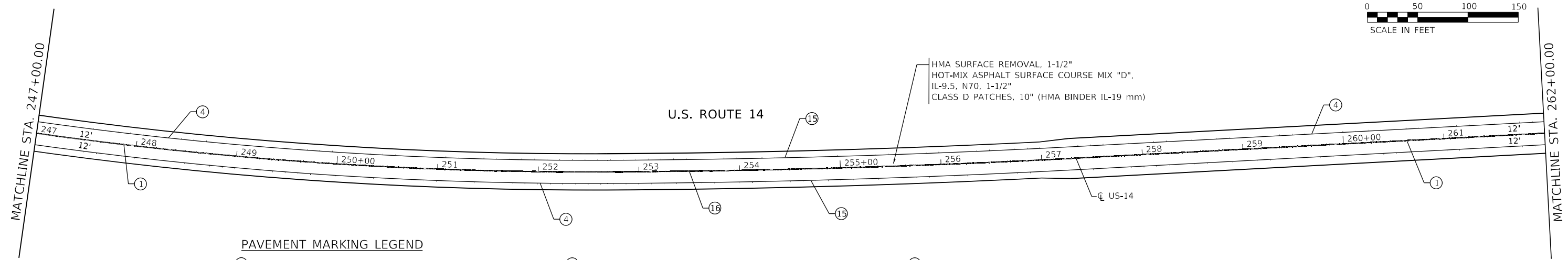
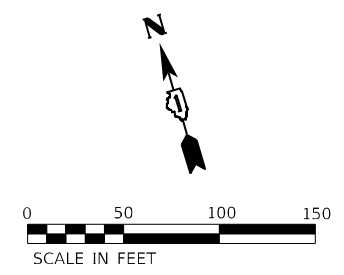
 INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9598 www.infrastructure-eng.com	USER NAME = Alane	DESIGNED - MLD	REVISED -
	PLOT SCALE = 1/200.0000 ' / ft.	DRAWN - MLD	REVISED -
	PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
		DATE - 10/15/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY AND PAVEMENT MARKING PLANS	
US-14 (HARTLAND RD TO IL-47)	
SCALE: 1"=50'	STA. TO STA.

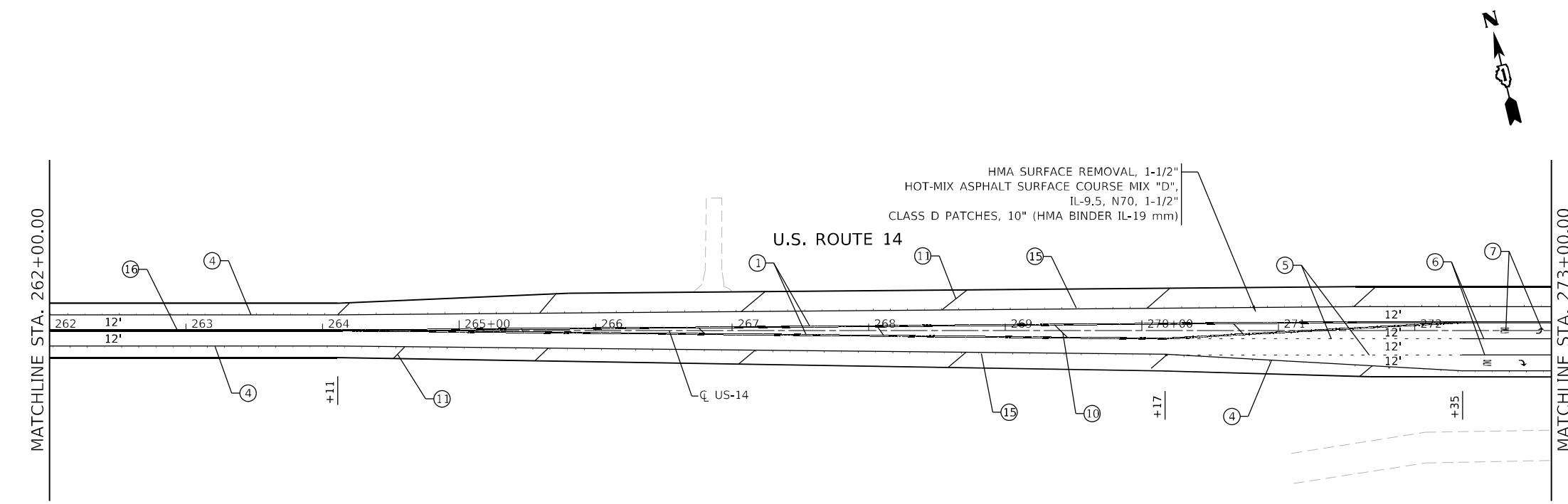
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	14
CONTRACT NO. 62N45				

ILLINOIS FED. AID PROJECT



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
- ④ LINE 4", SOLID WHITE - EDGE LINE (TYP)
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- ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP)
- ⑦ LETTERS & SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP)
- ⑧ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)
- ⑨ LINE 24", SOLID, WHITE - STOP LINES (TYP)
- ⑩ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑪ LINE 12", SOLID, WHITE - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
- ⑫ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
- ⑬ LINE 4", SOLID/SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - PASSING LANE (TYP)
- ⑭ LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP)
- ⑮ SHOULDER RUMBLE STRIPS, 8"
- ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH



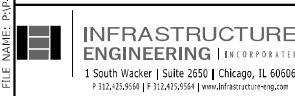
LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

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MODEL Path: \\... \Infrastructure\Phase 2\ITPB\196 Item: 16\Work Order: 131\DCM\CADD_Sheets\162N45-SHT-PLAN-09.dgn



USER NAME = ALane	DESIGNED - MLD	REVISED -	
PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -	
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -	
	DATE - 10/15/2021	REVISED -	

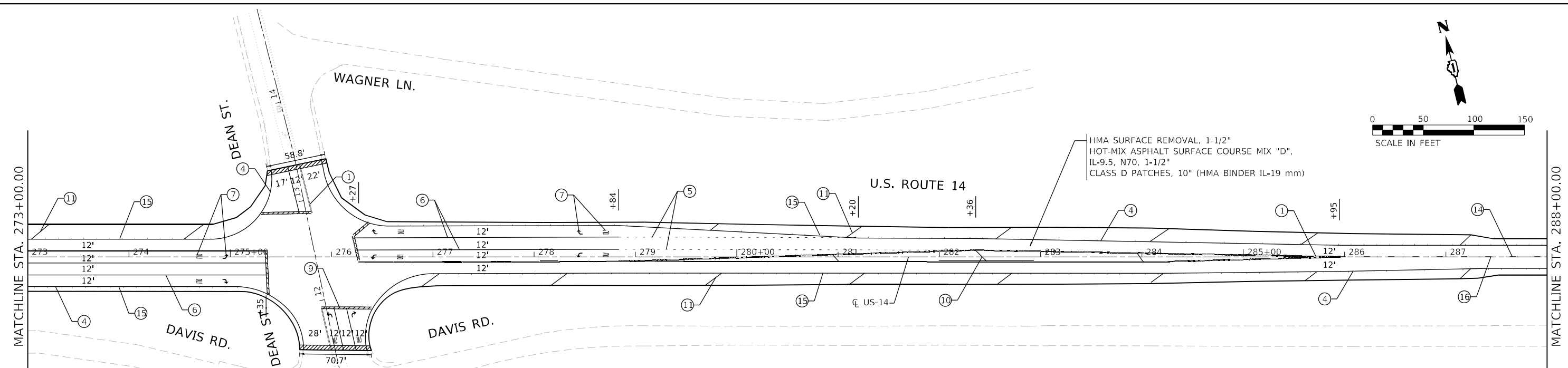
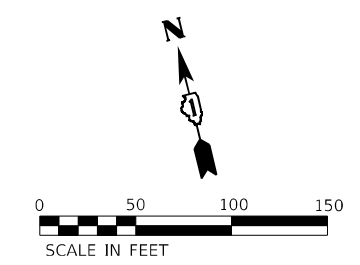
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)**

SCALE: 1"=50'

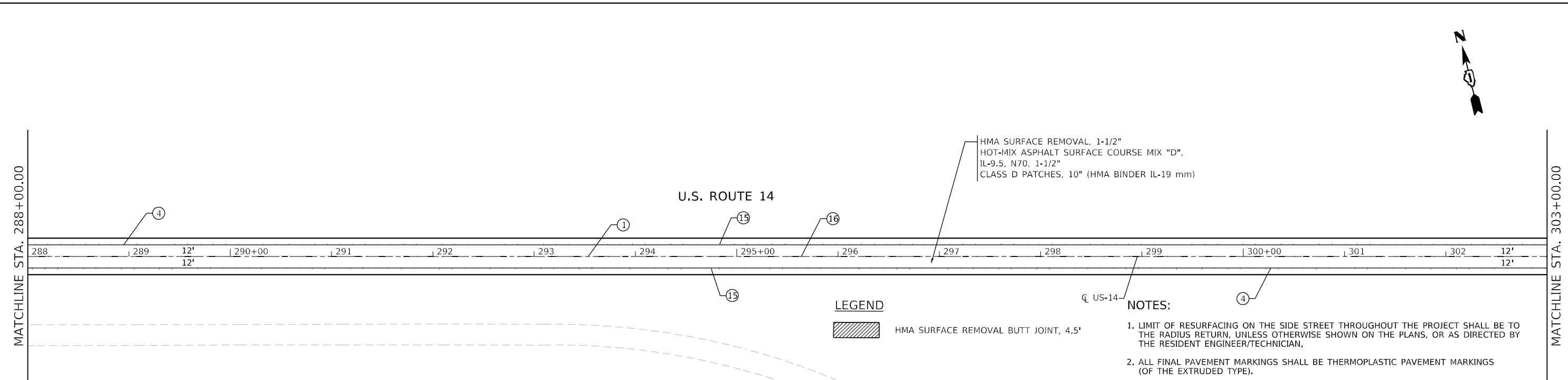
STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	15
CONTRACT NO. 62N45				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
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- ⑦ LETTERS & SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP)
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- ⑩ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑪ LINE 12", SOLID, WHITE - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
- ⑫ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
- ⑬ LINE 4", SOLID/SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - PASSING LANE (TYP)
- ⑭ LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP)
- ⑮ SHOULDER RUMBLE STRIPS, 8"
- ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH



LEGEND

HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

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MODEL Path: \\... \Infrastructure\Phase 2\ITPL 106 Item 16\Work Order 1310\GIS\CADD_Sheets\162N45-SHT-PLAN-10.dwg
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INFRASTRUCTURE ENGINEERING INCORPORATED
1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.425.9598 | F 312.425.9599 | www.infrastructure-eng.com

USER NAME = ALane	DESIGNED - MLD	REVISED -
PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

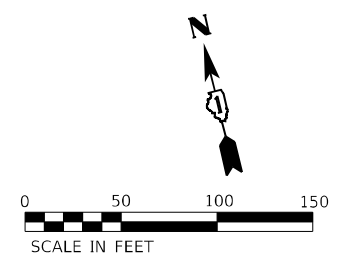
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
US-14 (HARTLAND RD TO IL-47)**

SCALE: 1"=50'

STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	16
CONTRACT NO. 62N45			ILLINOIS FED. AID PROJECT	



MATCHLINE STA. 303+00.00

MATCHLINE STA. 318+00.00

HMA SURFACE REMOVAL, 1-1/2"
HOT-MIX ASPHALT SURFACE COURSE MIX "D",
IL-9.5, N70, 1-1/2"
CLASS D PATCHES, 10" (HMA BINDER IL-19 mm)

U.S. ROUTE 14

PAVEMENT MARKING LEGEND

- | | | |
|---|---|--|
| ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP) | ⑧ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP) | ⑬ LINE 4", SOLID/SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - PASSING LANE (TYP) |
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| ④ LINE 4", SOLID WHITE - EDGE LINE (TYP) | ⑪ LINE 12", SOLID, WHITE - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING) | ⑯ SHOULDER RUMBLE STRIPS, 16" & HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH |
| ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP) | ⑫ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING) | |
| ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP) | | |
| ⑦ LETTERS & SYMBOLS, 8', SOLID, WHITE - TURN LANE MARKINGS (TYP) | | |

MATCHLINE STA. 318+00.00



HMA SURFACE REMOVAL, 1-1/2"
HOT-MIX ASPHALT SURFACE COURSE MIX "D",
IL-9.5, N70, 1-1/2"
CLASS D PATCHES, 10" (HMA BINDER IL-19 mm)

U.S. ROUTE 14

EASTWOOD DR.

DAVIS RD.

EASTWOOD DR.

LEGEND

HMA SURFACE REMOVAL BUTT JOINT, 4.5'

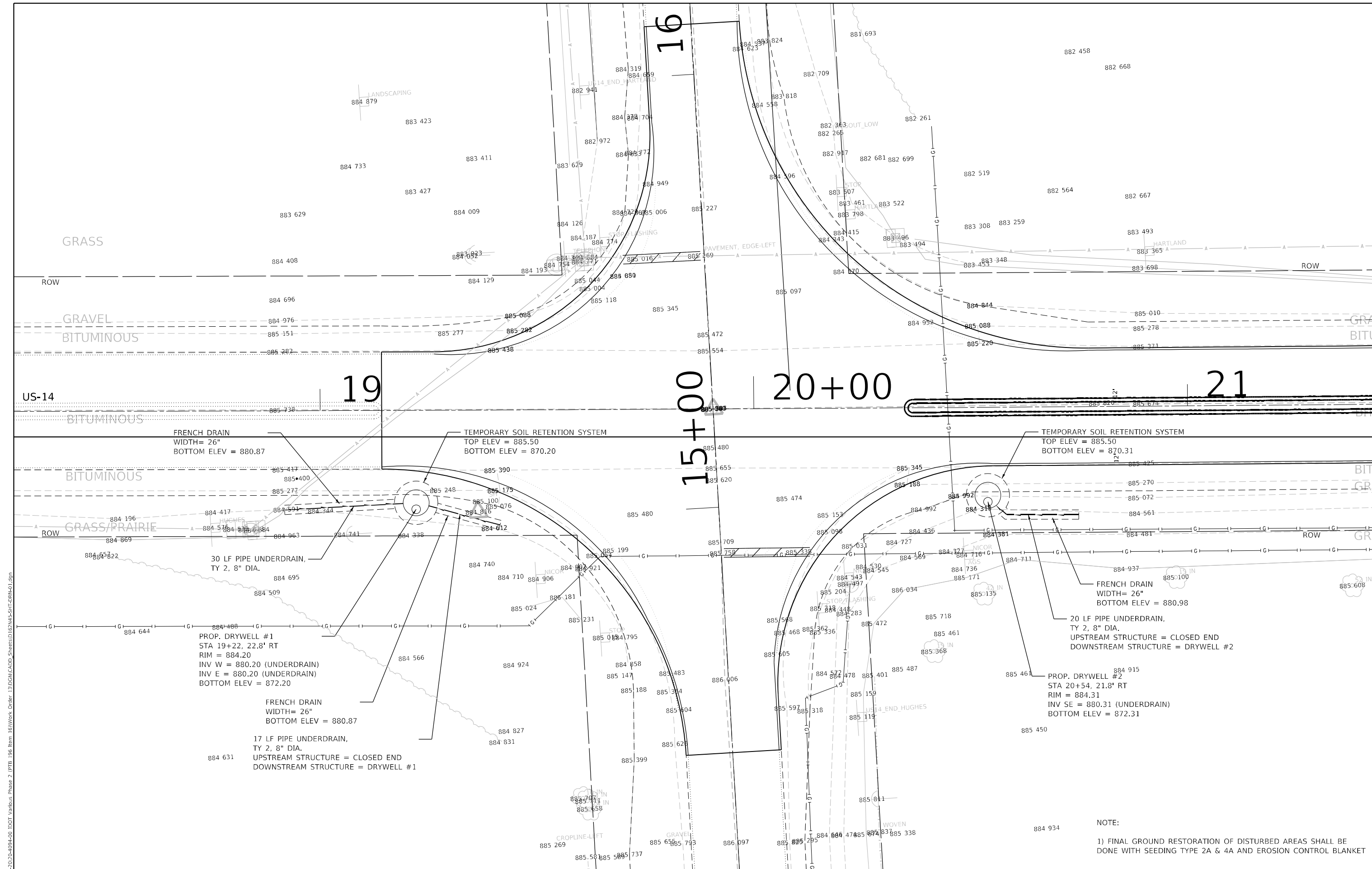
NOTES:

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PROJECT ENDS STA 331+05

MODEL: P:\dms\11... FILE NAME: P:\2020\2020-09-04-00 IDOT Various Phase 2 (RFB) 196 Item: 16\Work Order: 1310261CADD-Sheets\162N45-SHT-PLAN-11.dgn

INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9594 www.infrastructure-eng.com	USER NAME = ALane DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 10/15/2021	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS US-14 (HARTLAND RD TO IL-47)	F.A.P. RTE. = 305	SECTION = 2021-022-R5	COUNTY = MCHENRY	TOTAL SHEETS = 35	SHEET NO. = 17
	PLOT SCALE = 1200.0000' / ft. PLOT DATE = 12/13/2021	DATE - 10/15/2021			SCALE: 1"=50'	STA. TO STA.	CONTRACT NO. 62N45 ILLINOIS FED. AID PROJECT		



FRENCH DRAIN
WIDTH= 26"
BOTTOM ELEV = 880.87

TEMPORARY SOIL RETENTION SYSTEM
TOP ELEV = 885.50
BOTTOM ELEV = 870.20

TEMPORARY SOIL RETENTION SYSTEM
TOP ELEV = 885.50
BOTTOM ELEV = 870.31

30 LF PIPE UNDERDRAIN,
TY 2, 8" DIA.

PROP. DRYWELL #1
STA 19+22, 22.8' RT
RIM = 884.20
INV W = 880.20 (UNDERDRAIN)
INV E = 880.20 (UNDERDRAIN)
BOTTOM ELEV = 872.20

FRENCH DRAIN
WIDTH= 26"
BOTTOM ELEV = 880.87

17 LF PIPE UNDERDRAIN,
TY 2, 8" DIA.
UPSTREAM STRUCTURE = CLOSED END
DOWNSTREAM STRUCTURE = DRYWELL #1

FRENCH DRAIN
WIDTH= 26"
BOTTOM ELEV = 880.98

20 LF PIPE UNDERDRAIN,
TY 2, 8" DIA.
UPSTREAM STRUCTURE = CLOSED END
DOWNSTREAM STRUCTURE = DRYWELL #2

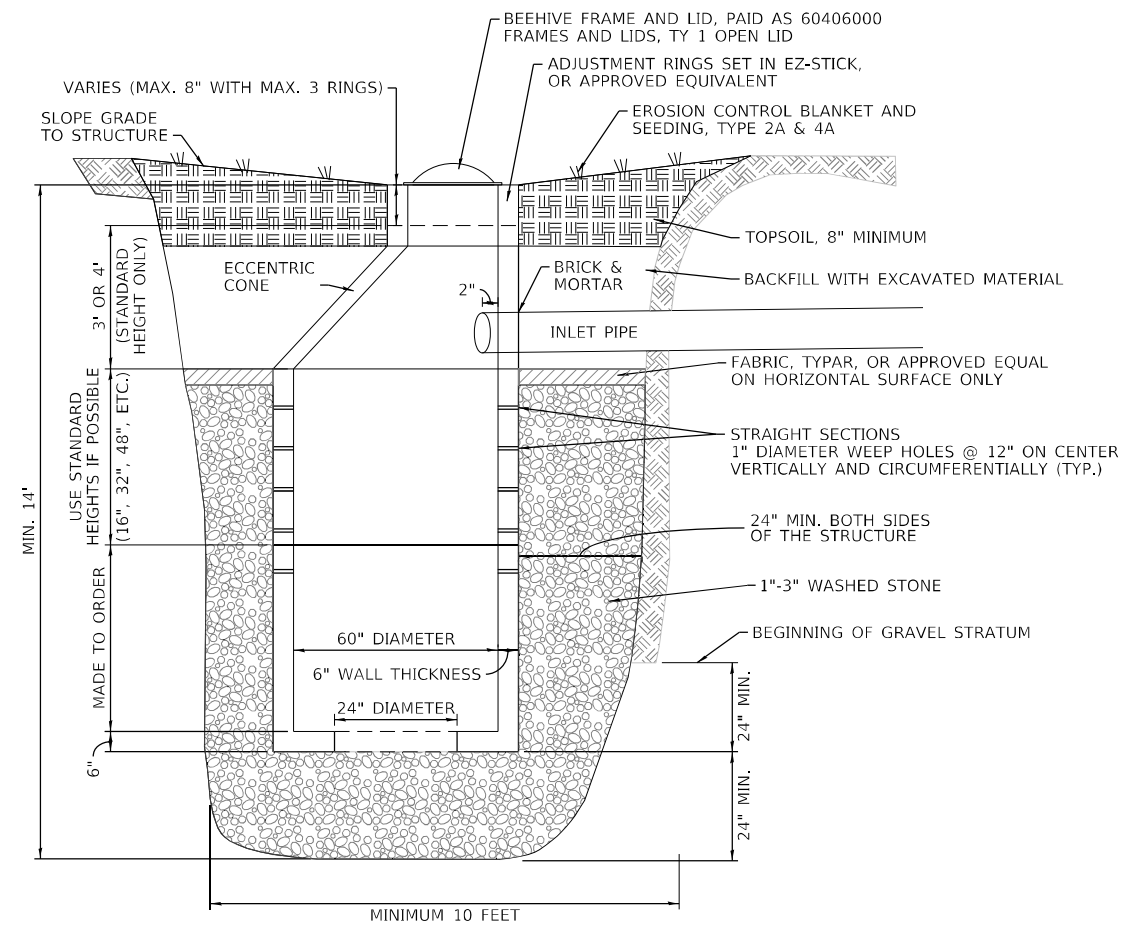
PROP. DRYWELL #2
STA 20+54, 21.8' RT
RIM = 884.31
INV SE = 880.31 (UNDERDRAIN)
BOTTOM ELEV = 872.31

NOTE:
1) FINAL GROUND RESTORATION OF DISTURBED AREAS SHALL BE DONE WITH SEEDING TYPE 2A & 4A AND EROSION CONTROL BLANKET

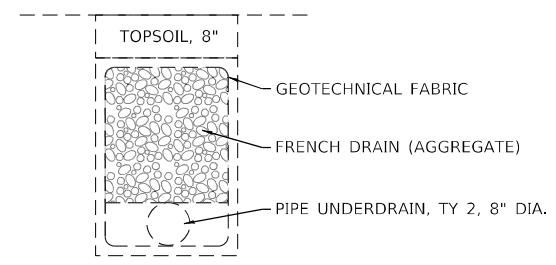
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PLOT SCALE = 20.0000' / in.	DRAWN - ACR	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 11/15/2021	REVISED -

F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 18
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	

MODEL Path: \\... \Public\... \DOT\Various Phase 2\ITB\196 Item 16\Work Order 131\DC\G\ADD_Sheets\162N45-SHT_DET\DETAIL_01.dwg
 FILE NAME: PLOT_2020-4094-00_IDOT_Various Phase 2\ITB\196 Item 16\Work Order 131\DC\G\ADD_Sheets\162N45-SHT_DET\DETAIL_01.dwg



DRYWELL DETAIL



FRENCH DRAIN DETAIL

DRYWELLS

DESCRIPTION. THIS WORK SHALL CONSIST OF CONSTRUCTING DRYWELLS.

GENERAL. DRYWELLS SHALL BE CONSTRUCTED ACCORDING TO APPLICABLE PORTIONS OF SECTION 602 OF THE STANDARD SPECIFICATIONS, THIS DETAIL, TO THE LINES AND GRADES SHOWN ON THE PLANS AND AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.

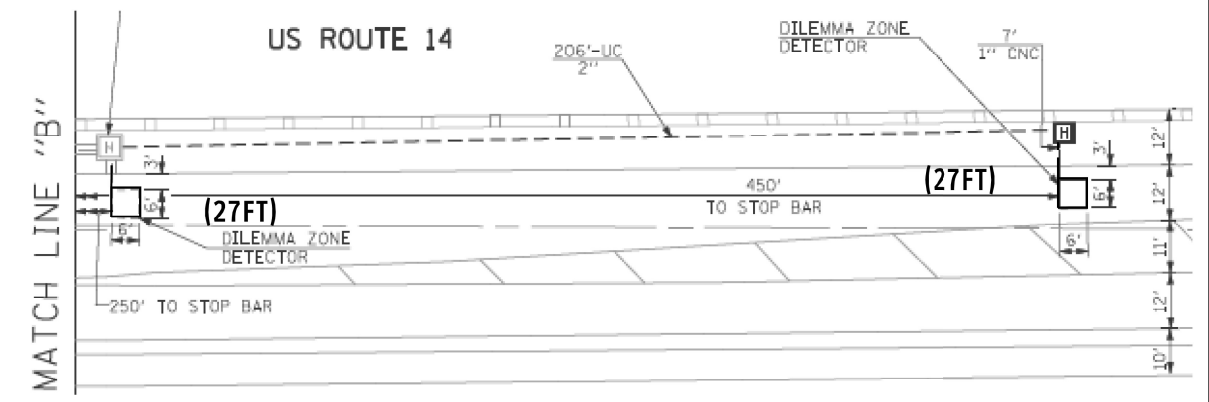
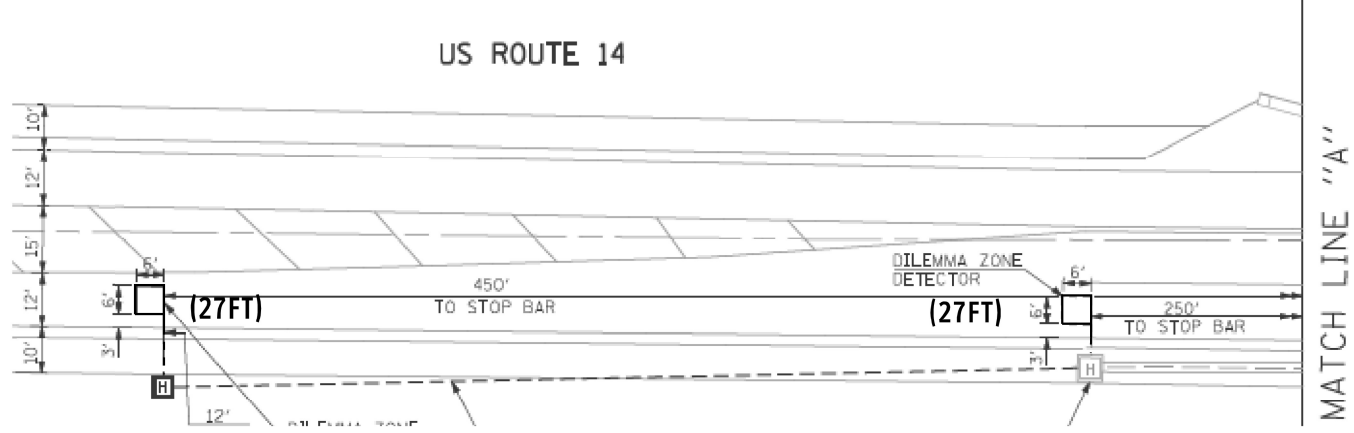
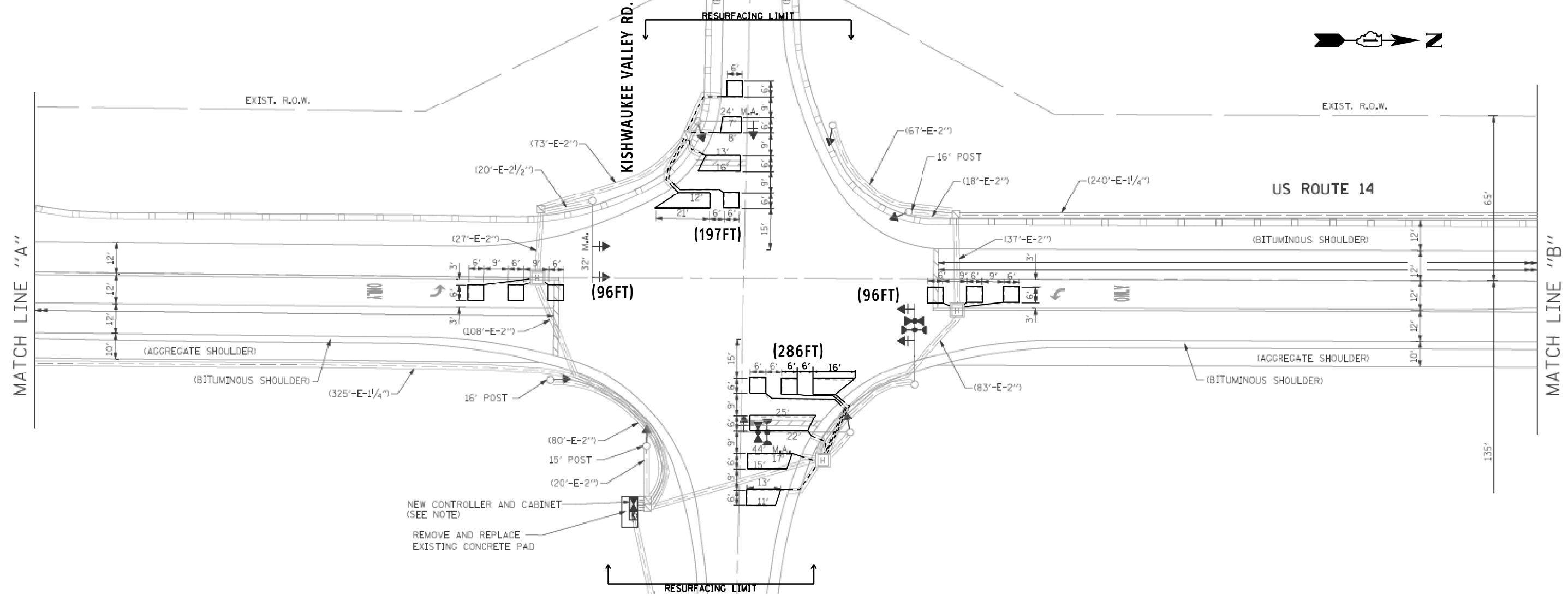
ALL EXCAVATED MATERIAL NOT NEEDED ON THE WORK SHALL BE DISPOSED OF ACCORDING TO ARTICLE 202.03.

BASIS OF PAYMENT. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR DRYWELL.

ALL EXCAVATION REQUIRED IN THE PERFORMANCE OF THE WORK, DISPOSAL OF EXCAVATION, BACKFILL, AND MATERIALS CALLED OUT IN THIS DETAIL (UNLESS OTHERWISE NOTED) SHALL BE INCLUDED IN THE UNIT BID PRICE PER EACH FOR DRYWELL.

INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9598 www.infrastructure-eng.com	USER NAME = A.Ralph DESIGNED - ACR DRAWN - ACR CHECKED - ACL DATE - 11/15/2021	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRYWELL - DETAILS		F.A.P. RTE. 305 SECTION 2021-022-R5 COUNTY MCHENRY TOTAL SHEETS 35 SHEET NO. 19
	PLOT SCALE = 100,0000 ' / in. PLOT DATE = 12/28/2021	DATE - 11/15/2021		SCALE: 1"=50'	STA. TO STA.	ILLINOIS FED. AID PROJECT

MODEL Path: \\...
 FILE NAME: P:\2020\2020-04094-00_IDOT_Various Phase 2 (ITB) 106 Item 16\Work Order 131\DC\CAD\Drawings\Sheet\162\MS-SHT\LOOPS-01.dwg



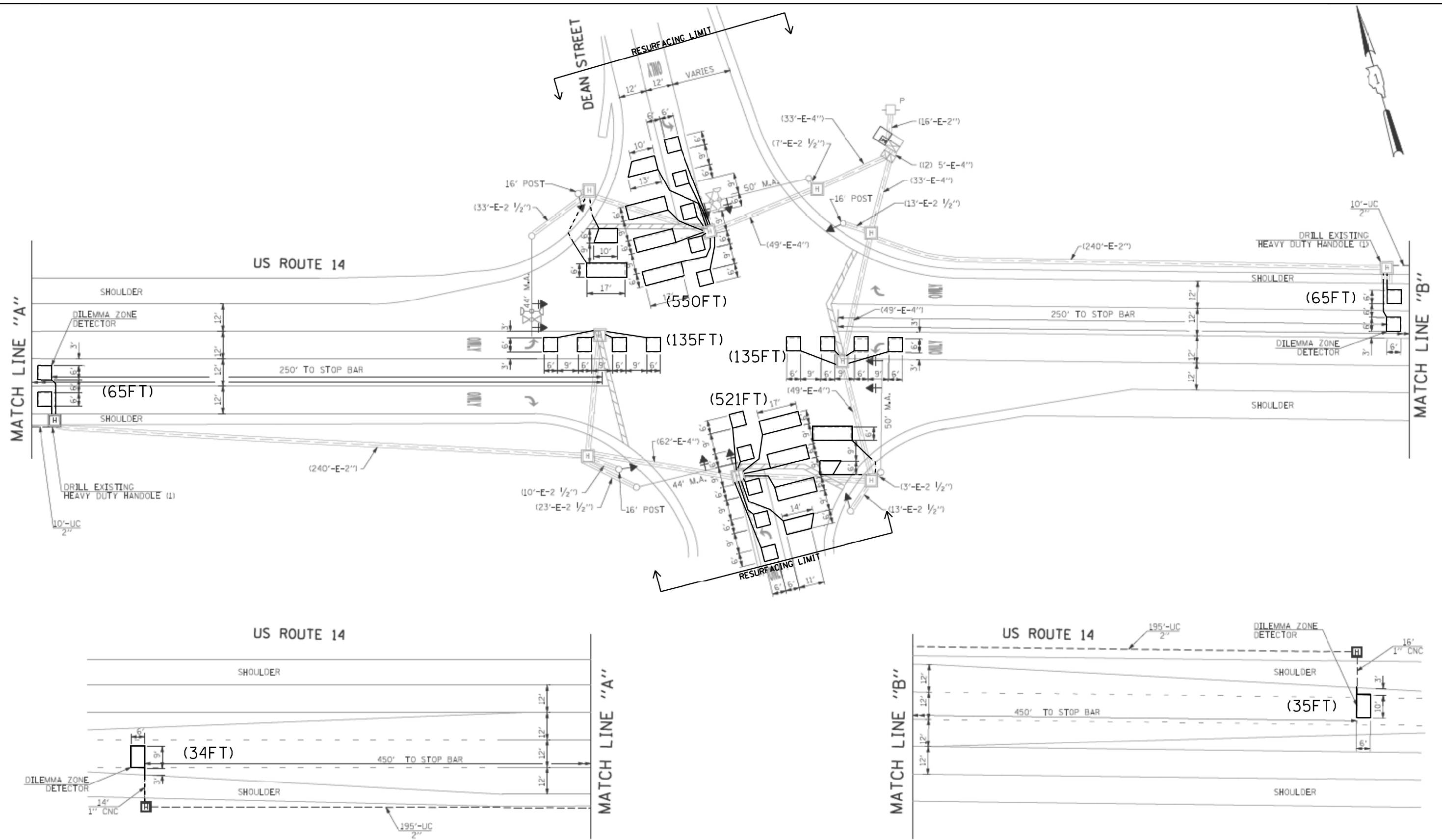
- NOTES:**
- 1.- WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
 - 2.- THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT	TS#21470
88600600	DETECTOR LOOP REPLACEMENT	783	FOOT	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DETECTOR LOOP REPLACEMENT PLAN US-14 / KISHWAUKEE VALLEY RD.		
		SCALE: 1"=50'	STA. TO STA.	
F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35 SHEET NO. 20	CONTRACT NO. 62N45
		ILLINOIS FED. AID PROJECT		

INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.8588 F 312.425.5594 www.infrastructure-eng.com	USER NAME = ALane	DESIGNED - MLD	REVISED -
	PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
	PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
		DATE - 10/15/2021	REVISED -



MODEL Path: \\...
 FILE NAME: P:\...
 ORDER: 131021CADD_Sheets\162145-SHT1.L005-02.dwg
 FROM: 161Work Order
 DATE: 12/13/2021

- NOTES:**
- 1.- WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
 - 2.- THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	1540	FOOT

TS#7238

<p>INFRASTRUCTURE ENGINEERING 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9594 www.infrastructure-eng.com</p>	USER NAME = ALane	DESIGNED - MLD	REVISED -
	PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
	PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
		DATE - 10/15/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

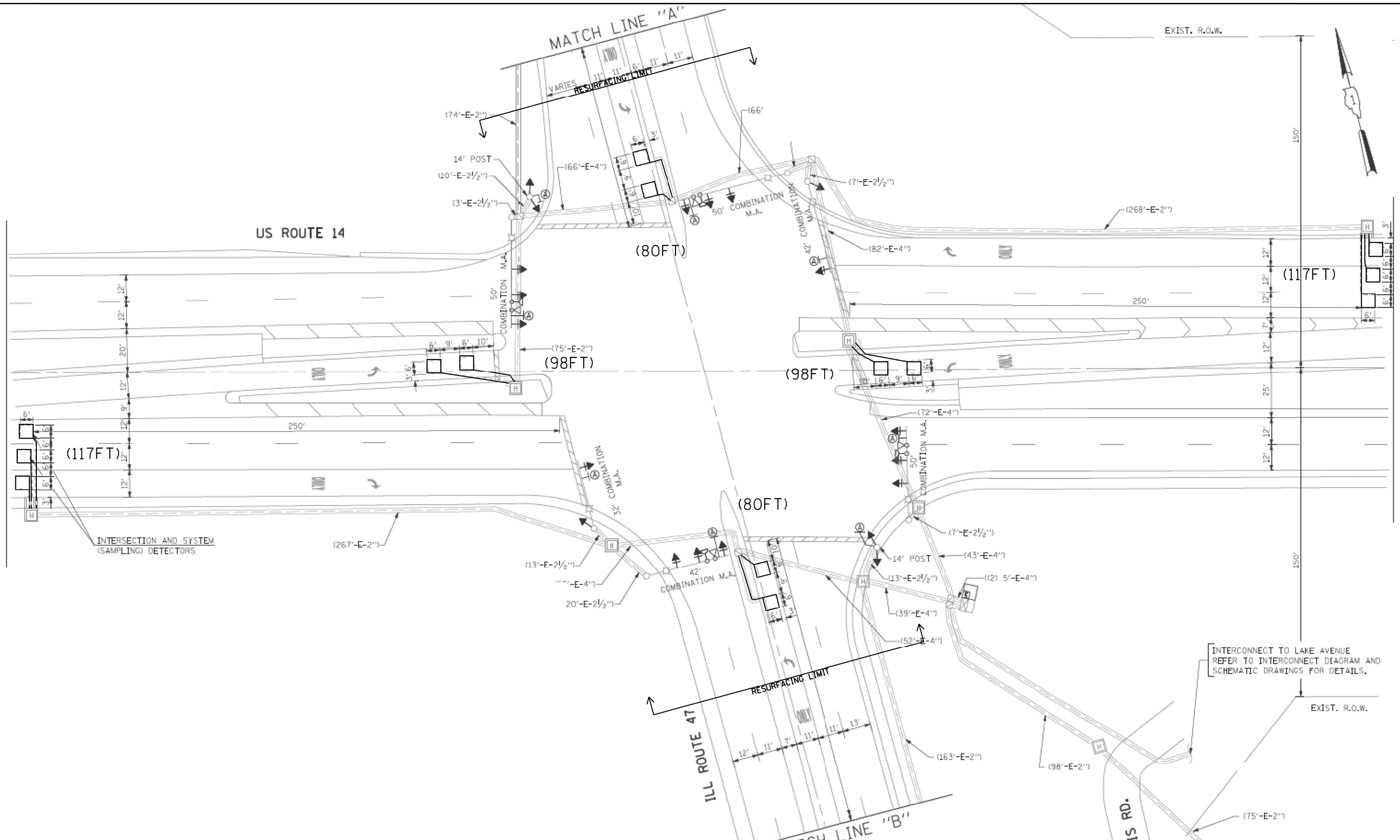
**DETECTOR LOOP REPLACEMENT PLAN
US-14 / DEAN ST.**

SCALE: 1"=50'

STA. TO STA.

F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 21
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	

MODEL Path: \\... \DOT_Vehicle Phase 2 (ITB) 106 Item 161Work Order 131DCMCA00_Sheets\162144-SHT_LOOP5-02.dwg
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NOTES:

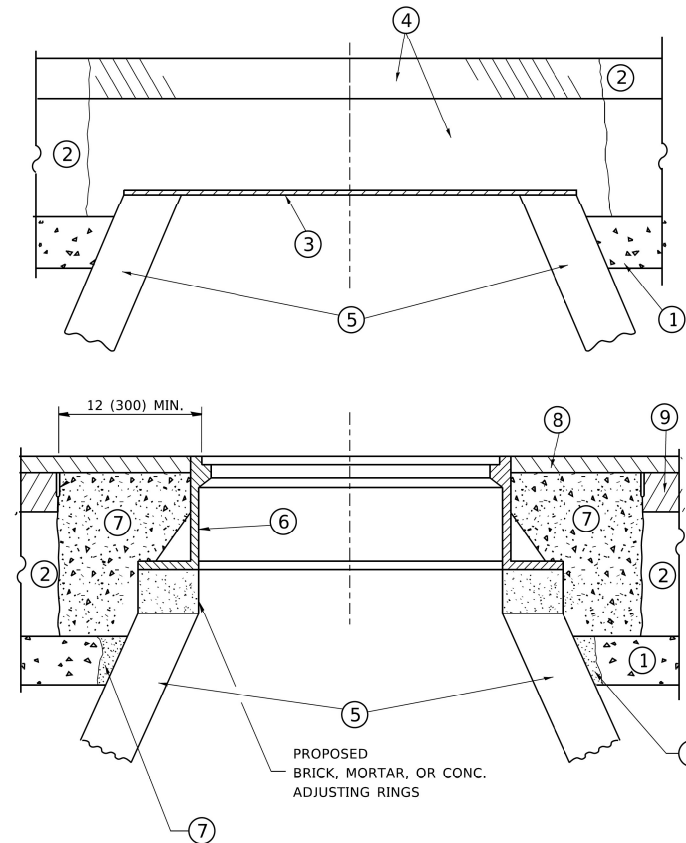
- 1.- WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
- 2.- THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	590	FOOT

TS#7235

<p>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.475.9598 www.infrastructure-eng.com</p>	USER NAME = ALane DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 10/15/2021	DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 10/15/2021	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN US-14 / EASTWOOD DR.	F.A.P. RTE. 305 SECTION 2021-022-R5 COUNTY MCHENRY CONTRACT NO. 62N45	TOTAL SHEETS 35 SHEET NO. 22	
	PLOT SCALE = 1200.0000' / ft. PLOT DATE = 12/13/2021				SCALE: 1"=50' STA. TO STA.	ILLINOIS FED. AID PROJECT		
	INTERCONNECT TO LAKE AVENUE REFER TO INTERCONNECT DIAGRAM AND SCHEMATIC DRAWINGS FOR DETAILS.							



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

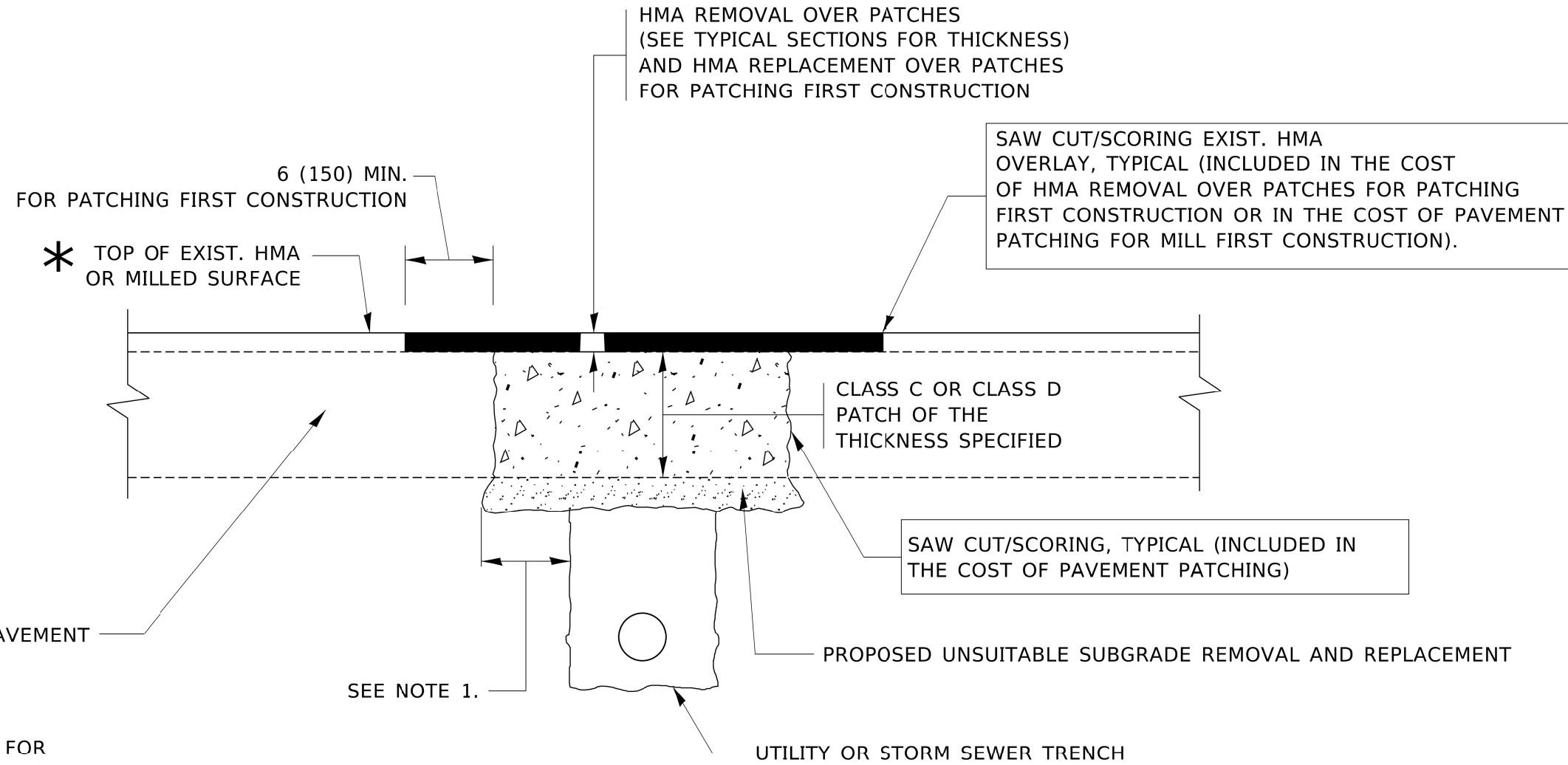
WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL Path: \\... \Infrastructure\Phase 2 (PFE) 106 Item: 16\Work Order: 131\DCM\CADD_Sheets\162N45-SHT-STANDARD05-01.dgn

	USER NAME = ALane	DESIGNED - MLD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE – DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1/200.0000' / ft.	CHECKED - ACL	REVISED -			305	2021-022-R5	MCHENRY	35	23
PLOT DATE = 12/29/2021	DATE - 10/15/2021	REVISED -	REVISED -	SCALE: 1"=50'	STA.	TO STA.	ILLINOIS	FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

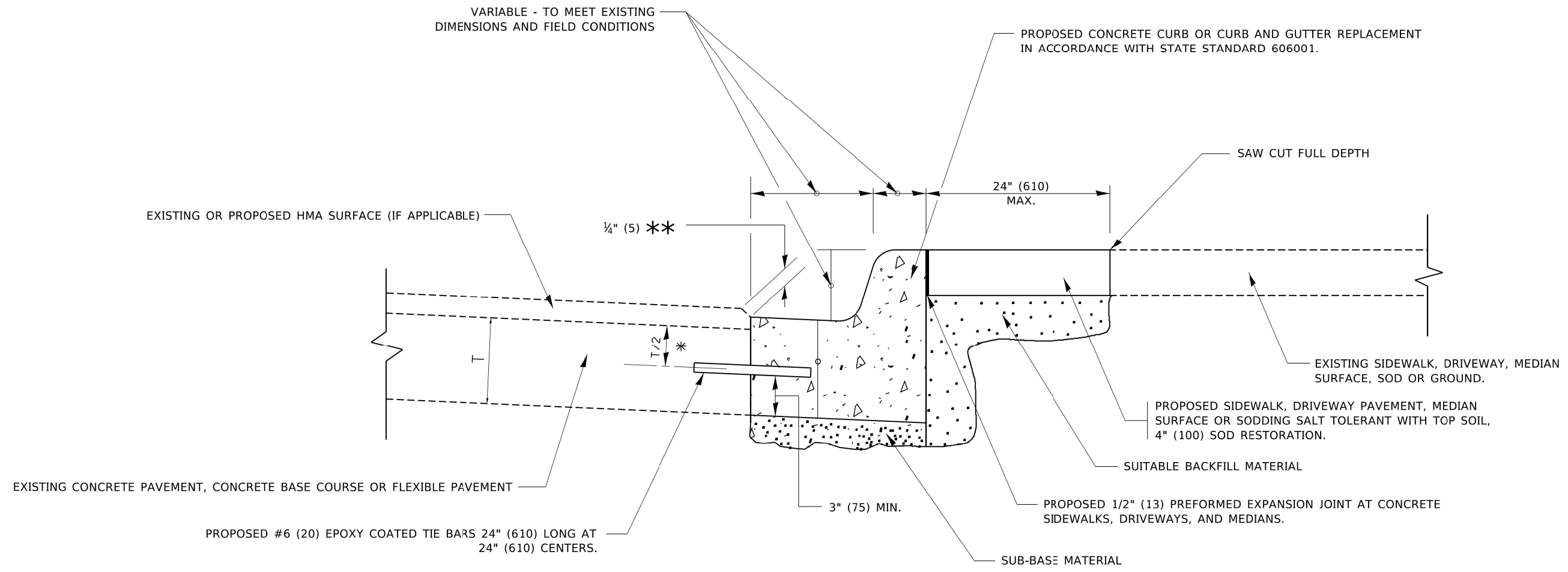
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: P:\d\p\h\h... FILE NAME: P:\d\p\h\h... ORDER: 131021CADD_Sheets\162N45-SHT-STANDARD05-02.dgn

<p>INFRASTRUCTURE ENGINEERING 1 South Wacker Suite 2650 Chicago, IL 60606 P: 312.425.9598 F: 312.425.9598 www.infrastructure-eng.com</p>	USER NAME = ALane PLOT SCALE = 1/200.0000' / ft. PLOT DATE = 12/13/2021	DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 10/15/2021	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE – PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	F.A.P. RTE. = 305 SECTION = 2021-022-R5 COUNTY = MCHENRY TOTAL SHEETS = 35 SHEET NO. = 24 CONTRACT NO. 62N45	ILLINOIS FED. AID PROJECT
	SCALE: 1"=50' STA. TO STA.						



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL Path: \\... \Infrastructure\Phase 2\ITB\106 Item: 16\Work Order: 131\DC\CA\DD_Sheets\162\N4-SHT-STANDARD05-03.dgn

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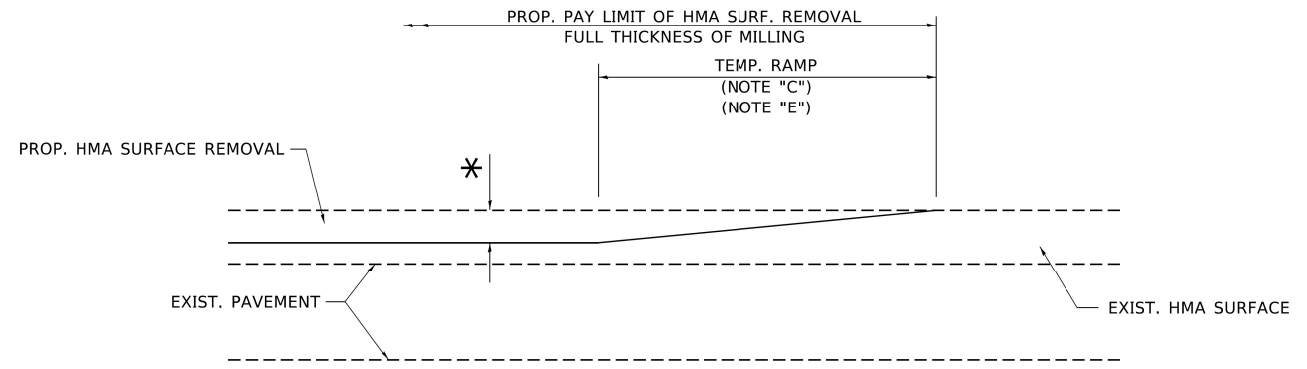
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	DRAWN - MLD	REVISED -
PLOT SCALE = 1200.0000' / ft.	CHECKED - ACL	REVISED -
PLOT DATE = 12/13/2021	DATE - 10/15/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE - CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT (BD-24)**

SCALE: 1"=50' STA. TO STA.

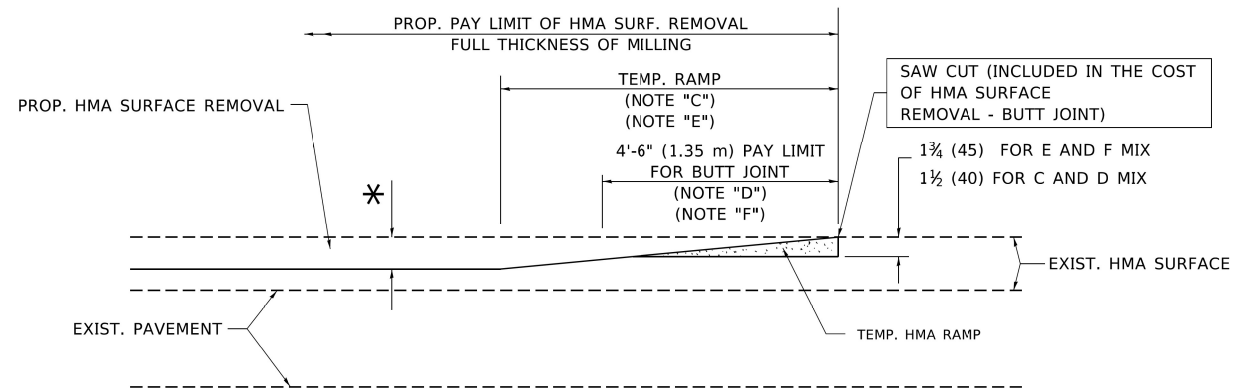
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305	2021-022-R5	MCHENRY	35	25
CONTRACT NO. 62N45				
ILLINOIS		FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

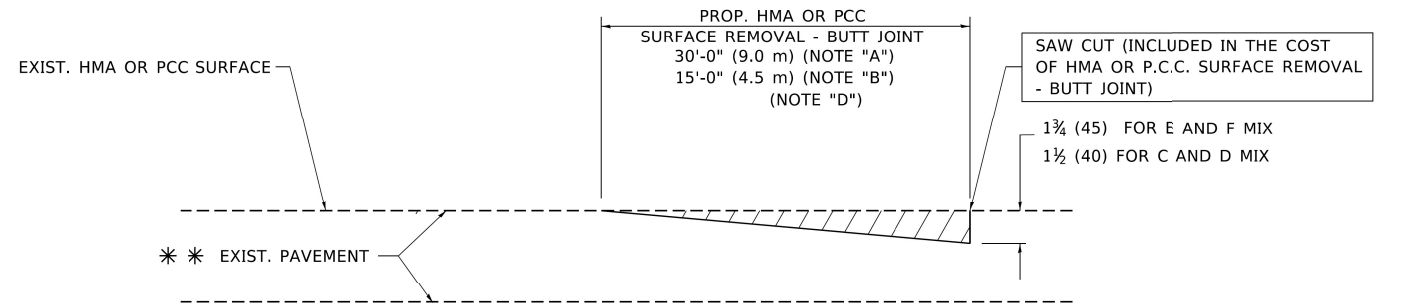


HMA CONSTRUCTED TEMPORARY RAMP

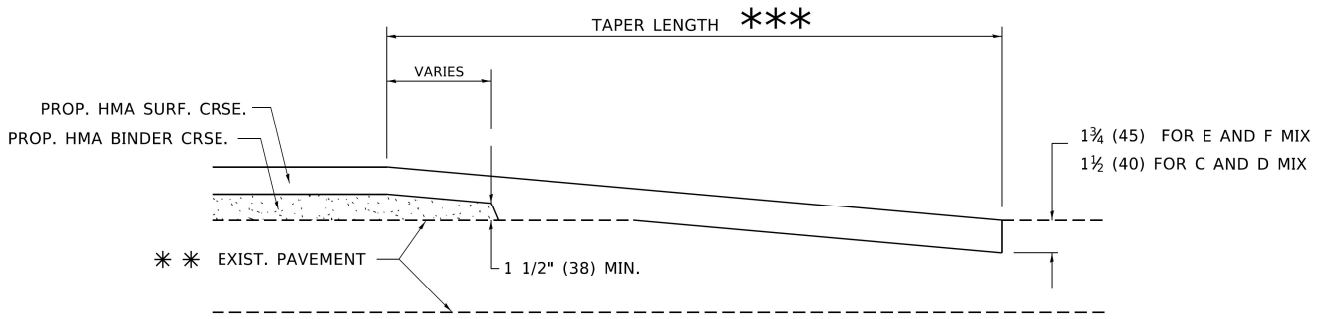
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

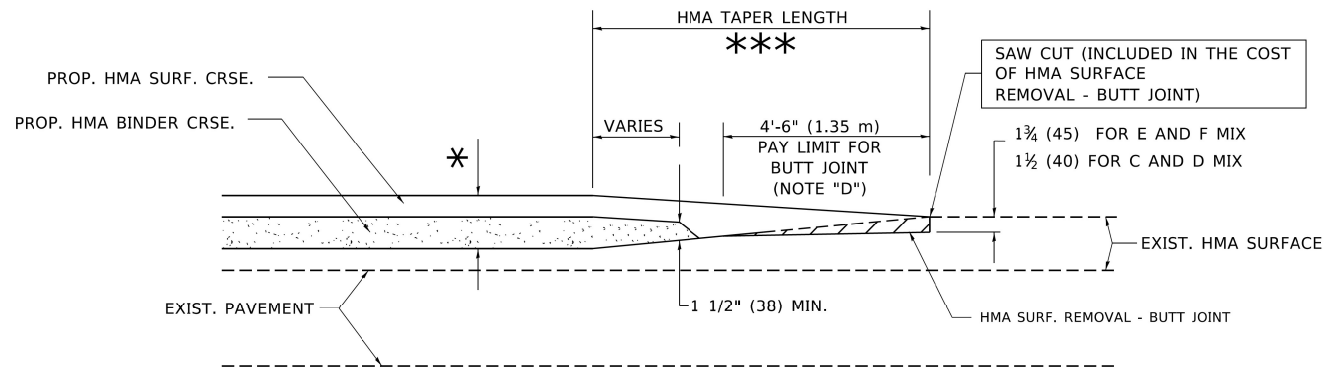
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL Path: \\p0304094-00_IDOT_Various_Phase_2_IPTL_106_Item_161\Work_Order_131\DCM\CADD_Sheets\162\N4-SHT-STANDARD\BD-04.dwg
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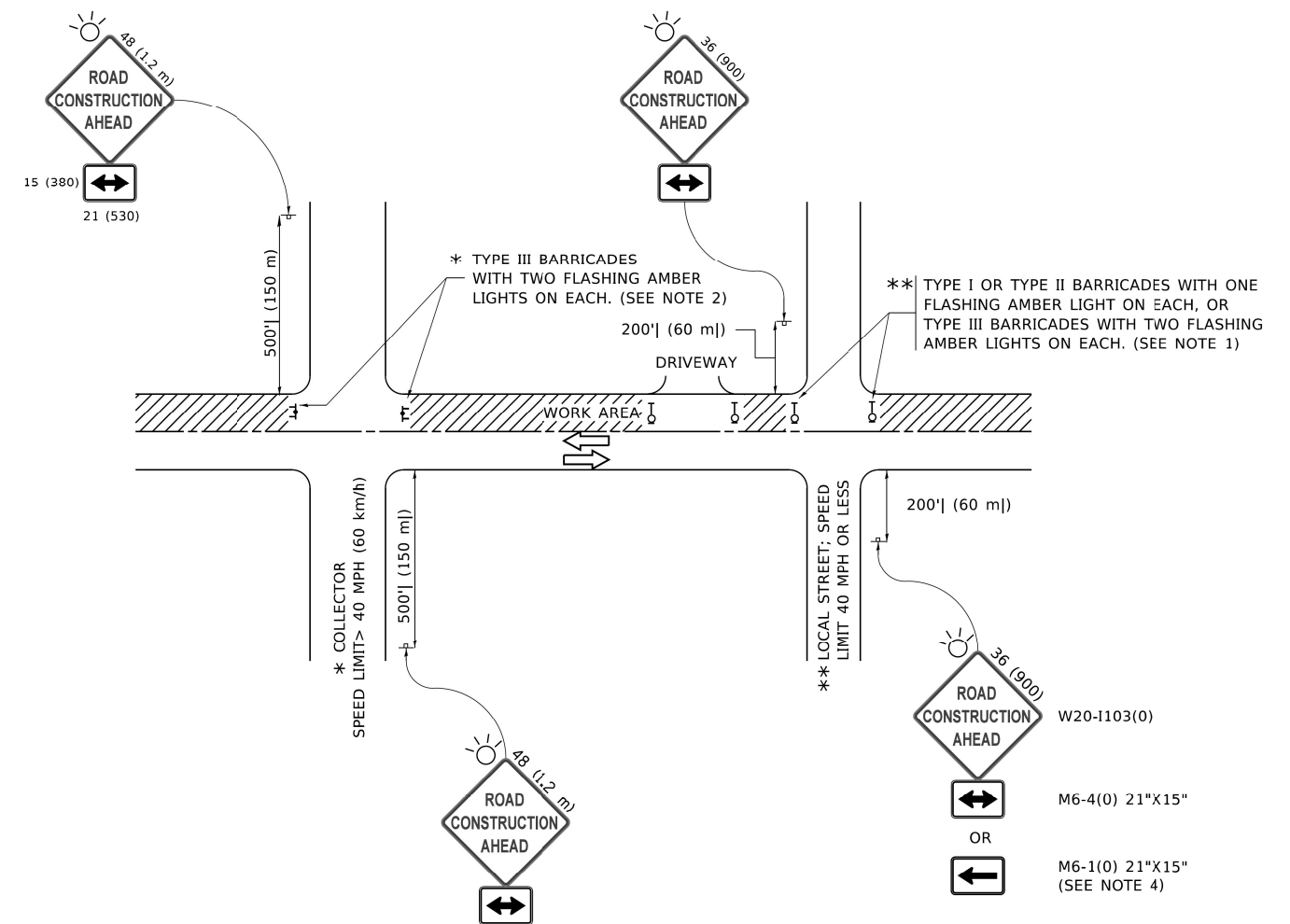
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PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - BUTT JOINT AND HMA TAPER DETAILS (BD-32)

SCALE: 1"=50' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	26
			CONTRACT NO. 62N45	
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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INFRASTRUCTURE ENGINEERING <small>1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9598 www.infrastructure-eng.com</small>	USER NAME = ALane	DESIGNED - MLD	REVISED -
	PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
	PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
		DATE - 10/15/2021	REVISED -

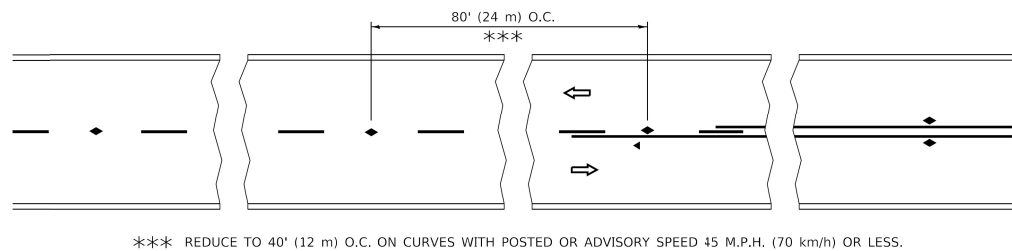
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE – RUMBLE STRIPS FOR CENTERLINE,
NON-FREEWAY (BD-55)**

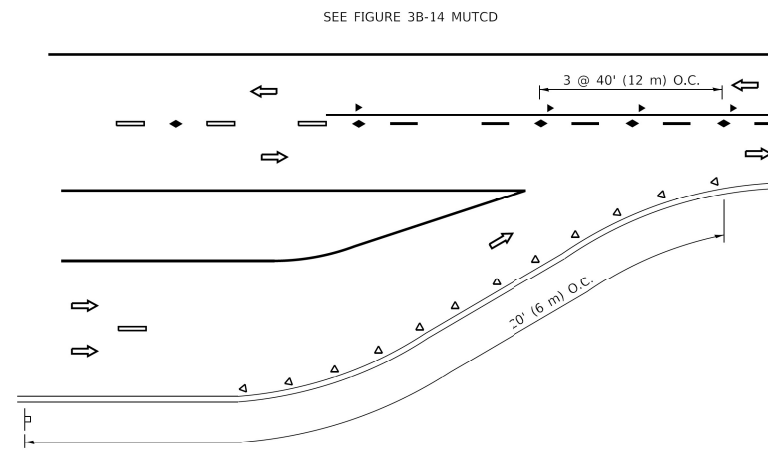
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STA. TO STA.

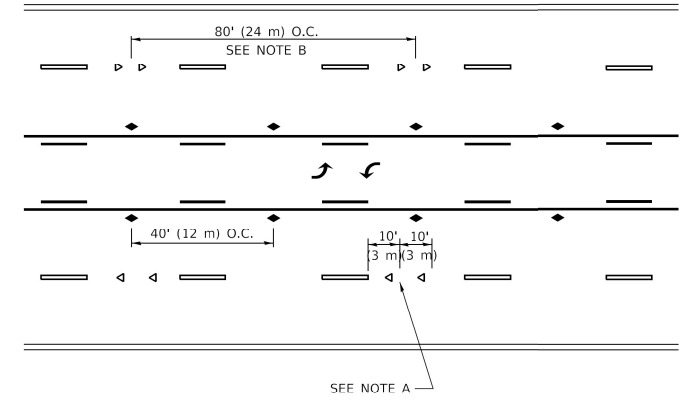
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	27
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	



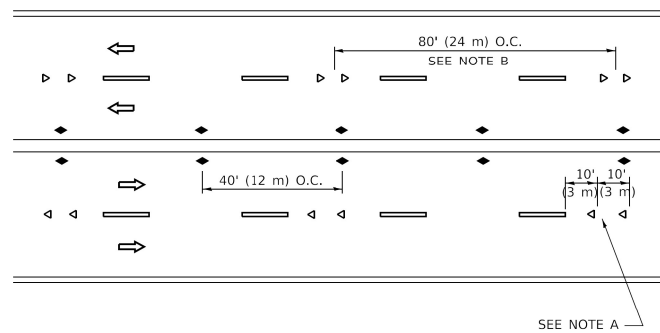
TWO-LANE/TWO-WAY



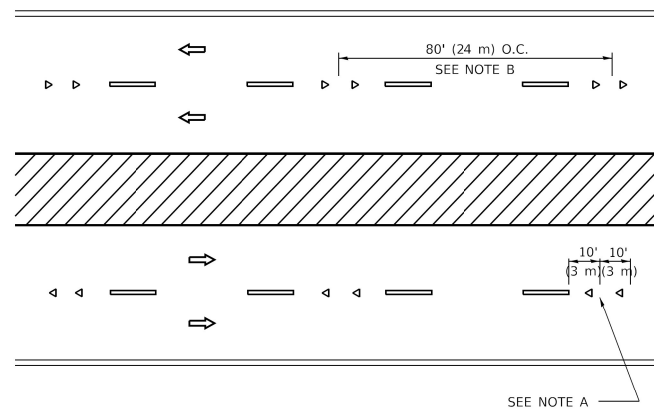
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- 1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- 4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

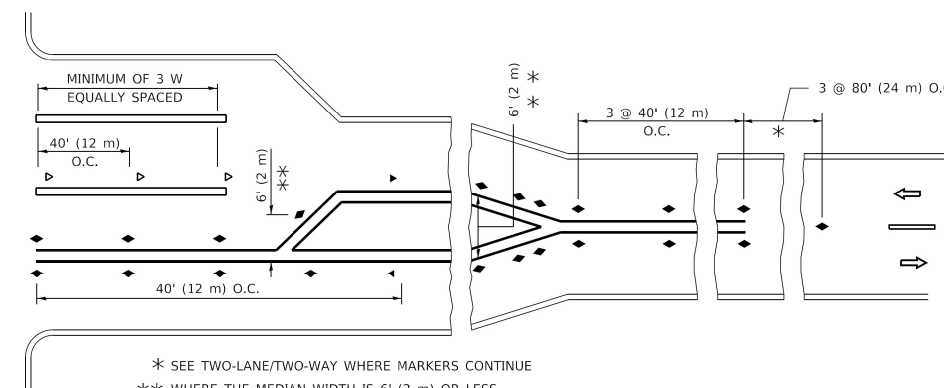
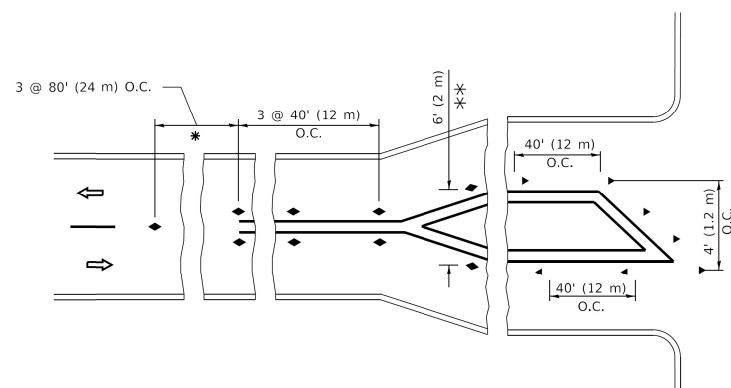
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

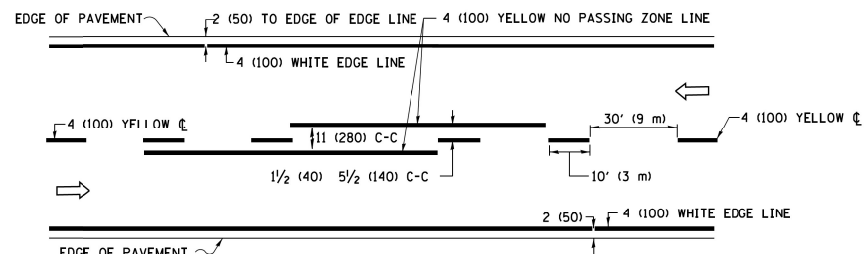
- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- 3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- 4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



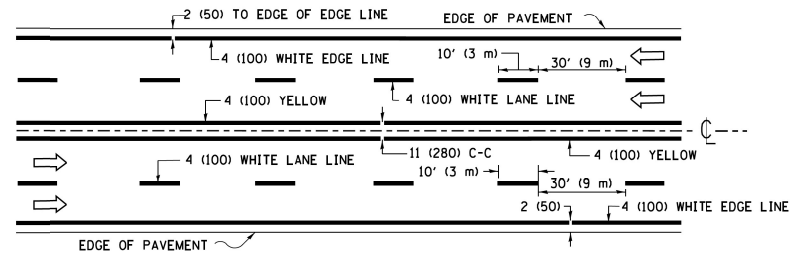
TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

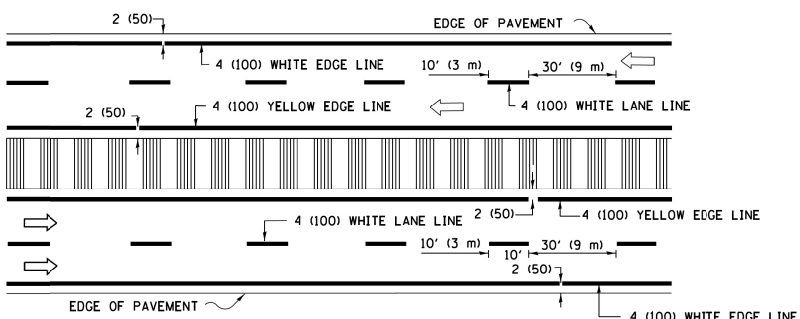
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2-LANE ROADWAY

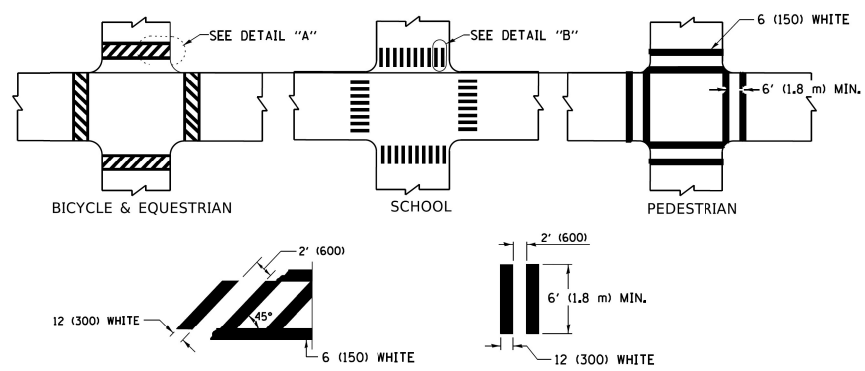


MULTI-LANE UNDIVIDED



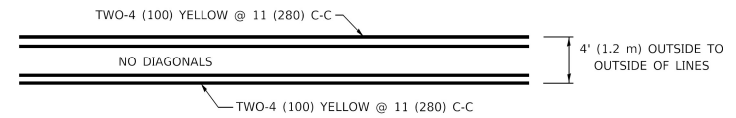
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

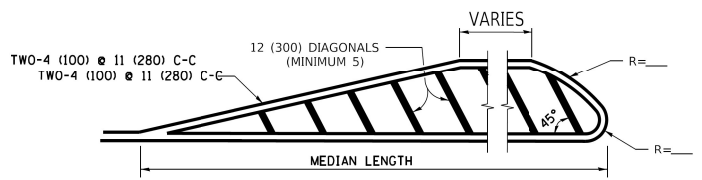


TYPICAL CROSSWALK MARKING

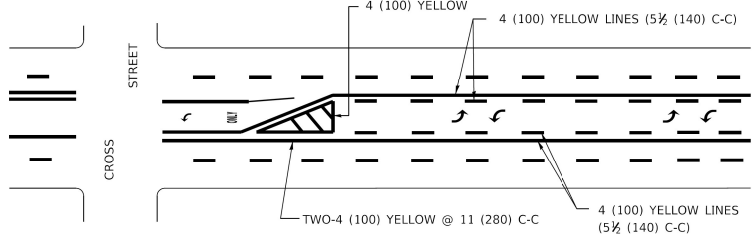
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



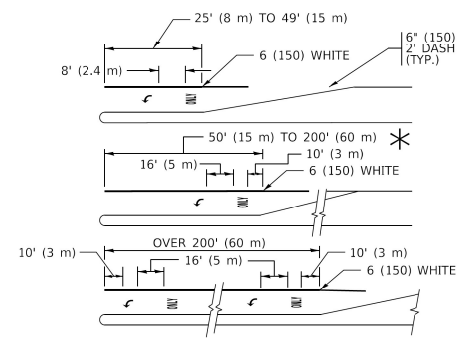
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

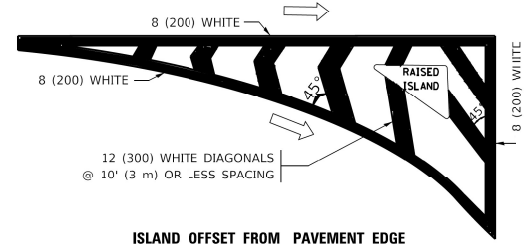


MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

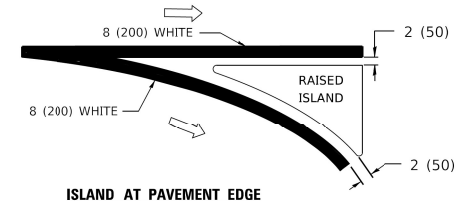


TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING

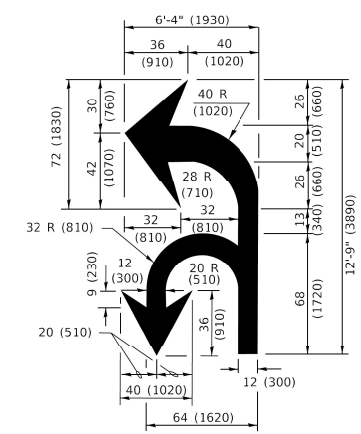
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



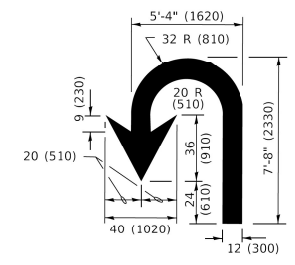
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
 * LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15' 6" (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL Path: \\p01\infrastructure\Phase 2\ITPL 106 Item 16\Work Order 131021\CADD_Sheets\62N45-SHT-STANDARD-RS-01.dwg
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 P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

USER NAME = ALane	DESIGNED - MLD	REVISED -
PLOT SCALE = 1/200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
 SCALE: 1"=50'

F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 29
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

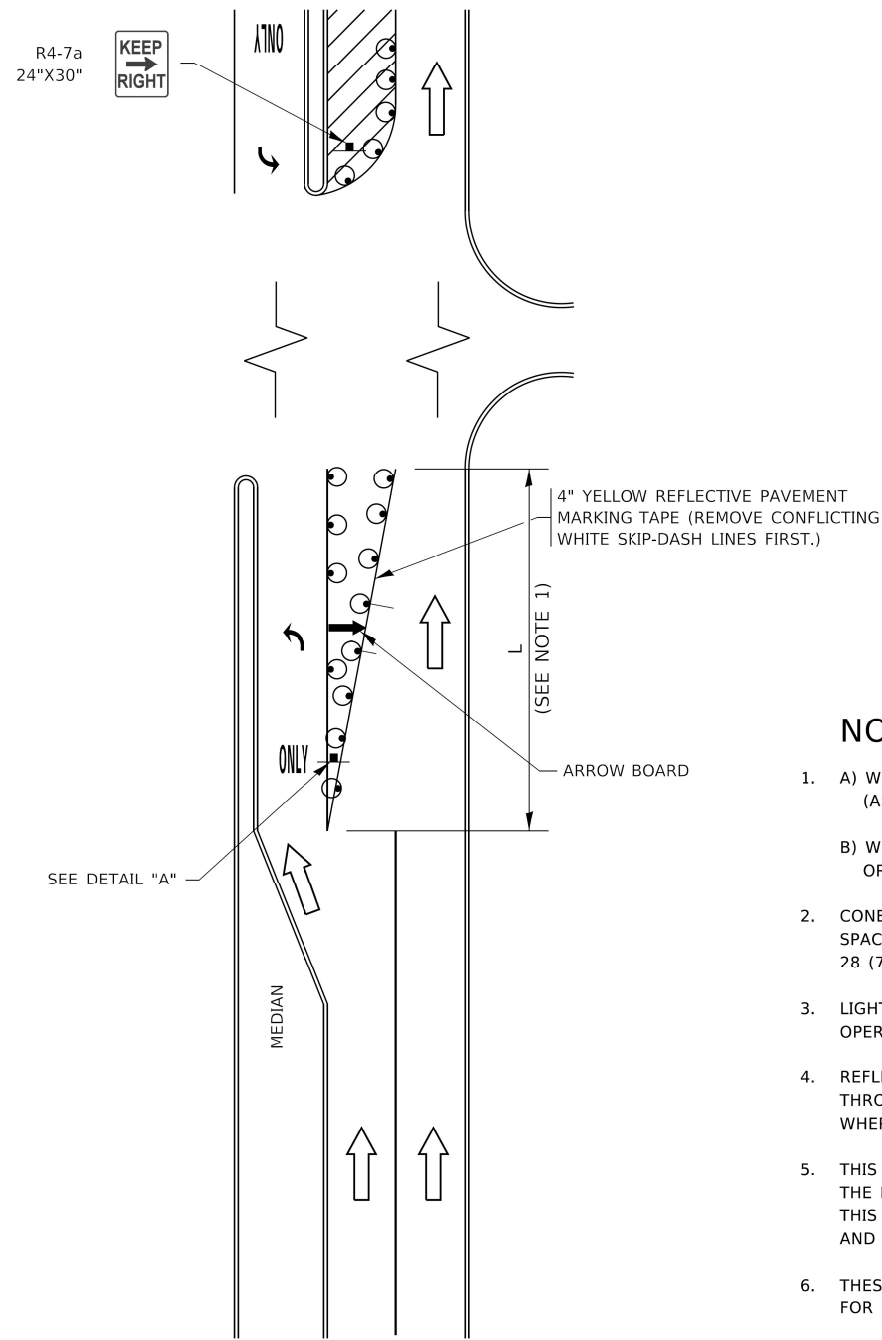


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

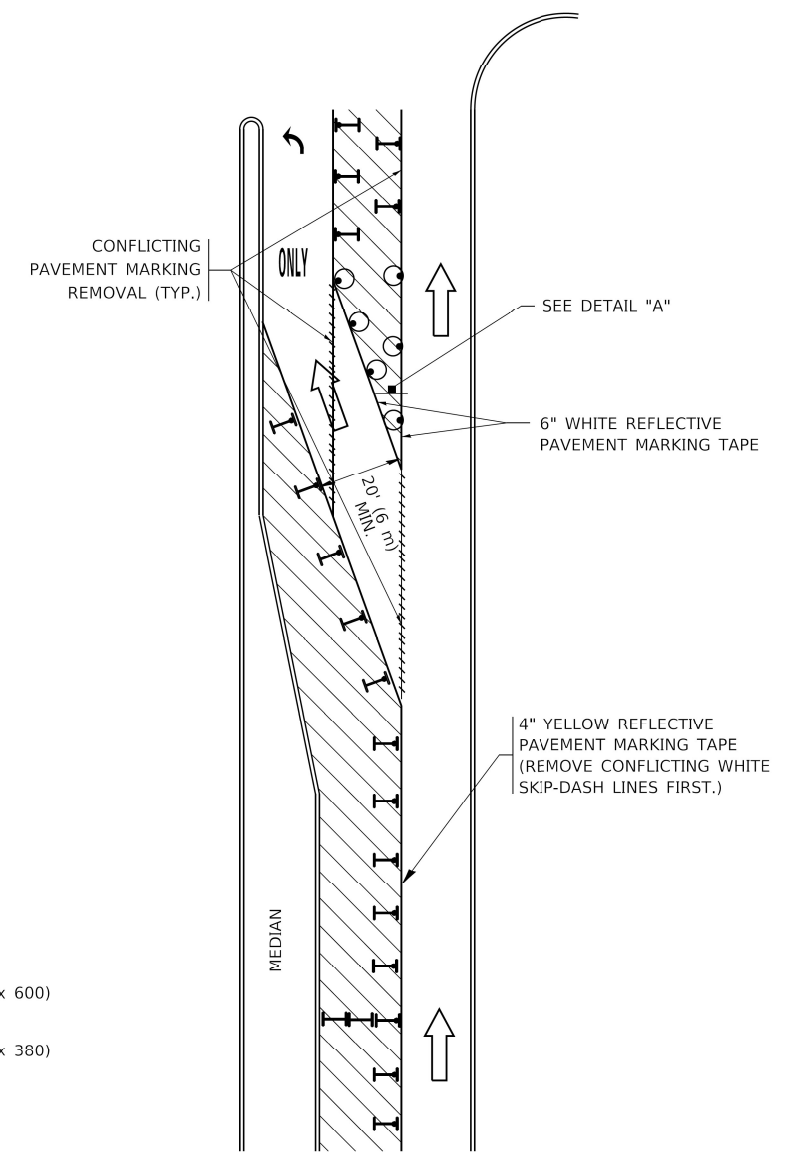


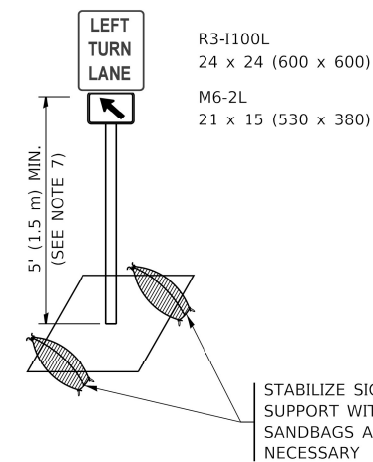
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
 B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 78 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL Path: \\p1\pub\16\work\161001\161001.dwg; FILE NAME: P:\161001\161001.dwg; ORDER: 131001CADD_Sheets\161001-SHT-STANDARD-05.dwg

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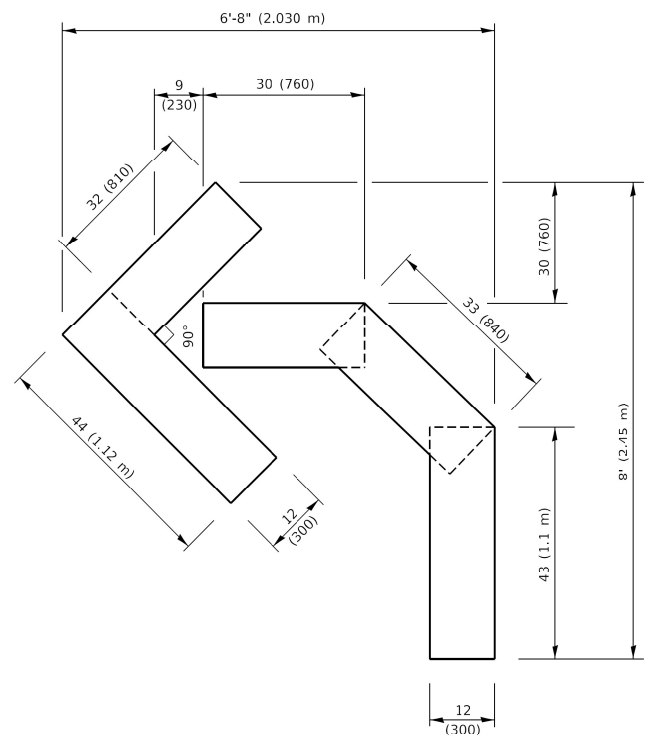
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PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE – TYPICAL PAVEMENT MARKINGS
(TC-13)**

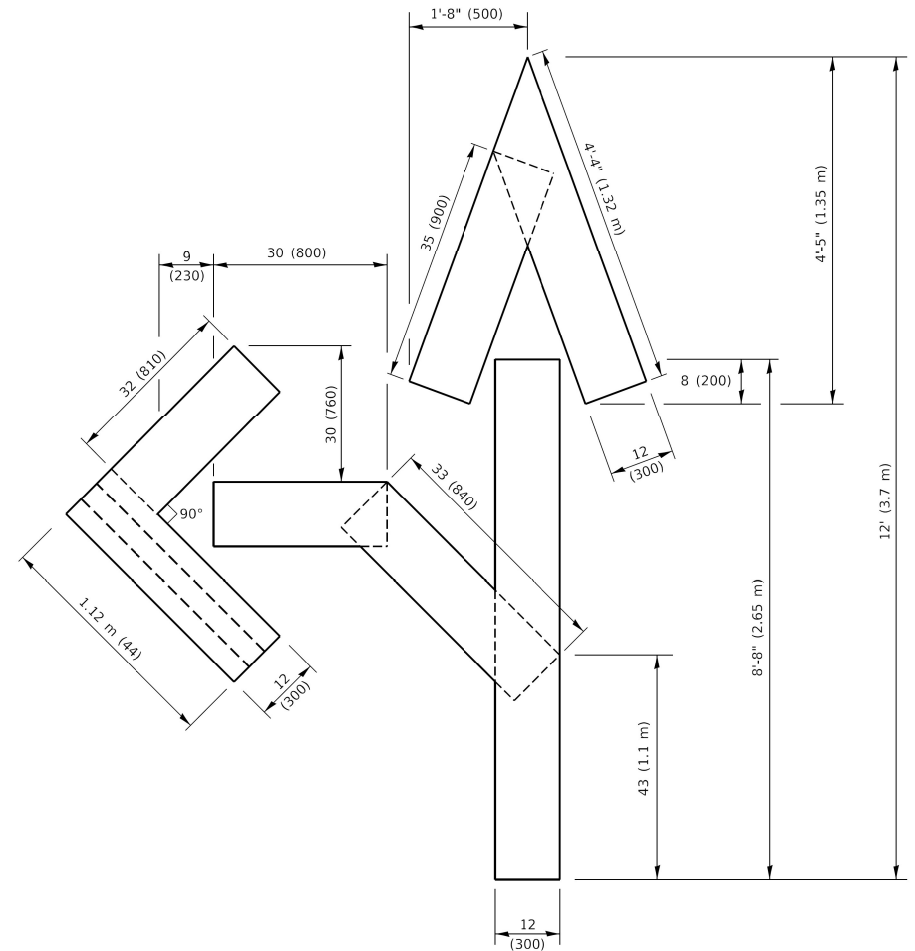
SCALE: 1"=50'

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	30
CONTRACT NO. 62N45				
ILLINOIS FED. AID PROJECT				



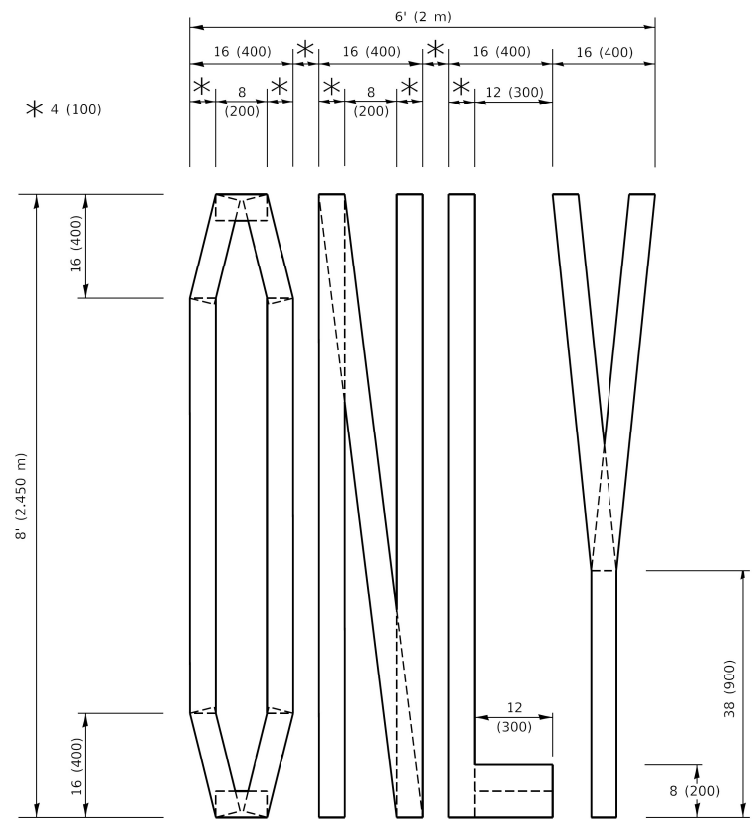
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



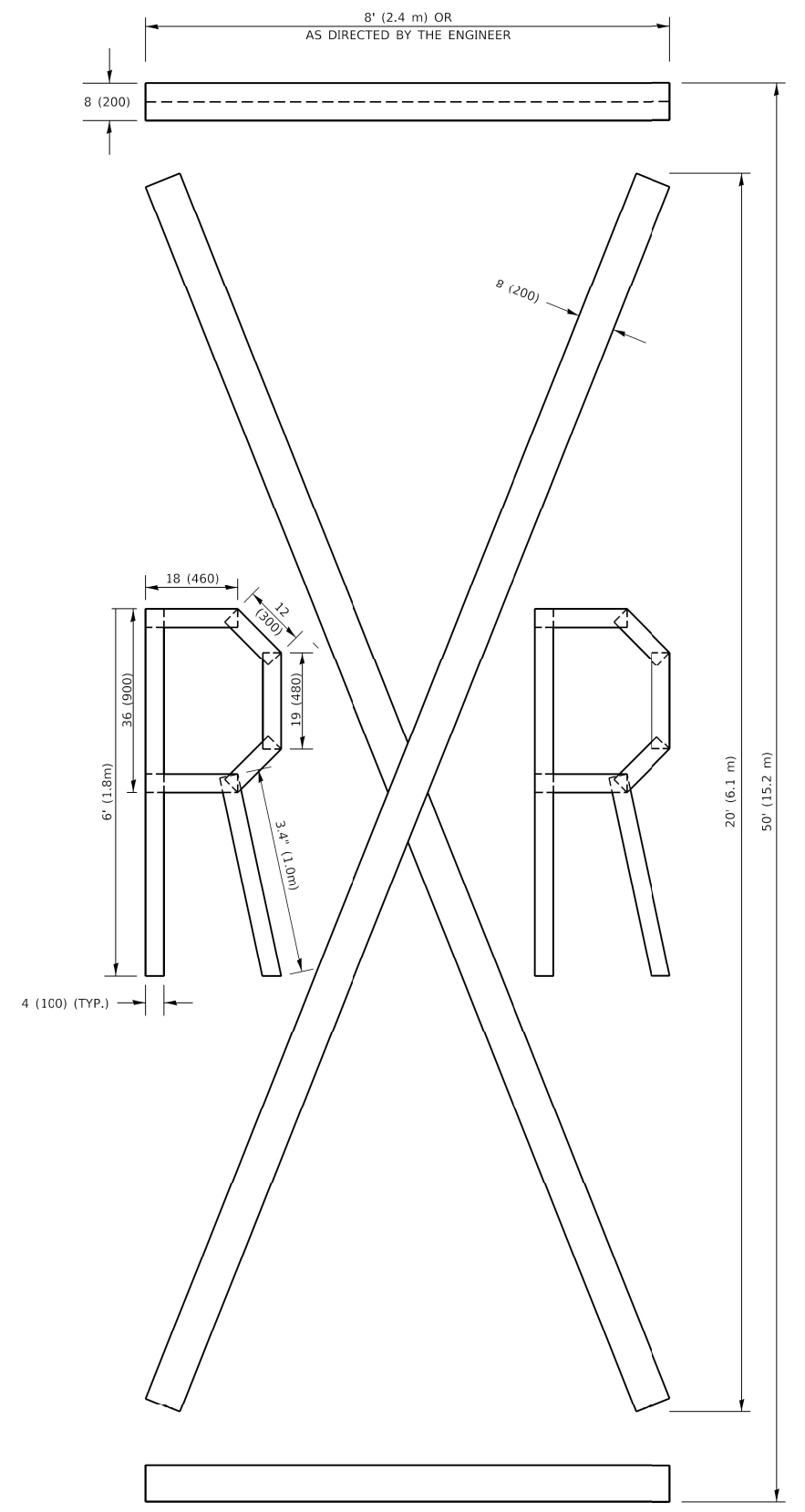
QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

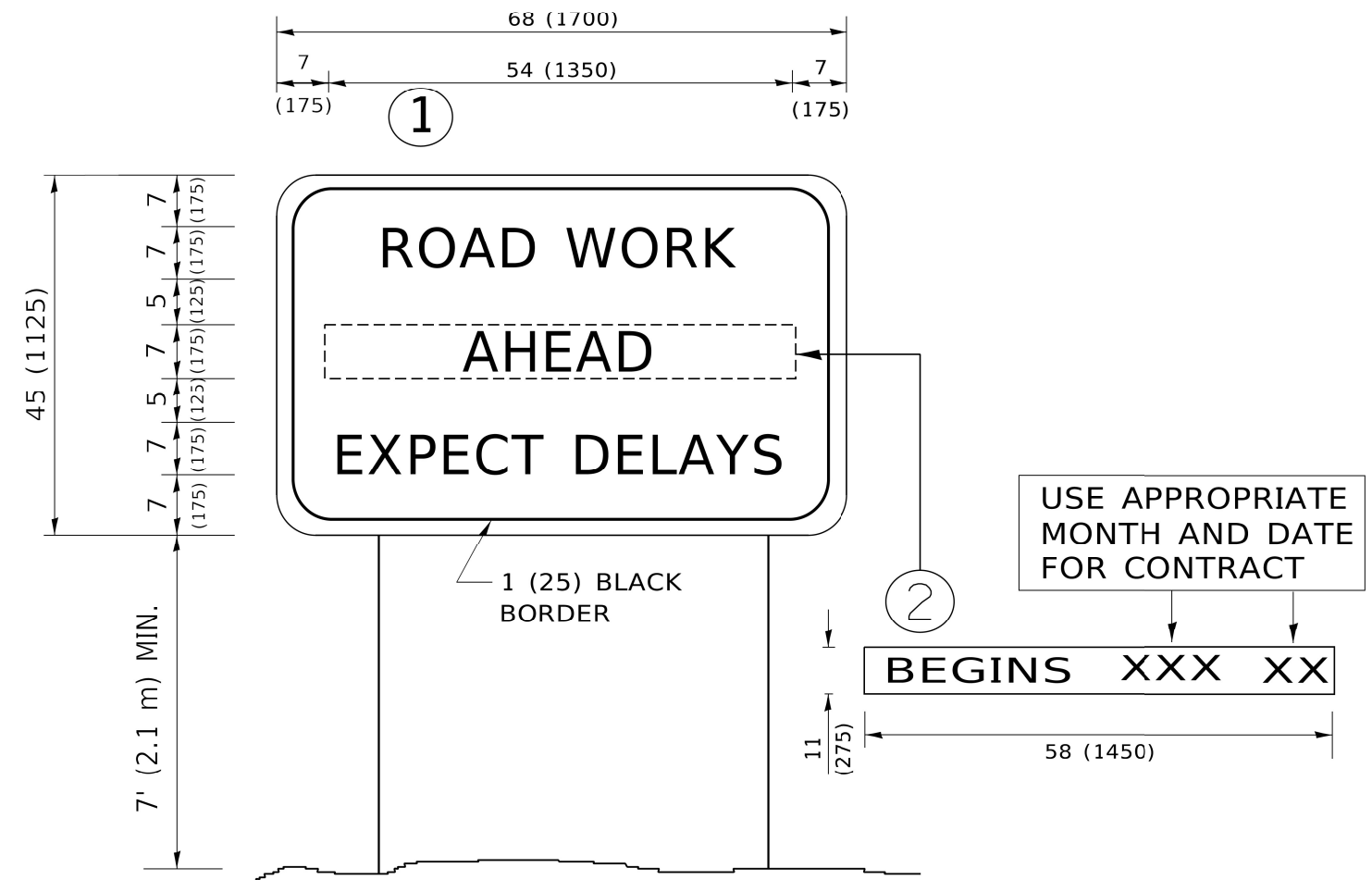
NOTE:
ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL Path: \\... \Infrastructure\Phase 2 (TPE) 106 Item 16\Work Order 131\DCM\CADD_Sheets\162N45-SHT-STANDARD-RDS-05.dgn

USER NAME = ALane	DESIGNED - MLD	REVISED -
PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 31
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

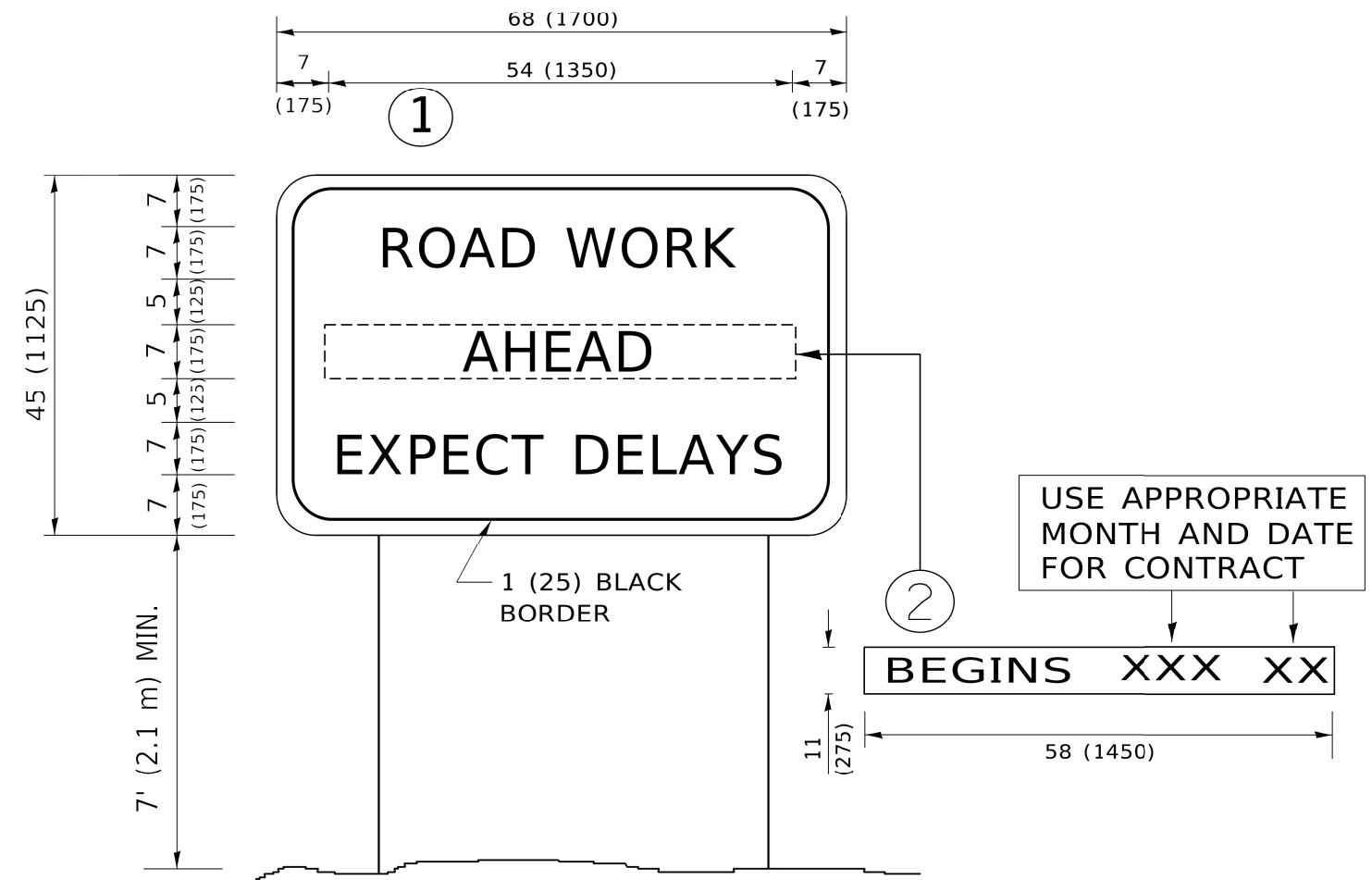
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INFRASTRUCTURE ENGINEERING 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.475.9598 F 312.475.9599 www.infrastructure-eng.com	USER NAME = ALane	DESIGNED - MLD	REVISED -
	PLOT SCALE = 1200.0000' / ft.	DRAWN - MLD	REVISED -
	PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
		DATE - 10/15/2021	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE – SHORT TERM PAVEMENT MARKING
LETTERS AND SYMBOLS (TC-16)
 SCALE: 1"=50' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2021-022-R5	MCHENRY	35	32
CONTRACT NO. 62N45			ILLINOIS FED. AID PROJECT	



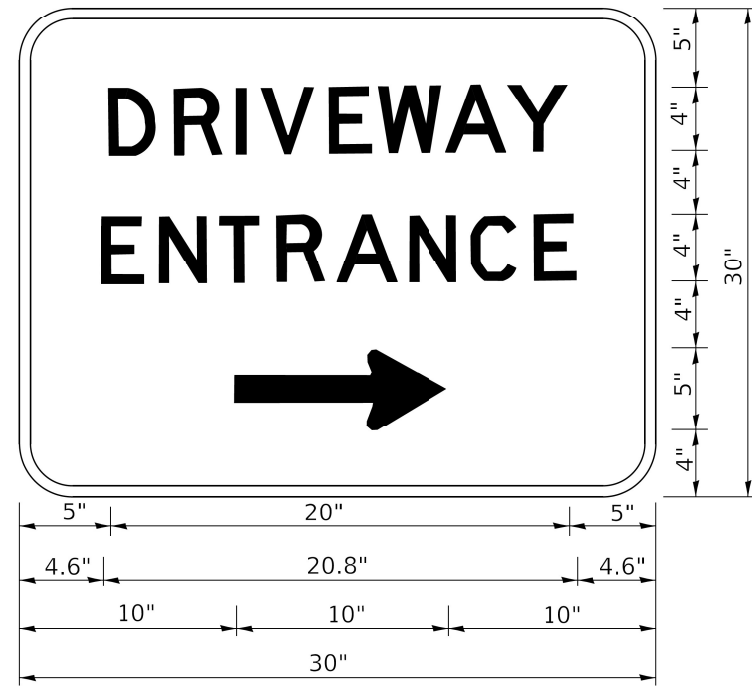
NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL Path: \\... \Infrastructure\Phase 2\ITPL 106 Item 16\Work Order 131\DC\CLCADD_Sheets\162\MS-SHT-STANDARD\BDS-11.dgn
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INFRASTRUCTURE ENGINEERING <small>1 South Wacker Suite 2650 Chicago, IL 60606 P 312.475.9598 F 312.475.9598 www.infrastructure-eng.com</small>	USER NAME = ALane PLOT SCALE = 1200.0000' / ft. PLOT DATE = 12/13/2021	DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 10/15/2021	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE – ARTERIAL ROAD INFORMATION SIGN (TC-22)	F.A.P. RTE. = 305 SECTION = 2021-022-R5 COUNTY = MCHENRY TOTAL SHEETS = 35 SHEET NO. = 33	CONTRACT NO. 62N45 ILLINOIS FED. AID PROJECT
	SCALE: 1"=50' STA. TO STA.						



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

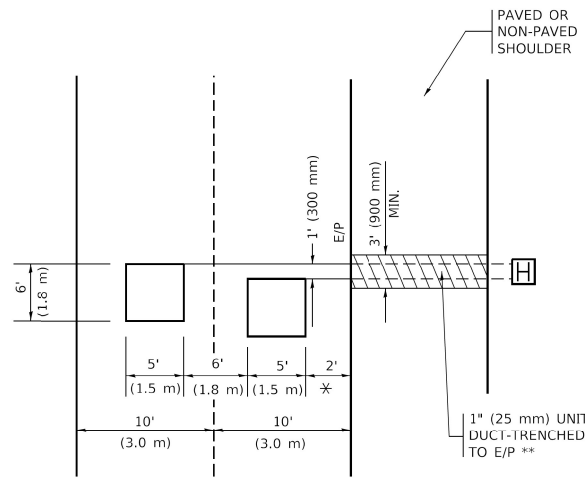
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL Path: \\...
 FILE NAME: P:\2020\4094-00 IDOT\Various Phase 2\ITB\ 106 Item 16\Work Order 13\DCM\CADD_Sheets\162N45-SHT-STANDARD05-12.dgn

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

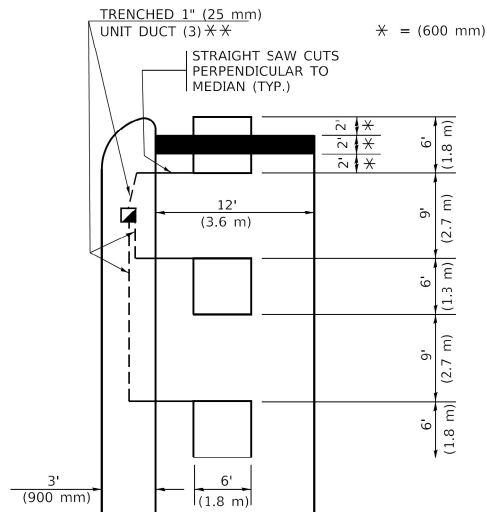


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

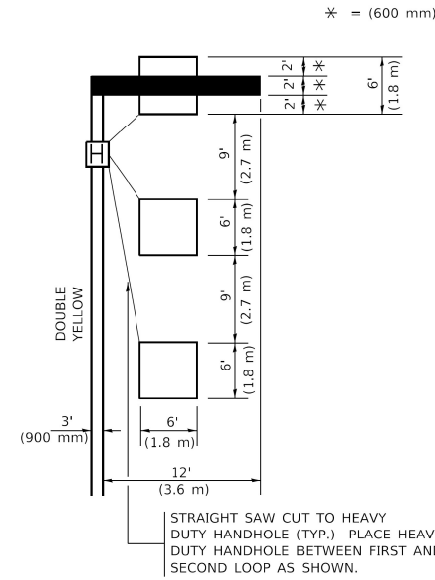


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

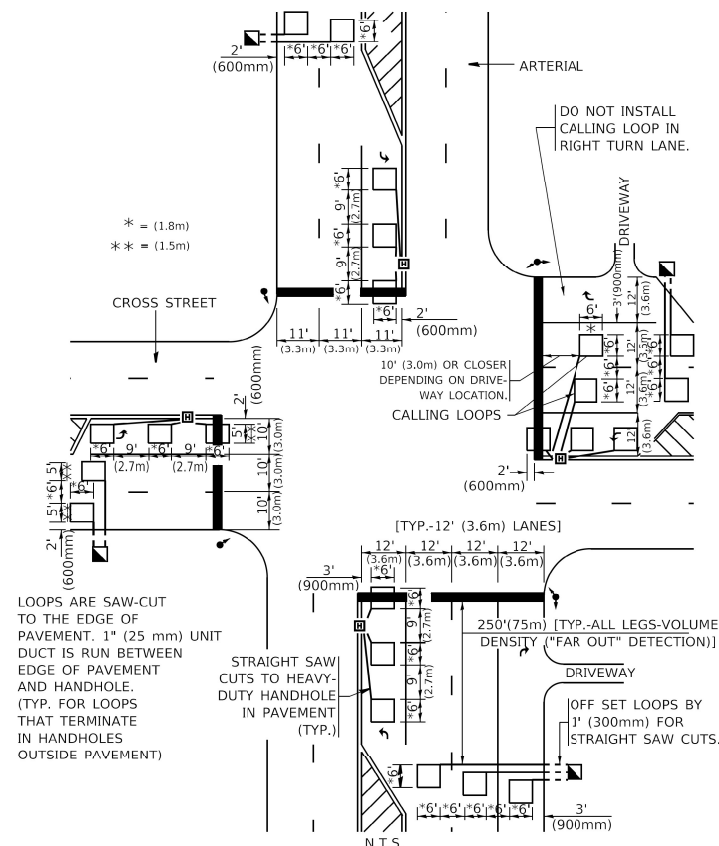
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



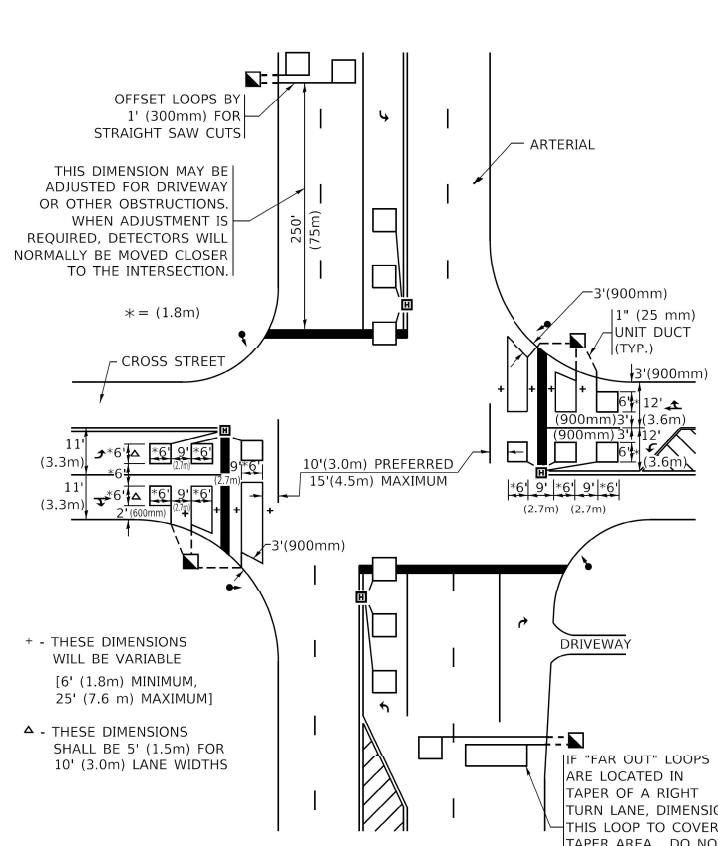
* = (1.8m)
 ** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



OFFSET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS!
 THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
 Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
 N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

MODEL Path: \\...
 FILE NAME: P:\2020\2020-09-04-00 IDOT Various Phase 2 (TPE) 106 Item 161\Work Order 131\DC\CAD\Drawings\162\N45-SHT-STANDARD\RD5-13.dwg

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USER NAME = ALane	DESIGNED - MLD	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN - MLD	REVISED -
PLOT DATE = 12/13/2021	CHECKED - ACL	REVISED -
	DATE - 10/15/2021	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING (TS-07)

SCALE: 1"=50'

F.A.P. RTE. 305	SECTION 2021-022-R5	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 35
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N45	