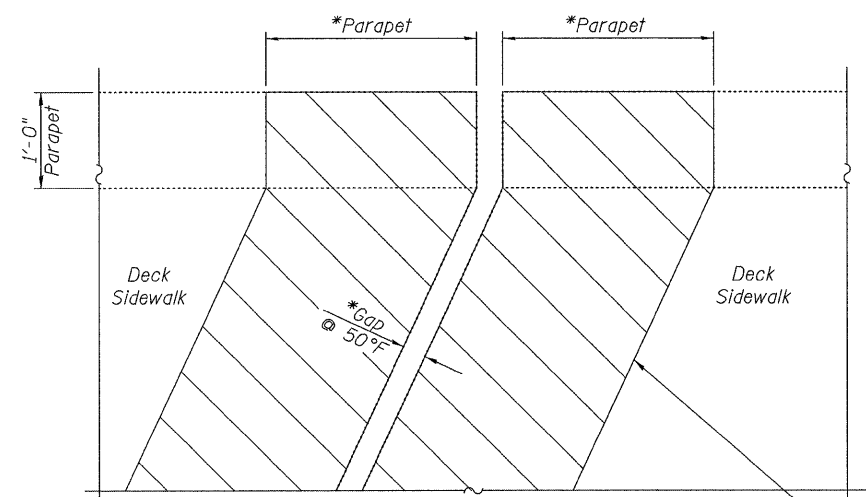


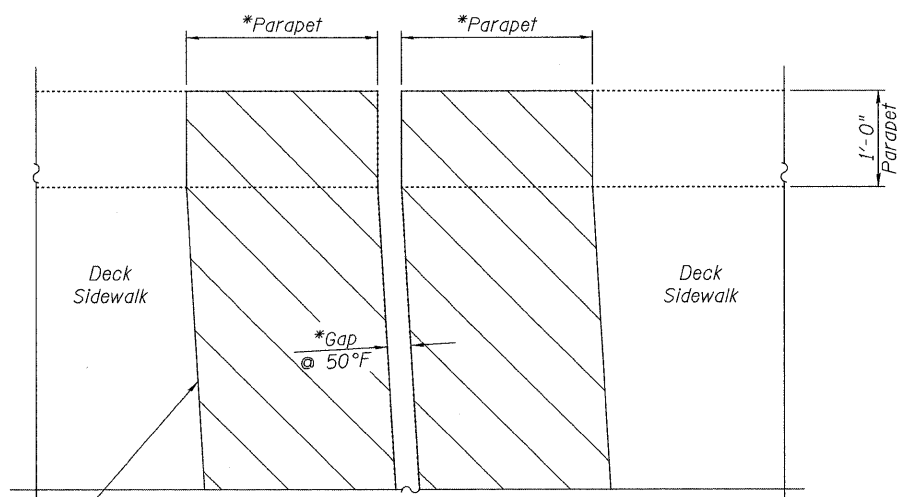
Contract # 60D74



PLAN - CONCRETE REMOVAL

(West ends of Deck Joints at Piers 3, 7, 11, 18, and 21 Shown, East ends similar except for Pier 11 which has no Sidewalk or Parapet)

3/4" maximum sawcut (Typ. on all exposed of parapet and soffit at construction joint)



PLAN - CONCRETE REMOVAL

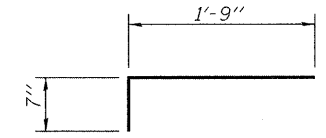
(West end of Deck Joint at Pier 15 shown, East end at Pier 15 and both ends at Pier 24 Similar)

***CONCRETE PARAPET REMOVAL DIMENSIONS**

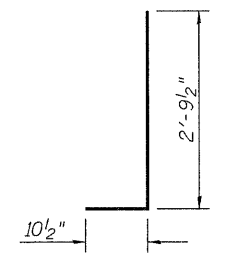
Location	Skew	Parapet	Gap
Pier 3	24° 38' 27"	2'-2 5/8"	3"
Pier 7	24° 17' 54"	2'-1 3/4"	4 1/2"
Pier 11	24° 4' 22"	2'-2 1/2"	3"
Pier 15	3° 21' 28"	2'-0 1/4"	3"
Pier 18	25°	2'-2 3/4"	3"
Pier 21	25°	2'-2 3/4"	3"
Pier 24W	0°	2'-0 1/4"	3"
Pier 24E	0°	1'-1 1/2"	4 1/2"

BILL OF MATERIAL

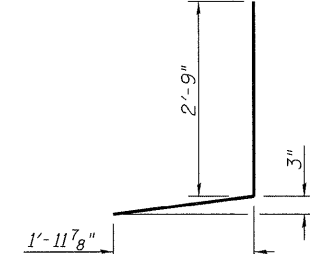
Bar	No.	Size	Length	Shape
a ₂ (E)	108	#5	20'-10"	—
a ₃ (E)	54	#5	28'-9"	—
a ₄ (E)	228	#5	22'-11"	—
a ₅ (E)	114	#5	31'-8"	—
c ₂ (E)	34	#5	7'-6"	—
c ₃ (E)	64	#5	8'-3"	—
c ₄ (E)	98	#5	2'-8"	┌
d(E)	90	#4	3'-8"	┌
d ₁ (E)	90	#4	4'-9"	┌
x(E)	878	#5	2'-4"	┌
Concrete Removal			Cu. Yd.	98.2
Reinforcement Bars, Epoxy Coated			Pound	16,920
Concrete Superstructure			Cu. Yd.	100.2
Bar Splicers			Each	336
Protective Coat			Sq. Yd.	318



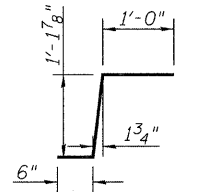
x(E) bars



d(E) bars



d₁(E) bars



c₄(E) bars

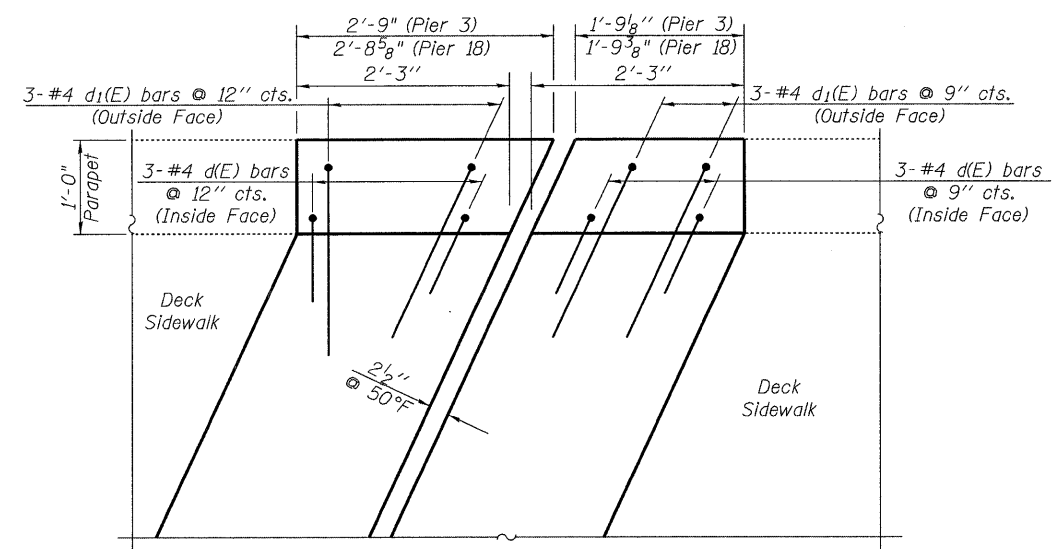
NOTES:

For simplicity, only proposed reinforcement critical to parapet replacement is shown. See Sheet 11 of 35 for details at Median and Sidewalk.

Hatched areas indicate concrete sections to be removed. Care shall be exercised by the contractor during and following Concrete Removal operations to ensure that the existing rebar remaining in place are not damaged. All existing reinforcement to be incorporated into new construction shall be blast-cleaned, straightened, and properly positioned prior to concrete placement. Any reinforcement damaged during Concrete Removal shall be repaired or replaced using an approved Bar Splicer or Mechanical System. Cost included with Concrete Removal.

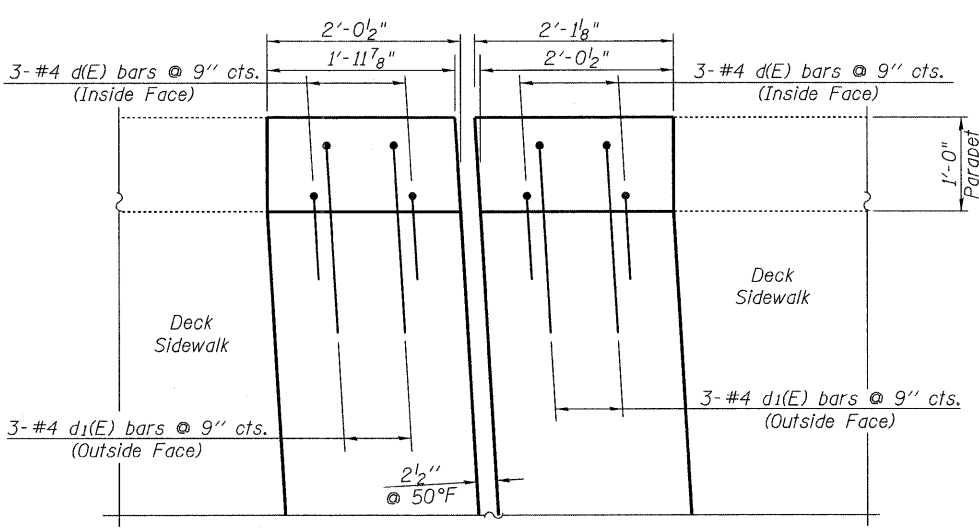
Interference is expected from the existing aluminum rail posts. The Contractor shall remove and reerect the existing bridge rail and post to complete the work as detailed. The existing aluminum rail post shall be reconnected to the reconstructed parapet utilizing the existing mounting bracket. Care shall be taken not to damage the existing bracket. The base of the rail post shall be sealed with two component non-staining gray sealing compound with polysulfide liquid polymers-gun grade with primer. All labor, equipment, and materials necessary for removing and reinstalling the bridge rail and bridge rail post shall be included in the cost for Concrete Superstructure.

Care shall be taken to avoid damaging the existing electrical conduit located directly under the edge of deck on the East side of the Expansion Joint at Pier 11. When replacing the Joint at this location, extra concrete may be needed under the deck to accommodate a Support Box for the Modular Expansion Joint, similar to Section C-C on sheet 10 of 35. Any extra concrete used shall not interfere with the existing electrical conduit.



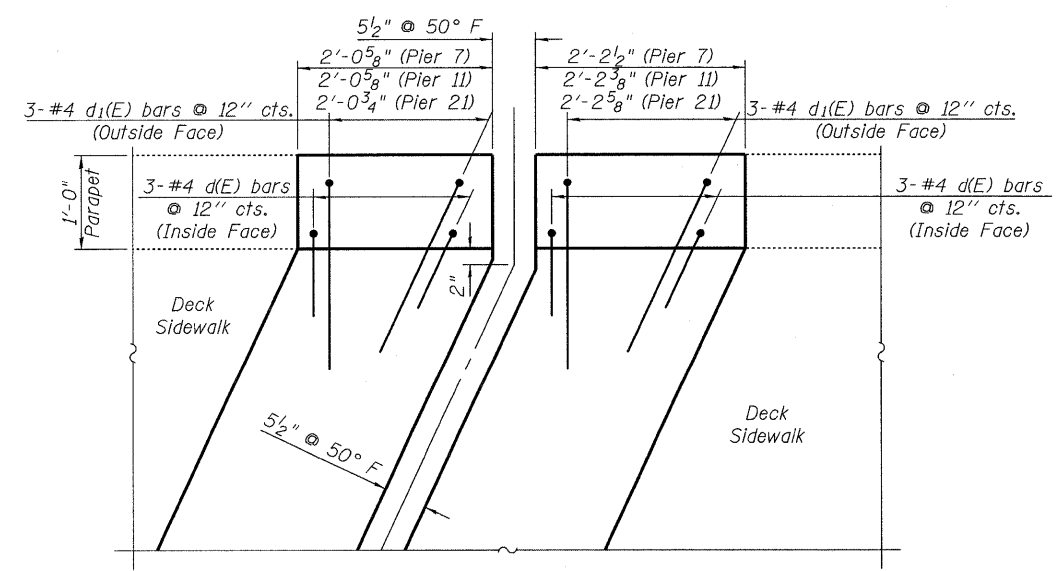
PLAN - CONCRETE REPLACEMENT

(West ends of Deck Joints at Piers 3 and 18 Shown, East ends similar)



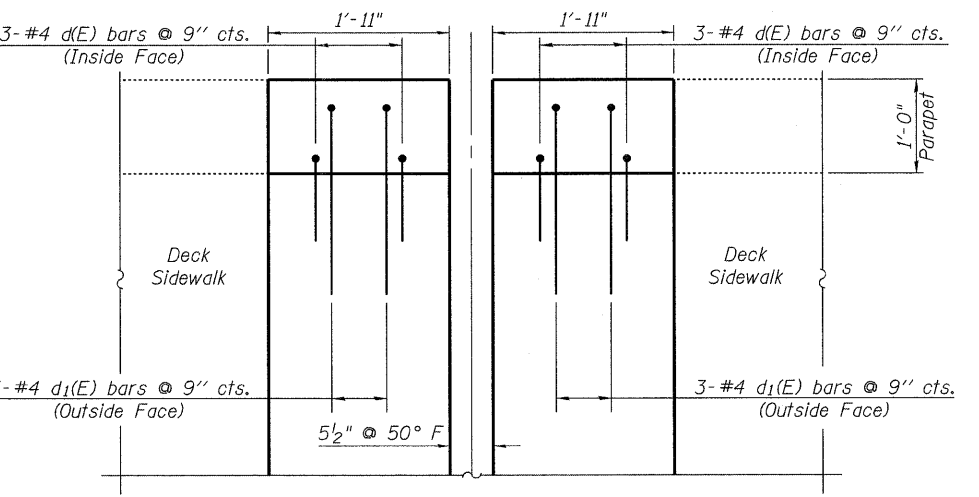
PLAN - CONCRETE REPLACEMENT

(West end of Deck Joint at Pier 15 shown, East end similar)



PLAN - CONCRETE REPLACEMENT

(West ends of Deck Joints at Piers 7, 11, and 21 Shown, East ends similar except for Pier 11 which has no Sidewalk or Parapet)



PLAN - CONCRETE REPLACEMENT

(West end of Deck Joint at Pier 24 shown, East end similar)

rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, IL 60187

ILLINOIS DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT REHABILITATION
DETAILS II
BURNHAM AVE OVER STATE ST, R.R. & RIVER
FAU RTE 2943 SECTION 1212.8-I-1
COOK COUNTY
STATION 231+79.04
STRUCTURE NO. 016-0806
DATE: 1-14-2009
DRAWN BY: WJV
CHECKED BY: BLB

K:\11225903\Structures\Burnham Ave. Over R.R. & River\Final Plans\Exp_Joints.dgn 1/13/2009