

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. 847-705-4021, SCHAUMBURG IL CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC.

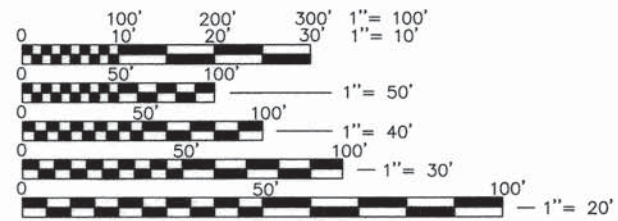
**FOR INDEX OF SHEETS AND  
HIGHWAY STANDARDS  
SEE SHEET NO. 2**

STATE OF ILLINOIS 04-22-2016 LETTING ITEM 009  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**  
MUN 0062/0062A (DENNY ROAD)  
FAP 520 (BLISS ROAD) TO EAST VILLAGE LIMITS  
RESURFACING  
SECTION: 14-00029-00-RS  
PROJECT NUMBER: M-4003(657)  
VILLAGE OF SUGAR GROVE  
KANE COUNTY  
C-91-194-16

MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	1
CONTRACT NO. 61C62				

**DESIGN DESIGNATION**

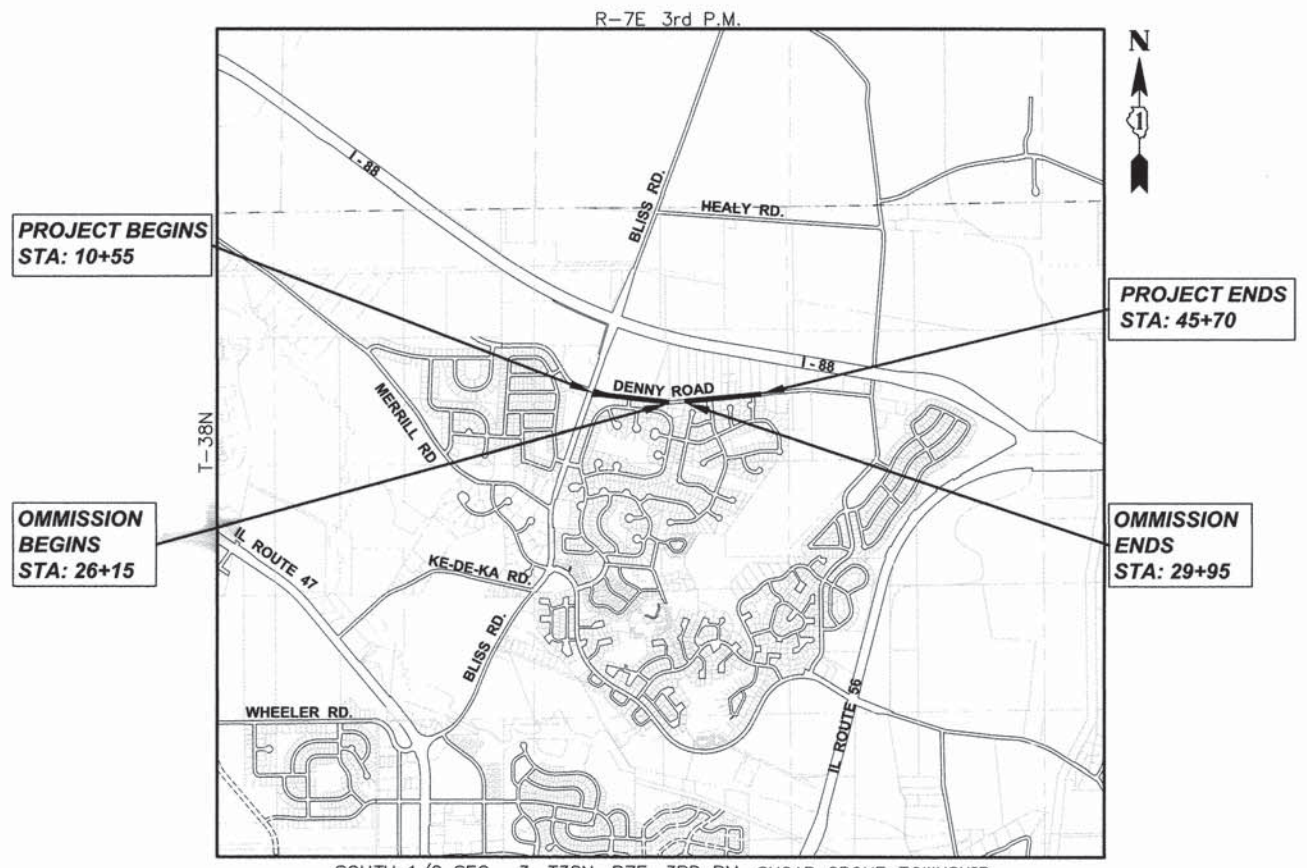
**DENNY ROAD  
MAJOR COLLECTOR  
DESIGN SPEED = 45 M.P.H.  
POSTED SPEED = 45 M.P.H.  
ADT (2014) = 1,500 VPD**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



LOCATION OF SECTION INDICATED THUS: - ■ -



SOUTH 1/2 SEC . 3, T38N, R7E, 3RD PM, SUGAR GROVE TOWNSHIP

**LOCATION MAP**  
1" = 2,000'

GROSS LENGTH OF PROJECT = 3,515 FEET (0.666 MILES)  
NET LENGTH OF PROJECT = 3,135 FEET (0.594 MILES)



**Simply Call 811**  
CONTRACT NO. 61C62



**Engineering Enterprises, Inc.**  
Consulting Engineers  
52 Wheeler Road  
Sugar Grove, Illinois 60554  
630.466.6700 / www.eeiweb.com

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

APPROVED JANUARY 25 2016  
*P. Dan Nichols*  
VILLAGE OF SUGAR GROVE, VILLAGE PRESIDENT

PASSED FEBRUARY 2, 2016 2016  
*CHP*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW February 2 2016  
*John S. Edwards*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: January 25, 2016

BY: *Timothy V. Weidner*  
TIMOTHY V. WEIDNER

LICENSE EXPIRES: NOVEMBER 30, 2017

SEAL

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Plotted: January 19, 2016 @ 3:28 PM By: Jim Schmidt - Tab: 01 Cover 22x34 Path: H:\SDSKPROJ\SG1501\DWG\FINAL ENG\SG1501-COVER

**GENERAL NOTES**

**SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS**

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED APRIL 1, 2016 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS", SEVENTH EDITION, THE CODES AND ORDINANCES OF THE VILLAGE OF SUGAR GROVE, ILLINOIS, THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

NO SUBSTITUTIONS OR VARIANCES WILL BE PERMITTED TO ANY STANDARD NOTES OR ORDINANCES UNLESS APPROVED OTHERWISE IN WRITING PRIOR TO COMMENCING CONSTRUCTION ACTIVITY.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS AND THE PLANS

**UTILITIES**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL EXISTING AND PROPOSED UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, FIELD TILES AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND NOT NECESSARILY COMPLETE; THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF GRADING AND SHAPING DITCHES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED. THIS WORK SHALL BE ARRANGED BY THE UTILITY COMPANY AND SHALL BE AT THE CONTRACTOR'S EXPENSE.

IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR OPERATE ANY VALVES OR HYDRANTS.

**STAKING**

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THE OWNER'S AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS ARE FROM THE CENTERLINE AS SHOWN ON THE PLANS.

**MISCELLANEOUS**

SAWING OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR AS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

AT ALL MAINLINE SIDE ROAD AND DRIVEWAY BUTT JOINT LOCATIONS, THE EXISTING SURFACE SHALL BE CUT TO A MINIMUM THICKNESS OF TWO (2) INCHES AS INDICATED ON THE PLANS.

THE THICKNESS OF ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE ASPHALT MIXTURES ARE TO BE PLACED.

THE CONTRACTOR SHALL BE REQUIRED TO MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS, STRUCTURES, POLES, CABLES AND PIPE LINES, BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE ENGINEER AT THEIR OWN EXPENSE. ANY SHEETING AND/OR SHORING USED FOR THIS IMPROVEMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF GRADING AND SHAPING DITCHES.

THE CONTRACTOR SHALL PROTECT ALL EXISTING FACILITIES (E.G. CURB, DRIVEWAYS, PAVEMENT) THAT ARE NOT INDICATED TO BE REMOVED ON THE PLANS. ANY FACILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE CONTRACTOR'S EXPENSE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.

EXISTING PAVEMENT THICKNESSES SHOWN ON THE PLANS ARE APPROXIMATE, BASED ON AVAILABLE INFORMATION AT THE TIME OF DESIGN. ANY ADDITIONAL COSTS REQUIRED BY THE CONTRACTOR DUE TO THICKNESSES OTHER THAN THOSE SHOWN ON THE PLANS WILL BE INCLUDED IN THE COST OF THE CONTRACT.

WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, THE CONTRACTOR SHALL FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER AND NOISE POLLUTION. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE IMPROVEMENT.

ALL DISTURBED AREAS WITHIN THE PROJECT THAT ARE NOT OTHERWISE SURFACED SHALL BE CLEANED, LAYERED WITH TOPSOIL, AND SODDED AS SHOWN IN THE PLANS. LIMITS SHOWN ON THE PLANS ARE THE MAXIMUM PAY WIDTHS FOR PAYMENT PURPOSES. ADDITIONAL AREAS DAMAGED BY MACHINERY, CONSTRUCTION EQUIPMENT, CONTRACTOR NEGLIGENCE OR OVER-EXCAVATION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE COST OF THE CONTRACTOR.

THE CONTRACTOR SHALL DISPOSE OF AND REMOVE FROM THE SITE EACH DAY ALL CURB AND GUTTER, PAVEMENT AND ALL OTHER EXCAVATED MATERIAL NOT FOR SALVAGE. THE COST FOR HAULING AND TRUCKING TO DISPOSAL LOCATIONS WILL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

THE ENGINEER IS NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FRESH CONCRETE FROM DAMAGE AND VANDALISM. ANY DAMAGED OR VANDALIZED CONCRETE SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

BACKFILL AREAS ADJACENT TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE BACKFILLED WITH CLASS SI CONCRETE AND HAVE AN HMA OVERLAY AS SHOWN IN THE SPECIAL DETAIL. THE CLASS SI CONCRETE WILL BE CONSIDERED INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

PATCHING LOCATIONS SHALL BE MARKED IN THE FIELD BY THE ENGINEER.

TOPSOIL, SEEDING, FERTILIZER AND EROSION CONTROL BLANKET SHALL BE PLACED ADJACENT TO AGGREGATE SHOULDER AS DIRECTED BY THE ENGINEER. THE ENGINEER SHALL REVIEW THE SITE CONDITIONS WITH THE CONTRACTOR AFTER SHOULDER CONSTRUCTION.

ANY EARTH EXCAVATION REQUIRED TO CONSTRUCT NEW SIDEWALK OR ANY SPOILS FROM GRADING AND SHAPING DITCHES SHALL BE DISPOSED OF ON SITE ADJACENT TO SHOULDERS AND AS DIRECTED BY THE ENGINEER.

DENNY ROAD SHALL BE OPEN TO TRAFFIC AT ALL TIMES. WHEN IT IS NECESSARY TO CLOSE ONE LANE OF TRAFFIC DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING CONSTRUCTION HOURS WITH THE USE OF TRAFFIC CONTROL DEVICES, SIGNS AND FLAGGERS AS APPLICABLE IN THE TRAFFIC CONTROL STANDARDS.

ANY AREAS ON DRIVEWAYS OUTSIDE THE LIMITS OF MAINLINE PAVING SHALL BE RESURFACED WITH INCIDENTAL HOT-MIX ASPHALT SURFACING PAY ITEM AS INDICATED ON THE PLANS.

THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ANY RESIDENT OR BUSINESS OF ANY REMOVAL AND REPLACEMENT ACTIVITIES THAT WILL INHIBIT OR PROHIBIT ACCESS TO THEIR DRIVEWAY, IN WRITING, A MINIMUM OF 48 HOURS BUT NOT MORE THAN 72 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORNING OF THE WORK, THE CONTRACTOR SHALL AGAIN NOTIFY THE OWNER VERBALLY, TO ALLOW THE OWNER TIME TO MOVE THEIR VEHICLE SO AS NOT TO PROHIBIT THE VEHICLE FROM LEAVING THE DRIVEWAY UPON REMOVAL OF ANY MATERIAL. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHALL PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BY RESTORED. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW ALL WORK TO BE DONE IN A TIMELY MANNER SO AS TO PERMIT ACCESS TO THE ROADWAY. ANY ADDITIONAL COST OF STAGING REQUIRED TO MAINTAIN ACCESS IS CONSIDERED INCLUDED IN THE COST OF INCIDENTAL HOT-MIX ASPHALT SURFACING.

**INDEX OF SHEETS**

SHEET NO.	SHEET DESCRIPTION
1.	COVER SHEET AND LOCATION MAP
2.	GENERAL NOTES, HIGHWAY STANDARDS, SUPPLEMENTAL LEGEND, AND INDEX OF SHEETS
3.	SUMMARY OF QUANTITIES
4.-5.	TYPICAL SECTIONS
6.-9.	GENERAL PLAN
10.	DETAILS
<b>DISTRICT ONE DETAILS</b>	
11.	(TC-10) TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
12.	(TC-11) TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
13.	(TC-13) DISTRICT ONE - TYPICAL PAVEMENT MARKINGS
14.	(TC-16) PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
15.	(BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
16.	(BD-24) CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
17.	(BD-32) BUTT JOINT AND HMA TAPER DETAILS

**HIGHWAY STANDARDS**

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-08	PERPENDICULAR CURB RAMP FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-ROAD OPERATIONS 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701336-06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS ≥ 45 MPH
701801-06	SIDEALK, CORNER OR CROSSWALK CLOSURE
701901-05	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

**SUPPLEMENTAL LEGEND**

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

-  HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
-  HOT-MIX ASPHALT SURFACE REMOVAL - 1" AND VARIABLE DEPTH
-  INCIDENTAL HOT-MIX ASPHALT SURFACING

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**DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, HIGHWAY STANDARDS,**  
**SUPPLEMENTAL LEGEND AND INDEX OF SHEETS**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	2
				CONTRACT NO. 61C62
FED. ROAD DIST. NO. 1 ILLINOIS				

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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY ROADWAY 75% FEDERAL 25% LOCAL 0005
20200100	EARTH EXCAVATION	CU YD	13
* 21400100	GRADING AND SHAPING DITCHES	FOOT	115
25000210	SEEDING, CLASS 2A	ACRE	0.6
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	54
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	54
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	54
25100630	EROSION CONTROL BLANKET	SQ YD	2,880
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	57
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	6,750
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	2
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	561
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	239
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,122
40800029	BITUMINOUS MATERIALS (TACK COAT)	POUND	145
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	40
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	500
42400800	DETECTABLE WARNINGS	SQ FT	40
44000153	HOT-MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	9,165
44000600	SIDEWALK REMOVAL	SQ FT	320
44201690	CLASS D PATCHES, TYPE I, 4 INCH	SQ YD	10
44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	30
44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	60
44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	900
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	280
67100100	MOBILIZATION	LSUM	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	LSUM	1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	LSUM	1
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	LSUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2
70300100	SHORT TERM PAVEMENT MARKING	FOOT	930
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	130
Δ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	64
Δ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11,942
Δ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	322
Δ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	42
Δ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	69
Δ 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	29
Δ 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	29
* X2110100	TOPSOIL FURNISH AND PLACE, SPECIAL	CU YD	320
* X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	835
* Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	436

Δ INDICATES SPECIALTY ITEM  
\* SEE SPECIAL PROVISIONS

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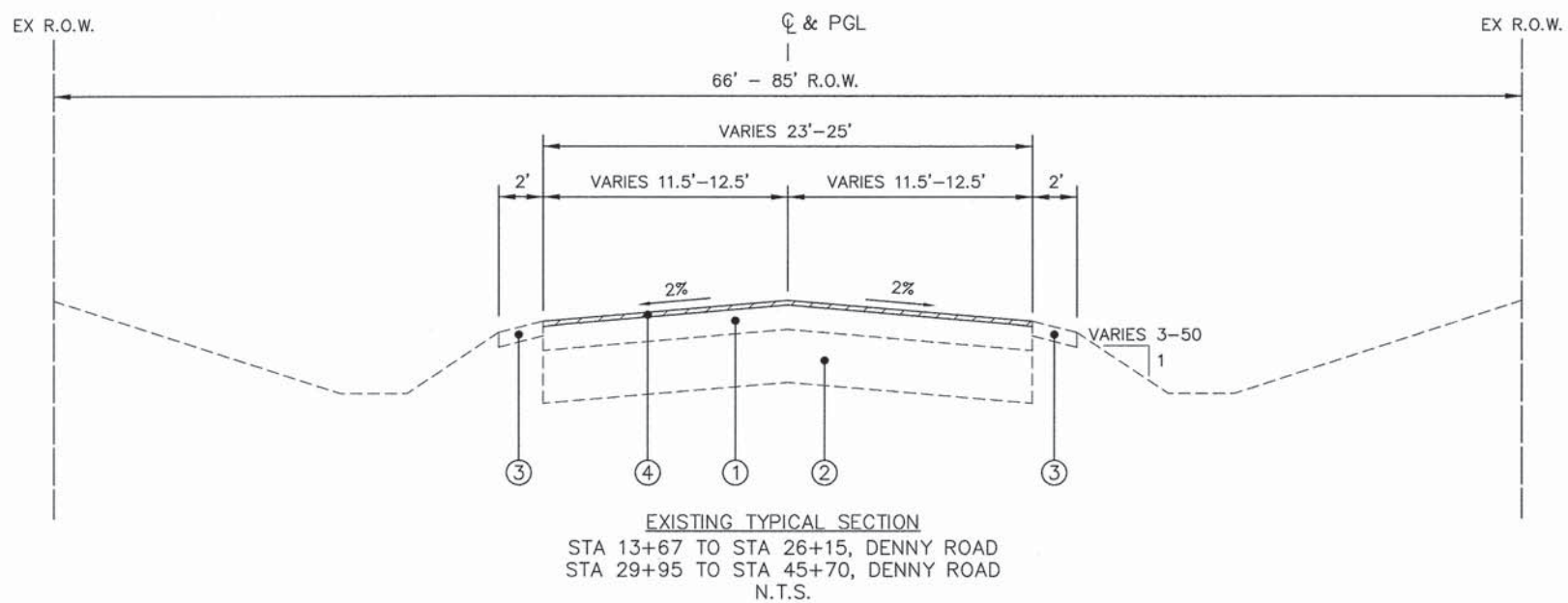
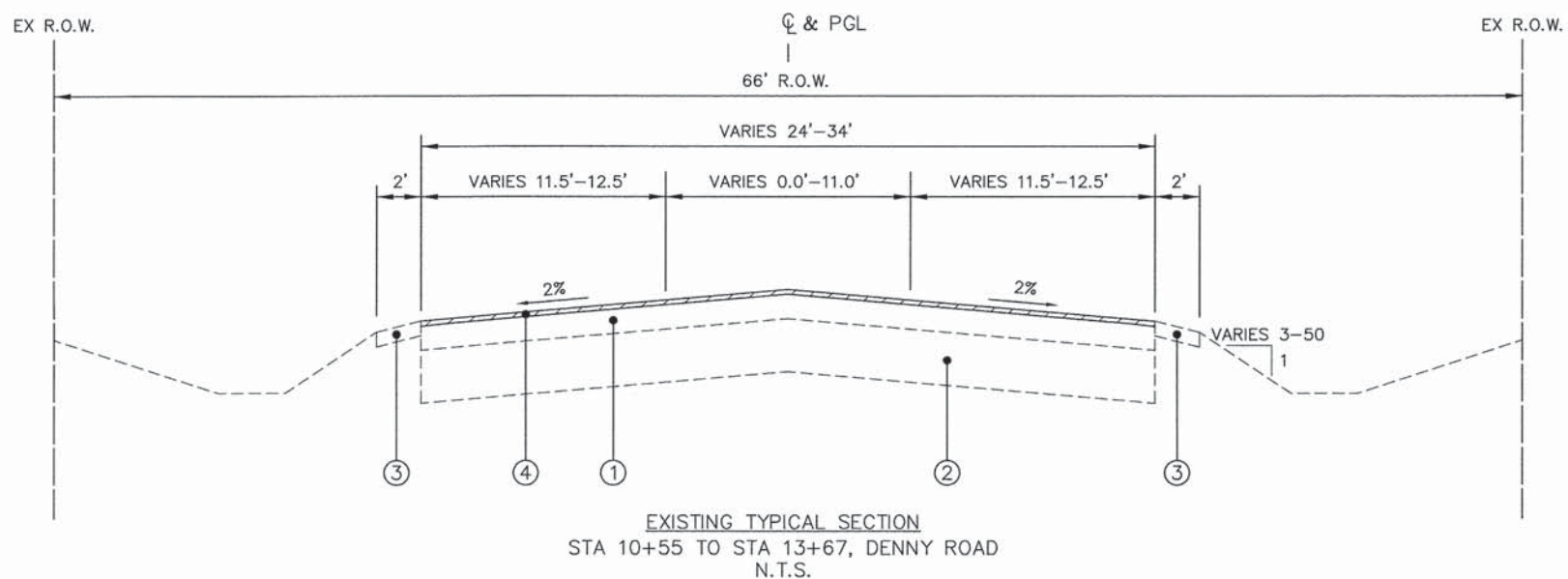
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	3
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61C62	

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LEGEND	
①	EXISTING 3" - 9" ASPHALT PAVEMENT
②	EXISTING 10" - 21" AGGREGATE BASE
③	EXISTING 2' AGGREGATE SHOULDER
④	HOT-MIX ASPHALT SURFACE REMOVAL, 1.0"
⑤	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
⑥	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0"
⑦	BITUMINOUS MATERIALS (TACK COAT)
⑧	AGGREGATE WEDGE SHOULDER, TYPE B
⑨	TOPSOIL, SEEDING CLASS 2A, FERTILIZER AND EROSION CONTROL BLANKET (LOCATIONS AS DIRECTED BY THE ENGINEER)

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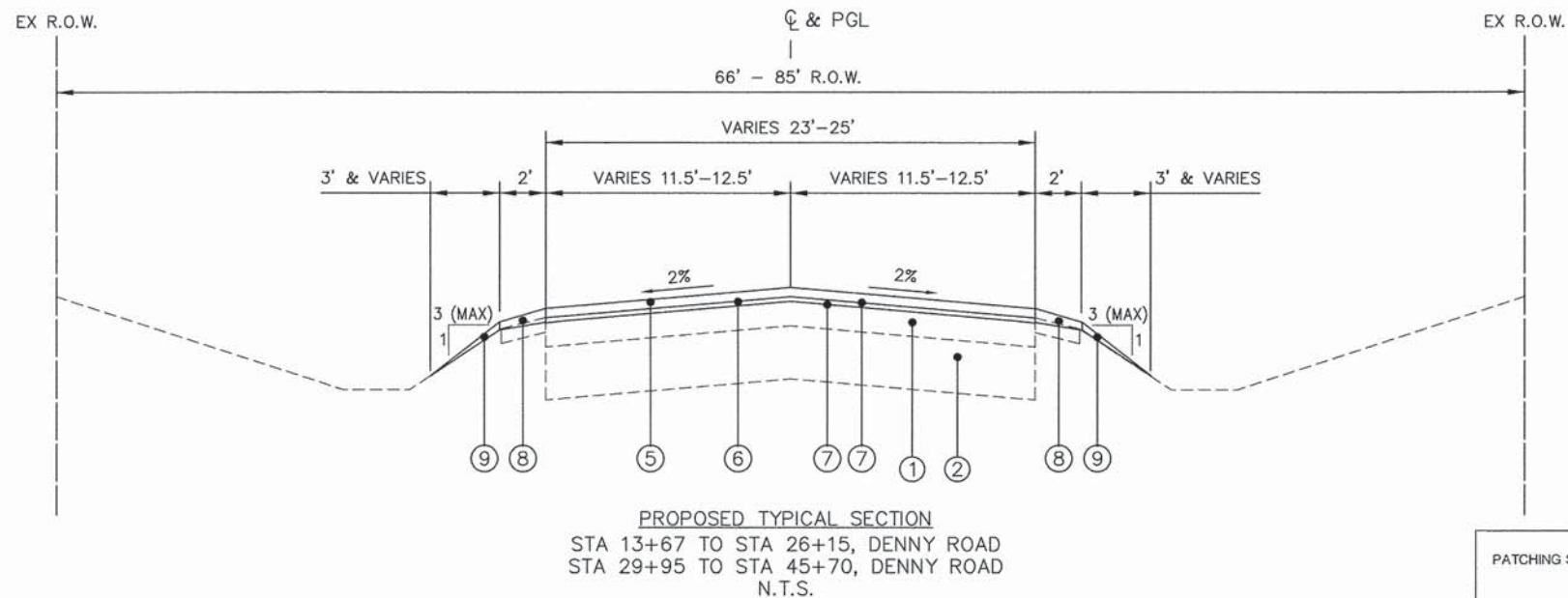
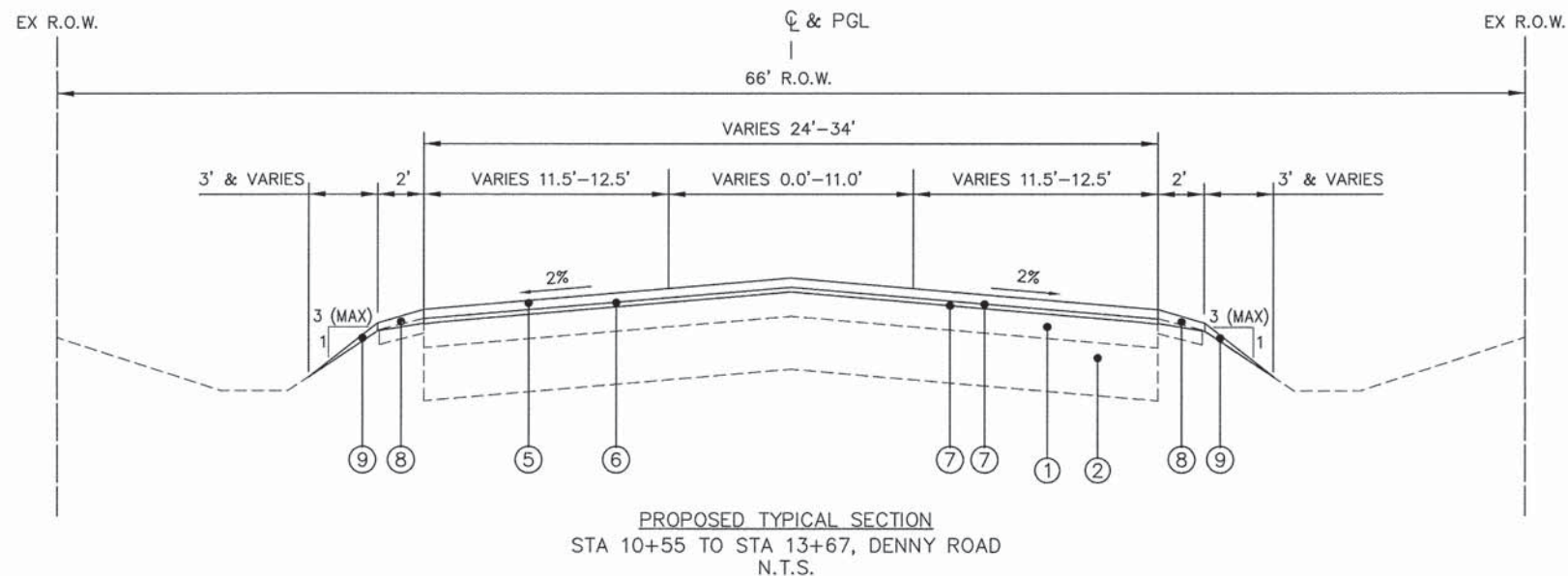
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS	
SCALE: N.T.S.	SHEET NO. 1 OF 2 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	4
CONTRACT NO. 61C62			FED. ROAD DIST. NO. 1 ILLINOIS	

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LEGEND	
①	EXISTING 3" - 9" ASPHALT PAVEMENT
②	EXISTING 10" - 21" AGGREGATE BASE
③	EXISTING 2' AGGREGATE SHOULDER
④	HOT-MIX ASPHALT SURFACE REMOVAL, 1.0"
⑤	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
⑥	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0"
⑦	BITUMINOUS MATERIALS (TACK COAT)
⑧	AGGREGATE WEDGE SHOULDER, TYPE B
⑨	TOPSOIL, SEEDING CLASS 2A, FERTILIZER AND EROSION CONTROL BLANKET (LOCATIONS AS DIRECTED BY THE ENGINEER)

PATCHING SHALL BE PERFORMED AFTER MILLING

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

OPERATION	MIXTURE TYPE	AIR VOIDS @ N <sub>des</sub>
DENNY ROAD RESURFACING	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 2"	3.5% @ 50 Gyr. 4% @ 50 Gyr.
DRIVEWAYS	INCIDENTAL HOT-MIX ASPHALT SURFACING HMA SURFACE COURSE, IL-9.5mm, MIX D, N50, 2"	4% @ 50 Gyr.
PATCHING	CLASS D PATCHES, 4 INCH HMA BINDER COURSE, IL-19.0, N70, 4" (IN 1 LIFT)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/INCH.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

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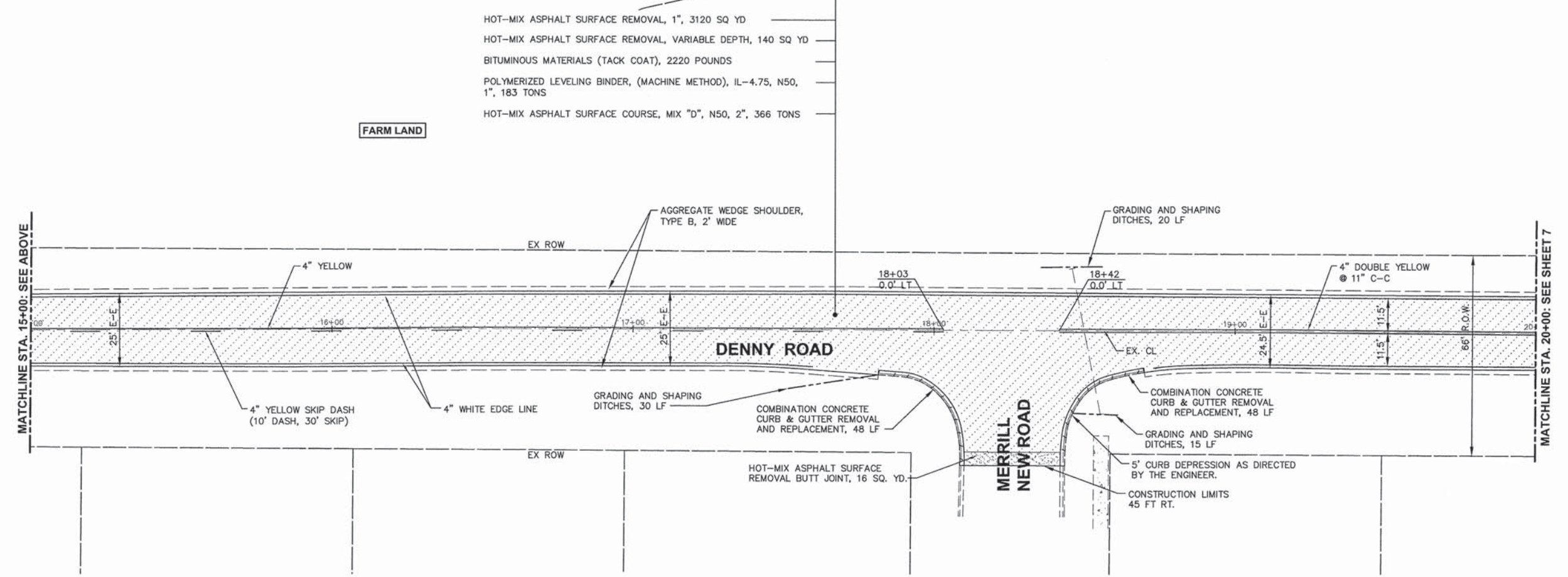
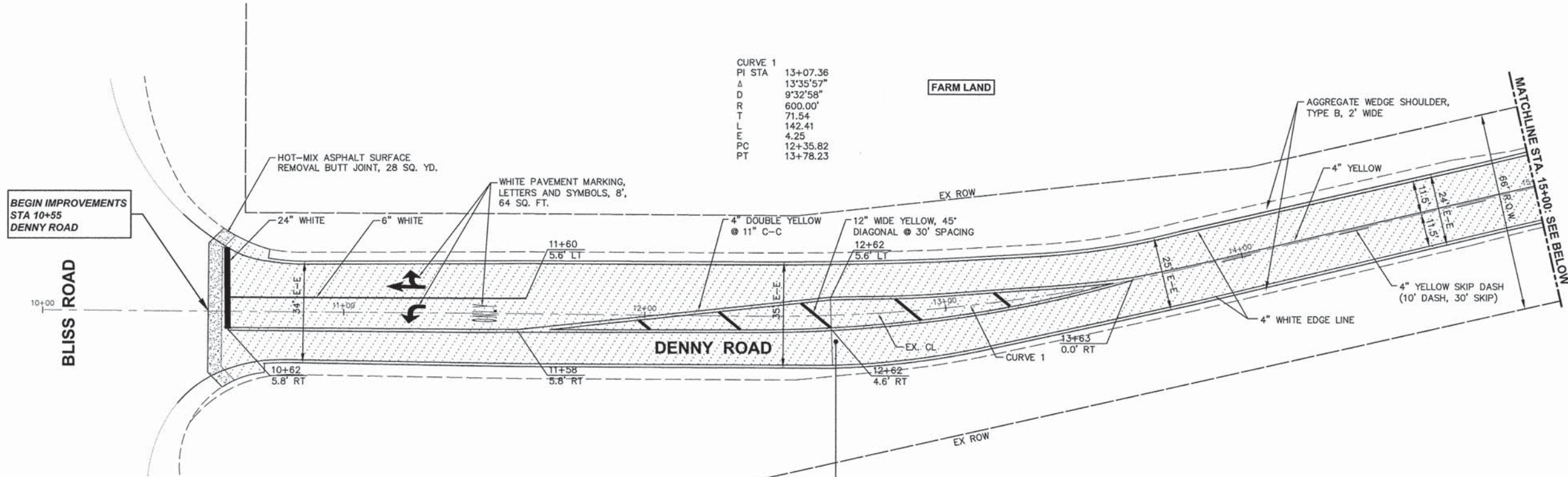
<b>Engineering Enterprises, Inc.</b> CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com	USER NAME =	DESIGNED -	REVISED -
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	PLOT DATE =	DATE -	REVISED -

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TYPICAL SECTIONS	
MUN. RTE. 0062	SECTION 14-00029-00-RS
SCALE: N.T.S.	SHEET NO. 2 OF 2 SHEETS
STA. _____	TO STA. _____

COUNTY	TOTAL SHEETS	SHEET NO.
KANE	17	5
CONTRACT NO. 61C62		
FED. ROAD DIST. NO. 1 ILLINOIS		

Path: H:\SOS\KPROJ\SG1501\DWG\FINAL ENG\SG1501-COVER



- HOT-MIX ASPHALT SURFACE REMOVAL, 1", 3120 SQ YD
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH, 140 SQ YD
- BITUMINOUS MATERIALS (TACK COAT), 2220 POUNDS
- POLYMERIZED LEVELING BINDER, (MACHINE METHOD), IL-4.75, N50, 1", 183 TONS
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 366 TONS

- NOTES:
1. ALL CLASS D PATCHES ARE 4" IN DEPTH (TYP.) AND SHALL BE MARKED IN THE FIELD BY THE ENGINEER.
  2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.
  3. EXISTING RAISED REFLECTIVE PAVEMENT MARKERS ARE LOCATED FROM STA. 10+55 TO STA. 13+63 AND ARE TO BE REMOVED. RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED FROM STA. 10+55 TO STA. 13+63 ACCORDING TO TC-11.

Plot: January 22, 2016 @ 8:04 AM By: Jim Schmidt - Job: 06 Plan: 1 22-34  
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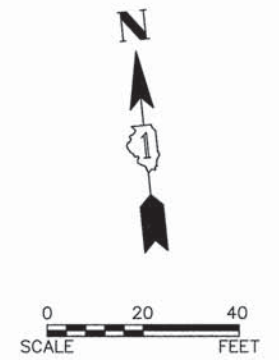
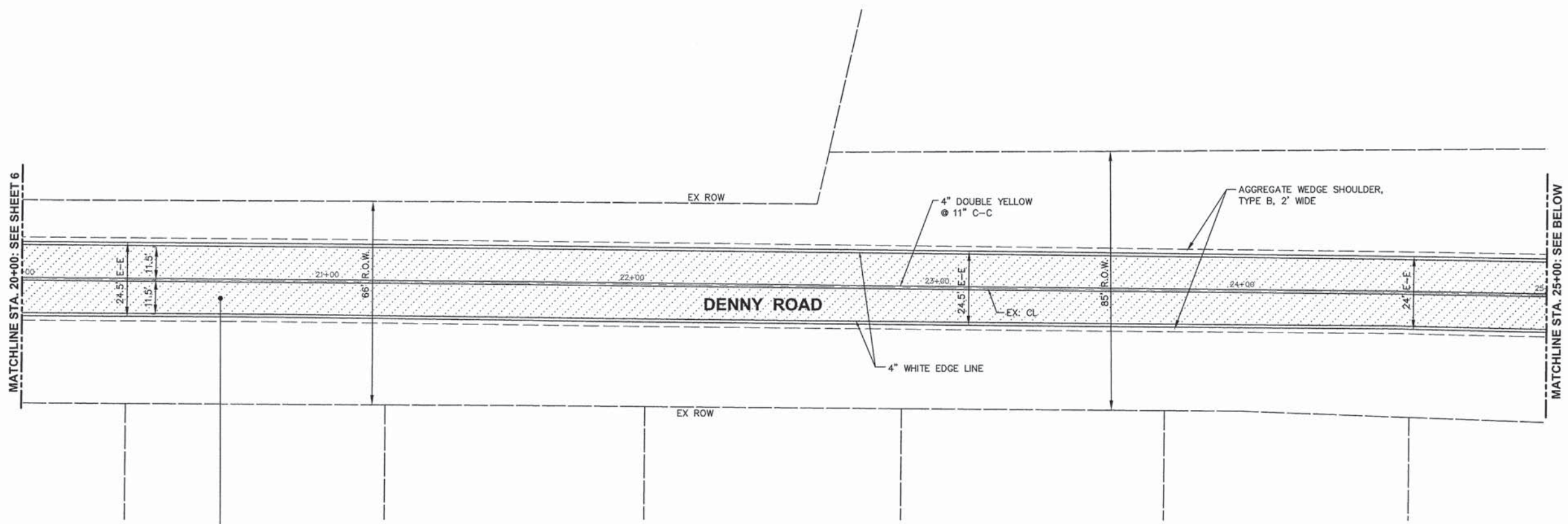
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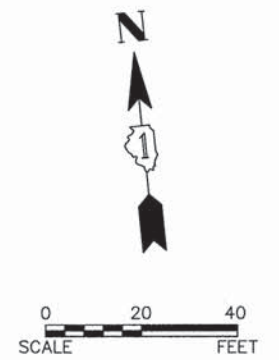
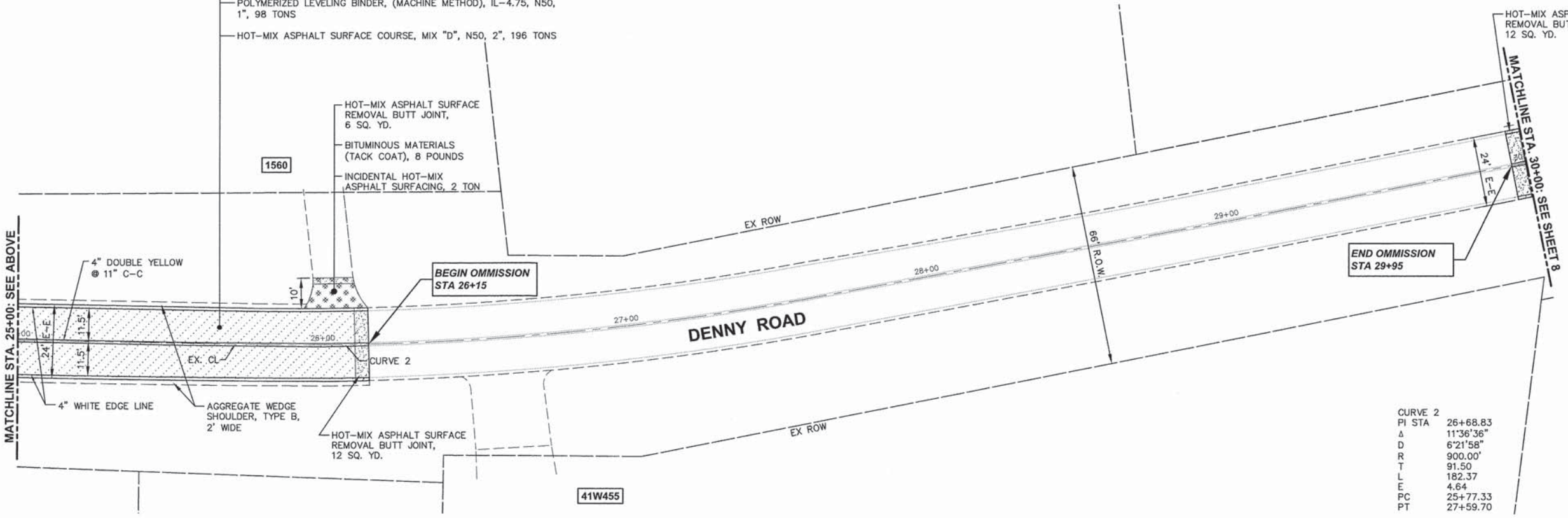
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>DENNY ROAD</b> <b>STA 10+00 TO STA 20+00</b>	
SCALE: 1"=20'	SHEET NO. 1 OF 4 SHEETS STA. 10+00 TO STA. 20+00

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-R5	KANE	17	6
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61C62	



- HOT-MIX ASPHALT SURFACE REMOVAL, 1", 1,750 SQ YD
- BITUMINOUS MATERIALS (TACK COAT), 1,181 POUNDS
- POLYMERIZED LEVELING BINDER, (MACHINE METHOD), IL-4.75, N50, 1", 98 TONS
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 196 TONS



- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT, 6 SQ. YD.
- BITUMINOUS MATERIALS (TACK COAT), 8 POUNDS
- INCIDENTAL HOT-MIX ASPHALT SURFACING, 2 TON

CURVE 2

PI STA	26+68.83
Δ	11°36'36"
D	6'21'58"
R	900.00'
T	91.50'
L	182.37'
E	4.64'
PC	25+77.33'
PT	27+59.70'

- NOTES:
1. ALL CLASS D PATCHES ARE 4" IN DEPTH (TYP.) AND SHALL BE MARKED IN THE FIELD BY THE ENGINEER.
  2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

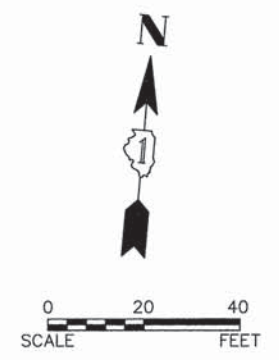
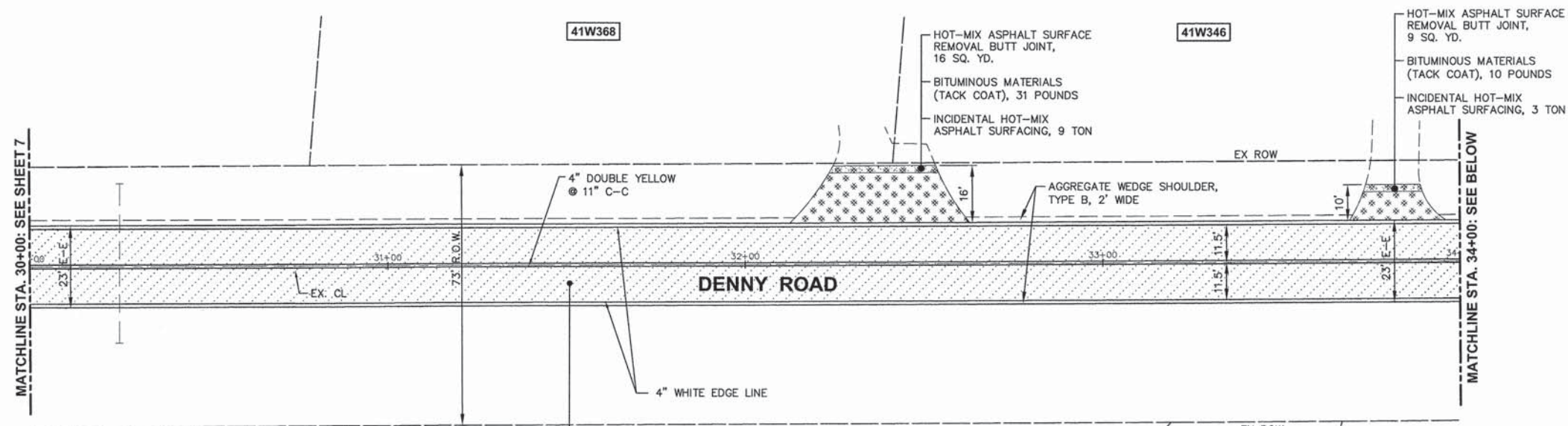
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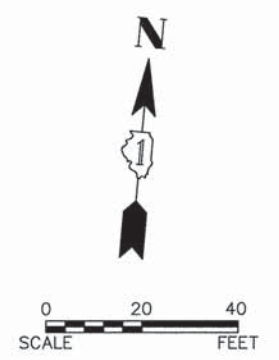
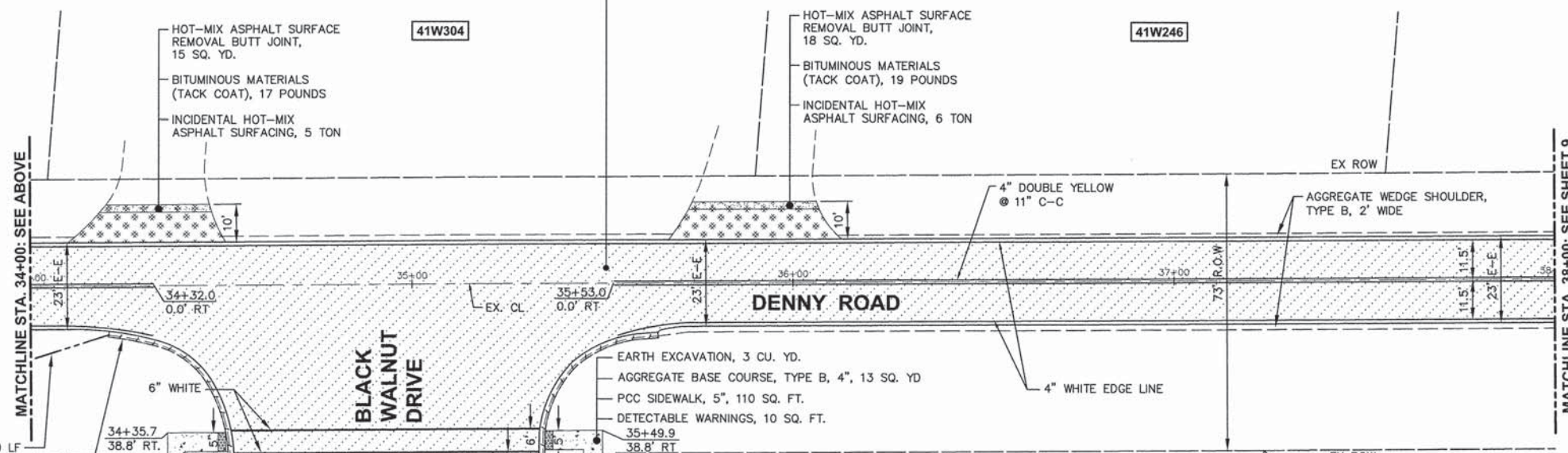
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PLOT DATE =	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>DENNY ROAD</b> <b>STA 20+00 TO STA 30+00</b>		MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: 1"=20'	SHEET NO. 2 OF 4 SHEETS	0062	14-00029-00-RS	KANE	17	7
STA. 20+00	TO STA. 30+00	FED. ROAD DIST. NO. 1 ILLINOIS				CONTRACT NO. 61C62



-HOT-MIX ASPHALT SURFACE REMOVAL, 1", 2,160 SQ YD  
 -HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH, 520 SQ YD  
 -BITUMINOUS MATERIALS (TACK COAT), 1,809 POUNDS  
 -POLYMERIZED LEVELING BINDER, (MACHINE METHOD), IL-4.75, N50, 1", 150 TONS  
 -HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 300 TONS



GRADING AND SHAPING DITCHES, 20 LF  
 COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT, 76 LF  
 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT, 15 SQ. YD.  
 BITUMINOUS MATERIALS (TACK COAT), 17 POUNDS  
 INCIDENTAL HOT-MIX ASPHALT SURFACING, 5 TON  
 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT, 18 SQ. YD.  
 BITUMINOUS MATERIALS (TACK COAT), 19 POUNDS  
 INCIDENTAL HOT-MIX ASPHALT SURFACING, 6 TON  
 6" WHITE  
 34+35.7 38.8' RT.  
 35+04.0 49.3' RT.  
 35+49.9 38.8' RT.  
 EARTH EXCAVATION, 3 CU. YD.  
 AGGREGATE BASE COURSE, TYPE B, 4", 13 SQ. YD.  
 PCC SIDEWALK, 5", 110 SQ. FT.  
 DETECTABLE WARNINGS, 10 SQ. FT.  
 4" WHITE EDGE LINE  
 AGGREGATE WEDGE SHOULDER, TYPE B, 2' WIDE  
 SIDEWALK REMOVAL, 80 SQ. FT.  
 24" WHITE  
 COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT, 76 LF  
 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT, 23 SQ. YD.  
 CONSTRUCTION LIMITS 70 FT RT.  
 SIDEWALK REMOVAL, 80 SQ. FT.  
 EARTH EXCAVATION, 2.5 CU. YD.  
 AGGREGATE BASE COURSE, TYPE B, 4", 11 SQ. YD.  
 PCC SIDEWALK, 5", 100 SQ. FT.  
 DETECTABLE WARNINGS, 10 SQ. FT.  
 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT, 23 SQ. YD.  
 CONSTRUCTION LIMITS 70 FT RT.  
 COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT, 66 LF

- NOTES:**
- ALL CLASS D PATCHES ARE 4" IN DEPTH (TYP.) AND SHALL BE MARKED IN THE FIELD BY THE ENGINEER.
  - ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

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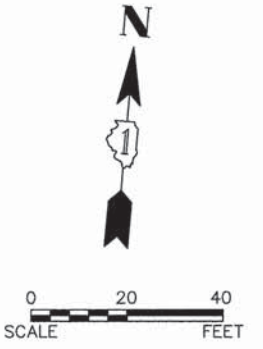
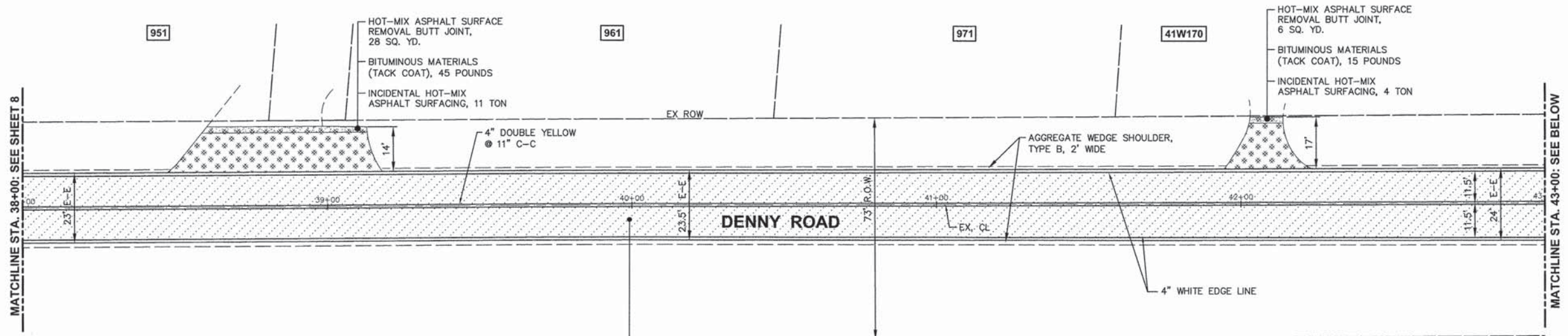
**DENNY ROAD**  
**STA 30+00 TO STA 38+00**  
 SCALE: 1"=20'  
 SHEET NO. 3 OF 4 SHEETS  
 STA. 30+00 TO STA. 38+00

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	8
CONTRACT NO. 61C62				
FED. ROAD DIST. NO. 1 ILLINOIS				

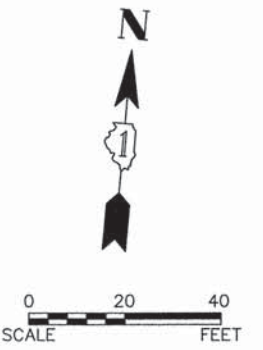
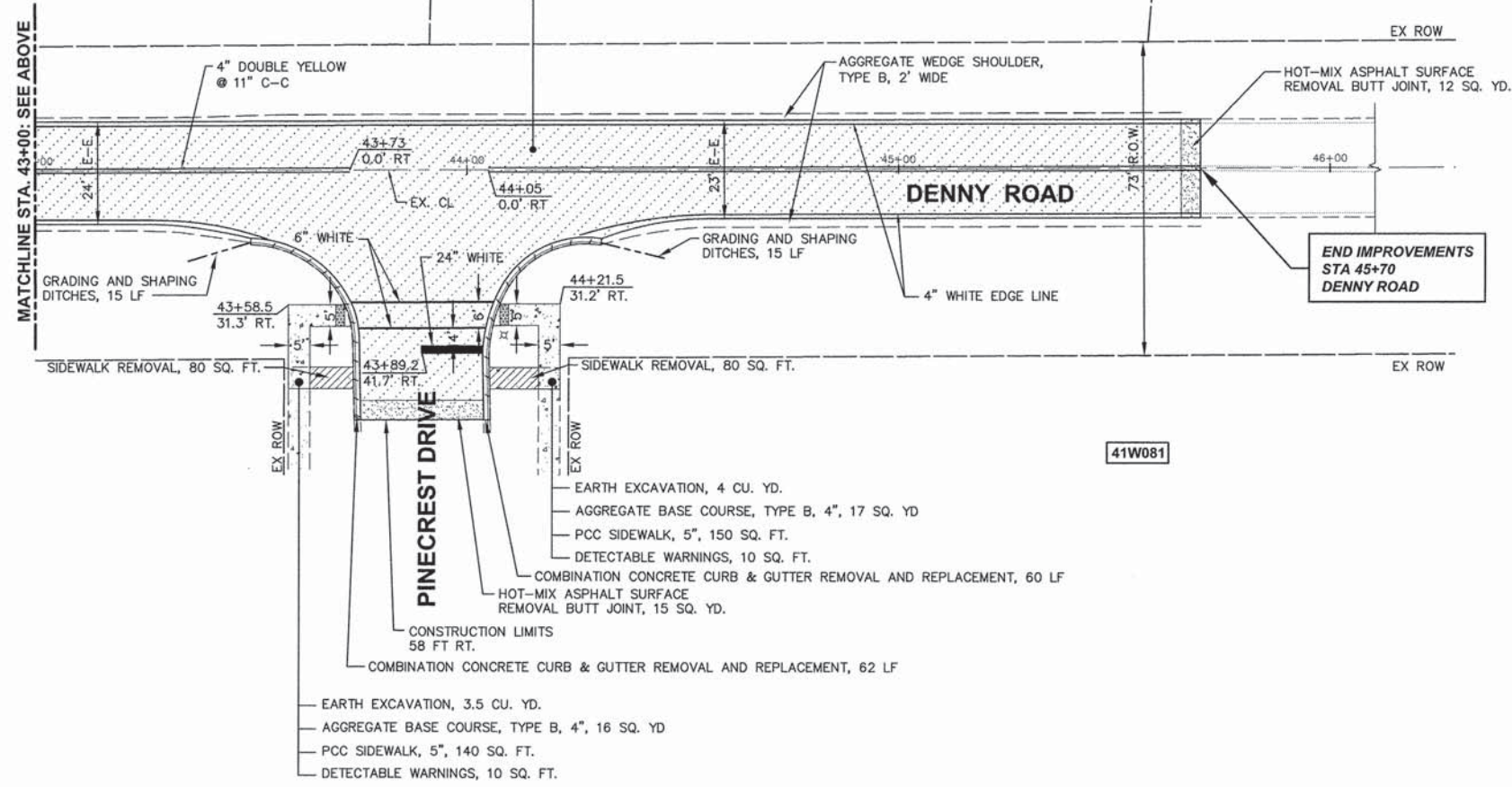
Plotted: January 22, 2016 @ 8:18 AM By: Jim Schmidt - Tab: 08 Plan: 3 22x34

FINAL ENG:SG1501-PLAN





- HOT-MIX ASPHALT SURFACE REMOVAL, 1", 2,135 SQ YD
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH, 175 SQ YD
- BITUMINOUS MATERIALS (TACK COAT), 1,560 POUNDS
- POLYMERIZED LEVELING BINDER, (MACHINE METHOD), IL-4.75, N50, 1", 130 TONS
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 260 TONS



- NOTES:**
1. ALL CLASS D PATCHES ARE 4" IN DEPTH (TYP.) AND SHALL BE MARKED IN THE FIELD BY THE ENGINEER.
  2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.

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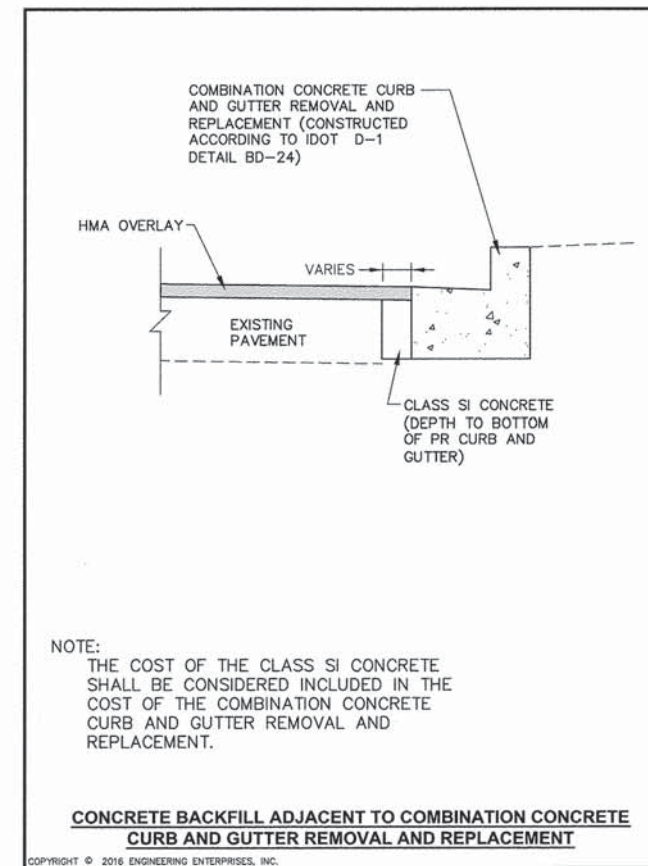
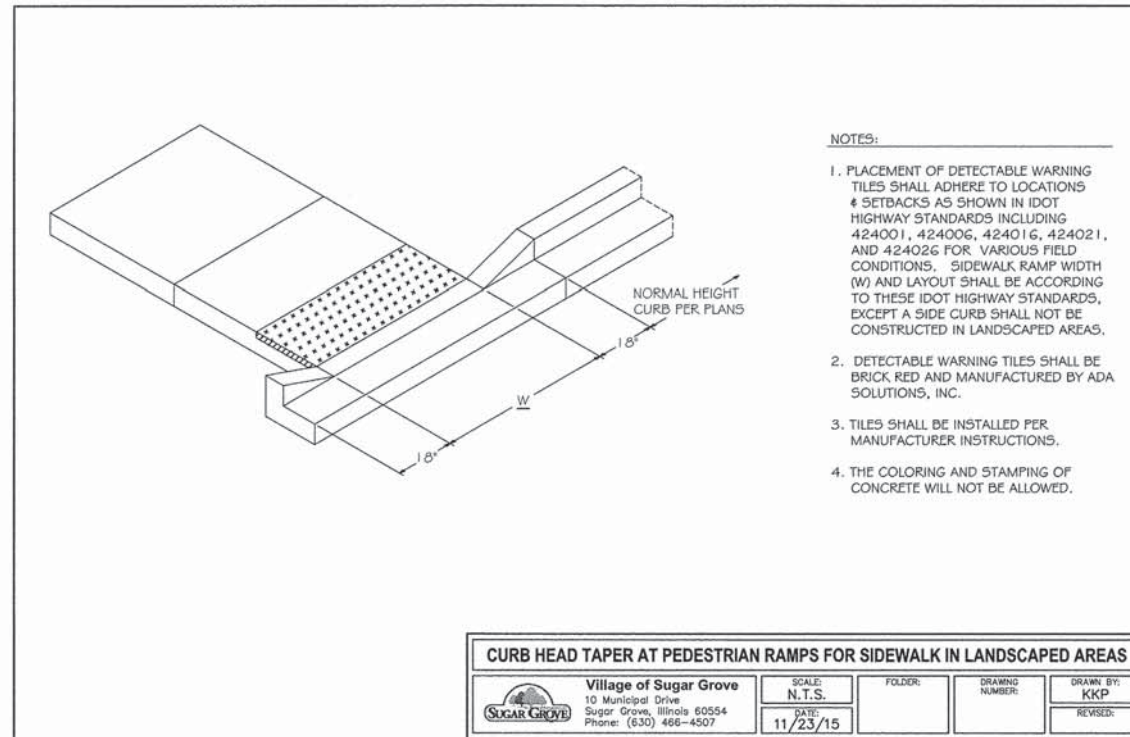
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	DATE -	REVISED -

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**DEPARTMENT OF TRANSPORTATION**

<b>DENNY ROAD</b> <b>STA 38+00 TO STA 47+00</b>	
SCALE: 1"=20'	SHEET NO. 4 OF 4 SHEETS STA. 38+00 TO STA. 47+00

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	9
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61C62	



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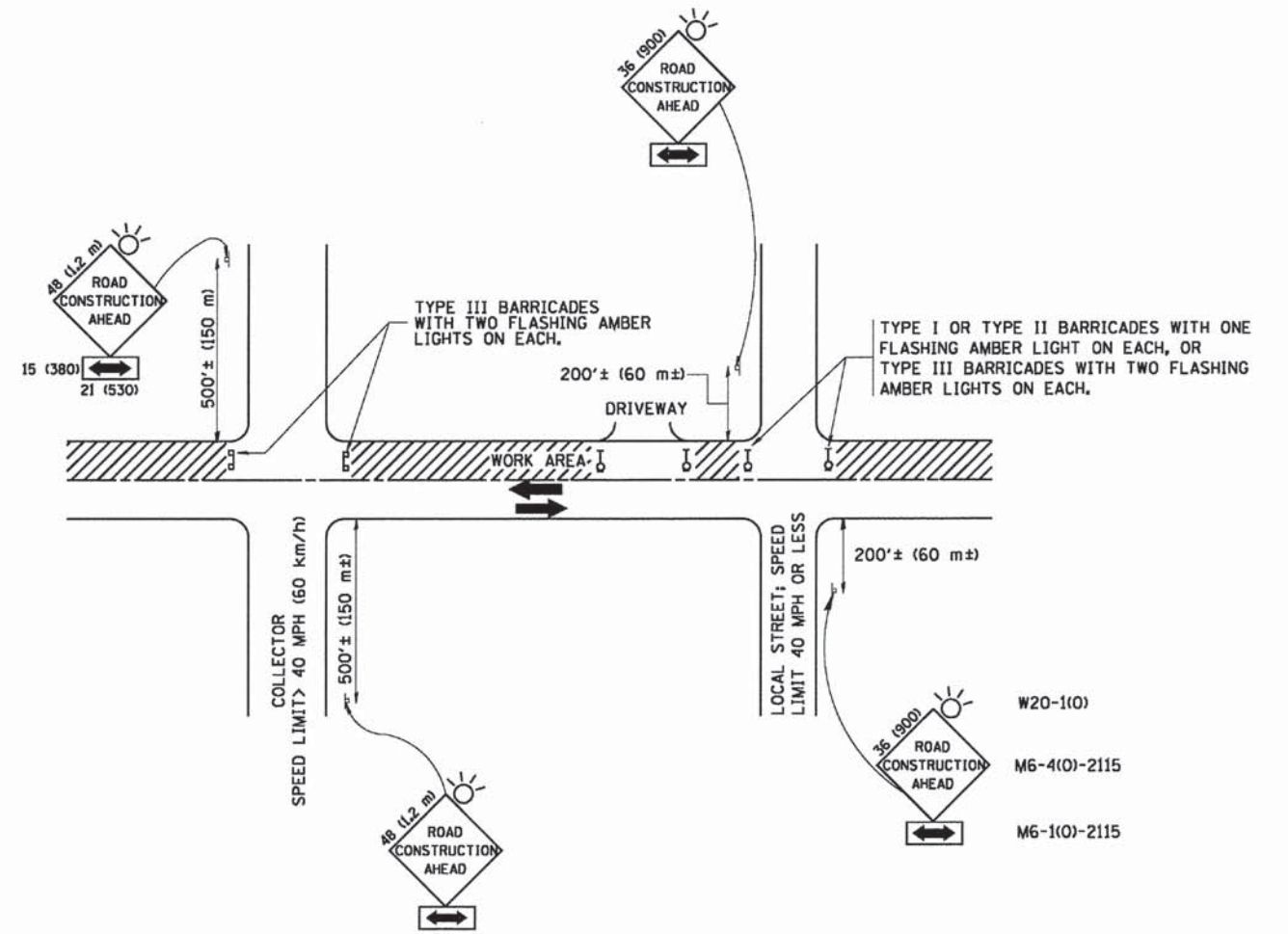
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	10
CONTRACT NO. 61C62				
FED. ROAD DIST. NO. 1 ILLINOIS				

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TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

Plotfile: January 19, 2016 @ 2:38 PM By: Jim Schmidt - Tab: 11 TC-10 22x34

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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACH 01-06-00

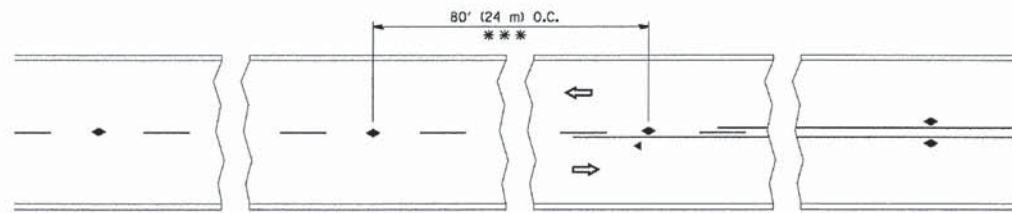
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

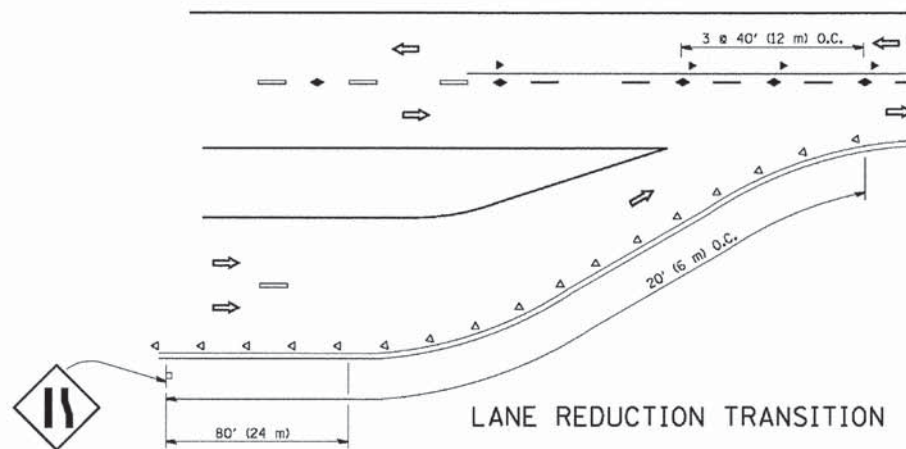
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TC-10			CONTRACT NO. 61C62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

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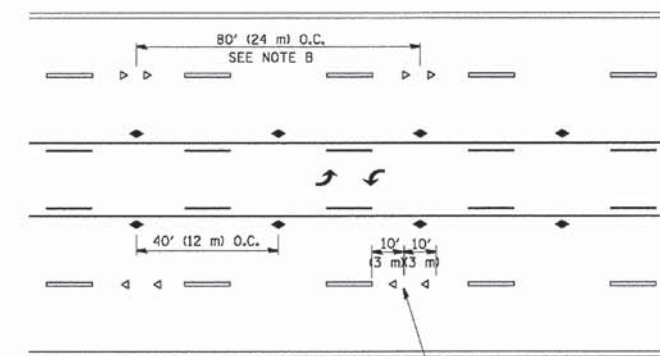


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

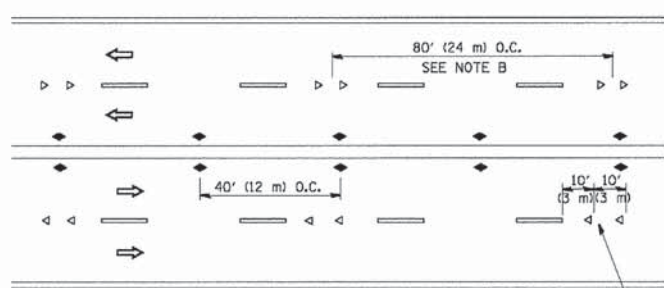
TWO-LANE/TWO-WAY



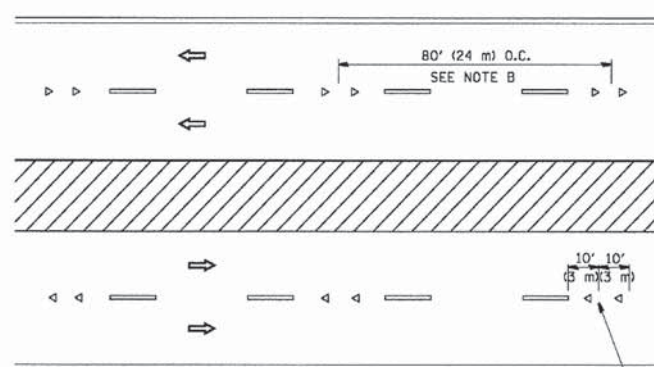
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

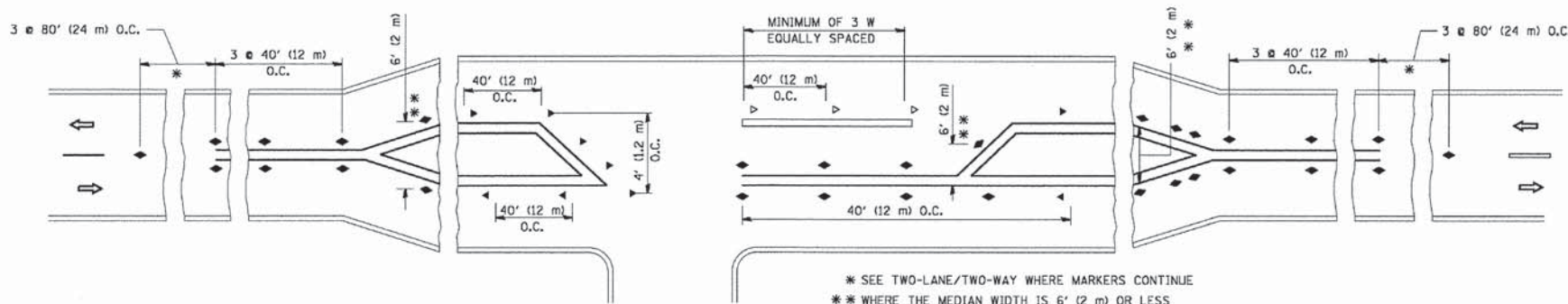
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in Inches (millimeters) unless otherwise shown.

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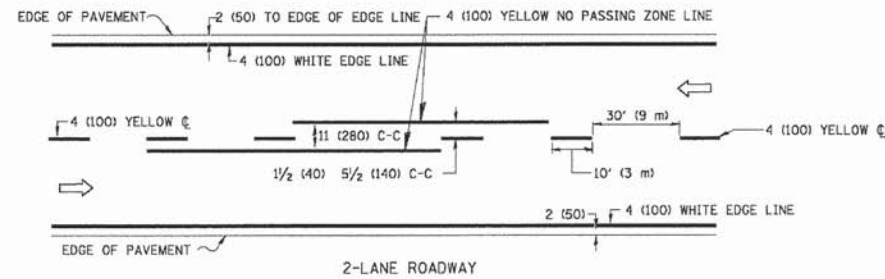
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

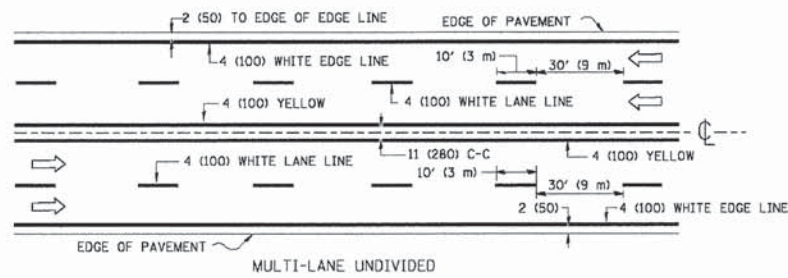
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MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	12
TC-11			CONTRACT NO. 61C62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

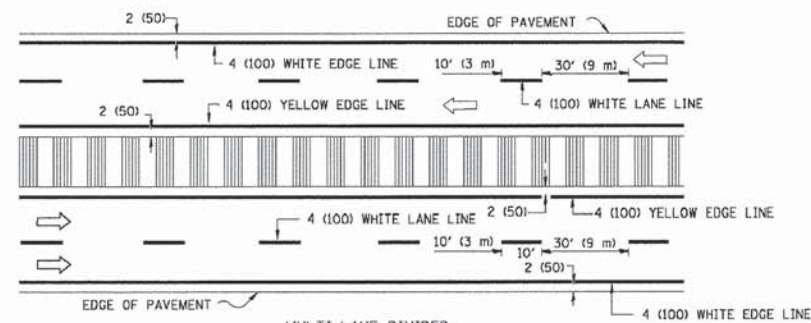
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2-LANE ROADWAY



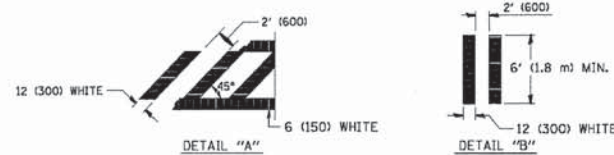
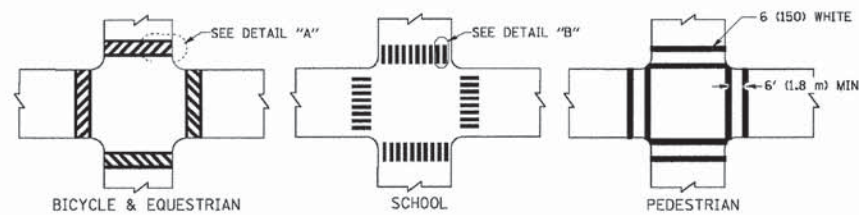
MULTI-LANE UNDIVIDED



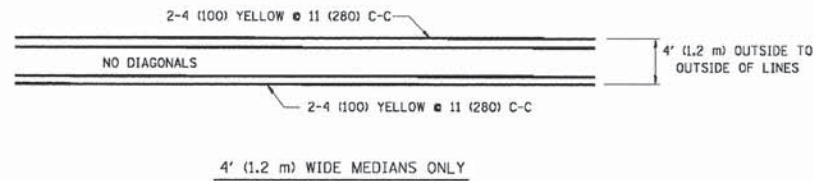
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

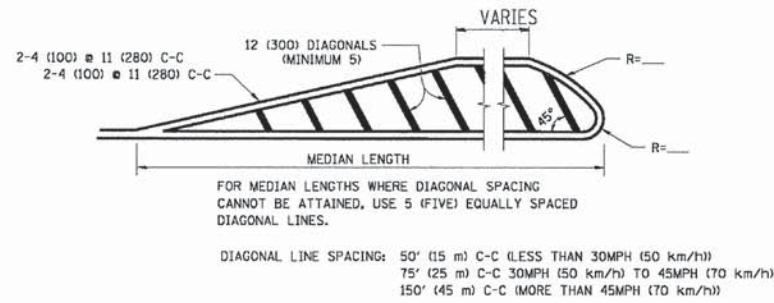
TYPICAL LANE AND EDGE LINE MARKING



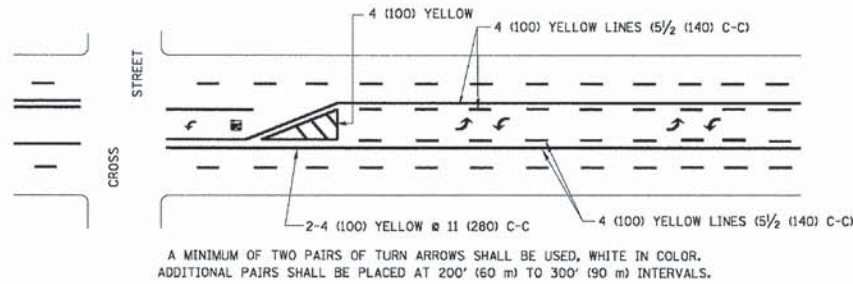
TYPICAL CROSSWALK MARKING



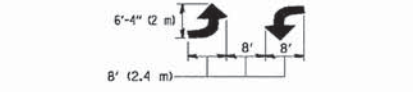
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

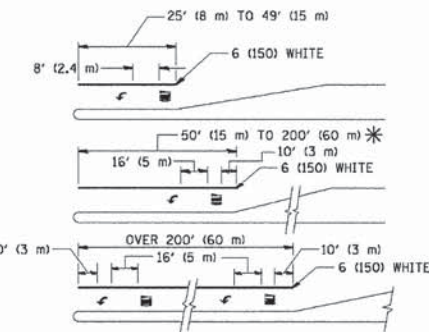


TYPICAL PAINTED MEDIAN MARKING



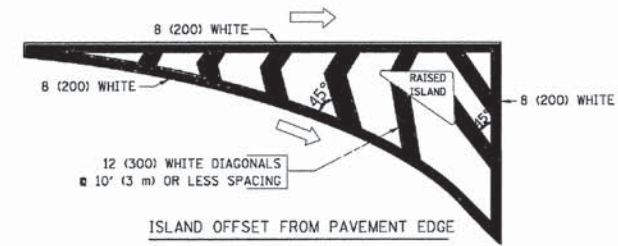
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL LEFT (OR RIGHT) TURN LANE

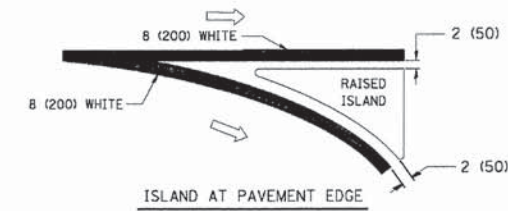


TYPICAL TURN LANE MARKING

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



ISLAND OFFSET FROM PAVEMENT EDGE



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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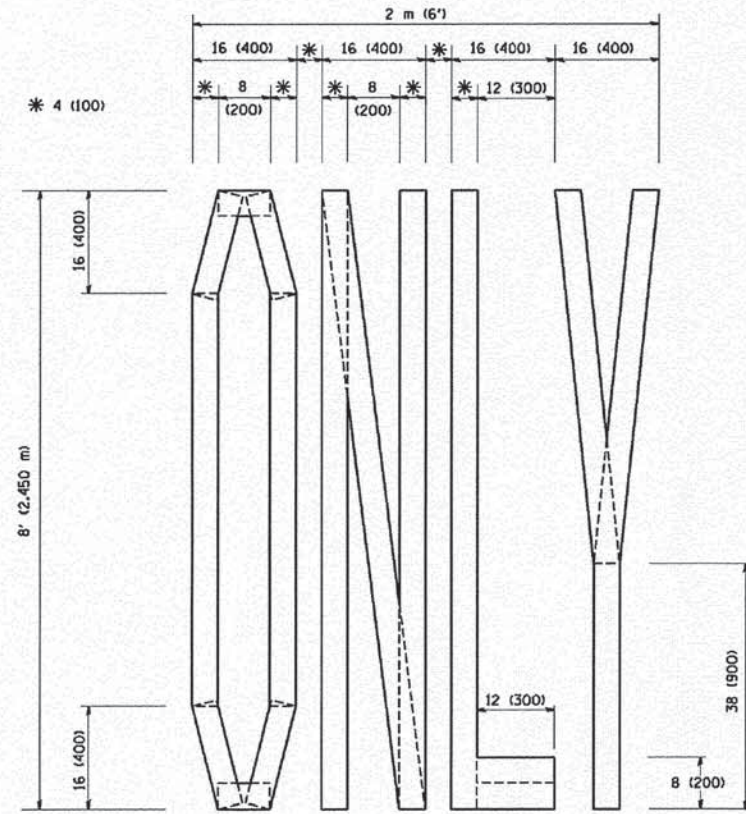
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

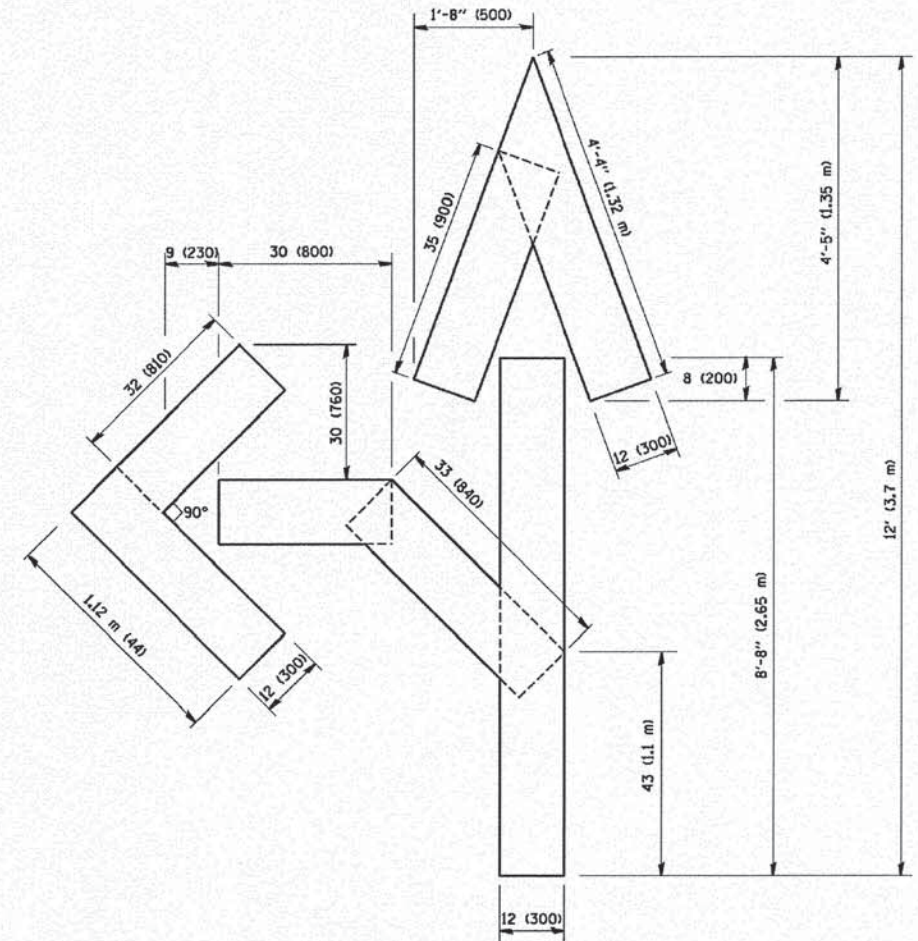
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TC-13		CONTRACT NO. 61C62		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

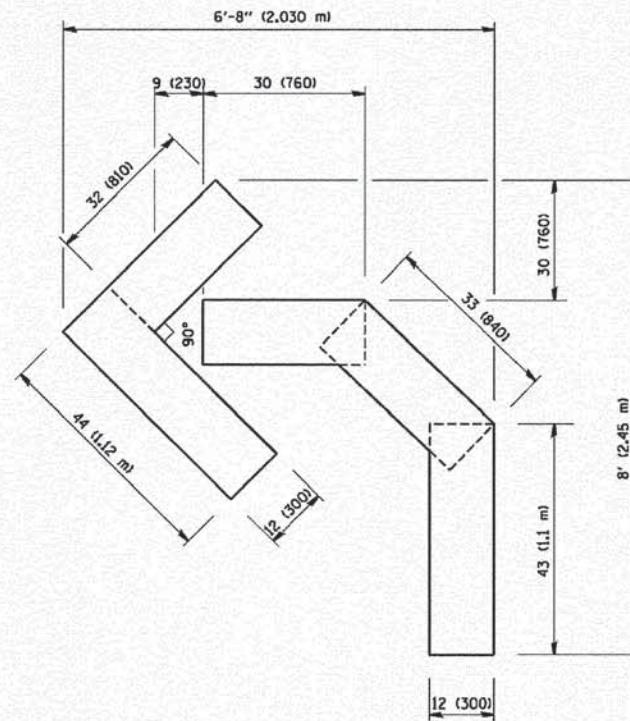
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QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

Plotted: January 19, 2016 @ 2:50 PM By: Jim Schmidt - Tab: 14 TC-16 2/3/14

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 PLOT DATE = 1/4/2008

DESIGNED -	REVISD -T. RAMMACHER 06-05-96
DRAWN -	REVISD -T. RAMMACHER 11-04-97
CHECKED -	REVISD -T. RAMMACHER 03-02-98
DATE - 09-18-94	REVISD -E. GOMEZ 08-28-00

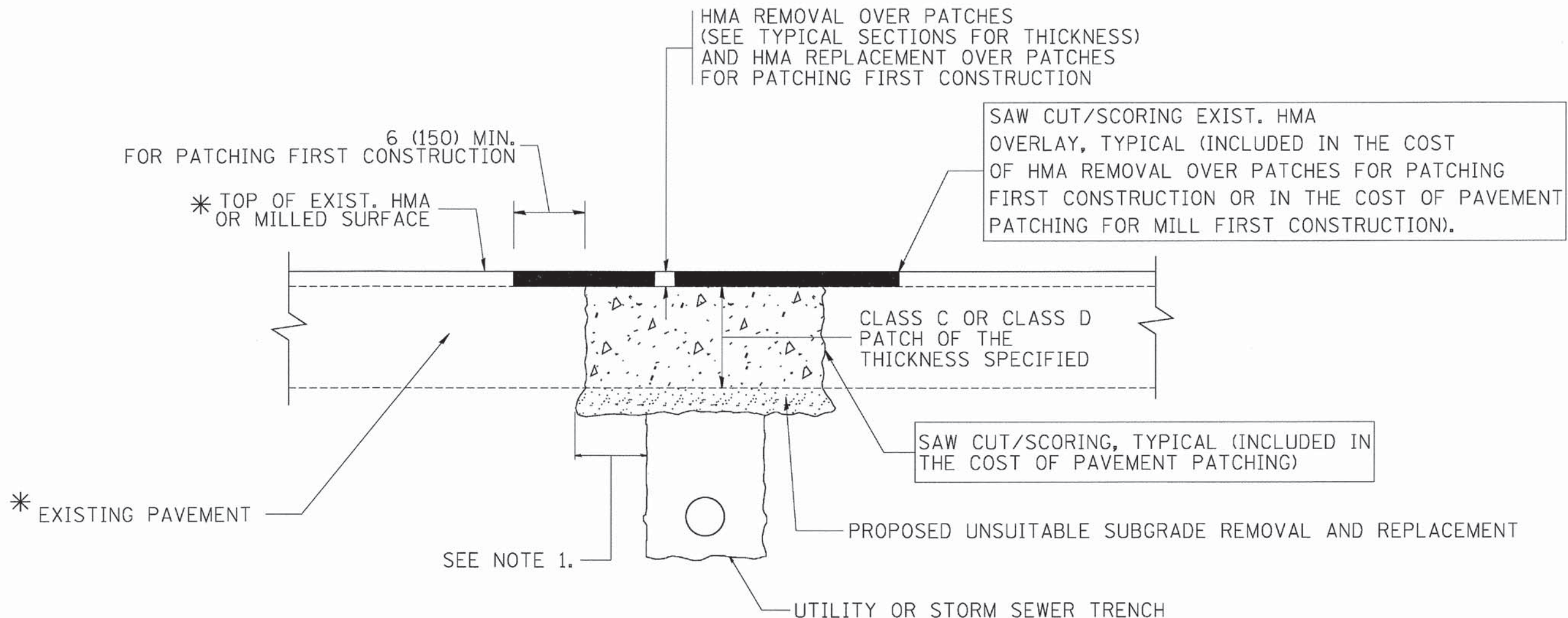
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	14
TC-16			CONTRACT NO. 61C62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

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\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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 FILE NAME =  
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DESIGNED - R. SHAH	REVISOR - A. ABBAS 04-27-98
DRAWN -	REVISOR - R. BORO 01-01-07
CHECKED -	REVISOR - R. BORO 09-04-07
DATE - 10-25-94	REVISOR - K. ENG 10-27-08

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0062	14-00029-00-RS	KANE	17	15
<b>BD400-04 (BD-22)</b>			CONTRACT NO. 61C62	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT - STP				

Plotted: January 19, 2016 @ 2:51 PM By: Jim Schmidt - Tab: 15 BD-22 22x34

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VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

Plotted: January 19, 2016 @ 2:53 PM By: Jim Schmidt - Tab: 16 BD-2423a34

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PLOT DATE = 12/15/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

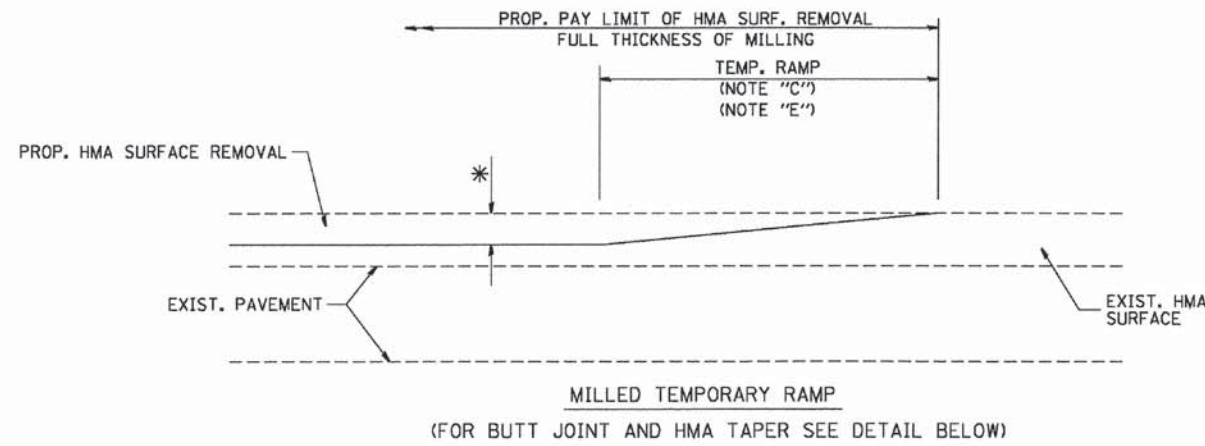
**CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

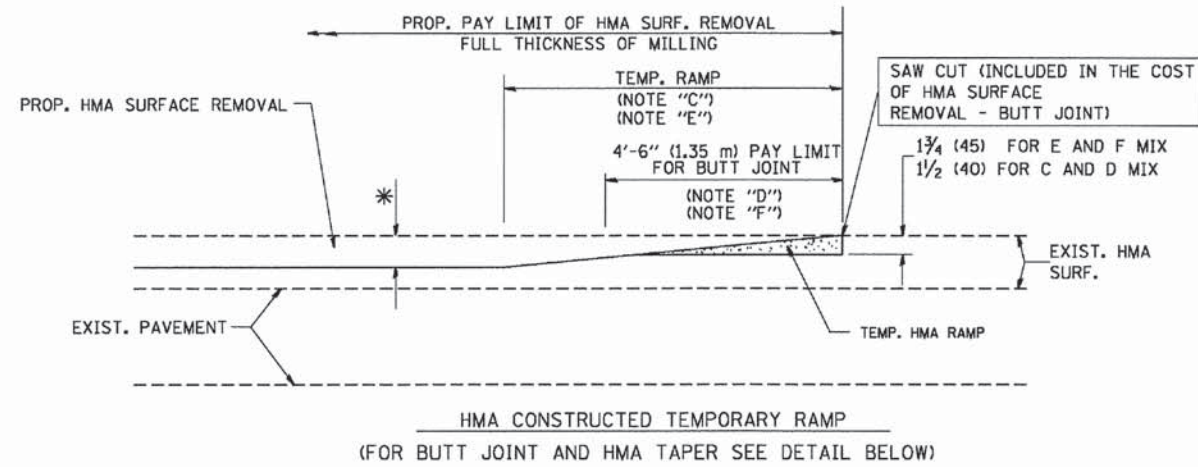
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<b>BD600-06 (BD-24)</b>		CONTRACT NO. 61C62		
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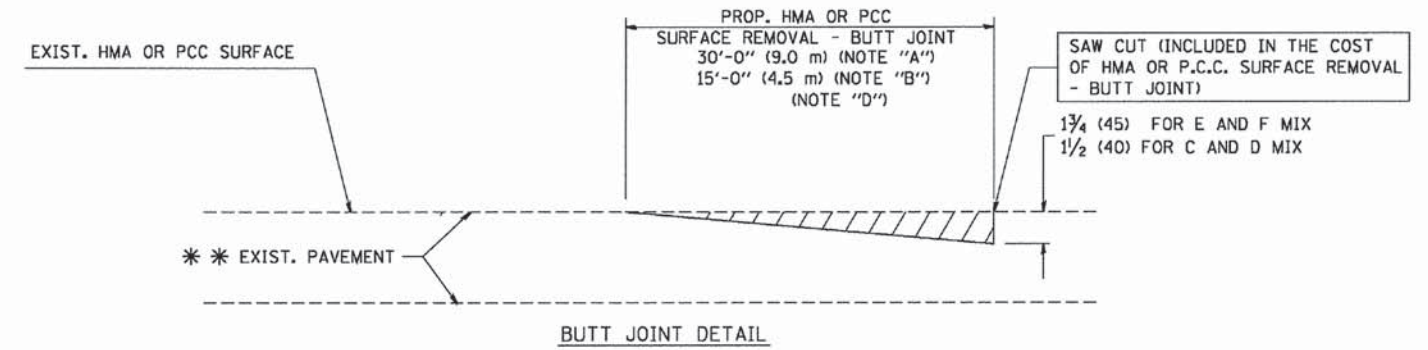


**OPTION 1**

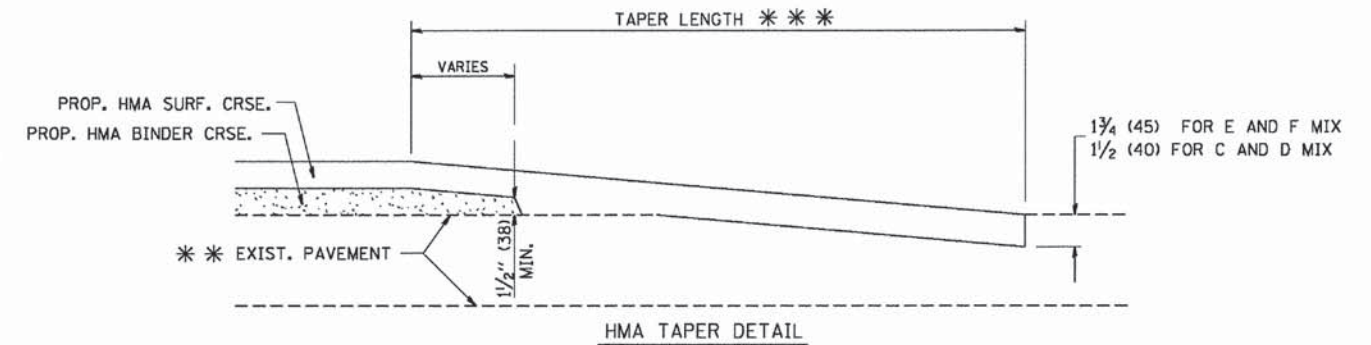


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

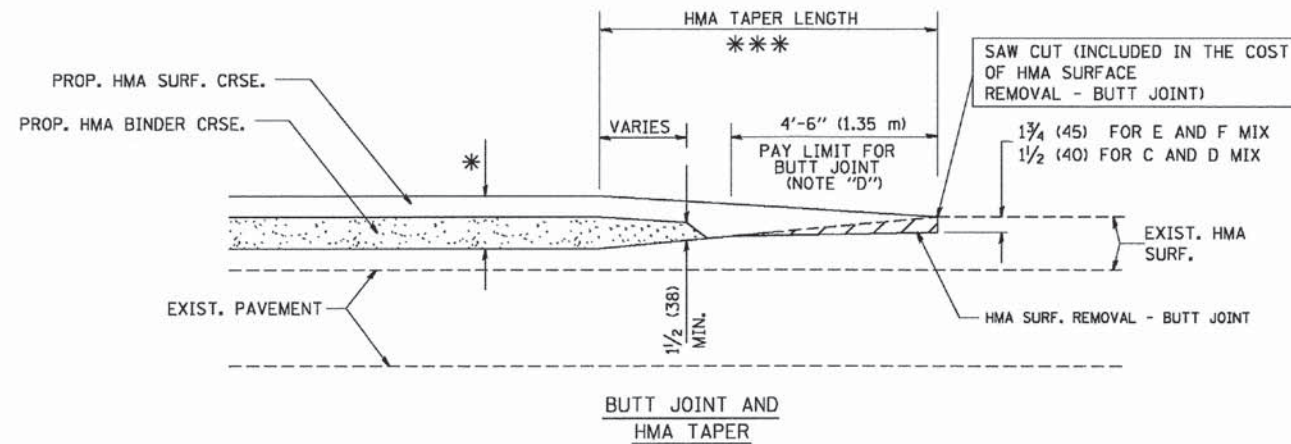
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

Plotted: January 19, 2016 @ 2:54 PM By: Jim Schmidt - Tab: 17 BD-32 22x34

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PLOT DATE = 1/4/2009	

DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
DRAWN -	REVISED - A. ABBAS 03-21-97
CHECKED -	REVISED - M. GOMEZ 04-06-01
DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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