

GENERAL

- IN THE FOLLOWING, THE ILLINOIS DEPARTMENT OF TRANSPORTATION WILL BE REFERRED TO AS IDOT.
- THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" PREPARED BY IDOT, ADOPTED APRIL 1, 2016, AND THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" PREPARED BY IDOT, ADOPTED APRIL 1, 2016. ALL SEWER AND WATER MAIN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", SEVENTH EDITION. ALL WORK ON THE ROCK RIVER WATER RECLAMATION DISTRICT'S FACILITIES SHALL BE IN ACCORDANCE WITH THE RRWRD'S GENERAL PROVISIONS AND TECHNICAL SPECIFICATIONS FOR SANITARY SEWER CONSTRUCTION. SIGN CONSTRUCTION AND PAVEMENT MARKINGS SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", CURRENT EDITION.
- GENERAL SAFETY PROVISION: TO PROVIDE DRIVERS WITH SAFE TRAVEL CONDITIONS DURING THE CONSTRUCTION PROJECT, AND TO PROVIDE SAFE WORKING CONDITIONS FOR ALL EMPLOYEES, THE RULES, REGULATIONS, AND CONDITIONS STATED BELOW WILL PREVAIL FOR THE DURATION OF THIS CONTRACT. ANY EMPLOYEE OF THE CONTRACTOR OR HIS SUBCONTRACTORS WHO REFUSES TO COMPLY WITH THESE GENERAL SAFETY PROVISIONS SHALL BE REMOVED FROM THE JOB SITE IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS. THE CONTRACTOR AND ANY SUBCONTRACTORS RETAINED BY HIM SHALL COMPLY WITH THE STATE AND FEDERAL REQUIREMENTS OF THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 (OSHA), AS IT RELATES TO HIS OPERATIONS, CURRENT REVISION.
- THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE AND LOCAL REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. HE WILL NOT BE ALLOWED TO BUILD FIRES ON THE SITE.
- WHEN ARTIFICIAL LIGHTING IS UTILIZED DURING NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC, AS WELL AS ADJOINING RESIDENTIAL AREAS.
- THE SCALE SHOWN ON THE DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS AND NOT REDUCED SIZE PLANS. DO NOT SCALE REDUCED SIZE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE IDOT STANDARD DRAWINGS LISTED ON THE COVER SHEET ARE INTENDED TO BE THE LATEST REVISIONS AND SHALL TAKE PRECEDENCE OVER EARLIER DRAWINGS THAT MAY BE REFERRED TO ELSEWHERE IN THE PLANS OR SPECIAL PROVISIONS.
- THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING HIS CONSTRUCTION OPERATIONS WITH OTHER CONSTRUCTION, UTILITY, AND/OR MAINTENANCE OPERATIONS WITHIN OR ADJACENT TO THE PROJECT.
- THE CONTRACTOR SHALL FIELD VERIFY THE ELEVATIONS OF THE BENCHMARKS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL ALSO FIELD VERIFY LOCATION, ELEVATION AND SIZE OF EXISTING UTILITIES, AND VERIFY PAVEMENT ELEVATIONS WHERE MATCHING INTO EXISTING WORK. THE CONTRACTOR SHALL FIELD VERIFY HORIZONTAL CONTROL BY REFERENCING KNOWN PROPERTY LINES, OR SURVEY MARKERS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF DISCREPANCIES IN EITHER VERTICAL OR HORIZONTAL CONTROL PRIOR TO PROCEEDING WITH WORK.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL PLAN QUANTITIES.
- THE CONTRACTOR SHALL REMOVE, STORE, AND RELOCATE TO THE SATISFACTION OF THE ENGINEER ALL EXISTING STREET NAME SIGNS WHICH ARE TO BE RELOCATED IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS. THE PLACEMENT OF ADDITIONAL SIGNING SHALL BE THE RESPONSIBILITY OF THE CITY OF ROCKFORD, OR THEIR APPROVED REPRESENTATIVES.
- ALL MATERIALS SHALL MEET OR EXCEED IDOT STANDARDS.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF ROCKFORD, PROPERTY OWNERS, THE ENGINEER, AND ALL UTILITY COMPANIES AT LEAST 48 HOURS PRIOR TO THE START OF CONSTRUCTION OR CHANGE IN CLOSURES OR DETOURS.
- ALL RADII AND DIMENSIONS ARE REFERENCED TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- NO WORK SHALL BE PERFORMED BEYOND THE PROJECT LIMITS WITHOUT PRIOR WRITTEN AUTHORIZATION FROM THE PROPERTY OWNER AND ENGINEER.
- CONSTRUCTION STAKING AND INSPECTION SHALL BE THE RESPONSIBILITY OF THE OWNER. CONSTRUCTION STAKES SHALL BE REQUESTED A MINIMUM OF 48 HOURS IN ADVANCE. ANY RESTAKING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CADD DATA WILL BE AVAILABLE TO CONTRACTORS AND CONSULTANTS WORKING ON THIS PROJECT. THIS INFORMATION WILL BE PROVIDED UPON REQUEST AS AUTOCAD CADD FILES AND AUTOCAD COORDINATE GEOMETRY FILES ONLY. IF DATA IS REQUIRED IN OTHER FORMATS IT WILL BE YOUR RESPONSIBILITY TO MAKE THESE CONVERSIONS. IF ANY DISCREPANCY OR INCONSISTENCY ARISES BETWEEN THE ELECTRONIC DATA AND THE INFORMATION ON THE HARD COPY, THE INFORMATION ON THE HARD COPY SHOULD BE USED. CONTACT THE DISTRICT'S PROJECT ENGINEER TO REQUEST THESE FILES.
- ALL BORROW/WASTE/USE SITES MUST BE APPROVED BY THE DEPARTMENT PRIOR TO REMOVING ANY MATERIAL FROM THE PROJECT OR INITIATING ANY EARTHMOVING ACTIVITIES, INCLUDING TEMPORARY STOCKPILING OUTSIDE THE LIMITS OF CONSTRUCTION.

ROADWAY ITEMS

- PREVIOUSLY PUGMILLED STOCKPILES OF "TYPE A" OLDER THAN 1 MONTH WILL NOT BE APPROVED FOR USE UNTIL A MOISTURE CHECK IS RUN TO VERIFY MOISTURE CONTENT. MATERIAL SHIPPED TO PROJECTS WITHOUT BEING TESTED WILL NOT BE ACCEPTED.
- CLOSED EXPANSION JOINTS ON JOINTED PAVEMENTS SHALL BE RE-ESTABLISHED DURING THE PATCHING OPERATIONS. CLASS B PATCHES - WHEN THE PAVEMENT REQUIRES PATCHING AT THE LOCATION OF THE EXPANSION JOINT, A NEW JOINT SHOULD BE ESTABLISHED USING A DOWELLED EXPANSION PATCH AS SHOWN ON HIGHWAY STANDARD 442101. WHEN THE JOINT IS CLOSED, BUT DOES NOT REQUIRE PATCHING, AN EXPANSION JOINT MAY BE FORMED BY SAWING THE PAVEMENT AND FILLING THE SAW CUT WITH A PREFORMED EXPANSION JOINT FILLER MEETING THE REQUIREMENTS OF SECTION 1051 OF THE STANDARD SPECIFICATIONS AS SHOWN ON STANDARD 420001.
- WHEN LAYING OUT FOR PATCHING, THE MINIMUM DISTANCE BETWEEN NEW PATCHES (SAW CUT TO SAW CUT) SHALL BE 15 FEET. WHEN PATCH SPACING IS LESS THAN 15 FEET, THE PAVEMENT BETWEEN PATCHES SHALL ALSO BE REMOVED AND REPLACED.
- ALL MANDATORY JOINT SEALING FOR CLASS A, CLASS B, AND CLASS B (HINGE JOINTED) PATCHES AS SHOWN ON THE PLANS WILL NOT BE MEASURED FOR PAYMENT. OPTIONAL SAWING OF THE JOINT FOR THE SEALANT RESERVOIR WILL NOT BE MEASURED FOR PAYMENT.

FOR ALL CONCRETE PATCHING THAT WILL NOT BE RESURFACED, THE CONCRETE SHALL BE STRUCK OFF FLUSH WITH THE EXISTING PAVEMENT SURFACE AT EACH END OF THE PATCH.

THE ENGINEER RESERVES THE RIGHT TO CHECK ALL PATCHES FOR SMOOTHNESS BY THE USE OF A 10' ROLLING STRAIGHT EDGE SET TO A 3/16" TOLERANCE IN THE WHEEL PATHS. ANY PATCH AREAS HIGHER THAN 3/16" MUST BE GROUND SMOOTH WITH AN APPROVED GRINDING DEVICE CONSISTING OF MULTIPLE SAWS. THE USE OF BUSHHAMMER OR OTHER IMPACT DEVICES WILL NOT BE PERMITTED. ANY PATCH WITH DEPRESSIONS GREATER THAN 3/16" SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER.

THE MANDATORY SAW CUTS FOR PAVEMENT PATCHING ARE:

a. CLASS B PATCH: CUT TWO TRANSVERSE SAW CUTS OUTLINING THE PATCH AND ONE TRANSVERSE PRESSURE RELIEF SAW CUT. THE LONGITUDINAL EDGES OF THE PATCH SHALL BE CUT FULL DEPTH. WHEN THE PATCH IS ADJACENT TO A PCC SHOULDER, TWO SAW CUTS ALONG THE SHOULDER WILL BE REQUIRED.

THE MANDATORY SAW CUTS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR SAW CUTS.
- MILLING MACHINES ON THIS PROJECT SHALL BE CAPABLE OF REMOVING A LAYER OF BITUMINOUS A MINIMUM 6" WIDE AND 1-1/2 INCHES IN DEPTH IN A SINGLE PASS.
- THE AREA TO BE PRIMED SHALL BE LIMITED TO THAT WHICH CAN BE COVERED WITH HMA ON THE NEXT DAYS PRODUCTIVITY, BUT NO MORE THAN FIVE DAYS IN ADVANCE OF THE PLACEMENT OF THE HMA, UNLESS APPROVED BY THE ENGINEER.
- REFLECTIVE CRACK CONTROL SHALL BE PLACED ON THE EXISTING SURFACE PRIOR TO ANY RESURFACING, UNLESS PAVEMENT IS MILLED THEN IT WILL BE PLACED ON THE BINDER COURSE.
- PAVEMENT MARKING SHALL BE DONE ACCORDING TO STANDARD 780001, EXCEPT AS FOLLOWS:

a. ALL WORDS, SUCH AS ONLY, SHALL BE 8 FEET HIGH.
b. ALL NON-FREEWAY ARROWS SHALL BE THE LARGE SIZE.
c. THE DISTANCE BETWEEN YELLOW NO-PASSING LINES SHALL BE 8 INCHES, NOT 7 INCHES, AS SHOWN IN THE DETAIL OF TYPICAL LANE AND EDGE LINES.
d. CENTERLINE SKIP DASH PAVEMENT MARKING ON MULTI-LANE DIVIDED, MULTI-LANE UNDIVIDED, AND ONE-WAY ROADWAY SHALL BE ACCORDING TO DISTRICT STANDARD 41.1.
e. TIE BARS SHALL BE INSTALLED TO TIE PCC APPURTENANCE TO ADJACENT EXISTING CONCRETE PAVEMENT.
- TIE THE FOLLOWING TO THE EXISTING CONCRETE PAVEMENT

a. GUTTER OR CURB & GUTTER - STD. 606001, 24" LONG NO. 6 @ 24" CENTERS
b. PCC BASE COURSE - STD. 353001, 24" LONG NO. 6 @ 30" CENTERS
c. PCC PAVEMENT - STD. 420101, 24" LONG NO. 6 @ 30" CENTERS
- TIE BARS TO BE INSTALLED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF ARTICLE 420.05(B) OF THE STANDARD SPECIFICATIONS. SEE HIGHWAY STANDARD 420001 FOR DETAIL ON LONGITUDINAL CONSTRUCTION JOINT GROUDED-IN PLACE TIE BAR. THE COST OF THE TIE BARS TO BE INCLUDED IN THE COST OF THE PCC APPURTENANCE ADJACENT TO THE EXISTING PAVEMENT.
- THE CONTRACTOR SHALL INSTALL 18" DIAMETER FORMED OPENINGS IN THE CONCRETE MEDIAN SURFACE, SPACED AT INTERVALS NO GREATER THAN 250', AND/OR AS DIRECTED BY THE ENGINEER. ALL EXISTING PAVEMENT SURFACES OR OTHER EXISTING OBSTRUCTIONS BENEATH THESE OPENINGS SHALL BE REMOVED BY THE CONTRACTOR. AFTER THE MEDIAN IS IN PLACE, CORE EACH OPENING DOWN 4' AND FILL WITH DIRT. ALL COSTS INCURRED SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR CONCRETE MEDIAN TYPE SM-6.24 (SPECIAL).

TRAFFIC CONTROL

- ACCESS MUST BE MAINTAINED TO ALL EXISTING PROPERTIES DURING CONSTRUCTION PER ARTICLE 107.09 UNLESS ARRANGEMENTS ARE MADE IN WRITING BY THE CONTRACTOR WITH THE PROPERTY OWNER FOR SHORT-TERM CLOSURES. A COPY OF SAID ARRANGEMENT MUST BE GIVEN TO THE ENGINEER.
- THE CONTRACTOR IS ADVISED THAT IN THE EVENT OF SNOW, HE WILL ADJUST ANY TRAFFIC CONTROL AND PROTECTION/MAINTENANCE OF TRAFFIC DEVICES REQUIRED FOR HIS OPERATIONS THAT WOULD INTERFERE WITH SNOW REMOVAL OPERATIONS BUT SHALL CONTINUE TO PROVIDE PROTECTION FOR TRAFFIC.
- ANY DROP-OFF GREATER THAN 1-1/2 INCHES ADJACENT TO THE EDGE OF PAVEMENT SHALL BE PROTECTED WITH BARRICADES, AND SHALL BE INCLUDED IN THE UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION, SPECIAL.
- TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN CONDITIONS MAY REQUIRE THE ENGINEER TO MODIFY THE LOCATION OF THE TRAFFIC CONTROL DEVICES. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES FROM THE TIME OF NOTIFICATION BY THE ENGINEER TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION, IMPROVEMENT OR MODIFICATION OF THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
- DURING CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO PROTECT ADJACENT TRAFFIC LANES OPEN TO TRAFFIC FROM DEBRIS BEING BLOWN OR OTHERWISE REMOVED FROM THE CONSTRUCTION AREAS. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR KEEPING DEBRIS OFF THE ADJACENT TRAVELED LANE SURFACE.

TOPSOIL & SEEDING

- THE FINAL TOP FOUR INCHES OF SOIL IN ANY RIGHT-OF-WAY AREA DISTURBED BY THE CONTRACTOR MUST BE CAPABLE OF SUPPORTING VEGETATION. THE SOIL MUST BE FROM THE A HORIZON (ZERO TO 2' DEEP) OF SOIL PROFILES OF LOCAL SOILS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID PER CUBIC YARD FOR PARKWAY RESTORATION.
- THE CONTRACTOR SHALL SEED ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS. SEEDING CLASS 4 OR 2A SHALL BE USED, EXCEPT IN FRONT OF PROPERTIES WHERE THE GRASS WILL BE MOWED, THEN USE SEEDING, CLASS 1. CLASS 2A SHALL BE USED ON FRONT SLOPES AND DITCH BOTTOMS. CLASS 4 SHALL BE USED BEHIND TYPE A GUTTER, ON ALL BACKSLOPES AND AREAS BEHIND THE BACKSLOPE, AND BEYOND THE TOE OF FRONT SLOPE ON FILL SECTIONS WITHOUT DITCHES. (INCLUDE THE FOLLOWING SENTENCE ONLY IF SEEDING IS LESS THAN 0.5 ACRE.) THIS WORK WILL BE INCLUDED IN THE CONTRACT UNIT PRICE PER CUBIC YARD FOR PARKWAY RESTORATION.
- FERTILIZER SHALL BE APPLIED TO ALL DISTURBED AREAS AND INCORPORATED INTO THE SEEDBED PRIOR TO SEEDING OR PLACEMENT OF SOD AT THE RATE SPECIFIED IN SECTIONS 250 AND 252 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST OF PARKWAY RESTORATION.
- MULCH METHOD II SHALL BE APPLIED OVER ALL SEEDED AREAS. THIS SHALL BE INCLUDED IN THE COST OF THE PARKWAY RESTORATION.

MIXTURES TABLE

	LEVELING BINDER (MACHINE METHOD)	HMA SURFACE
PG GRADE	PG 64-22	SBS 70-22
MAX % RAP ALLOWABLE	PER IDOT BDE SP	PER IDOT BDE SP
DESIGN AIR VOIDS	4.0% @ N50	4.0% @ N50
MIXTURE COMPOSITION	IL-9.5	IL-9.5
FRICITION AGGREGATE		MIX D

* MIX TONNAGES ARE BASED ON THE DENSITY VALUE OF 112.0 LB/50 YD/IN.

UTILITIES

- UTILITIES SHOWN ON THE PLANS ARE FOR ILLUSTRATIVE PURPOSES ONLY AND NO GUARANTEE OF THEIR ACCURACY IS MADE OR IMPLIED. THE LOCATIONS OF EXISTING UTILITIES AS SHOWN ON THE DRAWINGS REPRESENT DATA RECEIVED FROM VARIOUS SOURCES. IT IS NOT GUARANTEED TO BE CORRECT OR ALL-INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATION INTO THE LOCATION, SIZE, DEPTH, NUMBER AND NATURE OF ANY AND ALL EXISTING UTILITIES WHICH MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES WHICH ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION OPERATIONS SHALL BE IMMEDIATELY REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER. THIS WORK SHALL BE AT THE CONTRACTORS EXPENSE.
- THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 811 OR 1-800-892-0123 48 HOURS PRIOR TO CONSTRUCTION FOR CONFIRMATION OF CURRENT UTILITY LOCATIONS AND FOR ALL NON-EMERGENCY WORK. UTILITIES WHICH ARE NOT MEMBERS OF J.U.L.I.E. SHOULD BE NOTIFIED INDIVIDUALLY BY THE CONTRACTOR. IDOT IS NOT A MEMBER OF JULIE. IF YOU ARE NEAR ANY OVERHEAD LIGHTING, INTERSECTION LIGHTING OR TRAFFIC SIGNALS, CONTACT THE IDOT TRAFFIC OFFICE AT 815/284-5469 AT LEAST 48 HOURS PRIOR TO WORK.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK. THE JULIE NUMBER IS 800-892-0123.
- NEW MANHOLE AND INLET LIDS ON THIS PROJECT SHALL HAVE THE WORDS "STORM" OR "SANITARY" ON THE LID. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE APPROPRIATE TYPE OF LID. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE MUNICIPALITY TO DETERMINE APPROVED METHODS OF UTILITY STRUCTURE ADJUSTMENT. UTILITY STRUCTURES MAY INCLUDE, BUT ARE NOT LIMITED TO, MANHOLES, WATER VALVES, HANDHOLES, ETC. ALL MATERIALS AND WORK NECESSARY TO COMPLETE ADJUSTMENTS PER MUNICIPALITY REQUIREMENTS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED ADJUSTMENT PAY ITEM.
- IF THE SANITARY MANHOLE CASTING BEING ADJUSTED DOES NOT MEET ROCK RIVER WATER RECLAMATION DISTRICT STANDARDS, THE DISTRICT SUPPORTING SERVICES DEPARTMENT SHOULD BE CONTACTED PRIOR TO THE ADJUSTMENT. THE CONTRACTOR SHALL COORDINATE WITH THE DISTRICT REGARDING THE DELIVERY OF THE REPLACEMENT CASTING.
- ALL SANITARY SEWER MANHOLES ARE TO BE ADJUSTED PER RRWRD STANDARDS. SANITARY MANHOLE ADJUSTMENT BY GROUTING IS NOT PERMITTED. THE CONTRACTOR SHALL ADVISE THE DISTRICT WHEN MANHOLE ADJUSTMENTS ARE TO BE MADE, SO THE DISTRICT MAY WITNESS THEM. THE METHOD AND AMOUNT OF ADJUSTMENT SHOULD BE NOTED ON THE PLANS. DISTRICT SPECIFICATIONS REQUIRE:

a. A MINIMUM OF 4" OF ADJUSTING RINGS (4" ADJUSTMENT RING NOT REQUIRED IN TURF AREA OR CURB AND GUTTER ROADWAYS)
b. MAXIMUM OF 12" ADJUSTING RINGS.
c. NO MORE THAN ONE (1) 2" ADJUSTING RING PER MANHOLE.
d. A MAXIMUM OF 30" FROM THE TOP OF CASTING TO THE FIRST STEP.

UTILITY CONTACTS

GAS:

NICOR
MR. BRUCE KOPPANG
1844 FERRY ROAD
NAPERVILLE, ILLINOIS 60563-9600
OFFICE (630) 388-3830
CELL (708) 243-5136

ELECTRIC:

COMED
MR. SCOTT DELESKIEWICZ
123 ENERGY AVENUE
ROCKFORD, ILLINOIS 61109
CELL (815) 391-4650

TELEPHONE:

AT&T - ROCKFORD AREA
MR. DAVID SAINT-GERMAIN
2707 HUFFMAN BOULEVARD
ROCKFORD, ILLINOIS 61103
(815) 245-4307

CABLE TELEVISION:

COMCAST
MR. TOM YUCCAS
4450 KISHWAUKEE STREET
ROCKFORD, ILLINOIS 61109
OFFICE (224) 229-3035
CELL (815) 509-8653

SANITARY SEWER:

ROCK RIVER WATER RECLAMATION DISTRICT
MR. DANA CARROLL, P.E.
ENGINEERING MANAGER
P.O. BOX 7480
3501 KISHWAUKEE STREET
ROCKFORD, ILLINOIS 61126
(815) 387-7660

WATER:

CITY OF ROCKFORD - WATER DEPARTMENT
MR. JAMIE ROTT
425 EAST STATE STREET
ROCKFORD, ILLINOIS 61104
(815) 967-6740

DISPOSAL SERVICES:

ROCK RIVER DISPOSAL
MR. ROCCO GUGLIELMETTI
4002 SOUTH MAIN STREET
ROCKFORD, ILLINOIS 61102
(815) 965-2489

ROADWAY - TRANSPORTATION:

CITY OF ROCKFORD - PUBLIC WORKS
MR. MATT WITNER - CITY ENGINEER
425 EAST STATE STREET
ROCKFORD, ILLINOIS 61104
(815) 987-5570

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS - DISTRICT 2
819 DEPOT AVENUE
DIXON, ILLINOIS 61021

SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE
1	ADDENDUM NO. 1	April 15, 2016

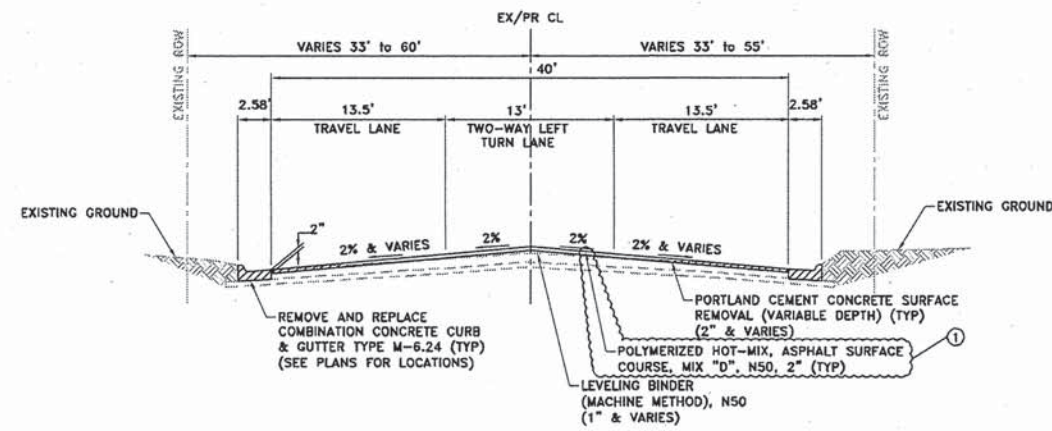
PLOT SCALE:	1:1
PLOT DATE:	4/15/2016
DRAWN BY:	BSL
CHECKED BY:	KRL
SUBMITTAL DATE:	01/25/2016

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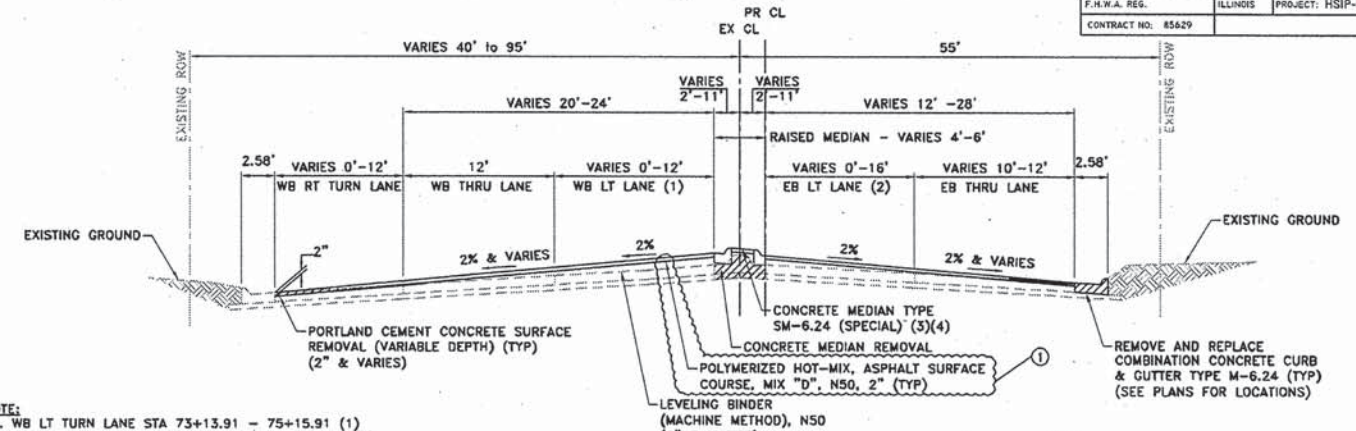
GENERAL NOTES		
SANDY HOLLOW HSIP		
SANDY HOLLOW ROAD	ROCKFORD, ILLINOIS	
FILE: H-114-013 SANDY HOLLOW HSIP\DESIGN\DRAWINGS\114-013 GENERAL NOTES.DWG		
MEAI JOB: 04-28-14-015		

SHEET NO.	2
OF	62

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5089	13-00599-00-SP	WINNEBAGO	62	5
F.H.W.A. REG.	ILLINOIS	PROJECT:	HSIP-5099 (116)	
CONTRACT NO. 85429				



PROPOSED SECTION
SANDY HOLLOW ROAD
STATION 1+78.00 TO STATION 73+13.91



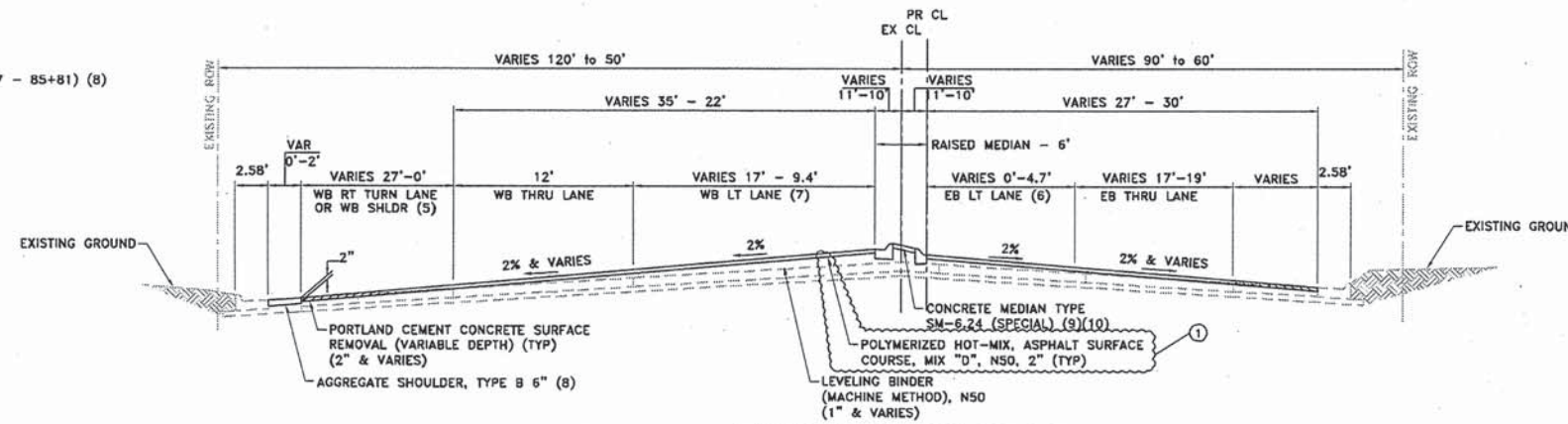
PROPOSED SECTION
SANDY HOLLOW ROAD
STATION 73+13.91 TO STATION 77+82.40

NOTE:
PR. WB LT TURN LANE STA 73+13.91 - 75+15.91 (1)
PR. CORRUGATED MEDIAN STA 73+13.91 - 74+15.91 (3)
PR. CONCRETE MEDIAN STA 74+15.91 - 77+59.91 (4)

NOTE:
PR. EB LT TURN LANE STA 74+15.91 - 77+59.91 (2)

OMISSION STATION 77+82.40 - 78+42.71 (ALPINE ROAD)

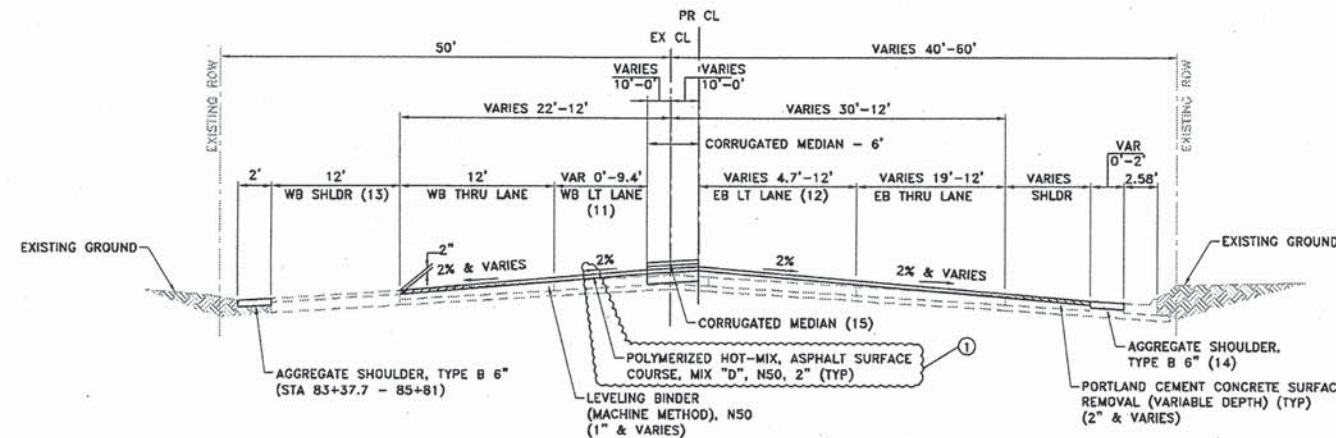
NOTE:
PR. WB RT TURN LANE STA 78+42.71 - 81+23.16 (5)
PR. WB LT TURN LANE STA 78+82.36 - 81+47.36 (7)
PR. WB AGGREGATE SHOULDER STA 79+43.80 - 80+17.7 & 83+37.7 - 85+81 (8)
PR. CONCRETE MEDIAN STA 78+82.36 - 80+47.36 (9)
PR. CORRUGATED MEDIAN STA 80+47.36 - 81+47.36 (10)



PROPOSED SECTION
SANDY HOLLOW ROAD
STATION 78+42.71 TO STATION 80+88.18

NOTE:
PR. EB LT TURN LANE STA 80+47.36 - 87+27.36 (6)

NOTE:
PR. WB LT TURN LANE STA 78+82.36 - 81+47.36 (11)
PR. WB SHOULDER STA 80+78.78 - 82+02.69 (13)
PR. CORRUGATED MEDIAN STA 80+47.36 - 81+47.36 (15)



PROPOSED SECTION
SANDY HOLLOW ROAD
STATION 80+88.18 TO STATION 85+81.00

NOTE:
PR. EB LT TURN LANE STA 80+47.36 - 87+27.36 (12)
PR. EB AGGREGATE SHOULDER STA 83+95.3 - 85+06.1 (14)

SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE
1	ADDENDUM NO. 1	April 15, 2016

PLOT SCALE:	1:1
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PROPOSED TYPICAL SECTIONS		
SANDY HOLLOW ROAD	SANDY HOLLOW HSIP	ROCKFORD, ILLINOIS
FILE:H:\14-013 SANDY HOLLOW HSIP\DESIGN\DRAWINGS\14-013 TYPICAL SECTIONS.DWG		JOB:04-28-14-013

SHEET NO.
5
OF
62