

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO.	60J21	

D-91-208-10

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

F.A.P. 344: ILLINOIS ROUTE 83
WASHINGTON STREET TO ILLINOIS ROUTE 56
SECTION: 543 R-1-RS
RESURFACING (3P)
PROJECT: --
DUPAGE COUNTY
C-91-208-10

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED WITHIN THE CITY OF ELMHURST, THE VILLAGE OF VILLA PARK, AND THE CITY OF OAKBROOK TERRACE



LOCATION OF SECTION INDICATED THUS: - ■ -

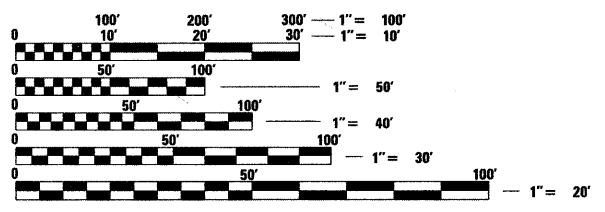
TRAFFIC DATA
ILLINOIS ROUTE 83
2007 ADT = 76,500
SPEED LIMIT = 45-50 MPH

IMPROVEMENT ENDS
 STA. 650 + 57

BRIDGE OMISSION
 STA. 585 + 02 TO STA. 587 + 13

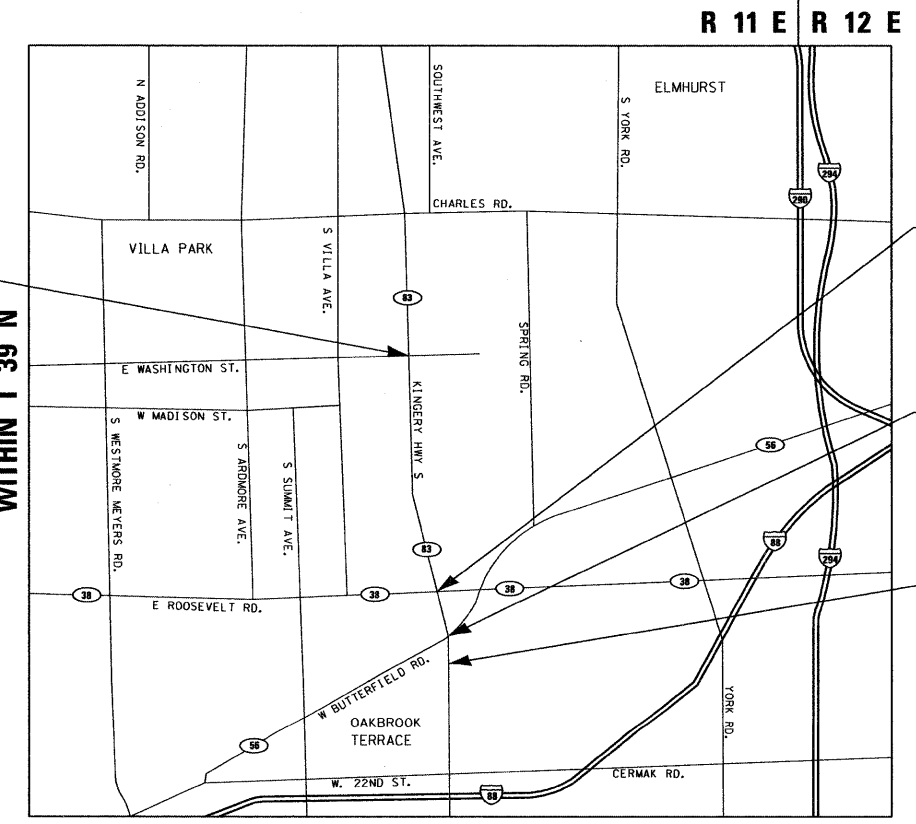
BRIDGE OMISSION
 STA. 571 + 30 TO STA. 573 + 38

IMPROVEMENT BEGINS
 STA. 566 + 74



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811



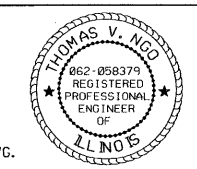
SCALE: NTS

YORK TOWNSHIP

GROSS LENGTH OF PROJECT = 8,383 FT = 1.59 MILES
 NET LENGTH OF PROJECT = 7,964 FT = 1.51 MILES

MILLENNIA PROFESSIONAL SERVICES
 THOMAS V. NGO, P.E.
 * 062-058379

Thomas V. Ngo
 DATE: 2/16/2010
 SIGNATURE AND SEAL APPLIES TO DRWG.




STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 16, 2010

Diana M. O'Keefe
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 19, 2010
Scott E. Stitt, P.E.
 Acting ENGINEER OF DESIGN AND ENVIRONMENT

March 19, 2010
Christine M. Reed
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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MILLENNIA PROFESSIONAL SERVICES

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DISTRICT ONE - PLAN PREP ENGINEER: KEN ENG (847) 705-4247

CONTRACT NO. 60J21

INDEX OF SHEETS

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- 26 STANDARD TRAFFIC SIGNAL DESIGN DETAIL, SHEET 1 OF 6 (TS-05)
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LIST OF ILLINOIS DOT HIGHWAY STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 442201-03 CLASS C AND D PATCHES
- 606001-04 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 606301-04 PC CONCRETE ISLANDS AND MEDIANS
- 606306-03 CORRUGATED PC CONCRETE MEDIANS
- 635011-02 REFLECTOR MARKER AND MOUNTING DETAILS
- 701421-02 LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY FOR SPEEDS > OR = 45 MPH TO 55 MPH
- 701426-03 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS > OR = 45 MPH
- 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701901-01 TRAFFIC CONTROL DEVICES
- 814001-02 HANDHOLES

COMMITMENTS

NO COMMITMENTS FOR THIS PROJECT

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES, CITY OF ELMHURST AND VILLAGE OF VILLA PARK.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
4. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
5. ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE. NO ADDITIONAL COST TO THE DEPARTMENT.
6. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
7. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
8. LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
9. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.
11. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
12. THE ENGINEER SHALL CONTACT DON CHIARUGI, THE TRAFFIC FIELD TECHNICIAN AT (847)741-9857 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
13. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
14. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
15. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN ON THE PLANS.
16. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
17. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.
18. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND IT'S REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
19. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
20. THE CONTRACTOR SHALL INSTALL THE BARRIER WALL MARKERS ALONG THE EXISTING CONCRETE BARRIER WALL IN ACCORDANCE WITH HIGHWAY STANDARD 635011 AND AS DIRECTED BY THE ENGINEER.

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 PLO USER = Millenna Professional Services



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DATE - 2/22/2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 83 (SOUTH)
ILLINOIS RTE 56 TO WASHINGTON ST

INDEX OF SHEETS, LIST OF IDOT
HIGHWAY STANDARD, GENERAL NOTES,
AND COMMITMENTS

F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 2
CONTRACT NO. 60J21			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

SCALE: N/A SHEET NO. OF SHEETS STA. TO STA.

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URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITY	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	100% STATE	1000 QUANTITY
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	43	43
40600300	AGGREGATE (PRIME COAT)	TON	211	211
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	160	160
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	4350	4350
40600895	CONSTRUCTING TEST STRIP	EACH	1	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT	SQ YD	794	794
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	400	400
40603153	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	10,520	10,520
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2300	2300
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	15570	15570
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	27380	27380
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	62928	62928
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	60	60
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	1785	1785
44213200	SAW CUTS	FOOT	9632	9632
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	50	50
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	1067	1067
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	212	212
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	339	339
45100200	CRACK FILLING	POUND	3564	3564
55039700	STORM SEWERS TO BE CLEANED	FOOT	456	456
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	5	5
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	5	5
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	5	5
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	6	6
60404950	FRAMES AND GRATES, TYPE 24	EACH	5	5
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5	5
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6
67100100	MOBILIZATION	L SUM	1	1

* SPECIALTY ITEM

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITY	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	100% STATE	1000 QUANTITY
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	9985	9985
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	583	583
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	67782	67782
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2230	2230
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	9316	9316
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	8976	8976
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	372	372
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1110	1110
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	291	291
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	33891	33891
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1115	1115
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	4648	4648
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	4488	4488
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	186	186
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	863	863
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	151	151
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	863	863
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	789	789
* X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	52
* X8950200	REBUILD EXISTING HANDHOLE	EACH	2	2
* X8950210	REBUILD EXISTING HANDHOLE TO HEAVY-DUTY HANDHOLE	EACH	2	2
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	98	98
X0324685	TEST STRIP (STONE MATRIX ASPHALT)	EACH	1	1

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 USER NAME = Millennium Professional Services



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 CHECKED - RPD
 DATE - 2/23/2010

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

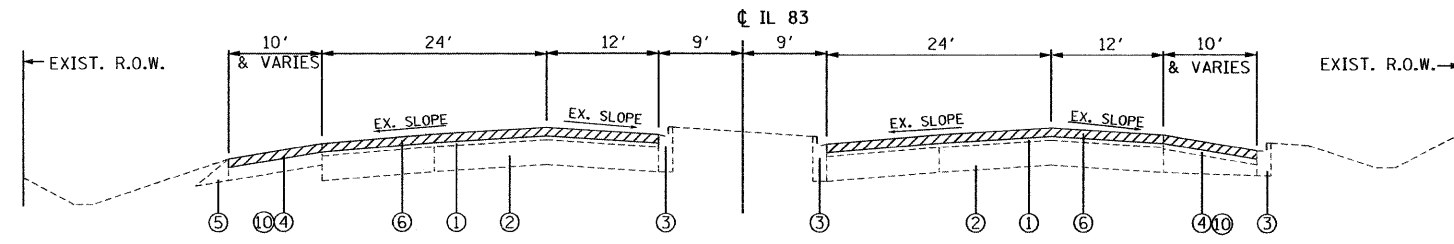
ILLINOIS ROUTE 83 (SOUTH)
 ILLINOIS RTE 56 TO WASHINGTON ST

SUMMARY OF QUANTITIES

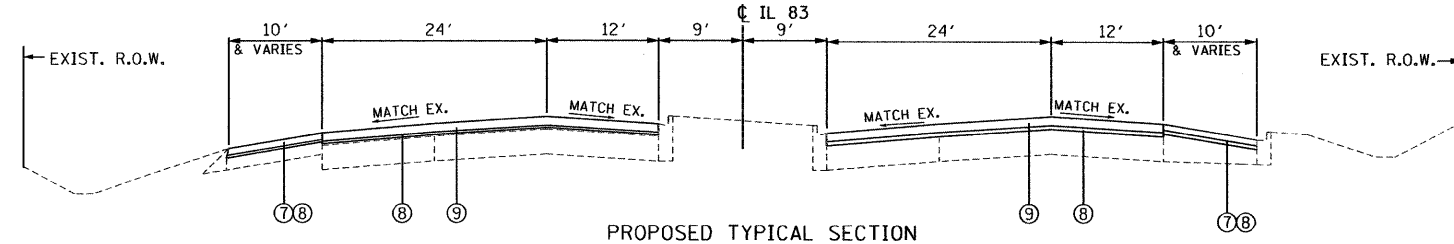
F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 3
CONTRACT NO. 60J21			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

SCALE: NTS SHEET NO. OF SHEETS STA. TO STA.

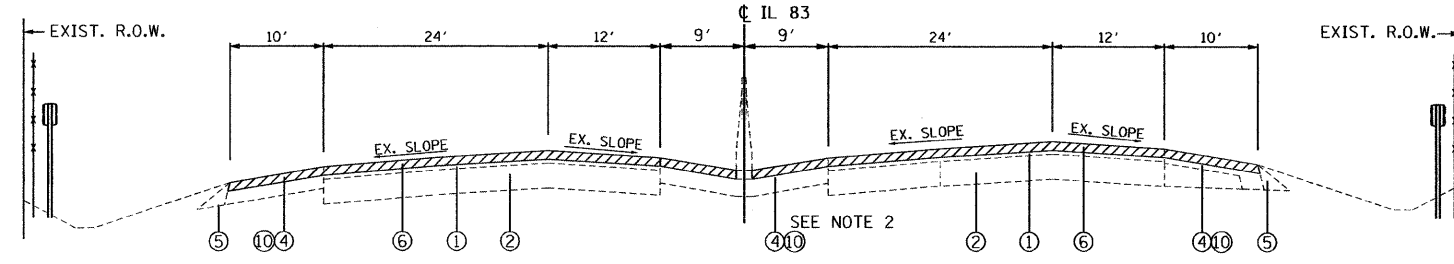
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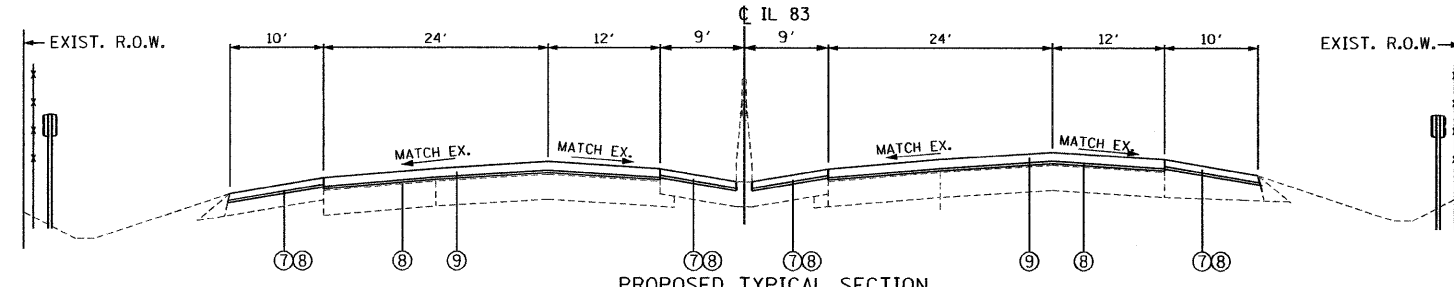
EXISTING TYPICAL SECTION
 STA. 566+73 TO STA. 571+82
 STA. 625+26 TO STA. 650+48



PROPOSED TYPICAL SECTION
 STA. 566+00 TO STA. 571+00
 STA. 625+26 TO STA. 650+48



EXISTING TYPICAL SECTION
 STA. 573+80 TO STA. 596+06
 STA. 608+76 TO STA. 619+80



PROPOSED TYPICAL SECTION
 STA. 573+80 TO STA. 596+06
 STA. 573+80 TO STA. 619+80

LEGEND

- ① EXISTING +/-4" HMA (1 3/4" SURFACE, 2 1/4" BINDER CSE)
- ② EXISTING +/-8" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
- ④ EXISTING BITUMINOUS SHOULDER +/-8"
- ⑤ EXISTING +/- 8" AGGREGATE SHOULDER, TYPE B
- ⑥ HMA SURFACE REMOVAL - 2 3/4" (SEE NOTE 1)
- ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5MM) -1 1/2"
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"
- ⑩ HMA SURFACE REMOVAL - 2 1/4"
- ⑪ HMA SURFACE REMOVAL - 2"

NOTES

1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.
2. NO ADDITIONAL COMPENSATION WILL BE GIVEN TO THE CONTRACTOR FOR ANY EXTRA WORK REQUIRED TO REMOVE THE HMA SHOULDER ADJACENT TO THE EXISTING BARRIER WALL. THIS WORK WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR HMA SURFACE REMOVAL - 2 1/4" .

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	3.5% @ 80 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 GYR.
HMA SHOULDER RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm)	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 MM)	4% @ 70 GYR.
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 MM)	4% @ 70 GYR.

MIXTURE NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/50 YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

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 PLOT SCALE = 1/8"=1'-0" / IN.
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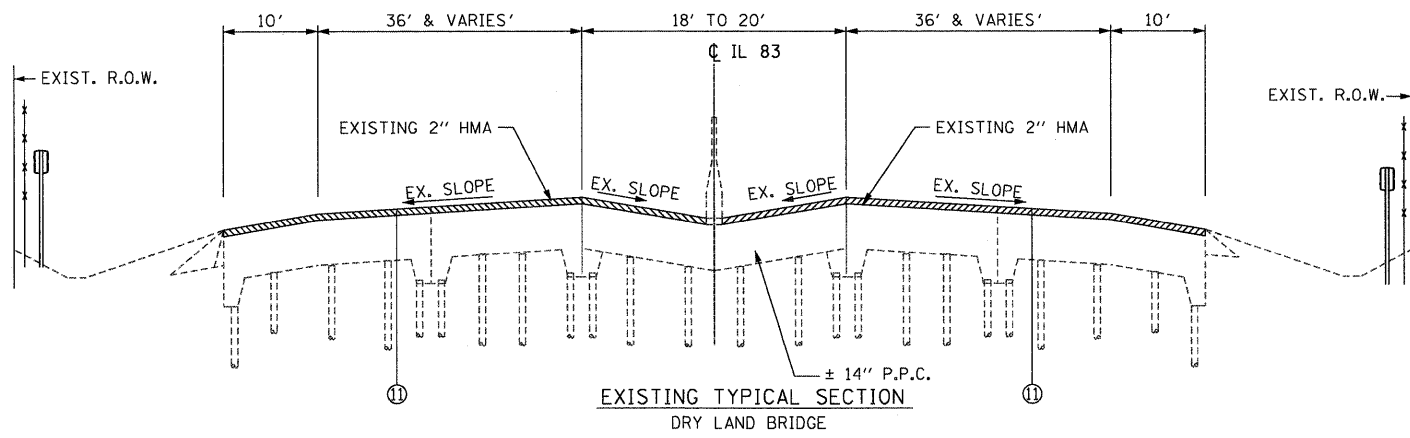
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CHECKED - RPD	REVISED -
DATE - 3/3/2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 83 (SOUTH)
ILLINOIS RTE 56 TO WASHINGTON ST

TYPICAL SECTIONS
 SCALE: SHEET NO. OF SHEETS STA. TO STA.

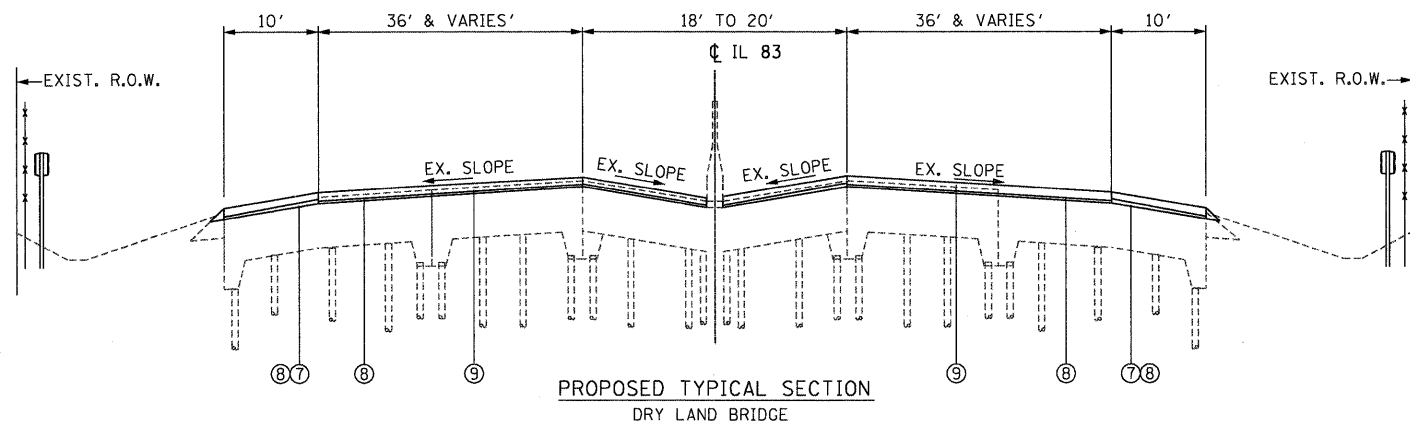
F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 4
CONTRACT NO. 60J21				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



EXISTING TYPICAL SECTION
 DRY LAND BRIDGE

SOUTHBOUND
 STA. 596+06 TO STA. 608+76
 STA. 619+86 TO STA. 624+56

NORTHBOUND
 STA. 596+06 TO STA. 607+06
 STA. 619+86 TO STA. 625+56



PROPOSED TYPICAL SECTION
 DRY LAND BRIDGE

SOUTHBOUND
 STA. 596+06 TO STA. 608+77
 STA. 619+80 TO STA. 624+56

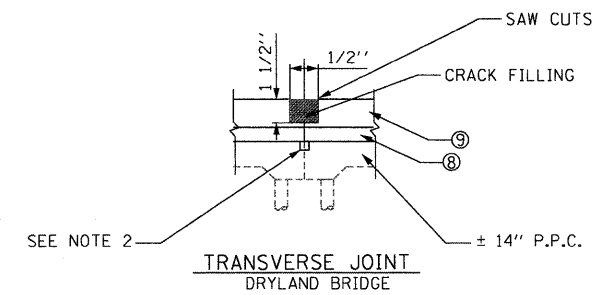
NORTHBOUND
 STA. 596+06 TO STA. 607+06
 STA. 619+86 TO STA. 625+56

LEGEND

- ① EXISTING +/-4" HMA (1 3/4" SURFACE, 2 1/4" BINDER CSE)
- ② EXISTING +/-8" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
- ④ EXISTING BITUMINOUS SHOULDER +/-8"
- ⑤ EXISTING +/- 8" AGGREGATE SHOULDER, TYPE B
- ⑥ HMA SURFACE REMOVAL - 2 3/4"
- ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5MM) -1 1/2"
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4" (SEE NOTE 2)
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2" (SEE NOTE 2)
- ⑩ HMA SURFACE REMOVAL - 2 1/4"
- ⑪ HMA SURFACE REMOVAL - 2" (SEE NOTE 1)

NOTES

1. THE CONTRACTOR SHALL CONTACT SARAH WILSON, DISTRICT ONE BRIDGE MAINTENANCE ENGINEER, AT (847) 705-4181, IMMEDIATELY AFTER THE REMOVAL OF THE EXISTING HMA SURFACE SO THE EXISTING BRIDGE DECK CAN BE EVALUATED.
2. THE CONTRACTOR SHALL RECORD THE LOCATIONS OF THE TRANSVERSE JOINTS PRIOR TO INSTALLING THE LEVELING BINDER AND SURFACE COURSE. THE CONTRACTOR SHALL SAWCUT AND CRACK FILL OVER THE EXISTING JOINTS AFTER PLACEMENT OF THE SURFACE COURSE. SEE TRANSVERSE JOINT DETAIL BELOW.



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MILLENNIA PROFESSIONAL SERVICES

DESIGNED - CJD	REVISED -
DRAWN - CJD	REVISED -
CHECKED - RPD	REVISED -
DATE - 2/23/2010	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

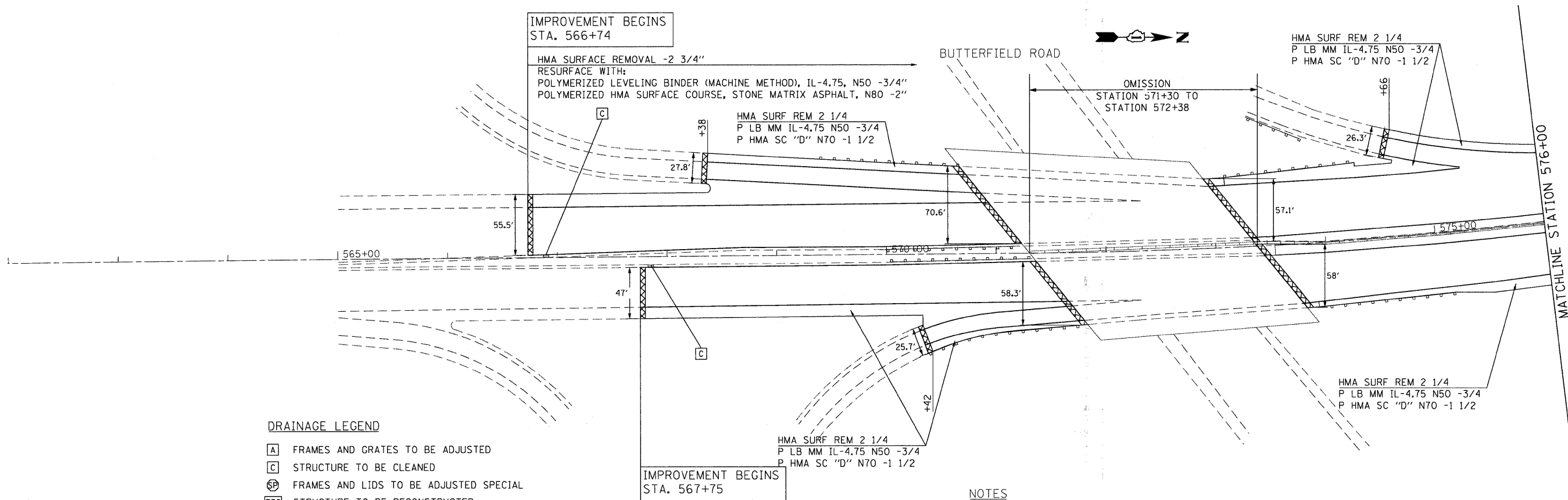
**ILLINOIS ROUTE 83 (SOUTH)
 ILLINOIS RTE 56 TO WASHINGTON ST**

TYPICAL SECTIONS

SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.
--------	-----------	----	--------	------	---------

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J21	

P:\2009\ME\0906_Ver-Yer-PR\1\ACADD\M08_IL635\Sh1a\04-D180J21-sh1-IL635-TYP.dgn

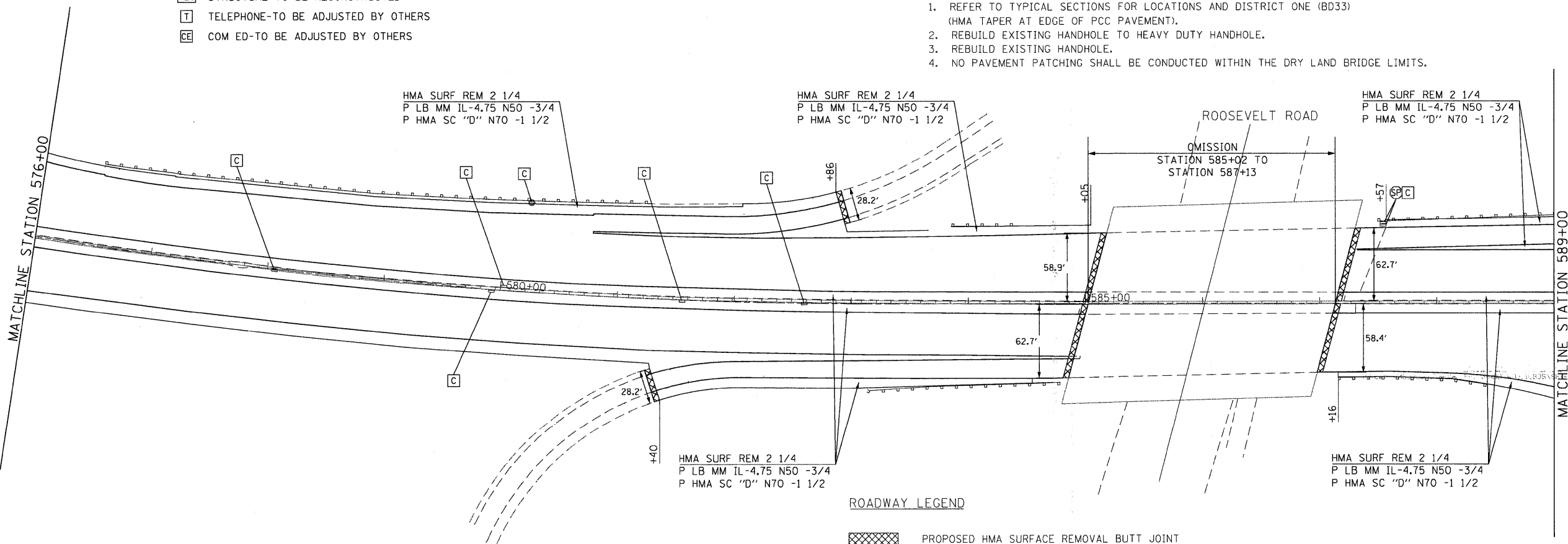


DRAINAGE LEGEND

- A** FRAMES AND GRATES TO BE ADJUSTED
- C** STRUCTURE TO BE CLEANED
- SP** FRAMES AND LIDS TO BE ADJUSTED SPECIAL
- REC** STRUCTURE TO BE RECONSTRUCTED
- T** TELEPHONE TO BE ADJUSTED BY OTHERS
- CE** COM ED TO BE ADJUSTED BY OTHERS

NOTES

1. REFER TO TYPICAL SECTIONS FOR LOCATIONS AND DISTRICT ONE (BD33) (HMA TAPER AT EDGE OF PCC PAVEMENT).
2. REBUILD EXISTING HANDHOLE TO HEAVY DUTY HANDHOLE.
3. REBUILD EXISTING HANDHOLE.
4. NO PAVEMENT PATCHING SHALL BE CONDUCTED WITHIN THE DRY LAND BRIDGE LIMITS.



ROADWAY LEGEND

- PROPOSED HMA SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

FILE NAME = P:\2009\ME09006L\Var-Yar-Phl\CADD\W08_IL035\Sheet\0160J21-sh1-p1-n-01.dgn
 PLOT SCALE = 50.0000 / IN.
 USER NAME = Millennium Professional Services



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DRAWN -	CJD	REVISED -	
CHECKED -	RPD	REVISED -	
DATE -	2/17/2010	REVISED -	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 83 (SOUTH)
 ILLINOIS RTE 56 TO WASHINGTON ST**

ROADWAY PLAN

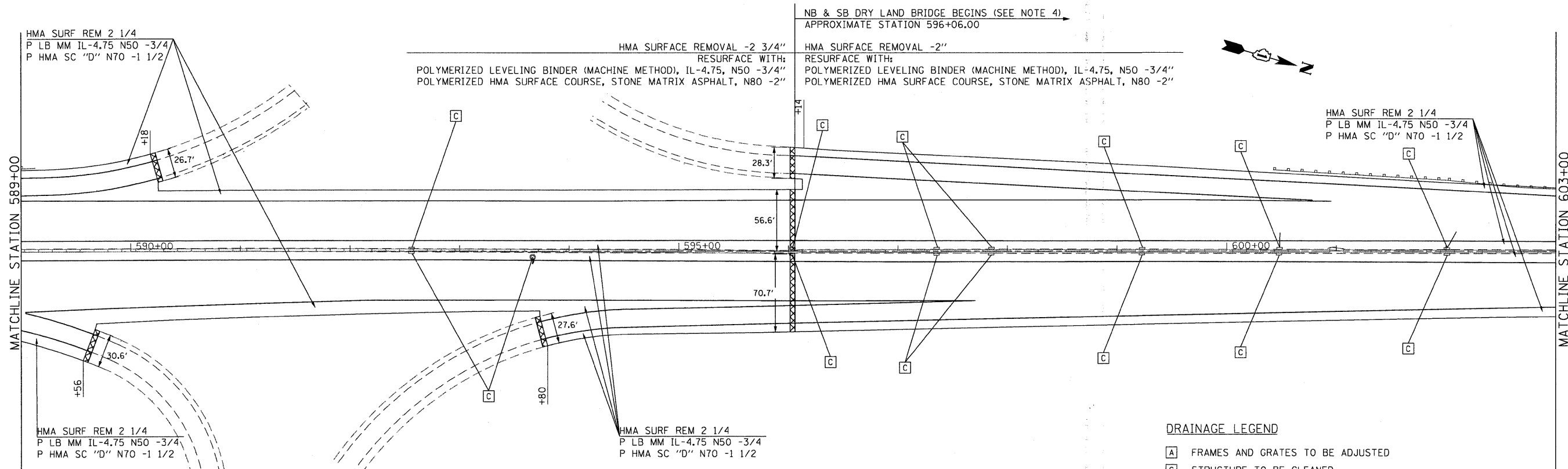
SCALE: 1"=100' SHEET NO. OF SHEETS STA. 566+73.76 TO STA. 589+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	6
CONTRACT NO. 60J21				

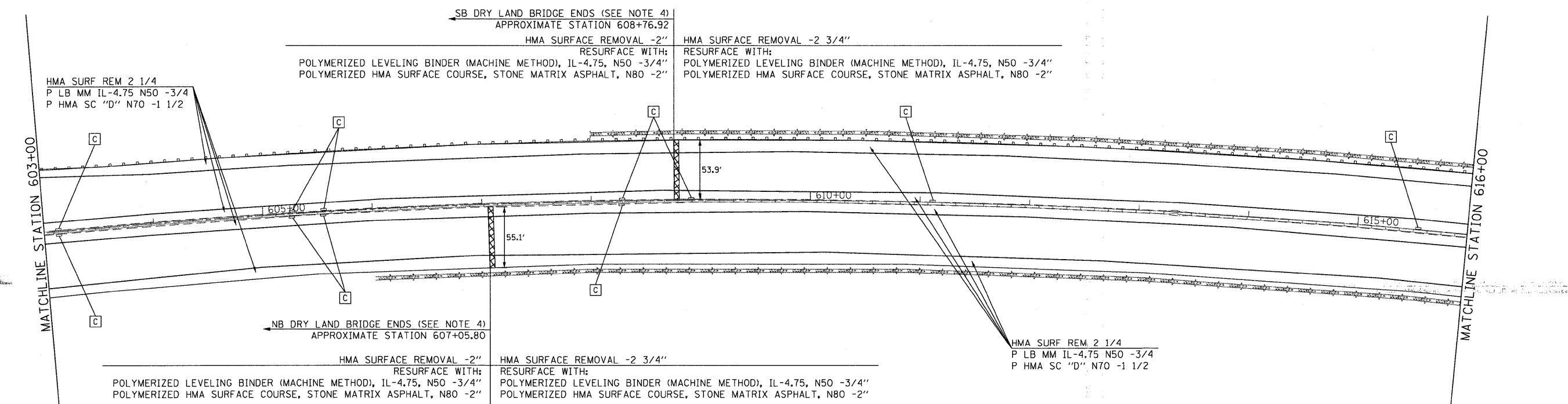
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

PLN-01

P:\2009\ME09006L\Var-Yar-Phl\CADD\W08_IL035\Sheet\0160J21-sh1-p1-n-01.dgn



- DRAINAGE LEGEND**
- [A] FRAMES AND GRATES TO BE ADJUSTED
 - [C] STRUCTURE TO BE CLEANED
 - [SP] FRAMES AND LIDS TO BE ADJUSTED SPECIAL
 - [REC] STRUCTURE TO BE RECONSTRUCTED
 - [T] TELEPHONE-TO BE ADJUSTED BY OTHERS
 - [CE] COM ED-TO BE ADJUSTED BY OTHERS



- NOTES**
- REFER TO TYPICAL SECTIONS FOR LOCATIONS AND DISTRICT ONE (BD33) (HMA TAPER AT EDGE OF PCC PAVEMENT).
 - REBUILD EXISTING HANDHOLE TO HEAVY DUTY HANDHOLE.
 - REBUILD EXISTING HANDHOLE.
 - NO PAVEMENT PATCHING SHALL BE CONDUCTED WITHIN THE DRY LAND BRIDGE LIMITS.

- ROADWAY LEGEND**
- [Hatched Box] PROPOSED HMA SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

FILE NAME : P:\2009\ME09006_VerVer_PlanCAD\MOB_IL83S_Shas\0160.121-shs-pln-02.dgn
 PLOT SCALE : 50.0000 / 1 IN.
 USER NAME : Millennium Professional Services

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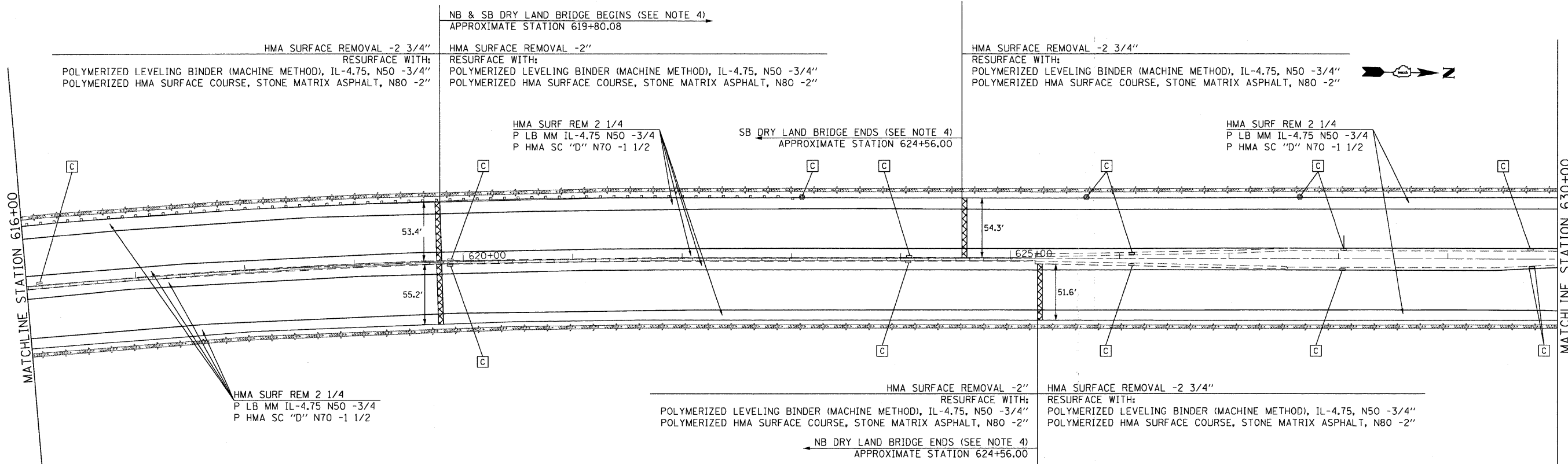
DESIGNED - CJD	REVISED -
DRAWN - CJD	REVISED -
CHECKED - RPD	REVISED -
DATE - 2/17/2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 83 (SOUTH)		ROADWAY PLAN	
ILLINOIS RTE 56 TO WASHINGTON ST			
SCALE: 1"=100'	SHEET NO. OF SHEETS	STA. 589+00 TO STA. 616+00	

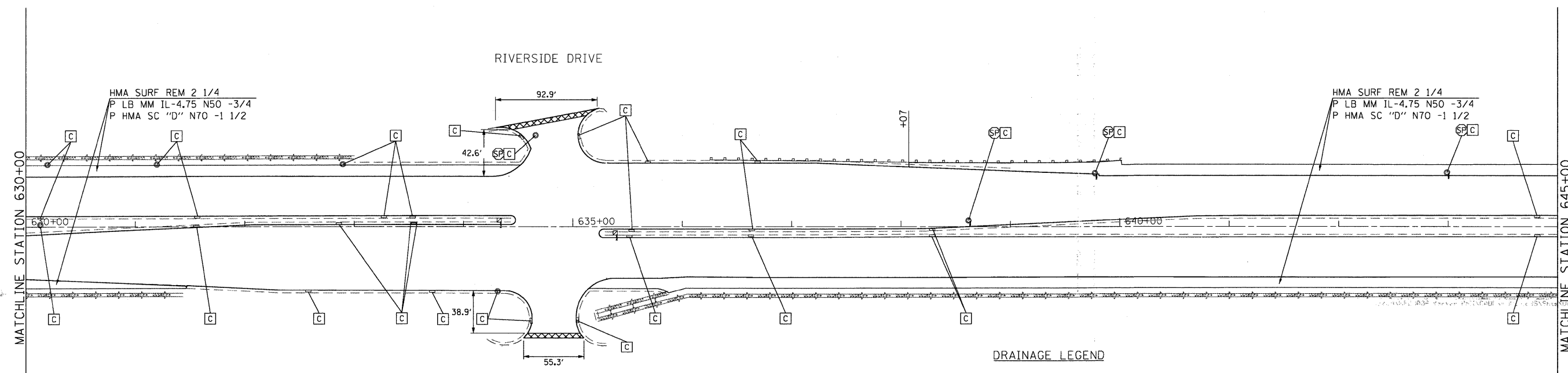
F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 7
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J21	

PLN-02



NOTES

1. REFER TO TYPICAL SECTIONS FOR LOCATIONS AND DISTRICT ONE (BD33) (HMA TAPER AT EDGE OF PCC PAVEMENT).
2. REBUILD EXISTING HANDHOLE TO HEAVY DUTY HANDHOLE.
3. REBUILD EXISTING HANDHOLE.
4. NO PAVEMENT PATCHING SHALL BE CONDUCTED WITHIN THE DRY LAND BRIDGE LIMITS.



ROADWAY LEGEND

PROPOSED HMA SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

DRAINAGE LEGEND

- FRAMES AND GRATES TO BE ADJUSTED
- STRUCTURE TO BE CLEANED
- FRAMES AND LIDS TO BE ADJUSTED SPECIAL
- STRUCTURE TO BE RECONSTRUCTED
- TELEPHONE-TO BE ADJUSTED BY OTHERS
- COM ED-TO BE ADJUSTED BY OTHERS

FILE NAME = P:\2009\MEP\006_Ver\Ver_Plan\CADD\M08_IL835\Shs\0160J21-ahc-pln-03.dgn
 PLOT SCALE = 1"=100'
 USER NAME = Millennium Professional Services



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MILLENNIA PROFESSIONAL SERVICES

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DRAWN - CJD	REVISED -
CHECKED - RPD	REVISED -
DATE - 2/17/2010	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 83 (SOUTH)
 ILLINOIS RTE 56 TO WASHINGTON ST**

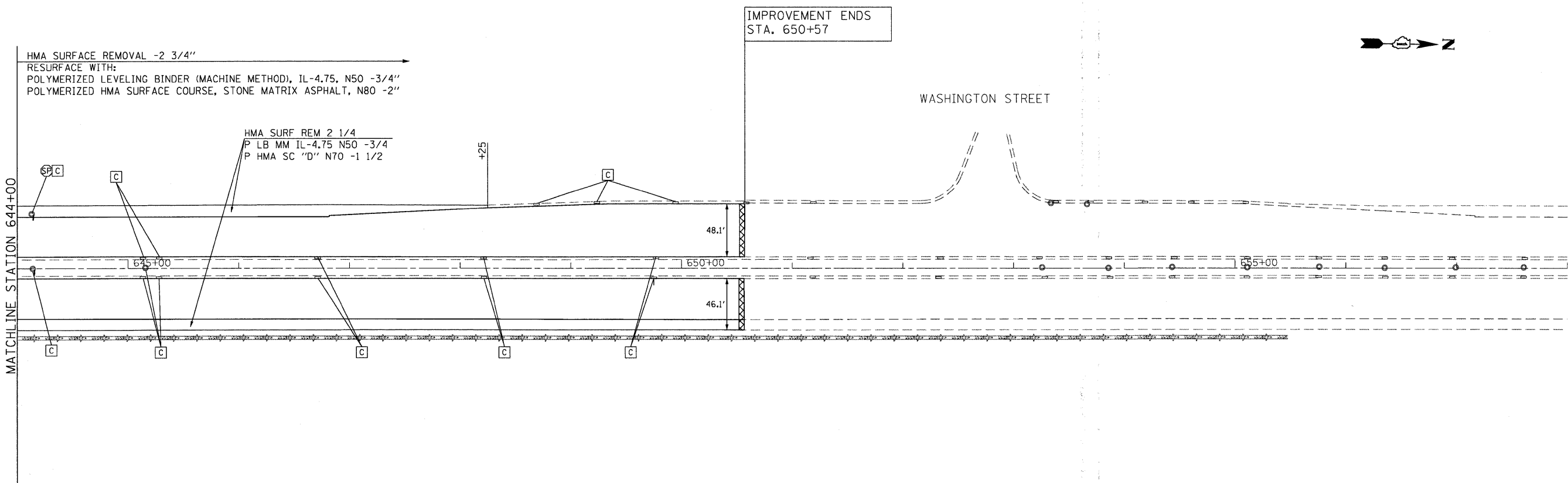
ROADWAY PLAN

SCALE: 1"=100' SHEET NO. OF SHEETS STA. 616+00 TO STA. 645+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	8
CONTRACT NO. 60J21				

PLN-03

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
 P:\2009\MEP\006_Ver\Ver_Plan\CADD\M08_IL835\Shs\0160J21-ahc-pln-03.dgn



MATCHLINE STATION 644+00

HMA SURFACE REMOVAL -2 3/4"
RESURFACE WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"


HMA SURF REM 2 1/4
P LB MM IL-4.75 N50 -3/4
P HMA SC "D" N70 -1 1/2







IMPROVEMENT ENDS
STA. 650+57

WASHINGTON STREET

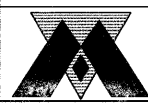
- NOTES**
- REFER TO TYPICAL SECTIONS FOR LOCATIONS AND DISTRICT ONE (BD33) (HMA TAPER AT EDGE OF PCC PAVEMENT).
 - REBUILD EXISTING HANDHOLE TO HEAVY DUTY HANDHOLE.
 - REBUILD EXISTING HANDHOLE.
 - NO PAVEMENT PATCHING SHALL BE CONDUCTED WITHIN THE DRY LAND BRIDGE LIMITS.

ROADWAY LEGEND

 PROPOSED HMA SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

- DRAINAGE LEGEND**
-  FRAMES AND GRATES TO BE ADJUSTED
 -  STRUCTURE TO BE CLEANED
 -  FRAMES AND LIDS TO BE ADJUSTED SPECIAL
 -  STRUCTURE TO BE RECONSTRUCTED
 -  TELEPHONE-TO BE ADJUSTED BY OTHERS
 -  COM ED-TO BE ADJUSTED BY OTHERS

FILE NAME = P:\2009\ME09006_Ver-Ver-Plt\CADD\WDR_IL03\Shas\0160J21-ah1-pln-04.dgn
PLOT SCALE = 1"=100'
USER NAME = Millennium Professional Services



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DATE -	2/17/2010	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 83 (SOUTH)
ILLINOIS RTE 56 TO WASHINGTON ST**

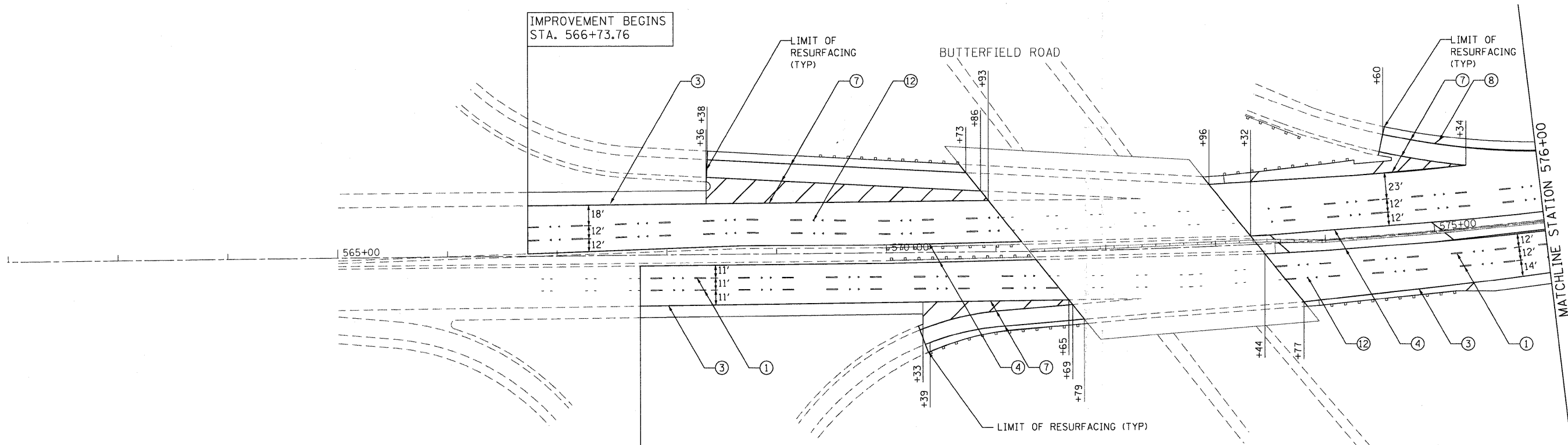
ROADWAY PLAN

SCALE: 1"=100' SHEET NO. OF SHEETS STA. 644+00 TO STA. 655+56.919

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	9
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J21	

PLN-04

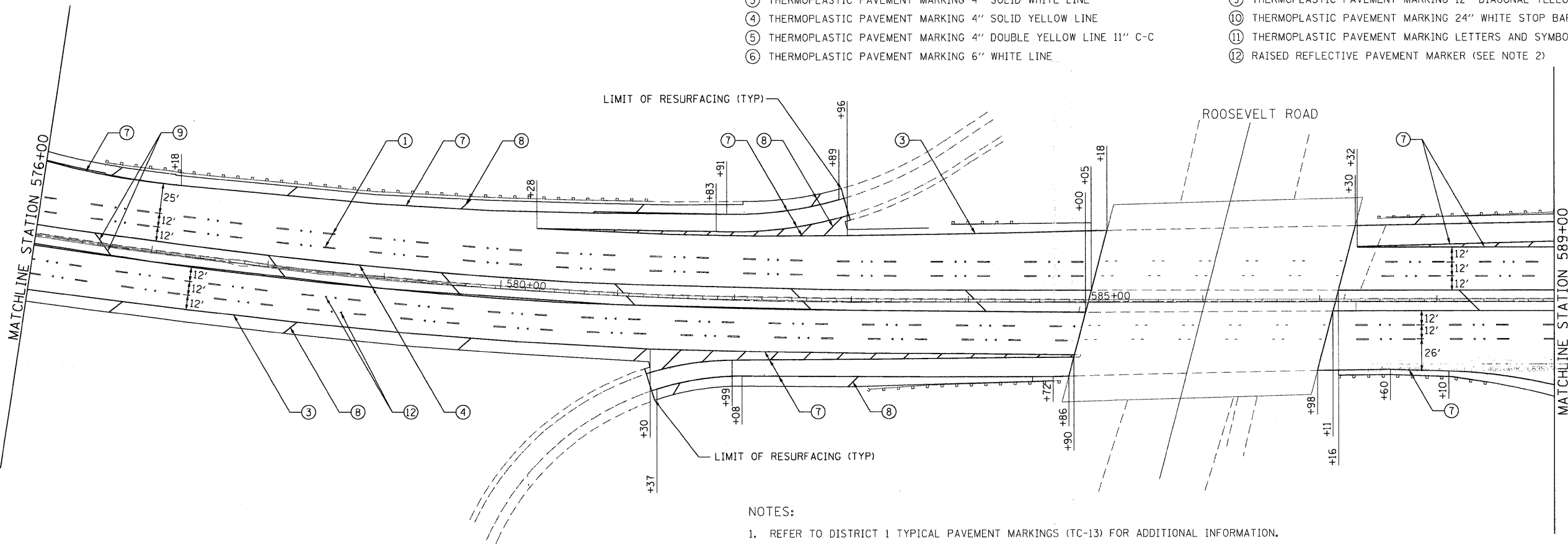
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IMPROVEMENT BEGINS
STA. 567+75.31

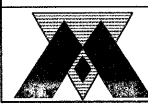
PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
- ⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)



- NOTES:
1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

FILE NAME = P:\2009\ME09006_Ver-Var_Plan\CADD\W08_IL835\Sheets\160121-ah-t-pmk-01.dgn
 PLOT SCALE = 50.0000 / 1 IN.
 USER NAME = Millennium Professional Services



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DRAWN - CJD	REVISED -
CHECKED - RPD	REVISED -
DATE - 2/23/2010	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

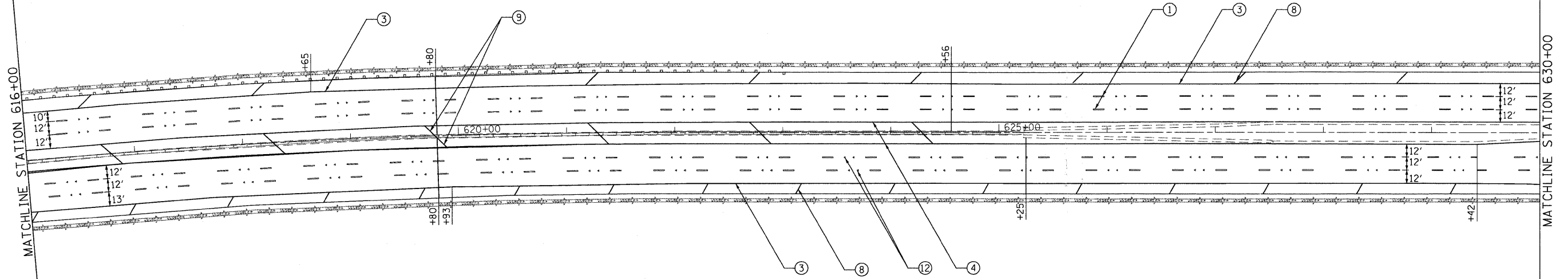
ILLINOIS ROUTE 83 (SOUTH)
ILLINOIS RTE 56 TO WASHINGTON ST
 SCALE: 1"=100' SHEET NO. OF SHEETS STA. 27+18 TO STA. 589+00

PAVEMENT MARKING PLAN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	10
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J21	

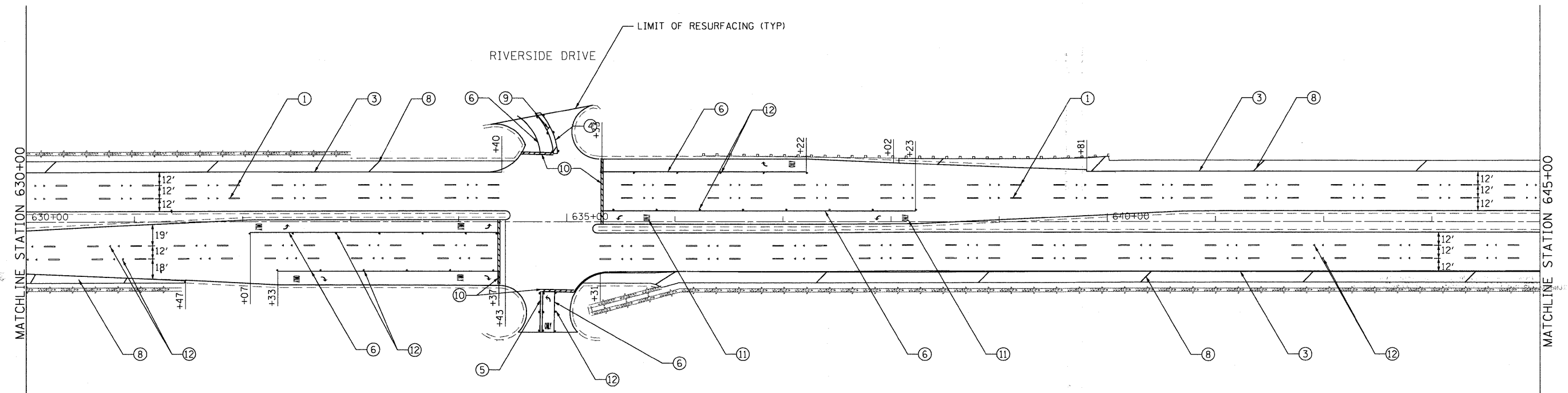
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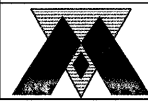
PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
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- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
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- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
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- ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)



- NOTES:
1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

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 PLOT SCALE = 50.0000 - 7 IN.
 USER NAME = Millennium Professional Services



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DRAWN - CJD	REVISED -
CHECKED - RPD	REVISED -
DATE - 2/23/2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 83 (SOUTH)
ILLINOIS RTE 56 TO WASHINGTON ST
 SCALE: 1"=100' SHEET NO. OF SHEETS STA. 616+00 TO STA. 645+00

PAVEMENT MARKING PLAN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	12
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J21	

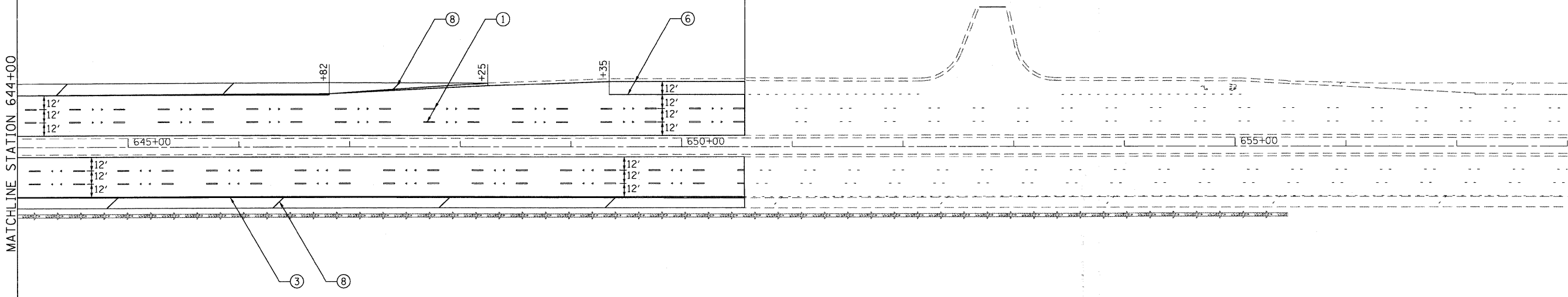
PMK-03

P:\2009\ME09006_Ver-Vor-Ph\IN\CADD\W08_IL835\Shas\0160121-shr-pmk-03.dgn

IMPROVEMENT ENDS
STA. 655+56.919

WASHINGTON STREET

MATCHLINE STATION 644+00



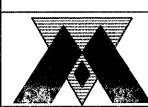
PAVEMENT MARKING LEGEND (SEE NOTE 1)

- | | |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH) | ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE |
| ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH) | ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE |
| ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE | ⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE |
| ④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE | ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR |
| ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C | ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE | ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2) |

NOTES:

- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
- REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

FILE NAME = PAZ2009\ME09006-Var-Ver-Philly\CADD\W08_IL035\Shets\DI60J21-plt-pmk-04.dgn
 PLOT SCALE = 50.0000 / IN.
 USER NAME = Millennium Professional Services



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CHECKED - RPD	REVISED -
DATE - 2/23/2010	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 83 (SOUTH)
 ILLINOIS RTE 56 TO WASHINGTON ST**

PAVEMENT MARKING PLAN

SCALE: 1"=100' SHEET NO. OF SHEETS STA. 645+00 TO STA. 672+00

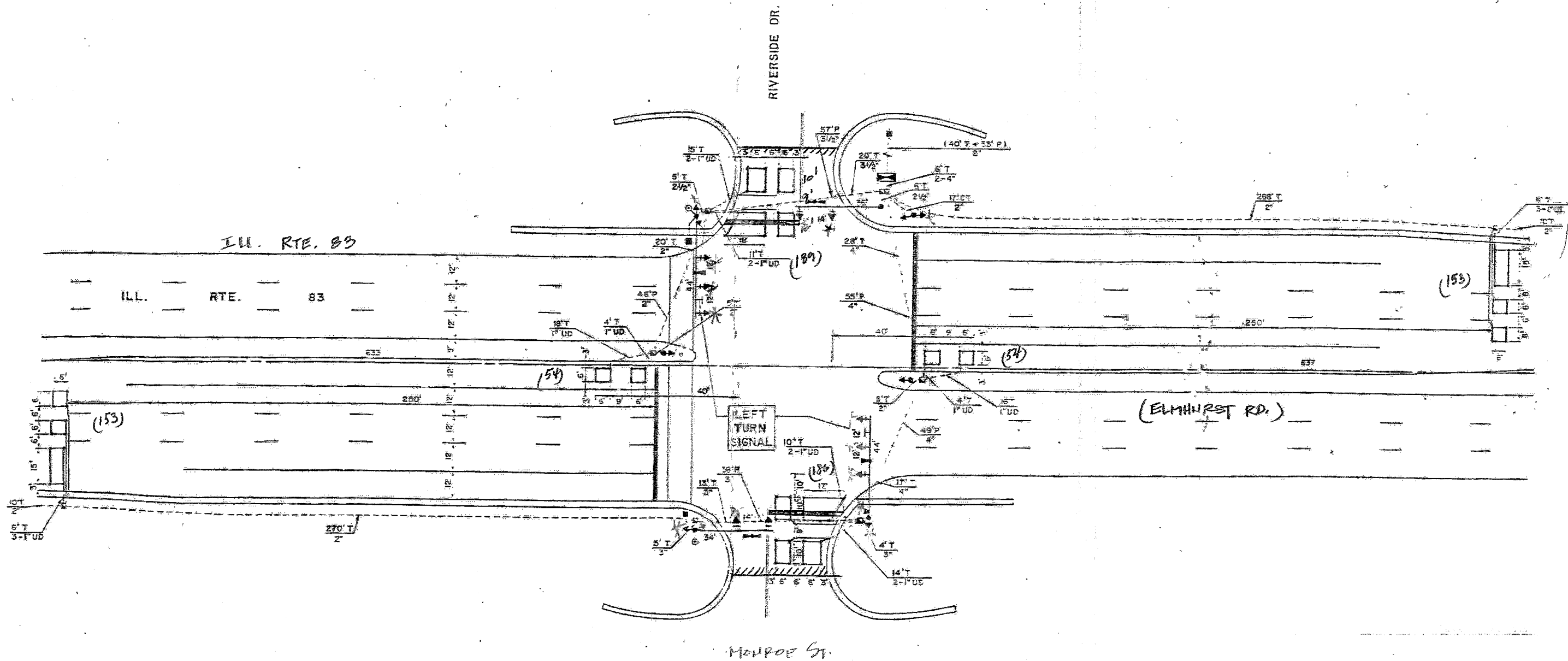
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	13
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J21	

PMK-04

PAZ2009\ME09006-Var-Ver-Philly\CADD\W08_IL035\Shets\DI60J21-plt-pmk-04.dgn

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		

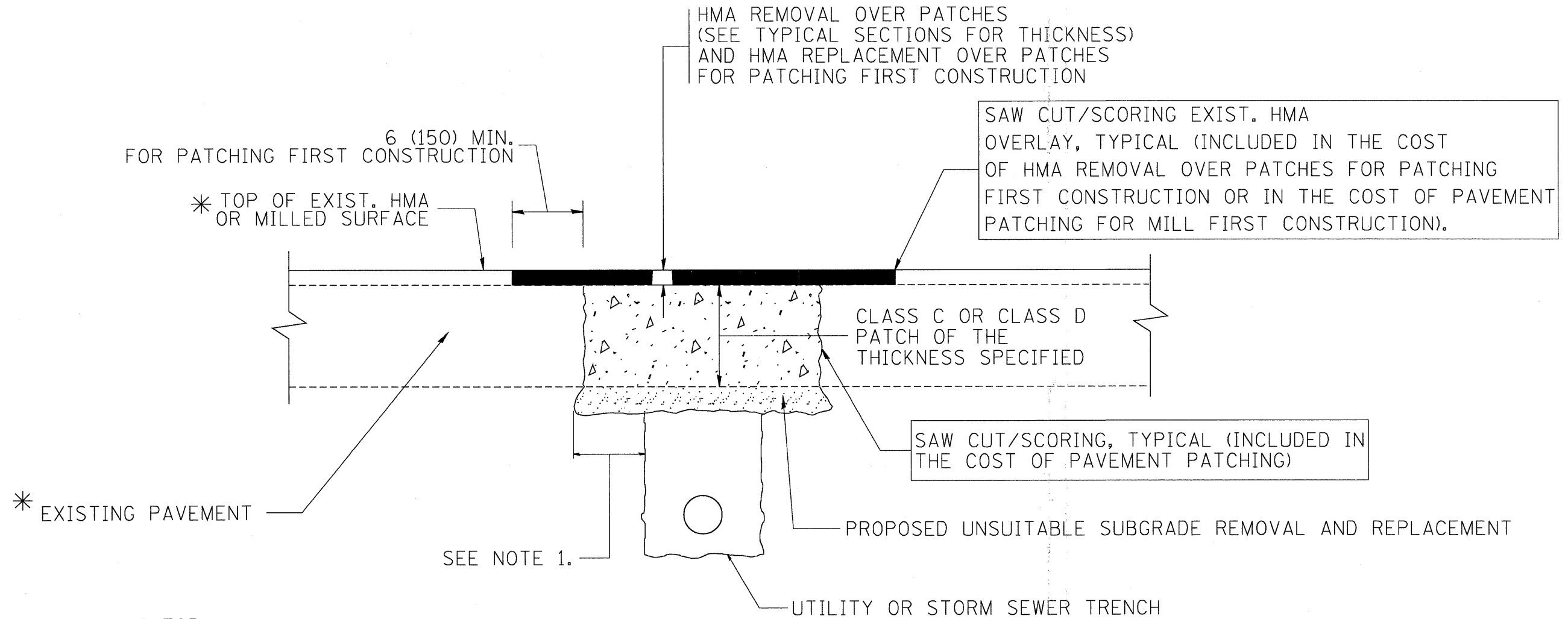


THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	789	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME = c:\pwwork\VP\DOT\KANTHAPHIXAYBC\d01126\1\trf\Fic.Legend.v7.dgn	USER NAME = kanthaphixaybc	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 83 @ RIVERSIDE DR.	F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY Du Page	TOTAL SHEETS 27	SHEET NO. 14
	PLOT SCALE = 3/4" = 1'	CHECKED - DAD	REVISED -			CONTRACT NO. 60J21				
	PLOT DATE = 4/3/2009	DATE -	REVISED -			SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.				
						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bouerd1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 16
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD400-04 (BD-22)				
		PLOT SCALE = 50.000' / IN.	REVISED - R. BORO 09-04-07		CONTRACT NO. 60J21							
		PLOT DATE = 10/27/2008	REVISED - K. ENG 10-27-08		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

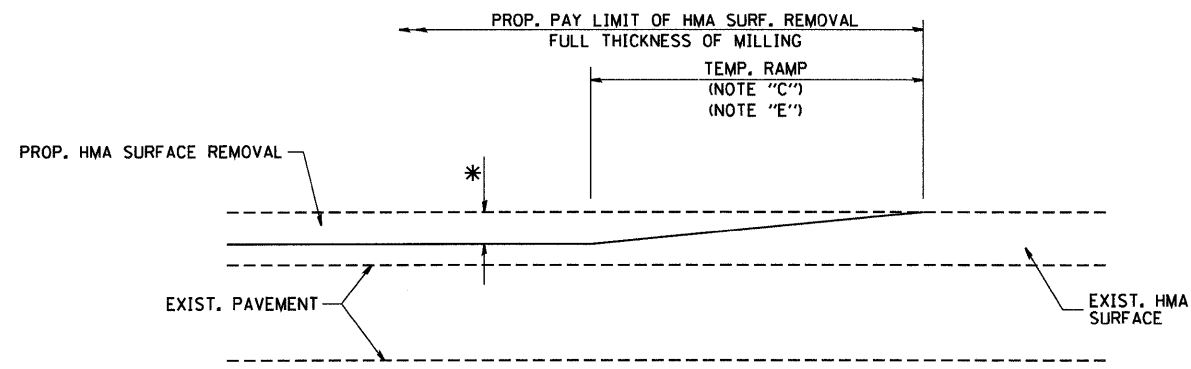
EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

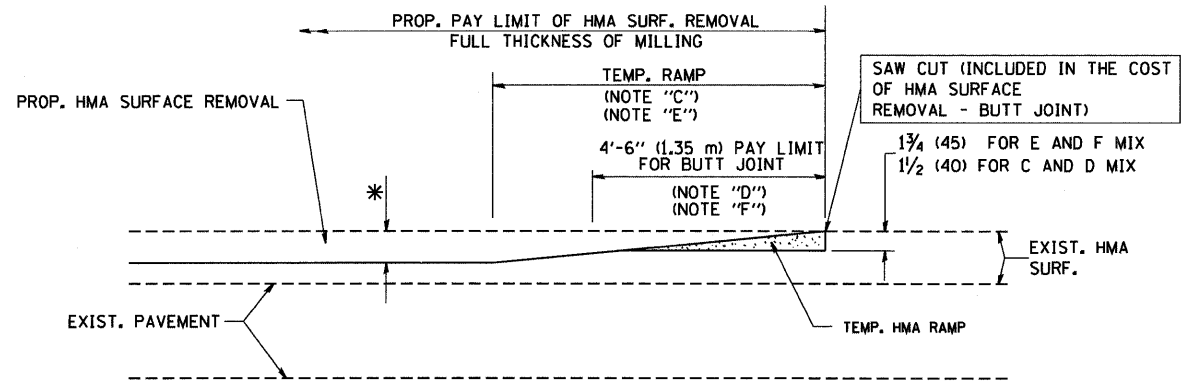
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr\pwork\pwork\drivakosgn\d0108315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		344	543 R-1-RS	DUPAGE	27	17			
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)			CONTRACT NO. 60J21				
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

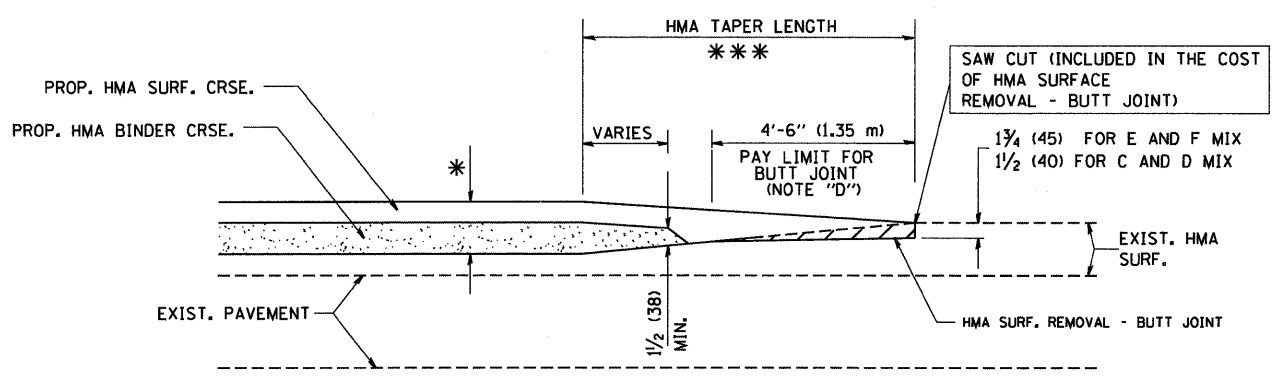
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

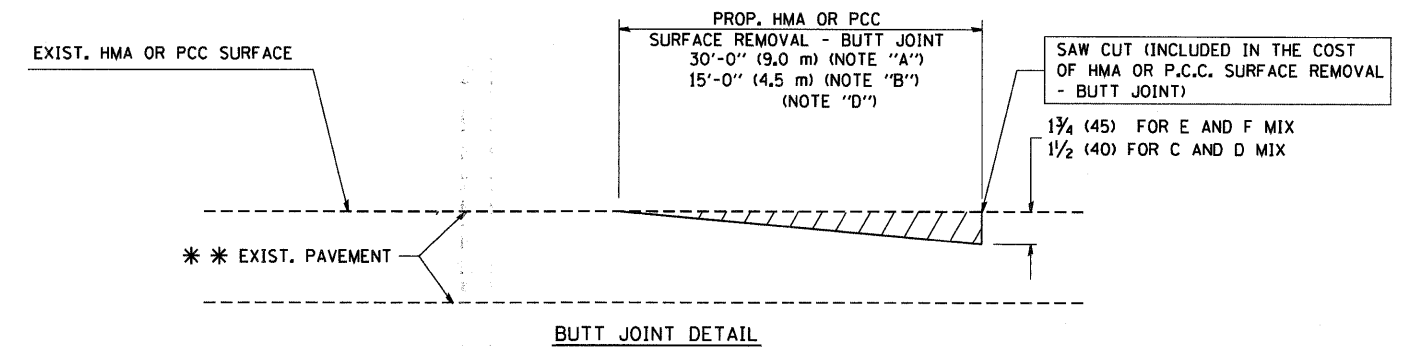
OPTION 2

TYPICAL TEMPORARY RAMP

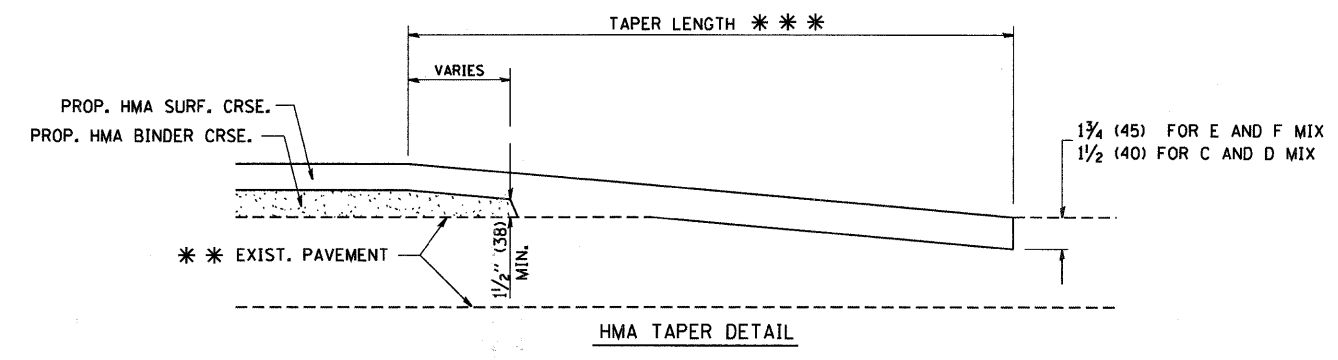


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

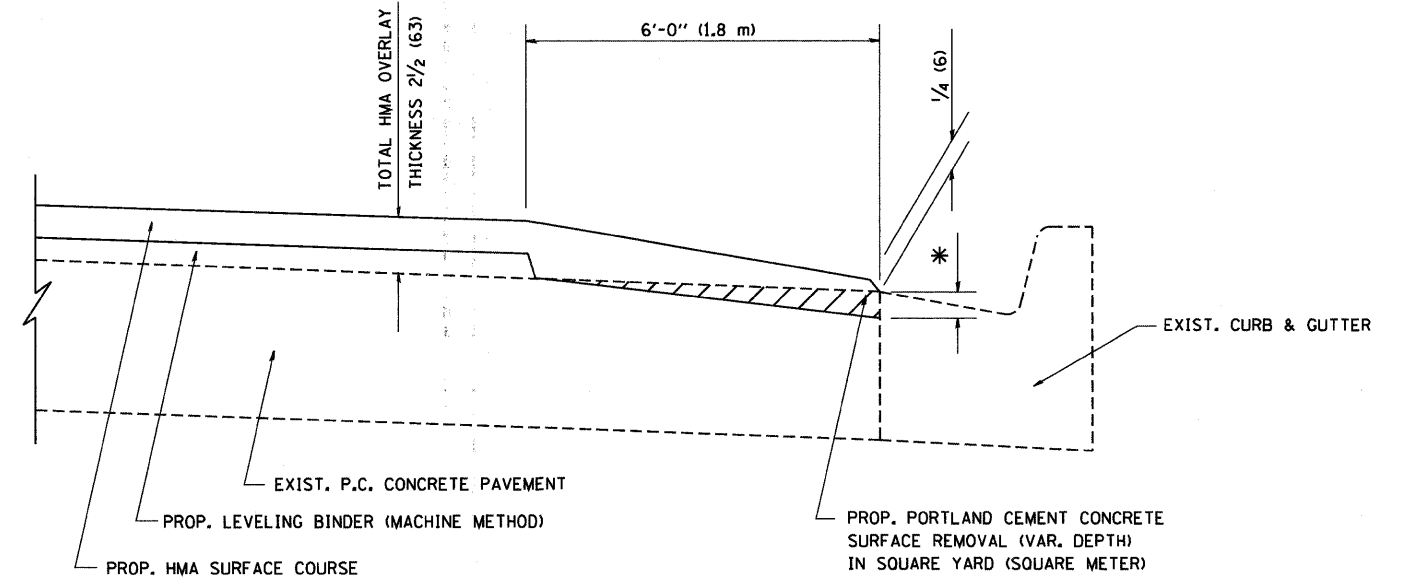
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USER NAME = gaglienob	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
		REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	18
BD400-05 BD32			CONTRACT NO. 60J21	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**HMA TAPER AT
EDGE OF P.C.C. PAVEMENT**

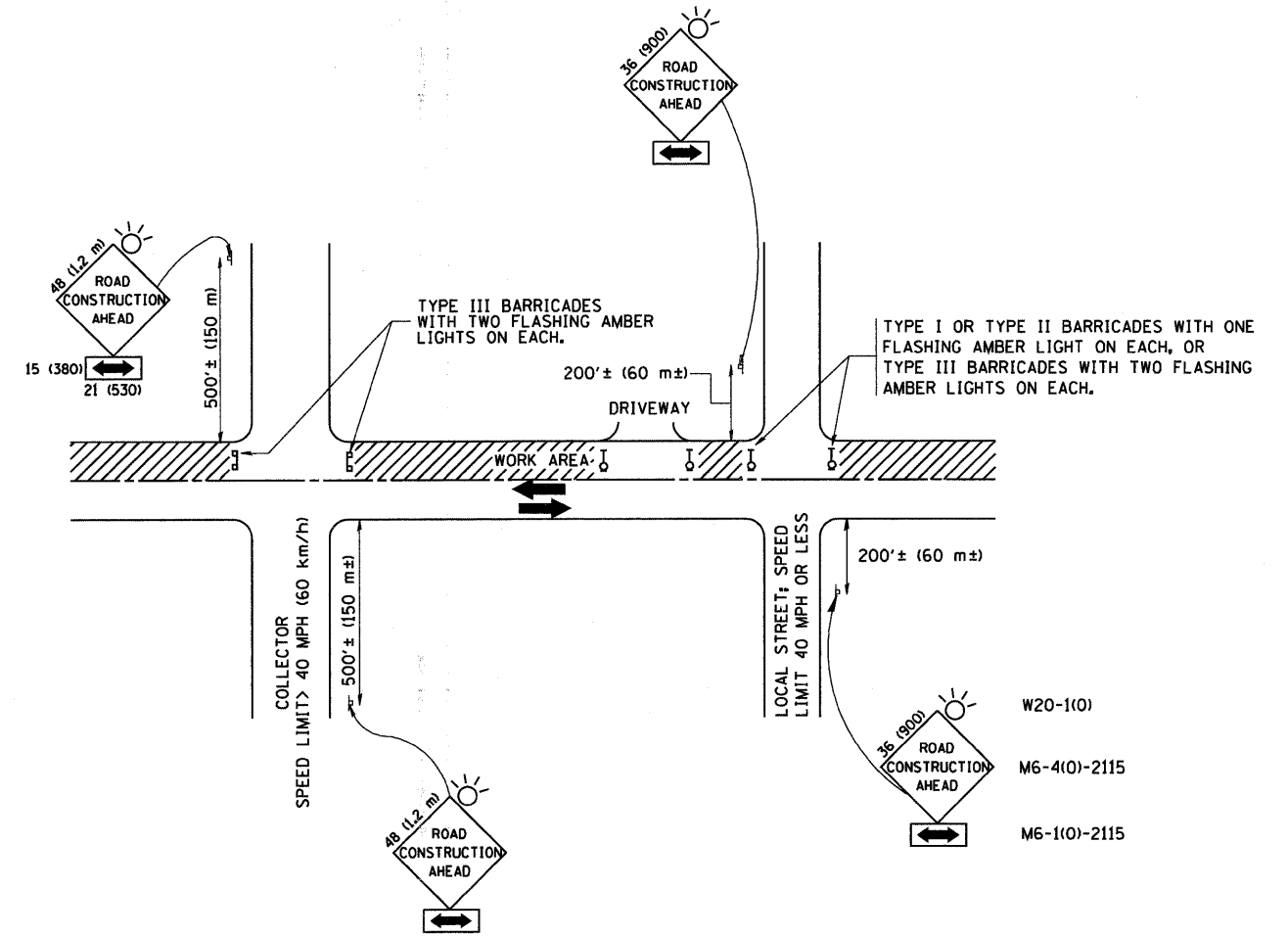
HMA SURFACE MIX	THICKNESS	LEVELING BINDER THICKNESS	* MILLING AT GUTTER FLAG
			C OR D
F	1 3/4 (44)	3/4 (19)	1/2 (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distatd\22x34\bd33.dgn	USER NAME = gaglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 10-25-94
		DRAWN - JIS	REVISED - A. ABBAS 05-05-99
	PLOT SCALE = 50.0000 / IN.	CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00
	PLOT DATE = 1/4/2008	DATE - 09-10-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HMA TAPER AT EDGE OF P.C.C. PAVEMENT		F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 19
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 60J21		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

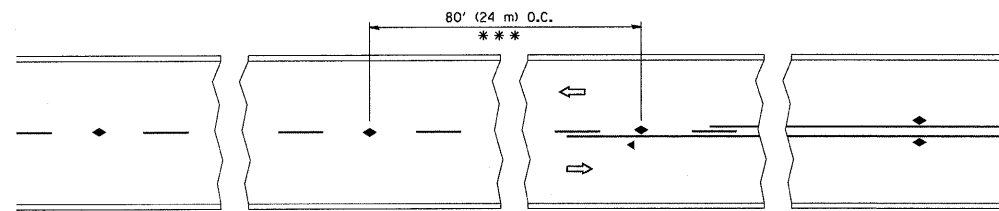
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

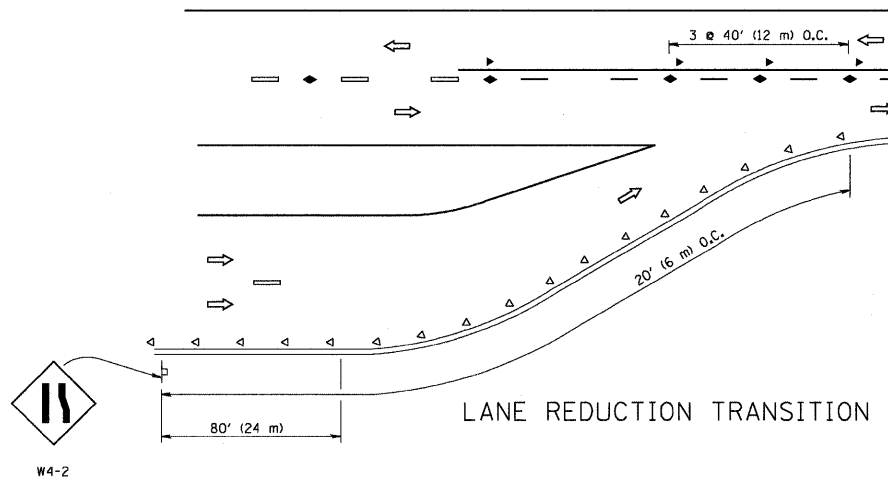
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\diststd\22x34\to10.dgn	USER NAME = gog1anob1	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - A. HOUSEH 03-06-96		344	543 R-1-RS	DUPAGE	27	20			
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - A. HOUSEH 10-15-96		TC-10			CONTRACT NO. 60J21				
	DATE - 06-89	DATE -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

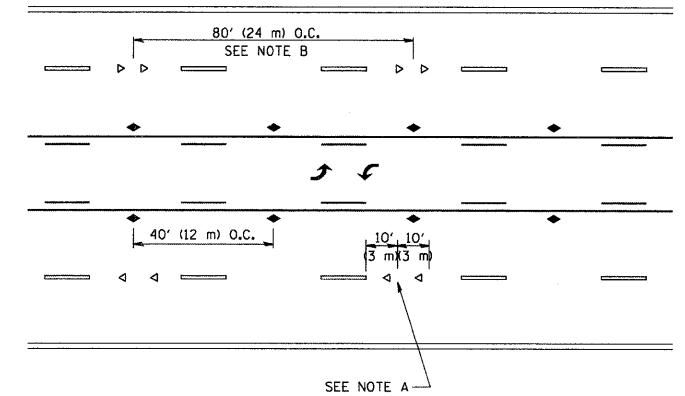


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

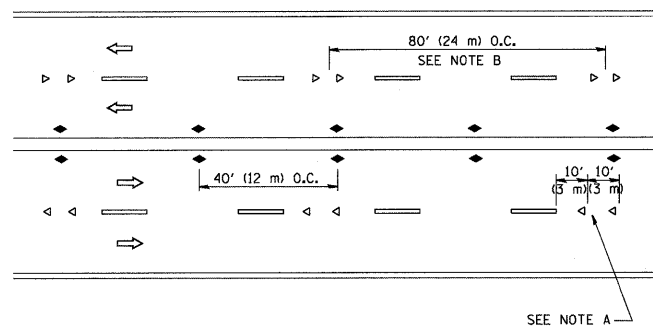
TWO-LANE/TWO-WAY



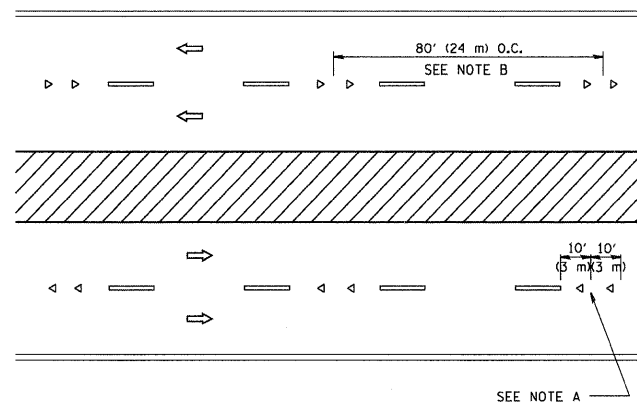
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

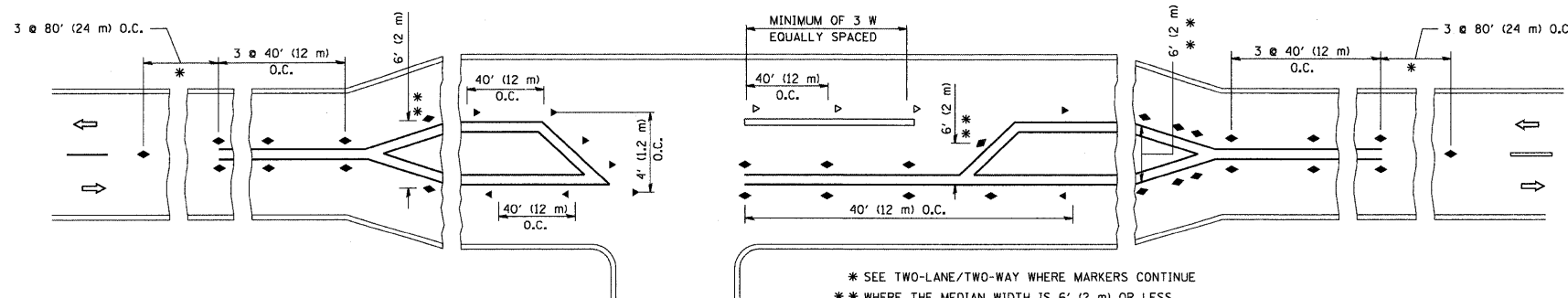
- YELLOW STRIPE
- ▬ WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



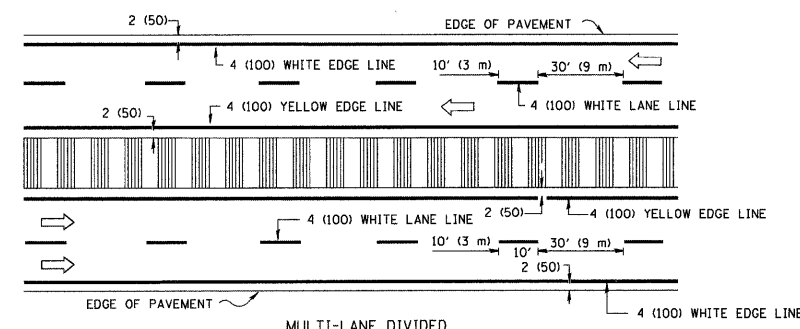
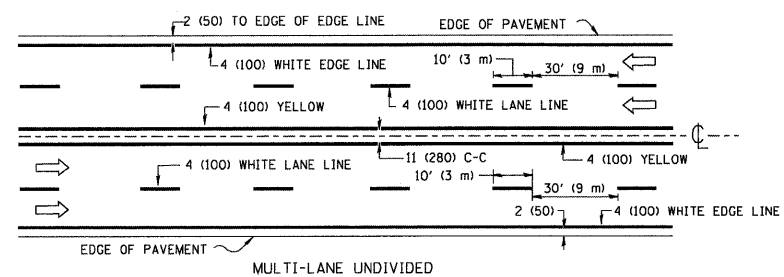
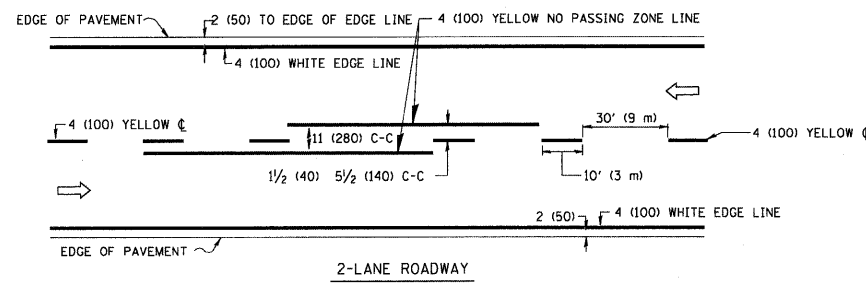
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
...	...	DRAWN -	REVISED - T. RAMMACHER 03-12-99
...	...	CHECKED -	REVISED - T. RAMMACHER 01-06-00
...	...	DATE -	REVISED - C. JUCIUS 09-09-09

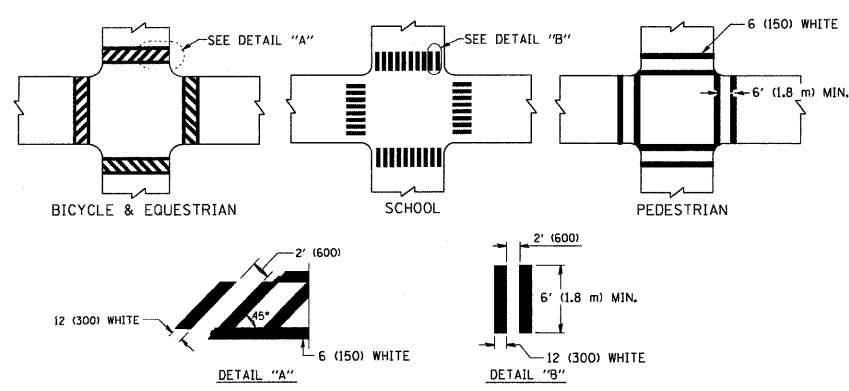
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		344	543 R-1-RS	DUPAGE	27	21
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
		TC-11		CONTRACT NO. 60J21		

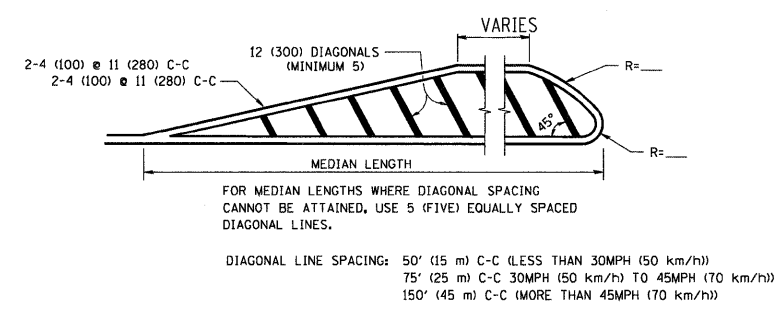
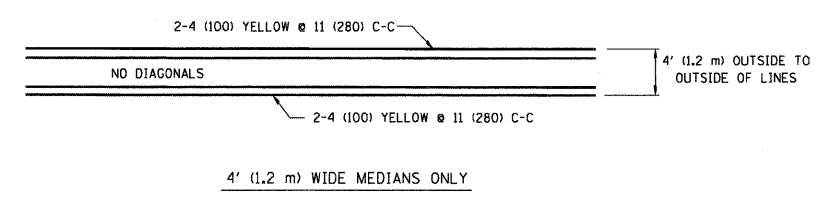


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

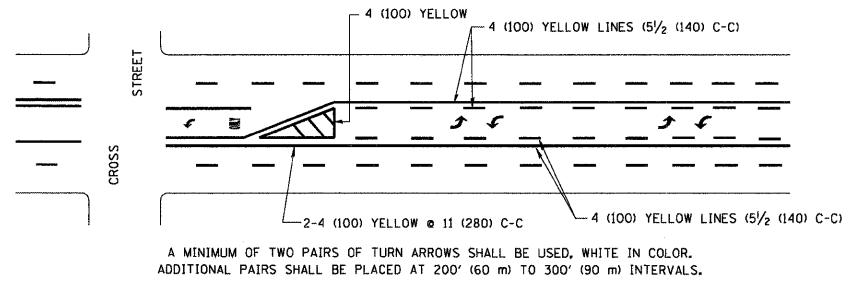
TYPICAL LANE AND EDGE LINE MARKING



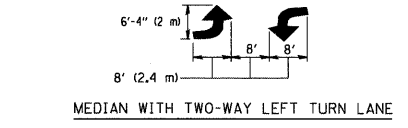
TYPICAL CROSSWALK MARKING



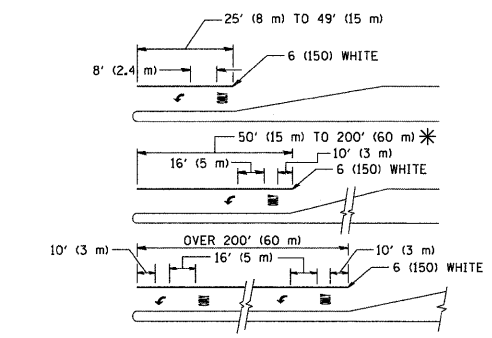
MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



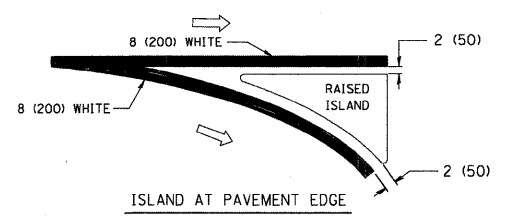
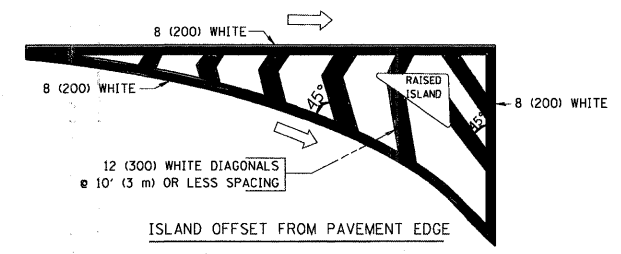
MEDIAN WITH TWO-WAY LEFT TURN LANE



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

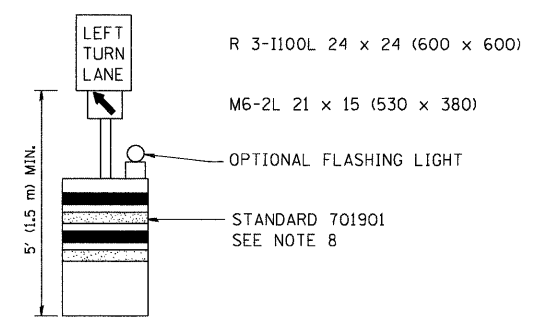
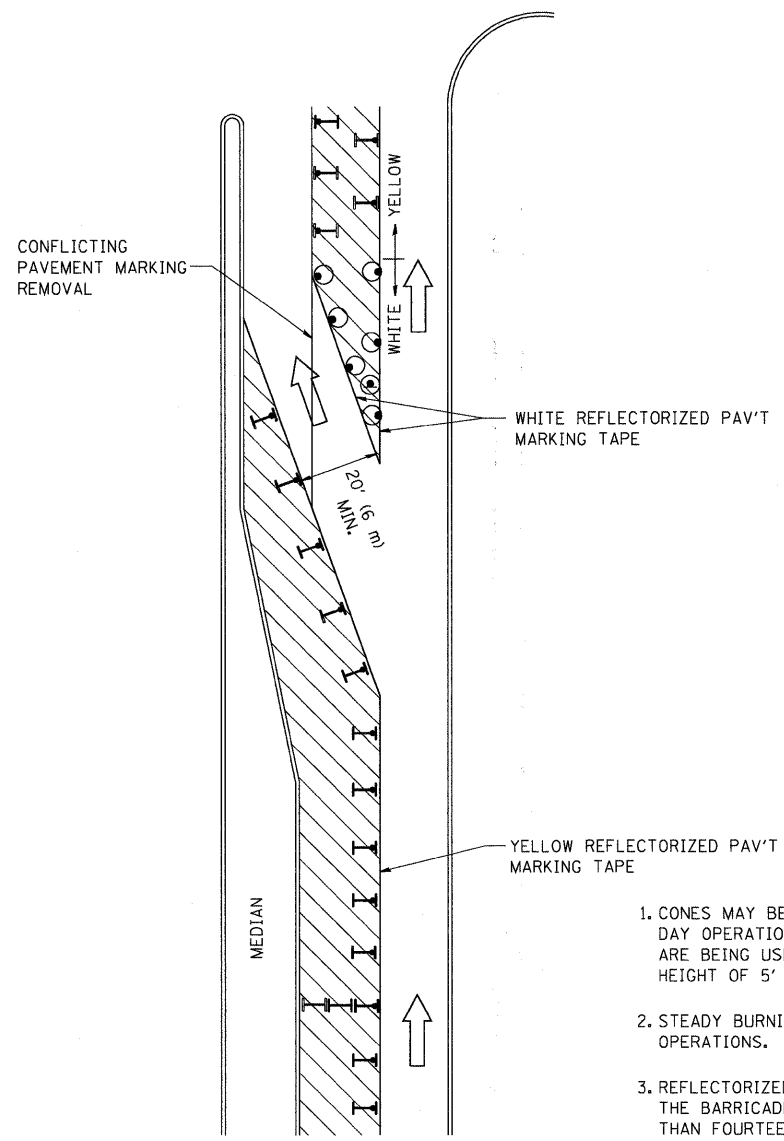
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	22
TC-13		CONTRACT NO. 60J21		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

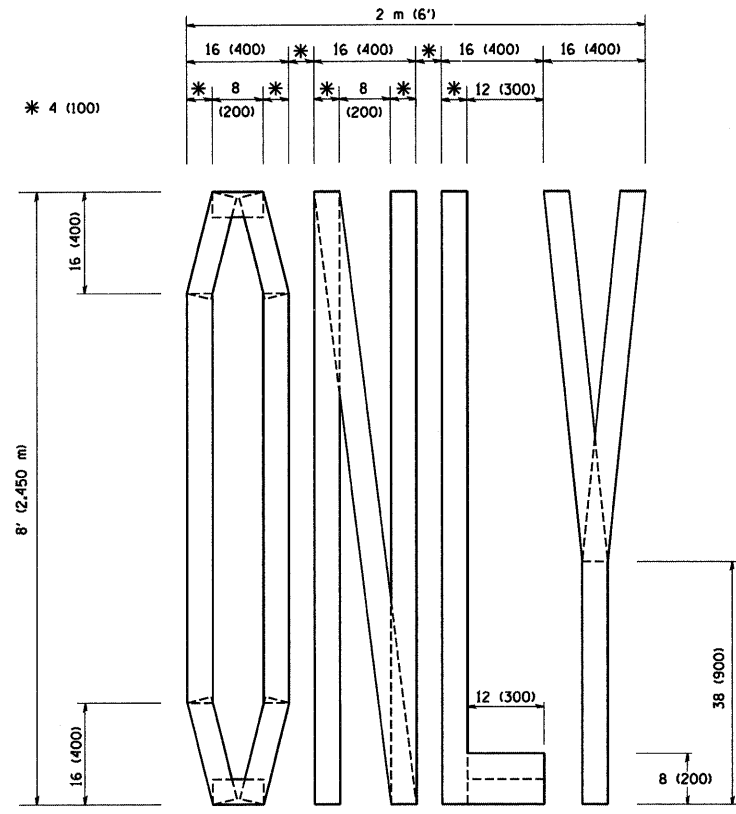
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	PLOT SCALE = 49.9999 // IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

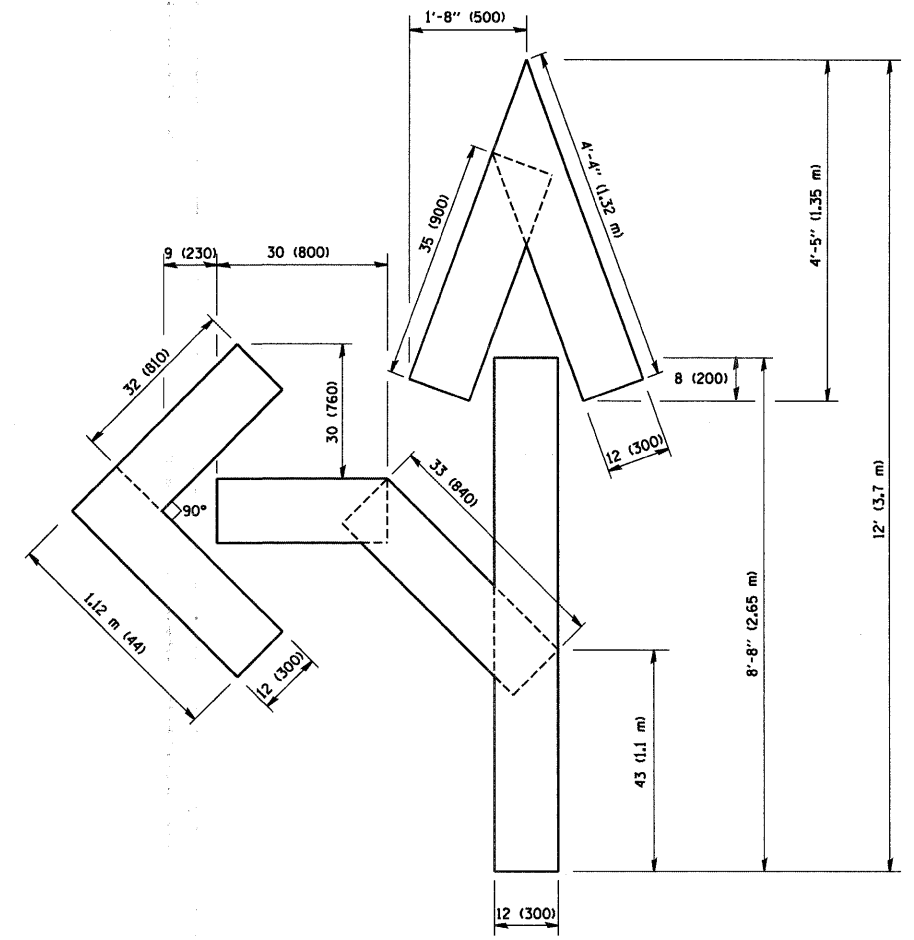
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

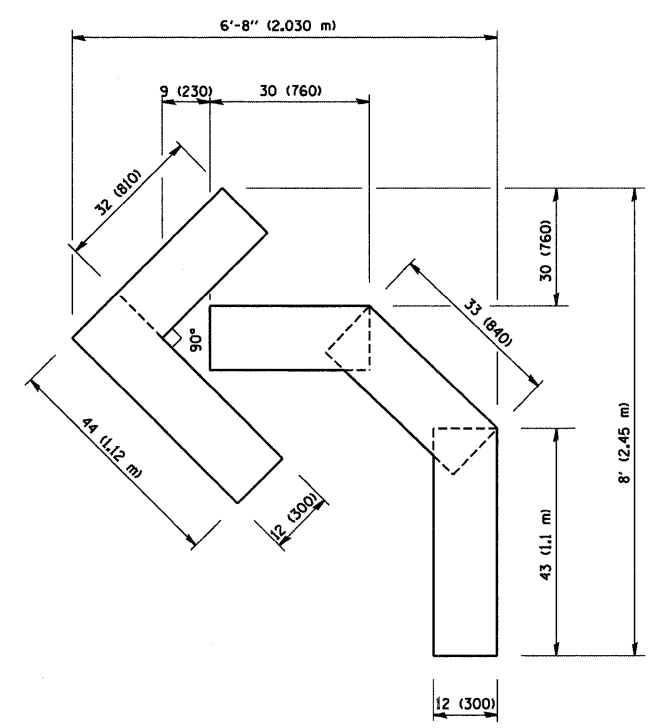
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	23
TC-14		CONTRACT NO. 60J21		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

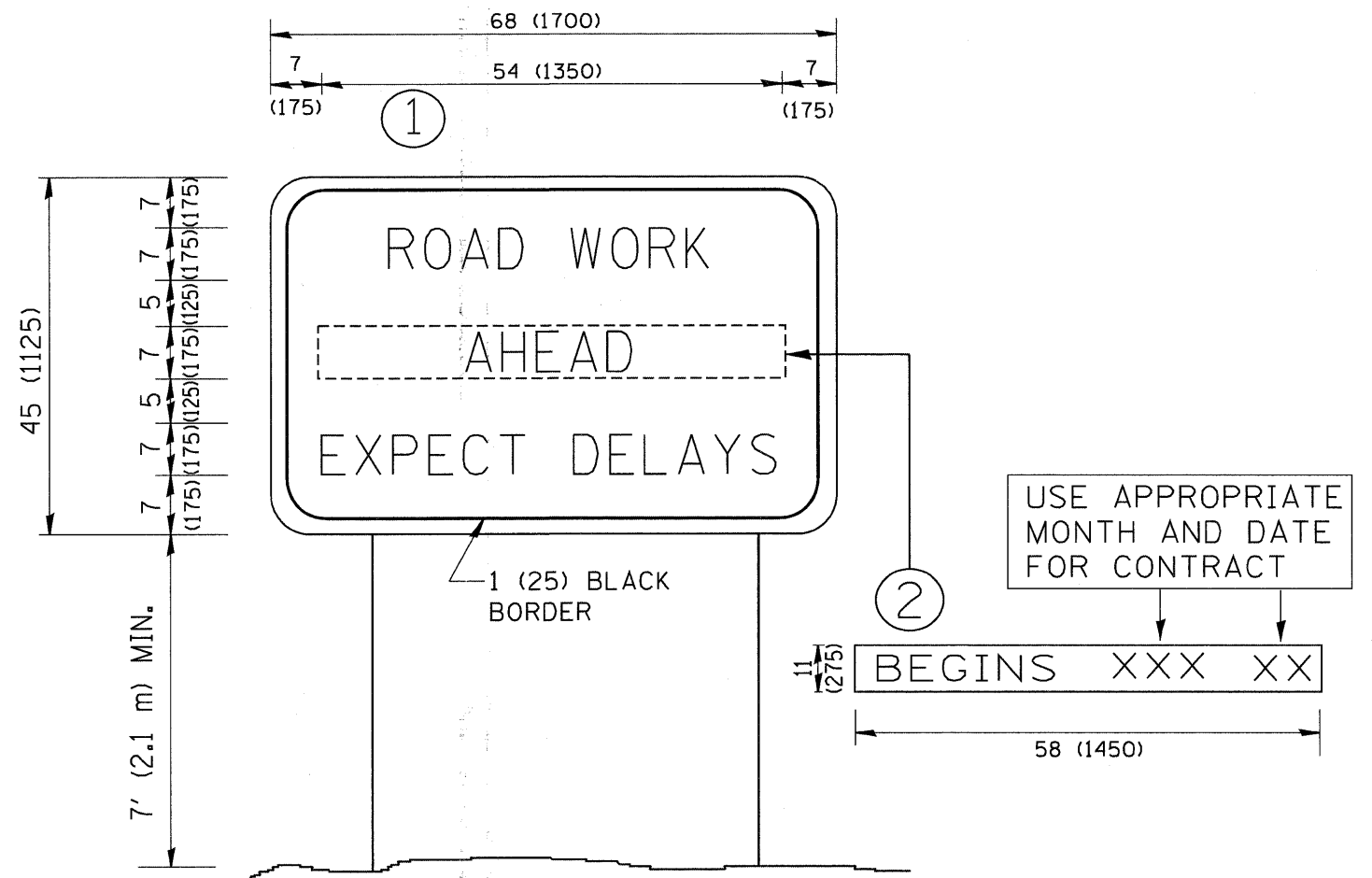
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	PLOT SCALE = 50.0000" / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 24
TC-16		CONTRACT NO. 60J21		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

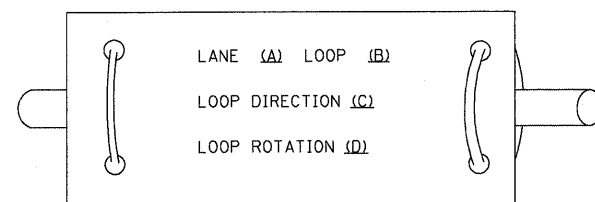
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		CHECKED -	REVISED - T. RAMMACHER 02-02-99		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE -	REVISED - C. JUCIUS 01-31-07									

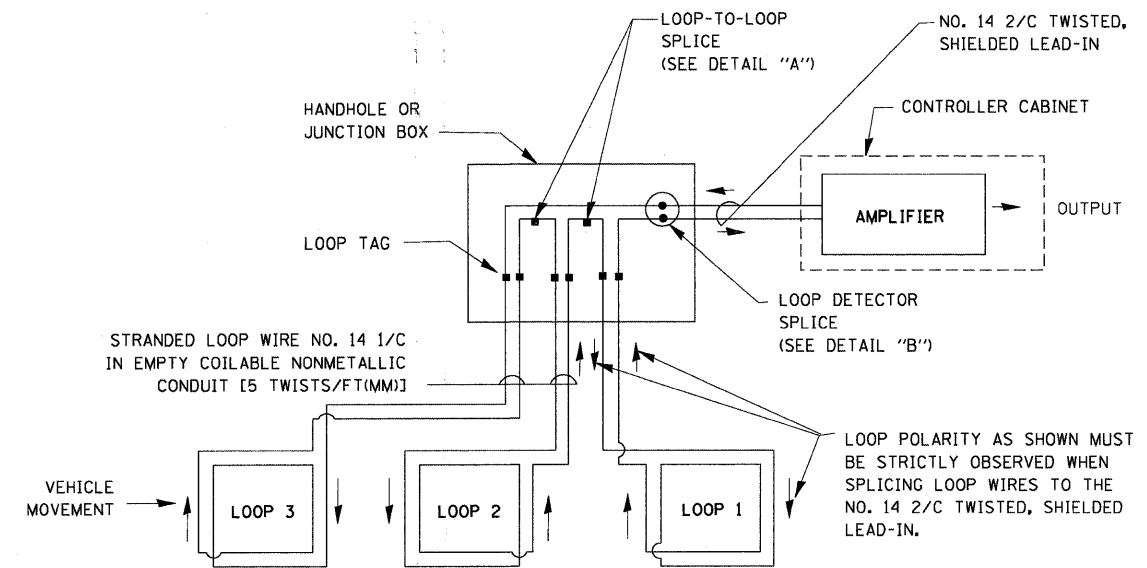
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

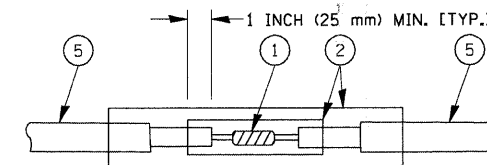


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

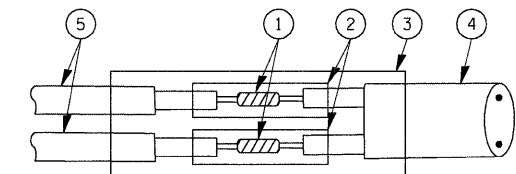


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

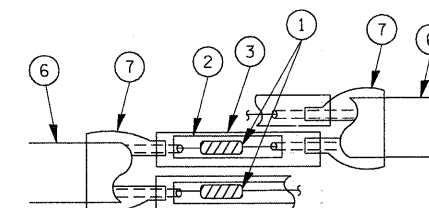


DETAIL "A"
LOOP-TO-LOOP SPLICE

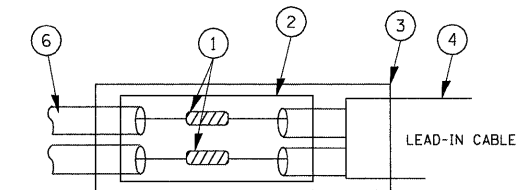


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

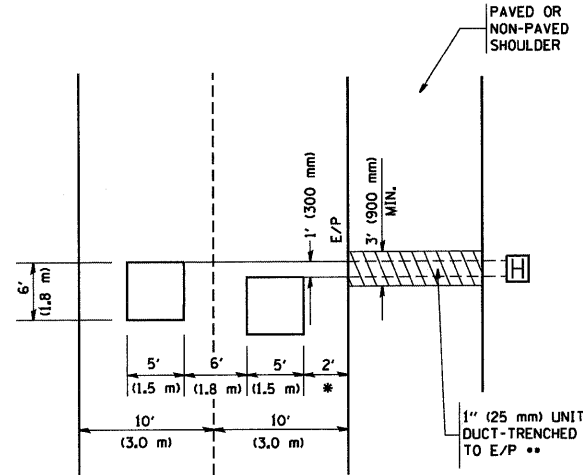
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.P. RTE. 344	SECTION 543 R-1-RS	COUNTY DUPAGE	TOTAL SHEETS 27	SHEET NO. 26	
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PLOT DATE = 11/4/2009	REVISED -										

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

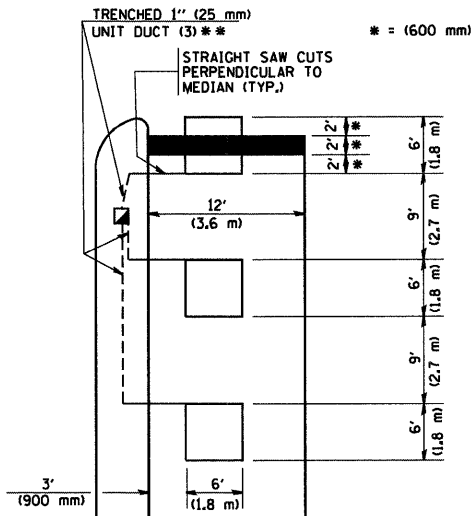


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

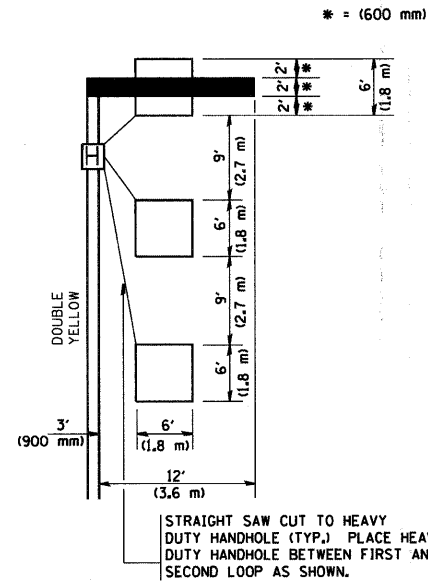
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

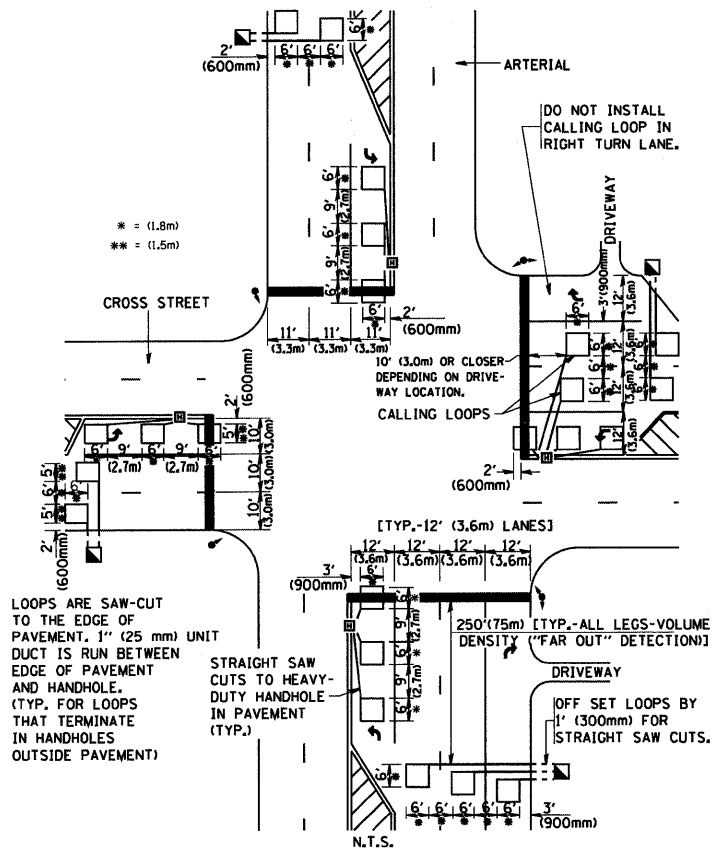
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



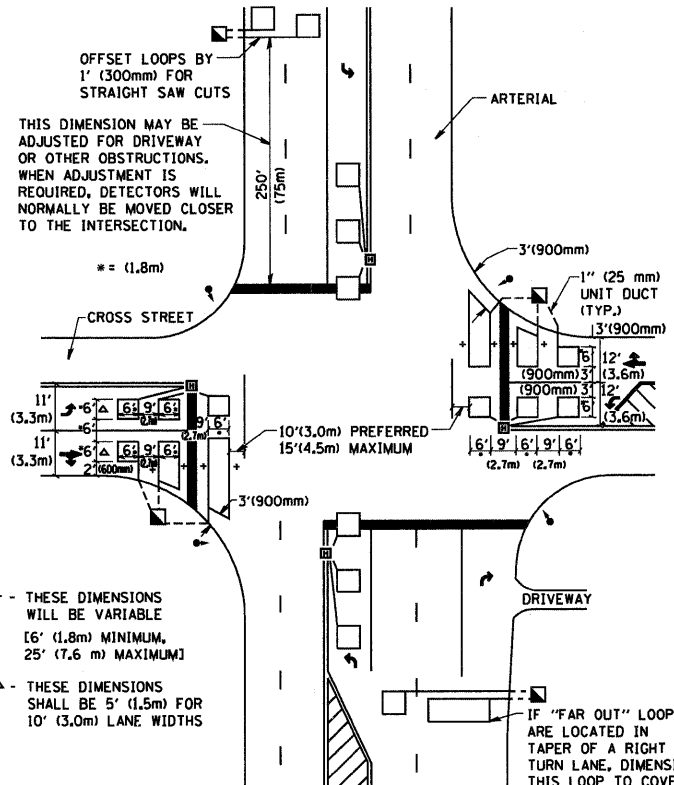
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22\34\ts07.dgn

USER NAME = gaglianob	DESIGNED -	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -
PLOT DATE = 1/4/2008	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543 R-1-RS	DUPAGE	27	27
TS-07			CONTRACT NO. 60J21	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				