4-23-10 LETTING ITEM 021

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	135RS-6	LAKE	28	1
	ILLINOIS	CONTRACT NO. 60J65		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

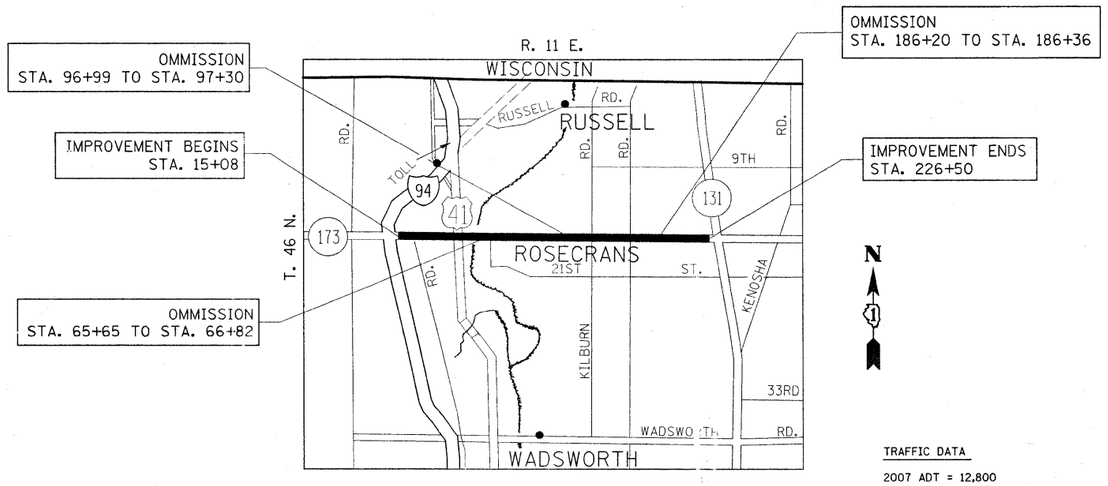
PROPOSED
HIGHWAY PLANS

F.A.P. ROUTE 303 (IL. RTE. 173)
0.1 MILE EAST OF I–94 TO 0.1 MILE WEST OF IL. 131
SECTION 136RS–6
RESURFACING (3P)

LAKE COUNTY
C–91–255–10

D–91–255–10

LOCATION OF SECTION INDICATED THUS: –



NEWPORT TOWNSHIP

TRAFFIC DATA

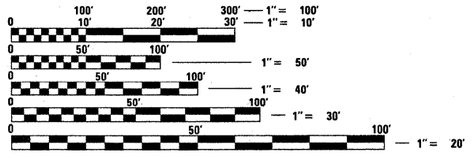
2007 ADT = 12,800
SPEED LIMIT = 45-55 MPH

LOCATION MAP

GROSS LENGTH = 21,142 FT. = 4.00 MILE
NET LENGTH = 20,978 FT. = 3.97 MILE

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT IS LOCATED IN THE VILLAGE OF WADSWORTH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 20, 2010
Diane M. O'Keefe, PE
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 19, 2010
Scott E. Stitt, P.E. /BD
Acting ENGINEER OF DESIGN AND ENVIRONMENT
March 19, 2010
Christine M. Reed, PE
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROJECT ENGINEER JENPAI CHANG 847–705–4432
PROJECT MANAGER KEN ENG

CONTRACT NO. 60J65

DISTRICT 1 DESIGN PLAN PREPARATION ENGINEER: KEN ENG / JENPAI CHANG (847) 705-4432

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, LIST OF STATE STANDARDS, AND PLAN NOTES
3	SUMMARY OF QUANTITIES
4, 5	TYPICAL CROSS SECTIONS
6-13	EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLANS
14-16	DETECTOR LOOP REPLACEMENT PLANS
17	DETAILS FOR FRAMES & LIDS ADJUSTMENT WITH MILLING
18	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
19	CURB & GUTTER REMOVAL & REPLACEMENT
20	BUTT JOINT AND HMA TAPER DETAIL
21	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
22	DETAILS FOR STEEL PLATE BEAM GUARDRAIL AT TBT TYPE 1 SPECIAL
23	TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
24	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
25	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
26	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
27	ARTERIAL ROAD INFORMATION SIGN
28	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

STANDARDS

STD. NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C & D PATCHES
606001-04	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
701011-02	OFF ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY FOR SPEED 45 MPH
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING, OPERATIONS DAYTIME ONLY FOR SPEED 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY
701336-05	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES FOR SPEED $\geq$45 MPH
701901-01	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATIONS

PLAN NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF WADSWORTH.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSIONS FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS," SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITTING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

THE RESIDENT ENGINEER SHALL CONTACT MS. DEBBIE HANLON, AREA TRAFFIC FIELD ENGINEER AT (847) 438-2300 A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

10 FEET TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL OVERHEAD, SURFACE, AND UNDERGROUND UTILITIES WITHIN THE PROJECT LIMITS WHETHER OR NOT THE UTILITES ARE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.

ALL PROPOSED DRIVEWAYS SHALL BE HOT-MIX ASPHALT UNLESS OTEHRWISE SPECIFIED AS PORTLAND CEMENT CONCRETE ON THE PLAN SHEETS.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TRAFFIC CONTROL DEVICES.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKINGS LINES (AND RAISED REFLECTIVE MARKINGS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 173
0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131 (GREEN BAY RD.)

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	2
		CONTRACT NO. 60J65		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

REV. 02-10-10

SUMMARY OF QUANTITIES

100% STATE

CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE I000					
20200100	EARTH EXCAVATION	CU YD	30	30					
20201006	GRADING AND SHAPING SHOULDERS	UNIT	420	420					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	56	56					
25200110	SODDING, SALT TOLERANT	SQ YD	56	56					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	73	73					
40600300	AGGREGATE (PRIME COAT)	TON	362	362					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	136	136					
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	3731	3731					
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	174	174					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	7597	7597					
42001300	PROTECTIVE COAT	SQ YD	84	84					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	90429	90429					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	250	250					
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	120	120					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	1500	1500					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	700	700					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	250	250					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1376	1376					
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	176	176					
55039700	STORM SEWERS TO BE CLEANED	FOOT	500	500					
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	1	1					
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	23	23					
63200310	GUARDRAIL REMOVAL	FOOT	1150	1150					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
67100100	MOBILIZATION	L SUM	1	1					
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1					
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1					

SUMMARY OF QUANTITIES

100% STATE

CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE I000					
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1					
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	11446	11446					
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	485	485					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	81918	81918					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2158	2158					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1001	1001					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	343	343					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	3816	3816					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	485	485					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	81918	81918					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2158	2158					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1001	1001					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	343	343					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1050	1050					
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	23	23					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1050	1050					
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1252	1252					
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	154.2	154.2					
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	4	4					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	40	40					
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1					

* - Specialty Items

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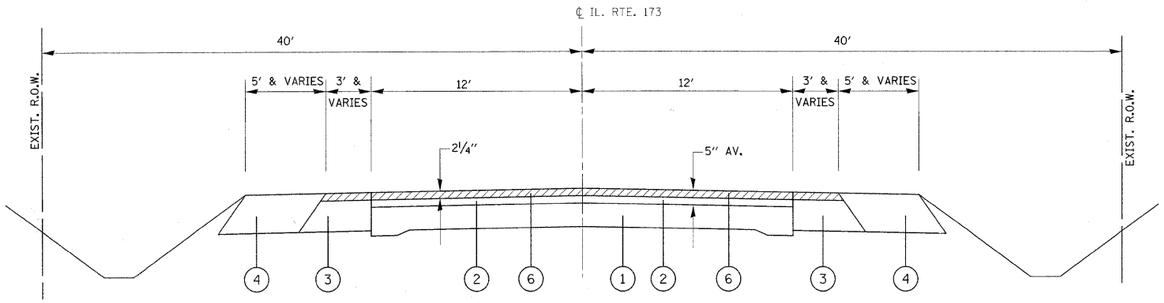
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 173
SUMMARY OF QUANTITIES

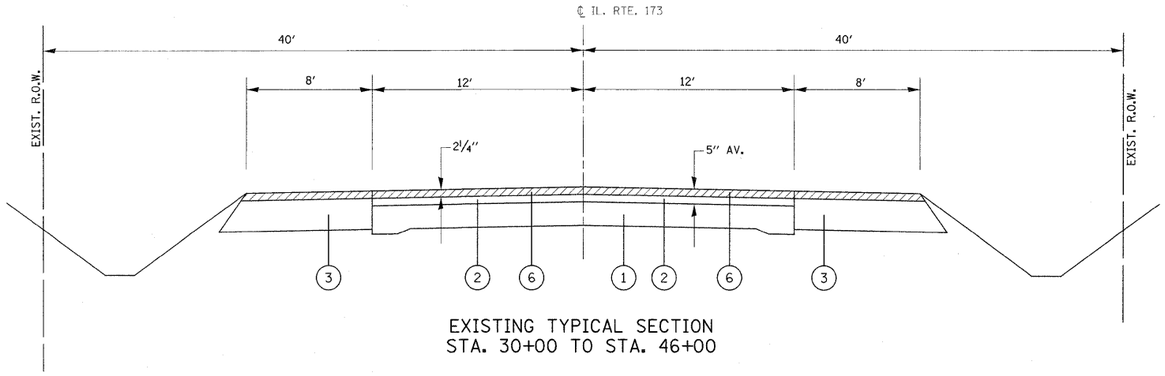
SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

Rev.

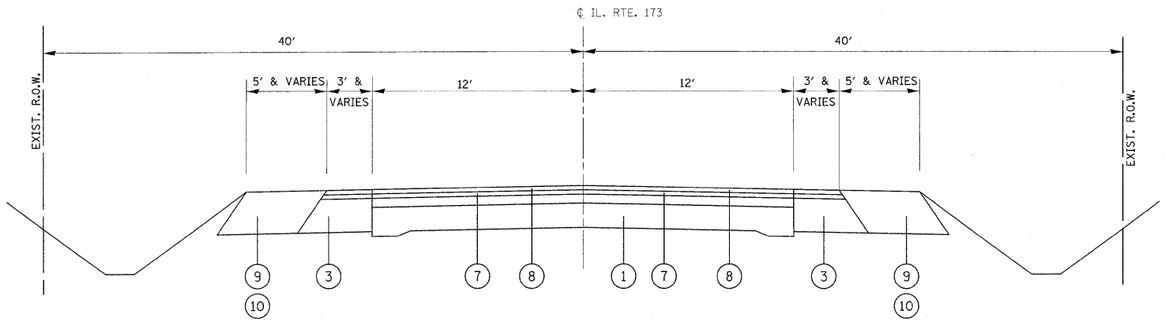
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	3
		CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



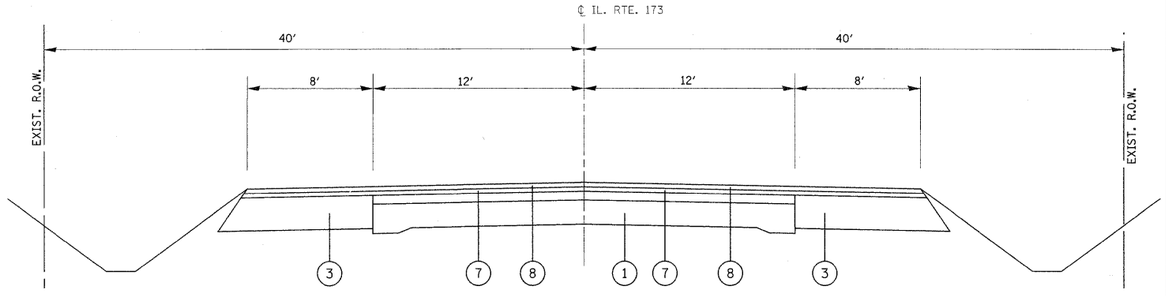
EXISTING TYPICAL SECTION
STA. 20+00 TO STA. 30+00
STA. 84+00 TO STA. 226+00



EXISTING TYPICAL SECTION
STA. 30+00 TO STA. 46+00



PROPOSED TYPICAL SECTION
STA. 20+00 TO STA. 30+00
STA. 84+00 TO STA. 226+00



PROPOSED TYPICAL SECTION
STA. 30+00 TO STA. 46+00

LEGEND

(1) EXISTING P.C.C. PAVEMENT, (5.5″-9.78″, AV. 7.5″)
(2) EXISTING HMA SURFACE, (3″-6.75″, AV. 5″)
(3) EXISTING HMA SHOULDER, 8″
(4) EXISTING AGGREGATE SHOULDER, 8″
(4A) EXISTING BINDER COURSE, MIX B, TYPE 2, 10″
(5) EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
(6) PROPOSED HMA SURFACE REMOVAL, 2¼″
(7) PROPOSED POLYMERIZED LEVELING BINDER, (MM), IL. 4.75, N50, ¾″
(8) PROPOSED HMA SURFACE COURSE, MIX D, N70, 1½″
(9) PROPOSED GRADING & SHAPING AGGREGATE SHOULDER
(10) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B

NOTE: CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE USES	DESIGN AIR VOIDS
HMA SURFACE COURSE, MIX "D", N70 (IL.-9.5 mm)	4% AT 70 GYR.
POLYMERIZED LEVELING BINDER (MM), IL.-4.75, N50	4% AT 50 GYR.
CLASS D PATCHES, (HMA BINDER IL.-19.0 mm)	4% AT 70 GYR.
HMA SHOULDER, 6" (HMA BINDER IL.-19.0 mm)	4% AT 70 GYR.

NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURES QUANTITIES IS 112 LBS./ SQ. YD./IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR "PECENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

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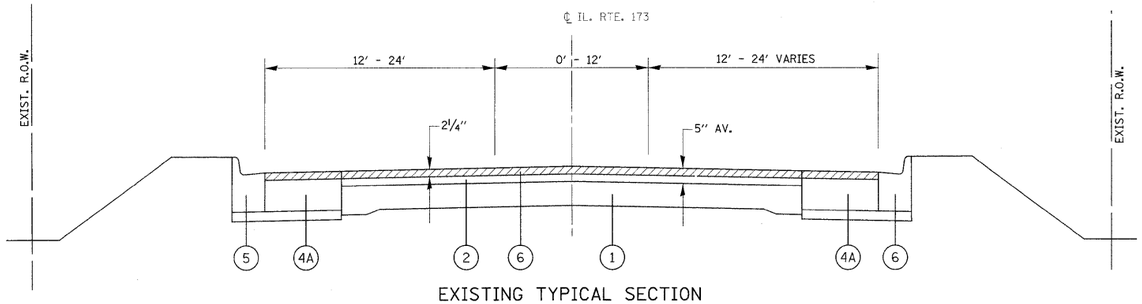
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

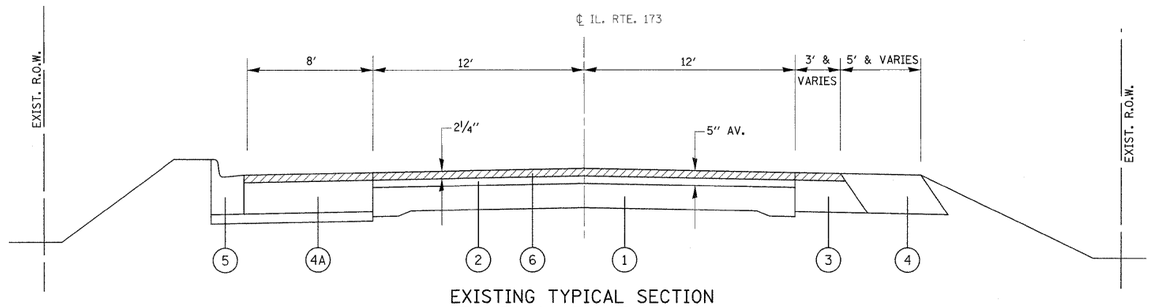
EXISTING & PROPOSED TYPICAL SECTIONS
IL. 173 (0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131)

SCALE: 1″=50′ | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	4
		CONTRACT NO. 60J65		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



EXISTING TYPICAL SECTION
STA. 46+80 TO STA. 76+00



EXISTING TYPICAL SECTION
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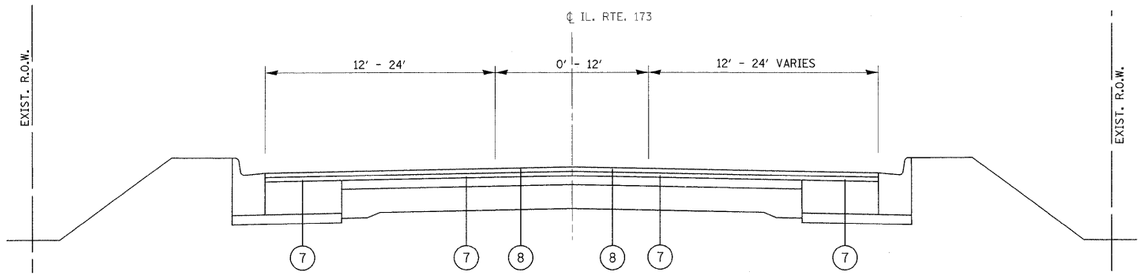
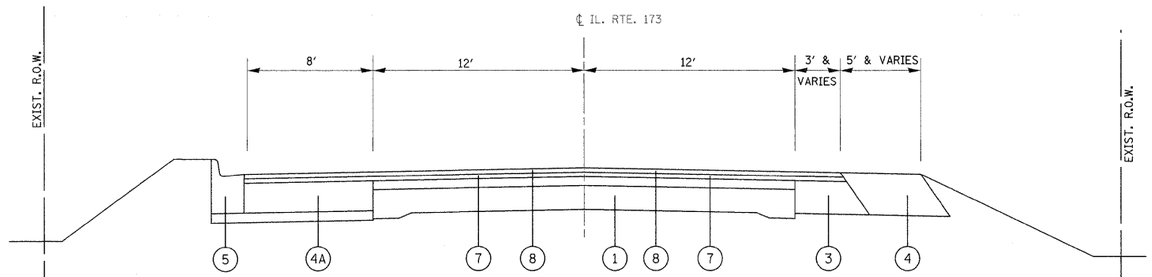


PROPOSED TYPICAL SECTION
STA. 46+80 TO STA. 76+00



PROPOSED TYPICAL SECTION
STA. 76+00 TO STA. 84+00

NOTE: CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

LEGEND

(1) EXISTING P.C.C. PAVEMENT, (5.5″-9.78″, AV. 7.5″)
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(10) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B

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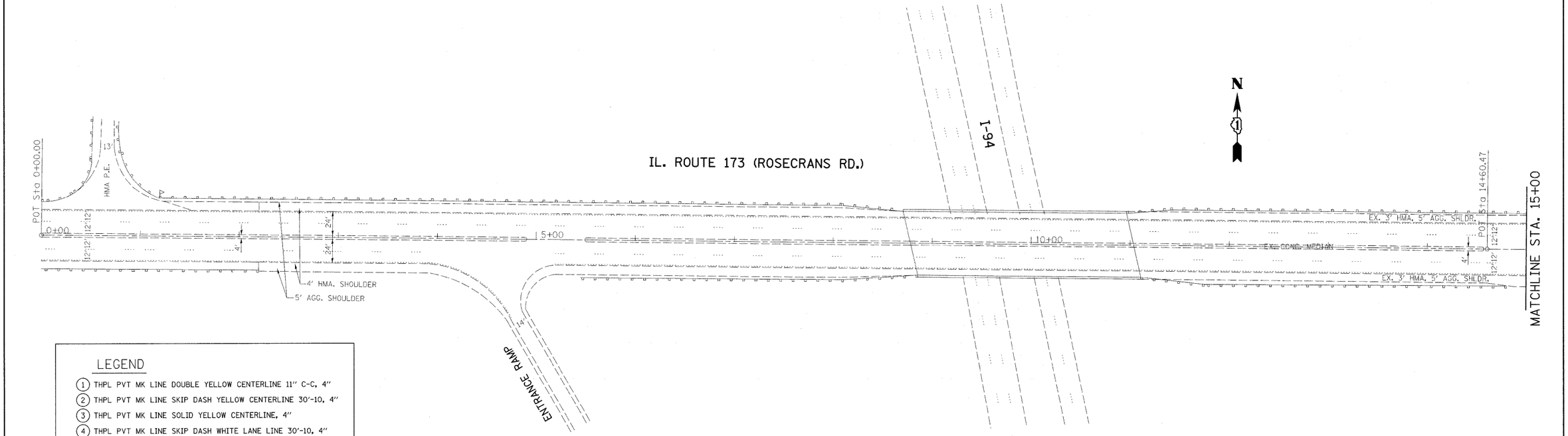
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING & PROPOSED TYPICAL SECTIONS
IL. 173 (0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131)

SCALE: 1″=50′ | SHEET NO. OF SHEETS | STA. TO STA.

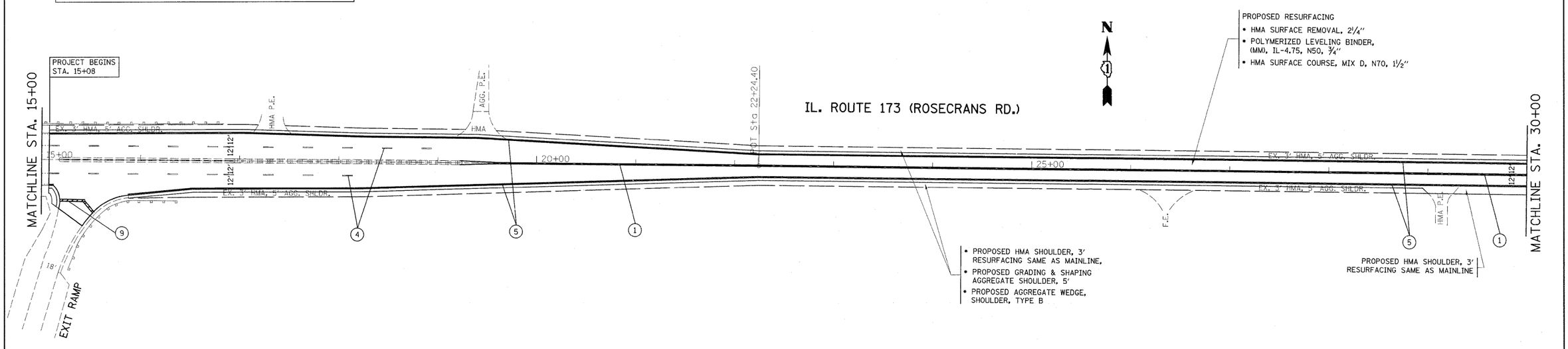
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	5
		CONTRACT NO. 60J65		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

•REF-plan02



LEGEND

1. THPL PVT MK LINE DOUBLE YELLOW CENTERLINE 11" C-C, 4"
2. THPL PVT MK LINE SKIP DASH YELLOW CENTERLINE 30'-10, 4"
3. THPL PVT MK LINE SOLID YELLOW CENTERLINE, 4"
4. THPL PVT MK LINE SKIP DASH WHITE LANE LINE 30'-10, 4"
5. THPL PVT MK LINE WHITE EDGE LINE, 4"
6. THPL PVT MK LINE DIAGONAL YELLOW, 45° 20' C-C, 12"
7. THPL PVT MK LINE SOLID WHITE LANE LINE, 6"
8. THPL PVT MK LINE SOLID WHITE CROSSWALK, 6' C-C, 6"
9. THPL PVT MK LINE SOLID WHITE STOP BAR, 24"
10. THPL PVT MK LINE SOLID WHITE LETTER & SYMBOLS
11. THPL PVT MK LINE DIAGONAL WHITE, 45° 20' C-C, 12"
12. THPL PVT MK LINE DIAGONAL WHITE CROSSWALK, 2' C-C, 8"



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	PLOT DATE = 2/11/2010	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 173
0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131 (GREEN BAY RD.)

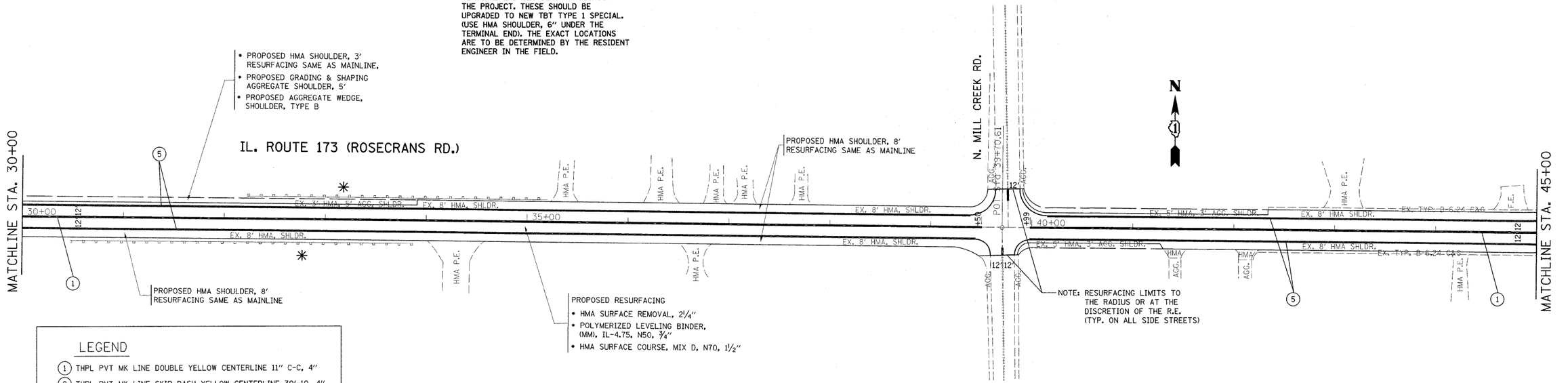
SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. 0+00 TO STA. 30+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	6
		CONTRACT NO. 60J65		
	ILLINOIS FED. AID PROJECT			

REV. 02-10-10

*REF-plan04

✳ THERE ARE 23 OLD GUARDRAIL TERMINAL END SECTIONS THROUGHOUT THE PROJECT. THESE SHOULD BE UPGRADED TO NEW TBT TYPE 1 SPECIAL. (USE HMA SHOULDER, 6" UNDER THE TERMINAL END). THE EXACT LOCATIONS ARE TO BE DETERMINED BY THE RESIDENT ENGINEER IN THE FIELD.



LEGEND

1. THPL PVT MK LINE DOUBLE YELLOW CENTERLINE 11" C-C, 4"
2. THPL PVT MK LINE SKIP DASH YELLOW CENTERLINE 30'-10, 4"
3. THPL PVT MK LINE SOLID YELLOW CENTERLINE, 4"
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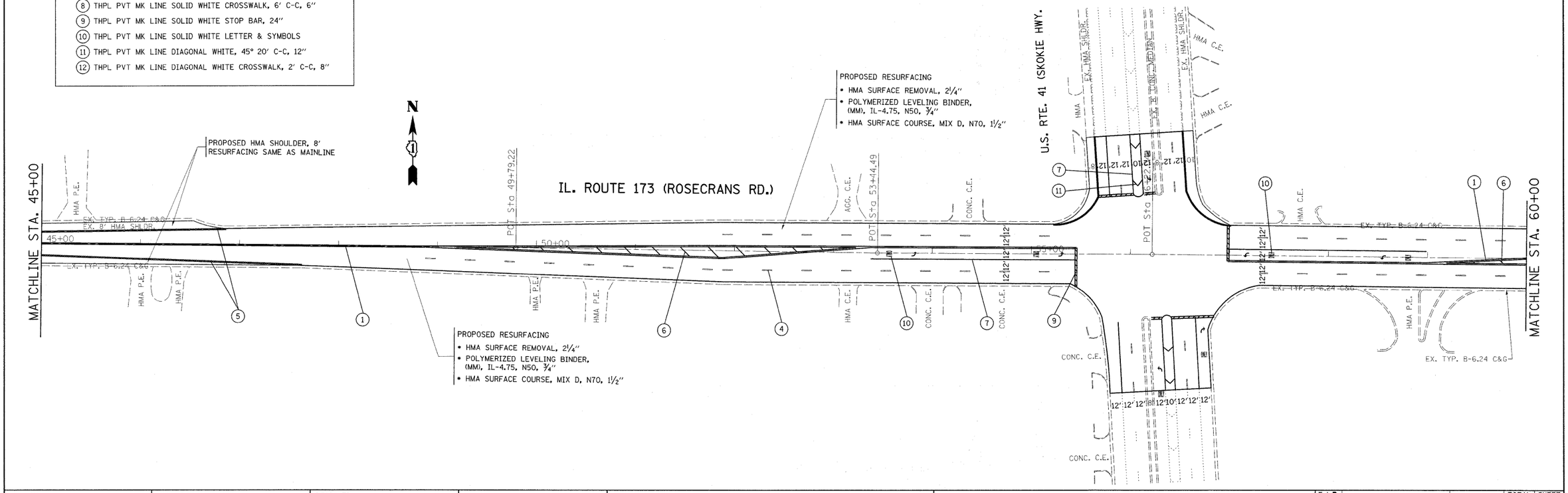
11. THPL PVT MK LINE DIAGONAL WHITE, 45° 20' C-C, 12"
12. THPL PVT MK LINE DIAGONAL WHITE CROSSWALK, 2' C-C, 8"



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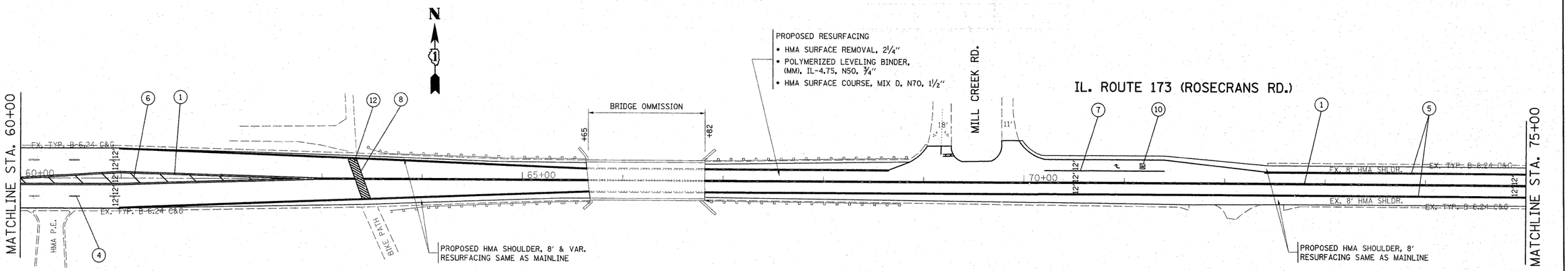
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -		SCALE: 1"=50'	SHEET NO. OF SHEETS	STA. 30+00	TO STA. 60+00	
	PLOT DATE = 2/11/2010	DATE -	REVISED -		F.A.P. RTE. 303	SECTION 136RS-6	COUNTY LAKE	TOTAL SHEETS 28	SHEET NO. 7
								CONTRACT NO. 60J65	
							ILLINOIS	FED. AID PROJECT	

REV. 02-10-10

*REF=plan06



LEGEND

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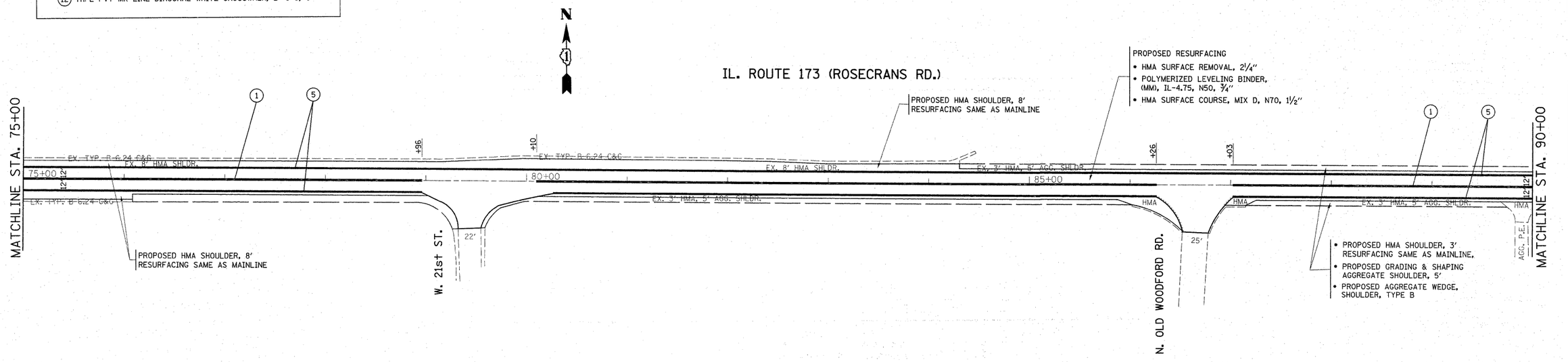

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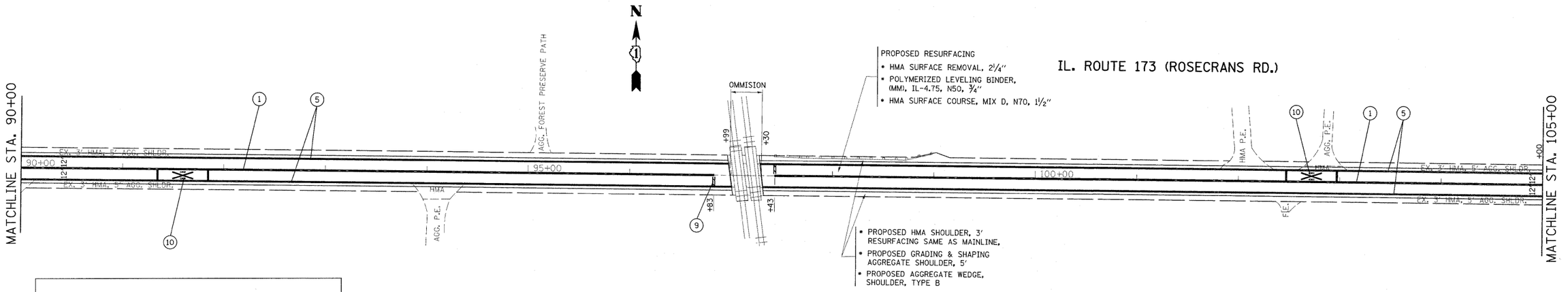
	PLOT DATE = 1/27/2010	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 173
0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131 (GREEN BAY RD.)

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. 60+00 TO STA. 90+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	8
		CONTRACT NO. 60J65		
	ILLINOIS FED. AID PROJECT			



LEGEND

1. THPL PVT MK LINE DOUBLE YELLOW CENTERLINE 11" C-C, 4"
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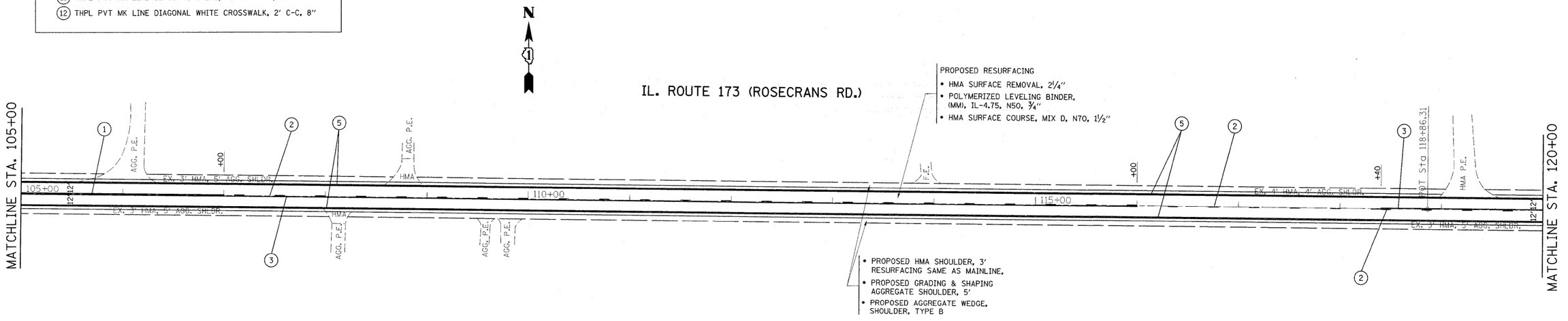

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	PLOT DATE = 2/11/2010	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

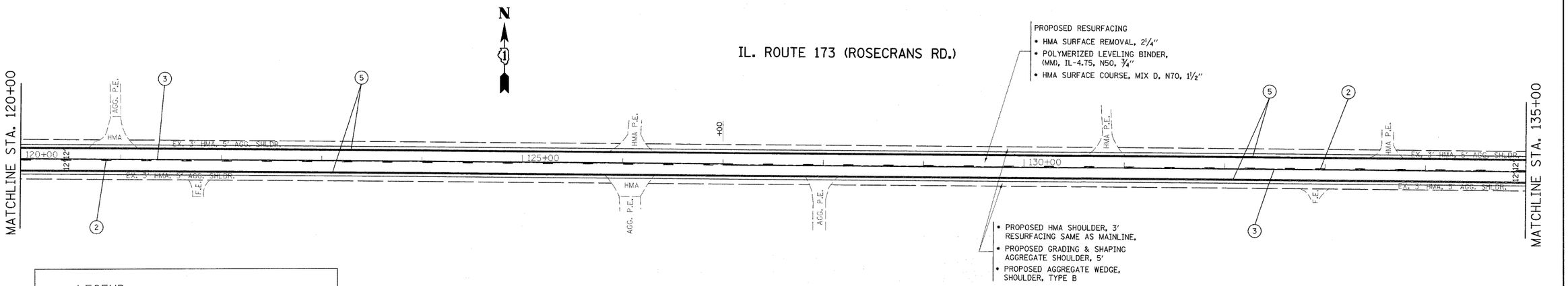
IL. RTE. 173
0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131 (GREEN BAY RD.)

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. 90+00 | TO STA. 120+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	9
		CONTRACT NO. 60J65		
	ILLINOIS FED. AID PROJECT			

REV. 02-10-10

*REF-plan10



LEGEND

1. THPL PVT MK LINE DOUBLE YELLOW CENTERLINE 11" C-C, 4"
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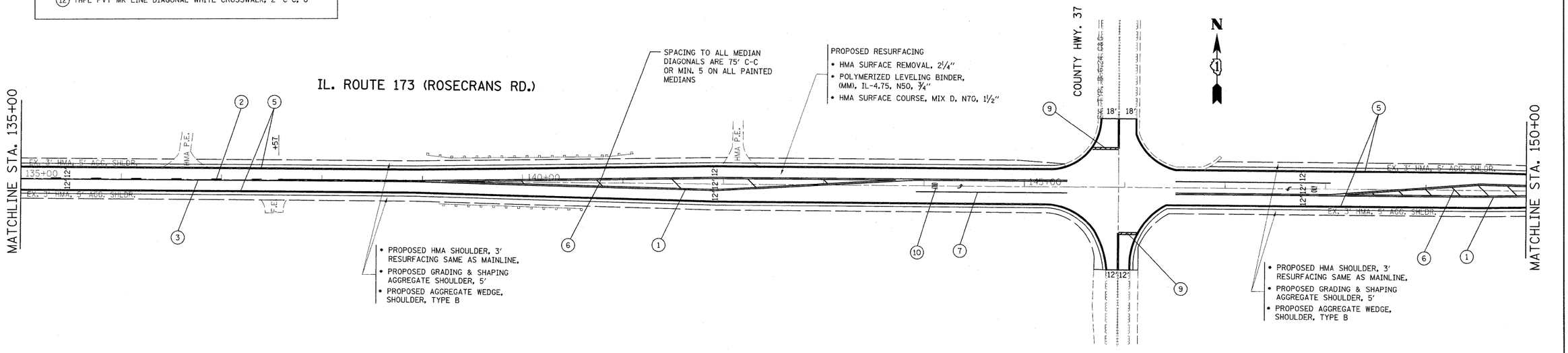

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	PLOT DATE = 2/11/2010	DATE -	REVISED -

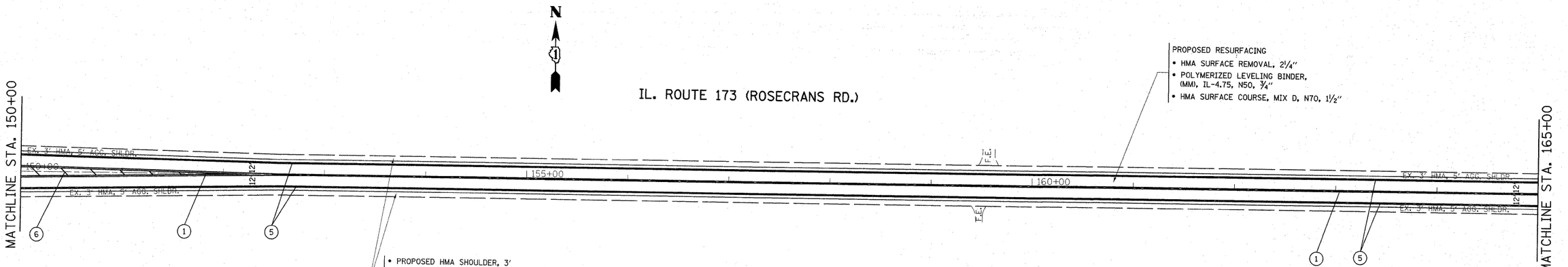
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 173
0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131 (GREEN BAY RD.)

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. 120+00 TO STA. 150+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	10
		CONTRACT NO. 60J65		
	ILLINOIS FED. AID PROJECT			

REV. 02-10-10



LEGEND

1. THPL PVT MK LINE DOUBLE YELLOW CENTERLINE 11" C-C, 4"
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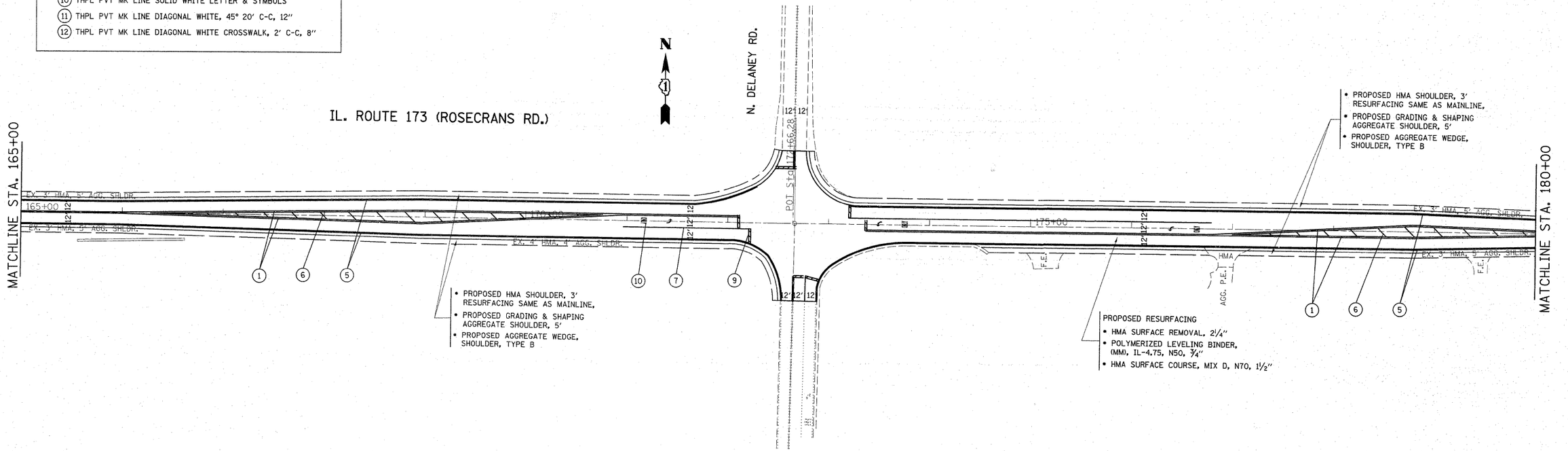

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	PLOT DATE = 1/27/2010	DATE -	REVISED -

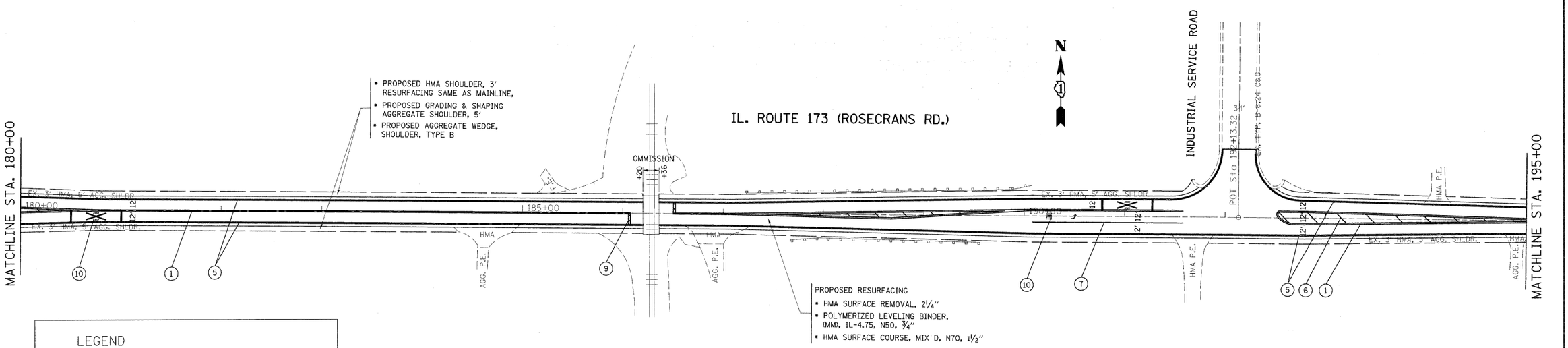
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 173
0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131 (GREEN BAY RD.)

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. 150+00 TO STA. 180+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	11
		CONTRACT NO. 60J65		
	ILLINOIS	FED. AID PROJECT		

*REF-plan14



LEGEND

1. THPL PVT MK LINE DOUBLE YELLOW CENTERLINE 11" C-C, 4"
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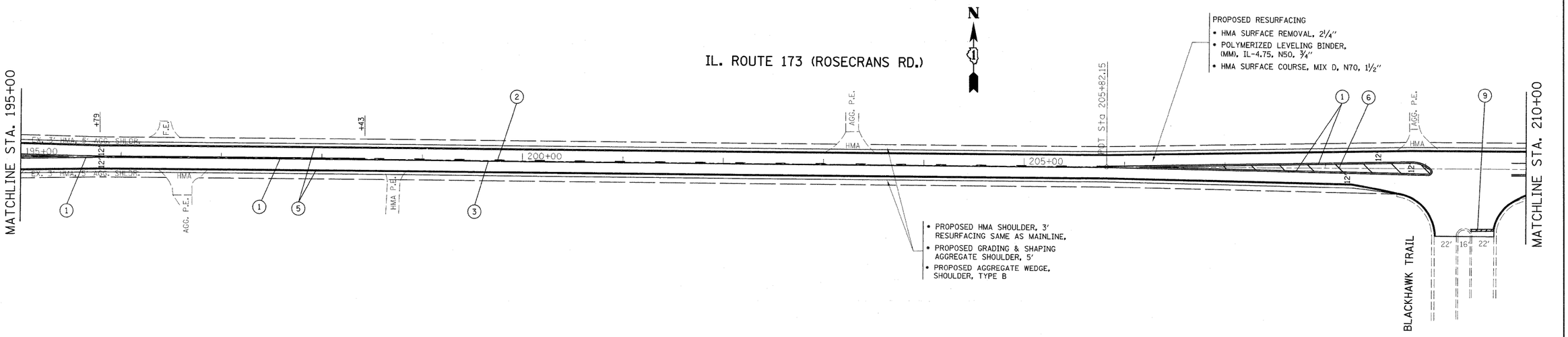

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	PLOT DATE = 2/11/2010	DATE -	REVISED -

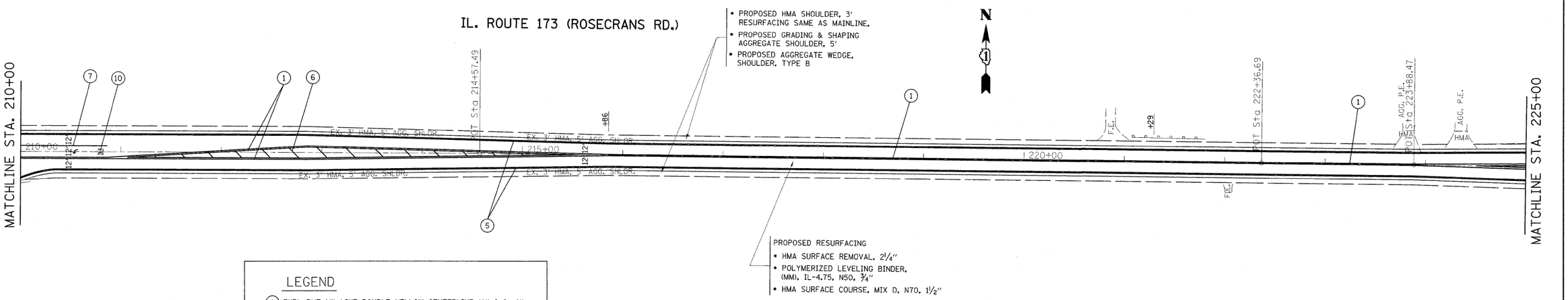
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 173
0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131 (GREEN BAY RD.)

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. 180+00 TO STA. 210+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	12
		CONTRACT NO. 60J65		
	ILLINOIS FED. AID PROJECT			

REV. 02-10-10



LEGEND

1. THPL PVT MK LINE DOUBLE YELLOW CENTERLINE 11" C-C, 4"
2. THPL PVT MK LINE SKIP DASH YELLOW CENTERLINE 30'-10, 4"
3. THPL PVT MK LINE SOLID YELLOW CENTERLINE, 4"
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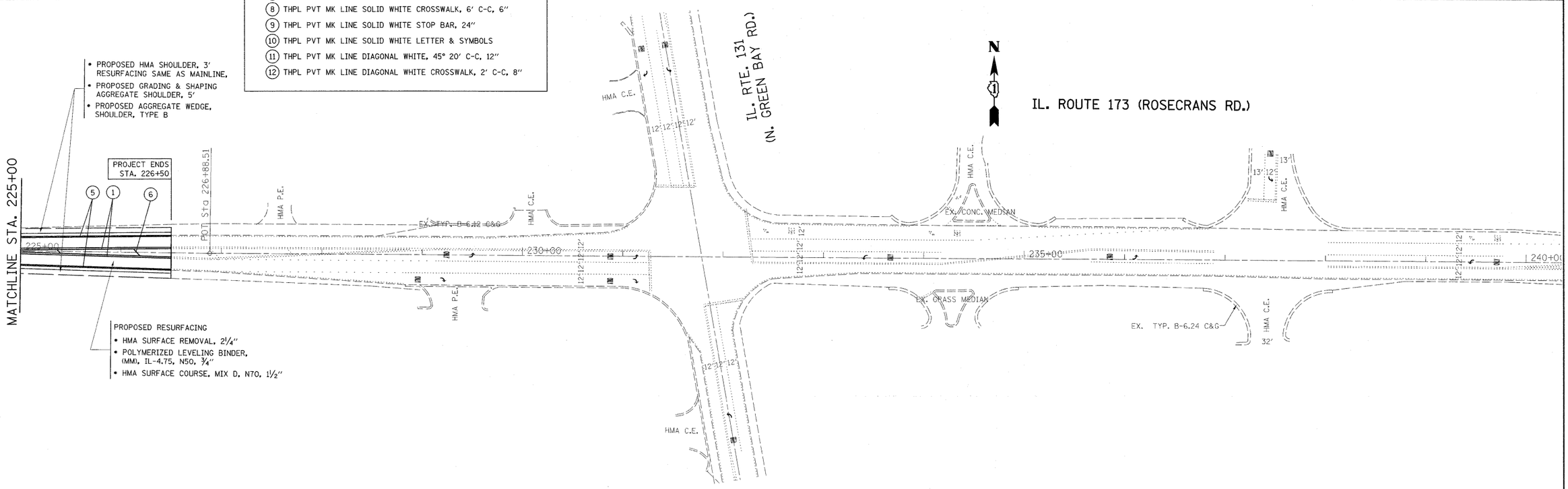

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	PLOT DATE = 2/11/2010	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 173
0.1 M EAST OF I-94 TO 0.1 M WEST OF IL. 131 (GREEN BAY RD.)

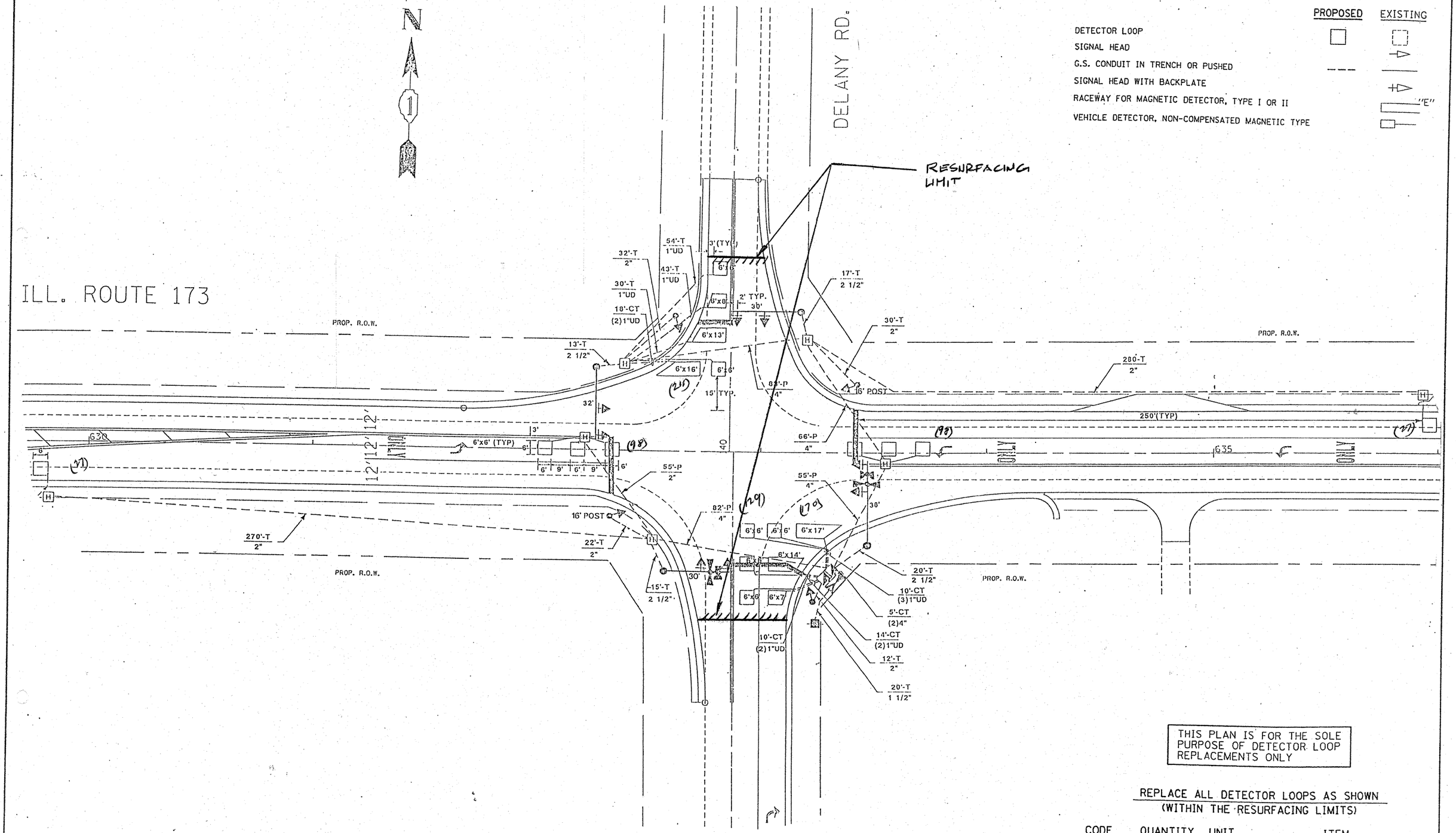
SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. 210+00 TO STA. 240+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	13
		CONTRACT NO. 60J65		
	ILLINOIS FED. AID PROJECT			

REV. 02-10-10

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP	□	⬚
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED	- - -	———
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		"E"
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	760	FOOT	DETECTOR LOOP, REPLACEMENT

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		DRAWN - BCK	REVISED -
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	PLOT DATE = 4/3/2009	DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

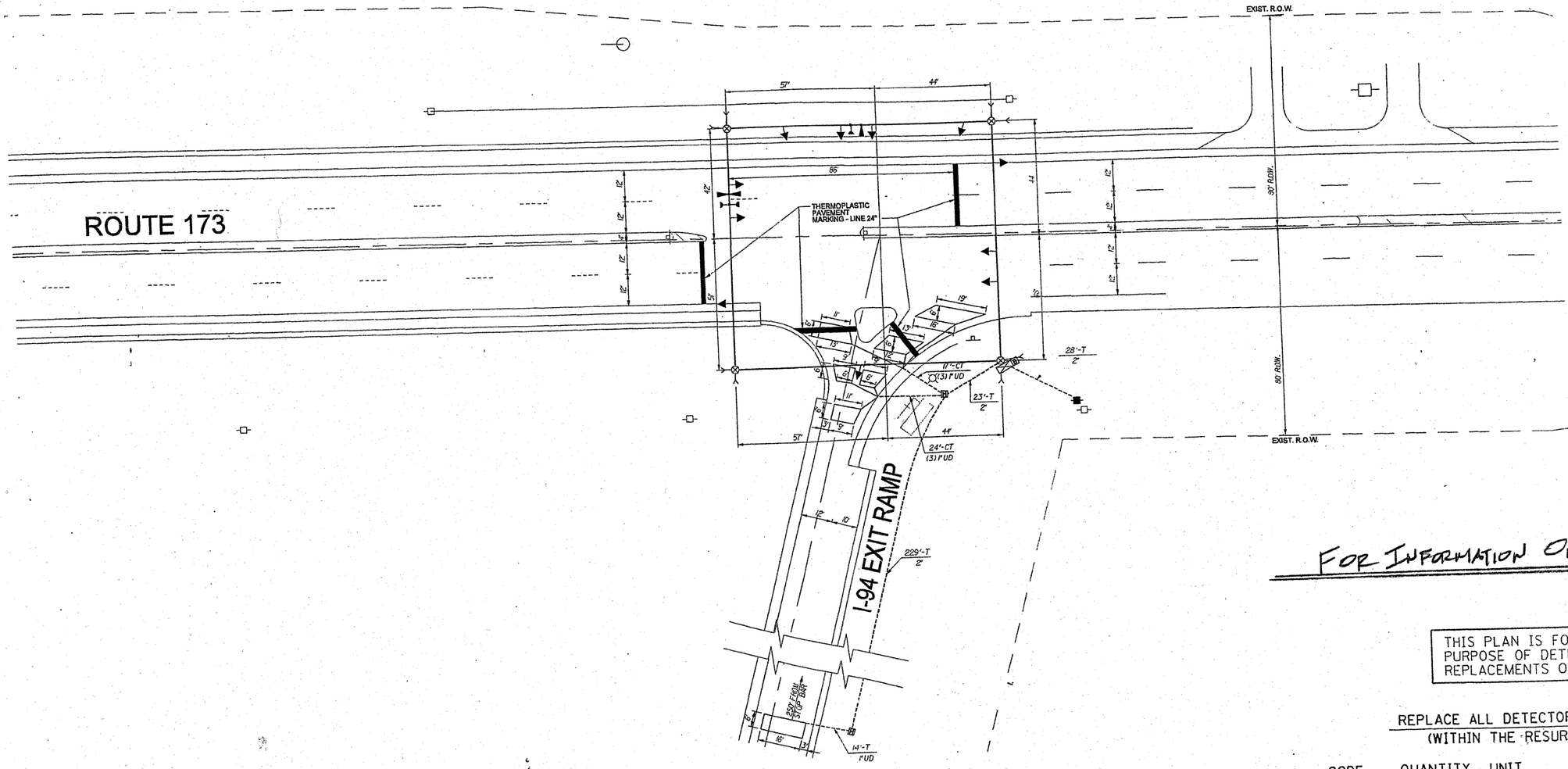
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 173 @ DELANY RD.

SCALE: NONE | SHEET NO. OF SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		LAKE	28	14
		CONTRACT NO.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	60J65	

	PROPOSED	EXISTING
DETECTOR LOOP	□	□
SIGNAL HEAD		→
G.S. CONDUIT IN TRENCH OR PUSHED	- - -	———
SIGNAL HEAD WITH BACKPLATE		+→
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		"E"
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		

SCALE: 1"=20'



FOR INFORMATION ONLY

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600		FOOT	DETECTOR LOOP, REPLACEM

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	PLOT DATE = 4/3/2009	DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

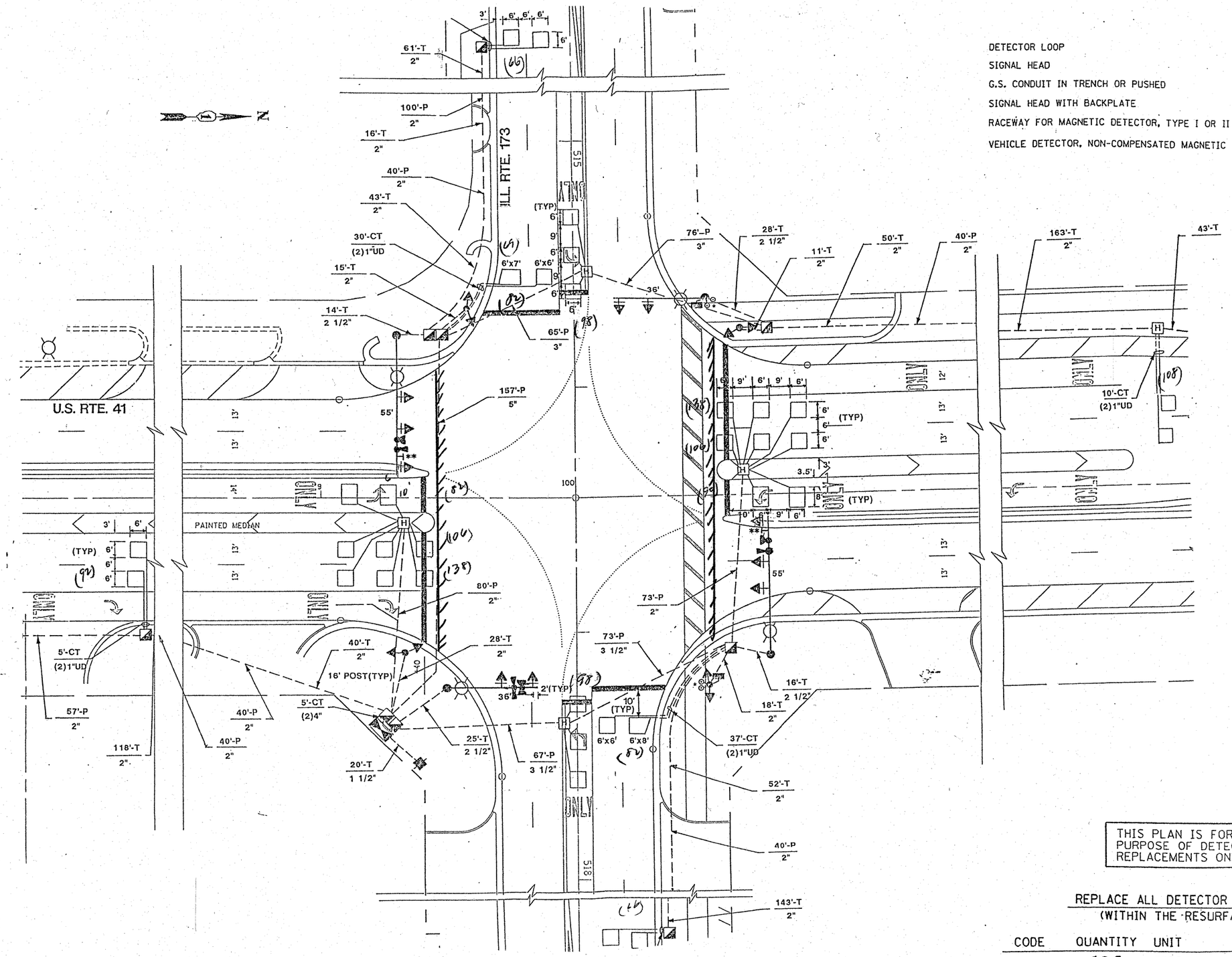
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 173 @ I-94 RAMP NB.

SCALE: NONE	SHEET NO. OF SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
303	13685-6	LAKE	28/15
		CONTRACT	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP	□	⬚
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED	- - -	———
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		"E"
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	492	FOOT	DETECTOR LOOP, REPLACEMEN

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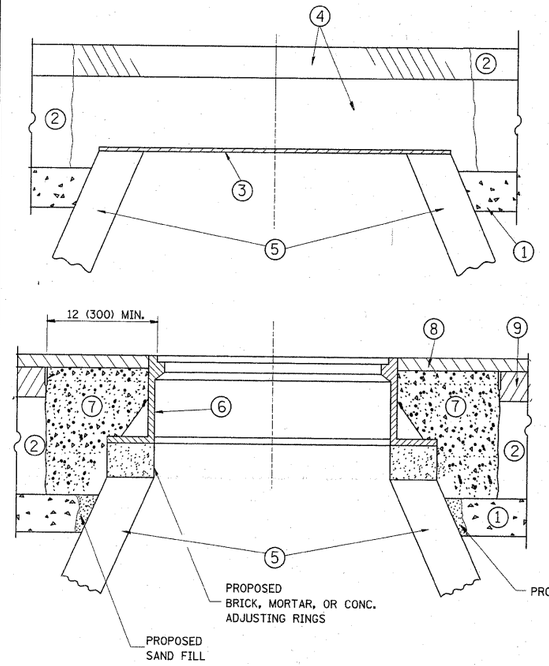
	PLOT DATE = 4/3/2009	DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
U.S. ROUTE 41 @ ILL. ROUTE 173

SCALE: NONE | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHT NO
305	136RS-6	LAKE	28	16
		CONTRACT NO		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	60565		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

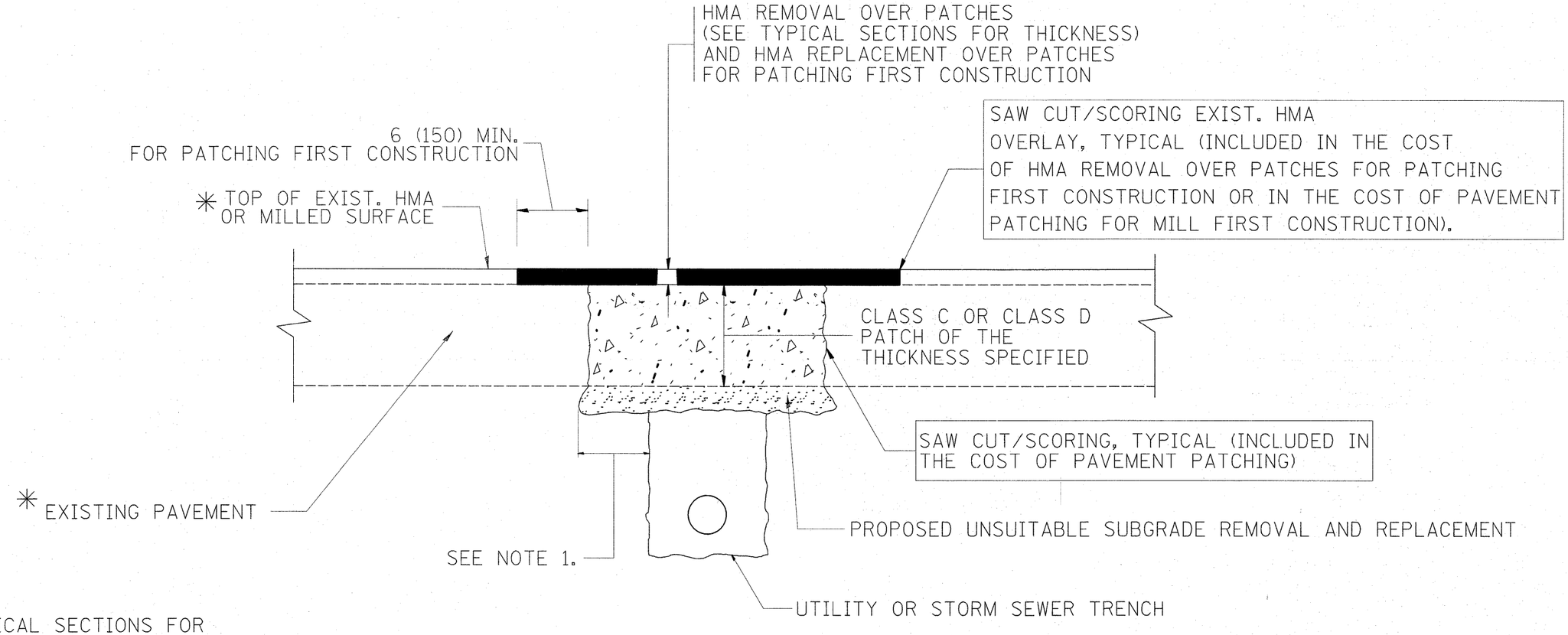
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D125510-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 1/15/2010	DATE - 10-25-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	17
	BD600-03 (BD-8)	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



✳ SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

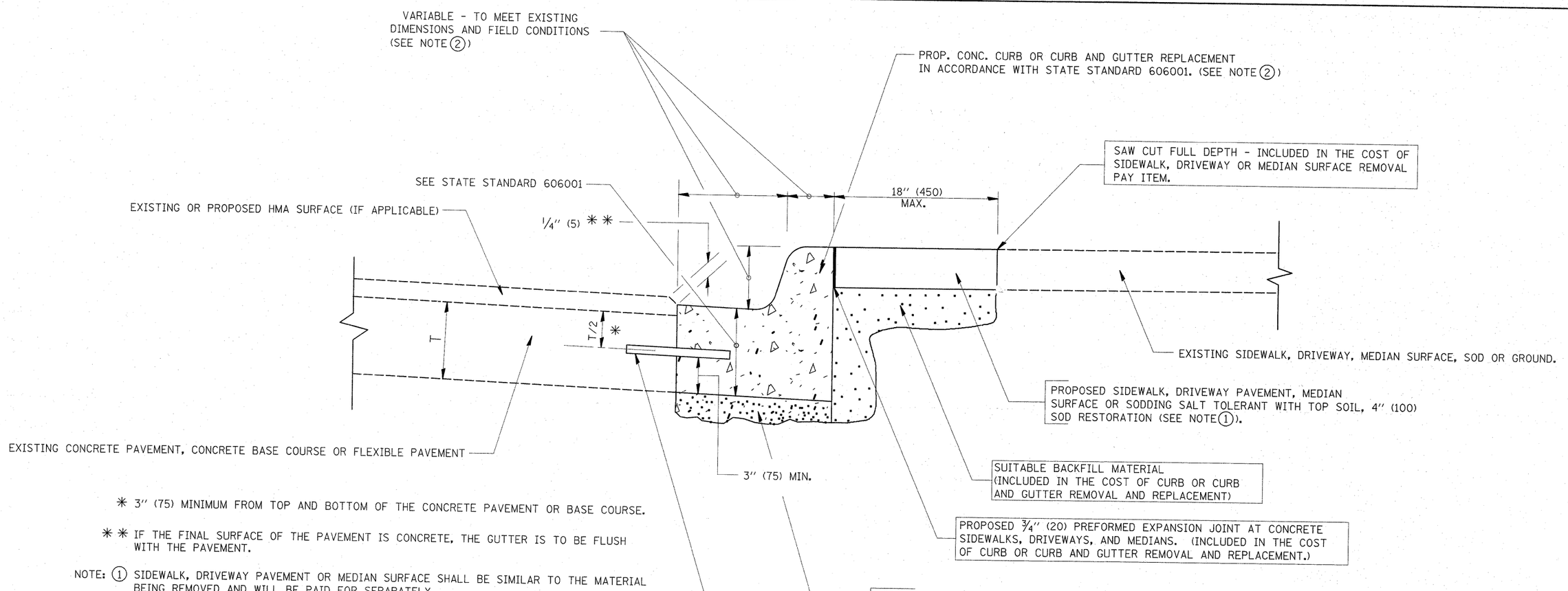
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D126510-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 1/15/2010	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	18
	BD400-04 (BD-22)	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

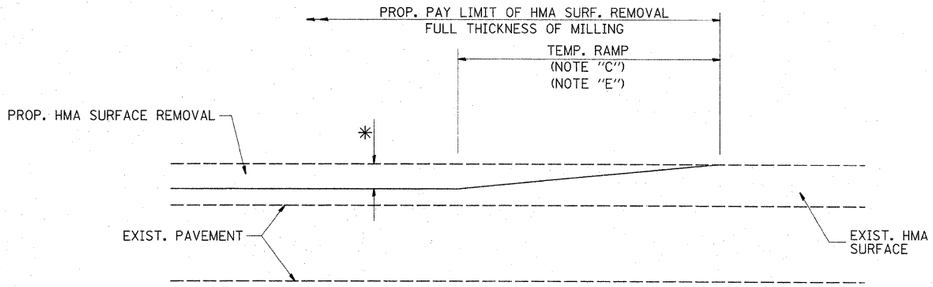
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D126512-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
	PLOT SCALE = 50.0000 ' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT DATE = 1/15/2010	CHECKED -	REVISED - M. GOMEZ 01-22-01
		DATE - 03-11-94	REVISED - R. BORO 12-15-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

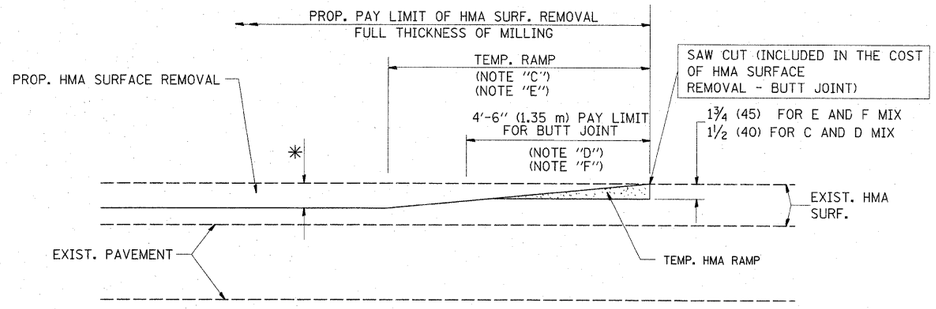
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	19
	BD600-06 (BD-24)	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

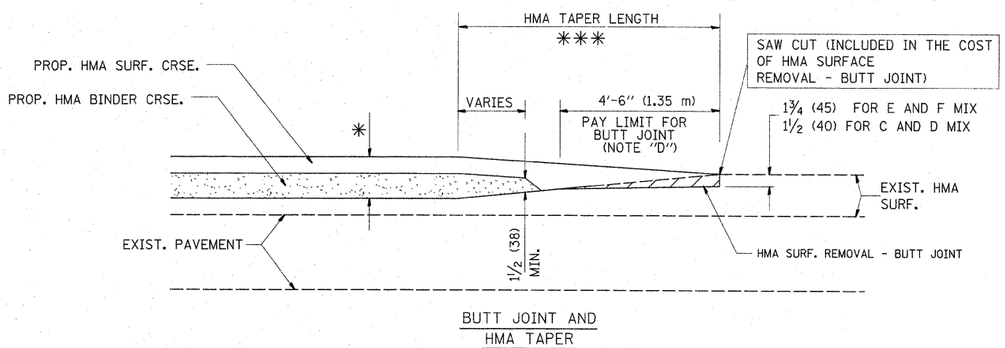


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

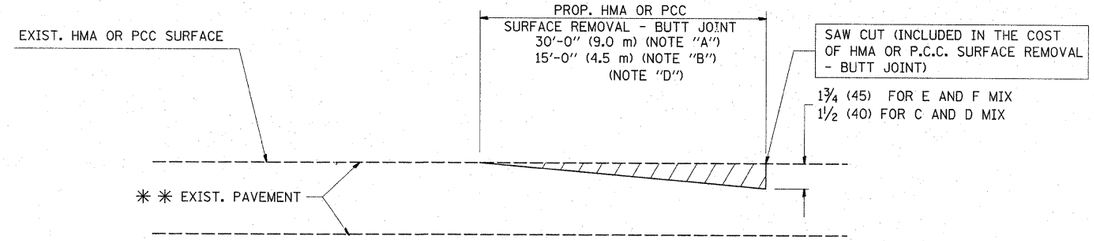
OPTION 2

TYPICAL TEMPORARY RAMP

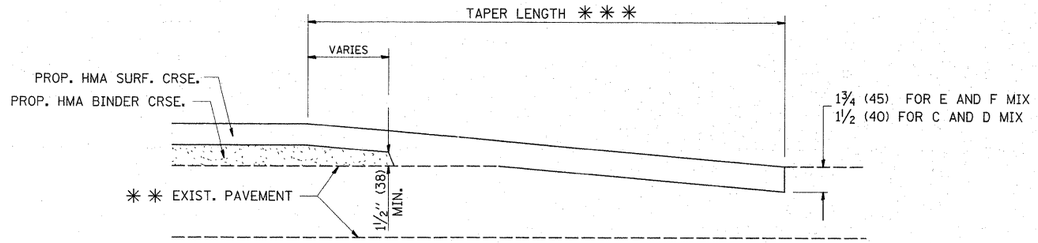


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

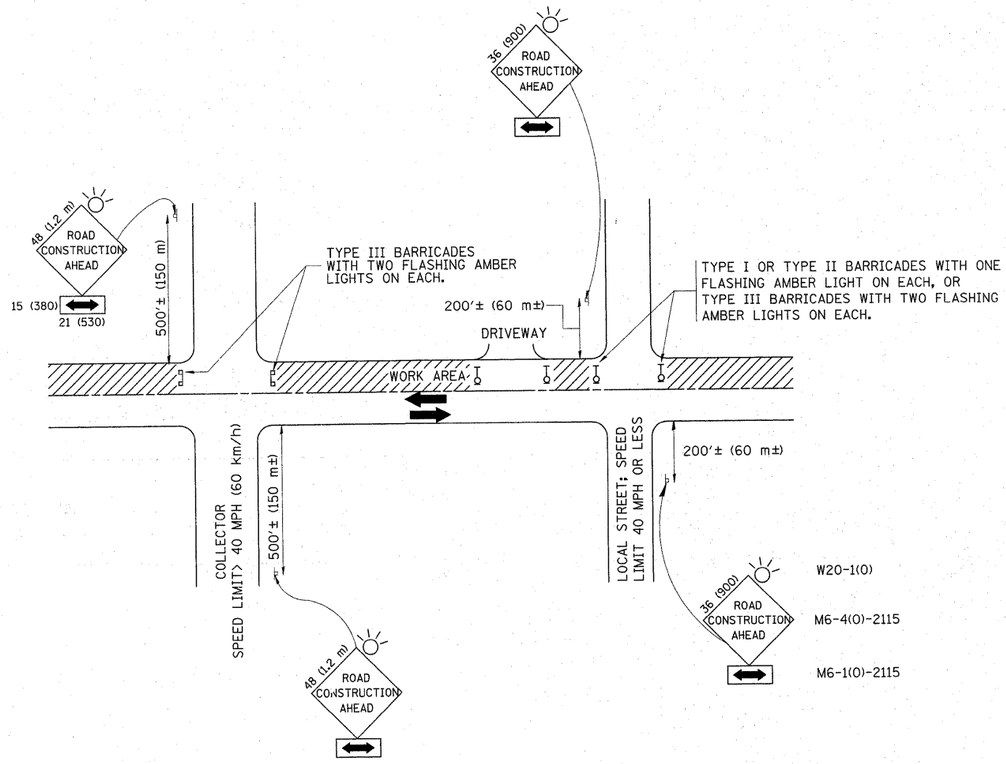
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D1206510-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/15/2010	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	20
	BD400-05 BD32	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

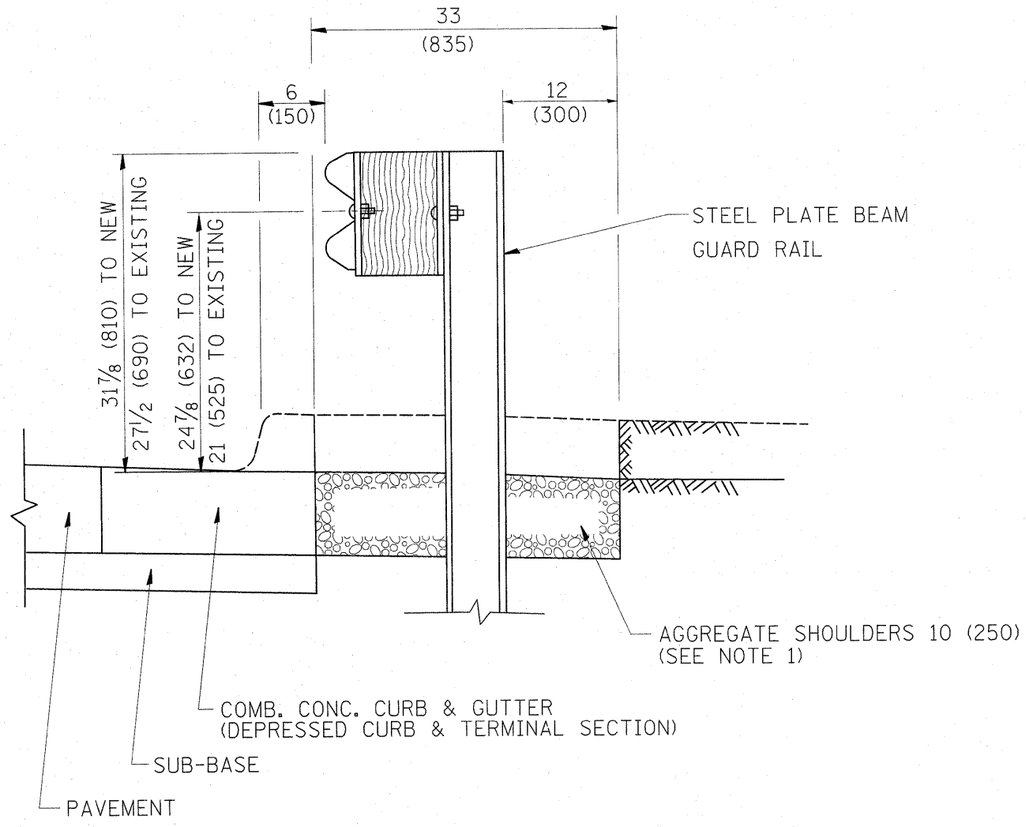
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D125510-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/15/2010	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	21
	TC-10	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

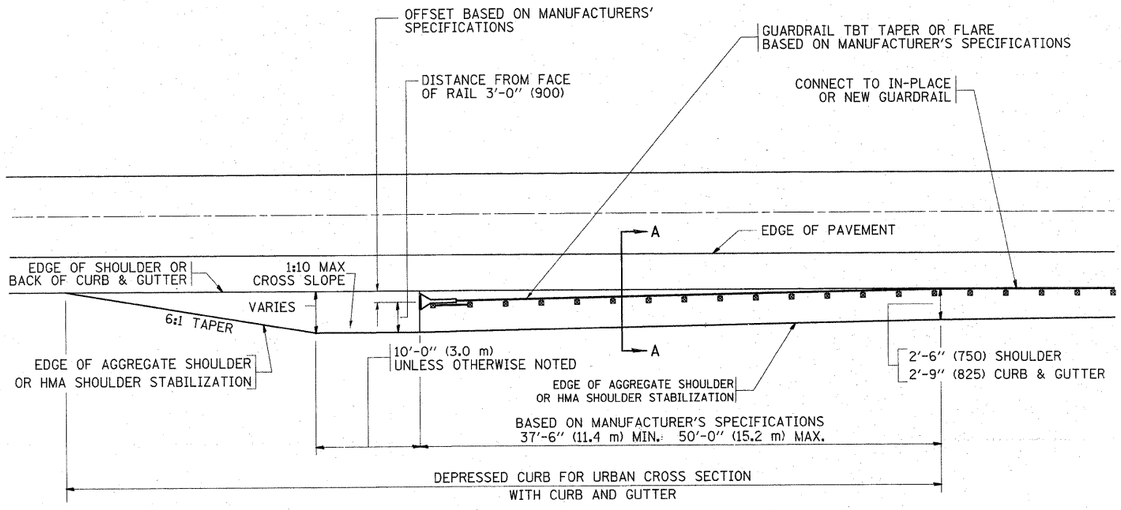


SECTION A-A

NOTES:
1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER

[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



DEPRESSED CURB AND GUTTER AND SHOULDER TREATMENT AT TBT TY. 1 SPL.

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

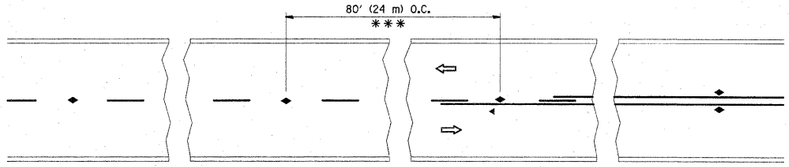
STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

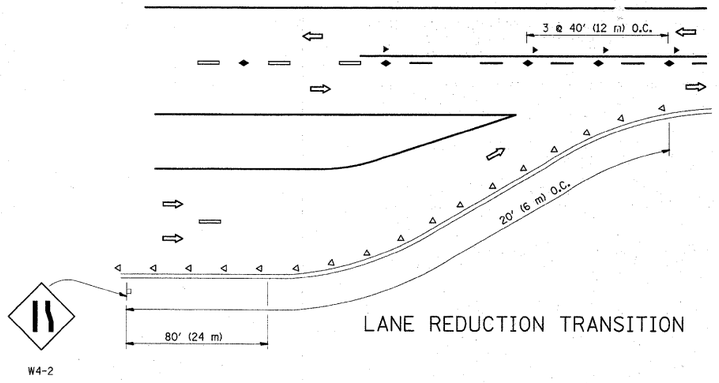
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\Dis	USER NAME = ulrichkd	DESIGNED - M. DE YONG	REVISED - E. GOMEZ 08-28-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL.
	Std.dgn	DRAWN -	REVISED - R. BORO 01-01-07		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. BORO 12-08-2008		
	PLOT DATE = 1/22/2010	DATE - 09-22-90	REVISED - R. BORO 09-14-2009		SCALE: NONE / SHEET NO. 1 OF 1 SHEETS / STA. / TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	22
BD600-10 (BD 34)		CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

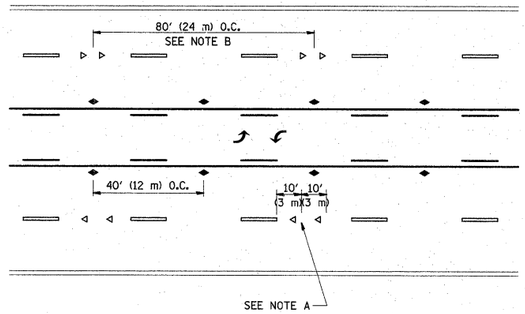


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

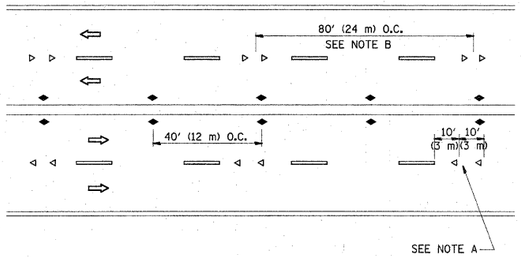
TWO-LANE/TWO-WAY



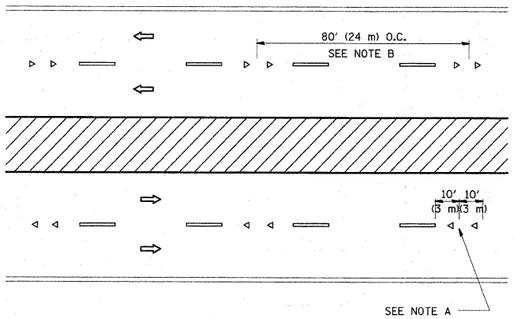
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

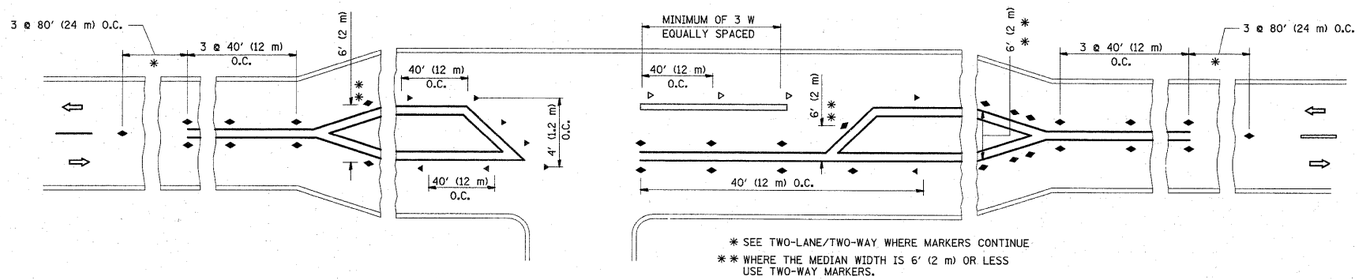
SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

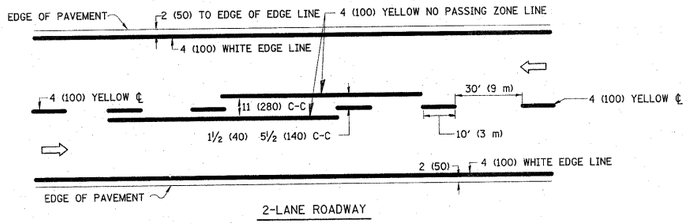
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D1255l0-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED -	REVISED -T. RAMMACHER 09-19-94
		DRAWN -	REVISED -T. RAMMACHER 03-12-99
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 01-06-00
	PLOT DATE = 1/15/2010	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

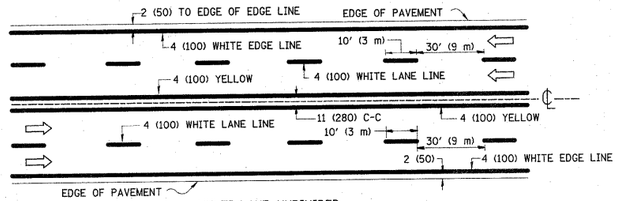
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

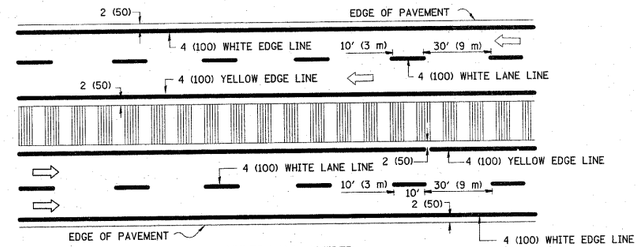
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	23
	TC-11	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2-LANE ROADWAY



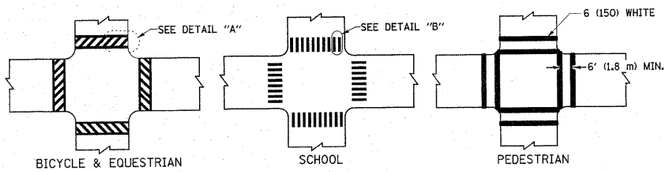
MULTI-LANE UNDIVIDED

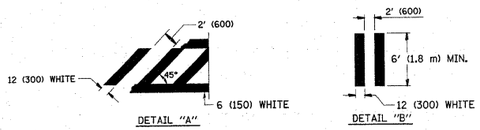


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

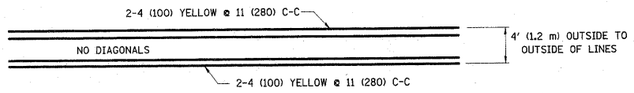
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

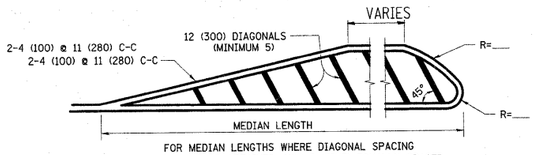




TYPICAL CROSSWALK MARKING



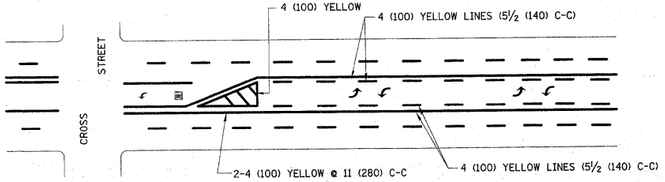
4' (1.2 m) WIDE MEDIANS ONLY



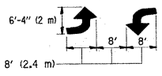
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

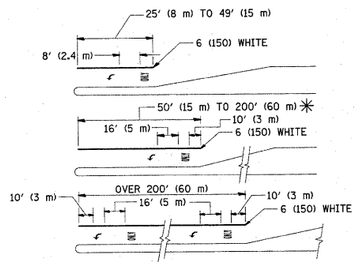


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

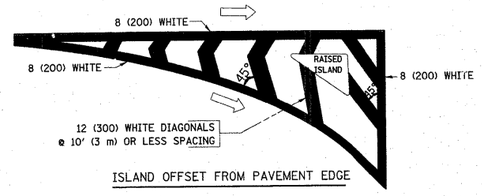


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m²) ↑ **ONLY** AREA = 20.8 SQ. FT. (1.9 m²)

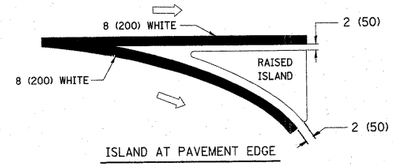
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⊄ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⊄ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⊄ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⊄ 6 (150) 12 (300) ⊄ 45° 12 (300) ⊄ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⊄ 4 (100) WITH 12 (300) DIAGONALS ⊄ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⊄ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) ⊄ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

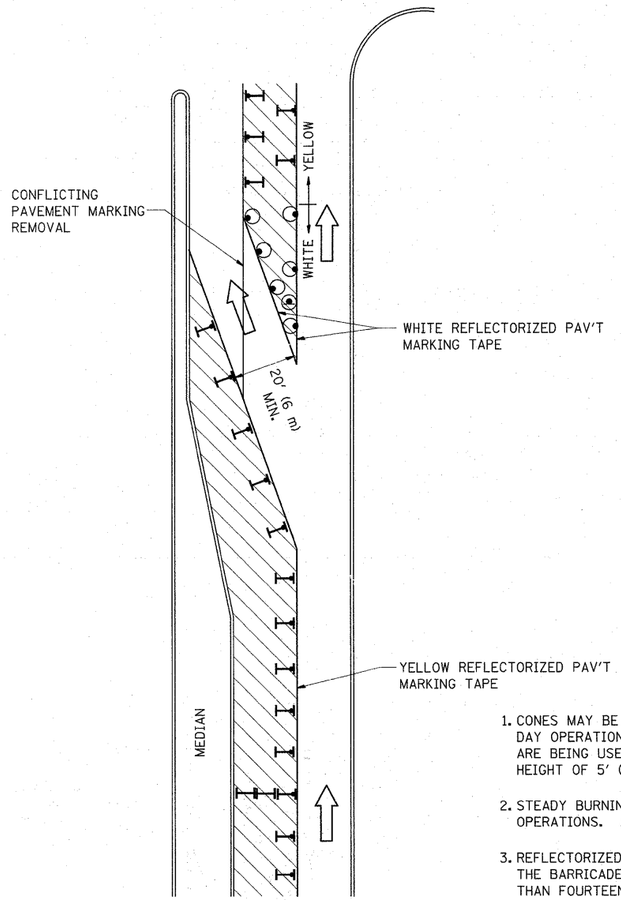
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175498\D129510-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
		DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/15/2010	DATE - 03-19-90	REVISED -

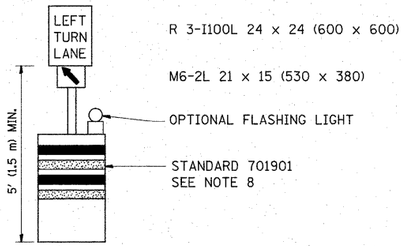
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	24
	TC-13	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SFECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D125510-sht-plan.dgn	USER NAME = ulrichkd	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	PLOT SCALE = 50.0000 ' / IN.	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT DATE = 1/15/2010	REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED -T. RAMMACHER 01-06-00	REVISED -

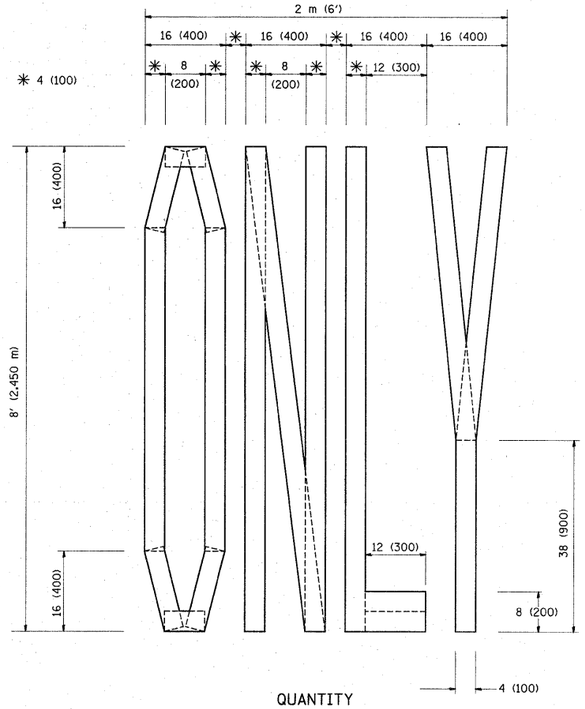
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

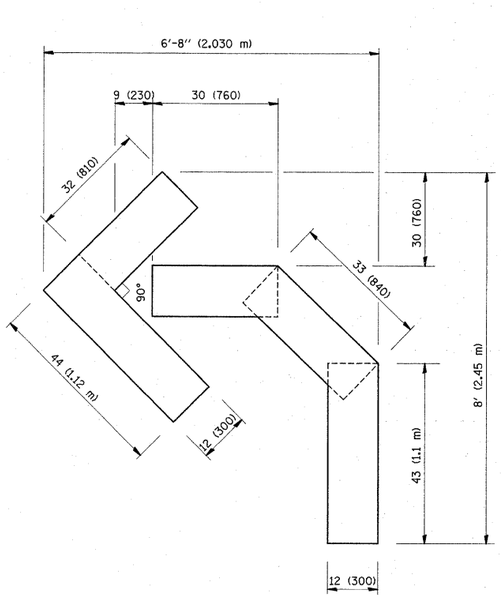
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	25
	TC-14	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

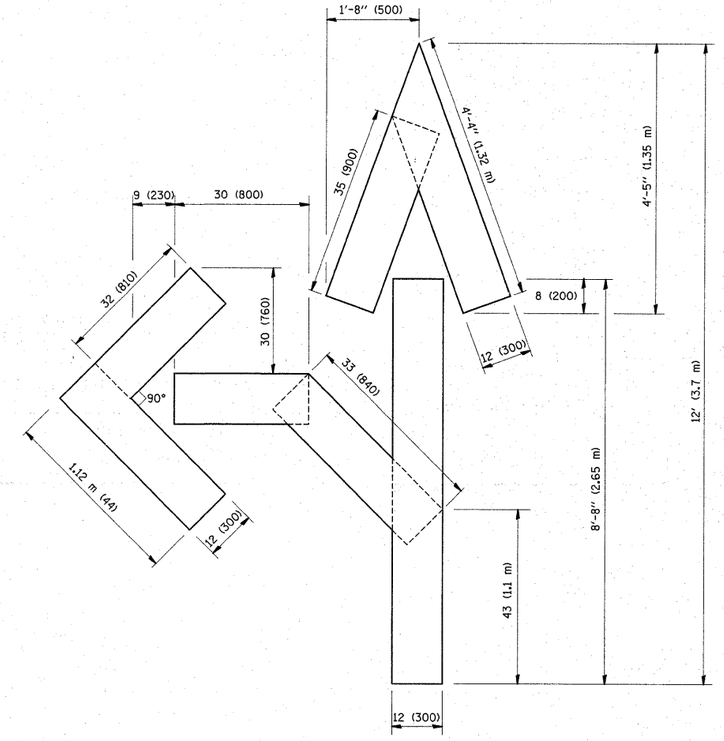
25



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

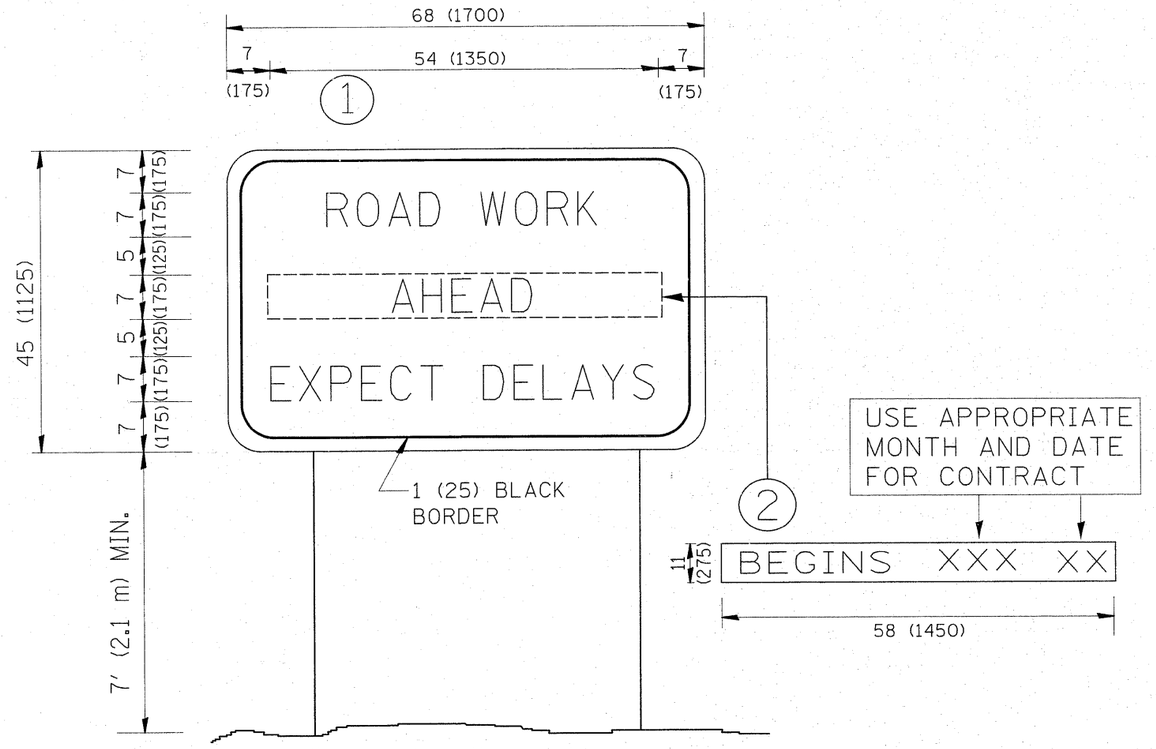
FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D126510-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/15/2010	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	26
	TC-16	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D125510-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/15/2010	DATE -	REVISED - C. JUCIUS 01-31-07

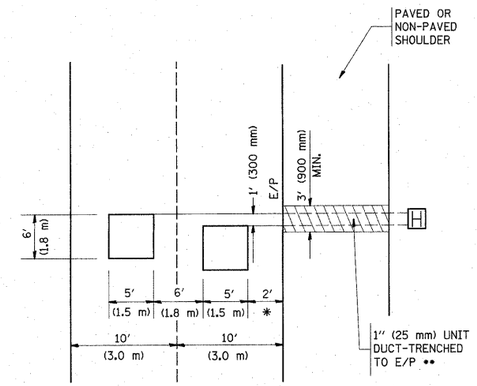
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	27
	TC-22	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

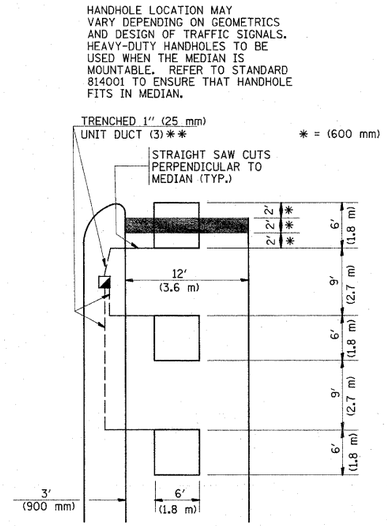
LOOPS NEXT TO SHOULDERS



* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

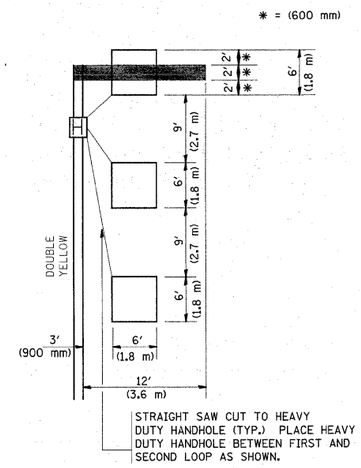
LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

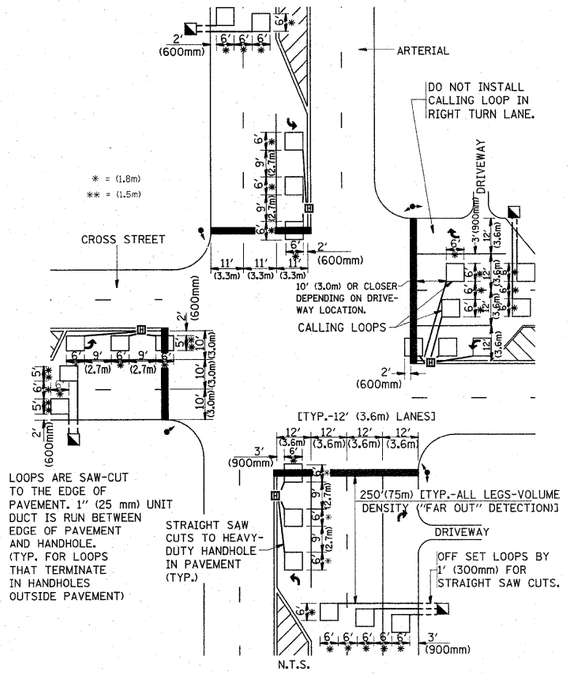
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



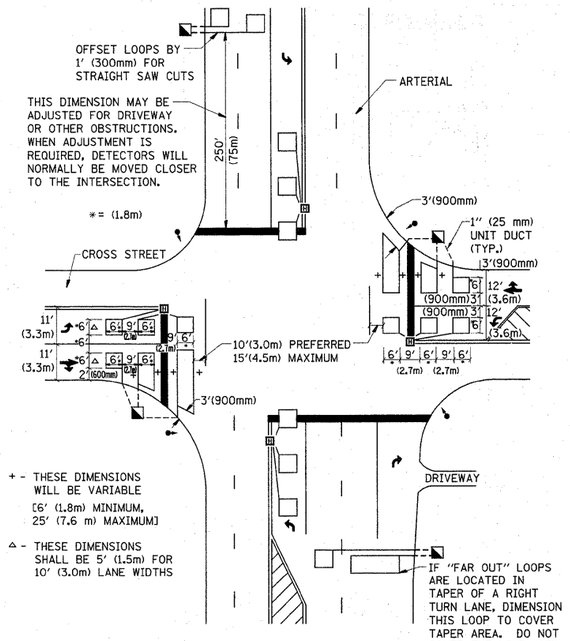
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = c:\pw_work\PWIDOT\ULRICHKD\d0175499\D126510-sht-plan.dgn	USER NAME = ulrichkd	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/15/2010	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	136RS-6	LAKE	28	28
	TS-07	CONTRACT NO. 60J65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		