

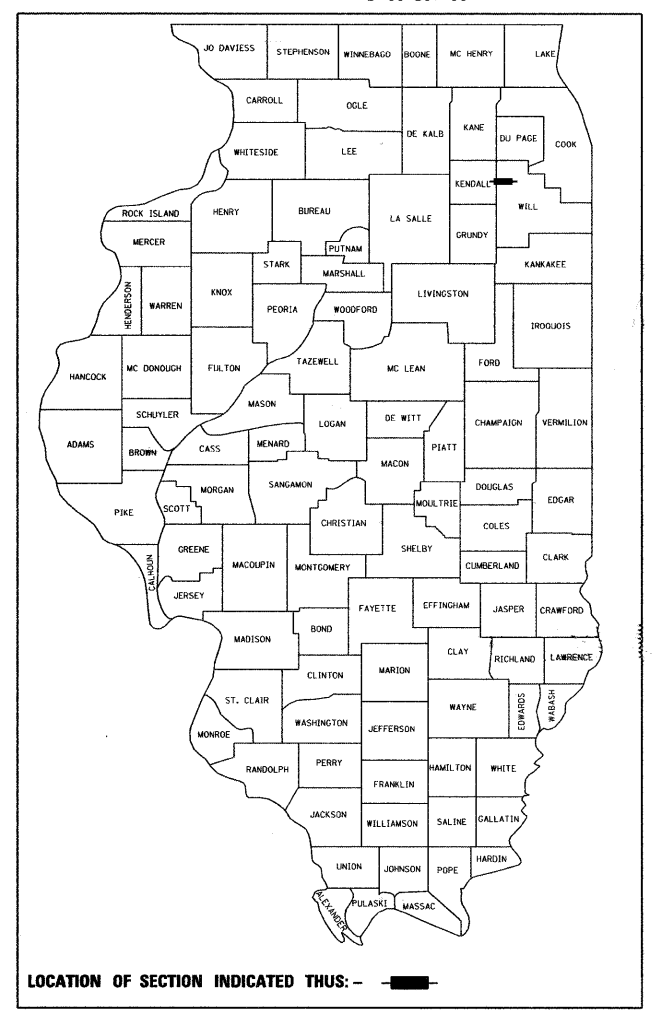
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0379	119 RS-14	WILL	20	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60J95		

D-91-317-10

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS
F.A.U. 0379: ILLINOIS ROUTE 126
COUNTY LINE ROAD TO WALLIN DRIVE
SECTION: 119 RS-14
PROJECT: - -
RESURFACING (3P)
WILL COUNTY
C-91-317-10

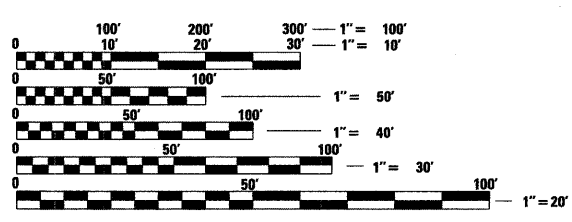
FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED WITHIN THE VILLAGE OF PLAINFIELD



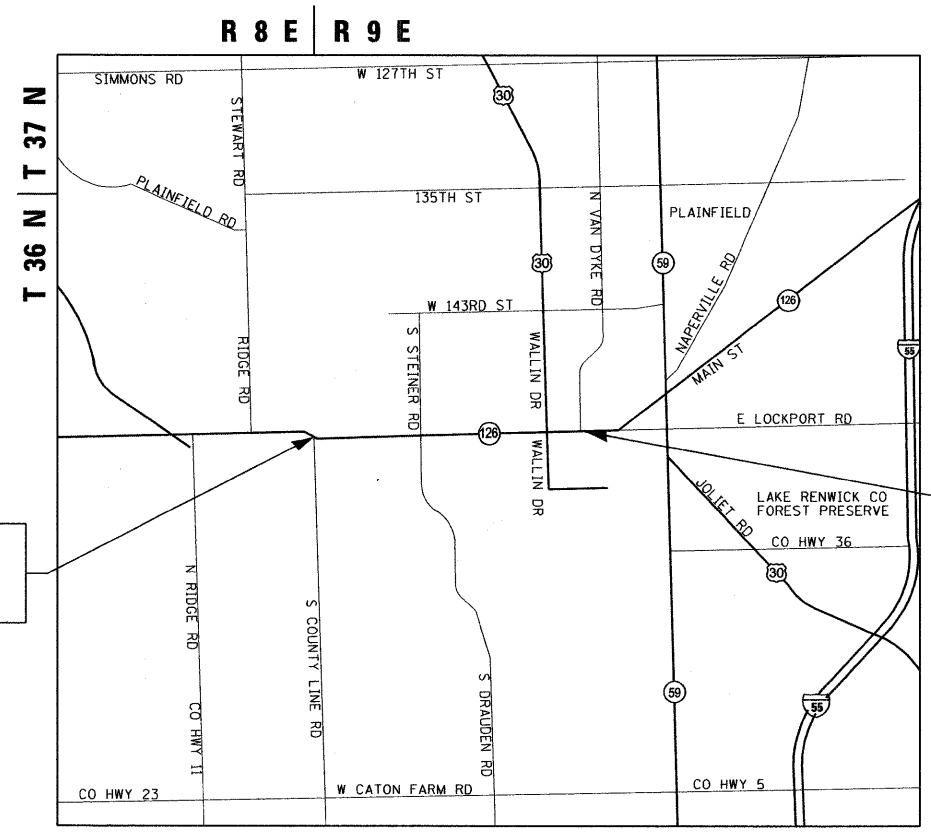
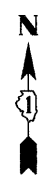
LOCATION OF SECTION INDICATED THUS: - - -

TRAFFIC DATA
ILLINOIS ROUTE 126
2007 ADT = 17,100
SPEED LIMIT = 40-55 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



IMPROVEMENT
BEGINS
STA. 558 + 64.41

IMPROVEMENT
ENDS
STA. 661 + 87.37

SCALE: NTS

PLAINFIELD TOWNSHIP

GROSS LENGTH OF PROJECT = 10,322.96 FT = 1.96 MILES
NET LENGTH OF PROJECT = 10,322.96 FT = 1.96 MILES

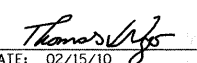
CONTRACT NO. 60J95

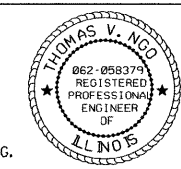
DISTRICT ONE - PLAN PREP ENGINEER: KEN ENG (847) 705-4247

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 16, 2010
Diane M. O'Keefe
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 19, 2010
Scott E. Stitt, P.E./e
 ACTING ENGINEER OF DESIGN AND ENVIRONMENT
March 19, 2010
Christine M. Reed/e
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER


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 www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

MILLENNIA PROFESSIONAL SERVICES
 THOMAS V. NGO, P.E.
 * 062-058379

 DATE: 02/15/10
 SIGNATURE AND SEAL APPLIES TO DRWG.



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- 16 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC14)
- 17 PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
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LIST OF ILLINOIS DOT HIGHWAY STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 442201-03 CLASS C AND D PATCHES
- 482011-03 HMA SHLD, STRIPS/SHLDS, WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
- 701301-03 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-02 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > OR = 45 MPH
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
- 701336-05 LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS > OR = 45 MPH
- 701901-01 TRAFFIC CONTROL DEVICES

COMMITMENTS

NO COMMITMENTS FOR THIS PROJECT

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES, VILLAGE OF PLAINFIELD.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
4. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
5. ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE. NO ADDITIONAL COST TO THE DEPARTMENT.
6. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
7. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
8. LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
9. DRAINAGE ADJUSTMENTS OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE BY THE ENGINEER.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.
11. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
12. THE ENGINEER SHALL CONTACT CORA MATHIS, THE TRAFFIC FIELD TECHNICIAN AT (847) 715-8428 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
13. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
14. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
15. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN ON THE PLANS.
16. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
17. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.
18. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND IT'S REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
19. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).

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 USER NAME = Millennium Professional Services



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 126		INDEX OF SHEETS, LIST OF IDOT	
COUNTY LINE ROAD TO WALLIN DRIVE		HIGHWAY STANDARD, GENERAL NOTES AND COMMITMENTS	
SCALE: N/A	SHEET NO. OF SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0379	119 RS-14	WILL	20	2
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J95	

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URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITY	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	100% STATE	1000 QUANTITY
20201006	GRADING AND SHAPING SHOULDERS	UNIT	207	207
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	18	18
40600300	AGGREGATE (PRIME COAT)	TON	87	87
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	65	65
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1824	1824
40600895	CONSTRUCTING TEST STRIP	EACH	2	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT	SQ YD	365	365
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	938	938
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4290	4290
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	43780	43780
44002224	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 6"	SQ YD	2791	2791
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	64	64
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	1243	1243
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	285	285
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	949	949
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	826	826
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	5
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6
67100100	MOBILIZATION	L SUM	1	1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1
* 70300100	SHORT-TERM PAVEMENT MARKING	FOOT	5146	5146
* 70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	18	18
* 70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	76620	76620
* 70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2915	2915
* 70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	870	870
* 70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	310	310
* 70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	858	858
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	9	9

* SPECIALTY ITEM

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITY	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	100% STATE	1000 QUANTITY
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	38310	38310
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1460	1460
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	435	435
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	155	155
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	456	456
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	456	456
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	418	418
* X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	52
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	5	5

FILE NAME: P:\2009\VEP0906-Var-Vor-Ph11\CADD\W09_IL126\Shts\03-D168J95-sht-IL126-S00.dgn
USER NAME: Millennium Professional Services



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DEPARTMENT OF TRANSPORTATION

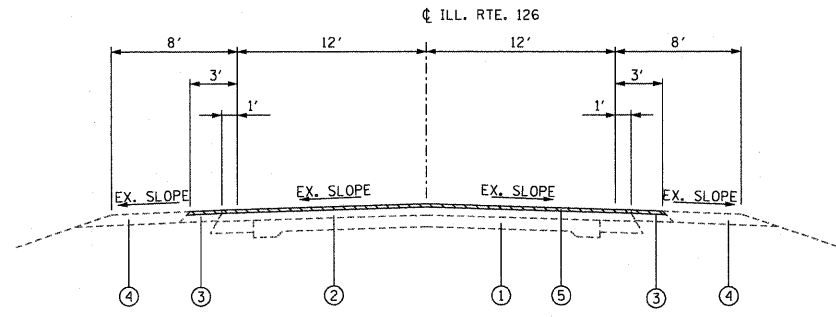
ILLINOIS ROUTE 126
COUNTY LINE ROAD TO WALLIN DRIVE

SUMMARY OF QUANTITIES

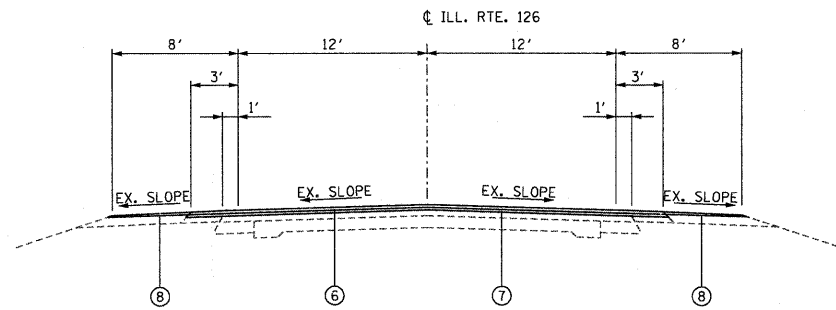
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0379	119 RS-14	WILL	20	3
CONTRACT NO. 60J95				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NTS SHEET NO. OF SHEETS STA. TO STA.

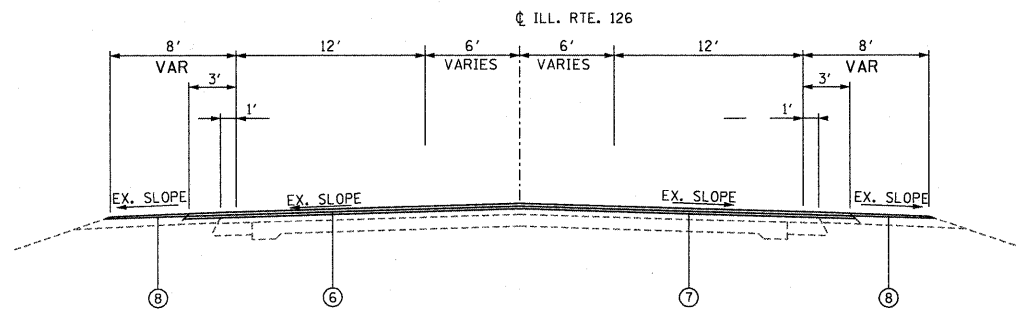
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EXISTING TYPICAL CROSS SECTION
ILLINOIS ROUTE 126
STA. 558+64.41 TO STA. 661+87.37



PROPOSED TYPICAL CROSS SECTION
ILLINOIS ROUTE 126
STA. 558+64 TO STA. 596+51
STA. 616+52 TO STA. 625+76
STA. 641+58 TO STA. 651+50



PROPOSED TYPICAL CROSS SECTION
ILLINOIS ROUTE 126
STA. 596+51 TO STA. 616+52
STA. 625+76 TO STA. 641+58
STA. 651+50 TO STA. 661+87

LEGEND

- ① EXISTING P.C.C. PAVEMENT 9" (6 1/2"-9")
- ② EXISTING BITUMINOUS SURFACE +/-6"
- ③ EXISTING BITUMINOUS SHOULDER +/-8"
- ④ EXISTING +/- 8" AGGREGATE SHOULDER, TYPE B
- ⑤ HMA SURFACE REMOVAL - 2 1/2" (SEE NOTE 1)
- ⑥ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 MM)-1 3/4"
- ⑦ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50-3/4"
- ⑧ GRADING AND SHAPING SHOULDERS & AGGREGATE WEDGE SHOULDER, TYPE B

NOTES

1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm)	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 GYR.
CLASS D PATCHES (HMA BINDER IL-19 MM)	4% @ 70 GYR.
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19MM)	4% @ 70 GYR.

MIXTURE NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 126
COUNTY LINE ROAD TO WALLIN DRIVE

MAINLINE TYPICAL SECTIONS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0379	119 RS-14	WILL	20	4
CONTRACT NO. 60J95				

SCALE: SHEET NO. OF SHEETS STA. TO STA.

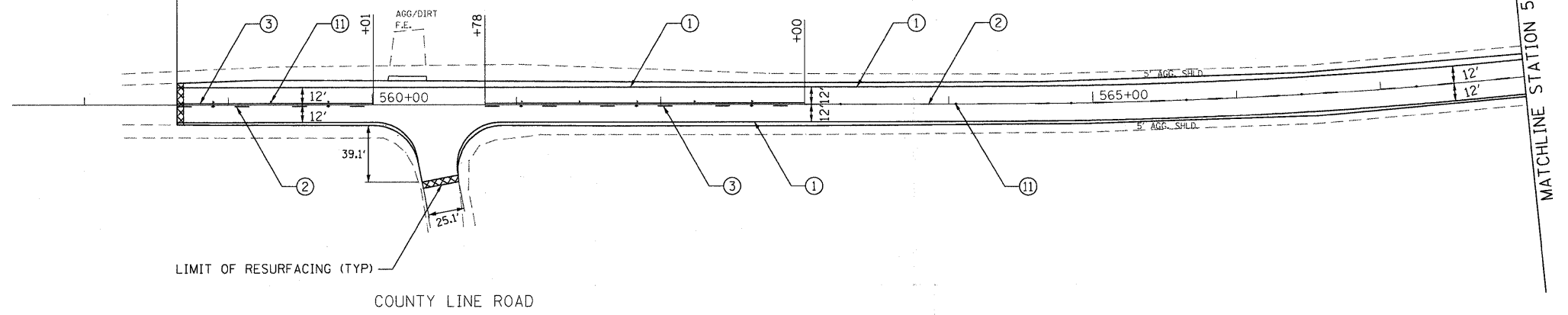
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

TYP-01



IMPROVEMENT BEGINS
STA. 558+64.41

HMA SURFACE REMOVAL -2 1/2"
RESURFACE WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
HMA SURFACE COURSE, MIX D, N70 (IL-9.5MM) -1 3/4"

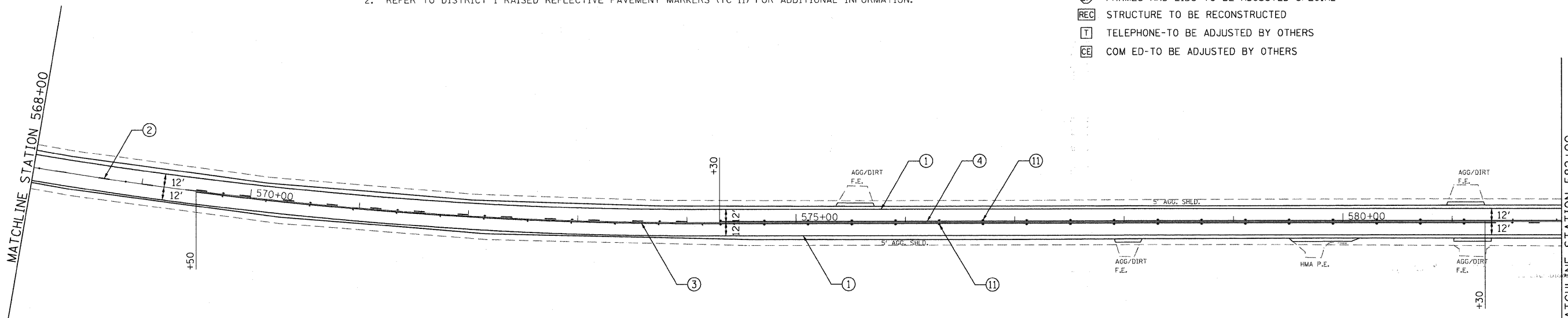


NOTES:

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

DRAINAGE LEGEND

- [A] FRAMES AND GRATES TO BE ADJUSTED
- [C] STRUCTURE TO BE CLEANED
- [A] FRAMES AND LIDS TO BE ADJUSTED
- [SP] FRAMES AND LIDS TO BE ADJUSTED SPECIAL
- [REC] STRUCTURE TO BE RECONSTRUCTED
- [T] TELEPHONE-TO BE ADJUSTED BY OTHERS
- [CE] COM ED-TO BE ADJUSTED BY OTHERS



PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
- ② THERMOPLASTIC PAVEMENT MARKING 4" YELLOW CENTER LINE (30' SKIP 10' DASH)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑤ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING 12" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑨ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑩ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑪ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

ROADWAY LEGEND

- [Hatched Box] PROPOSED SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

FILE NAME = P:\2009\ME09006_Var\Var_Plan\CADD\W09_IL126\Shets\0160\J95-sht-plan-01.dgn
 PLOT SCALE = 1"=50' / IN.
 USER NAME = Millennium Professional Services



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DATE - 2/15/2010	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 126
COUNTY LINE ROAD TO WALLIN DRIVE**

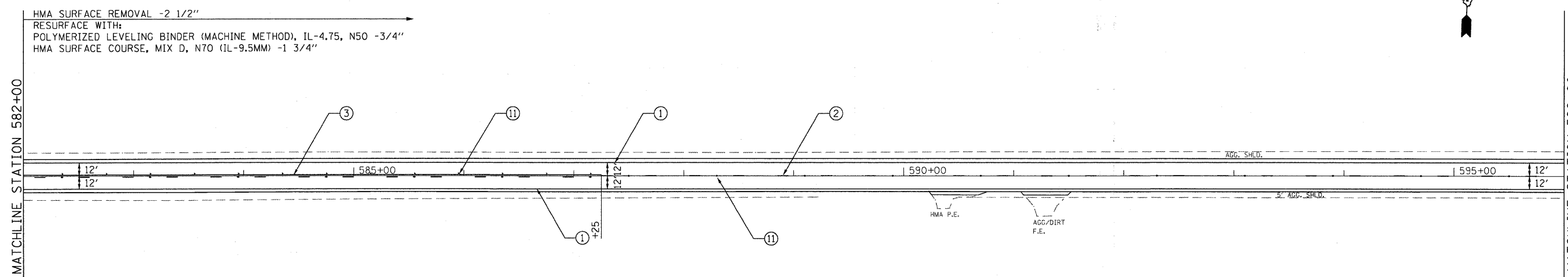
ROADWAY PLAN

SCALE: 1"=50' SHEET NO. OF SHEETS STA. 558+64 TO STA. 582+00

F.A.U. RTE. 0379	SECTION 119 RS-14	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J95	

PLN-01

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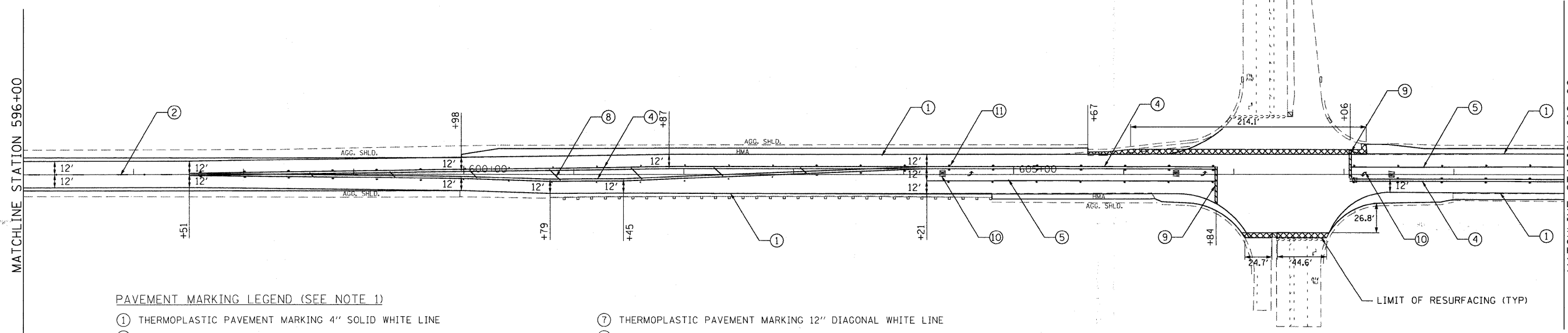
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2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

DRAINAGE LEGEND

- [A] FRAMES AND GRATES TO BE ADJUSTED
- [C] STRUCTURE TO BE CLEANED
- [A] FRAMES AND LIDS TO BE ADJUSTED
- [SP] FRAMES AND LIDS TO BE ADJUSTED SPECIAL
- [REC] STRUCTURE TO BE RECONSTRUCTED
- [T] TELEPHONE-TO BE ADJUSTED BY OTHERS
- [CE] COM ED-TO BE ADJUSTED BY OTHERS

ROADWAY LEGEND

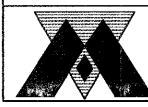
- [Cross-hatched box] PROPOSED HMA SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)



PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
- ② THERMOPLASTIC PAVEMENT MARKING 4" YELLOW CENTER LINE (30' SKIP 10' DASH)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑤ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING 12" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑨ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑩ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑪ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

FILE NAME = P:\2009\ME09006_Ver-Vor_Phi\CADD\W09_IL126\Shets\0160195-sh1-plan-02.dgn
 PLOT SCALE = 50.0000 / 1 IN.
 USER NAME = Millennia Professional Services



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 630.785.0110 voice, 630.839.2566 fax
 www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

DESIGNED - CJD	REVISED -
DRAWN - CJD	REVISED -
CHECKED - RPD	REVISED -
DATE - 2/15/2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

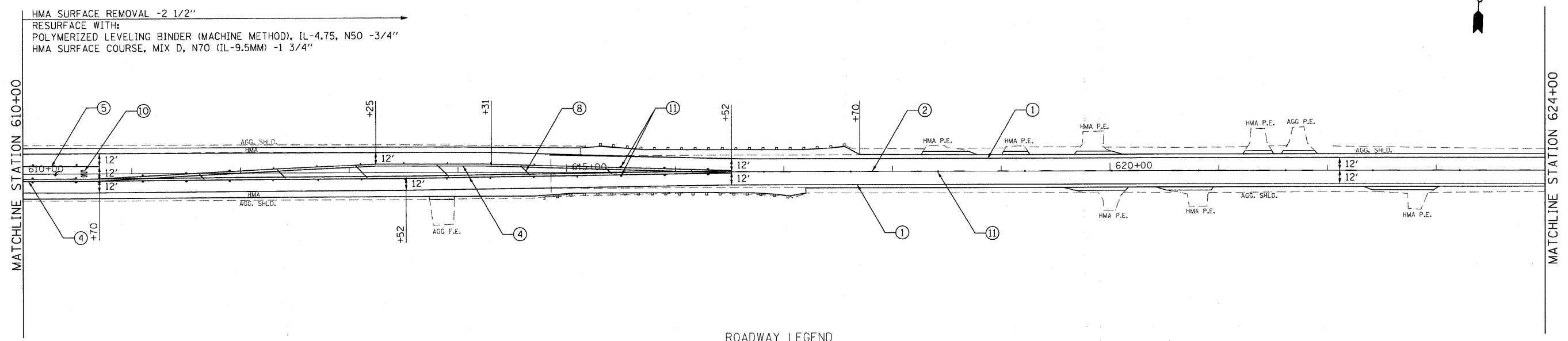
ILLINOIS ROUTE 126
COUNTY LINE ROAD TO WALLIN DRIVE
ROADWAY PLAN

SCALE: 1"=50' SHEET NO. OF SHEETS STA. 582+00 TO STA. 610+00

F.A.U. RTE. 0379	SECTION 119 RS-14	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 6
CONTRACT NO. 60J95				


PLN-02

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








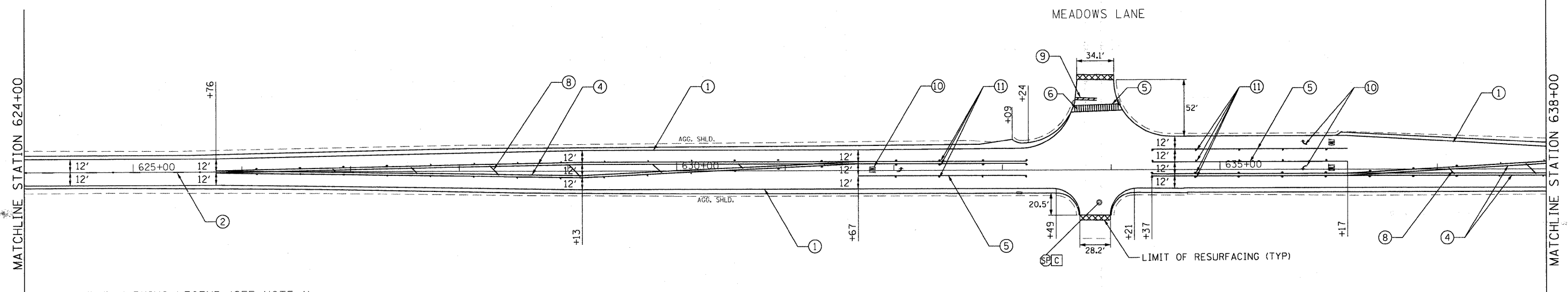
- NOTES:**
- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 - REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

ROADWAY LEGEND












 PROPOSED HMA SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

DRAINAGE LEGEND

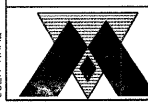
-  FRAMES AND GRATES TO BE ADJUSTED
-  STRUCTURE TO BE CLEANED
-  FRAMES AND LIDS TO BE ADJUSTED
-  FRAMES AND LIDS TO BE ADJUSTED SPECIAL
-  STRUCTURE TO BE RECONSTRUCTED
-  TELEPHONE-TO BE ADJUSTED BY OTHERS
-  COM ED-TO BE ADJUSTED BY OTHERS



PAVEMENT MARKING LEGEND (SEE NOTE 1)

-  THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
-  THERMOPLASTIC PAVEMENT MARKING 4" YELLOW CENTER LINE (30' SKIP 10' DASH)
-  THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
-  THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
-  THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
-  THERMOPLASTIC PAVEMENT MARKING 12" WHITE LINE
-  THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
-  THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
-  THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
-  THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
-  RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

FILE NAME = P:\2009\ME09005_Ver-Var-Plan\CADD\W09_IL126\Shets\1126\03.dgn
 PLOT SCALE = 50.0000 / IN.
 USER NAME = Millennia Professional Services



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DESIGNED - CJD	REVISED -
DRAWN - CJD	REVISED -
CHECKED - RPD	REVISED -
DATE - 2/15/2010	REVISED -

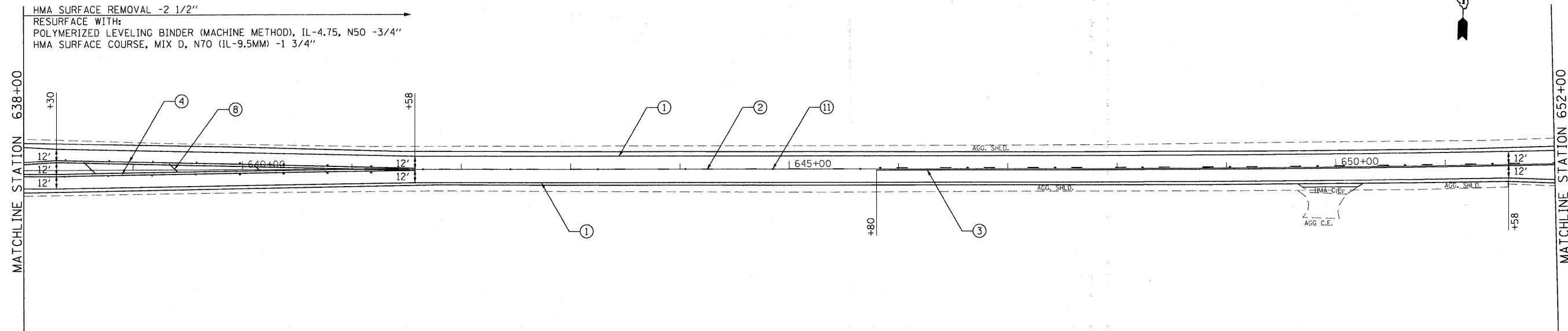
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 126
COUNTY LINE ROAD TO WALLIN DRIVE
ROADWAY PLAN
 SCALE: 1"=50' SHEET NO. OF SHEETS STA. 610+00 TO STA. 638+00

F.A.U. RTE. 0379	SECTION 119 RS-14	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 7
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J95	

PLN-03

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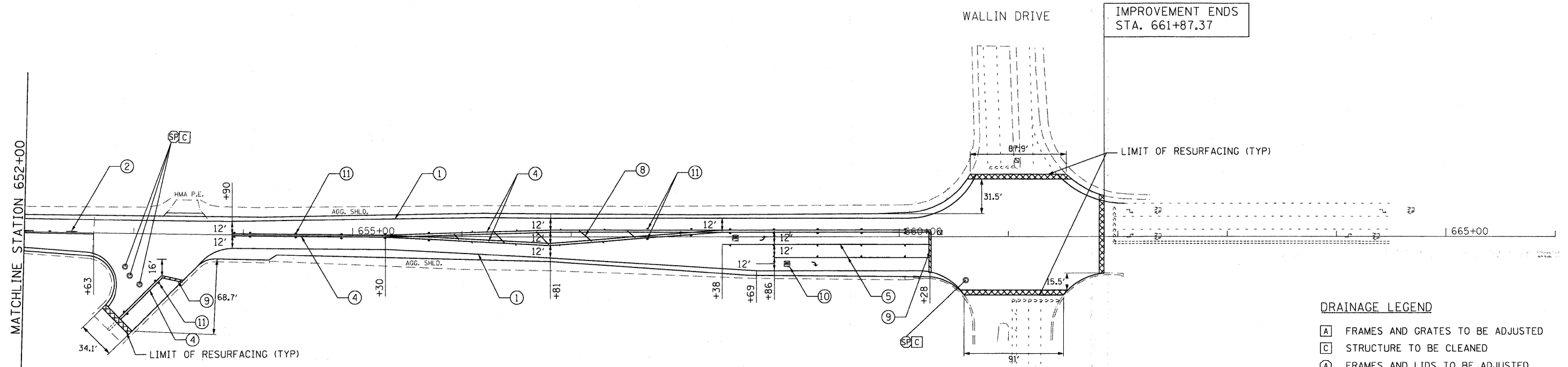


PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE
- ② THERMOPLASTIC PAVEMENT MARKING 4" YELLOW CENTER LINE (30' SKIP 10' DASH)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑤ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING 12" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑨ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑩ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑪ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

NOTES:

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.



INDIAN BOUNDARY ROAD

ROADWAY LEGEND

PROPOSED SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

DRAINAGE LEGEND

- A FRAMES AND GRATES TO BE ADJUSTED
- C STRUCTURE TO BE CLEANED
- A FRAMES AND LIDS TO BE ADJUSTED
- SP FRAMES AND LIDS TO BE ADJUSTED SPECIAL
- REC STRUCTURE TO BE RECONSTRUCTED
- T TELEPHONE-TO BE ADJUSTED BY OTHERS
- CE COM ED-TO BE ADJUSTED BY OTHERS

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PLOT SCALE = 50.0000 / IN.
USER NAME = Millennium Professional Services

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MILLENNIA PROFESSIONAL SERVICES

DESIGNED - CJD	REVISED -
DRAWN - CJD	REVISED -
CHECKED - RPD	REVISED -
DATE - 2/15/2010	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ILLINOIS ROUTE 126
COUNTY LINE ROAD TO WALLIN DRIVE**

ROADWAY PLAN

SCALE: 1"=50' SHEET NO. OF SHEETS STA. 638+00 TO STA. 665+00

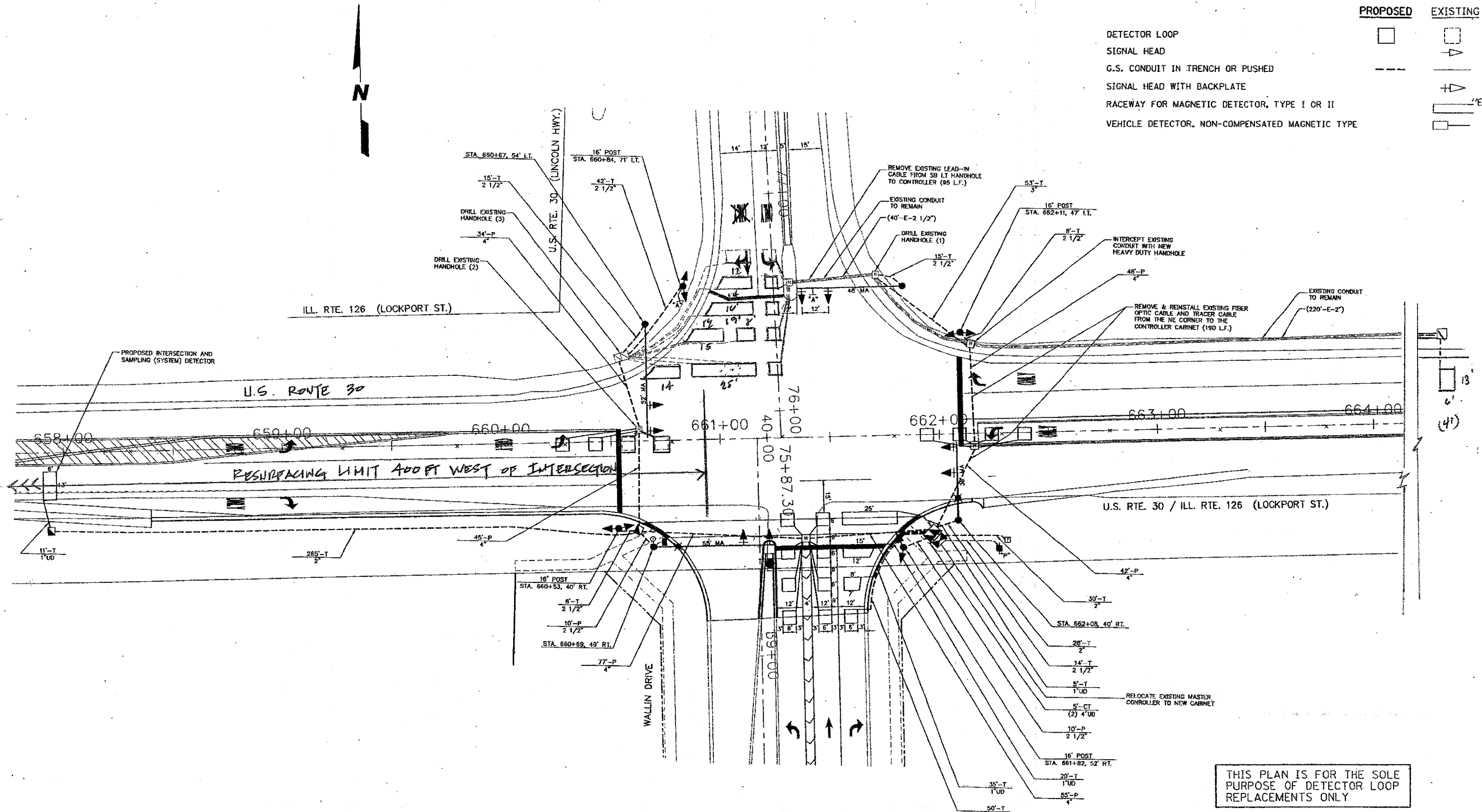
F.A.U. RTE. 0379	SECTION 119 RS-14	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60J95	

PLN-04

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TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

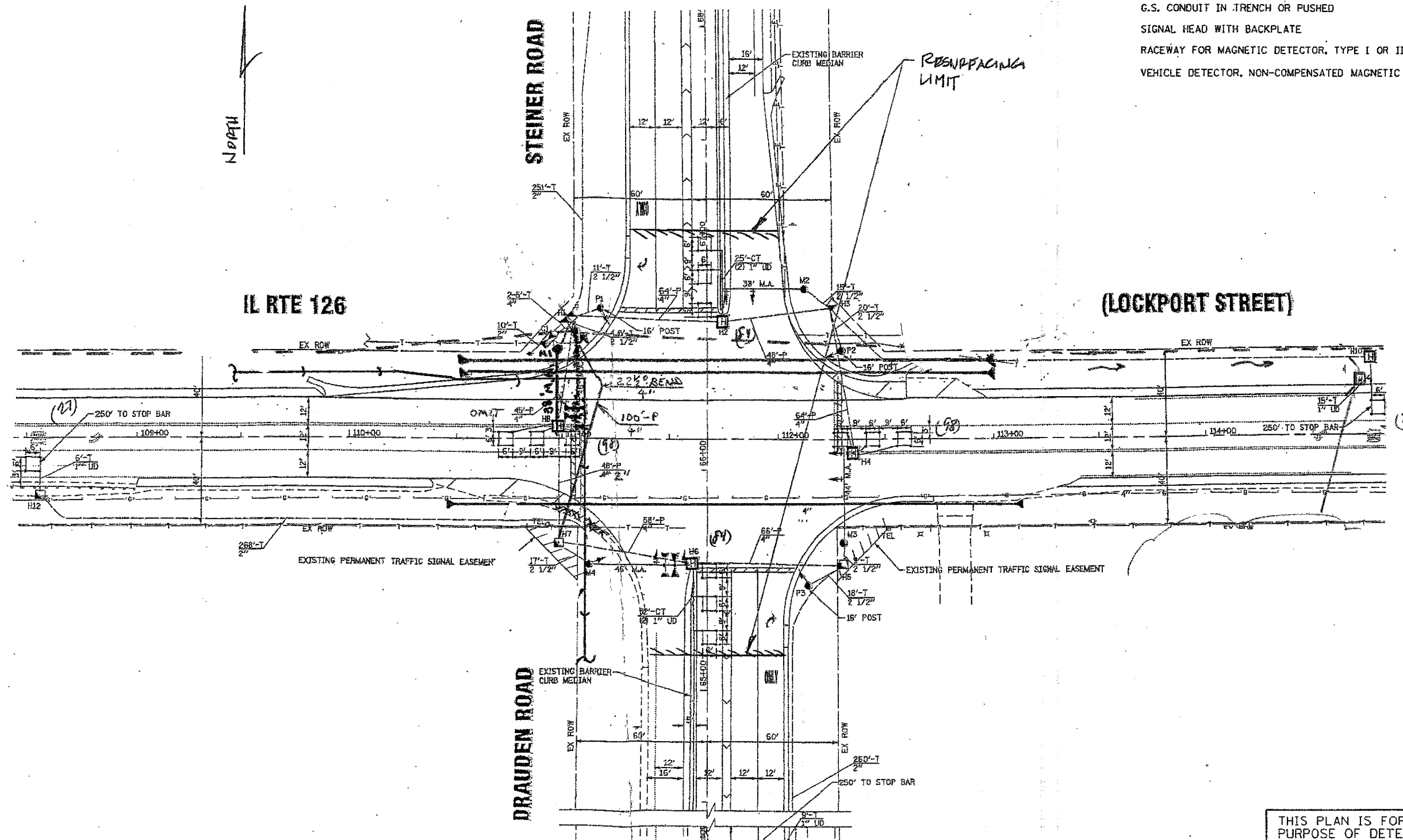
— FOR INFORMATION ONLY —

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86600600		FOOT	DETECTOR LOOP, REPLACEMENT

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	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -								
	PLOT DATE = 4/3/2007	DATE	REVISED -								

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

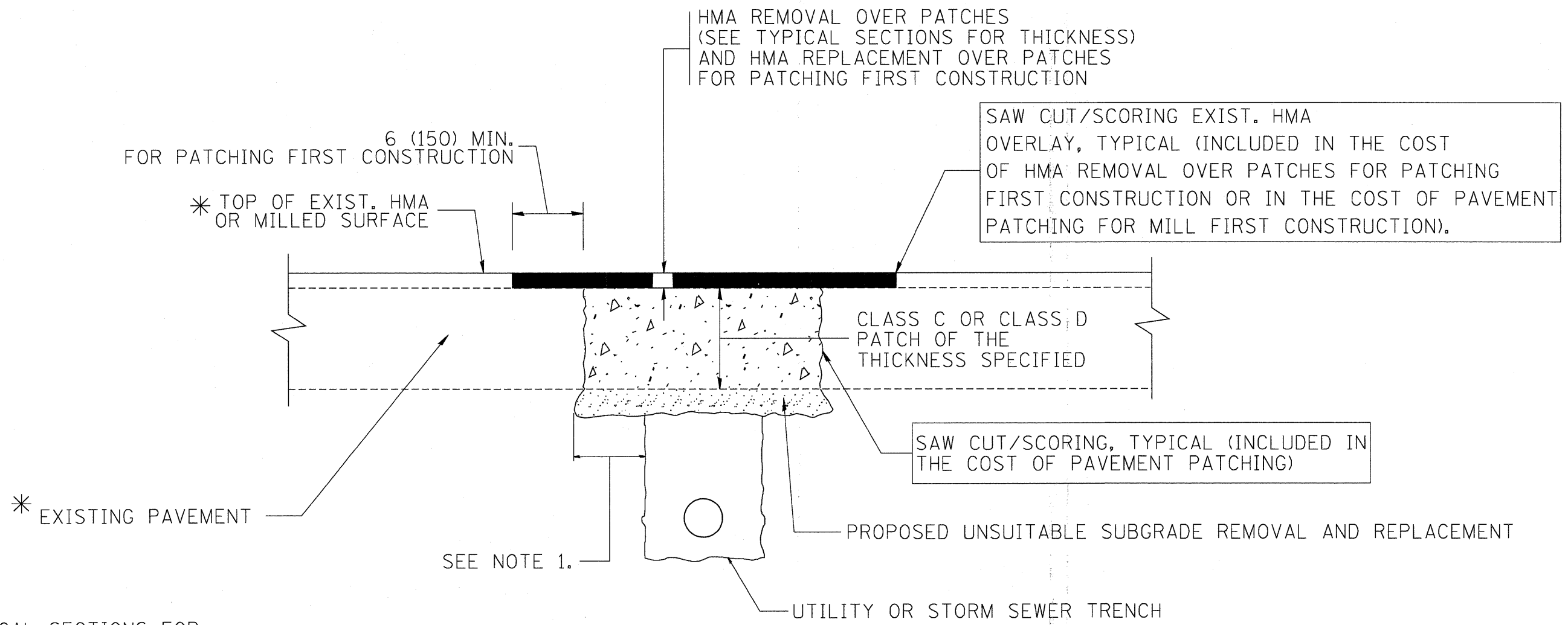
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 126 @ STEINER/DRAUDEN

F. ALL. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0379	119 RS-14	WILL	20	10
CONTRACT NO.			60J95	
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

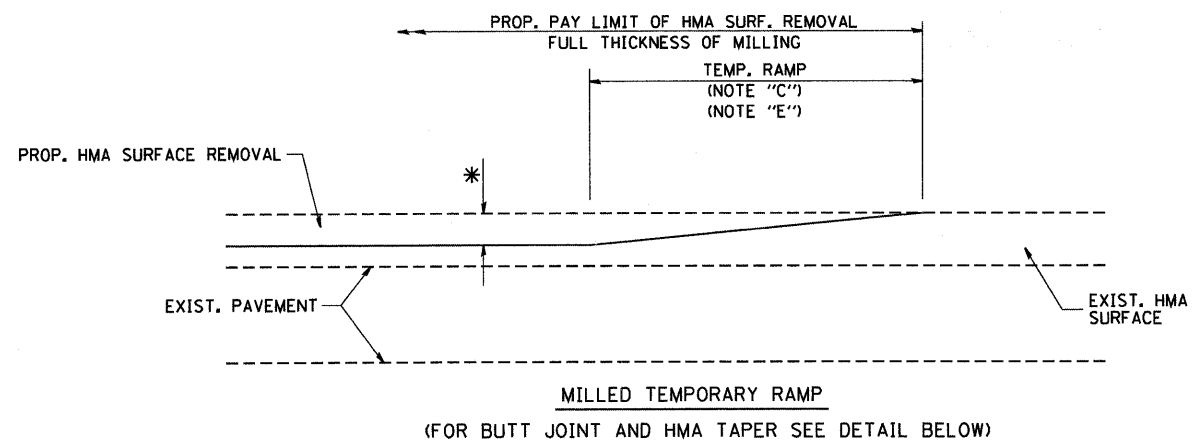
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

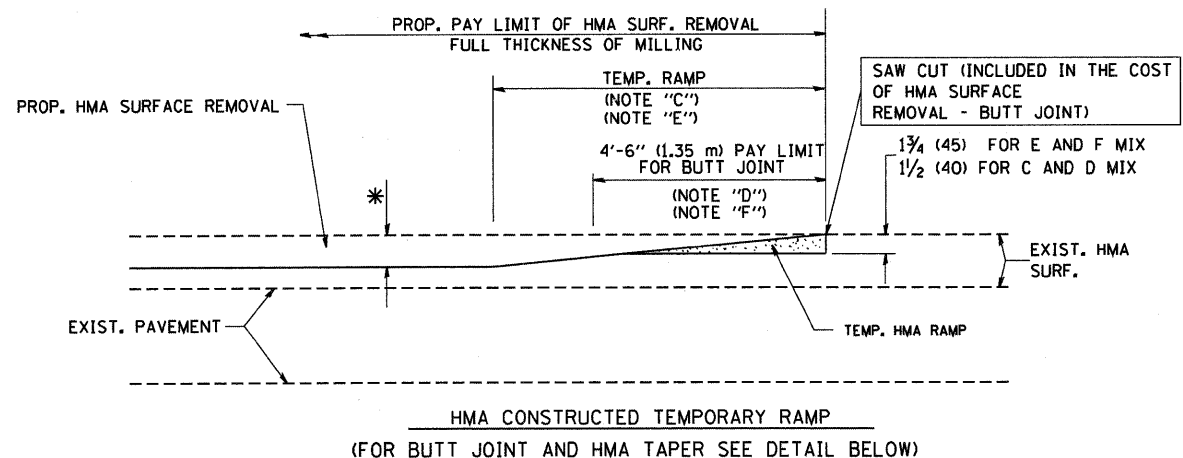
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

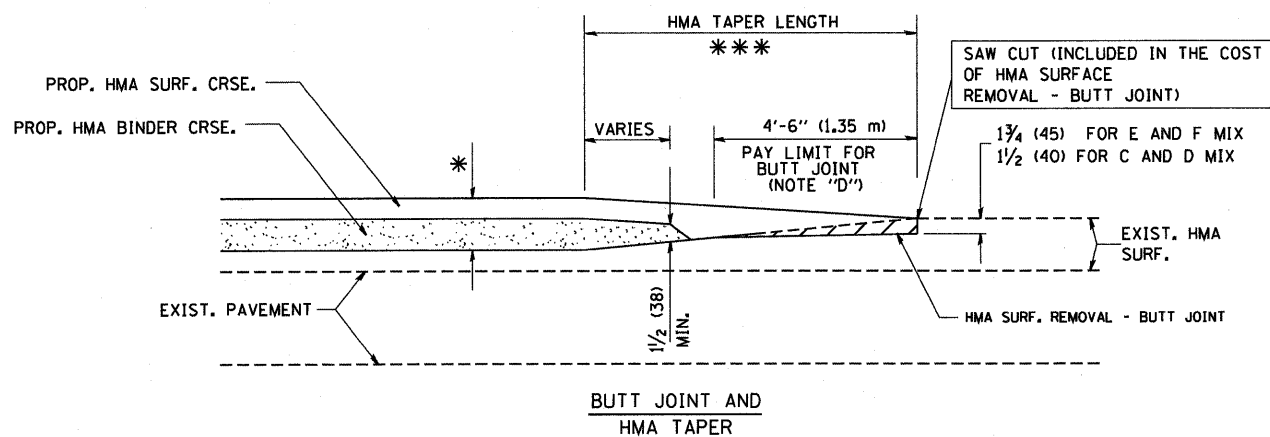
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07					0379	119 RS-14	WILL	20	11
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								FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



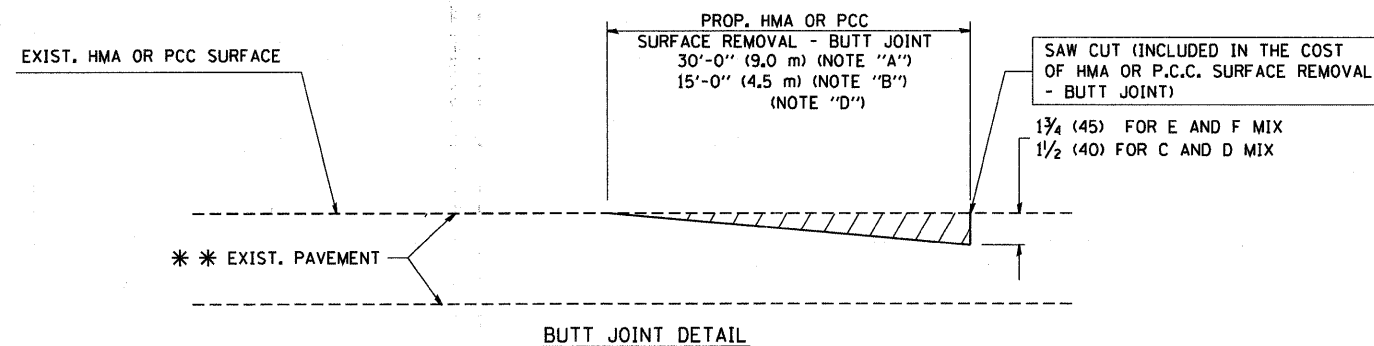
OPTION 1



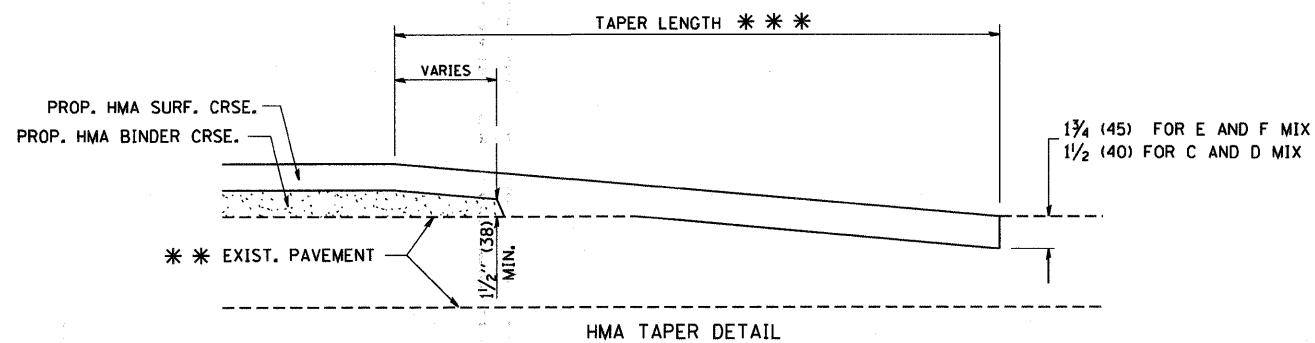
**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

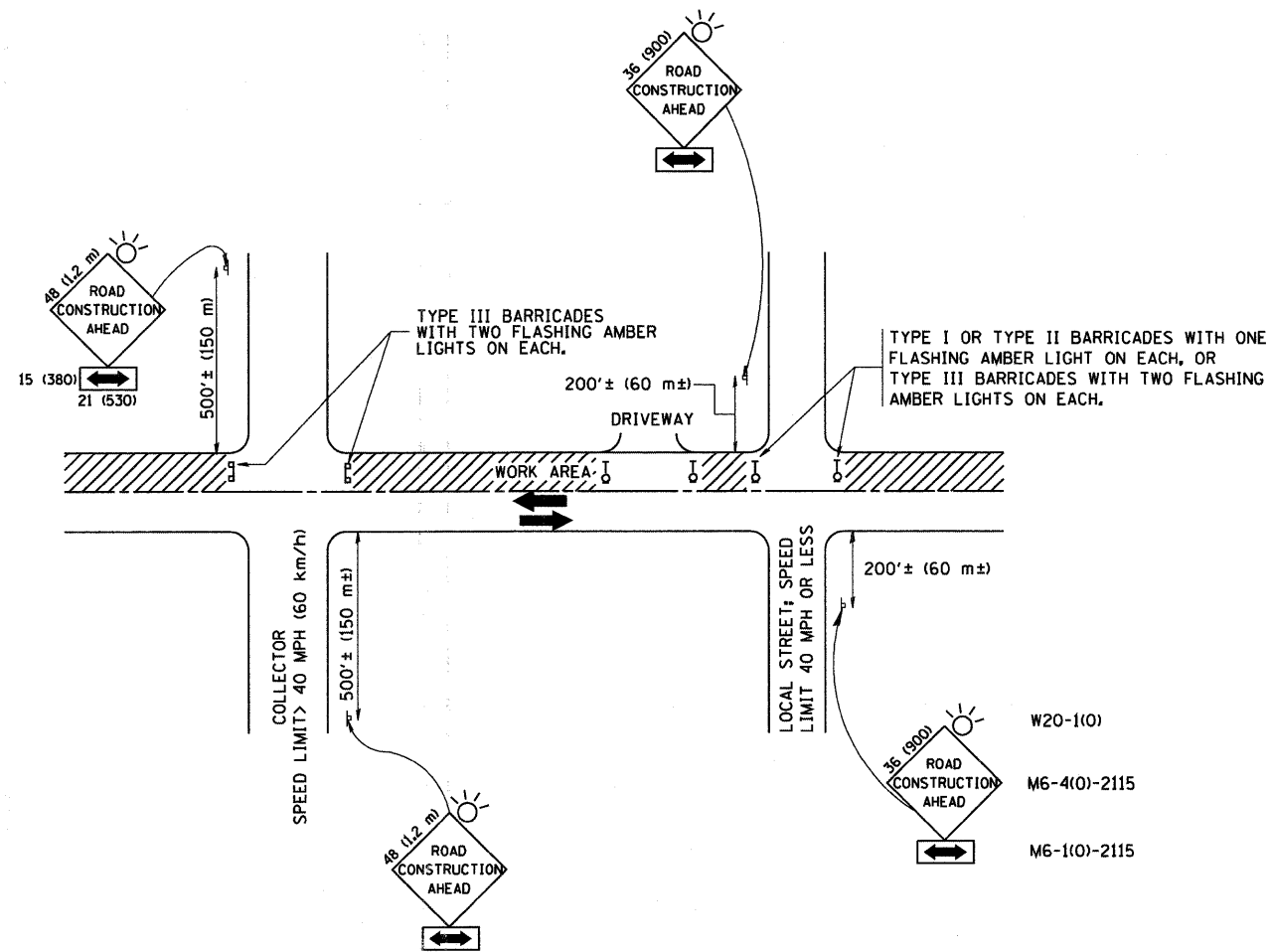
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	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2000	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0379	119 RS-14	WILL	20	12
BD400-05 BD32			CONTRACT NO. 60J95	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

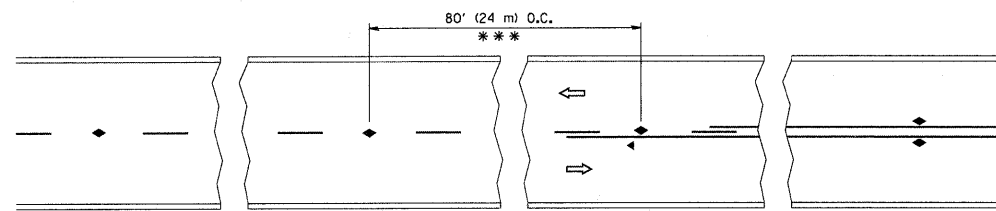
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

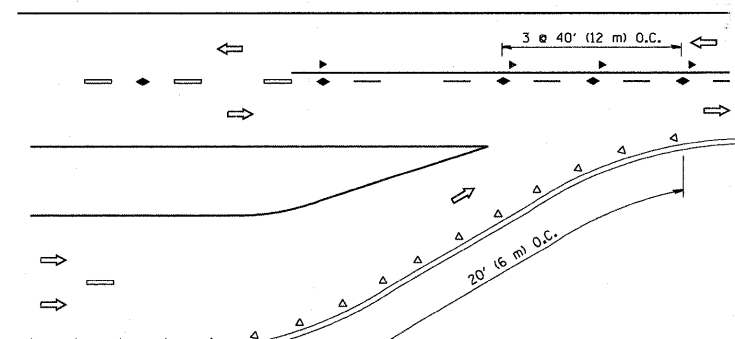
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F.A.R. RTE. 0379	SECTION 119 RS-14	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 13
TC-10		CONTRACT NO. 60J95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

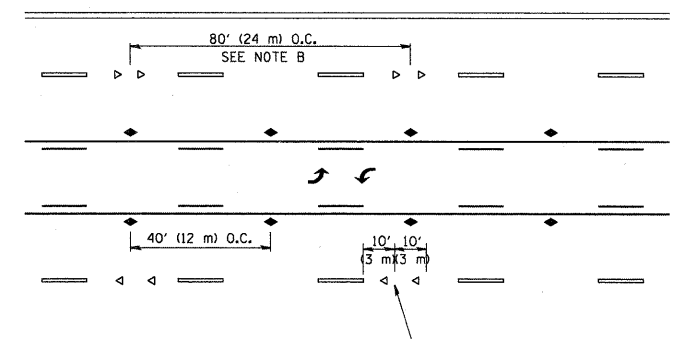


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

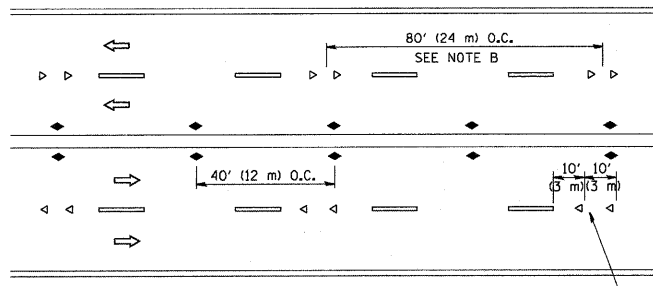
TWO-LANE/TWO-WAY



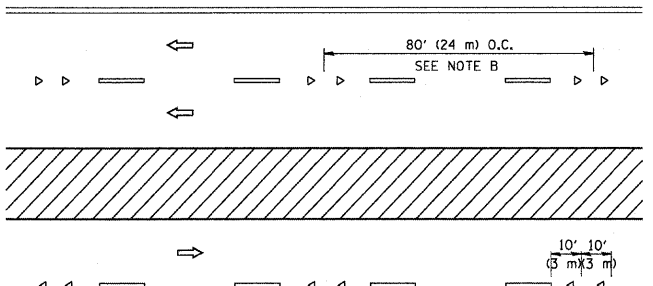
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

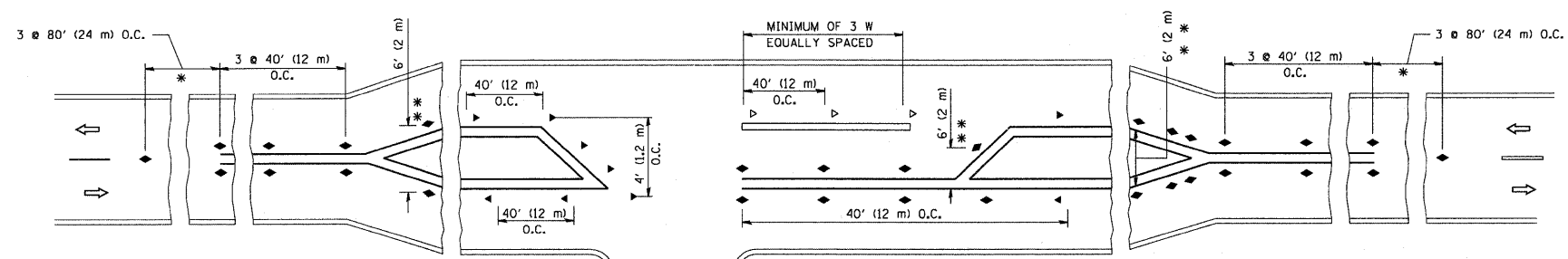
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

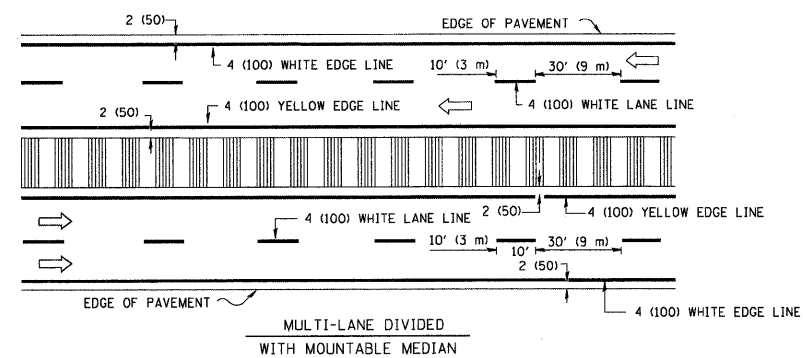
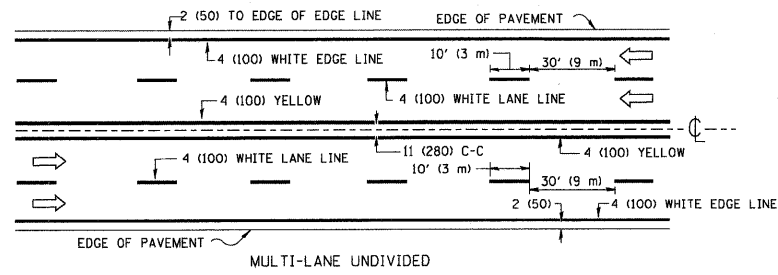
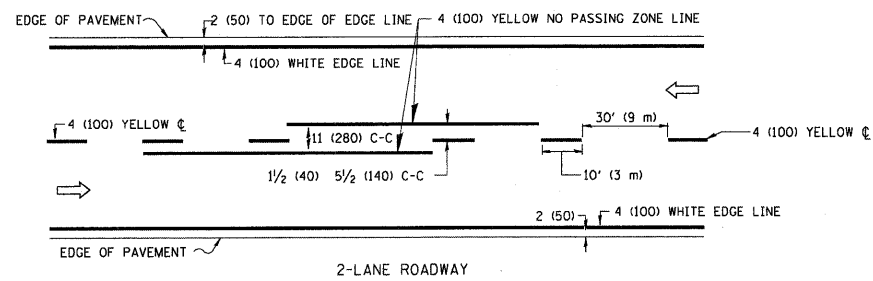
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		CHECKED -	REVISED - T. RAMMACHER 01-06-00
		DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLow RESISTANT)

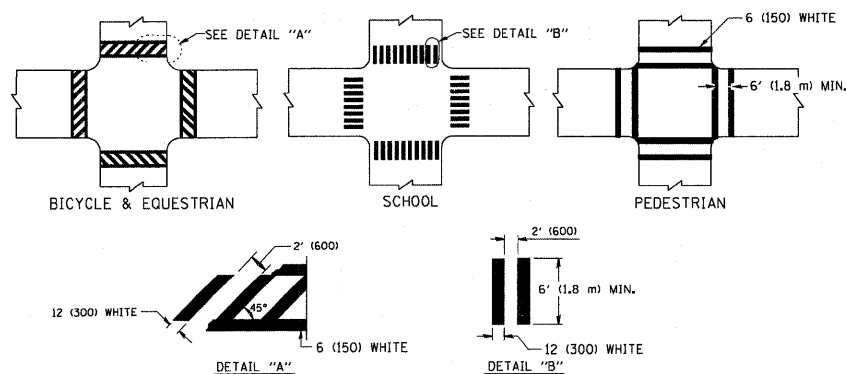
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0379	119 RS-14	WILL	20	14
TC-11			CONTRACT NO. 60J95	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

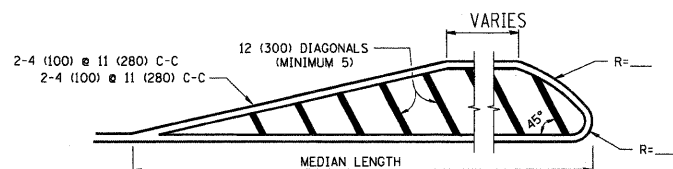
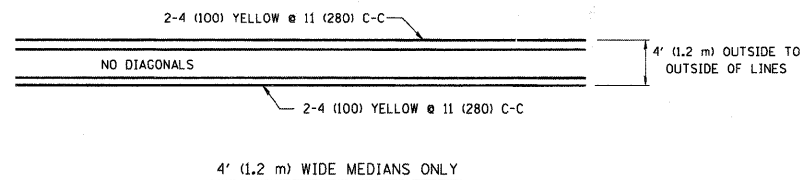


NOTE: MEDIANS WITH BARRIER CURBS DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

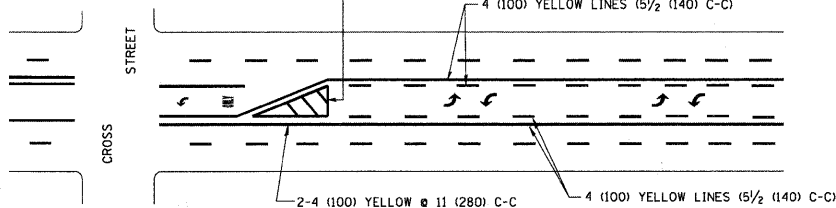


TYPICAL CROSSWALK MARKING

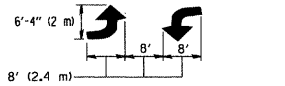


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

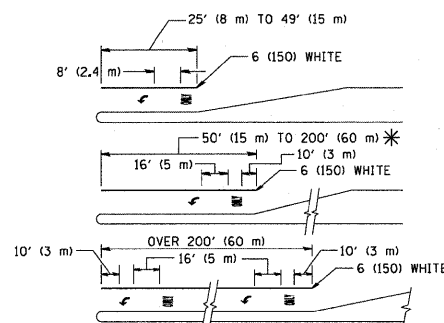


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

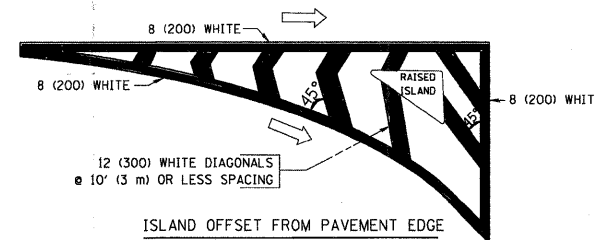


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

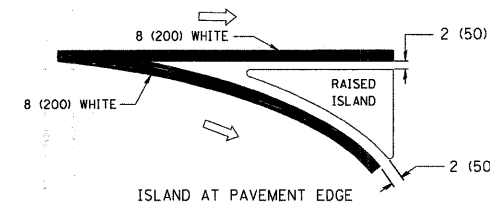
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



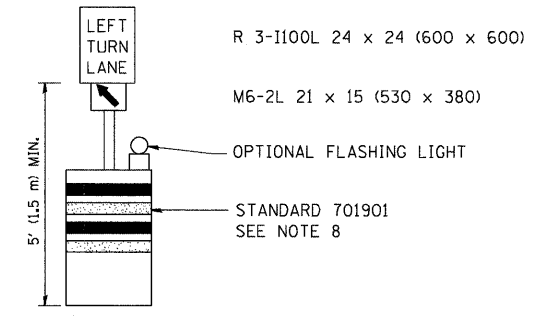
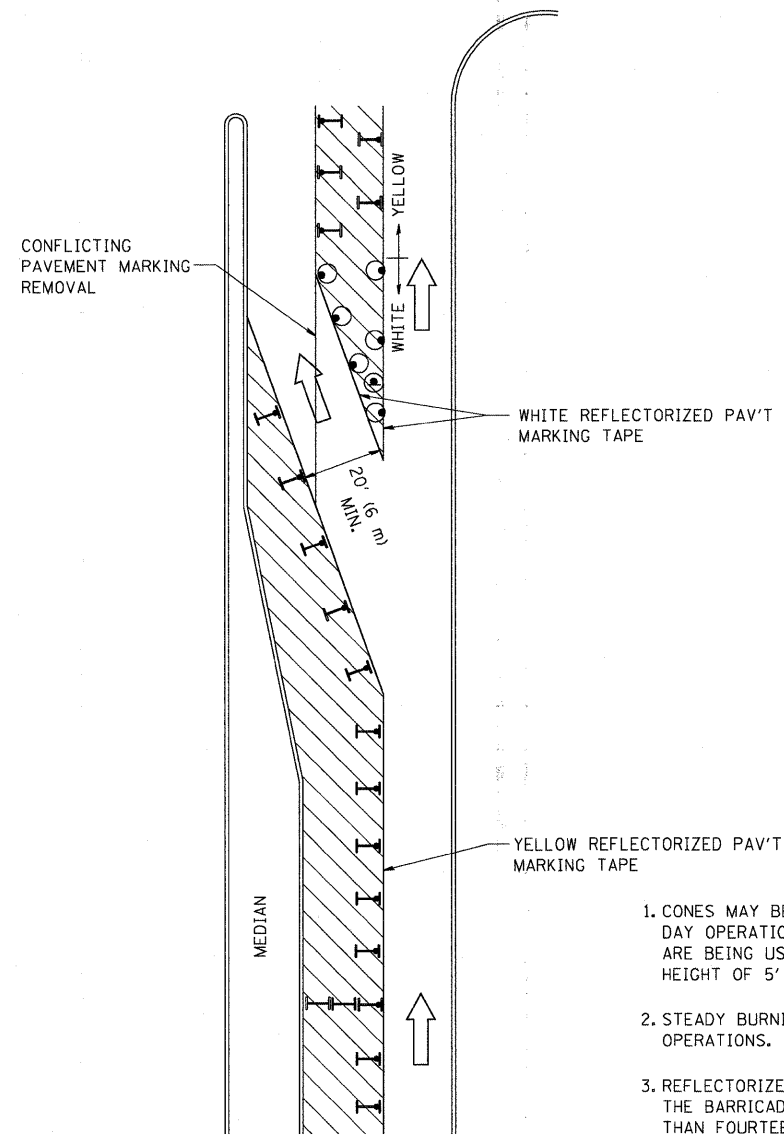
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

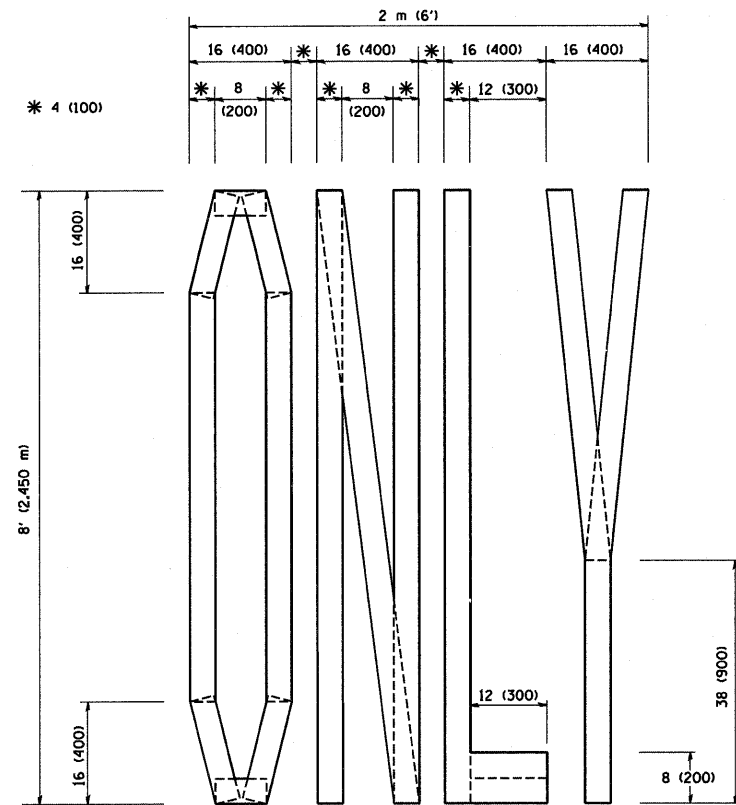
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PLOT DATE = 9/14/2009			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

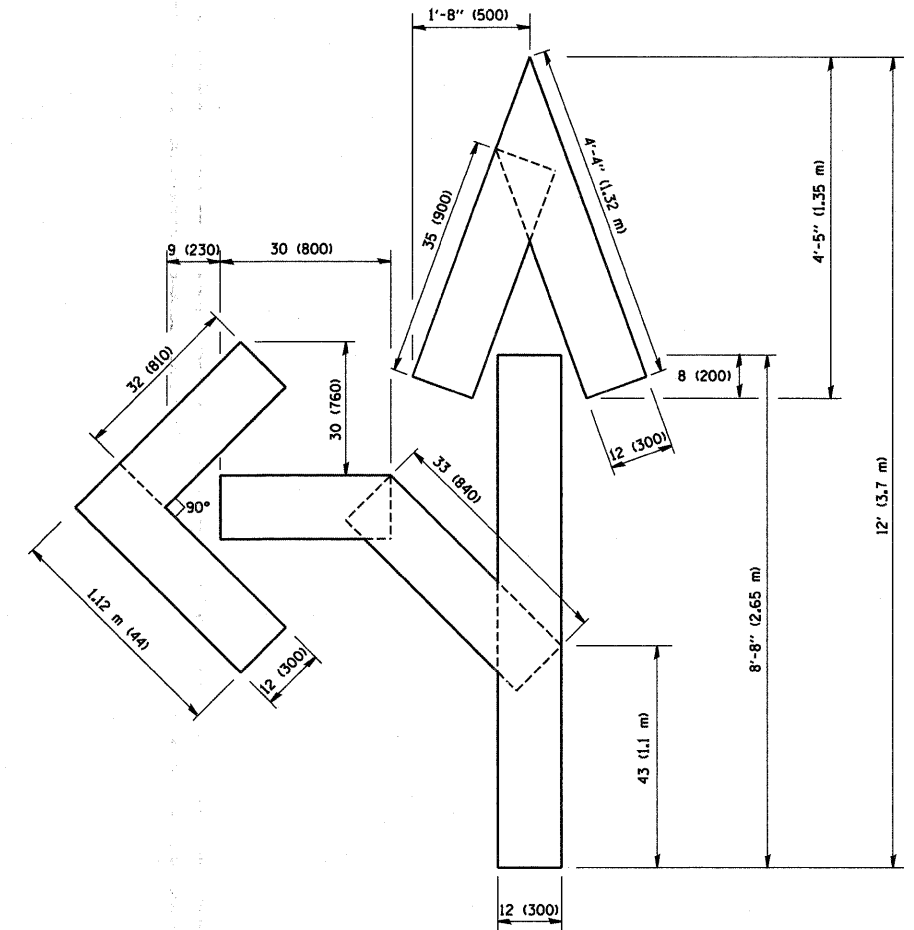
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

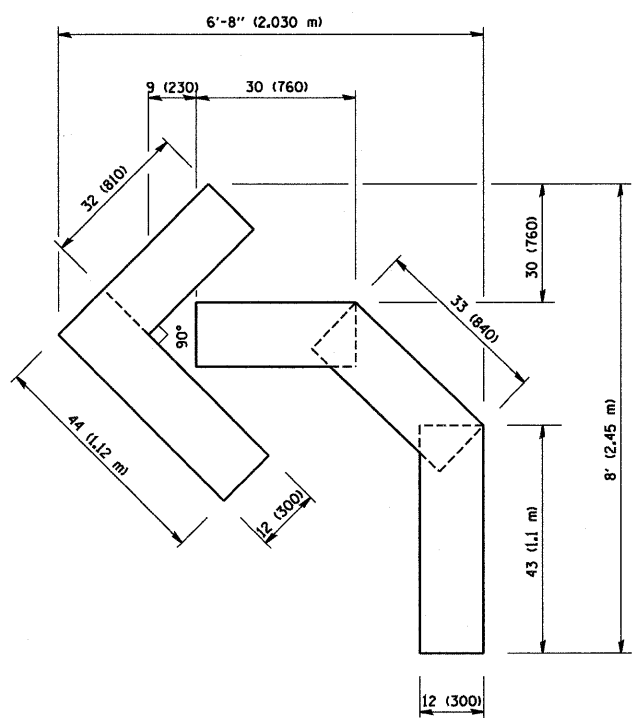
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0379	119 RS-14	WILL	20	16
TC-14		CONTRACT NO. 60J95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

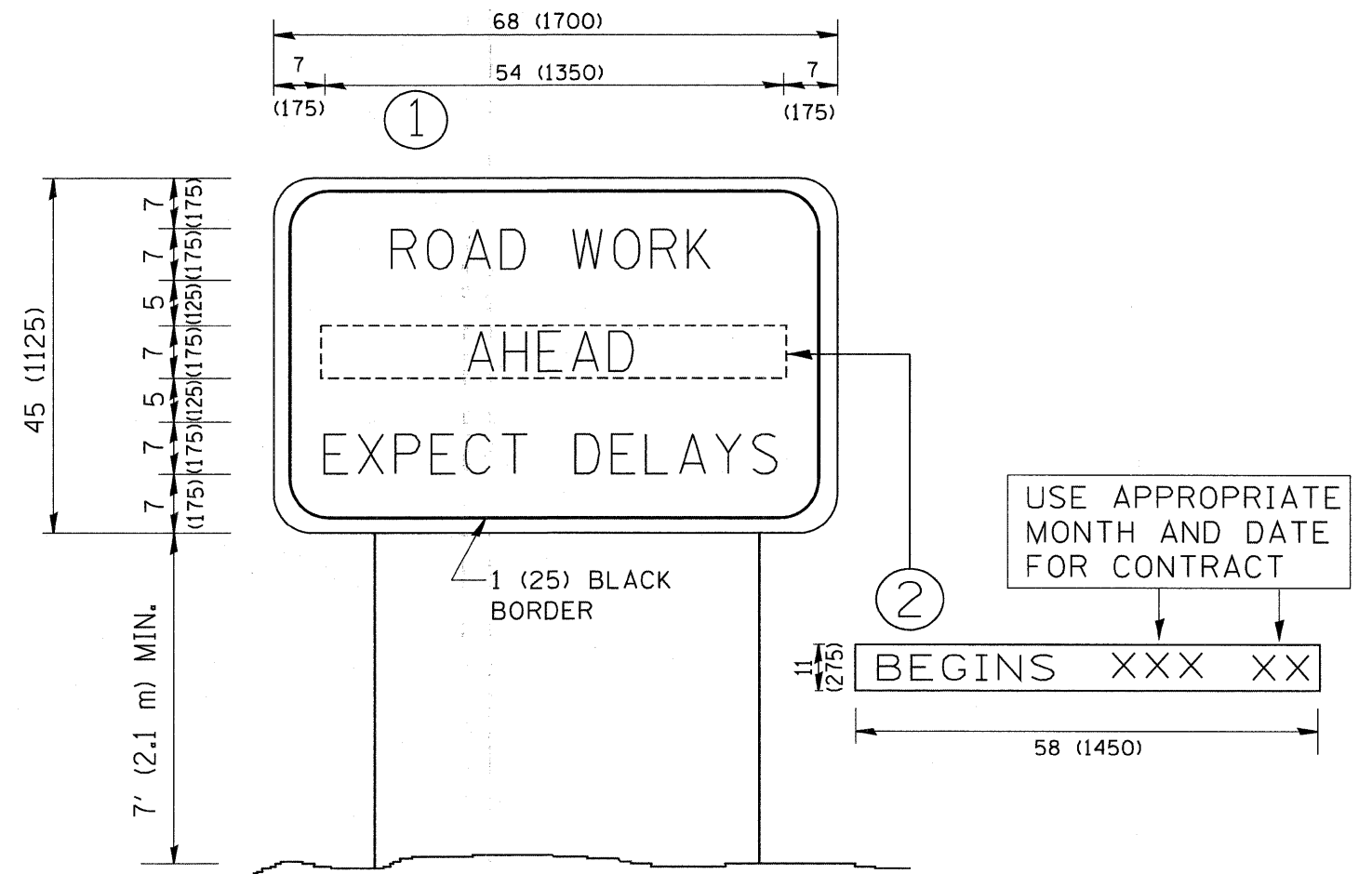
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		CHECKED -	REVISED - T. RAMMACHER 03-02-98
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 0379	SECTION 119 RS-14	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 17
TC-16		CONTRACT NO. 60J95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

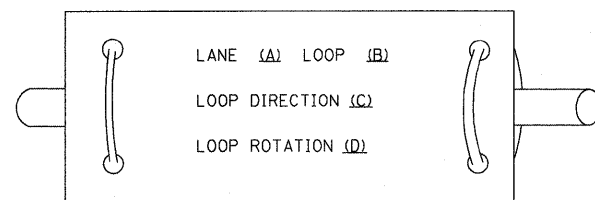
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-22 CONTRACT NO. 60J95			
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

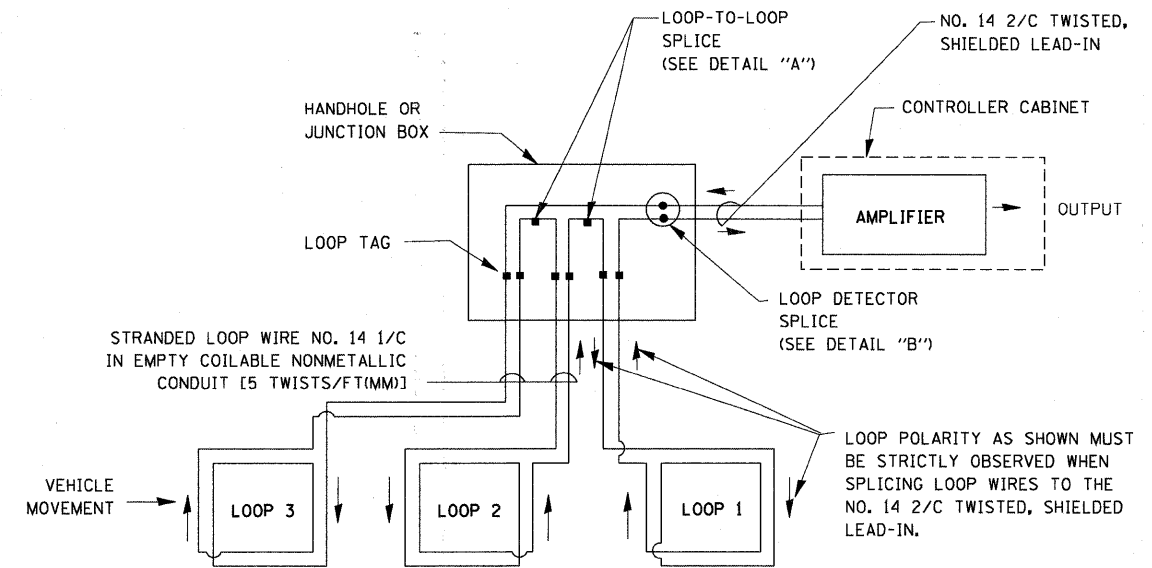
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

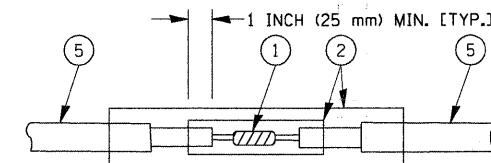


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

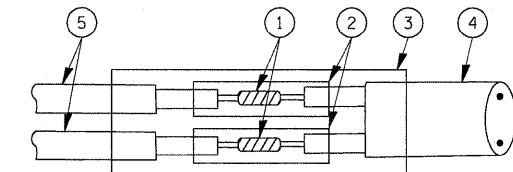


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

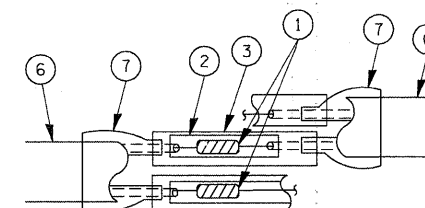


DETAIL "A"
LOOP-TO-LOOP SPLICE

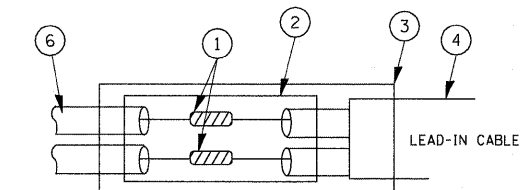


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

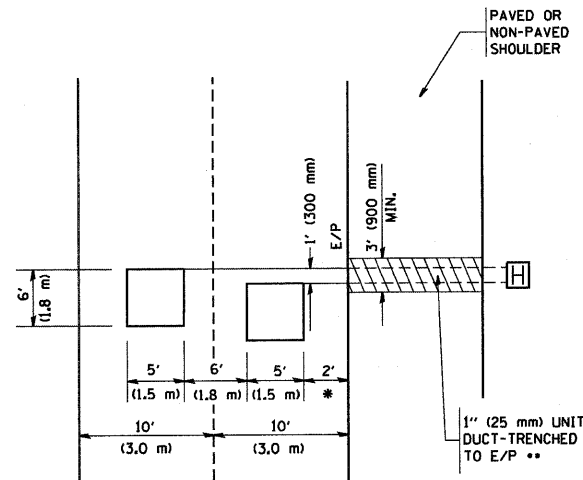
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P. RTE. 0379	SECTION 119 RS-14	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 19
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		CHECKED - DAD	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 10-28-09	REVISED -									

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

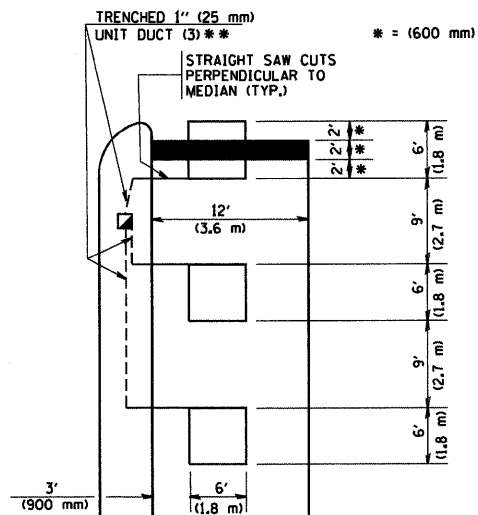


* = (600 mm)

*** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

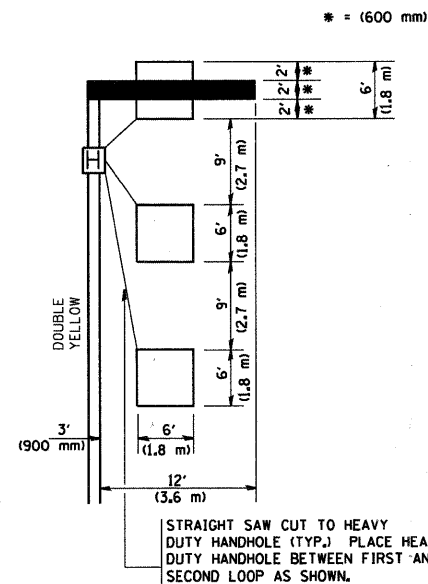
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



*** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

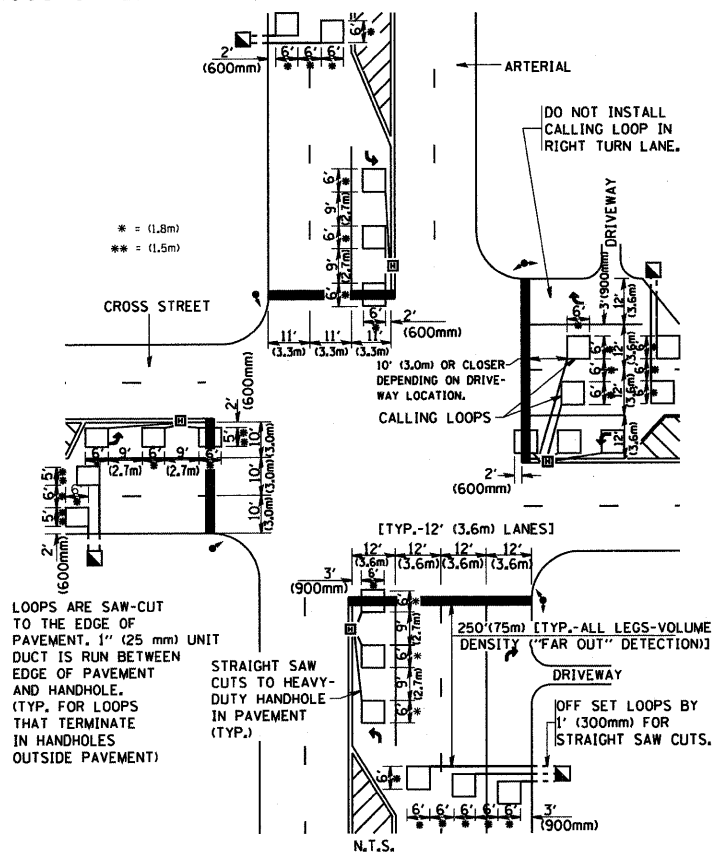
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



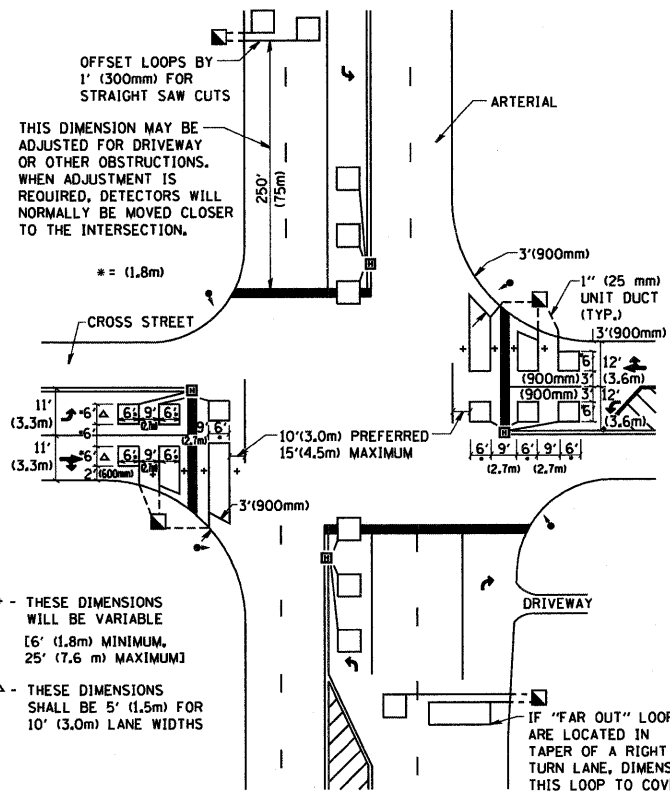
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn

USER NAME = geg1enobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 0379	SECTION 119 RS-14	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 20
TS-07		CONTRACT NO. 60J95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				