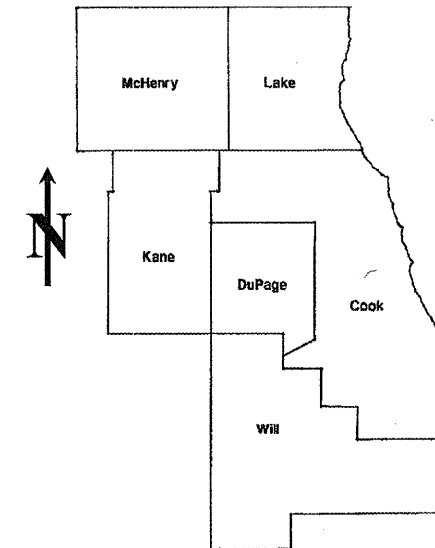


ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NUMBER
VARIOUS	2010-015 RS	COOK & WILL	26	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
DISTRICT ONE
PROPOSED HIGHWAY PLANS

CONTRACT NO. 60K34

D-91-473-10



LOCATION OF IMPROVEMENT INDICATED THUS: 

FOR INDEX OF SHEETS SEE SHEET 2

VARIOUS ROUTES
 SECTION: 2010-015 RS
 VARIOUS LOCATIONS IN SOUTHERN COOK COUNTY & WILL COUNTY
 INTERMITTENT PAVEMENT RESURFACING
 COOK & WILL COUNTY
 C-91-473-10

DISTRICT ONE - DESIGN - PLAN PREPARATION ENGINEER:
 KEN ENG / (847) 705-4247

CONTRACT NO. 60K34

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
 SUBMITTED: FEBRUARY 16, 2010
Diane M. O'Keefe
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 19, 2010
Scott E. Still P.E.
 acting ENGINEER OF DESIGN AND ENVIRONMENT
March 19, 2010
Christine M. Reed
 DIRECTOR, DIVISION OF HIGHWAYS

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

J.U.L.I.E.: JOINT UTILITY LOCATION
INFORMATION FOR EXCAVATION
(312) 744-7000

Rev.

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

<u>SHEET NO.</u>	<u>DESCRIPTION</u>	<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
1	TITLE SHEET	000001-05	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY
3	SUMMARY OF QUANTITIES	701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
4	GENERAL LOCATION MAP	701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY
5	SUMMARY OF PATCHING SCHEDULE	701336-05	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES
6-19	PATCHING SCHEDULE	701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
20	BUTT JOINT AND HMA TAPER DETAILS	701601-06	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
21	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS	701606-06	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
22	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
23	DISTRICT ONE TYPICAL PAVEMENT MARKINGS	701901-01	TRAFFIC CONTROL DEVICES
24	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)		
25	ARTERIAL ROAD INFORMATION SIGN		
26	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING		

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MS. PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER AT (708) 597-9800 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL PAVEMENT PATCHES SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR MILLING AND PATCHING SHALL BE TWO (2) FEET.

NO PATCHING OR RESURFACING IS TO BE DONE WITHIN FIFTY (50) FEET OF ANY RAILROAD CROSSING WITHOUT OBTAINING THE PROPER RAILROAD PROTECTIVE LIABILITY INSURANCE.

THE COST OF TRAFFIC CONTROL AND PROTECTION FOR THE PROJECT SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED ROAD WORK.

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

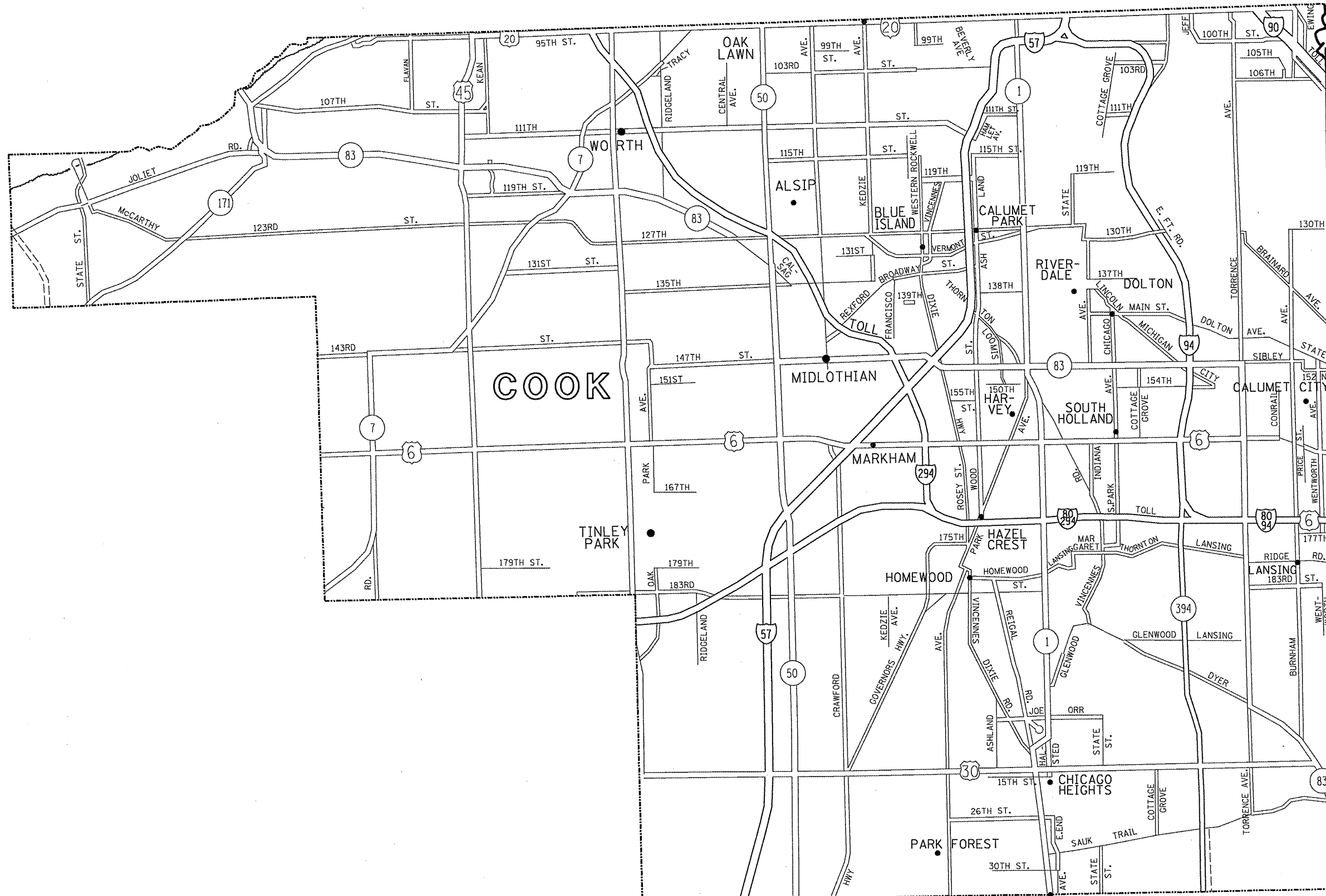
MIXTURE TYPE	AC TYPE	AIR VOIDS (%)
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5MM), 2"	PG 64-22	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\PW100T\WILGREENDP\08183625\design.dgn	DRAWN -	REVISED -	VAR.			2010-015 RS	COOK	26	2	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60K34							
PLOT DATE = 2/17/2010	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO. OF SHEETS STA. TO STA.					

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	COOK COUNTY	WILL COUNTY					TOTAL QUANTITIES							
				1000	1000												
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	11	9	2												
40600300	AGGREGATE (PRIME COAT)	TON	56	45	11												
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	42	33	9												
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	834	663	171												
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3113	2476	637												
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	27786	22103	5683												
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	5	1												
67100100	MOBILIZATION	L SUM	1	0.9	0.1												
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	6824	5066	1758												
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	2275	1689	586												
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	100	100													
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	34823	25223	9600												
*78000300	THERMOPLASTIC PAVEMENT MARKING - LINE 5"	FOOT	1650		1650												
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	740	740													
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	100	100													
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	150	150													
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1118	956	162												
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1118	956	162												
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	675	500	175												
X0322256	TEMPORARY INFORMATION SIGNING	SO FT	874	822	52												
* SPECIALTY ITEM																	

FILE NAME =	USER NAME = wlgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SUMMARY OF QUANTITIES				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw\work\FWIDOT\WILGREENDP\0103625\Designdp	DRAWN -	REVISED -	VAR.									2010-015 RS	COOK	26	3	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	SCALE:									SHEET NO.	OF	SHEETS	STA.	TO STA.
PLOT DATE = 2/19/2010	DATE -	REVISED -									CONTRACT NO. 60K34					



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PLOT DATE = 2/10/2010

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DATE -

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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL LOCATION MAP - SOUTH COOK COUNTY

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-015 RS	COOK	26	4
CONTRACT NO. 60K34				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SUMMARY - SOUTHERN COOK COUNTY ROUTES	HMA 2" MILL & RESURFACE (SY)
111TH ST. (RIDGELAND AVE. TO HARLEM AVE.)	1696
127TH ST (CENTRAL AVE. TO RIDGELAND AVE.)	143
135TH ST. (CENTRAL AVE. TO RIDGELAND AVE.)	3765
159TH ST. (CICERO AVE. TO I-294)	1155
CICERO AVE. (147TH ST. TO 159TH ST.)	493
CRAWFORD AVE. (159TH ST. TO 175TH ST.)	323
HALSTED ST. (JOE ORR RD. TO IL 1 CUTOFF)	225
IL 83 (127TH ST. TO ARCHER AVE.)	3593
PARK AVE. (154TH ST. TO 167TH ST.)	748
LAGRANGE RD. (112TH ST. TO 142ND ST.)	4345
LOOMIS ST. (THORNTON RD. TO 150TH ST.)	145
WOOD ST. (127TH ST. TO THORNTON RD.)	92
MAIN ST. IN LEMONT (IL 83 AT IL 171 TO PARKER RD.)	3072
SIBLEY BLVD. (I-94 TO TORRENCE AVE.)	419
TORRENCE AVE. (RIDGE RD. TO I-80/94)	1207
US 30 (I-394 TO HALSTED ST.)	682
IL 394 NB (STEGER RD. TO RELOCATED IL 1) - LOCATED IN WILL COUNTY	2484
IL 394 SB (STEGER RD. TO RELOCATED IL 1) - LOCATED IN WILL COUNTY	3199
SOUTHERN COOK COUNTY TOTALS =	27786 SY

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -
at\pwork\WIDOT\WILGREENDP\0183625\	design.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/19/2010	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF PATCHING SCHEDULE
SOUTHERN COOK COUNTY**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2010-015 RS	COOK	26	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60K34	

ROUTE: 135th St. (Central Ave. to Ridgeland Ave.)							
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB) (NB/SB)	NO. (1, 2, 3)	PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
Ridgeland		EB	1	12	200	2400	267
		EB	1	6	400	2400	267
		EB	1	12	20	240	27
		EB	1	12	20	240	27
		EB	1	6	100	600	67
		EB	1	12	12	144	16
		EB	1	12	12	144	16
		EB	1	6	200	1200	133
		EB	1	12	12	144	16
		EB	1	12	15	180	20
		EB	1	6	300	1800	200
		EB	1	12	12	144	16
		EB	1	12	12	144	16
		EB	1	12	12	144	16
		EB	1	6	300	1800	200
		EB	1	6	50	300	33
		EB	1	6	100	600	67
		EB	1	12	20	240	27
		EB	1	12	20	240	27
		EB	1	12	30	360	40
		EB	1	6	50	300	33
		EB	1	6	100	600	67
		EB	1	6	200	1200	133
		EB	1	12	20	240	27
		EB	1	12	40	480	53
		EB	1	6	200	1200	133
		EB	1	12	12	144	16
	Central	EB	1	6	300	1800	200
Centrall		WB	1	6	100	600	67
		WB	1	6	50	300	33
		WB	1	12	20	240	27
		WB	1	12	12	144	16
		WB	1	12	12	144	16
		WB	1	12	12	144	16
		WB	1	6	100	600	67
		WB	1	12	100	1200	133
		WB	1	12	100	1200	133
		WB	1	6	12	72	8
		WB	1	12	12	144	16
		WB	1	12	100	1200	133
		WB	1	12	100	1200	133
		WB	1	12	20	240	27
		WB	1	12	75	900	100
		WB	1	12	100	1200	133
		WB	1	12	100	1200	133
		WB	1	12	25	300	33
		WB	1	6	400	2400	267
		WB	1	12	12	144	16
	Ridgeland	WB	1	12	50	600	67
TOTALS:					4305		3765
					FT		SY

ROUTE: 127th St. (Central Ave. to Ridgeland Ave.)							
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB) (NB/SB)	NO. (1, 2, 3)	PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
Central Ave.	Ridgeland Ave.	WB	1	12	6	72	8
		WB	1	6	120	720	80
		EB	1	12	10	120	14
		EB	1	6	10	60	7
		EB	1	6	20	120	14
		EB	1	6	30	180	20
TOTALS:					196		143
					FT		SY

ROUTE: 159th St. (Cicero Ave. to I-294)							
CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	12	144	16
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	10	120	13
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Pulaski - Crawford	I - 57	EB	1	12	6	72	8
Pulaski - Crawford	I - 57	EB	1	12	6	72	8
Pulaski - Crawford	I - 57	EB	1	12	6	72	8
Pulaski - Crawford	I - 57	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	10	120	13
I - 57	Kedzie	EB	1	12	6	72	8
Kedzie	I - 294	EB	1	12	6	72	8
Kedzie	I - 294	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	10	120	13
Cicero	Pulaski - Crawford	EB	2	12	8	96	11
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	10	120	13
Pulaski - Crawford	I - 57	EB	2	12	6	72	8
Pulaski - Crawford	I - 57	EB	2	12	6	72	8
Pulaski - Crawford	I - 57	EB	2	12	12	144	16
I - 57	Kedzie	EB	2	12	6	72	8
I - 57	Kedzie	EB	2	12	6	72	8
I - 57	Kedzie	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	6	20	120	13
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	6	20	120	13
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	Richmond	EB	3	6	20	120	13
Kedzie	Richmond	EB	3	12	6	72	8
Kedzie	Richmond	EB	3	12	6	72	8
Kedzie	Richmond	EB	3	6	20	120	13

ROUTE: 159th St. (Cicero Ave. to I-294)							
CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
I - 294	Kedzie	WB	1	12	6	72	8
I - 294	Kedzie	WB	1	12	6	72	8
I - 294	Kedzie	WB	1	6	25	150	17
I - 294	Kedzie	WB	1	6	25	150	17
I - 294	Kedzie	WB	1	6	15	90	10
I - 294	Kedzie	WB	1	6	40	240	27
I - 294	Kedzie	WB	1	12	6	72	8
I - 294	Kedzie	WB	1	6	20	120	13
Kedzie	I - 57	WB	1	12	6	72	8
Kedzie	I - 57	WB	1	12	6	72	8
Kedzie	I - 57	WB	1	12	8	96	11
Kedzie	I - 57	WB	1	12	6	72	8
Kedzie	I - 57	WB	1	12	12	144	16
Kedzie	I - 57	WB	1	12	6	72	8
I - 57	Pulaski - Crawford	WB	1	12	6	72	8
I - 57	Pulaski - Crawford	WB	1	12	6	72	8
Pulaski - Crawford	Cicero	WB	1	12	6	72	8
Pulaski - Crawford	Cicero	WB	1	12	8	96	11
Pulaski - Crawford	Cicero	WB	1	12	6	72	8
Pulaski - Crawford	Cicero	WB	1	12	6	72	8
Pulaski - Crawford	Cicero	WB	1	12	6	72	8
I - 294	Kedzie	WB	2	12	6	72	8
I - 294	Kedzie	WB	2	12	6	72	8
I - 294	Kedzie	WB	2	6	20	120	13
I - 294	Kedzie	WB	2	6	20	120	13
Kedzie	I - 57	WB	2	12	6	72	8
Kedzie	I - 57	WB	2	12	10	120	13
Kedzie	I - 57	WB	2	12	6	72	8
Kedzie	I - 57	WB	2	12	6	72	8
Kedzie	I - 57	WB	2	12	6	72	8
Kedzie	I - 57	WB	2	12	6	72	8
Kedzie	I - 57	WB	2	12	6	72	8
Kedzie	I - 57	WB	2	12	8	96	11
Kedzie	I - 57	WB	2	12	6	72	8
Kedzie	I - 57	WB	2	6	30	180	20
Kedzie	I - 57	WB	2	6	50	300	33
Kedzie	I - 57	WB	2	6	20	120	13
I - 57	Pulaski - Crawford	WB	2	12	6	72	8
I - 57	Pulaski - Crawford	WB	2	12	6	72	8
I - 57	Pulaski - Crawford	WB	2	12	12	144	16
I - 57	Pulaski - Crawford	WB	2	6	40	240	27
I - 57	Pulaski - Crawford	WB	2	12	6	72	8
I - 57	Pulaski - Crawford	WB	2	12	6	72	8
I - 57	Pulaski - Crawford	WB	2	6	30	180	20
I - 57	Pulaski - Crawford	WB	2	6	30	180	20
Pulaski - Crawford	Cicero	WB	2	6	50	300	33
Pulaski - Crawford	Cicero	WB	2	6	50	300	33
Pulaski - Crawford	Cicero	WB	2	6	30	180	20
Pulaski - Crawford	Cicero	WB	2	12	8	96	11
Pulaski - Crawford	Cicero	WB	2	6	30	180	20

TOTALS: **1169** **1155**
FT **SY**

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ROUTE: Cicero Ave. (147th St. to 159th St.)							
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
159th	147th	NB	2	4	50	200	22
		NB	2	12	6	72	8
		NB	2	12	10	12	13
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	4	48	5
		NB	2	12	10	120	13
		NB	2	12	4	48	5
		NB	2	4	100	400	44
147th	159th	SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	40	480	53
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	8	96	11
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	4	48	5
		SB	2	4	50	200	22
159th	147th	SB	2	4	100	400	44
		SB	2	12	6	72	8
		NB	1	12	8	96	11
147th	159th	NB	1	12	6	72	8
		NB	1	12	6	72	8
147th	159th	SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	4	48	5
		SB	1	12	6	72	8
		SB	1	12	8	96	11
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	4	48	5
		SB	1	12	8	96	11
		SB	1	12	24	288	32
		SB	1	12	8	96	11
		SB	1	12	8	96	11
		SB	1	12	4	48	5
		SB	1	12	6	72	8
		TOTALS:					570
					FT	SY	

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE CICERO AVE.			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr\pwwork\pwwid\WILGREENDP\d0183625\design.dgn	DRAWN -	REVISED -	VAR.					2010-015 RS	COOK	26	9	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLOT DATE = 2/17/2010	DATE -	REVISED -						CONTRACT NO. 60K34				

ROUTE: Crawford Ave. (159th St. to 175th St.) - HMA Section							
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
167TH	I-57	NB	2	12	6	72	8
167TH	I-57	NB	2	12	20	240	27
167TH	I-57	NB	2	12	10	120	13
I-57	159TH	SB	2	12	200	2400	267
I-57	159TH	SB	2	12	6	72	8
TOTALS:					242		323
					FT		SY

ROUTE: Halsted St. (Joe Orr Rd. to IL 1 Cutoff)							
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	2	12	8	96	11
Joe Orr	ILL 1 Cutoff	SB	2	15	8	120	13
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	2	12	12	144	16
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
ILL 1 Cutoff	Joe Orr	NB	2	12	12	144	16
ILL 1 Cutoff	Joe Orr	NB	2	12	10	120	13
ILL 1 Cutoff	Joe Orr	NB	2	12	25	300	33
ILL 1 Cutoff	Joe Orr	NB	2	12	6	72	8
ILL 1 Cutoff	Joe Orr	NB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	1	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	1	12	10	120	13
Joe Orr	ILL 1 Cutoff	SB	1	12	12	144	16
Joe Orr	ILL 1 Cutoff	SB	1	12	6	72	8
ILL 1 Cutoff	Joe Orr	NB	1	12	6	72	8
ILL 1 Cutoff	Joe Orr	NB	1	12	10	120	13
TOTALS:					167		225
					FT		SY

ROUTE: IL 83 (127th St. / Cal Sag to Archer Ave.)

CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
127th/Cal-Sag	Archer Ave.	WB	1	4	500	2000	223
		WB	1,2,3	36	6	216	24
		WB	1,2	24	6	144	16
		WB	1,2	24	6	144	16
		WB	1	6	300	1800	200
		WB	1	6	250	1500	167
		WB	1	4	100	400	45
		WB	1	4	200	800	89
		WB	1	6	150	900	100
		WB	1	12	6	72	8
		WB	1	4	150	600	67
		WB	1	4	200	800	89
		WB	1	12	5	60	7
		WB	1	12	6	72	8
		WB	1	12	12	144	16
		WB	1	12	6	72	8
		WB	1	50	4	200	22
		WB	1	12	8	96	11
		WB	1	12	50	600	67
		WB	1	12	10	120	13
		WB	1	12	25	300	33
		WB	1	12	10	120	13
		WB	1	12	10	120	13
		WB	1	12	10	120	13
		WB	1	12	6	72	8
		WB	1	12	300	3600	400
		WB	1	6	50	300	33
		WB	1	12	50	600	67
		WB	1	6	200	1200	133
		WB	1	12	25	300	33
		WB	1	12	200	2400	267
		WB	1	6	200	1200	133
		WB	1	6	200	1200	133
		WB	1	12	20	240	27
		WB	1	6	30	180	20
Archer	127th/Cal-Sag	EB	1	12	4	48	5
		EB	1	12	20	240	27
		EB	1	4	150	600	67
		EB	1	4	125	500	56
		EB	1	12	12	144	16
		EB	1	4	30	120	13
		EB	1	4	70	280	31
		EB	1	4	100	400	44
		EB	1	4	50	200	22
		EB	1	4	50	200	22
		EB	1	12	8	96	11
		EB	1	12	8	96	11
		EB	1	12	100	1200	133
		EB	1	12	60	720	80
		EB	1	4	50	200	22
		EB	1	4	100	400	44
		EB	1	4	200	800	89
		EB	1	4	100	400	44
		EB	1	6	500	3000	333

TOTALS: 5048 FT 3593 SY

ROUTE: Park Ave. (154th St. to 167th St.) - HMA Section

CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
154th	155th	NB	2	6	12	72	8
155th	156th	NB	2	6	12	72	8
155th	156th	NB	2	6	12	72	8
156th	157th	NB	2	6	12	72	8
156th	157th	NB	2	6	12	72	8
156th	157th	NB	2	12	12	144	16
156th	157th	NB	2	6	12	72	8
156th	157th	NB	2	6	12	72	8
154th	155th	SB	2	12	70	840	93
155th	156th	SB	2	6	12	72	8
155th	156th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
157th	159th	SB	2	12	20	240	27
157th	159th	SB	2	6	12	72	8
157th	159th	SB	2	6	12	72	8
157th	159th	SB	2	12	15	180	20
157th	156th	NB	1	6	12	72	8
157th	156th	NB	1	6	12	72	8
157th	156th	NB	1	12	12	144	16
157th	156th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
155th	154th	NB	1	12	12	144	16
155th	154th	NB	1	6	12	72	8
155th	154th	NB	1	12	50	600	67
154th	155th	SB	1	12	100	1200	133
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8

TOTALS: 831 FT 748 SY

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p>	<p>PATCHING SCHEDULE IL 83 AND PARK AVE.</p>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	VAR.	2010-015 RS	COOK	26 11
		PLOT SCALE = 1/8" = 100' / IN.	REVISD -										
		PLOT DATE = 2/18/2010	REVISD -										
												<p>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 60K34</p>	

ROUTE: Loomis St. (Thornton Rd. to 150th St.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT		REPAIR	
FROM	TO			PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
Thornton Rd	147th	SB	1	10	4	40	4
Thornton Rd	147th	SB	1	10	4	40	4
Thornton Rd	147th	SB	1	10	4	40	4
Thornton Rd	147th	SB	1	10	4	40	4
Thornton Rd	147th	SB	1	10	4	40	4
Thornton Rd	147th	NB	1	10	4	40	4
Thornton Rd	147th	NB	1	10	4	40	4
Thornton Rd	147th	NB	1	10	4	40	4
Thornton Rd	147th	NB	1	10	4	40	4
Thornton Rd	147th	NB	1	10	4	40	4
Thornton Rd	147th	NB	1	10	10	100	11
Thornton Rd	147th	NB	1	10	15	150	17
147th	150th	SB	1	10	4	40	4
147th	150th	SB	1	10	4	40	4
147th	150th	SB	1	10	4	40	4
147th	150th	SB	1	10	4	40	4
147th	150th	SB	1	10	15	150	17
147th	150th	NB	1	10	4	40	4
147th	150th	NB	1	10	4	40	4
147th	150th	NB	1	10	4	40	4
147th	150th	NB	1	10	15	150	17
147th	150th	NB	1	10	4	40	4
147th	150th	NB	1	10	4	40	4
TOTALS:					131		145
					FT		SY

ROUTE: Wood St. (127th St. to Thornton Rd.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT		REPAIR	
FROM	TO			PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
127th	Vermont	SB	1	10	4	40	4
127th	Vermont	SB	1	10	4	40	4
127th	Vermont	SB	1	10	6	60	7
127th	Vermont	SB	1	10	8	80	9
127th	Vermont	SB	2	10	4	40	4
Vermont	Broadway	NB	2	12	8	96	11
Broadway	Thornton Rd	SB	2	10	8	80	9
Broadway	Thornton Rd	SB	1	10	4	40	4
Broadway	Thornton Rd	SB	1	10	4	40	4
Broadway	Thornton Rd	NB	1	10	4	40	4
Broadway	Thornton Rd	NB	1	10	4	40	4
Broadway	Thornton Rd	NB	1	10	4	40	4
Broadway	Thornton Rd	SB	2	12	8	96	11
Broadway	Thornton Rd	SB	2	12	8	96	11
TOTALS:					78		92
					FT		SY

ROUTE: Main St. in Lemont (IL 83 at IL 171 to Parker Rd.)

CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB) (NB/SB)	NO. (1, 2, 3)	PATCH WDMTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
Rt 83 @ ILL171		WB	1	12	10	120	13
		WB	1	12	6	72	8
		WB	1	12	25	300	33
		WB	1	12	15	180	20
		WB	1	12	30	360	40
		WB	1	12	55	660	73
		WB	2	12	10	120	13
		WB	2	12	6	72	8
		WB	2	12	25	300	33
		WB	2	12	15	180	20
		WB	2	12	30	360	40
	End of Dbl Lane, Conc Median	WB	2	12	55	660	73
Conc Median		WB	1	12	10	120	13
		WB	1	12	25	300	33
		WB	1	12	10	120	13
		WB	1	12	45	540	60
		WB	1	12	55	660	73
	Maley drive	WB	1	2	250	500	56
Maley Drive		WB	1	12	30	360	40
		WB	1	12	60	720	80
	K-Five Drive	WB	1	2	150	300	33
K-Five drive		WB	1	2	75	150	17
		WB	1	12	100	1200	133
		WB	1	12	10	120	13
		WB	1	12	10	120	13
		WB	1	12	10	120	13
	St Mary's Drive	WB	1	12	30	360	40
St. Mary's Drive		WB	1	2	75	150	17
		WB	1	12	100	1200	133
		WB	1	12	10	120	13
		WB	1	12	10	120	13
		WB	1	12	40	480	53
	Walker Road	WB	1	2	180	360	40
Walker Road		EB	1	2	50	100	11
		EB	1	2	200	400	44
		EB	1	2	150	300	33
		EB	1	12	20	240	27
		EB	1	12	50	600	67
		EB	1	12	40	480	53
		EB	1	12	30	360	40
		EB	1	12	50	600	67
	Mt. Assisi Drive	EB	1	12	100	1200	133
Mt. assisi Drive		EB	1	2	50	100	11
		EB	1	2	200	400	44
		EB	1	2	150	300	33
		EB	1	12	40	480	53
		EB	1	12	30	360	40
		EB	1	12	100	1200	133
		EB	1	12	20	240	27
		EB	1	12	300	3600	400
		EB	1	12	60	720	80
		EB	1	12	75	900	100
	Maley Drive	EB	1	2	250	500	56
Maley Drive		EB	1	2	60	120	13
		EB	1	2	150	300	33
		EB	1	12	40	480	53
		EB	1	12	30	360	40
	Conc Median	EB	1	12	10	120	13
Conc Median		EB	1	12	10	120	13
		EB	1	12	45	540	60
	Rt 83 @ IL 171	EB	1	12	75	900	100

TOTALS: 3962 FT 3072 SY

FILE NAME *	USER NAME = wilgreendp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE MAIN ST.				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pwork\FWIDOT\WILGREENDP\0183625\Design.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	COOK	26	14
		CHECKED -	REVISED -		CONTRACT NO. 60K34								
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								
PLOT SCALE = 100.0000 ' / IN.		PLOT DATE = 2/17/2010											

ROUTE: Sibley Blvd. (I-94 to Torrence Ave.)							
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB) (NB/SB)	NO. (1, 2, 3)	PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
I 94	west of Torrence	EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	12	144	16
		EB	1	12	8	96	11
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	8	96	11
west of Torrence	I 94	WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	12	144	16
I 94	west of Torrence	EB	2	12	6	72	8
		EB	2	12	8	96	11
		EB	2	12	8	96	11
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	20	240	27
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	10	120	13
		EB	2	12	10	120	13
		EB	2	12	6	72	8
west of Torrence	I 94	WB	2	12	6	72	8
		WB	2	12	20	240	27
		WB	2	12	6	72	8
		WB	2	12	10	120	13
		WB	2	12	6	72	8
		WB	2	12	20	240	27
TOTALS:					314		419
					FT		SY

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ROUTE: Torrence Ave. (Ridge Rd. to I-80/94)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT		REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO			PATCH WIDTH	PATCH LENGTH		
178TH STREET	THORTON LANSING RD	SB	1	2	36	72	8
		SB	1	2	30	60	7
		SB	1	2	400	800	89
		SB	1	5	12	60	7
		SB	1	1	12	12	1
		SB	1	4	12	48	5
		SB	1	2	200	400	44
		SB	1	2	12	24	3
		SB	1	2	12	24	3
		THORTON LANSING RD	RIDGE RD	SB	1	2	12
SB	1			2	12	24	3
SB	1			2	12	24	3
SB	1			2	50	100	11
SB	1			2	100	200	22
SB	1			4	12	48	5
SB	1			2	24	48	5
SB	1			2	24	48	5
SB	1			2	24	48	5
178TH STREET 178th / TORRENCE INTERSECTION	THORTON LANSING RD			SB	2	4	30
		SB	2	4	14	56	6
		SB	2	2	400	800	89
		SB	2	12	12	144	16
		SB	2	4	50	200	22
		SB	2	2	12	24	3
		SB	2	2	20	40	4
		SB	2	2	15	30	3
		SB	2	4	12	48	5
		THORTON LANSING RD	RIDGE RD	SB	2	2	12
SB	2			2	12	24	3
SB	2			1	8	8	1
SB	2			2	12	24	3
SB	2			4	4	16	2
SB	2			4	4	16	2
SB	2			4	4	16	2
SB	2			2	2	4	0
SB	2			4	12	48	5
SB	2			4	4	16	2
BURGER KING RADIUS		SB	2	8	15	120	13
		SB	2	2	8	16	2
		SB	2	2	4	8	1
RIDGE RD	THORTON LANSING RD	NB	1	2	200	400	44
		NB	1	4	12	48	5
		NB	1	2	50	100	11
		NB	1	4	100	400	44
		NB	1	2	60	120	13
		NB	1	2	20	40	4
		NB	1	2	80	160	18

ROUTE: Torrence Ave. (Ridge Rd. to I-80/94)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT		REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO			PATCH WIDTH	PATCH LENGTH		
RIDGE RD	THORTON LANSING RD	NB	1	2	12	24	3
		NB	1	2	12	24	3
		NB	1	4	12	48	5
		NB	1	12	12	144	16
		NB	1	2	60	120	13
		NB	1	2	12	24	3
		NB	1	2	12	24	3
		NB	1	2	12	24	3
		NB	1	2	12	24	3
		NB	1	2	12	24	3
		NB	1	2	12	24	3
		NB	1	2	50	100	11
RIDGE RD	THORTON LANSING RD	NB	2	12	60	720	80
		NB	2	4	60	240	27
		NB	2	12	40	480	53
		NB	2	2	100	200	22
		NB	2	4	20	80	9
		NB	2	4	12	48	5
		NB	2	2	12	24	3
		NB	2	3	30	90	10
		NB	2	2	6	12	1
		NB	2	4	50	200	22
		NB	2	2	20	40	4
		NB	2	2	10	20	2
THORTON LANSING RD	178TH STREET	NB	2	4	12	48	5
		NB	2	6	12	72	8
		NB	2	4	10	40	4
		NB	2	2	8	16	2
		NB	2	2	8	32	4
		NB	2	2	6	12	1
		NB	2	4	12	48	5
		NB	2	2	12	24	3
		NB	2	2	900	1800	200
		NB	2	4	12	48	5
		NB	2	2	12	24	3
		NB	2	4	50	200	22
NB	2	4	50	200	22		
NB	2	4	30	120	13		
NB	2	2	10	20	2		
NB	2	4	50	200	22		

TOTALS: 4056 FT 1207 SY

FILE NAME: c:\pwork\PWIDOT\WILGREENDP\0183625\Design.dgn		USER NAME: wilgreendp	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE TORRENCE AVE.				F.A. RTE.:	SECTION:	COUNTY:	TOTAL SHEETS:	SHEET NO.:	
PLOT SCALE: 100.0000' / IN.		CHECKED: -	REVISED: -	SCALE:		SHEET NO.:	OF:	SHEETS:	STA.:	TO STA.:	VAR.:	2010-015 RS	COOK	26	16
PLOT DATE: 2/17/2010		DATE: -	REVISED: -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					CONTRACT NO. 60K34						

ROUTE: US 30 (I-394 to Halsted St.)							
CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB) (NB/SB)	NO. (1, 2, 3)	PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
I-394	Woodlawn	EB	2	12	8	96	11
Woodlawn	Ellis	EB	2	12	6	72	8
Ellis	Park	EB	2	12	6	72	8
Park	Ford Entrance	EB	2	12	6	72	8
		EB	2	12	6	72	8
Ford Entrance	Indian Ct	EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	10	6	60	7
		EB	2	8	6	48	5
		EB	2	12	6	72	8
Indian Ct	State	EB	2	12	150	1800	200
State	Wentworth	EB	2	12	75	900	100
		EB	2	12	6	72	8
		EB	2	12	6	72	8
Wentworth	East End Ave	EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
East End Ave	Halsted	EB	-	-	-	-	-
Halsted	East End Ave	WB	2	12	6	72	8
East End Ave	Wentworth	WB	2	12	8	96	11
		WB	2	12	6	72	8
		WB	2	12	6	72	8
		WB	2	12	6	72	8
		WB	2	12	8	96	11
Wentworth	State	WB	2	12	8	96	11
		WB	2	12	6	72	8
State	Indian Ct	WB	2	6	100	600	67
		WB	2	6	75	450	50
Indian Ct	Ford Entrance	WB	2	12	6	72	8
Ford Entrance	Park	WB	-	-	-	-	-
Park	Ellis	WB	-	-	-	-	-
Ellis	Woodlawn	WB	-	-	-	-	-
Woodlawn	I-394	WB	2	6	100	600	67
TOTALS:						652	682
						FT	SY

ROUTE: NB IL 394 (Steger Rd to Relocated Route 1)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Relocated Route 1	Steger Rd	NB	2	6	20	120	13
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	20	240	27
Relocated Route 1	Steger Rd	NB	2	12	10	120	13
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	8	96	11
Relocated Route 1	Steger Rd	NB	2	4	75	300	33
Relocated Route 1	Steger Rd	NB	2	12	20	240	27
Relocated Route 1	Steger Rd	NB	2	4	15	60	7
Relocated Route 1	Steger Rd	NB	2	12	10	120	13
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	4	100	400	44
Relocated Route 1	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	2	12	10	120	13
Relocated Route 1	Steger Rd	NB	2	4	100	400	44
Relocated Route 1	Steger Rd	NB	2	4	100	400	44
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	4	500	2000	222
Relocated Route 1	Steger Rd	NB	2	12	200	2400	267
Relocated Route 1	Steger Rd	NB	2	12	10	120	13
Relocated Route 1	Steger Rd	NB	2	12	15	180	20
Relocated Route 1	Steger Rd	NB	2	12	10	120	13
Relocated Route 1	Steger Rd	NB	2	4	125	500	56
Relocated Route 1	Steger Rd	NB	2	4	120	480	53
Relocated Route 1	Steger Rd	NB	2	12	20	240	27
Relocated Route 1	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	2	12	8	96	11
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	15	180	20
Relocated Route 1	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	2	12	8	96	11
Relocated Route 1	Steger Rd	NB	2	4	100	400	44
Relocated Route 1	Steger Rd	NB	2	4	75	300	33
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	10	120	13
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	1	4	50	200	22
Relocated Route 1	Steger Rd	NB	1	12	6	72	8

ROUTE: NB IL 394 (Steger Rd to Relocated Route 1)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	4	50	200	22
Relocated Route 1	Steger Rd	NB	1	12	25	300	33
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	8	96	11
Relocated Route 1	Steger Rd	NB	1	12	8	96	11
Relocated Route 1	Steger Rd	NB	1	12	10	120	13
Relocated Route 1	Steger Rd	NB	1	12	10	120	13
Relocated Route 1	Steger Rd	NB	1	4	75	300	33
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	4	25	100	11
Relocated Route 1	Steger Rd	NB	1	4	250	1000	111
Relocated Route 1	Steger Rd	NB	1	12	8	96	11
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	15	180	20
Relocated Route 1	Steger Rd	NB	1	12	8	96	11
Relocated Route 1	Steger Rd	NB	1	12	8	96	11
Relocated Route 1	Steger Rd	NB	1	4	50	200	22
Relocated Route 1	Steger Rd	NB	1	4	50	200	22
Relocated Route 1	Steger Rd	NB	1	4	150	600	67
Relocated Route 1	Steger Rd	NB	1	12	25	300	33
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	25	300	33
Relocated Route 1	Steger Rd	NB	1	4	75	300	33
Relocated Route 1	Steger Rd	NB	1	12	15	180	20
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	4	50	200	22
Relocated Route 1	Steger Rd	NB	1	4	100	400	44
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	4	100	400	44
Relocated Route 1	Steger Rd	NB	1	12	20	240	27
Relocated Route 1	Steger Rd	NB	1	12	15	180	20
Relocated Route 1	Steger Rd	NB	1	4	25	100	11
Relocated Route 1	Steger Rd	NB	1	12	15	180	20
Relocated Route 1	Steger Rd	NB	1	4	75	300	33
Relocated Route 1	Steger Rd	NB	1	4	100	400	44
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	12	6	72	8
Relocated Route 1	Steger Rd	NB	1	6	50	300	33
Relocated Route 1	Steger Rd	NB	1	12	30	360	40

TOTALS: 3788 FT 2484 SY

NOTE: THIS SECTION OF IL 394 IS LOCATED IN WILL COUNTY

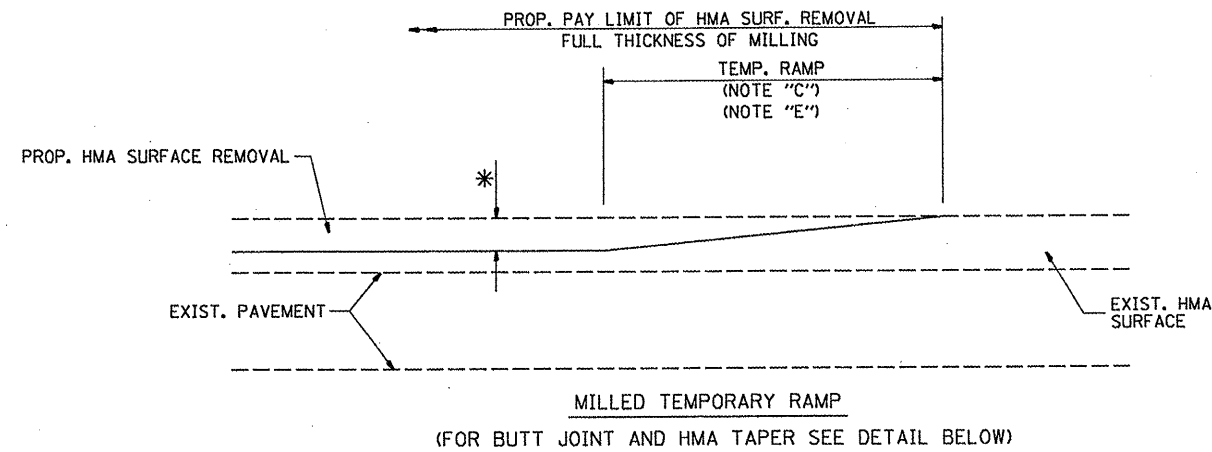
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		CHECKED -	REVISED -			CONTRACT NO. 60K34					
		DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

ROUTE: SB IL 394 (Steger Rd to Relocated Route 1)							
CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	4	100	400	44
Steger Rd	Relocated Route 1	SB	2	4	75	300	33
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	6	50	300	33
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	6	50	300	33
Steger Rd	Relocated Route 1	SB	2	4	25	100	11
Steger Rd	Relocated Route 1	SB	2	6	60	360	40
Steger Rd	Relocated Route 1	SB	2	6	125	750	83
Steger Rd	Relocated Route 1	SB	2	6	125	750	83
Steger Rd	Relocated Route 1	SB	2	4	250	1000	111
Steger Rd	Relocated Route 1	SB	2	4	300	1200	133
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	6	300	1800	200
Steger Rd	Relocated Route 1	SB	2	12	20	240	27
Steger Rd	Relocated Route 1	SB	2	12	50	600	67
Steger Rd	Relocated Route 1	SB	2	4	250	1000	111
Steger Rd	Relocated Route 1	SB	2	4	250	1000	111
Steger Rd	Relocated Route 1	SB	2	4	500	2000	222
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	8	96	11
Steger Rd	Relocated Route 1	SB	2	4	25	100	11
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	4	150	600	67
Steger Rd	Relocated Route 1	SB	2	12	35	420	47
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	10	120	13
Steger Rd	Relocated Route 1	SB	2	12	10	120	13
Steger Rd	Relocated Route 1	SB	2	4	25	100	11
Steger Rd	Relocated Route 1	SB	2	4	25	100	11
Steger Rd	Relocated Route 1	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Relocated Route 1	Steger Rd	SB	1	12	6	72	8

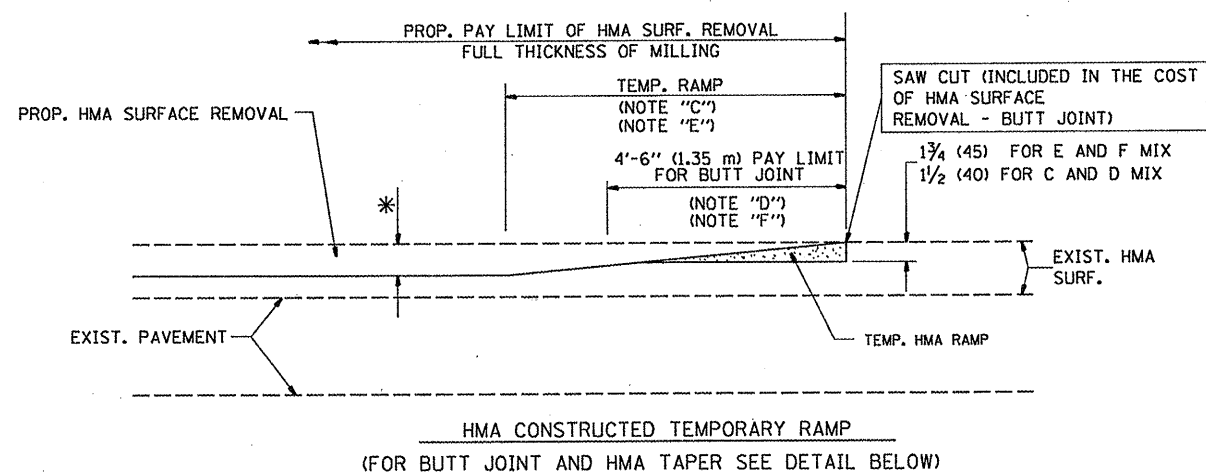
ROUTE: SB IL 394 (Steger Rd to Relocated Route 1)							
CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	12	10	120	13
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	6	50	300	33
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	15	60	7
Relocated Route 1	Steger Rd	SB	1	4	100	400	44
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Relocated Route 1	Steger Rd	SB	1	4	75	300	33
Relocated Route 1	Steger Rd	SB	1	4	100	400	44
Relocated Route 1	Steger Rd	SB	1	4	150	600	67
Relocated Route 1	Steger Rd	SB	1	4	75	300	33
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	250	1000	111
Relocated Route 1	Steger Rd	SB	1	4	250	1000	111
Relocated Route 1	Steger Rd	SB	1	4	500	2000	222
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	35	420	47
Relocated Route 1	Steger Rd	SB	1	4	200	800	89
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	12	16	192	21
Relocated Route 1	Steger Rd	SB	1	12	16	192	21
Relocated Route 1	Steger Rd	SB	1	6	50	300	33
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8

TOTALS: 5881 FT 3199 SY

NOTE: THIS SECTION OF IL 394 IS LOCATED IN WILL COUNTY

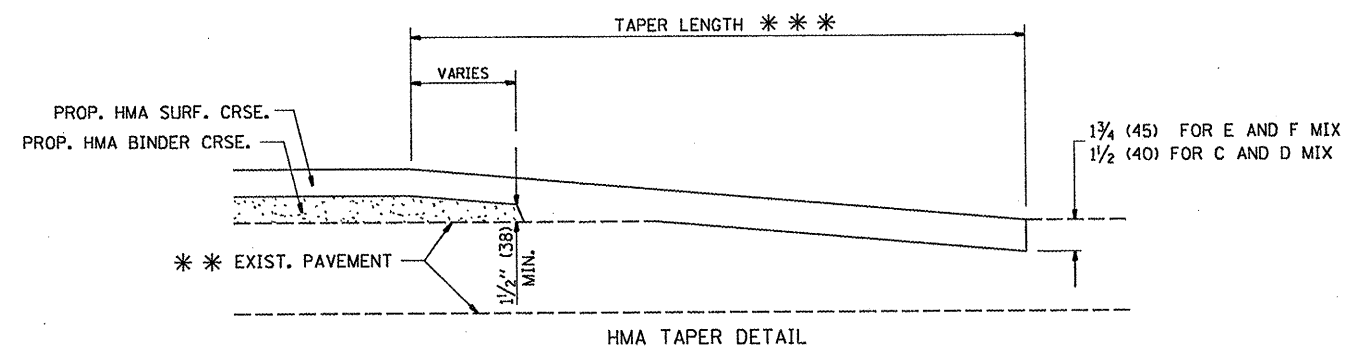
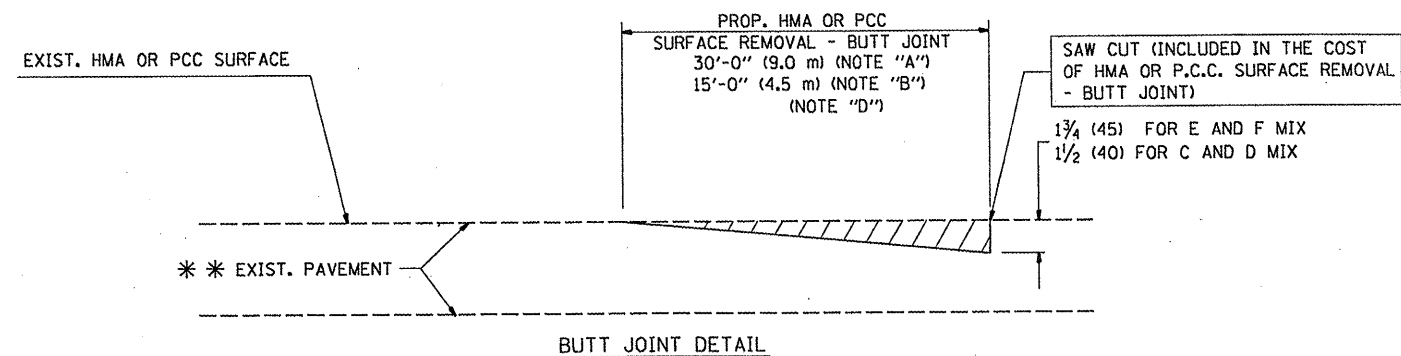


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

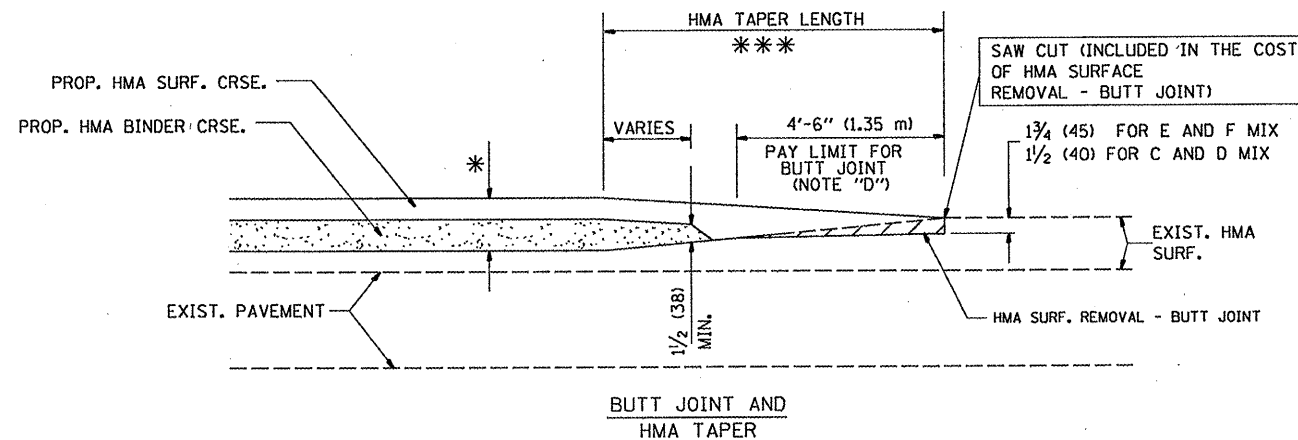
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

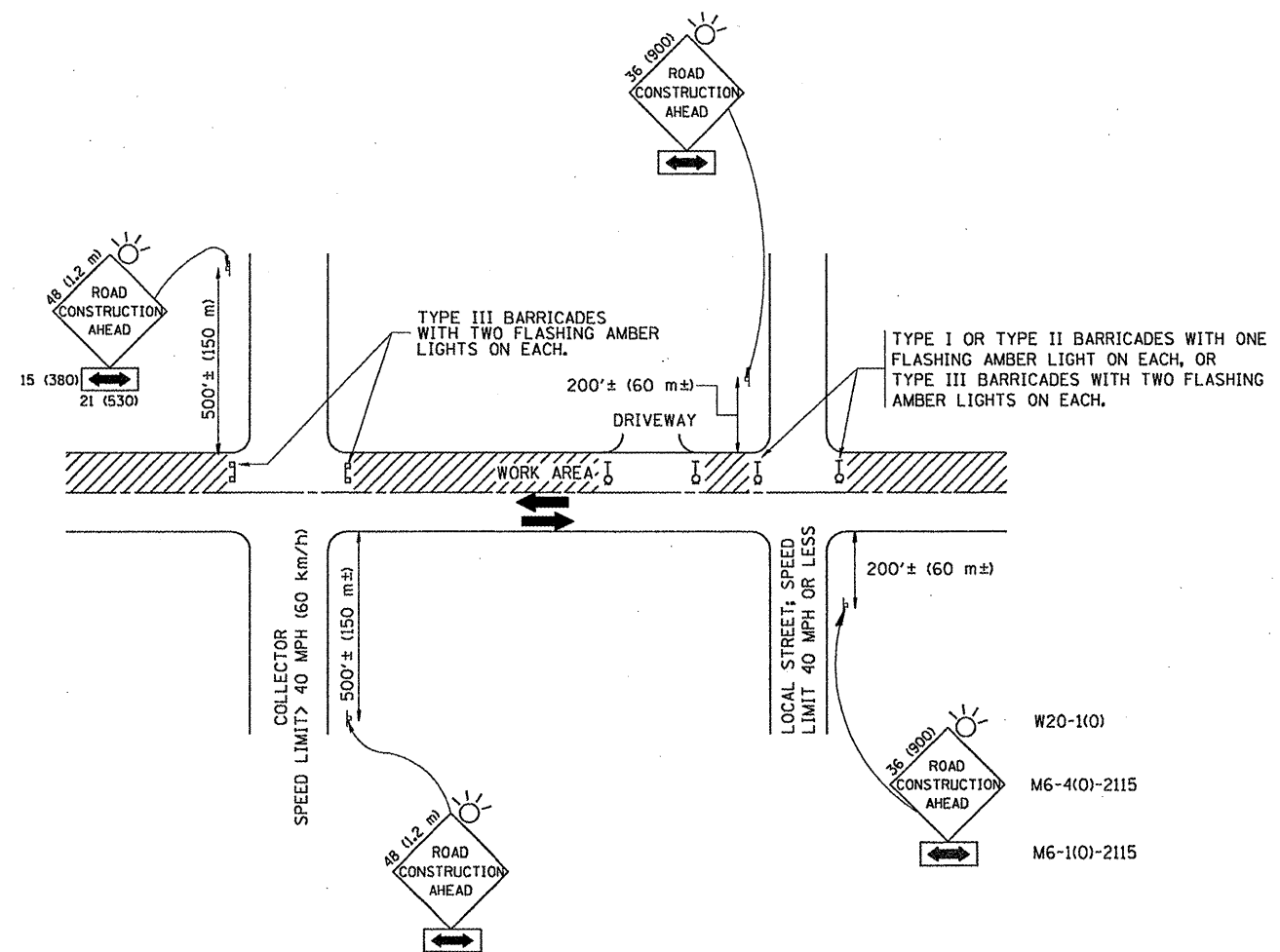
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2010-015 RS	COOK	26	20
BD400-05 BD32			CONTRACT NO. 60K34	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME =	USER NAME = whitetar	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
at:\pwwork\PIWIDOT\WHITETAR\0175544\DI	Std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000 1/1 IN.	CHECKED -	REVISOR - M. GOMEZ 04-06-01	
PLOT DATE = 2/10/2010	DATE - 06-13-90	REVISED - R. BORO 01-01-07	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

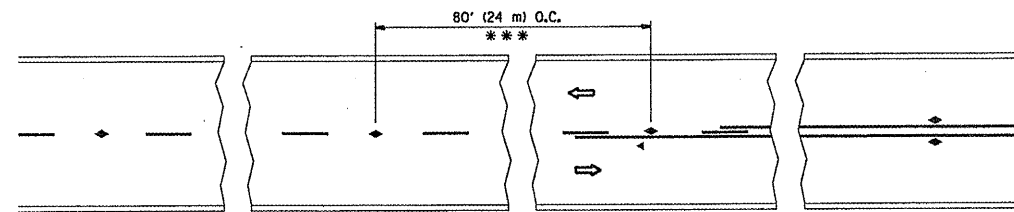
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	PLOT SCALE = 100.0000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 2/10/2010	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

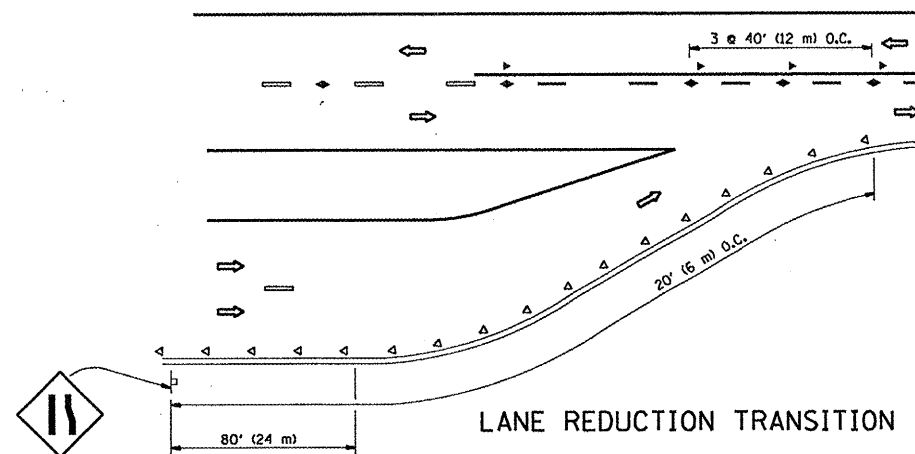
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2010-015 RS	COOK	26	21
TC-10		CONTRACT NO. 60K34		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

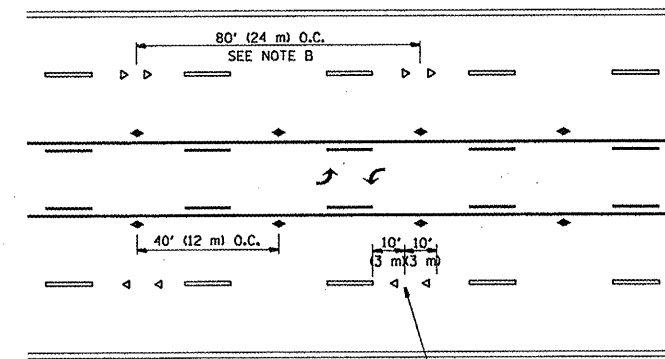


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

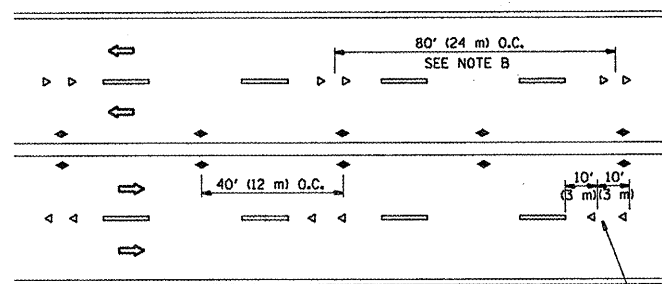
TWO-LANE/TWO-WAY



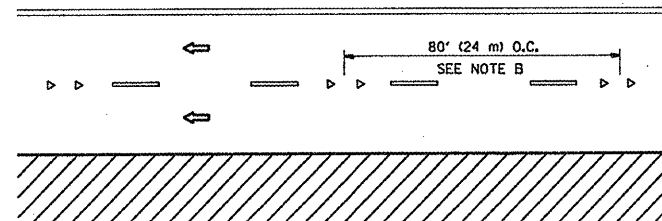
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

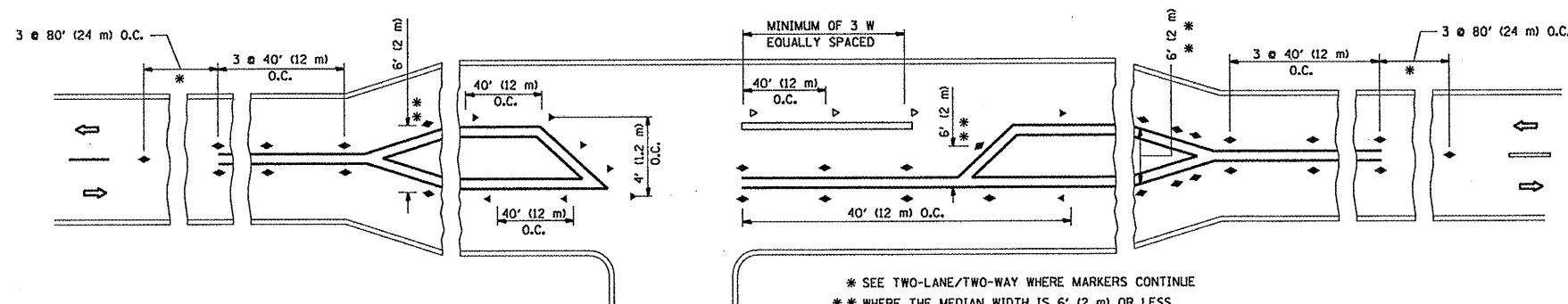
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

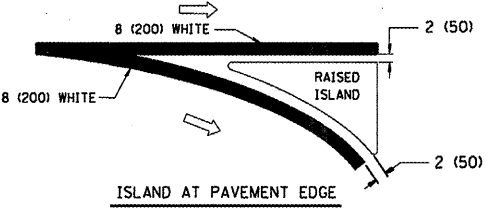
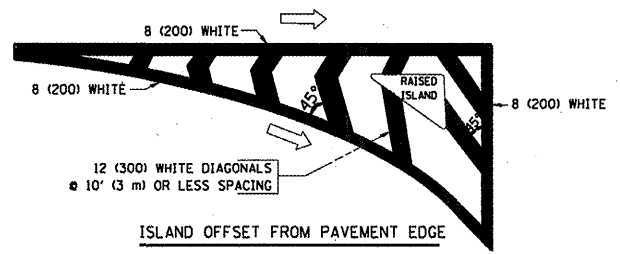
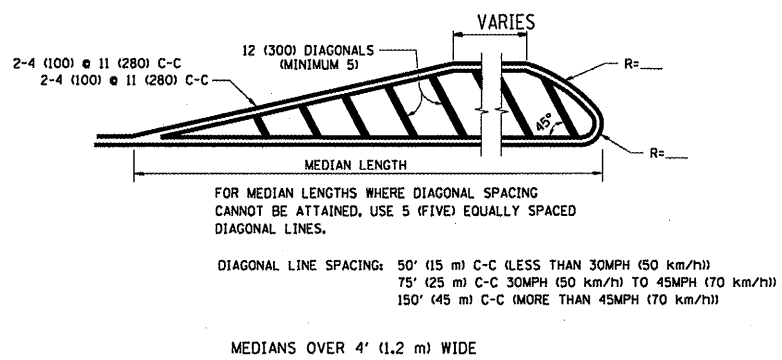
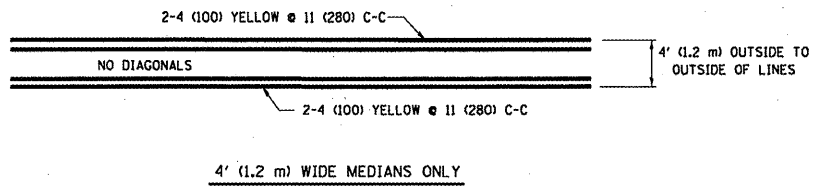
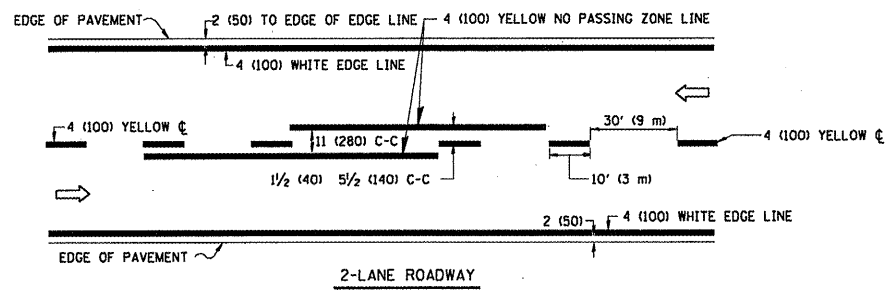
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = whitetor	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
ct:\pwork\VP\WIDOT\WHITETAR\d0175544\01s	Std.dgn	DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 2/10/2010	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2010-015 RS	COOK	26	22
TC-11			CONTRACT NO. 60K34	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

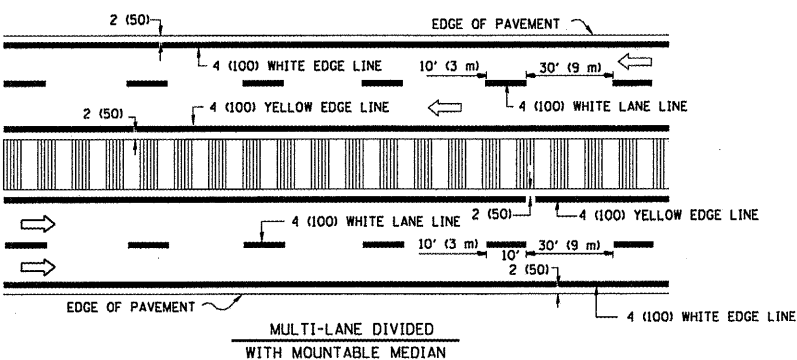
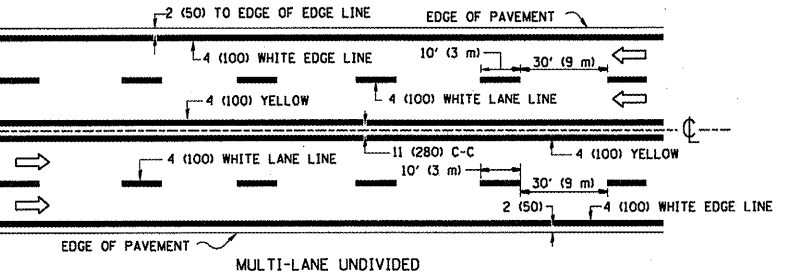


TYPICAL ISLAND MARKING

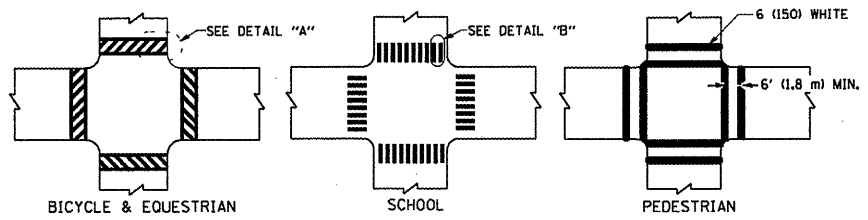
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

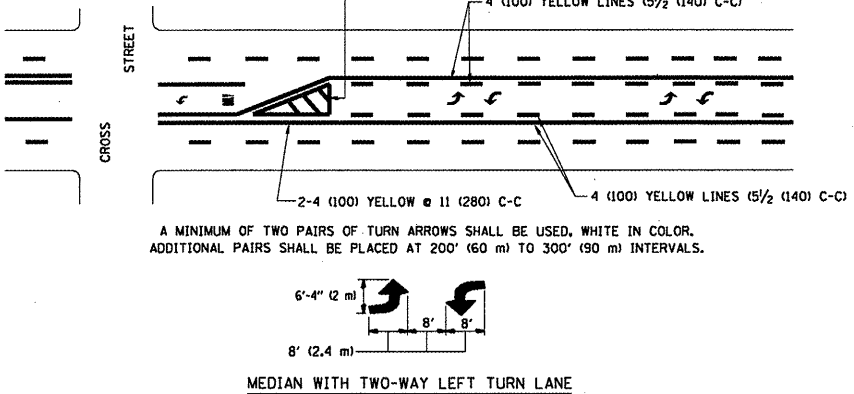
All dimensions are in inches (millimeters) unless otherwise shown.



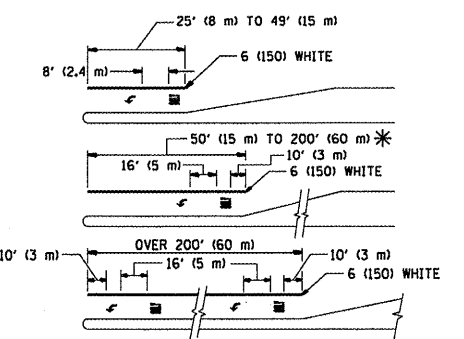
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
* AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

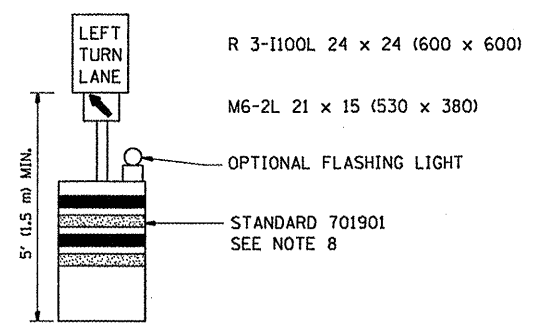
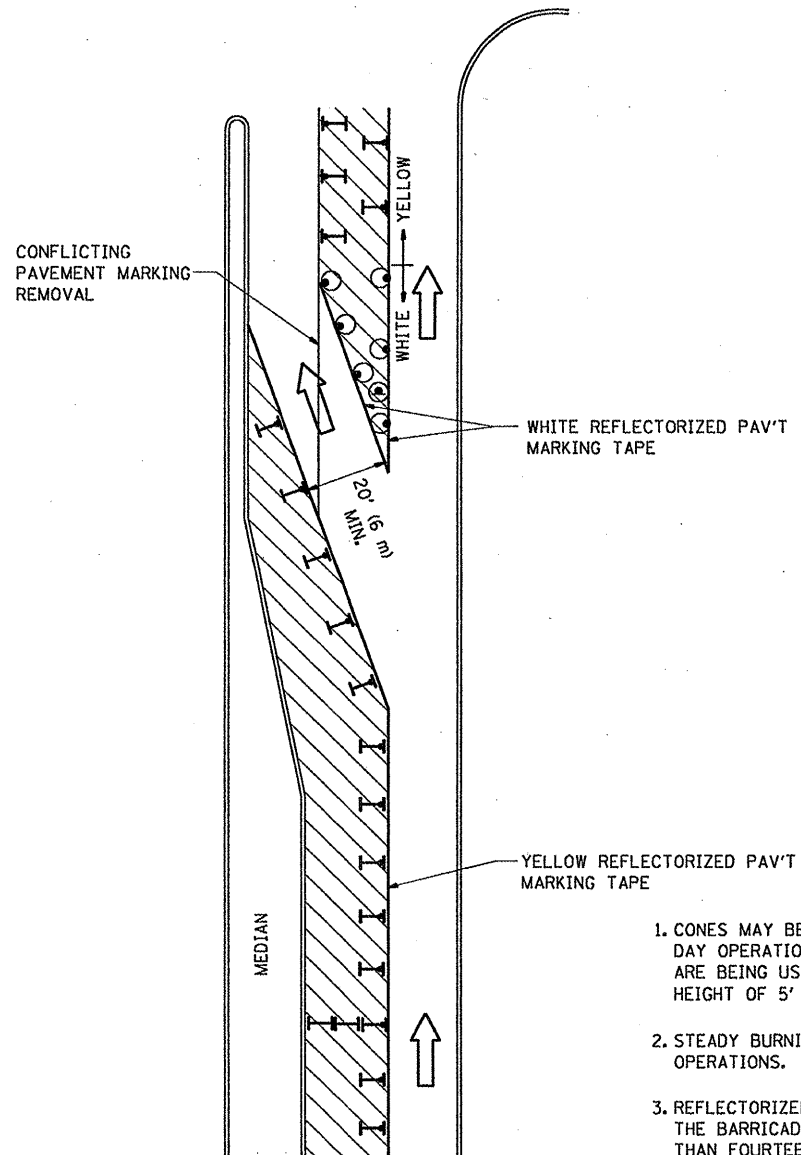
TYPICAL LEFT (OR RIGHT) TURN LANE MARKING

TYPICAL TURN LANE MARKING

FILE NAME =	USER NAME = wjgreendp	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
ct:\p\work\p\WIDOT\WILGREENOP\0183959\st5std.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
		CHECKED -	REVISED -
		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		VAR.	2010-015 RS	COOK	26	23
SCALE: NONE		TC-13		CONTRACT NO. 60K34		
SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

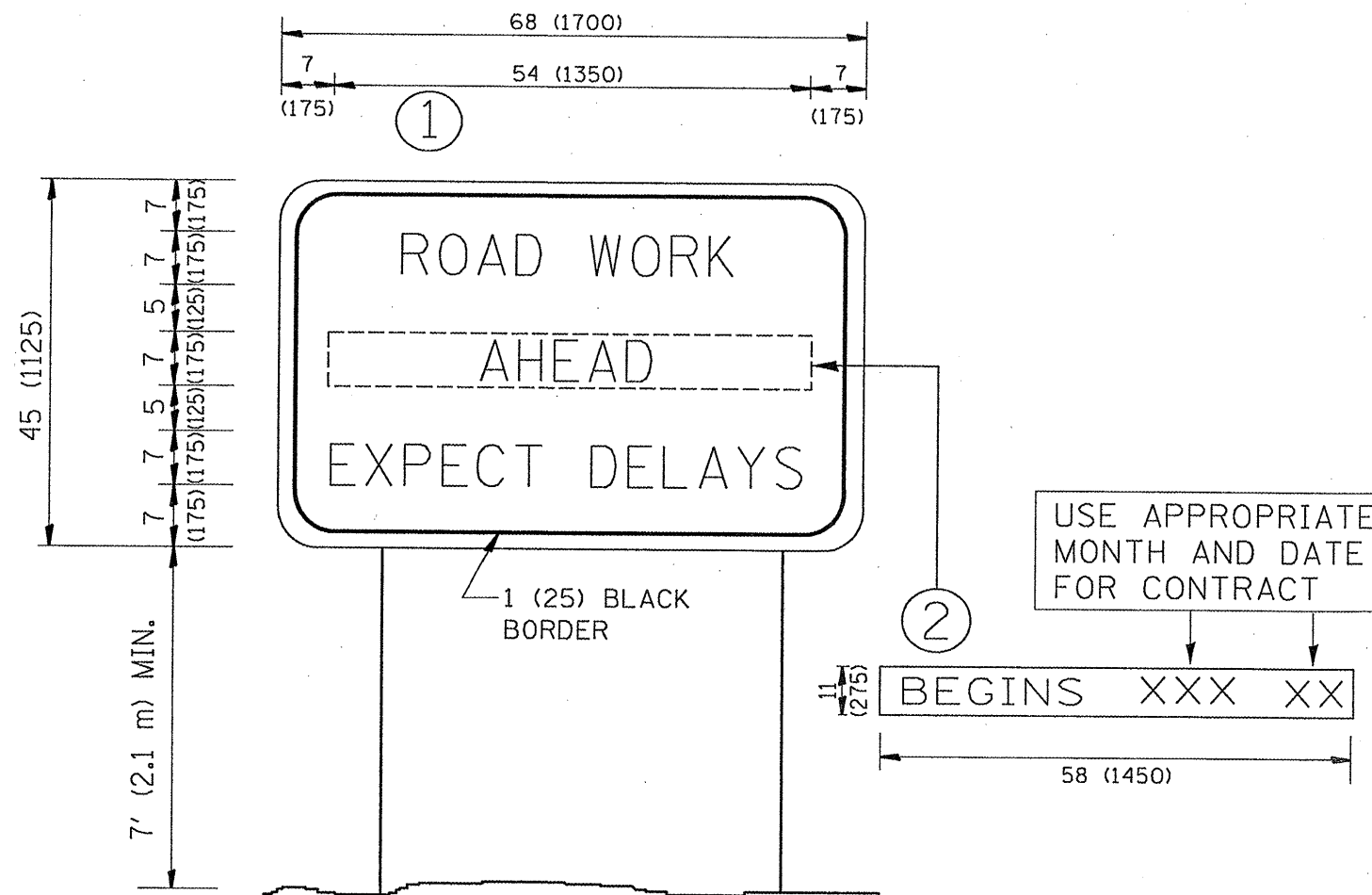
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = whitetr	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
ct:\pwr\work\PWIDOT\WHITETAR\d0175544\Dis	Std.dgn	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000 ' / IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 2/10/2010	REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2010-015 RS	COOK	26	24
TC-14			CONTRACT NO. 60K34	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = whitetar	DESIGNED -	REVISED - R. MIRS 09-15-97
ar\pm_work\PM\DOT\WHITETAR\0175544\019	Std.dgn	DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 2/10/2010	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

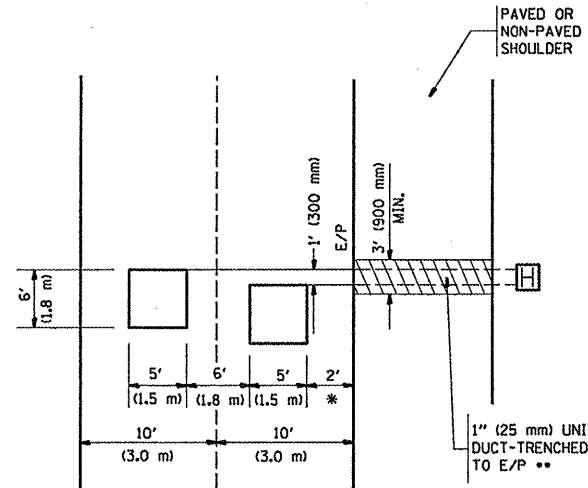
ARTERIAL ROAD
INFORMATION SIGN

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2010-015 RS	COOK	26	25
TC-22		CONTRACT NO. 60K34		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

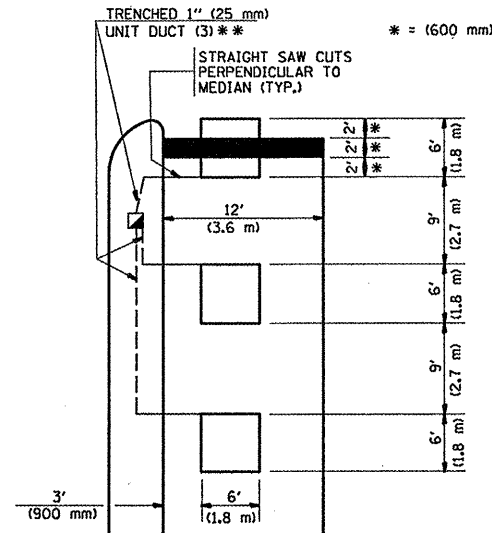


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

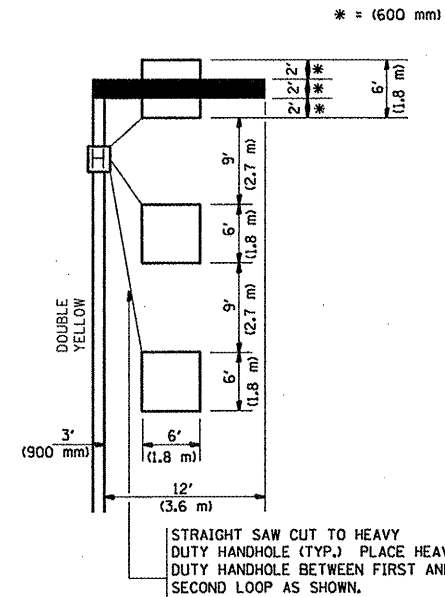
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

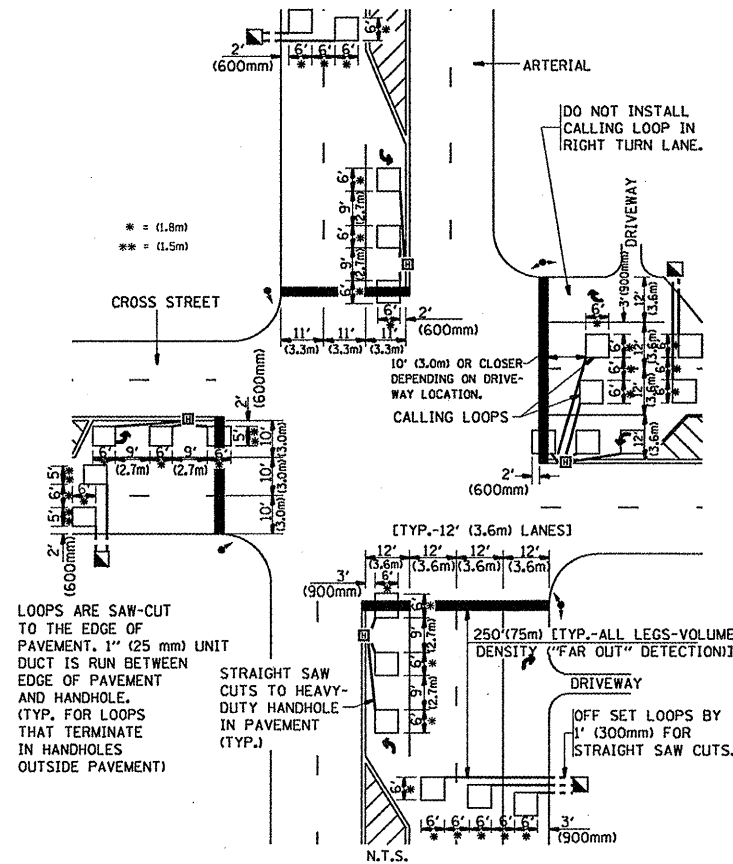
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

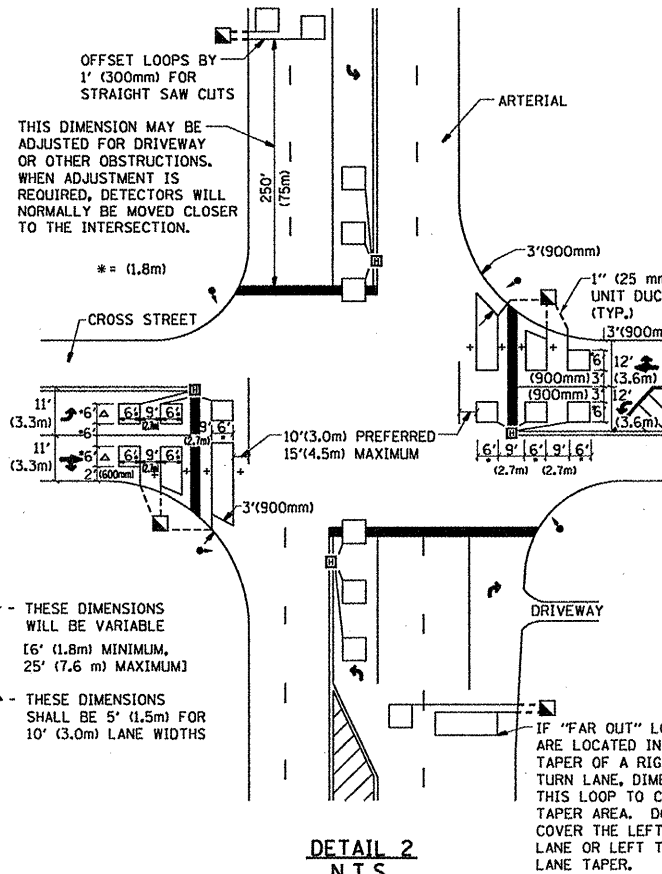
**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+- THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
▲ THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2010-015 RS	COOK	26	26
TS-07			CONTRACT NO. 60K34	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				