

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

**PROPOSED  
HIGHWAY PLANS**

**FAI ROUTE 55 (I-55) OVER THE KANKAKEE RIVER  
SECTION 88(B&B-1)BR  
BRIDGE REHABILITATION  
WILL COUNTY  
PROJECT: ESP-055-6(238)241  
C-91-166-05**

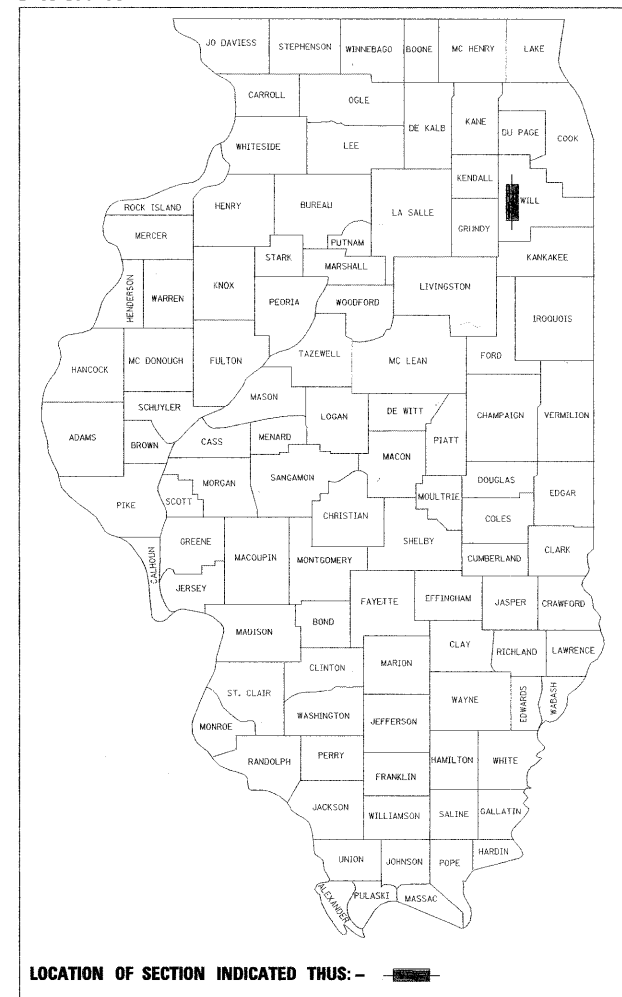
FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENTS LOCATED WITHIN  
THE CITY OF WILMINGTON, IL

|           |             |          |                    |           |
|-----------|-------------|----------|--------------------|-----------|
| F.A. RTE. | SECTION     | COUNTY   | TOTAL SHEETS       | SHEET NO. |
| 55        | 88 (B&B) BR | WILL.    | 72                 | 1         |
|           |             | ILLINOIS | CONTRACT NO. 62930 |           |

\*72+6 = 78

D-91-166-05



LOCATION OF SECTION INDICATED THUS: -

**rjngroup**  
 Excellence through Ownership  
 200 West Front Street  
 Wheaton, IL 60187  
 License # 184-000813

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

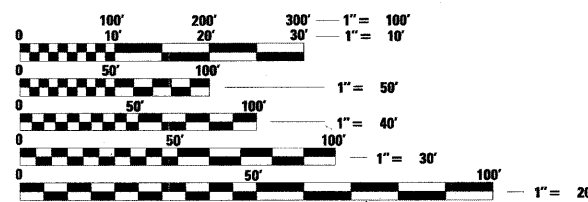
SUBMITTED Jan 24 2009  
Chris O'Neil  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

December 4, 2009  
Charles G. Ingersoll  
 ENGINEER OF DESIGN AND ENVIRONMENT

December 4, 2009  
Christine M. Reed  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**TRAFFIC DATA**

FUNCTIONAL CLASSIFICATION - INTERSTATE, CLASS I TRUCK ROUTE  
 AT NEW RIVER RD. 2005 ADT = 55,000  
 AT LORENZO RD. 2005 ADT = 40,900  
 INTERSTATE 55 POSTED SPEED LIMIT = 65 MPH

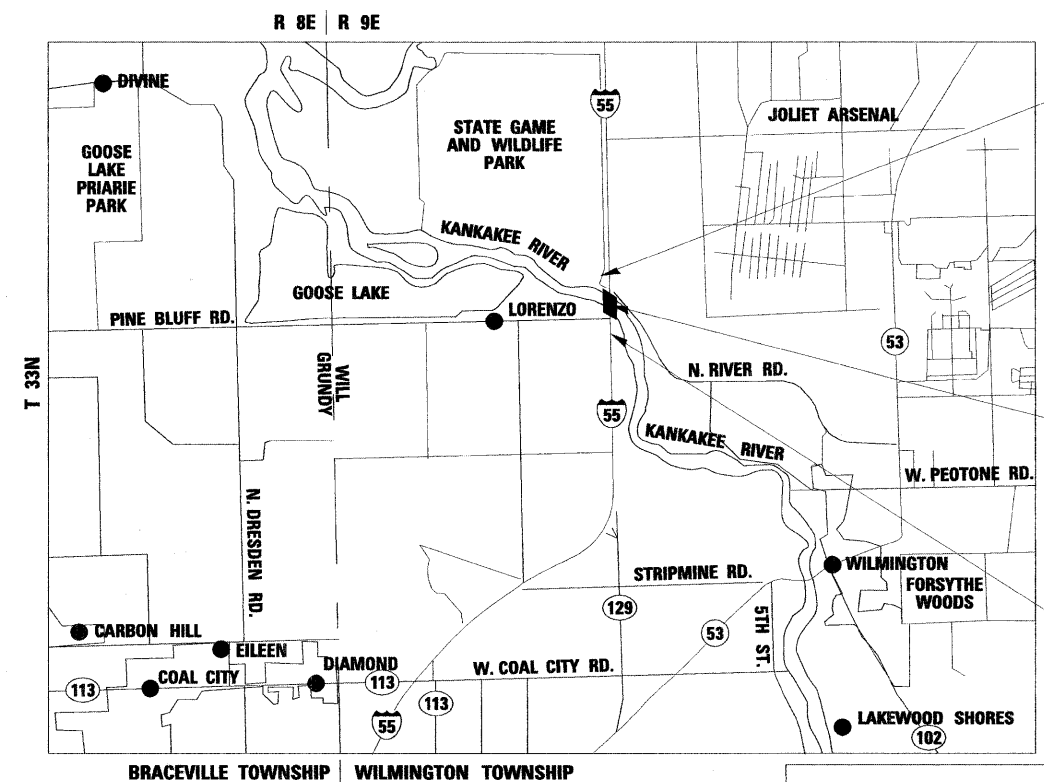


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT ENGINEER: MICHELLE AQUINO (847) 705-4606  
 PROJECT MANAGER: RAJENDRA SHAH (847) 705-4555

CONTRACT NO. 62930



**LOCATION MAP (NTS)**  
 GROSS LENGTH = NET LENGTH = 6320 FEET = 1.20 MILES

PROJECT ENDS  
STA. 90 + 70.00

LOCATION OF PROJECT  
FAI ROUTE 55 BRIDGE  
OVER KANKAKEE RIVER  
S.N. 099-0001 (NB)  
S.N. 099-0002 (SB)

PROJECT BEGINS  
STA. 27 + 50.00

|   |   |
|---|---|
| <br>MARY COOMBE BLOXDORF<br>DISCIPLINE: STRUCTURAL ENGINEER<br>LICENSE NO. 081-004859<br>LICENSE EXPIRES 11/30/2010 | <br>CHAD M. JANSON<br>DISCIPLINE - CIVIL ENGINEER<br>LICENSE NO. 062-053980<br>LICENSE EXPIRES 11/30/2009 |
|---|---|

**FINAL PLANS**

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

**INDEX OF SHEETS**

| SHEET NO. | SHEET TITLE   |
|-----------|---|
| 1         | COVER SHEET   |
| 2         | INDEX OF SHEETS & STANDARDS   |
| 3         | GENERAL NOTES   |
| 4-4A      | SUMMARY OF QUANTITIES   |
| 5         | ROADWAY TYPICAL SECTIONS  |
| 5A        | RAMP TYPICAL SECTIONS   |
| 6         | CROSSOVER TYPICAL SECTIONS  |
| 7         | ALIGNMENT & TIES  |
| 8-9       | DETOUR PLAN   |
| 10-12     | TRAFFIC CONTROL CONSTRUCTION STAGING NOTES & DETAILS                    |
| 13-14     | TRAFFIC CONTROL PLAN - STAGE II   |
| 15-16     | TRAFFIC CONTROL PLAN - STAGE III  |
| 17-21     | PLAN & PROFILE  |
| 21A       | N. RIVER ROAD EXIT RAMPS - PLAN   |
| 22-23     | DRAINAGE PLAN & PROFILE   |
| 24        | EROSION CONTROL PLAN  |
| 25-26     | PAVEMENT ELEVATION DETAILS  |
| 27-28     | PAVEMENT MARKING PLANS  |
| 29-47     | BRIDGE PLANS-SN 099-0001 (NB)   |
| 48-56     | BRIDGE PLANS-SN 099-0002 (SB)   |
| 57-64     | CROSS-SECTIONS  |
| 65        | STORM SEWER CONNECTION TO EXISTING SEWERS (BD-08)                       |
| 66        | BUTT JOINTS AND HMA TAPER (BD-32)                                       |
| 67        | ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)                          |
| 68        | FREEWAY SINGLE AND MULTI LANE WEAVE (TC-09)                             |
| 69        | RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)        |
| 70-71     | MULTI-LANE FREEWAY PAVEMENT MARKING (2 SHEETS) (TC-12)                  |
| 72        | ARTERIAL ROAD INFORMATION SIGN (TC-22)                                  |
| 72A       | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)                     |
| 72B       | TRAFFIC CLOSURE FOR SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17) |
| 72C       | SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS (TC-18)           |

**HIGHWAY STANDARDS**

| STANDARD NO. | STANDARD TITLE  |
|--------------|---|
| 000001-05    | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                            |
| 280001-04    | TEMPORARY EROSION CONTROL SYSTEMS                                       |
| 482001-02    | HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT                              |
| 482011-03    | HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING                          |
| 515001-03    | NAME PLATE FOR BRIDGES  |
| 601101-01    | CONCRETE HEADWALL FOR PIPE DRAIN  |
| 606401-01    | PAVED DITCH   |
| 630001-08    | STEEL PLATE BEAM GUARDRAIL  |
| 630301-05    | TRAFFIC BARRIER TERMINAL, TYPE 5 & 5A                                   |
| 635006-03    | REFLECTOR AND TERMINAL MARKER PLACEMENT                                 |
| 635011-02    | REFLECTOR MARKER AND MOUNTING DETAILS                                   |
| 701101-02    | OFF-ROAD OPERATIONS, MULTILANE  |
| 701321-10    | LANE CLOSURE, MULTI-LANE DIVIDED WITH CROSS OVER AND BARRIER            |
| 701400-03    | APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY                            |
| 701401-05    | LANE CLOSURE, FREEWAY/EXPRESSWAY  |
| 701411-05    | LANE CLOSURE, MULTI-LANE AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH |
| 701416-06    | LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH CROSSOVER AND BARRIER            |
| 701901-01    | TRAFFIC CONTROL DEVICES   |
| 704001-05    | TEMPORARY CONCRETE BARRIER  |
| 720001-01    | SIGN PANEL MOUNTING DETAILS   |
| 720006-02    | SIGN PANEL ERECTION DETAILS   |
| 780001-02    | TYPICAL PAVEMENT MARKINGS   |
| 781001-03    | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS                 |
| 442201-03    | CLASS C & D PATCHES   |
| 635001-01    | DELINEATORS   |
| 642001-01    | SHOULDER RUMBLE STRIPS  |

**GENERAL NOTES – ROADWAY**

- SAW CUTTING OF PAVEMENTS, SHOULDERS, ETC. SHALL BE FULL DEPTH AND SHALL RESULT IN A CLEAN, STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
- USE NO. 8 EPOXY-COATED TIE BARS (OR DOWEL BARS) CONFORMING TO ARTICLE 1003.10(B)(2) OF THE STANDARD SPECIFICATIONS FOR LONGITUDINAL CONSTRUCTION JOINT GROUTED-IN-PLACE TIE BAR AS SHOWN ON STATE STANDARD 420001 AND FOR TYING PORTLAND CEMENT CONCRETE PAVEMENT TO EXISTING CONCRETE PAVEMENT AS SHOWN ON THE PLANS. THE TIE BARS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PAVEMENT ITEMS BEING CONSTRUCTED.
- THE THICKNESS OF BITUMINOUS MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.
- THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS PAVEMENT LIFTS.

**GENERAL NOTES – TRAFFIC CONTROL & PROTECTION**

- STAGING PROCEDURES PRESENTED ARE THE SUGGESTED SEQUENCE OF OPERATIONS. AT HIS OPTION, THE CONTRACTOR MAY SUBMIT AN ALTERNATIVE STAGING PROPOSAL TO THE ENGINEER FOR HIS APPROVAL.
- TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OF CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM THE TIME OF NOTIFICATION.
- ALL TEMPORARY PAVEMENT MARKINGS PROPOSED WITHIN THE WORK AREA SHALL BE COMPLETED PRIOR TO THE CONSTRUCTION PHASE CHANGE.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE III BARRICADE USED - ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.
- THE RESIDENT ENGINEER SHALL CONTACT MS. CORA MATHIS, AREA TRAFFIC ENGINEER AT (815) 485-6475, A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE MEDIAN CROSSOVERS SHALL BE CONSTRUCTED WITH DAILY LANE CLOSURES PER DISTRICT 1 STANDARDS.
- WHEN WORKERS ARE PRESENT, CONSTRUCTION SPEED LIMIT AND FLAGGER SIGNS SHALL BE PLACED PER DISTRICT 1 STANDARDS.
- VARIABLE MESSAGE SIGNS SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER.
- TEMPORARY MOVEABLE CONCRETE BARRIER WALL SHALL BE LEASED BY FOOT FROM BARRIER SYSTEMS INC.
- THE CONTRACTOR SHALL REPLACE PRISMATIC BARRIER REFLECTORS ON THE NORTHBOUND I-55 BRIDGE STRUCTURE AS INDICATED ON THE TRAFFIC CONTROL PLAN. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR.
- REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL. REMOVAL OF TEMPORARY PAVEMENT MARKING TAPE SHALL BE PAID FOR AS WORK ZONE PAVEMENT MARKING REMOVAL.

**GENERAL NOTES – DRAINAGE & UTILITIES**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES. (48 HOUR NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES – LIGHTING**

- CONTRACTOR SHALL RELAMP, CLEAN, AND SERVICE THE EXISTING LUMINAIRES ON THE LIGHT TOWERS NORTH OF WILMINGTON ROAD. NEW SPLICES, FUSES, FUSE HOLDERS, AND SURGE PROTECTORS SHALL ALSO BE PROVIDED FOR THESE LIGHT TOWERS. THIS WORK SHALL BE PAID FOR UNDER "CLEAN AND RELAMP EXISTING LUMINAIRE" AND COMPLETED TO THE SATISFACTION OF THE ENGINEER.

**GENERAL NOTES – MISCELLANEOUS**

- ALL ELEVATIONS SHOWN ON THESE PLANS ARE ON THE U.S.G.S DATUM
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION FROM THE DEPARTMENT
- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- RESERVED.
- RESERVED.
- RESERVED.
- THE REMOVAL OF GUARDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR "GUARDRAIL REMOVAL."
- WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- RESERVED.
- USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS. USE THE "LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE)" DETAIL SHOWN ON HIGHWAY STANDARD 420001 FOR ALL LONGITUDINAL JOINTS AND FOR TYING PCC PAVEMENT WIDENING TO EXISTING CONCRETE PAVEMENT AS SHOWN ON THE PLANS.
- USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS. USE THE "LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE)" DETAIL SHOWN ON HIGHWAY STANDARD 420001 FOR ALL LONGITUDINAL JOINTS.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- RESERVED.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- RESERVED.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4151 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE ENGINEER SHALL CONTACT MS. CORA MATHIS, TRAFFIC FIELD ENGINEER, AT (815) 485-6475 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED THEIR LOCATION.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE (OR CONSTRUCTION OR BRIDGE INSPECTORS).
- STRIP REFLECTIVE CRACK CONTROL TREATMENT WILL BE INSTALLED IMMEDIATELY IN FRONT OF THE HMA SURFACING OPERATION SO TRAFFIC WILL NOT DISTURB IT BEFORE BEING COVERED BY HMA. ANY CRACK CONTROL TREATMENT DISTURBED BY TRAFFIC WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE PROPOSED HOT-MIX ASPHALT RESURFACING SHALL BE TAPERED OUT AT A RATE OF TWENTY (20) FEET PER INCH OF THICKNESS AT MAINLINE AND MAJOR SIDE STREET LOCATIONS, EXCEPT WHERE BUTT JOINTS ARE INDICATED. AT MINOR STREETS AND ENTRANCES, TAPER OUT IN TEN (10) FEET EXCEPT WHERE BUTT JOINTS ARE INDICATED. AT SEAL COATED STREETS AND ENTRANCES, TAPER OUT IN THREE (3) FEET UNLESS OTHERWISE SHOWN ON THE PLANS.

29. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

- RESERVED.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
- ALL CLASS B PAVEMENT PATCHING WHICH REQUIRES FRAMES AND GRATES TO BE ADJUSTED SHALL BE CONSTRUCTED UTILIZING "CAST IN PLACE" ALTERNATE ACCORDING TO STATE HIGHWAY STANDARD 420111 FOR "PCC PAVEMENT ROUNDOUTS".
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION OF ALL EMERGENCY SERVICES, SCHOOL DISTRICTS, I.D.O.T.'S COMMUNICATIONS CENTER, SPRINGFIELD TRUCK PERMIT SECTION AND OTHER AGENCIES AFFECTED BY THE CLOSURE. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR POSTING SIGNS THAT WILL INDICATE THE DATES THE CLOSURE WILL BE IN PLACE.

- THE ENGINEER SHALL OBTAIN APPROVAL FROM THE DISTRICT ONE BUREAU CHIEF OF MAINTENANCE FOR ANY PROPOSED REVISIONS IN THE SCHEDULED PAVEMENT PATCHING LOCATIONS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- RESERVED.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
- THE "ADVANCED WARNING SIGN DETAIL FOR ARTERIAL TRAFFIC" LOCATED IN THE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING FOR LANE CLOSURES" (AND THE SPECIAL PROVISION FOR "PUBLIC CONVENIENCE AND SAFETY") IS/ARE APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS/TOLLWAYS.

39. DISPOSAL OF SHOULDER CUTTING AND CLEAN SWEEPING OF THE SHOULDERS SHALL BE COMPLETED PRIOR TO THE OPENING OF THE SHOULDERS AND TRAFFIC. DISPOSAL AND SWEEPING SHALL BE INCLUDED IN THE COST OF "SHOULDER RUMBLE STRIPS".

**GENERAL NOTES – EROSION CONTROL & LANDSCAPING**

- ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND OF THE APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT, OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.
- ALL AREAS DISTURBED AND RESTORED WITH SEEDING SHALL BE COVERED WITH AN EROSION CONTROL BLANKET IN ACCORDANCE WITH SECTION 251 OF THE STANDARD SPECIFICATIONS.
- SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDER ARE AS OUTLINED IN THESE PLANS. ANY ADDITIONAL AREAS SHALL BE DETERMINED BY THE ENGINEER.
- BEFORE STARTING CONSTRUCTION ON STAGE 2 OF THE PROJECT, EROSION CONTROL BLANKET AND SEEDING, CLASS 3 SHALL BE PLACED AFTER FINAL GRADING HAS BEEN COMPLETED ON THE FIRST PHASE OF CONSTRUCTION.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
- LANDSCAPE CONTRACTOR SHALL STAKE THE LOCATION OF ALL SEEDING AND HAVE ALL PLANTING LAYOUTS APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- APPROVED WATERING EQUIPMENT SHALL BE AT THE SITE OF THE WORK AND IN OPERATING CONDITION PRIOR TO STARTING THE SEEDING OPERATION AND DURING ALL SEEDING OPERATIONS OR SEEDING WILL NOT BE ALLOWED.
- THE CONTRACTOR SHALL REPAIR IN KIND ANY AREAS DAMAGED AS A RESULT OF LANDSCAPING OPERATIONS
- THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP FOUR (4) INCHES IN AREAS TO BE SEEDER OR SODDED.

PLOT DATE = 12/23/2009  
 FILE NAME = K:\1182518\Addendum\182518\_000.dgn  
 PLOT SCALE = PLOT SCALE  
 MODEL NAME =  
 MODEL =

**rjngroup**  
 Excellence through Ownership  
 200 West Front Street  
 Wheaton, IL 60187

|                 |                       |
|-----------------|-----------------------|
| DESIGNED - KSD  | REVISED -  ADD. NO. 1 |
| DRAWN - KSD     | REVISED -             |
| CHECKED - CMJ   | REVISED -             |
| DATE - 03-04-09 | REVISED -             |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. ---- TO STA. ----

**I-55 OVER THE KANKAKEE RIVER  
 GENERAL NOTES**

1-12-10

| F.A.I. RTE.                                     | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------------|--------|--------------|-----------|
| 55  | 88 (B&B-1) BR | WILL   | 51           | 3         |
| CONTRACT NO. 62930                              |               |        |              |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |               |        |              |           |

SUMMARY OF QUANTITIES

| SUMMARY OF QUANTITIES |   |                  | URBAN<br>TOTAL<br>QUANTITY | CONSTRUCTION TYPE CODE |                                    |                                    |
|-----------------------|---|------------------|----------------------------|------------------------|------------------------------------|------------------------------------|
| CODE NO.              | ITEM DESCRIPTION  | UNIT             |                            | ROADWAY                | BRIDGE                             |                                    |
|                       |   |                  |                            | 1000-2A<br>QUANTITY    | SN 099-0001<br>X071-2A<br>QUANTITY | SN 099-0002<br>X071-2A<br>QUANTITY |
| 20200100              | EARTH EXCAVATION  | CU YD            | 3,610                      | 3,610                  |                                    |                                    |
| <del>20201550</del>   | <del>SUB-BASE GRANULAR MATERIAL, TYPE B</del>                       | <del>CU YD</del> | <del>783</del>             | <del>783</del>         |                                    |                                    |
| 20201200              | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL                         | CU YD            | 500                        | 500                    |                                    |                                    |
| 20400800              | FURNISHED EXCAVATION  | CU YD            | 200                        | 200                    |                                    |                                    |
| 20800150              | TRENCH BACKFILL   | CU YD            | 120                        | 120                    |                                    |                                    |
| 25000210              | SEEDING, CLASS 2A   | ACRE             | 0.56                       | 0.56                   |                                    |                                    |
| 25000400              | NITROGEN FERTILIZER NUTRIENT  | POUND            | 50                         | 50                     |                                    |                                    |
| 25000500              | PHOSPHORUS FERTILIZER NUTRIENT                                      | POUND            | 50                         | 50                     |                                    |                                    |
| 25000600              | POTASSIUM FERTILIZER NUTRIENT                                       | POUND            | 50                         | 50                     |                                    |                                    |
| 25100630              | EROSION CONTROL BLANKET   | SQ YD            | 2,713                      | 2,713                  |                                    |                                    |
| 28000500              | INLET AND PIPE PROTECTION   | EACH             | 4                          | 4                      |                                    |                                    |
| 40600895              | CONSTRUCTING TEST STRIP   | EACH             | 1                          | 1                      |                                    |                                    |
| 40603340              | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70                        | TON              | 1524                       | 1524                   |                                    |                                    |
| <del>40603595</del>   | <del>POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90</del> | <del>TON</del>   | <del>72</del>              | <del>72</del>          |                                    |                                    |
| 40800020              | BITUMINOUS MATERIALS (PRIME COAT)                                   | TON              | 45                         | 45                     |                                    |                                    |
| 42001300              | PROTECTIVE COAT   | SQ YD            | 8,802                      |                        | 4,353                              | 4,449                              |
| <del>44000155</del>   | <del>HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"</del>                  | <del>SQ YD</del> | <del>10,680</del>          | <del>10,680</del>      |                                    |                                    |
| <del>44000156</del>   | <del>HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"</del>                  | <del>SQ YD</del> | <del>2,250</del>           | <del>2,250</del>       |                                    |                                    |
| 44004250              | PAVED SHOULDER REMOVAL  | SQ YD            | 2,670                      | 2,670                  |                                    |                                    |
| 48203003              | HOT-MIX ASPHALT SHOULDERS, 1 1/2"                                   | SQ YD            | 10,680                     | 10,680                 |                                    |                                    |
| 50102400              | CONCRETE REMOVAL  | CU YD            | 93                         |                        | 85                                 | 8                                  |
| 50104400              | CONCRETE HEADWALL REMOVAL   | EACH             | 10                         | 10                     |                                    |                                    |
| 50200100              | STRUCTURE EXCAVATION  | CU YD            | 10                         |                        | 10                                 |                                    |
| 50300225              | CONCRETE STRUCTURES   | CU YD            | 46.8                       |                        | 17.3                               | 29.5                               |
| 50300255              | CONCRETE SUPERSTRUCTURE   | CU YD            | 90.8                       |                        | 90.8                               |                                    |
| 50300260              | BRIDGE DECK GROOVING  | SQ YD            | 8,306                      |                        | 4,087                              | 4,219                              |
| 50500505              | STUD SHEAR CONNECTORS   | EACH             | 19                         |                        | 19                                 |                                    |
| NP 55039700           | STORM SEWERS TO BE CLEANED  | FOOT             | 850                        | 850                    |                                    |                                    |
| 50800205              | REINFORCEMENT BARS, EPOXY COATED                                    | POUND            | 18,740                     |                        | 15,650                             | 3,090                              |
| 52000110              | PREFORMED JOINT STRIP SEAL  | FOOT             | 92                         |                        | 92                                 |                                    |
| 52000325              | NEOPRENE EXPANSION JOINT, 2 1/2"                                    | FOOT             | 97                         |                        |                                    | 97                                 |
| 52000340              | NEOPRENE EXPANSION JOINT, 4"  | FOOT             | 191                        |                        | 94                                 | 97                                 |
| 542A0217              | PIPE CULVERTS, CLASS A, TYPE 1 12"                                  | FOOT             | 339                        | 339                    |                                    |                                    |
| 542A0220              | PIPE CULVERTS, CLASS A, TYPE 1 15"                                  | FOOT             | 475                        | 475                    |                                    |                                    |
| 542A0223              | PIPE CULVERTS, CLASS A, TYPE 1 18"                                  | FOOT             | 30                         | 30                     |                                    |                                    |
| 54213657              | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"                 | EACH             | 2                          | 2                      |                                    |                                    |
| 54213660              | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"                 | EACH             | 1                          | 1                      |                                    |                                    |
| 54247090              | GRATING FOR CONCRETE FLARED END SECTION 12"                         | EACH             | 2                          | 2                      |                                    |                                    |
| 54247100              | GRATING FOR CONCRETE FLARED END SECTION 15"                         | EACH             | 1                          | 1                      |                                    |                                    |
| 59000200              | EPOXY CRACK INJECTION   | FOOT             | 902                        |                        | 627                                | 275                                |
| 60100060              | CONCRETE HEADWALL FOR PIPE DRAINS                                   | EACH             | 4                          | 4                      |                                    |                                    |
| 60107600              | PIPE UNDERDRAINS 4"   | FOOT             | 4,000                      | 4,000                  |                                    |                                    |
| 60236200              | INLETS, TYPE A, TYPE 8 GRATE  | EACH             | 3                          | 3                      |                                    |                                    |
| 60615400              | PAVED DITCH, TYPE A-15  | FOOT             | 517                        | 517                    |                                    |                                    |
| 63801200              | MODULAR GLARE SCREEN SYSTEM   | FOOT             | 4,300                      | 4,300                  |                                    |                                    |
| 67000400              | ENGINEER'S FIELD OFFICE, TYPE A                                     | CAL MO           | 12                         | 12                     |                                    |                                    |
| 67100100              | MOBILIZATION  | L SUM            | 1                          | 1                      |                                    |                                    |
| <del>70100405</del>   | <del>TRAFFIC CONTROL AND PROTECTION, STANDARD 701321</del>          | <del>EACH</del>  | <del>1</del>               | <del>1</del>           |                                    |                                    |
| 70106800              | CHANGEABLE MESSAGE SIGN   | CAL MO           | 12                         | 12                     |                                    |                                    |

| SUMMARY OF QUANTITIES |  |                  | URBAN<br>TOTAL<br>QUANTITY | CONSTRUCTION TYPE CODE |                                    |                                    |
|-----------------------|--|------------------|----------------------------|------------------------|------------------------------------|------------------------------------|
| CODE NO.              | ITEM DESCRIPTION   | UNIT             |                            | ROADWAY                | BRIDGE                             |                                    |
|                       |  |                  |                            | 1000-2A<br>QUANTITY    | SN 099-0001<br>X071-2A<br>QUANTITY | SN 099-0002<br>X071-2A<br>QUANTITY |
| 70300240              | TEMPORARY PAVEMENT MARKING - LINE 6"                                 | FOOT             | 12,250                     | 12,250                 |                                    |                                    |
| 70301000              | WORK ZONE PAVEMENT MARKING REMOVAL                                   | SQ FT            | 12,370                     | 12,370                 |                                    |                                    |
| 70400100              | TEMPORARY CONCRETE BARRIER   | FOOT             | 6,125                      | 6,125                  |                                    |                                    |
| 70400200              | RELOCATE TEMPORARY CONCRETE BARRIER                                  | FOOT             | 6,125                      | 6,125                  |                                    |                                    |
| * 78000200            | THERMOPLASTIC PAVEMENT MARKING - LINE 4"                             | FOOT             | 20,080                     | 20,080                 |                                    |                                    |
| * 78000500            | THERMOPLASTIC PAVEMENT MARKING - LINE 8"                             | FOOT             | 2,690                      | 2,690                  |                                    |                                    |
| * 78000600            | THERMOPLASTIC PAVEMENT MARKING - LINE 12"                            | FOOT             | 660                        | 660                    |                                    |                                    |
| * 78003120            | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 5"                 | FOOT             | 4,450                      | 4,450                  |                                    |                                    |
| * 78005110            | EPOXY PAVEMENT MARKING-LINE 4"                                       | FOOT             | 5,220                      | 5,220                  |                                    |                                    |
| * 78100100            | RAISED REFLECTIVE PAVEMENT MARKER                                    | EACH             | 350                        | 350                    |                                    |                                    |
| 78300100              | PAVEMENT MARKING REMOVAL   | SQ FT            | 6,430                      | 6,430                  |                                    |                                    |
| 78300200              | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL                            | EACH             | 350                        | 350                    |                                    |                                    |
| X0320887              | POLYMER CONCRETE   | CU FT            | 0.5                        |                        |                                    | 0.5                                |
| X0322185              | BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES                     | SQ YD            | 8,611                      |                        | 4,162                              | 4,449                              |
| <del>X0322250</del>   | <del>TEMPORARY INFORMATION SIGNING</del>                             | <del>SQ FT</del> | <del>1414</del>            | <del>1414</del>        |                                    |                                    |
| X0324744              | REMOVAL OF EXISTING PRECAST CONCRETE UNITS                           | SQ FT            | 180                        |                        | 180                                |                                    |
| X0325085              | TEMPORARY PAVEMENT (INTERSTATE)                                      | SQ YD            | 7,044                      | 7,044                  |                                    |                                    |
| X0325303              | STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)          | SQ FT            | 17                         |                        | 17                                 |                                    |
| X0325305              | STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES) | SQ FT            | 621                        |                        | 531                                | 90                                 |
| X0325349              | TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)                   | FOOT             | 1,038                      | 1,038                  |                                    |                                    |
| X0325416              | TRAFFIC CONTROL AND PROTECTION FOR DETOUR ROUTE                      | L SUM            | 1                          | 1                      |                                    |                                    |
| X0325426              | PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1 3/4"                      | SQ YD            | 850                        | 850                    |                                    |                                    |
| * X0325590            | HIGH TENSION CABLE MEDIAN BARRIER TERMINALS                          | EACH             | 4                          | 4                      |                                    |                                    |
| X0325775              | WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH                      | FOOT             | 37,110                     | 37,110                 |                                    |                                    |
| X4067107              | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50           | TON              | 56                         | 56                     |                                    |                                    |
| X5121800              | PERMANENT STEEL SHEET PILING   | SQ FT            | 468                        |                        | 468                                |                                    |
| XT011015              | TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)                         | L SUM            | 1                          | 1                      |                                    |                                    |
| XT013820              | TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS                            | CAL DA           | 80                         | 80                     |                                    |                                    |
| X5010522              | REMOVE CONCRETE END SECTION  | EACH             | 1                          | 1                      |                                    |                                    |
| * X0326888            | CLEAN AND RELAMP EXISTING LUMINAIRE                                  | EACH             | 57                         | 57                     |                                    |                                    |
| <del>Z0002820</del>   | <del>BARRICADES, TYPE III</del>                                      | <del>WEEK</del>  | <del>60</del>              | <del>60</del>          |                                    |                                    |
| Z0006201              | BRIDGE DECK HYDRO-SCARIFICATION 1"                                   | SQ YD            | 8,611                      |                        | 4,162                              | 4,449                              |
| Z0013798              | CONSTRUCTION LAYOUT  | L SUM            | 1                          | 1                      |                                    |                                    |
| NP Z0014700           | CULVERT TO BE CLEANED  | EACH             | 14                         | 14                     |                                    |                                    |
| Z0016001              | DECK SLAB REPAIR (FULL DEPTH, TYPE I)                                | SQ YD            | 6                          |                        | 5                                  | 1                                  |
| Z0016002              | DECK SLAB REPAIR (FULL DEPTH, TYPE II)                               | SQ YD            | 238                        |                        | 238                                |                                    |
| NP Z0018500           | DRAINAGE STRUCTURES TO BE CLEANED                                    | EACH             | 14                         | 14                     |                                    |                                    |
| Z0030150              | IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3                   | EACH             | 4                          | 4                      |                                    |                                    |
| Z0030250              | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3        | EACH             | 2                          | 2                      |                                    |                                    |
| Z0030350              | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3         | EACH             | 2                          | 2                      |                                    |                                    |
| Z0040530              | PIPE UNDERDRAIN REMOVAL  | FOOT             | 4,000                      | 4,000                  |                                    |                                    |
| Z0043800              | PRECAST PRESTRESSED CONCRETE I-BEAM REPAIR                           | SQ FT            | 11.5                       |                        | 11.5                               |                                    |
| Z0065740              | SLOTTED DRAIN 12" WITH VARIABLE SLOT                                 | FOOT             | 466                        | 466                    |                                    |                                    |
| ⊙ Z0076600            | TRAINEES   | HOUR             | 1000                       | 1000                   |                                    |                                    |
| Z0076870              | UNDERDRAIN CONNECTION TO STRUCTURE                                   | EACH             | 4                          | 4                      |                                    |                                    |
| * X0326676            | REPLACE SURFACE SENSOR AND TEMPERATURE PROBE                         | L SUM            | 1                          | 1                      |                                    |                                    |
| X0326677              | REMOVE HIGH TENSION CABLE MEDIAN BARRIER                             | FOOT             | 2,390                      | 2,390                  |                                    |                                    |
| Z0001050              | AGGREGATE SUBGRADE, 12"  | SQ YD            | 7,044                      | 7,044                  |                                    |                                    |
| Z0065760              | SLOTTED DRAIN 15" WITH VARIABLE SLOT                                 | FOOT             | 573                        | 573                    |                                    |                                    |

\* SPECIALTY ITEM  
NP = NON-PARTICIPATING  
⊙ = Y080

**rjn group**  
200 West Front Street  
Wheaton, IL 60187  
Excellence through Ownership

|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

I-55 OVER THE KANKAKEE RIVER  
SUMMARY OF QUANTITIES  
SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. ---- TO STA. ----

|   |                       |             |                 |             |
|---|-----------------------|-------------|-----------------|-------------|
| F.A.I. RTE. 55                                  | SECTION 88 (R&B-1) BR | COUNTY WILL | TOTAL SHEETS 72 | SHEET NO. 4 |
| CONTRACT NO. 62930                              |                       |             |                 | Rev.        |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                       |             |                 | Rev.        |

Rev. 12-30-09



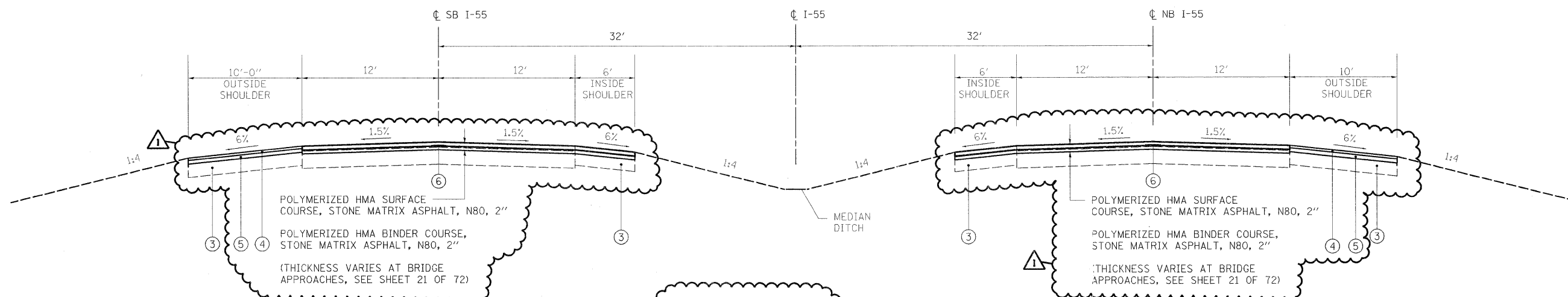
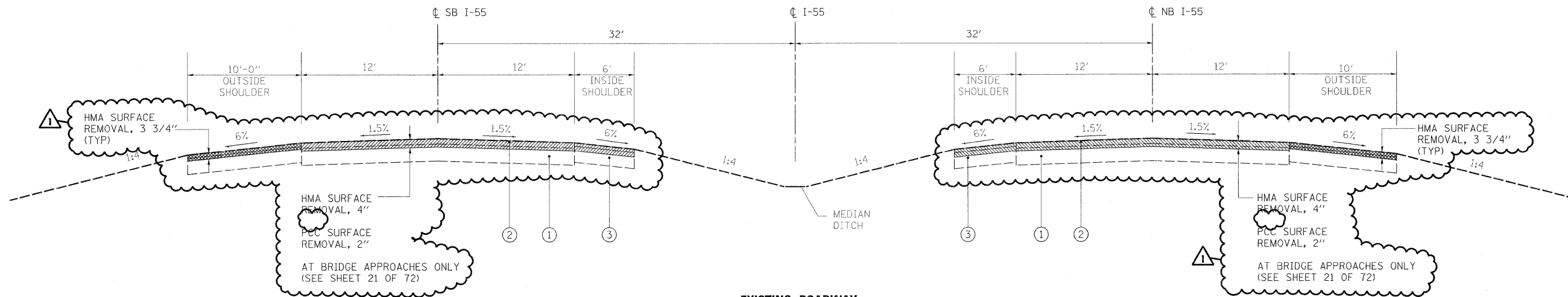
SUMMARY OF QUANTITIES

| SUMMARY OF QUANTITIES |   |       | URBAN<br>TOTAL<br>QUANTITY | CONSTRUCTION TYPE CODE         |  |  |
|-----------------------|---|-------|----------------------------|--------------------------------|--|--|
| CODE NO.              | ITEM DESCRIPTION  | UNIT  |                            | ROADWAY<br>I000-2A<br>QUANTITY | BRIDGE<br>SN 099-0001 X071-2A QUANTITY<br>SN 099-0002 X071-2A QUANTITY |  |
| 40600300              | AGGREGATE (PRIME COAT)  | TON   | 223                        | 223                            |  |  |
| 40600982              | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT                          | SQ YD | 144                        | 144                            |  |  |
| 40601005              | HOT MIX ASPHALT REPLACEMENT OVER PATCHES                              | TON   | 530                        | 530                            |  |  |
| 40603085              | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70                           | TON   | 2,286                      | 2,286                          |  |  |
| 44000164              | HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"                               | SQ YD | 18,147                     | 18,147                         |  |  |
| 44000165              | HOT-MIX ASPHALT SURFACE REMOVAL, 4"                                   | SQ YD | 37,548                     | 37,548                         |  |  |
| 44002216              | HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"                              | SQ YD | 2366                       | 2366                           |  |  |
| 44201765              | CLASS D PATCHES, TYPE II, 10 INCH                                     | SQ YD | 901                        | 901                            |  |  |
| 44201769              | CLASS D PATCHES, TYPE III, 10 INCH                                    | SQ YD | 676                        | 676                            |  |  |
| 44201771              | CLASS D PATCHES, TYPE IV, 10 INCH                                     | SQ YD | 676                        | 676                            |  |  |
| 44300900              | STRIP REFLECTIVE CRACK CONTROL TREATMENT SYSTEM A                     | FOOT  | 10,915                     | 10,915                         |  |  |
| 63500105              | DELINEATORS   | EACH  | 133                        | 133                            |  |  |
| 64200105              | SHOULDER RUMBLE STRIP   | FOOT  | 16,053                     | 16,053                         |  |  |
| X0322729              | MATERIAL TRANSFER DEVICE  | TON   | 10,138                     | 10,138                         |  |  |
| X0324685              | TEST STRIP (STONE MATRIX ASPHALT)                                     | EACH  | 2                          | 2                              |  |  |
| 40603153              | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 | TON   | 5,069                      | 5,069                          |  |  |
| 40603148              | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80  | TON   | 5,069                      | 5,069                          |  |  |



\* SPECIALTY ITEM

Added Sheet 12-30-09



**INSIDE SHOULDER MILLING & RESURFACING:**  
 STA 27+50.00 TO 33+50.00  
 STA 42+50.00 TO 61+12.50  
 STA 71+12.50 TO 76+50.00  
 STA 87+50.00 TO 90+70.00

**MAINLINE PAVEMENT & OUTSIDE SHOULDER MILLING & RESURFACING:**  
 STA 27+50.00 TO 61+12.50  
 STA 71+12.50 TO 90+70.00

**HOT-MIX ASPHALT MIXTURE**

| MIXTURE TYPE   | THICKNESS | AIR VOIDS      |
|--|-----------|----------------|
| HMA BINDER COURSE, IL-19.0, N70                            | 1 1/2"    | 4% @ 70 Gyr.   |
| HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)               | 2 1/4"    | 4% @ 70 Gyr.   |
| POLYMERIZED HMA BINDER COURSE, SMA, N80                    | 2"        | 3.5% @ 80 Gyr. |
| POLYMERIZED HMA SURFACE COURSE, SMA, N80                   | 2"        | 3.5% @ 80 Gyr. |
| HMA CROSSOVER, HMA BINDER IL-19mm                          | 13"       | 2% @ 30 Gyr.   |
| HMA SHOULDERS, HMA BINDER IL-19mm                          | 10"       | 2% @ 30 Gyr.   |
| STABILIZED SUBBASE (HMA IL-19mm)                           | 4 1/2"    | 2% @ 30 Gyr.   |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | VARIABLE  | 4% @ 50 Gyr.   |

THE UNIT WEIGHT TO CALCULATE ALL HMA STONE MATRIX ASPHALT MIXTURE QUANTITIES IS 135LBS/SY/IN THE UNIT WEIGHT TO CALCULATE ALL OTHER HMA MIXTURES QUANTITIES IS 112LBS/SY/IN  
 \*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.  
 THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.  
 FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

- LEGEND**
- ① EXISTING PCC PAVEMENT, 10"
  - ② EXISTING HOT-MIX ASPHALT SURFACE, VARIES
  - ③ EXISTING HOT-MIX ASPHALT SHOULDER, 10"
  - ④ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
  - ⑤ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/4"
  - ⑥ STRIP REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A

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 MODEL = Default



200 West Front Street  
 Wheaton, IL 60187

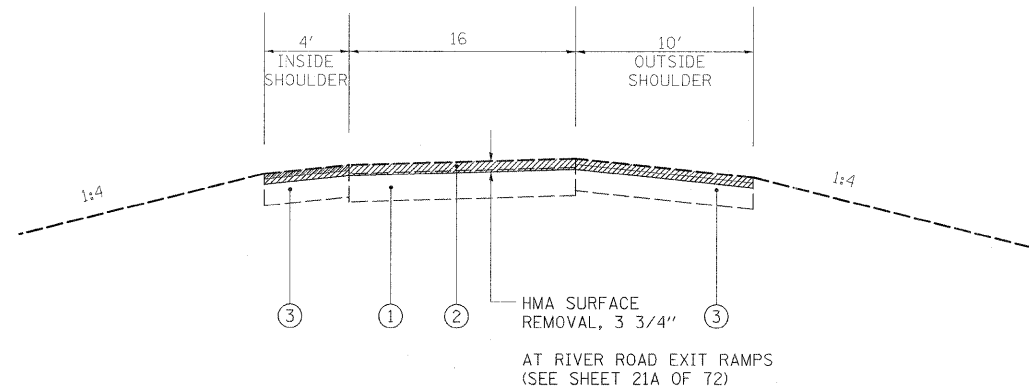
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| DESIGNED - KSD  | REVISED - ADD. NO. 1 |
| DRAWN - KSD     | REVISED -            |
| CHECKED - CMJ   | REVISED -            |
| DATE - 03-04-09 | REVISED -            |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

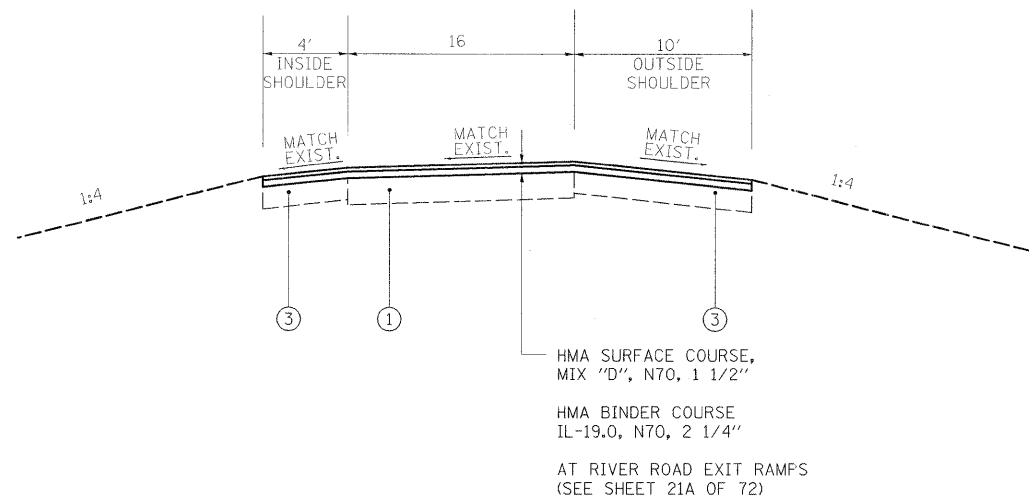
**I-55 OVER THE KANKAKEE RIVER  
 TYPICAL SECTIONS**

SCALE: NTS SHEET NO. 1 OF 3 SHEETS STA. ---- TO STA. ----

| F.A.I. RTE.                                       | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------------|--------|--------------|-----------|
| 55  | 88 (B&B-1) BR | WILL   | 72           | 5         |
| CONTRACT NO. 62930                                |               |        |              |           |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT |               |        |              |           |



**EXISTING  
N. RIVER ROAD EXIT RAMP  
TYPICAL SECTION**



**PROPOSED  
N. RIVER ROAD EXIT RAMP  
TYPICAL SECTION**

- LEGEND**
- ① EXISTING PCC PAVEMENT
  - ② EXISTING HOT-MIX ASPHALT SURFACE, VARIES
  - ③ EXISTING HOT-MIX ASPHALT SHOULDER, VARIES

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 USER NAME = dmadeg  
 MODEL = Default



200 West Front Street  
Wheaton, IL 60187

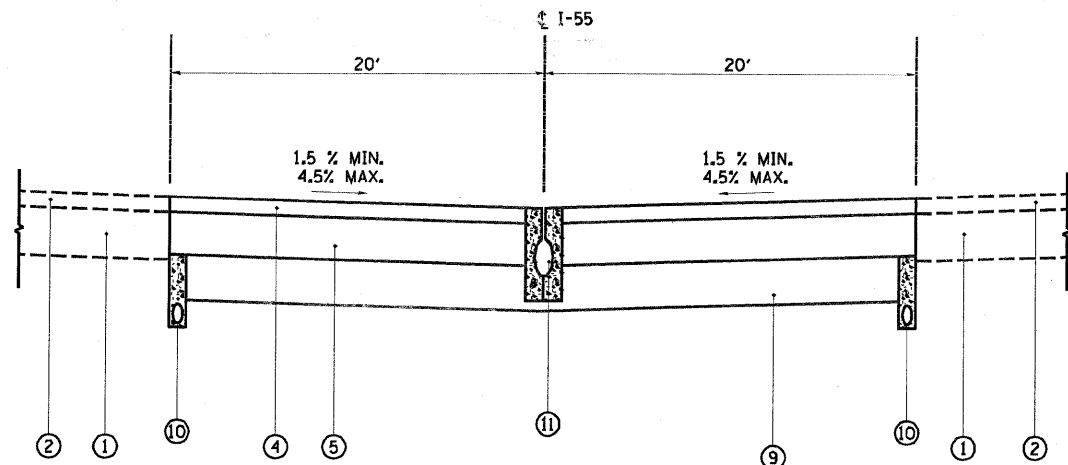
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| DESIGNED - KSD  | REVISED - ADD. NO. 1 |
| DRAWN - KSD     | REVISED -            |
| CHECKED - CMJ   | REVISED -            |
| DATE - 03-04-09 | REVISED -            |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**I-55 OVER THE KANKAKEE RIVER  
RAMP TYPICAL SECTIONS**

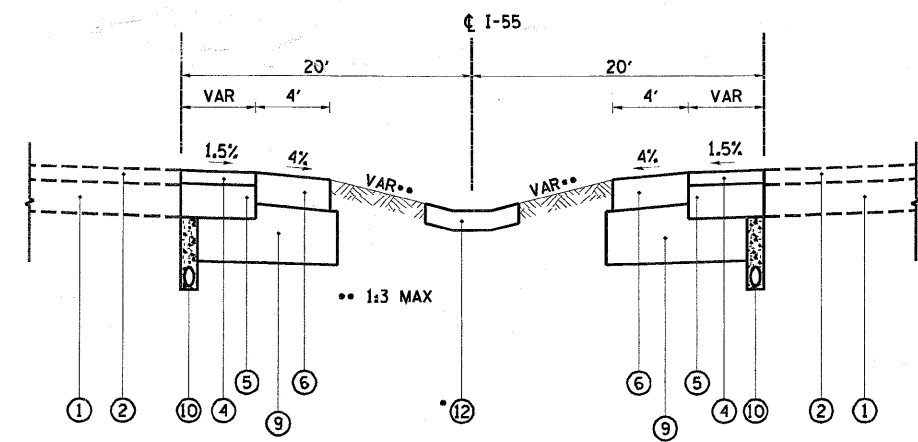
SCALE: NTS SHEET NO. 2 OF 3 SHEETS STA. ---- TO STA. ----

| F.A.I. RTE.  | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|---------------|--------|--------------|-----------|
| 55   | 88 (B&B-1) BR | WILL   | 72           | 5A        |
| <b>CONTRACT NO. 62930</b>                                      |               |        |              |           |
| <small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small> |               |        |              |           |



**TYPICAL SECTION - ALTERNATE A**

STA 35+97.64 TO 40+63.34  
STA 78+93.80 TO 84+65.56



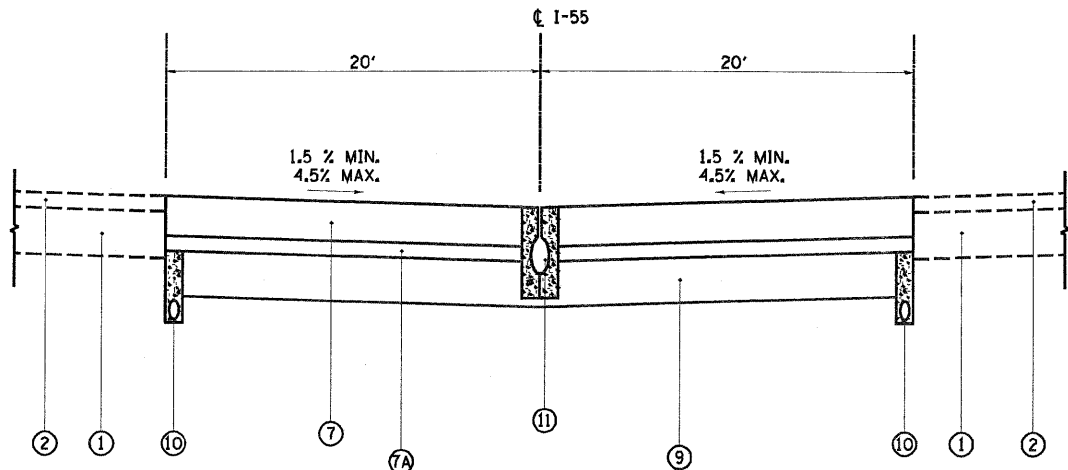
**TYPICAL SECTION - ALTERNATE A**

STA 33+50.00 TO 35+97.84  
STA 40+63.34 TO 42+50.00  
STA 76+50.00 TO 78+93.80  
STA 84+65.56 TO 87+50.00

\* SEE PLAN AND PROFILE FOR LOCATION OF STORM SEWER AND PAVED DITCH

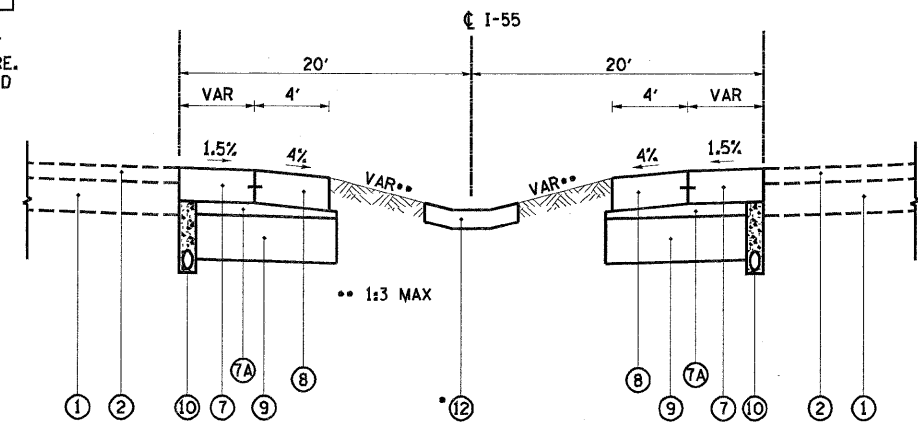
CONTRACTOR SHALL BID ACCORDING TO EITHER ALTERNATE A OR ALTERNATE B. PAVEMENT MATERIAL IN EITHER ALTERNATE WILL BE MEASURED AND PAID FOR PER SQUARE YARD AS TEMPORARY PAVEMENT (INTERSTATE). SEE SPECIAL PROVISIONS.

CROSS-SECTIONS (SHEETS 57-64) MAY NOT MATCH THE TYPICAL SECTIONS SHOWN HERE. PLAN QUANTITIES WERE CALCULATED BASED ON TYPICAL SECTIONS SHOWN HERE.



**TYPICAL SECTION - ALTERNATE B**

STA 35+97.64 TO 40+63.34  
STA 78+93.80 TO 84+65.56

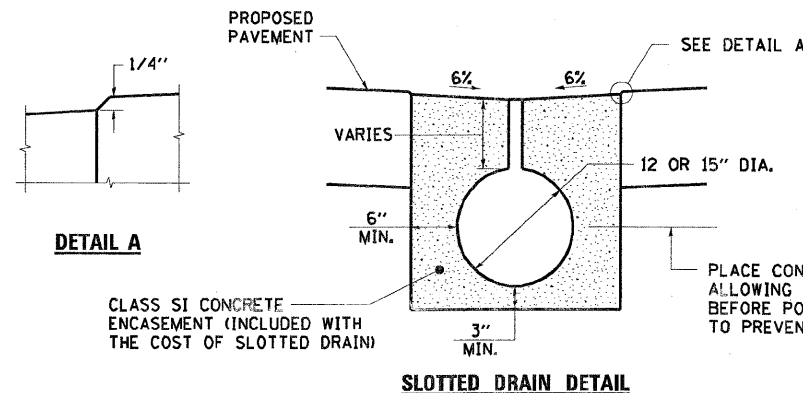


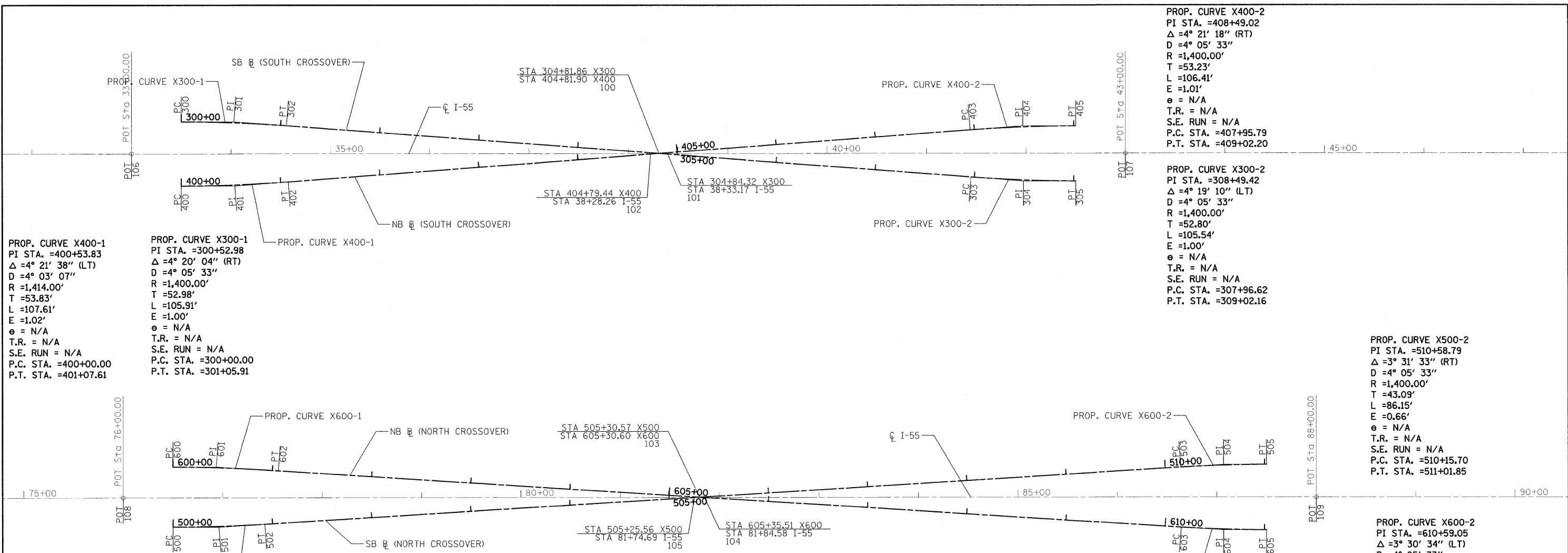
**TYPICAL SECTION - ALTERNATE B**

STA 33+50.00 TO 35+97.84  
STA 40+63.34 TO 42+50.00  
STA 76+50.00 TO 78+93.80  
STA 84+65.56 TO 87+50.00

- LEGEND**
- 1 EXISTING PCC PAVEMENT, 10"
  - 2 EXISTING BITUMINOUS CONCRETE SURFACE, VARIES
  - 3 EXISTING BITUMINOUS SHOULDER, 10"
  - 4 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
  - 5 PROPOSED HOT-MIX ASPHALT BINDER COURSE, 13"
  - 6 PROPOSED HOT-MIX ASPHALT SHOULDER, 10"
  - 7 PROPOSED JPCC PAVEMENT, 10"
  - 7A PROPOSED STABILIZED SUBBASE, 4 1/2"
  - 8 PROPOSED PCC SHOULDER, 10" (HINGED)
  - 9 PROPOSED AGGREGATE SUBGRADE, 12"
  - 10 PIPE UNDERDRAINS, 4"
  - 11 SLOTTED DRAIN
  - 12 PAVED DITCH TYPE A-15

**NOTE**  
~~CONCRETE SHALL BE ACCORDING TO STD. 883001.~~  
~~MIN. SUBBASE GRANULAR INTERLUM.~~  
~~TYPE B SUBSTITUTED FOR TYPE C.~~  
~~ALL CONCRETE PAVEMENT SHALL BE~~  
~~CLASS II.~~





PROP. CURVE X400-1  
 PI STA. =400+53.83  
 $\Delta = 4^\circ 21' 38''$  (LT)  
 $D = 4^\circ 03' 07''$   
 $R = 1,414.00'$   
 $T = 53.83'$   
 $L = 107.61'$   
 $E = 1.02'$   
 $e = N/A$   
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. =400+00.00  
 P.T. STA. =401+07.61

PROP. CURVE X300-1  
 PI STA. =300+52.98  
 $\Delta = 4^\circ 20' 04''$  (RT)  
 $D = 4^\circ 05' 33''$   
 $R = 1,400.00'$   
 $T = 52.98'$   
 $L = 105.91'$   
 $E = 1.00'$   
 $e = N/A$   
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. =300+00.00  
 P.T. STA. =301+05.91

PROP. CURVE X400-2  
 PI STA. =408+49.02  
 $\Delta = 4^\circ 21' 18''$  (RT)  
 $D = 4^\circ 05' 33''$   
 $R = 1,400.00'$   
 $T = 53.23'$   
 $L = 106.41'$   
 $E = 1.01'$   
 $e = N/A$   
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. =407+95.79  
 P.T. STA. =409+02.20

PROP. CURVE X300-2  
 PI STA. =308+49.42  
 $\Delta = 4^\circ 19' 10''$  (LT)  
 $D = 4^\circ 05' 33''$   
 $R = 1,400.00'$   
 $T = 52.80'$   
 $L = 105.54'$   
 $E = 1.00'$   
 $e = N/A$   
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. =307+96.62  
 P.T. STA. =309+02.16

PROP. CURVE X500-2  
 PI STA. =510+58.79  
 $\Delta = 3^\circ 31' 33''$  (RT)  
 $D = 4^\circ 05' 33''$   
 $R = 1,400.00'$   
 $T = 43.09'$   
 $L = 86.15'$   
 $E = 0.66'$   
 $e = N/A$   
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. =510+15.70  
 P.T. STA. =511+01.85

PROP. CURVE X600-2  
 PI STA. =610+59.05  
 $\Delta = 3^\circ 30' 34''$  (LT)  
 $D = 4^\circ 05' 33''$   
 $R = 1,400.00'$   
 $T = 42.89'$   
 $L = 85.75'$   
 $E = 0.66'$   
 $e = N/A$   
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. =610+16.16  
 P.T. STA. =611+01.91

PROP. CURVE X600-1  
 PI STA. =600+43.48  
 $\Delta = 3^\circ 33' 29''$  (RT)  
 $D = 4^\circ 05' 33''$   
 $R = 1,400.00'$   
 $T = 43.48'$   
 $L = 86.94'$   
 $E = 0.68'$   
 $e = N/A$   
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. =600+00.00  
 P.T. STA. =600+86.94

PROP. CURVE X500-1  
 PI STA. =500+46.16  
 $\Delta = 3^\circ 46' 36''$  (LT)  
 $D = 4^\circ 05' 33''$   
 $R = 1,400.00'$   
 $T = 46.16'$   
 $L = 92.28'$   
 $E = 0.76'$   
 $e = N/A$   
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. =500+00.00  
 P.T. STA. =500+92.28

|       |       | CONTROL POINTS |            |           |         |                                |
|-------|-------|----------------|------------|-----------|---------|--------------------------------|
|       | POINT | NORTHING       | EASTING    | STATION   | OFFSET  | DESCRIPTION                    |
| MISC  | 100   | 1702991.69     | 1023172.08 | 38+30.17  | 0.19 LT | INTERSECTION OF X300 & X400    |
|       | 101   | 1702994.15     | 1023172.21 | 38+28.26  | 0.00 RT | INTERSECTION OF X300 & ML I-55 |
|       | 102   | 1702989.25     | 1023172.31 | 38+33.17  | 0.00 RT | INTERSECTION OF X400 & ML I-55 |
|       | 103   | 1707339.72     | 1023081.78 | 81+79.68  | 0.31 LT | INTERSECTION OF X500 & X600    |
|       | 104   | 1707344.62     | 1023081.98 | 81+74.69  | 0.00 RT | INTERSECTION OF X600 & ML I-55 |
|       | 105   | 1707334.74     | 1023082.19 | 81+35.51  | 0.00 RT | INTERSECTION OF X500 & ML I-55 |
|       | 106   | 1702461.10     | 1023183.40 | 33+00.00  | 0.00 RT | P.O.T. ML I-55                 |
|       | 107   | 1703460.88     | 1023162.41 | 43+00.00  | 0.00 RT | P.O.T. ML I-55                 |
|       | 108   | 1706760.18     | 1023094.26 | 76+00.00  | 0.00 RT | P.O.T. ML I-55                 |
|       | 109   | 1707959.91     | 1023069.05 | 88+00.00  | 0.00 RT | P.O.T. ML I-55                 |
| X 300 | 300   | 1702510.43     | 1023149.71 | 300+00.00 | 0.00 RT | P.C. X300                      |
|       | 301   | 1702563.40     | 1023148.61 | 300+52.98 | 1.00 LT | P.I. X300                      |
|       | 302   | 1702616.30     | 1023151.51 | 301+05.91 | 0.00 RT | P.T. X300                      |
|       | 303   | 1703305.97     | 1023189.30 | 307+96.62 | 0.00 RT | P.C. X300                      |
|       | 304   | 1703358.69     | 1023192.19 | 308+49.42 | 1.00 RT | P.I. X300                      |
| X 400 | 305   | 1703411.48     | 1023191.10 | 309+02.16 | 0.00 RT | P.T. X300                      |
|       | 400   | 1702511.76     | 1023214.60 | 400+00.00 | 0.00 RT | P.C. X400                      |
|       | 401   | 1702565.58     | 1023213.49 | 400+53.83 | 1.02 RT | P.I. X400                      |
|       | 402   | 1702619.16     | 1023208.28 | 401+07.61 | 0.00 RT | P.T. X400                      |
|       | 403   | 1703304.10     | 1023141.71 | 407+95.79 | 0.00 RT | P.C. X400                      |
| X 500 | 404   | 1703357.09     | 1023136.57 | 408+49.02 | 1.01 LT | P.I. X400                      |
|       | 405   | 1703410.31     | 1023135.46 | 409+02.20 | 0.00 RT | P.T. X400                      |
|       | 500   | 1706810.77     | 1023122.37 | 500+00.00 | 0.00 RT | P.C. X500                      |
|       | 501   | 1706856.92     | 1023121.61 | 500+46.16 | 0.76 RT | P.I. X500                      |
|       | 502   | 1706902.93     | 1023117.81 | 500+92.28 | 0.00 RT | P.T. X500                      |
| X 600 | 503   | 1707823.22     | 1023041.89 | 510+15.70 | 0.00 RT | P.C. X500                      |
|       | 504   | 1707866.16     | 1023038.35 | 510+58.79 | 0.66 LT | P.I. X500                      |
|       | 505   | 1707909.24     | 1023037.45 | 511+01.85 | 0.00 RT | P.T. X500                      |
|       | 600   | 1706809.52     | 1023062.45 | 600+00.00 | 0.00 RT | P.C. X600                      |
|       | 601   | 1706853.00     | 1023061.55 | 600+43.48 | 0.68 LT | P.I. X600                      |
| X 700 | 602   | 1706896.44     | 1023063.36 | 600+84.94 | 0.00 RT | P.T. X600                      |
|       | 603   | 1707824.87     | 1023101.94 | 610+16.16 | 0.00 RT | P.C. X600                      |
|       | 604   | 1707867.72     | 1023103.72 | 610+59.05 | 0.66 RT | P.I. X600                      |
|       | 605   | 1707910.60     | 1023102.87 | 611+01.91 | 0.00 RT | P.T. X600                      |

**BENCHMARKS:**

- CHISELED "X" ON TOP OF CONCRETE PARAPET, SWW CORNER OF SB BRIDGE  
ELEVATION = 533.22
- ZDP OF LARGE CONCRETE LIGHT POLE BASE FOUNDATION IN MEDIAN ± STA. 9+85  
ELEVATION = 528.43



200 West Front Street  
Wheaton, IL 60187

|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

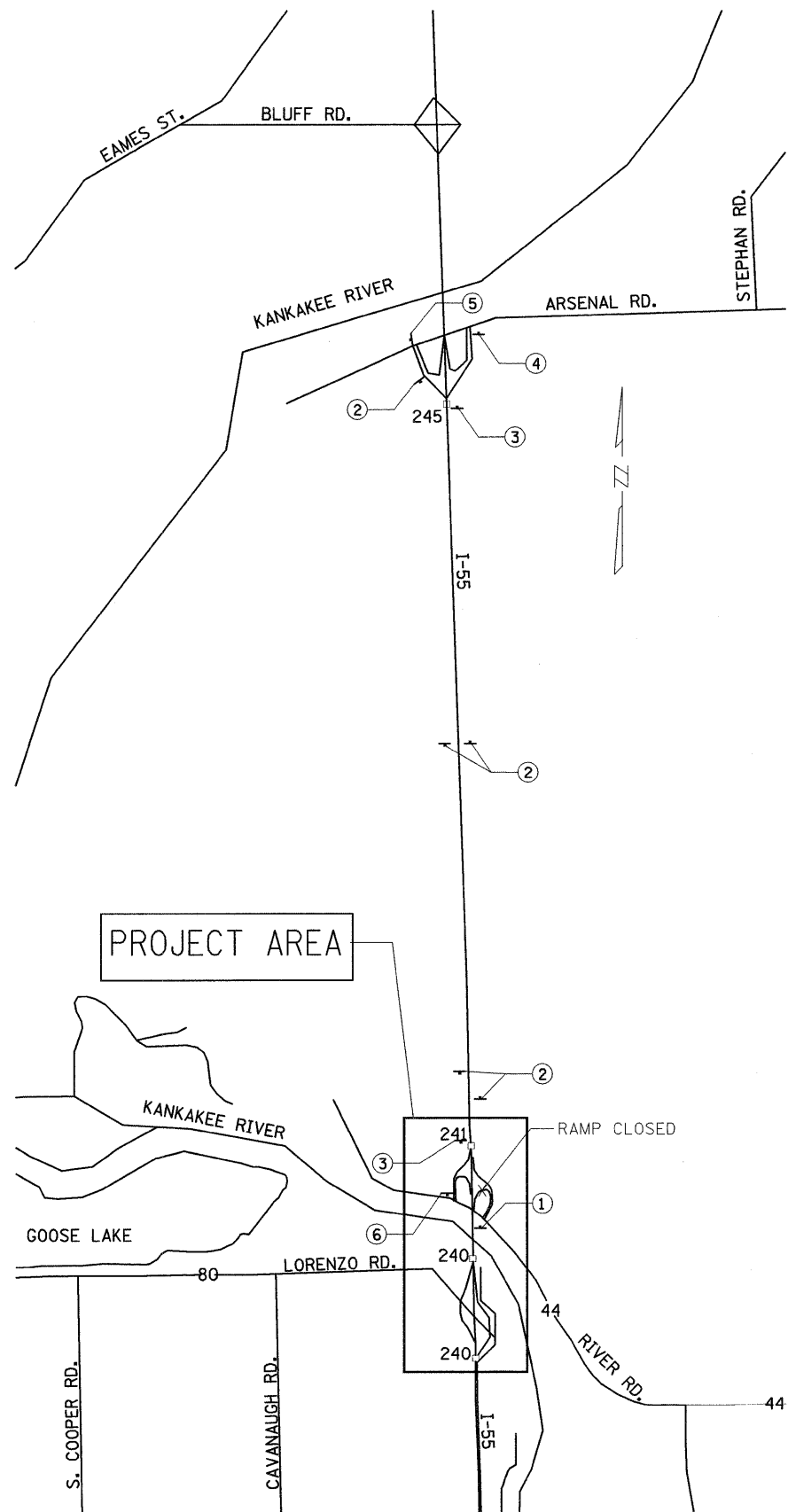
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**I-55 OVER THE KANKAKEE RIVER  
ALIGNMENTS & TIES**

SCALE: 1" = 50' SHEET NO. 1 OF 1 SHEETS STA. ---- TO STA. ----

|   |               |        |                    |           |
|---|---------------|--------|--------------------|-----------|
| F.A.I. RTE.   | SECTION       | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 55  | 88 (B&B-1) BR | WILL   | 72                 | 7         |
| FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT |               |        | CONTRACT NO. 62930 |           |





**STAGE II DETOUR SIGNING**

- ① W20-2(0)-48  
W/TYPE 2 FLASHING LIGHT
- ② 

|            |
|------------|
| RIVER ROAD |
| DETOUR     |
| ↑          |

 M4-8(0) 3015  
M6-3(0) 3018
- ③ 

|            |
|------------|
| RIVER ROAD |
| DETOUR     |
| ↗          |

 M4-8(0) 3015  
M6-2R(0) 3018
- ④ 

|            |
|------------|
| RIVER ROAD |
| DETOUR     |
| ↙          |

 M4-8(0) 3015  
M5-1L(0) 3018
- ⑤ 

|            |
|------------|
| RIVER ROAD |
| DETOUR     |
| ←          |

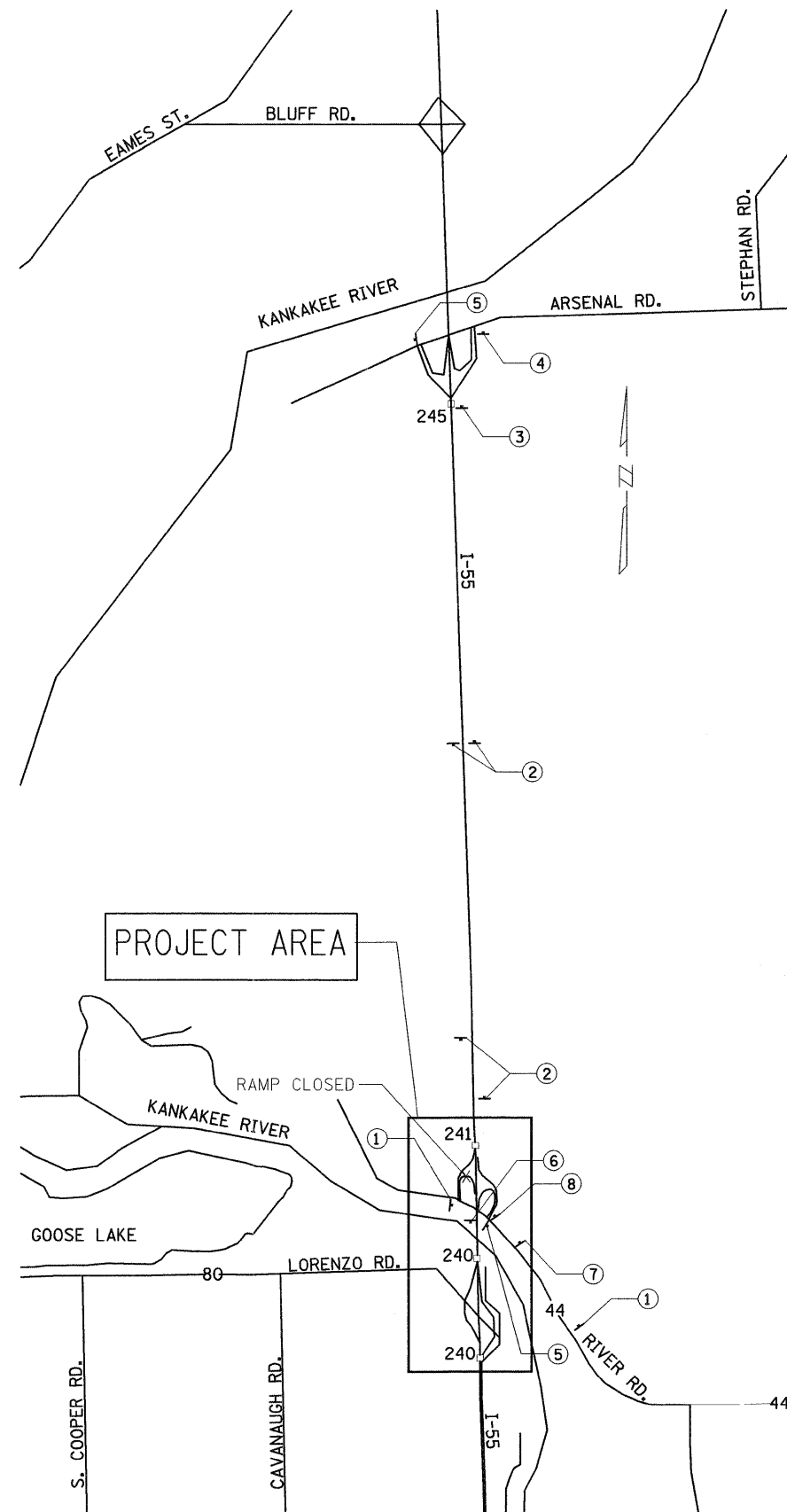
 M4-8(0) 3015  
M6-1(0) 3018
- ⑥ 

|        |
|--------|
| END    |
| DETOUR |

 M4-6(0) 3015  
M4-8(0) 3015

■ SIGN

NOTE: REFER TO DISTRICT 1 DETAIL "FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS" FOR ADDITIONAL SIGNAGE.



**STAGE III DETOUR SIGNING**

- ① W20-2(0)-48  
W/TYPE 2 FLASHING LIGHT
- ② 

|        |              |
|--------|--------------|
|        | M3-3         |
|        | M1-1         |
| DETOUR | M4-8(0) 3015 |
| ↑      | M6-3(0) 3018 |
- ③ 

|        |               |
|--------|---------------|
|        | M3-3          |
|        | M1-1          |
| DETOUR | M4-8(0) 3015  |
| ↗      | M6-2R(0) 3018 |
- ④ 

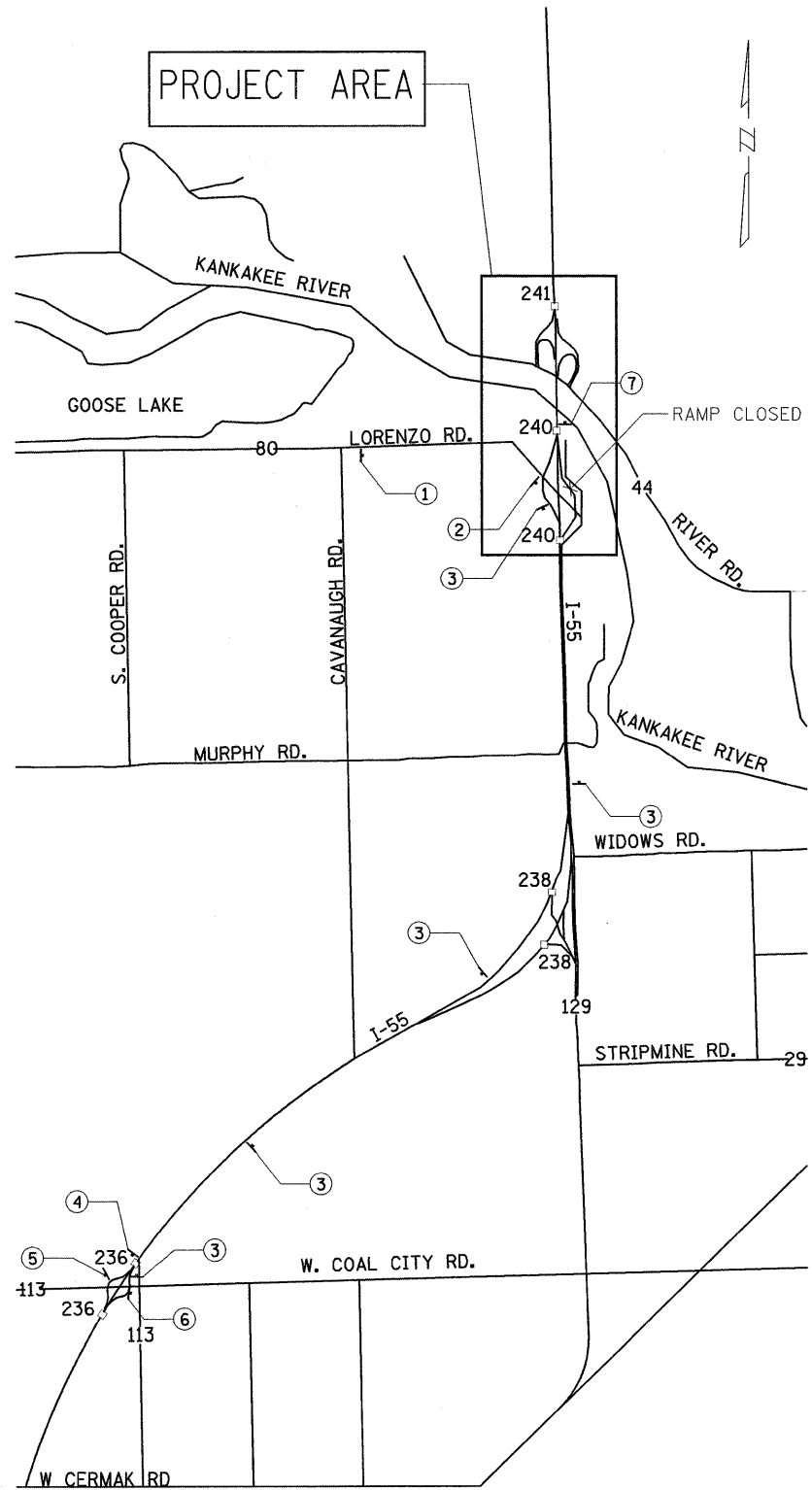
|        |               |
|--------|---------------|
|        | M3-3          |
|        | M1-1          |
| DETOUR | M4-8(0) 3015  |
| ↙      | M5-1L(0) 3018 |
- ⑤ 

|        |              |
|--------|--------------|
|        | M3-3         |
|        | M1-1         |
| DETOUR | M4-8(0) 3015 |
| ←      | M6-1(0) 3018 |
- ⑥ 

|        |              |
|--------|--------------|
| END    | M4-6(0) 3015 |
| DETOUR | M4-8(0) 3015 |
- ⑦ 

|        |               |
|--------|---------------|
|        | M3-3          |
|        | M1-1          |
| DETOUR | M4-8(0) 3015  |
| ↘      | M5-1R(0) 3018 |
- ⑧ 

|        |              |
|--------|--------------|
|        | M3-3         |
|        | M1-1         |
| DETOUR | M4-8(0) 3015 |
| →      | M6-1(0) 3018 |

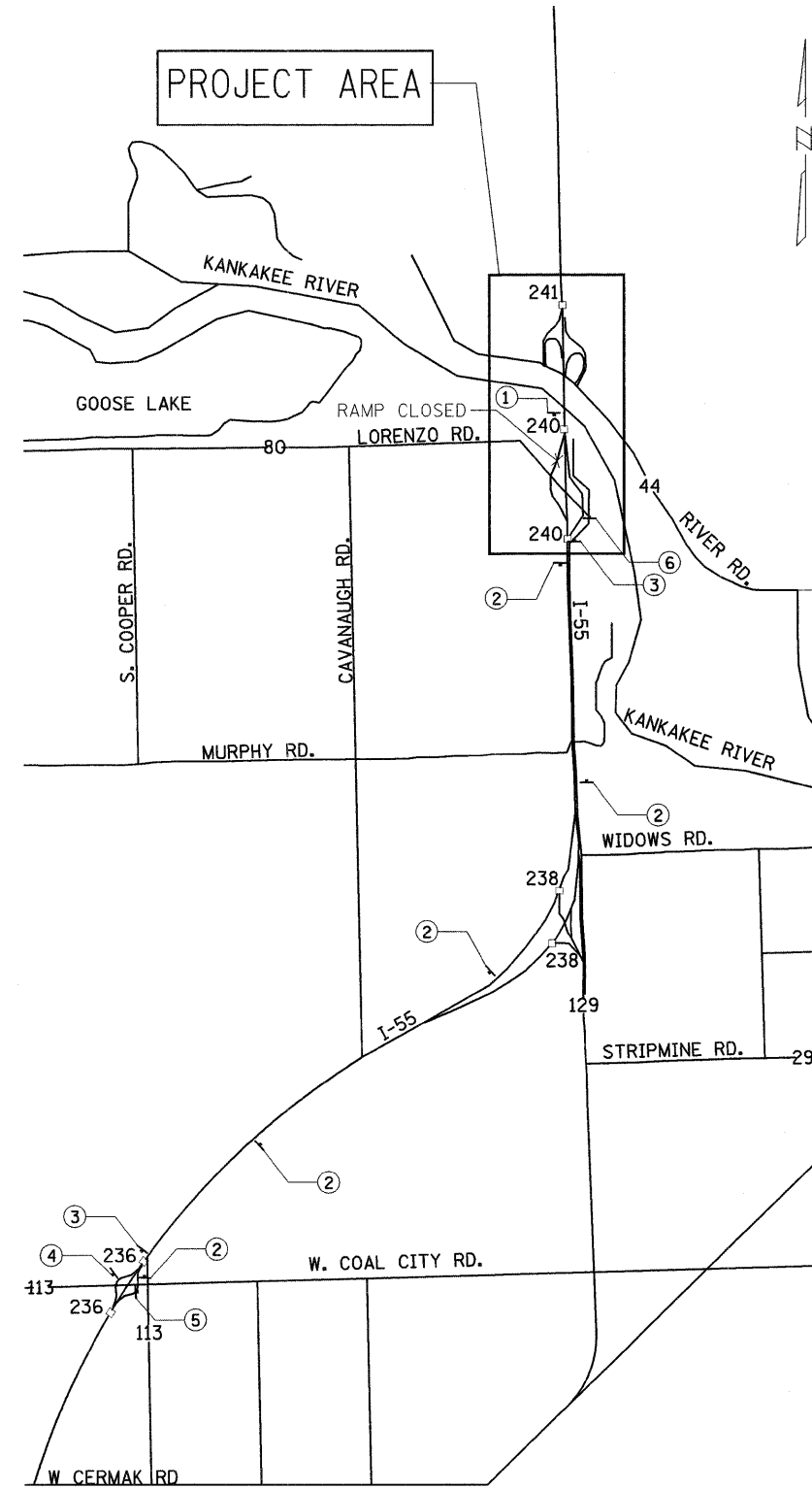


**STAGE II DETOUR SIGNING**

- ① W20-2(O)-48  
W/TYPE 2 FLASHING LIGHT
- ② M3-1  
M1-1  
DETOUR M4-8(O) 3015  
 M6-1(O) 3018
- ③ M3-1  
M1-1  
DETOUR M4-8(O) 3015  
 M6-3(O) 3018
- ④ M3-1  
M1-1  
DETOUR M4-8(O) 3015  
 M6-2R(O) 3018
- ⑤ M3-1  
M1-1  
DETOUR M4-8(O) 3015  
 M5-1L(O) 3018
- ⑥ M3-1  
M1-1  
DETOUR M4-8(O) 3015  
 M6-1(O) 3018
- ⑦ M4-6(O) 3015  
DETOUR M4-8(O) 3015

SIGN

NOTE: REFER TO DISTRICT 1 DETAIL "FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS" FOR ADDITIONAL SIGNAGE.

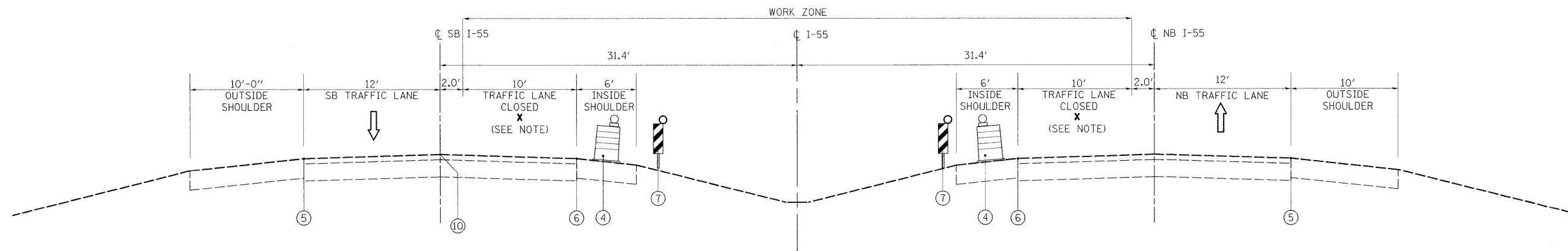


**STAGE III DETOUR SIGNING**

- ① W20-2(O)-48  
W/TYPE 2 FLASHING LIGHT
- ② LORENZO ROAD  
DETOUR M4-8(O) 3015  
 M6-3(O) 3018
- ③ LORENZO ROAD  
DETOUR M4-8(O) 3015  
 M6-2R(O) 3018
- ④ LORENZO ROAD  
DETOUR M4-8(O) 3015  
 M5-1L(O) 3018
- ⑤ LORENZO ROAD  
DETOUR M4-8(O) 3015  
 M6-1(O) 3018
- ⑥ END M4-6(O) 3015  
DETOUR M4-8(O) 3015

|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

| F.A.J. RTE.   | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------------|--------|--------------|-----------|
| 55  | 88 (B&B-1) BR | WILL   | 72           | 9         |
| CONTRACT NO. 62930                                  |               |        |              |           |
| FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT |               |        |              |           |



**TYPICAL SECTION - STAGE I B CROSSOVER CONSTRUCTION**

(SECTION LOOKING NORTH)  
**STA 33+50.00 TO 59+97.50**  
**STA 73+00.00 TO 87+50.00**

**BRIDGE OMMISION**  
**STA 59+97.50 TO 73+00.00**

NOTE: INSIDE LANE CLOSURES FOR STAGE I PERMITTED DURING ALLOWABLE HOURS PER TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).

**STAGE I (WORK IN MEDIAN)**

1. CLOSE INSIDE LANES FOR NB AND SB TRAFFIC USING FREEWAY STANDARD 701401-05: LANE CLOSURE, FREEWAY/EXPRESSWAY.
2. PERFORM EXCAVATION AND CONSTRUCT EMBANKMENT.
3. INSTALL CULVERTS AND DRAINAGE STRUCTURES.
5. CONSTRUCT CROSSOVER PAVEMENT AND SHOULDER IMPROVEMENTS.
6. CLOSE CROSSOVER TO TRAFFIC USING TEMPORARY MOVEABLE BARRIER WALL.

**STAGE I (WORK ON SHOULDERS)**

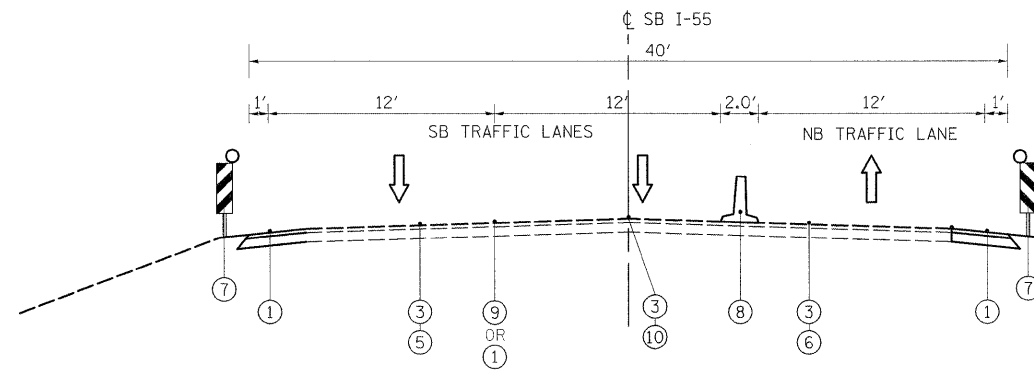
1. CLOSE OUTSIDE LANES FOR NB & SB TRAFFIC USING DISTRICT ONE FREEWAY STANDARD 701101: OFF ROAD OPERATIONS, MULTILANE.
2. REPAIR NB AND SB OUTSIDE SHOULDER.
3. TEMPORARY STRIPE PAVEMENT.

**LEGEND**

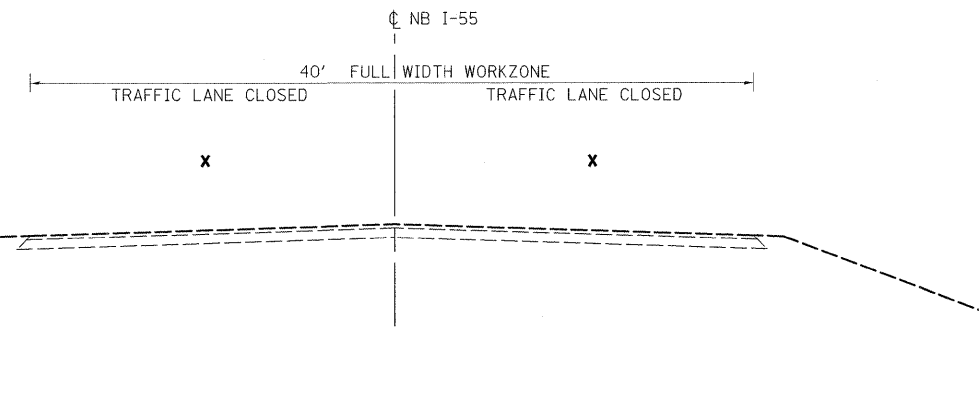
- ① WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
- ② WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
- ③ PAVEMENT MARKING REMOVAL
- ④ DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
- ⑤ EXISTING PAVEMENT MARKING, WHITE EDGE LINE
- ⑥ EXISTING PAVEMENT MARKING, YELLOW EDGE LINE
- ⑦ VERTICAL PANELS (BACK TO BACK)
- ⑧ TEMPORARY CONCRETE BARRIER
- ⑨ WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE SOLID CENTER LINE
- ⑩ EXISTING PAVEMENT MARKING, WHITE LANE LINE (10' LINE, 30' SKIP)

|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

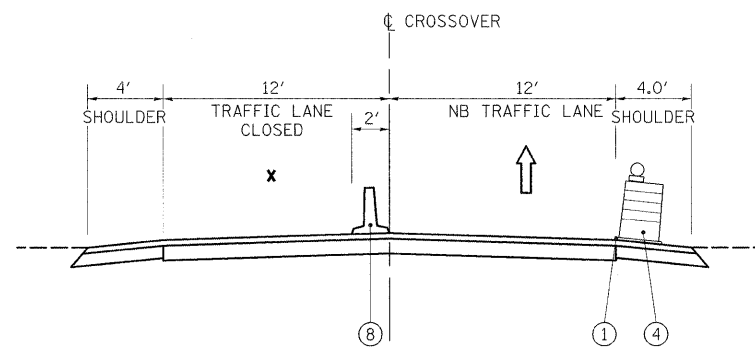
|   |               |        |                    |           |
|---|---------------|--------|--------------------|-----------|
| F.A.J. RTE.                                     | SECTION       | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 55  | 88 (B&B-1) BR | WILL   | 72                 | 10        |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |               |        | CONTRACT NO. 62930 |           |



**TYPICAL SECTION - STAGE II**  
**SOUTH BOUND**  
**(SECTION LOOKING NORTH)**  
**STA. 42+50 TO STA. 76+50**



**TYPICAL SECTION - STAGE II**  
**(SECTION LOOKING NORTH)**  
**STA. 42+50 TO STA. 76+50**



**CROSSOVER TYPICAL SECTION - STAGE II**  
**SOUTH BOUND PEAK**  
**(SECTION LOOKING NORTH)**  
**STA. 33+50 TO STA. 42+50**  
**STA. 76+50 TO STA. 87+50**

**STAGE II (FOR WORK AT NB I-55 BRIDGE)**

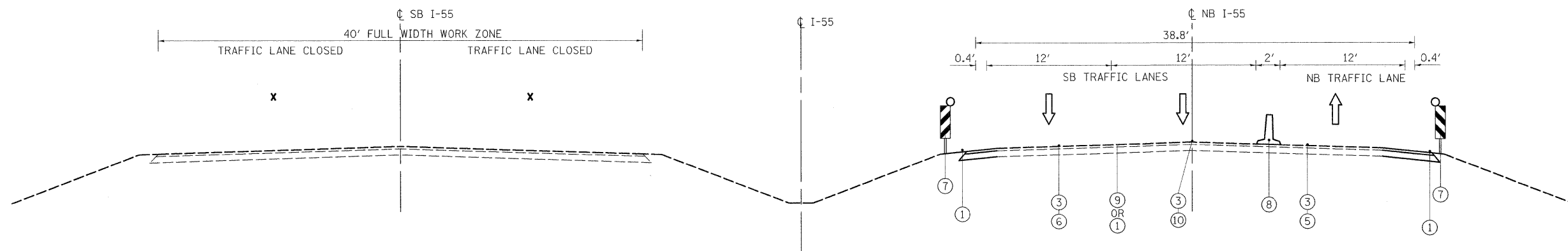
1. USING DAY LANE CLOSURE, PATCH AND REPAIR ANY NEEDED AREAS OF SOUTHBOUND BRIDGE DECK OR AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AS BITUMINOUS CONCRETE REMOVAL (DECK) (1 1/2 INCH), BITUMINOUS MATERIALS (PRIME COAT) AND INCIDENTAL BITUMINOUS SURFACING.
2. SHIFT TRAFFIC ONTO SOUTHBOUND PAVEMENT IN ACCORDANCE WITH TRAFFIC PLAN AND DISTRICT ONE STANDARD TC-09 "FREEWAY SINGLE AND MULTILANE WEAVE".
3. PERFORM STRUCTURAL REHABILITATION ON NB 1-55 BRIDGE.
4. LANDSCAPE RESTORATION.
5. TEMPORARY STRIPE PAVEMENT.

**LEGEND**

- ① WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
- ② WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
- ③ PAVEMENT MARKING REMOVAL
- ④ DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
- ⑤ EXISTING PAVEMENT MARKING, WHITE EDGE LINE
- ⑥ EXISTING PAVEMENT MARKING, YELLOW EDGE LINE
- ⑦ VERTICAL PANELS (BACK TO BACK)
- ⑧ TEMPORARY CONCRETE BARRIER
- ⑨ WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE (10' LINE, 30' SPACE) LINE
- ⑩ EXISTING PAVEMENT MARKING, WHITE LANE LINE (10' LINE, 30' SKIP)

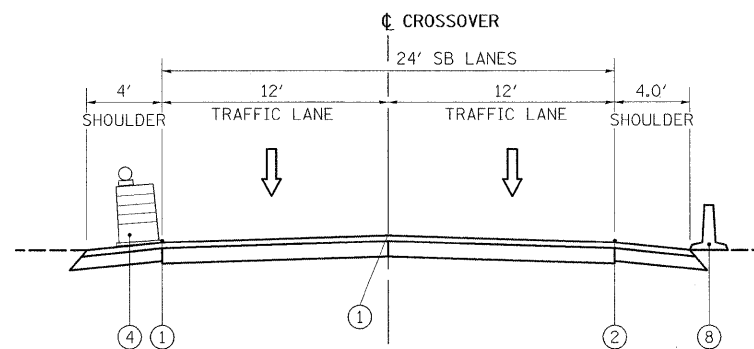
|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

| F.A.J. RTE.                                     | SECTION       | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------------|--------|--------------------|-----------|
| 55  | 88 (B&B-1) BR | WILL   | 72                 | 11        |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |               |        | CONTRACT NO. 62930 |           |



**TYPICAL SECTION – STAGE III REHABILITATION**  
 (SECTION LOOKING NORTH)  
 STA. 42+50 TO STA. 76+50

**TYPICAL SECTION – STAGE III REHABILITATION**  
 (SECTION LOOKING NORTH)  
 STA. 42+50 TO STA. 76+50



**CROSSOVER TYPICAL SECTION – STAGE III REHABILITATION**  
 (SECTION LOOKING NORTH)  
 STA. 33+50 TO STA. 43+50  
 STA. 76+50 TO STA. 90+70

**STAGE III (FOR WORK AT SB I-55 BRIDGE)**

1. SHIFT SB TRAFFIC ONTO NB PAVEMENT IN ACCORDANCE WITH TRAFFIC PLAN AND DISTRICT ONE STANDARD TC-09 "FREEWAY SINGLE AND MULTILANE WEAVE".
2. PERFORM STRUCTURAL REHABILITATION WORK ON SB I-55 BRIDGE.
3. AFTER COMPLETION OF BRIDGE WORK, REMOVE TRAFFIC CONTROL DEVICES, RESTORE PAVEMENT MARKINGS, LANDSCAPING AND BARRIER REFLECTORS AND SHIFT SB TRAFFIC TO SB PAVEMENT, MAINTAINING TWO (2) LANES IN EACH DIRECTION NB & SB.
4. CLOSE INSIDE NB LANE PER STAGE 1 AND RESTORE PAVEMENT MARKINGS.

**LEGEND**

- ① WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
- ② WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
- ③ PAVEMENT MARKING REMOVAL
- ④ DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
- ⑤ EXISTING PAVEMENT MARKING, WHITE EDGE LINE
- ⑥ EXISTING PAVEMENT MARKING, YELLOW EDGE LINE
- ⑦ VERTICAL PANELS (BACK TO BACK)
- ⑧ TEMPORARY CONCRETE BARRIER
- ⑨ WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE (10' LINE, 30' SPACE) LINE
- ⑩ EXISTING PAVEMENT MARKING, WHITE LANE LINE (10' LINE, 30' SKIP)

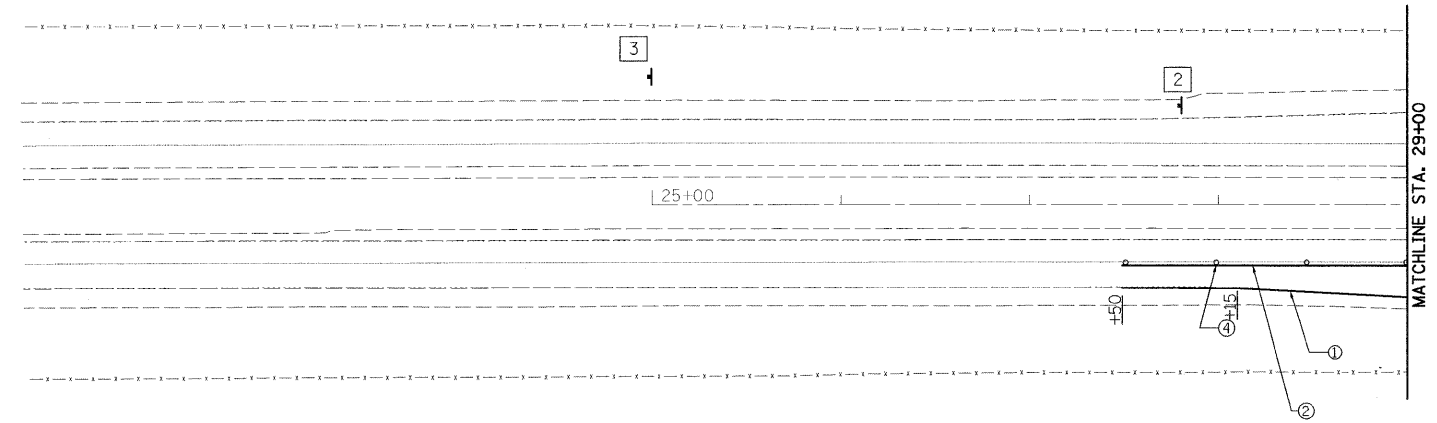
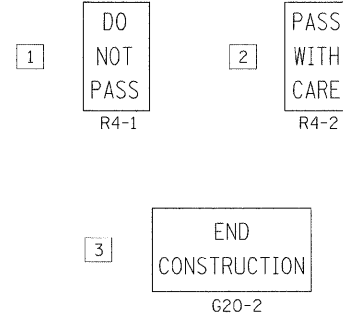




**LEGEND**

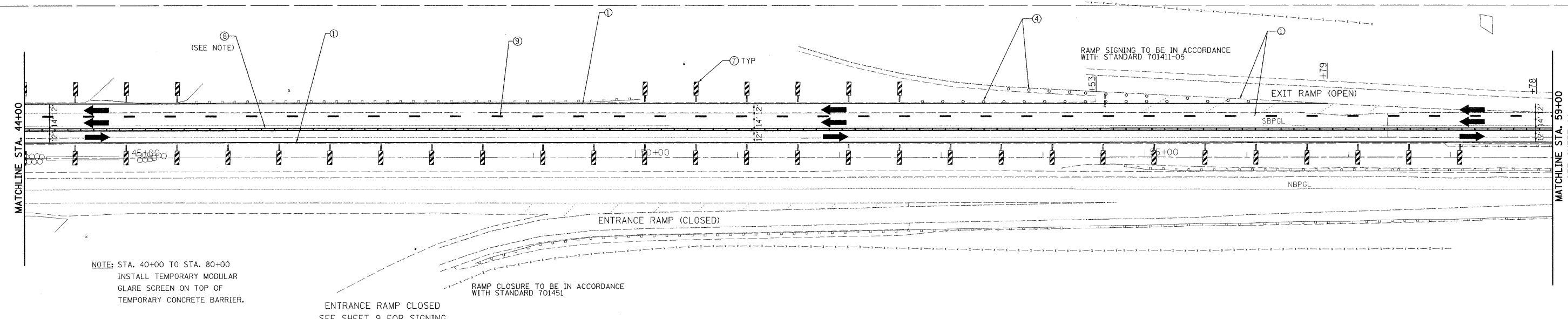
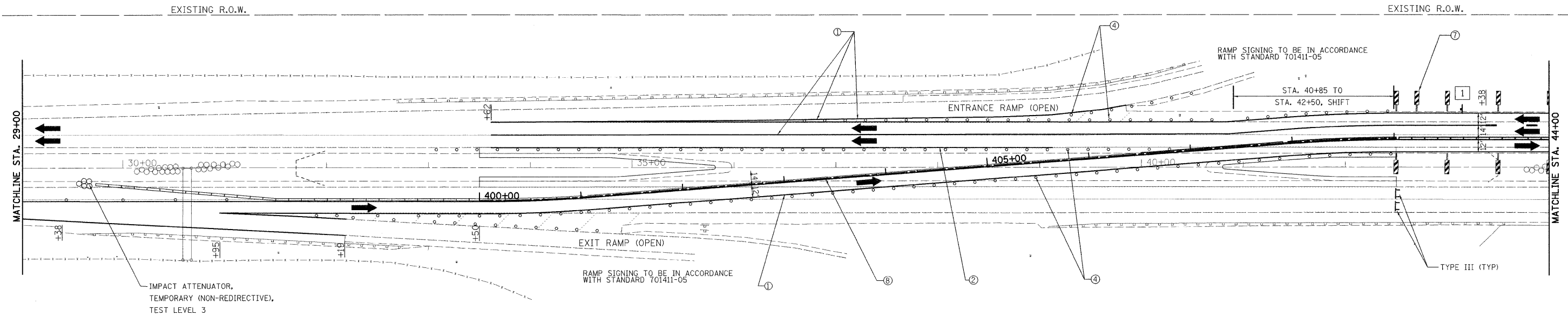
- ① WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE EDGE LINE
- ② WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" YELLOW EDGE LINE
- ③ NOT USED
- ④ DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHT
- ⑤ NOT USED
- ⑥ NOT USED
- ⑦ VERTICAL PANELS (BACK TO BACK)
- ⑧ TEMPORARY CONCRETE BARRIER
- ⑨ WET REFLECTIVE TEMPORARY TAPE TYPE III, 4" WHITE (10' LINE, 30' SPACE) LINE

- TEMPORARY CONCRETE BARRIER
- TEMPORARY CONCRETE BARRIER WITH MODULAR GLARE SCREEN



ADVANCED LANE CLOSURE SIGNING TO BE IN ACCORDANCE WITH STANDARD 701400-03

LANE CLOSURE TRAFFIC CONTROL (INCLUDING SIGNING) TO BE IN ACCORDANCE WITH STANDARD 701416-06



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 200 West Front Street  
 Wheaton, IL 60187

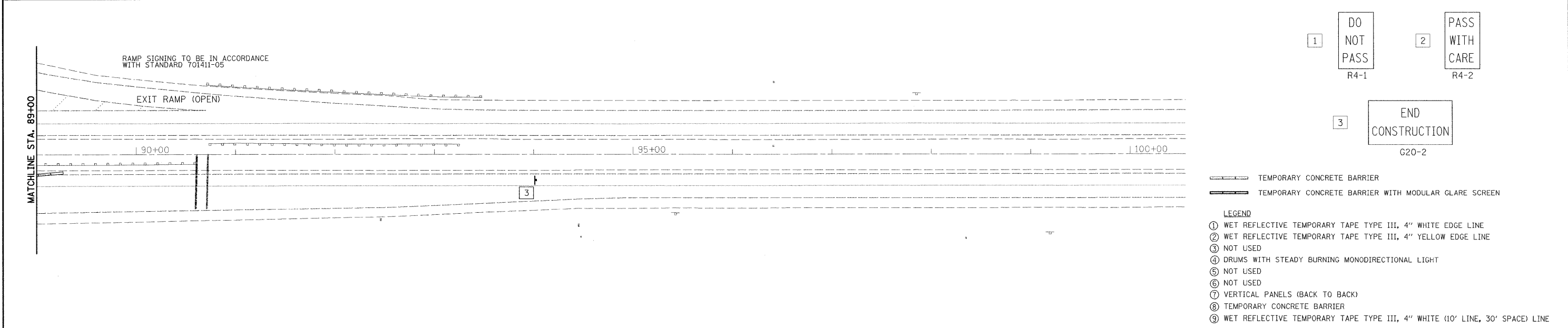
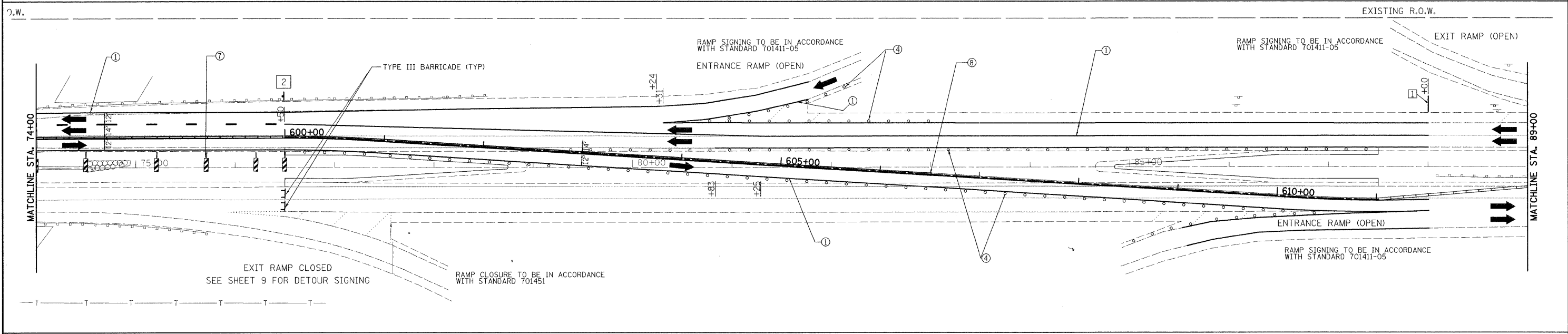
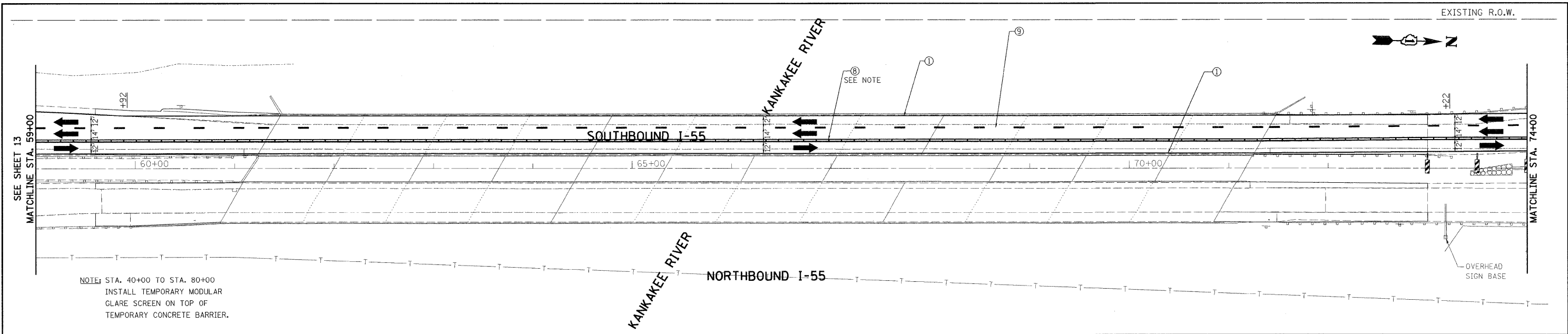
|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**IL-55 OVER THE KANKAKEE RIVER  
 TRAFFIC CONTROL PLAN STAGE II**

SCALE: 1" = 50'    SHEET NO. 1 OF 2 SHEETS    STA. 29+00.00 TO STA. 59+00.00

|   |                       |             |                 |              |
|---|-----------------------|-------------|-----------------|--------------|
| F.A.I. RTE. 55                                  | SECTION 88 (B&B-1) BR | COUNTY WILL | TOTAL SHEETS 72 | SHEET NO. 13 |
| CONTRACT NO. 62930                              |                       |             |                 |              |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                       |             |                 |              |



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 200 West Front Street  
 Wheaton, IL 60187  
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|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

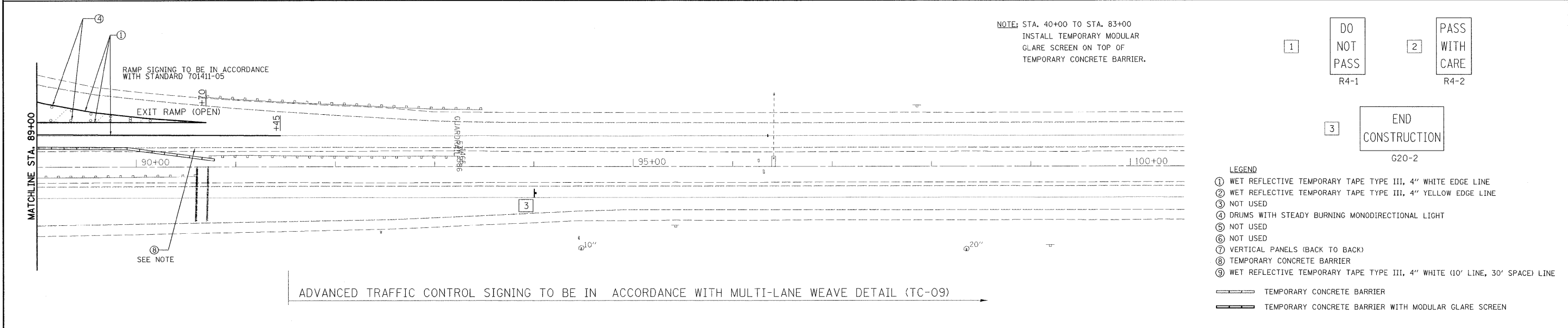
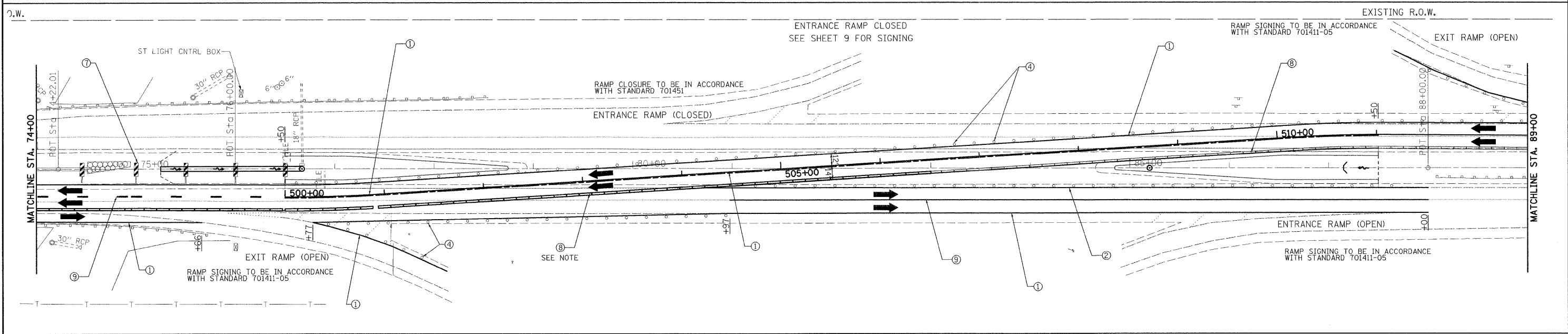
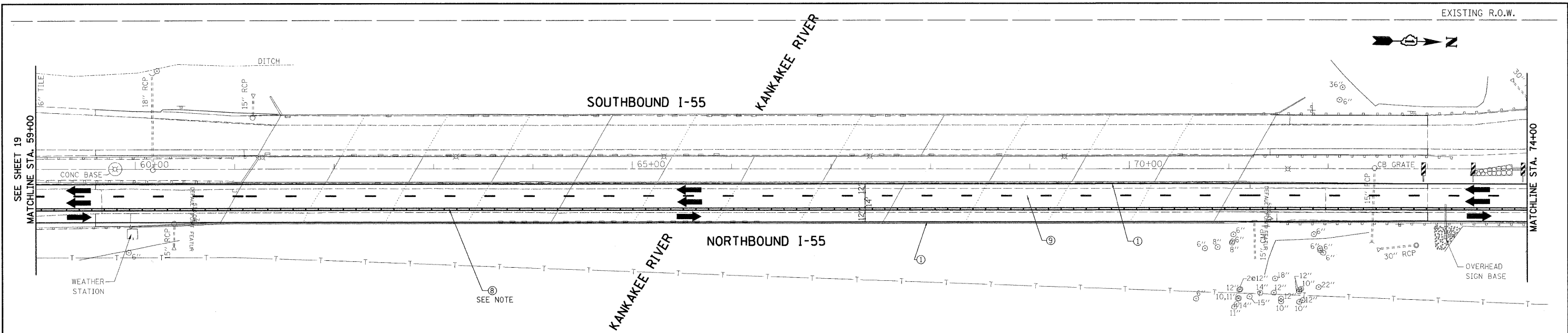
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-55 OVER THE KANKAKEE RIVER  
 TRAFFIC CONTROL PLAN STAGE II**

SCALE: 1" = 50'    SHEET NO. 2 OF 2 SHEETS    STA. 59+00.00 TO STA. 89+00.00

|   |                       |             |                    |              |
|---|-----------------------|-------------|--------------------|--------------|
| F.A.I. RTE. 55                                  | SECTION 88 (B&B-1) BR | COUNTY WILL | TOTAL SHEETS 72    | SHEET NO. 14 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                       |             | CONTRACT NO. 62930 |              |





200 West Front Street  
Wheaton, IL 60187

|            |          |           |  |
|------------|----------|-----------|--|
| DESIGNED - | KSD      | REVISED - |  |
| DRAWN -    | KSD      | REVISED - |  |
| CHECKED -  | CMJ      | REVISED - |  |
| DATE -     | 03-04-09 | REVISED - |  |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

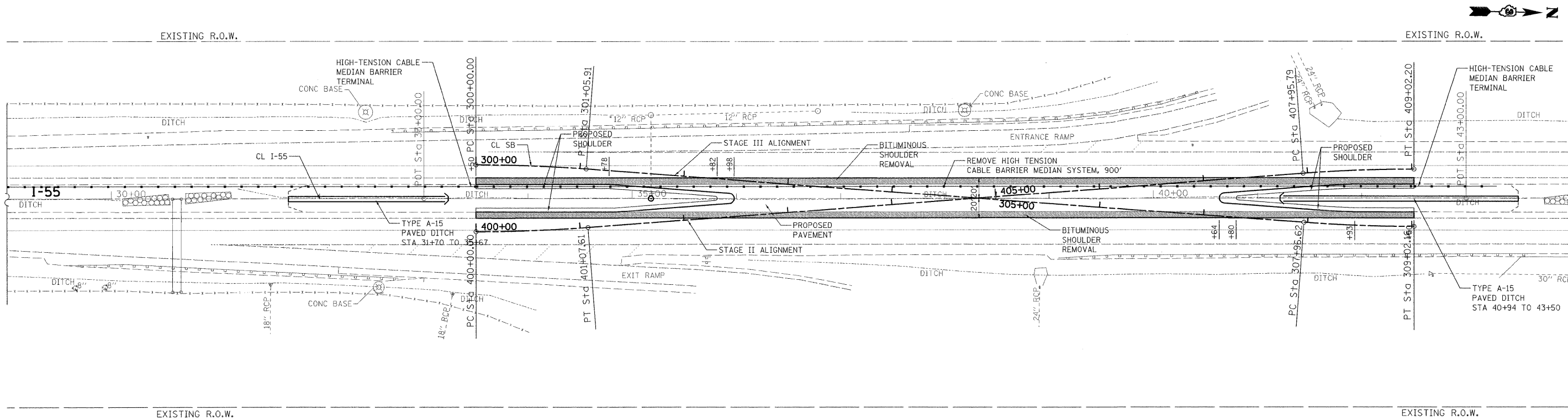
I-55 OVER THE KANKAKEE RIVER  
TRAFFIC CONTROL PLAN STAGE III

SCALE: 1" = 50'

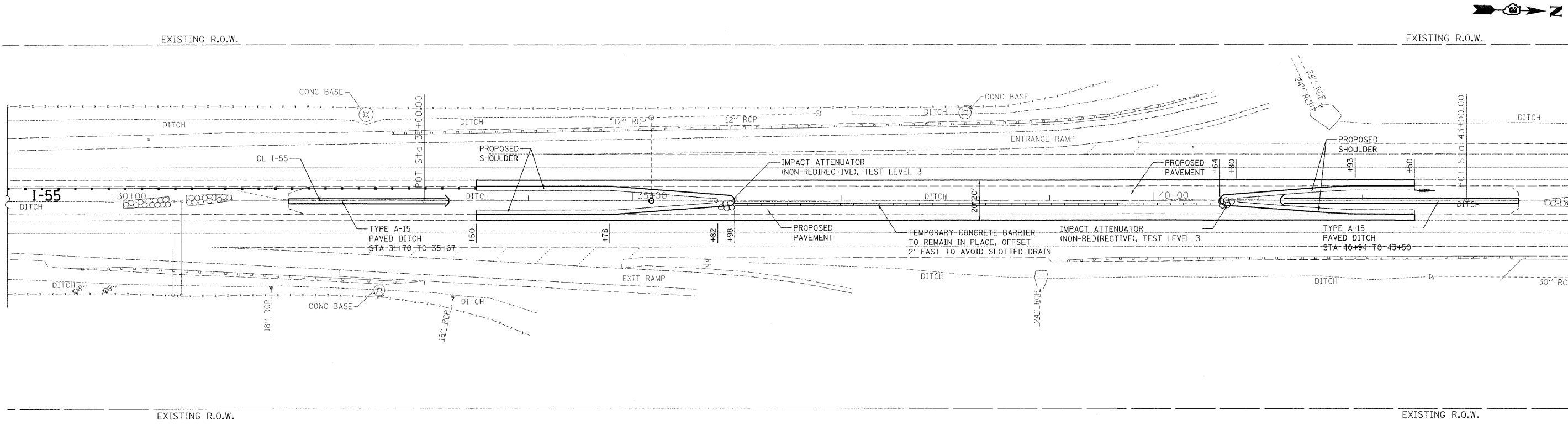
SHEET NO. 2 OF 2 SHEETS

STA. 59+00.00 TO STA. 89+00.00

| F.A.I. RTE.                                       | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------------|--------|--------------|-----------|
| 55  | 88 (B&B-1) BR | WILL   | 72           | 16        |
| CONTRACT NO. 62930                                |               |        |              |           |
| FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT |               |        |              |           |



**DURING CONSTRUCTION**



**PERMANENT CONDITION**

NOTE: STATION CALLOUTS ARE RELATIVE TO EXISTING BASELINE

PLOT DATE = 6/25/2009  
 FILE NAME = K:\1182518\ST LOUISA\86-18-09\118 Current\12 Sheets\B2E1B\_Crossover\_2Plan.dgn  
 USER NAME = jlscale  
 MODEL = Default



200 West Front Street  
Wheaton, IL 60187

|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SOUTH CROSSOVER  
PLAN**

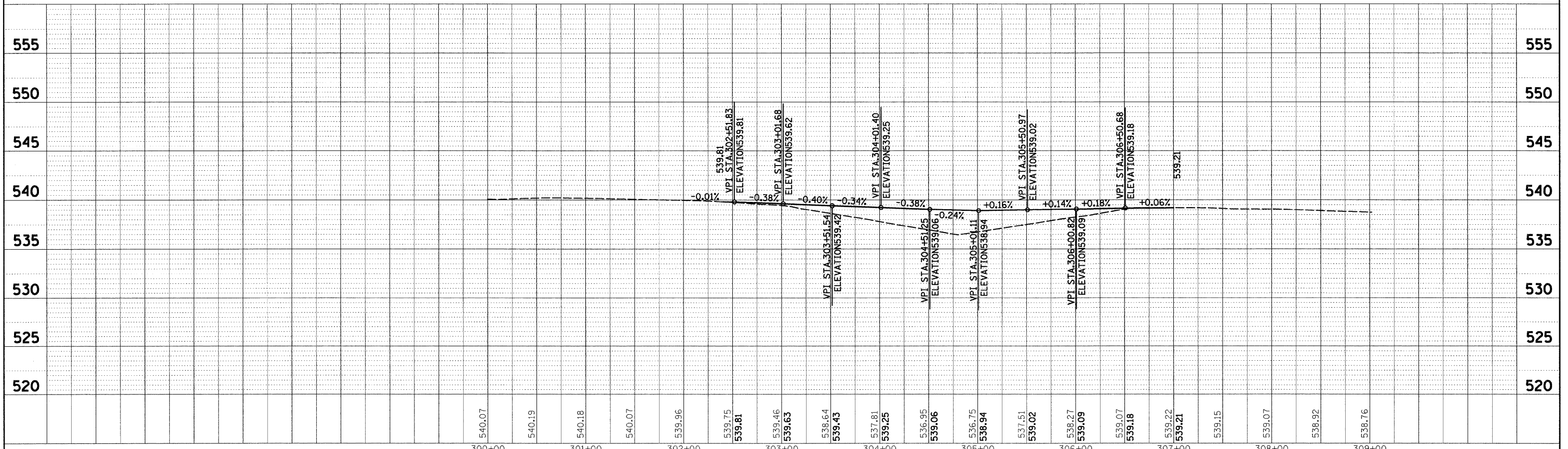
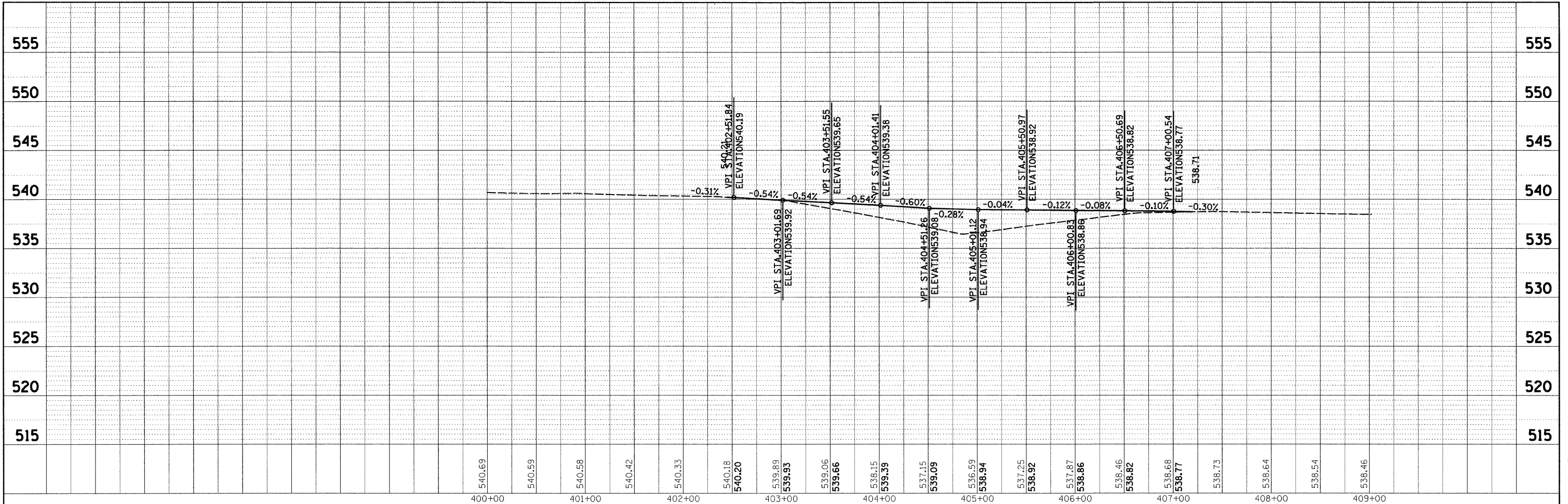
SCALE: 1" = 50' SHEET NO. 1 OF 4 SHEETS STA. 29+00.00 TO STA. 44+00.00

| F.A.I. RTE.        | SECTION       | COUNTY | TOTAL SHEETS                                    | SHEET NO. |
|--------------------|---------------|--------|---|-----------|
| 55                 | 88 (B&B-1) BR | WILL   | 72  | 17        |
| CONTRACT NO. 62930 |               |        | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |           |



PLAN  
 SURVEYED BY DATE  
 ALIGNED CHECKED  
 RT. OF WAY CHECKED  
 NO. ROAD FILE NAME

PROFILE  
 SURVEYED BY DATE  
 GRADES CHECKED  
 I.M. NOTED  
 NO. STRUCTURE NOTATIONS CHKD



200 West Front Street  
 Wheaton, IL 60187

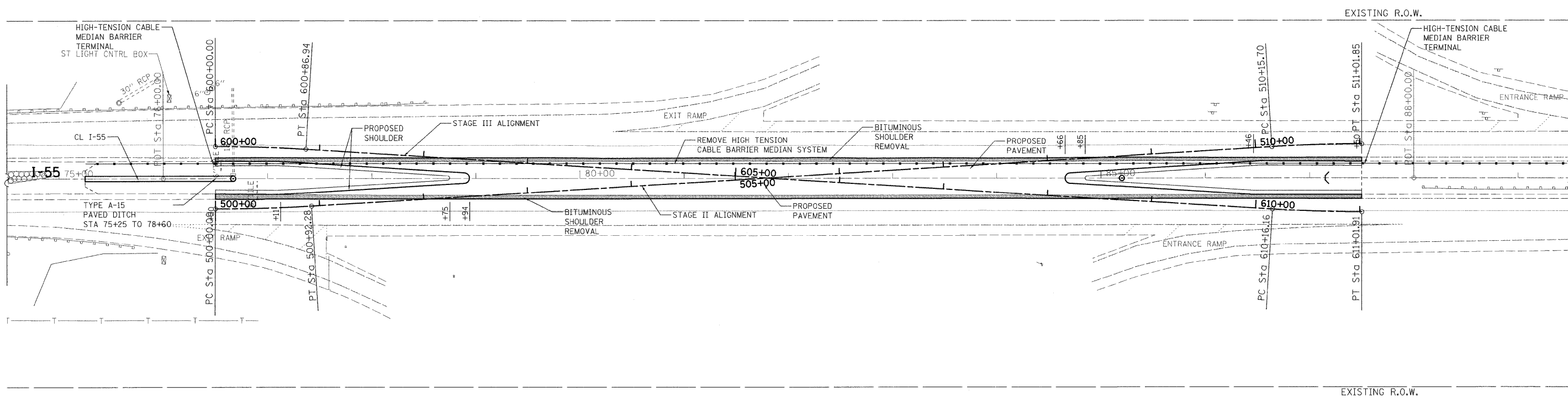
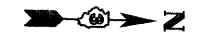
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|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN -    | REVISED - |
| CHECKED -  | REVISED - |
| DATE -     | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

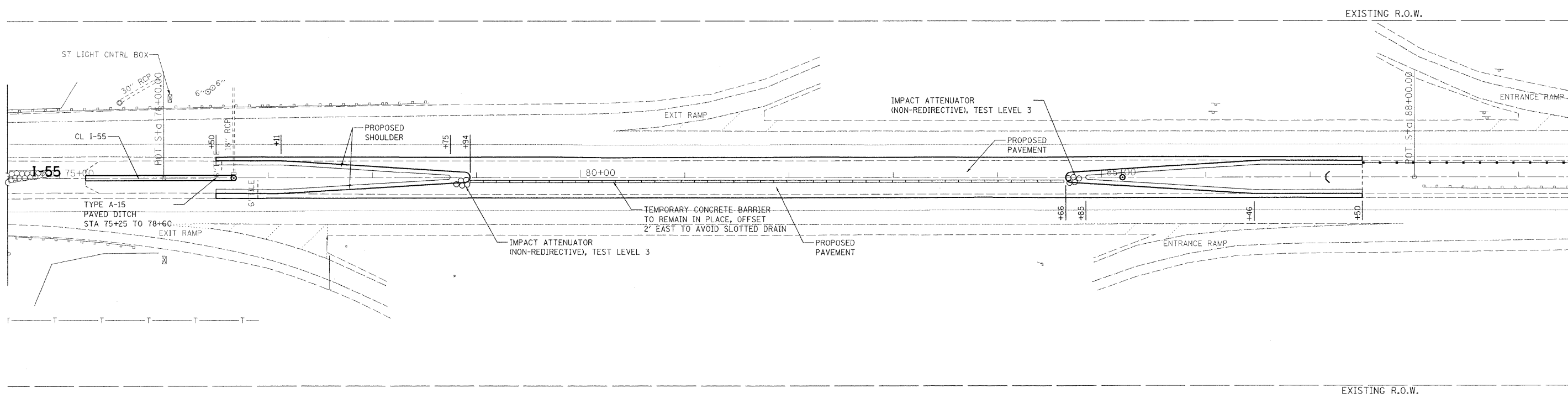
SOUTH CROSSOVER  
 PROFILE

SCALE: 1" = 50' SHEET NO. 3 OF 4 SHEETS STA. 29+00.00 TO STA. 44+00.00

|                           |               |        |                    |           |
|---------------------------|---------------|--------|--------------------|-----------|
| F.A.I. RTE.               | SECTION       | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 55                        | BB (B&B-1) BR | WILL   | 72                 | 18        |
| FED. ROAD DIST. NO.       |               |        | CONTRACT NO. 62930 |           |
| ILLINOIS FED. AID PROJECT |               |        |                    |           |



**DURING CONSTRUCTION**



**PERMANENT CONDITION**

NOTE: STATION CALLOUTS ARE RELATIVE TO EXISTING BASELINE

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200 West Front Street  
Wheaton, IL 60187

|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

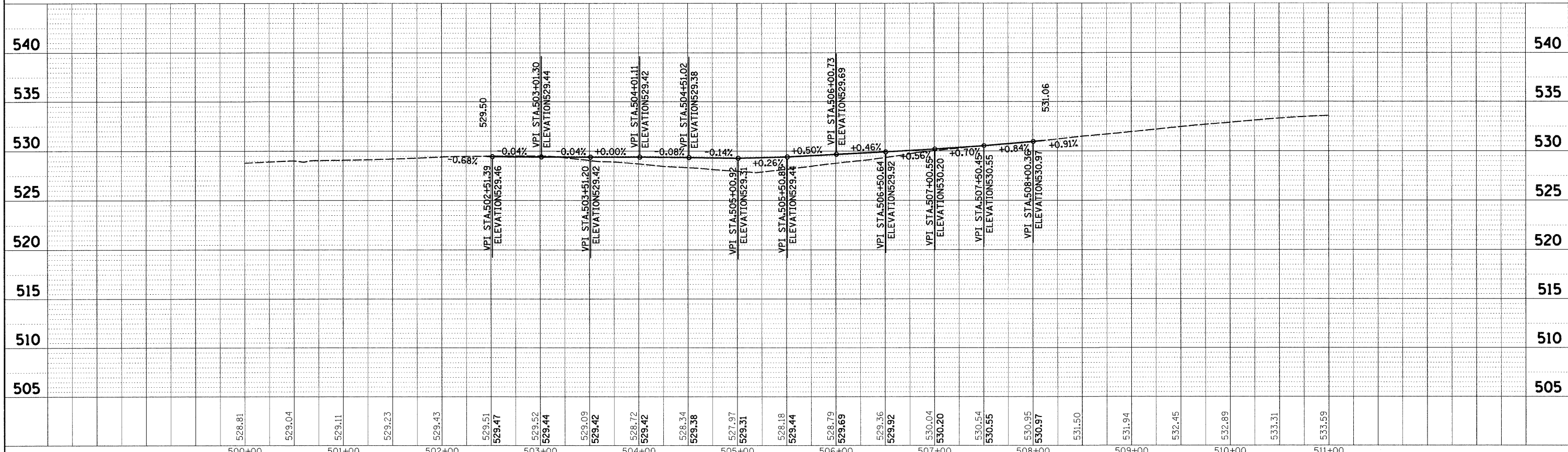
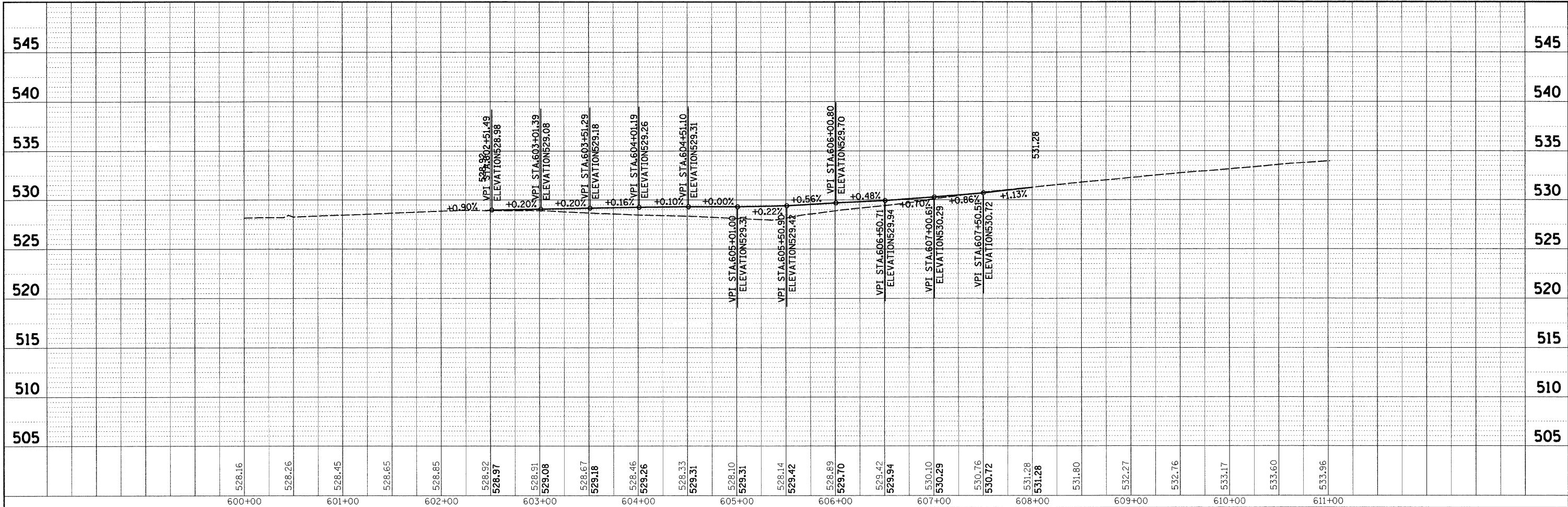
**NORTH CROSSOVER  
PLAN**

SCALE: 1" = 50' SHEET NO. 2 OF 4 SHEETS STA. 29+00.00 TO STA. 44+00.00

| F.A.I. RTE.                                     | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------------|--------|--------------|-----------|
| 55  | 88 (B&B-1) BR | WILL   | 72           | 19        |
| CONTRACT NO. 62930                              |               |        |              |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |               |        |              |           |

PLAN SURVEYED BY DATE  
 ALIGNED CHECKED  
 RT. OF WAY CHECKED  
 NO. DDD FILE NAME

PROFILE SURVEYED BY DATE  
 GRADES CHECKED  
 (B.M. NOTED)  
 NO. STRUCTURE NOTATIONS CHFD



200 West Front Street  
 Wheaton, IL 60187

|            |           |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN -    | REVISED - |
| CHECKED -  | REVISED - |
| DATE -     | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

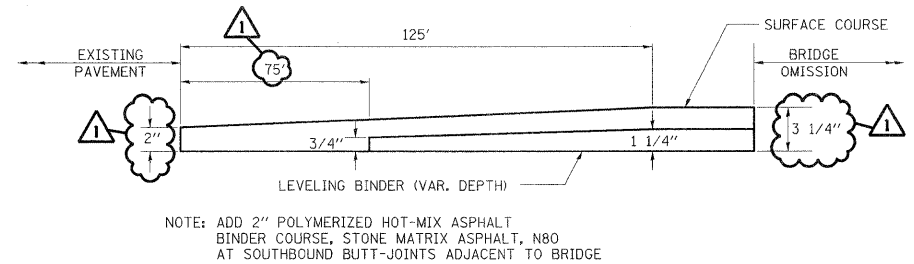
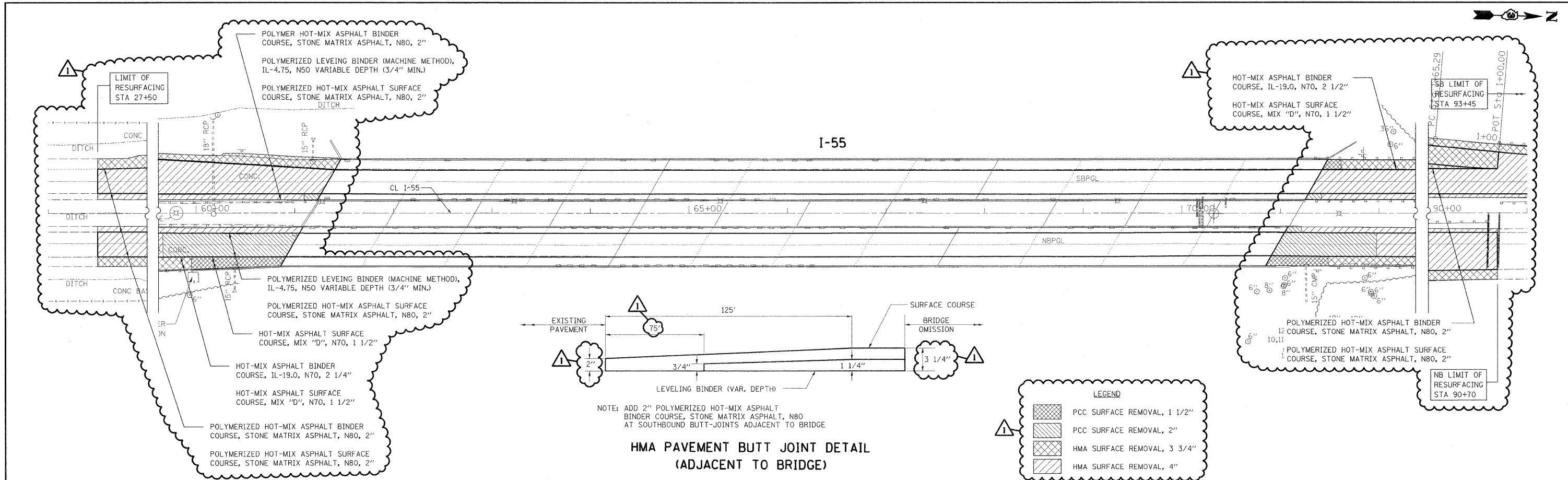
NORTH CROSSOVER  
 PROFILE

SCALE: 1" = 50' SHEET NO. 4 OF 4 SHEETS STA. 29+00.00 TO STA. 44+00.00

|   |                       |             |                 |              |
|---|-----------------------|-------------|-----------------|--------------|
| F.A.I. RTE. 55                                | SECTION BB (B&B-1) BR | COUNTY WILL | TOTAL SHEETS 72 | SHEET NO. 20 |
| CONTRACT NO. 62930                            |                       |             |                 |              |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                       |             |                 |              |

PLAN SURVEYED BY DATE  
 ALIGNED CHECKED BY  
 RT. OF WAY CHECKED BY  
 NO. FILE NAME

PROFILE SURVEYED BY DATE  
 PLOTTED CHECKED BY  
 B.A. NOTED BY  
 STRUCTURE NOTATION'S OK'D

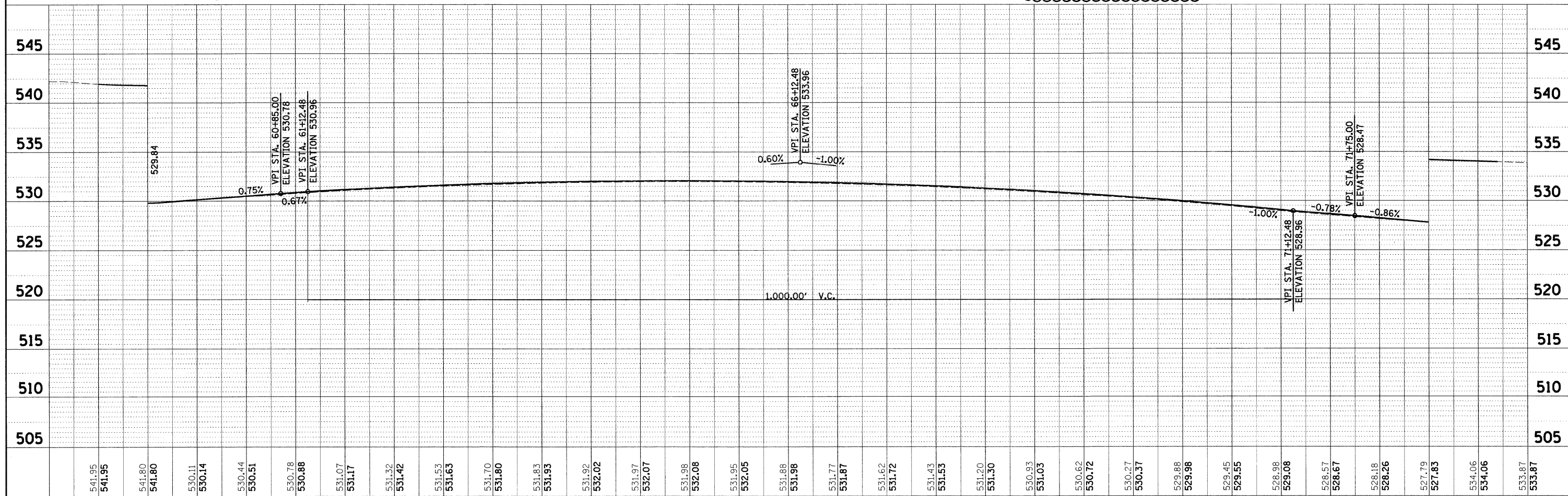


NOTE: ADD 2" POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80 AT SOUTHBOUND BUTT-JOINTS ADJACENT TO BRIDGE

**HMA PAVEMENT BUTT JOINT DETAIL  
 (ADJACENT TO BRIDGE)**

**LEGEND**

- PCC SURFACE REMOVAL, 1 1/2"
- PCC SURFACE REMOVAL, 2"
- HMA SURFACE REMOVAL, 3 3/4"
- HMA SURFACE REMOVAL, 4"



200 West Front Street  
 Wheaton, IL 60187

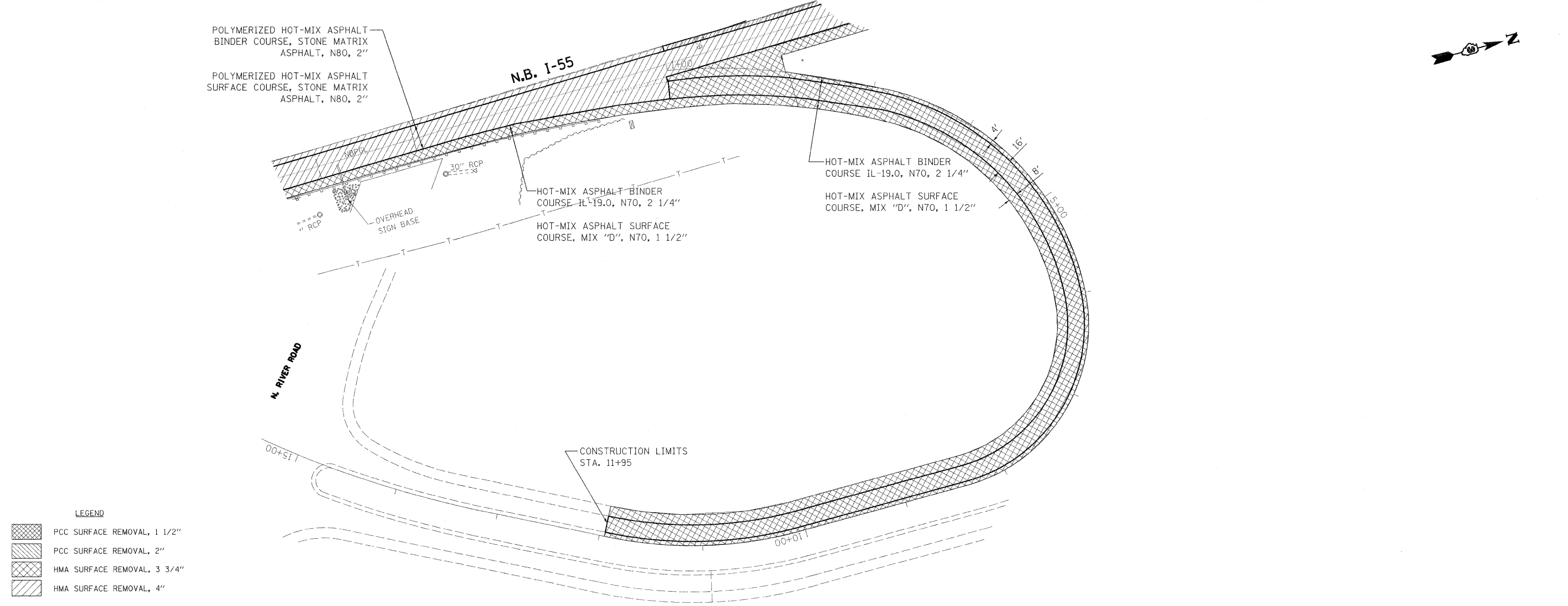
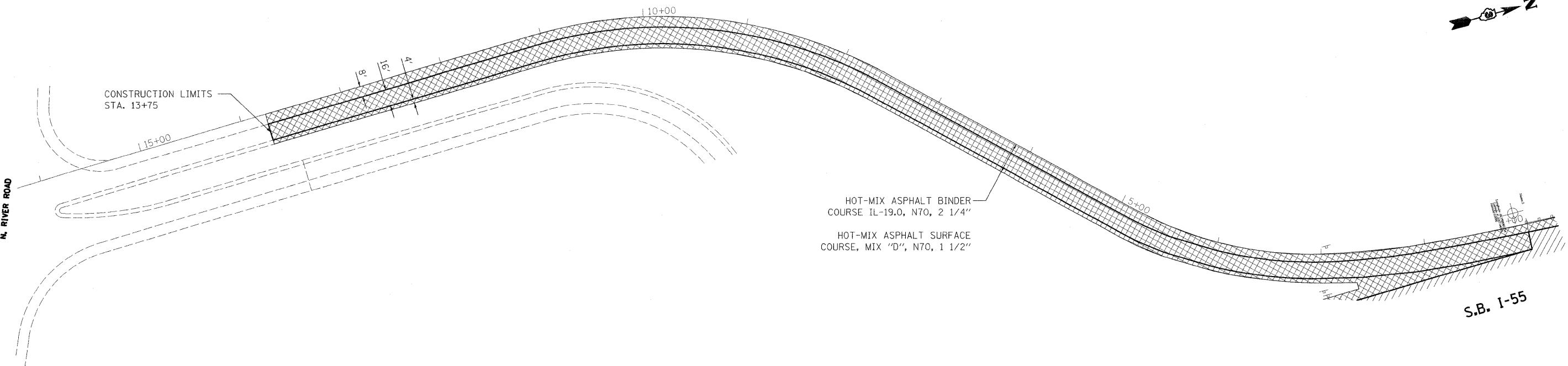
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|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN -    | REVISED - |
| CHECKED -  | REVISED - |
| DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-55 OVER THE KANKAKEE RIVER  
 BRIDGE PLAN & PROFILE**

SCALE: 1" = 50' SHEET NO. X OF 5 SHEETS STA. 59+00.00 TO STA. 74+00.00

|   |                       |             |                    |              |
|---|-----------------------|-------------|--------------------|--------------|
| F.A.I. RTE. 55                                | SECTION BB (B&B-1) BR | COUNTY WILL | TOTAL SHEETS 72    | SHEET NO. 21 |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                       |             | CONTRACT NO. 62930 |              |



**LEGEND**

|  |                             |
|--|-----------------------------|
|  | PCC SURFACE REMOVAL, 1 1/2" |
|  | PCC SURFACE REMOVAL, 2"     |
|  | HMA SURFACE REMOVAL, 3 3/4" |
|  | HMA SURFACE REMOVAL, 4"     |

PLOT DATE = 12/23/2009 5:01:00 PM  
 PLOT SCALE = 1/8\"/>



200 West Front Street  
Wheaton, IL 60187

|                 |                      |
|-----------------|----------------------|
| DESIGNED - KSD  | REVISED - ADD. NO. 1 |
| DRAWN - KSD     | REVISED -            |
| CHECKED - CMJ   | REVISED -            |
| DATE - 03-04-09 | REVISED -            |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|   |                         |                                |  |
|---|-------------------------|--------------------------------|--|
| <b>N. RIVER ROAD EXIT RAMP<br/>PLAN</b> |                         |                                |  |
| SCALE: 1" = 50'                         | SHEET NO. 2 OF 4 SHEETS | STA. 29+00.00 TO STA. 44+00.00 |  |

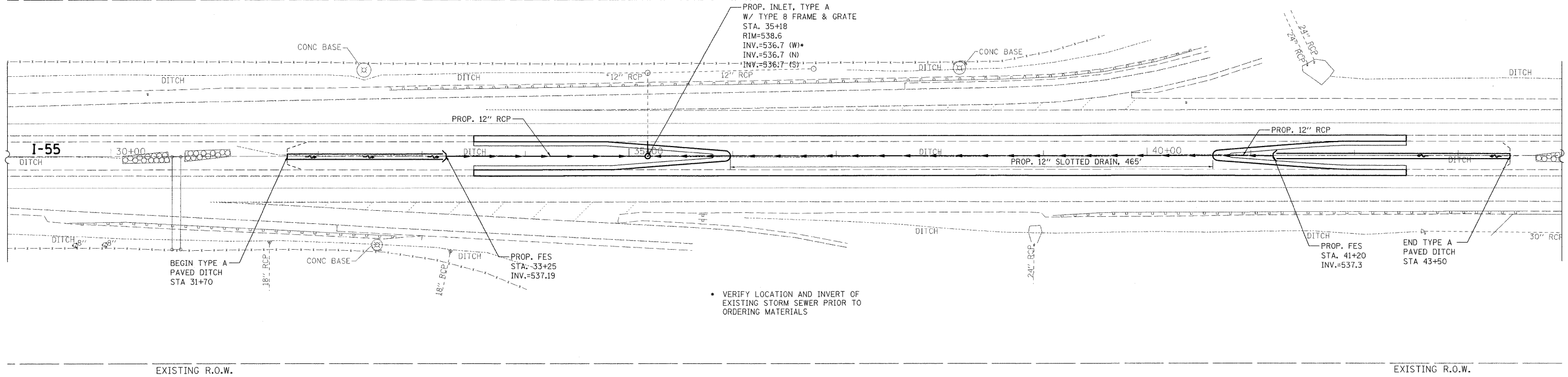
| F.A.I. RTE.                                       | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------------|--------|--------------|-----------|
| 55  | 88 (B&B-1) BR | WILL   | 72           | 21A       |
| CONTRACT NO. 62930                                |               |        |              |           |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT |               |        |              |           |





EXISTING R.O.W.

EXISTING R.O.W.

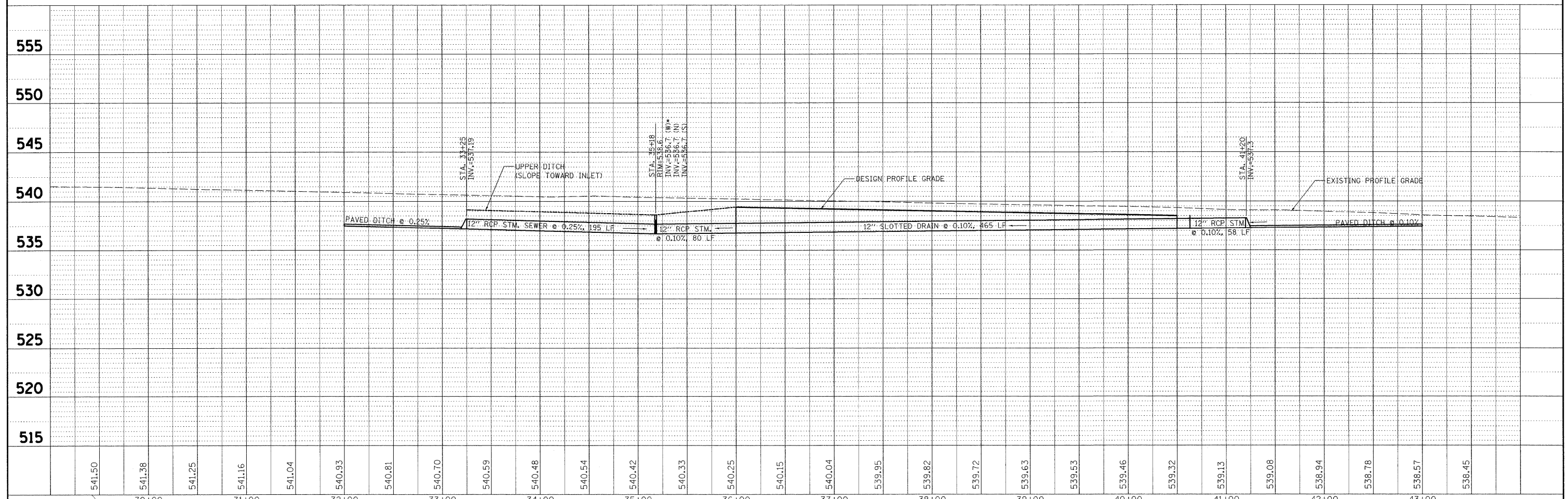


EXISTING R.O.W.

EXISTING R.O.W.

|      |                    |      |
|------|--------------------|------|
| PLAN | SURVEYED           | DATE |
|      | PLOTTED            | BY   |
|      | NOTED              |      |
|      | NO. OF WAY CHECKED |      |
|      | PAID FILE NAME     |      |

|         |                    |      |
|---------|--------------------|------|
| PROFILE | SURVEYED           | DATE |
|         | PLOTTED            | BY   |
|         | NOTED              |      |
|         | NO. OF WAY CHECKED |      |
|         | PAID FILE NAME     |      |



|        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| 541.50 | 541.38 | 541.25 | 541.16 | 541.04 | 540.93 | 540.81 | 540.70 | 540.59 | 540.48 | 540.54 | 540.42 | 540.33 | 540.25 | 540.15 | 540.04 | 539.95 | 539.82 | 539.72 | 539.63 | 539.53 | 539.46 | 539.32 | 539.13 | 539.08 | 538.94 | 538.78 | 538.57 | 538.45 |
| 30+00  | 31+00  | 32+00  | 33+00  | 34+00  | 35+00  | 36+00  | 37+00  | 38+00  | 39+00  | 40+00  | 41+00  | 42+00  | 43+00  |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |



200 West Front Street  
Wheaton, IL 60187

|          |   |         |   |
|----------|---|---------|---|
| DESIGNED | - | REVISED | - |
| DRAWN    | - | REVISED | - |
| CHECKED  | - | REVISED | - |
| DATE     | - | REVISED | - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

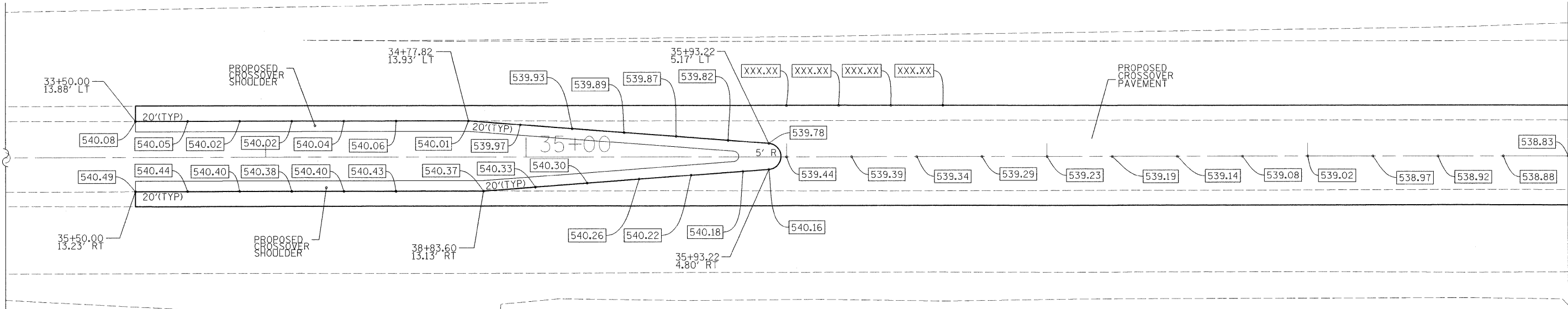
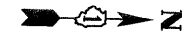
**SOUTH CROSSOVER  
DRAINAGE PLAN & PROFILE**

SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS STA. 29+00.00 TO STA. 44+00.00

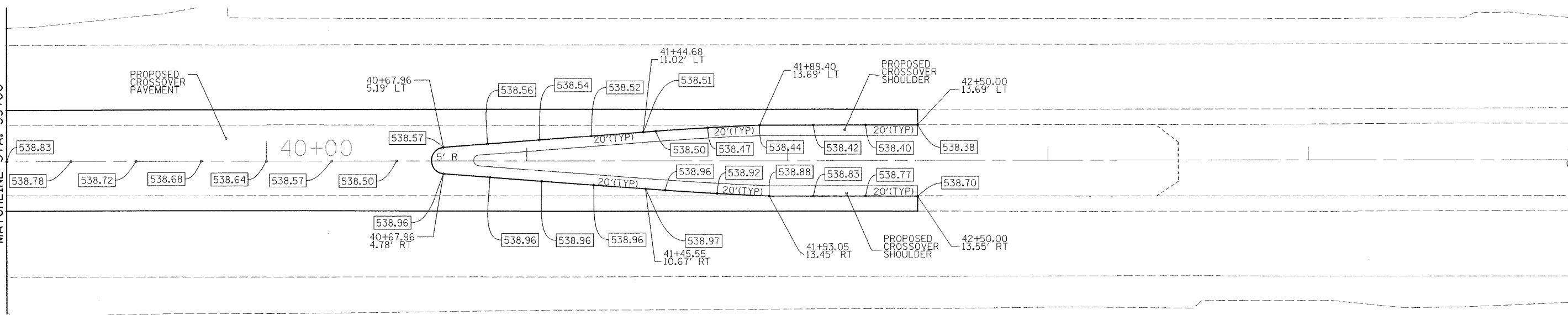
|                     |               |                           |              |           |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.I. RTE.         | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| 55                  | BB (B&B-1) BR | WILL                      | 72           | 22        |
| FED. ROAD DIST. NO. |               | ILLINOIS FED. AID PROJECT |              |           |
| CONTRACT NO. 62930  |               |                           |              |           |







MATCHLINE STA. 39+00



MATCHLINE STA. 39+00

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200 West Front Street  
Wheaton, IL 60187

DESIGNED - KSD  
DRAWN - KSD  
CHECKED - CMJ  
DATE - 03-04-09

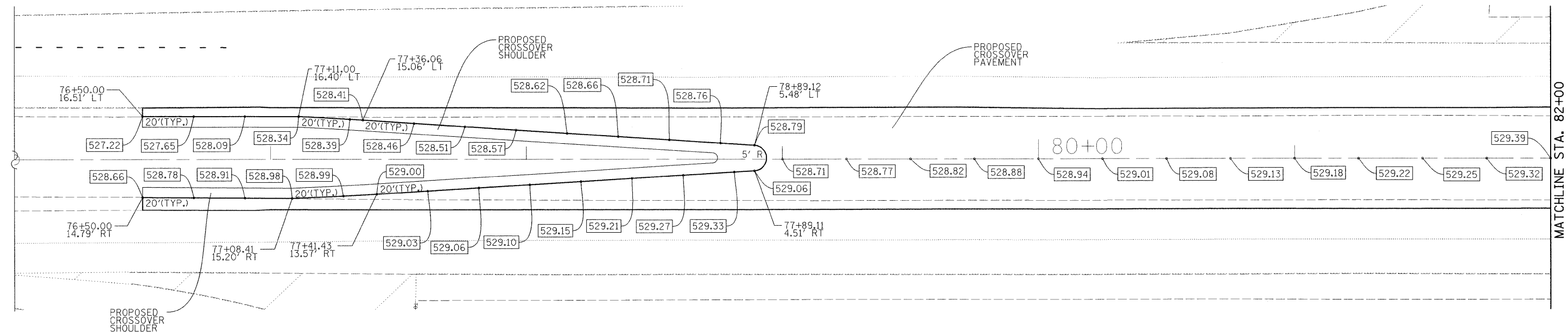
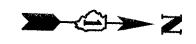
REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

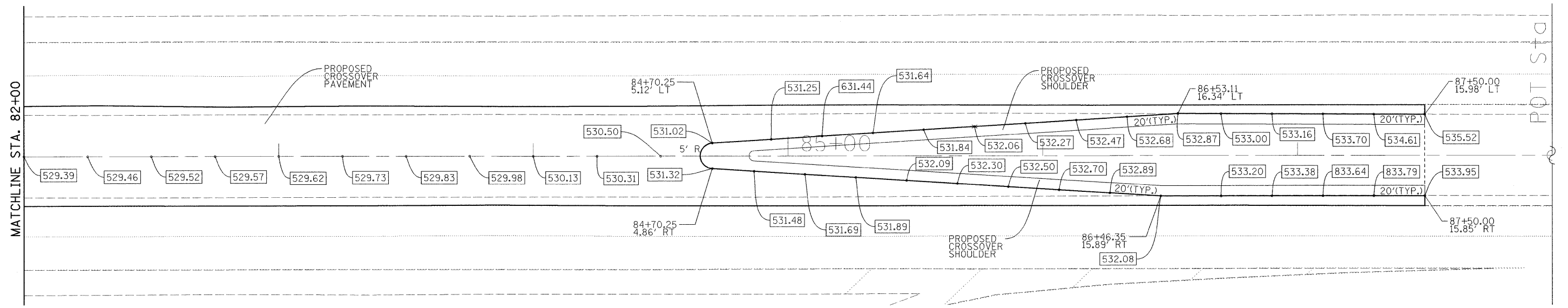
**I-55 OVER THE KANKAKEE RIVER  
PAVEMENT ELEVATION DETAILS - SOUTH CROSSOVER**

SCALE: 1" = 20' SHEET NO. 1 OF 2 SHEETS STA. 33+00.00 TO STA. 45+00.00

|   |               |        |                    |           |
|---|---------------|--------|--------------------|-----------|
| F.A.J. RTE.                                     | SECTION       | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 55  | 88 (B&B-1) BR | WILL   | 72                 | 25        |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |               |        | CONTRACT NO. 62930 |           |



MATCHLINE STA. 82+00



MATCHLINE STA. 87+50.00



200 West Front Street  
Wheaton, IL 60187

|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

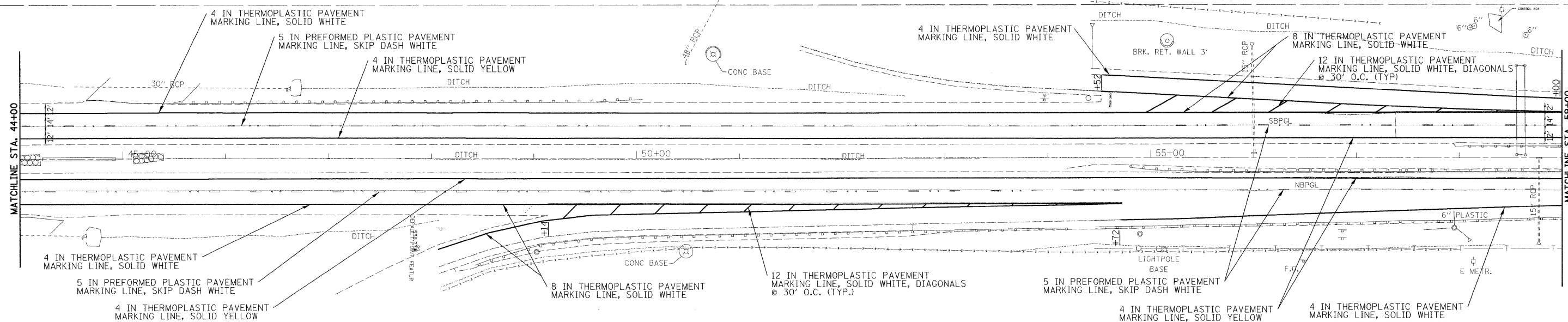
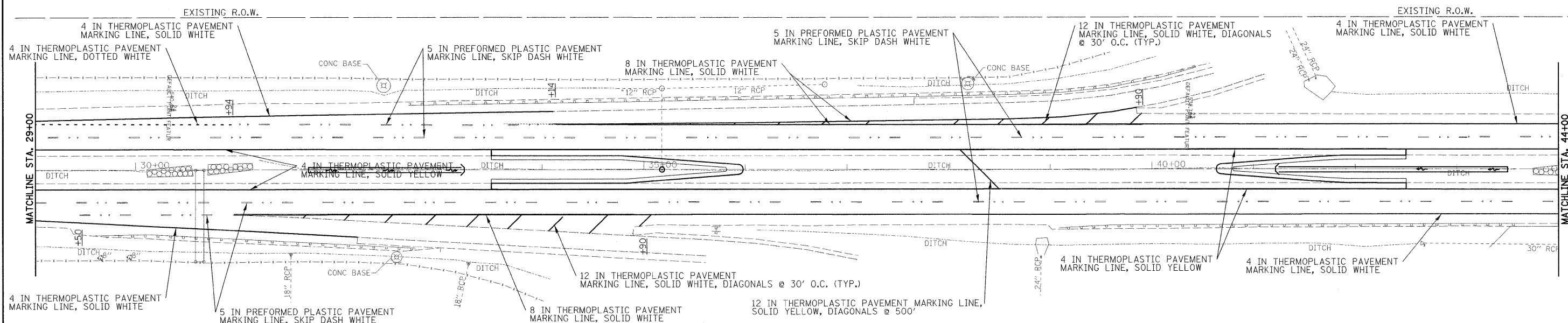
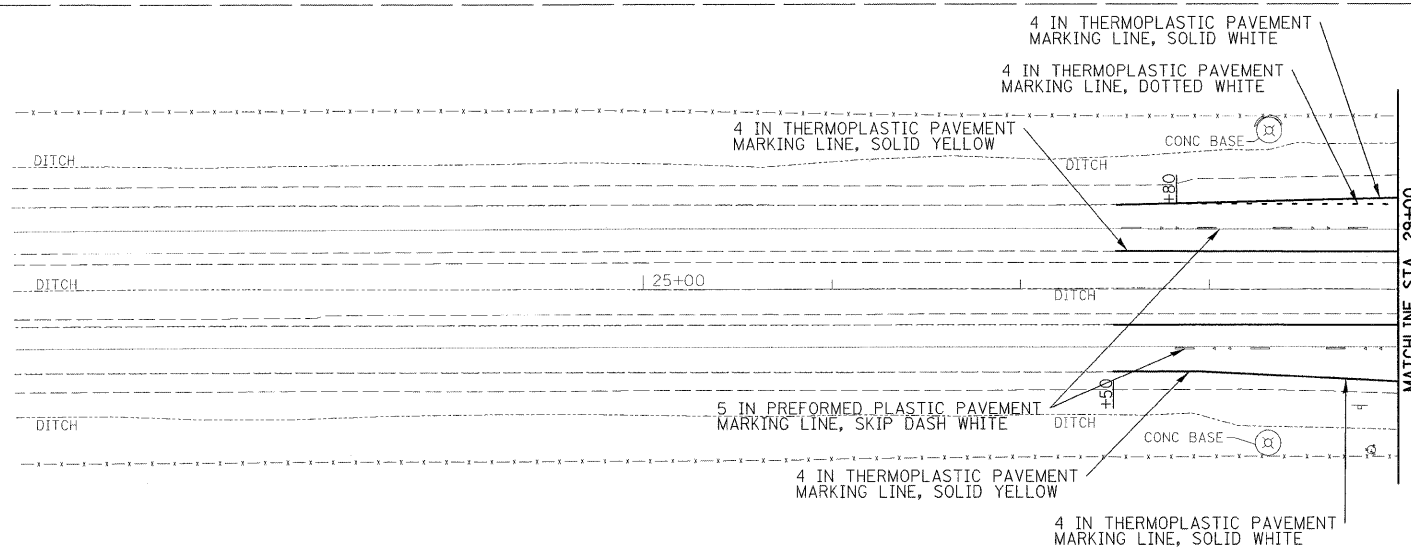
**I-55 OVER THE KANKAKEE RIVER  
PAVEMENT ELEVATION DETAILS - NORTH CROSSOVER**

SCALE: 1" = 20'    SHEET NO. 2 OF 2 SHEETS    STA. 76+00.00 TO STA. 88+00.00

|   |                       |             |                 |              |
|---|-----------------------|-------------|-----------------|--------------|
| F.A.I. RTE. 55                                  | SECTION 88 (B&B-1) BR | COUNTY WILL | TOTAL SHEETS 72 | SHEET NO. 26 |
| CONTRACT NO. 62930                              |                       |             |                 |              |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                       |             |                 |              |



LEGEND  
 ◁ ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER



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200 West Front Street  
 Wheaton, IL 60187

|                |           |
|----------------|-----------|
| DESIGNED - KSD | REVISED - |
| DRAWN - KSD    | REVISED - |
| CHECKED - CMJ  | REVISED - |
| DATE 03-04-09  | REVISED - |

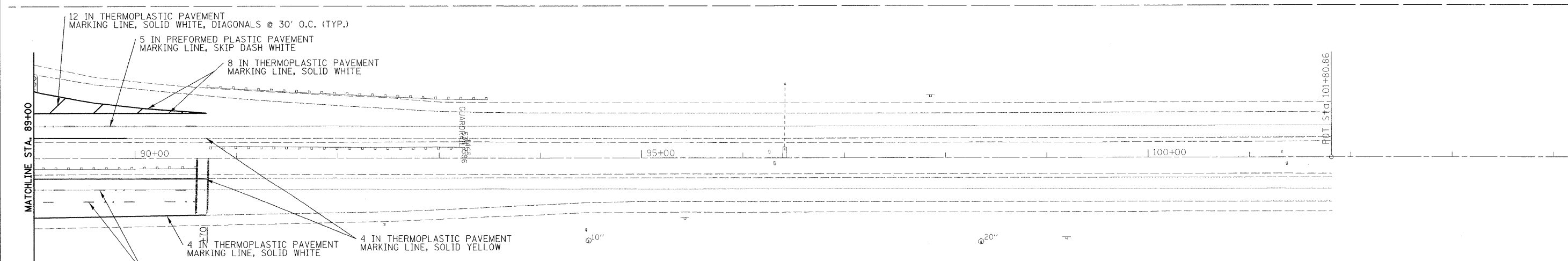
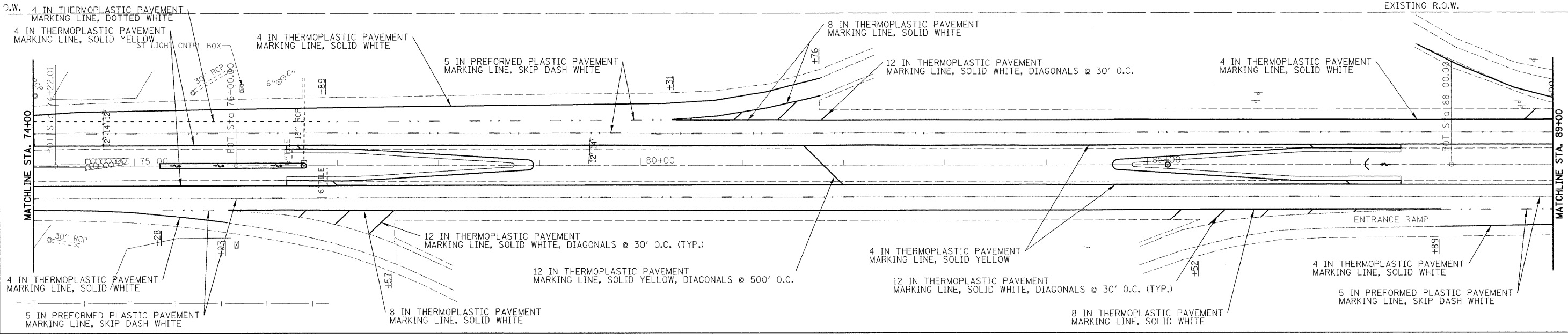
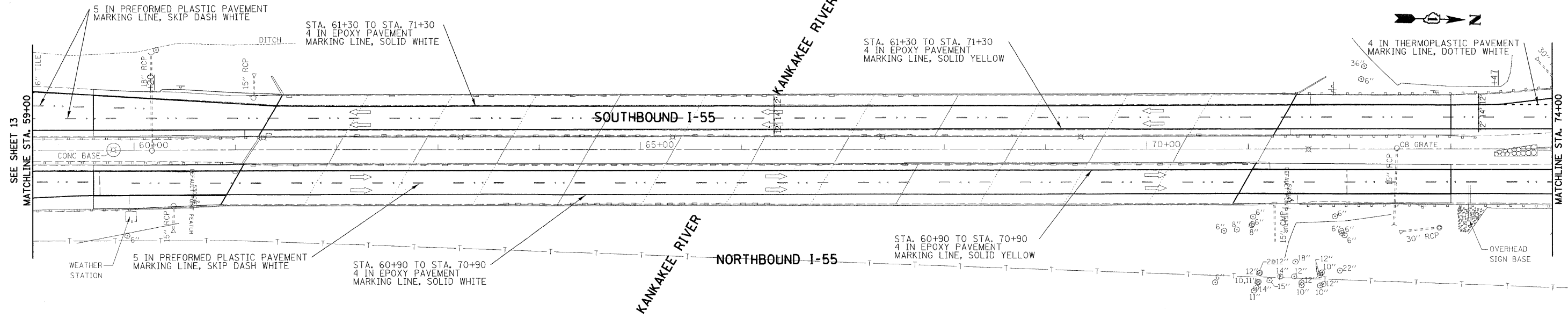
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

IL-55 OVER THE KANKAKEE RIVER  
 PAVEMENT MARKING PLAN

SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS STA. 29+00.00 TO STA. 59+00.00

|   |                       |             |                 |              |
|---|-----------------------|-------------|-----------------|--------------|
| F.A.I. RTE. 55                                  | SECTION 88 (B&B-1) BR | COUNTY WILL | TOTAL SHEETS 72 | SHEET NO. 27 |
| CONTRACT NO. 62930                              |                       |             |                 |              |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                       |             |                 |              |

SEE SHEET 14



LEGEND

< ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER

**rjngroup**  
 Excellence through Ownership  
 200 West Front Street  
 Wheaton, IL 60187

|                 |           |
|-----------------|-----------|
| DESIGNED - KSD  | REVISED - |
| DRAWN - KSD     | REVISED - |
| CHECKED - CMJ   | REVISED - |
| DATE - 03-04-09 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-55 OVER THE KANKAKEE RIVER  
 PAVEMENT MARKING PLAN**

SCALE: 1" = 50'    SHEET NO. 2 OF 2 SHEETS    STA. 59+00.00 TO STA. 89+00.00

| F.A.I. RTE.                                     | SECTION       | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------------|--------|--------------------|-----------|
| 55  | 88 (B&B-1) BR | WILL   | 72                 | 28        |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |               |        | CONTRACT NO. 62930 |           |

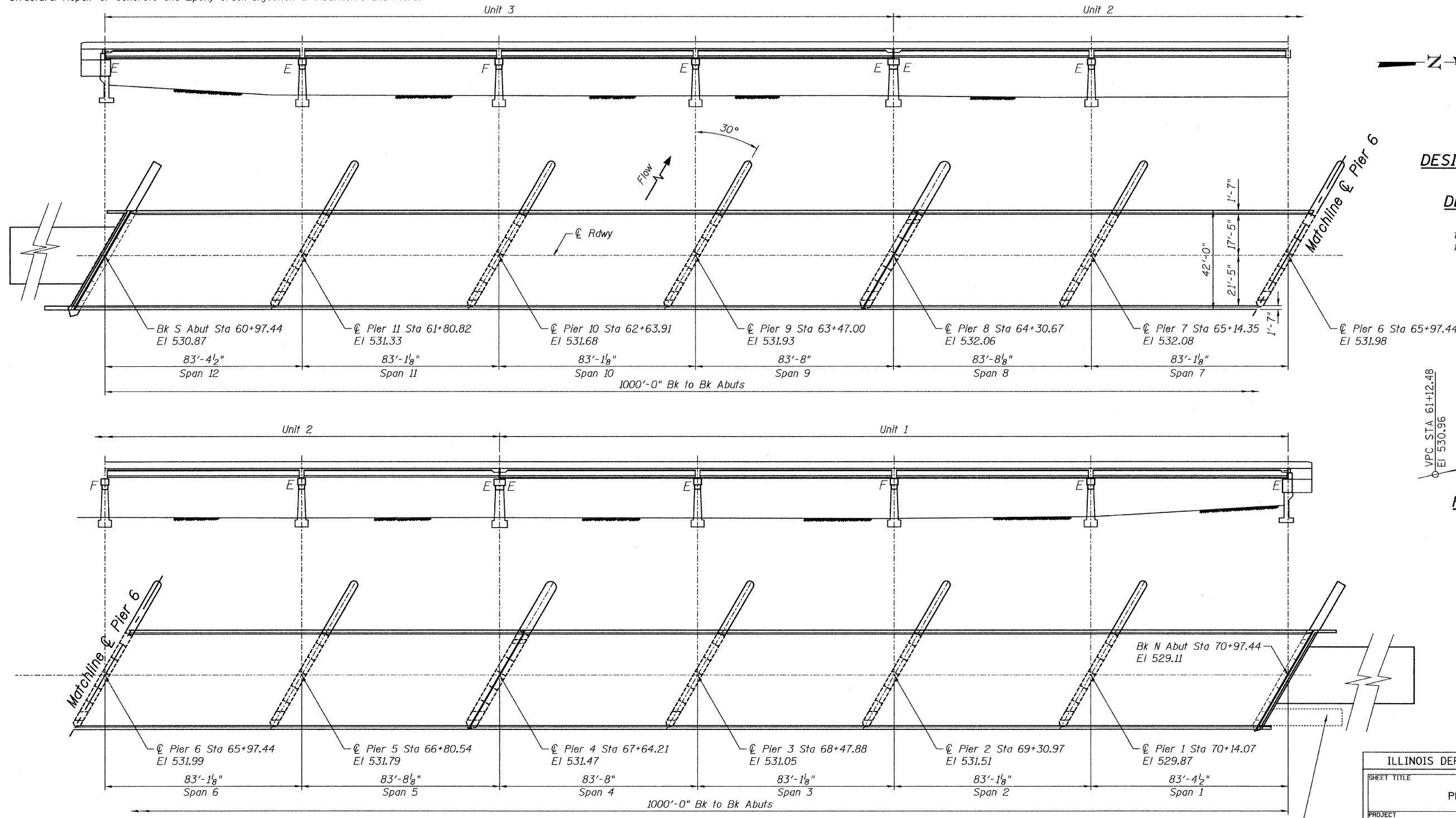


Benchmark: Chiseled "X" on top of west parapet, south end of SN 099-0002, Elev. 533.22

Existing Structure: 12 Span PPC I-Beam Superstructure Consisting of Three (4 Span) Continuous Units on Solid Concrete Piers and Closed Concrete Abutments. The Abutments and Piers Were Built in 1935 and New Caps and Superstructure Were Added in 1977.

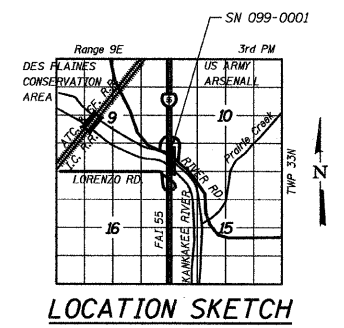
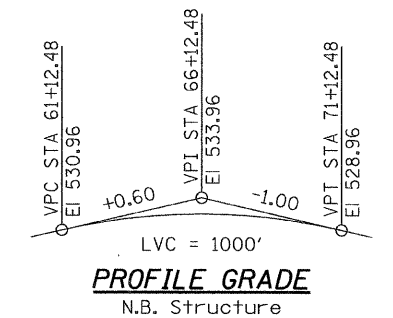
Proposed Improvement: Deck Repair. Scarify Deck 1" and Install Bridge Deck Latex Concrete Overlay 2 1/4".  
Neoprene Joint Replacement, P.P.C. Beam Repair.  
Addition of Wingwall at the North Abutment.  
Structural Repair of Concrete and Epoxy Crack Injection at Abutments and Piers.

|                       |         |                 |                   |               |
|-----------------------|---------|-----------------|-------------------|---------------|
| ROUTE NO.             | SECTION | COUNTY          | SHEET             | SHEET NO.     |
| FAI 55                | *       | Will            | 72                | 29            |
| FED. ROAD DIST. NO. 7 |         | ILLINOIS        | FED. AID PROJECT: |               |
|                       |         | Contract #62930 |                   | * 88(B&B-1)BR |



**DESIGN SPECIFICATIONS**  
2002 AASHTO

**DESIGN STRESSES**  
FIELD UNITS  
f<sub>c</sub> = 3,500 psi  
f<sub>y</sub> = 60,000 psi (reinforcement)



**PLAN & ELEVATION**

Existing Approach Beams to be removed. See sheet 14 of 19.



Mary Coombe Bloxdorf  
ILLINOIS STRUCTURAL NO. 4859  
EXPIRES: 11/30/10  
DATE: 11/23/09

|  |  |
|--|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION  |  |
| SHEET TITLE<br><b>PLAN AND ELEVATION</b>   |  |
| PROJECT<br>I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001                | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>9/23/09<br>DRAWN BY<br>CFC<br>CHECKED BY<br>MCB/KPS<br>DRAWING NO. |
| <b>COOMBE-BLOXDORF P.C.</b><br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 | <b>1</b><br>OF 19 SHTS   |

PLOT DATE = 11/23/09  
FILE NAME = \\snp-099-0001-ent-01-plan-eldgn  
PLOT SCALE = 214.0000 1" / IN.  
USER NAME = CFC

### INDEX OF SHEETS

- 1 PLAN AND ELEVATION
- 2 GENERAL NOTES AND TOTAL BILL OF MATERIAL
- 3 CROSS SECTION
- 4 EXPANSION JOINT DETAILS ABUTMENTS
- 5 EXPANSION JOINT DETAILS PIERS 4 & 8
- 6 PREFORMED JOINT STRIP SEAL
- 7 CONTINUOUS SEAL NEOPRENE EXPANSION JOINTS
- 8 -9 DECK SLAB REPAIR
- 10 BEAM REPAIR DETAILS
- 11 BEAM PRELOADING DETAILS
- 12 SOUTH ABUTMENT CONCRETE REMOVAL
- 13 SOUTH ABUTMENT DETAILS
- 14 NORTH ABUTMENT CONCRETE REMOVAL
- 15 NORTH ABUTMENT DETAILS
- 16 NORTH ABUTMENT WINGWALL DETAILS
- 17 ABUTMENT AND PIER REPAIR DETAILS
- 18 PIER REPAIR DETAILS
- 19 BORING LOGS

### GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

THE CONTRACTOR SHALL ENSURE THAT NO CONCRETE WILL BE ALLOWED TO DROP INTO THE RIVER. CONCRETE SHALL BE CAUGHT ON A FLOATING PLATFORM OR OTHER MEANS APPROVED BY THE ENGINEER. COST INCLUDED WITH DECK SLAB REPAIR (FULL DEPTH, TYPE II).

PRIOR TO BEGINNING ANY BEAM REPAIR WORK, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A PRELOADING SYSTEM ON THE BRIDGE DECK OVER THE EXISTING DAMAGED BEAM AT THE SPECIFIED LOCATIONS. THE PRELOADING SYSTEM SHOULD PRODUCE A TOTAL MAXIMUM SERVICE LOAD MOMENT AS SHOWN AT THE CENTERLINE OF THE DAMAGED AREA.

PRELOADING SHALL BE KEPT IN PLACE FOR AT LEAST THREE (3) DAYS AFTER COMPLETION OF CONCRETE REPAIR OR UNTIL THE CONCRETE HAS REACHED AN ULTIMATE STRENGTH OF 5,000 psi. THE CONTRACTOR'S PROPOSED PRELOADING SYSTEM, WITH COMPUTATIONS, SEALED AND SIGNED BY AN ILLINOIS STRUCTURAL ENGINEER SHALL BE SUBMITTED TO THE BUREAU OF BRIDGES AND STRUCTURES FOR APPROVAL. THE PRELOADING SYSTEM SHALL BE PLACED SHORTLY AFTER BRIDGE CLOSURE FOR REPAIRS.

THE PRELOADING SYSTEM SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE OF PPC-I BEAM REPAIRS.

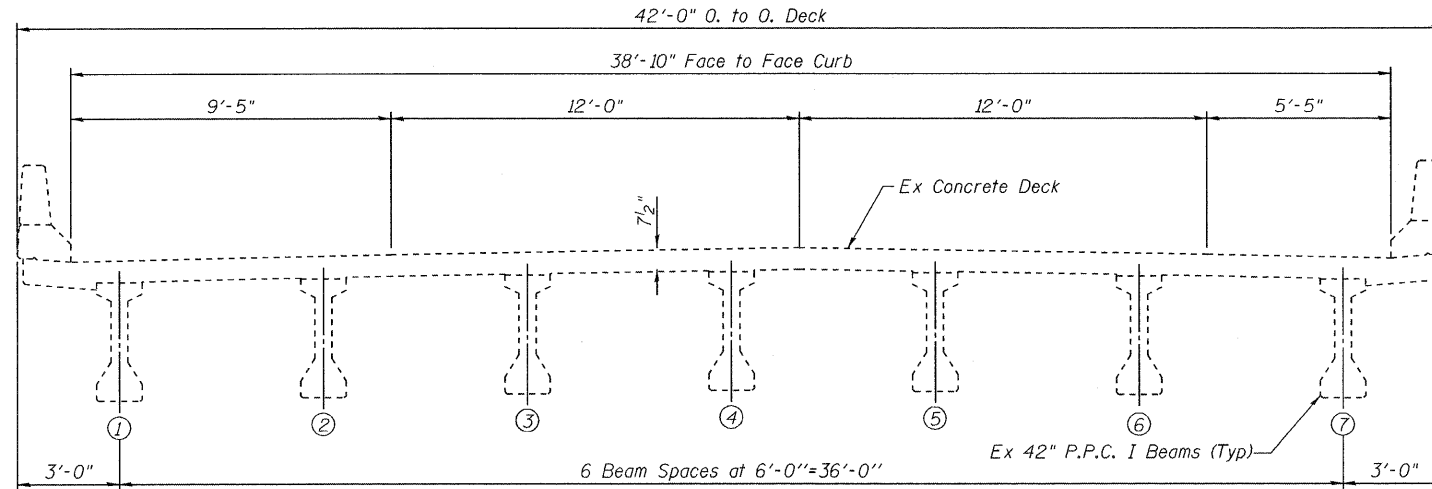
REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 760 Gr 60. SEE SPECIAL PROVISIONS.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

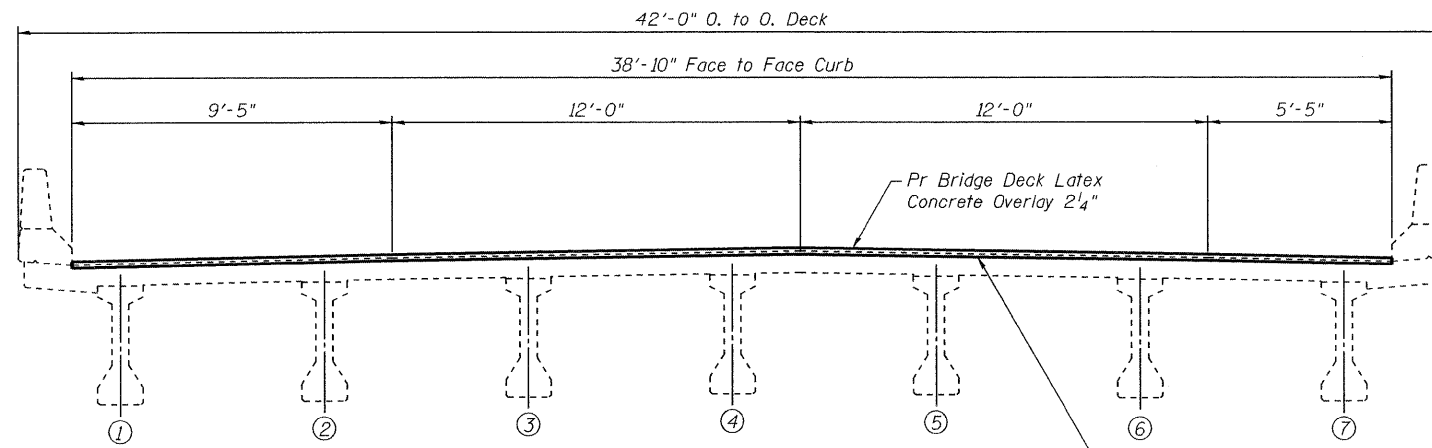
### TOTAL BILL OF MATERIAL

| ITEM   | UNIT   | SUPER  | SUB  | TOTAL  |
|--|--------|--------|------|--------|
| Bridge Deck Latex Concrete Overlay 2 1/4"                      | Sq Yd  | 4162   |      | 4162   |
| Bridge Deck Hydro Scarification 1"                             | Sq Yd  | 4162   |      | 4162   |
| Deck Slab Repair (Full Depth, Type I)                          | Sq Yd  | 5      |      | 5      |
| Deck Slab Repair (Full Depth, Type II)                         | Sq Yd  | 238    |      | 238    |
| Preformed Joint Strip Seal                                     | Foot   | 92     |      | 92     |
| Neoprene Expansion Joint 4"                                    | Foot   | 94     |      | 94     |
| Precast Prestressed Concrete I-Beam Repair                     | Sq Ft  | 11.5   |      | 11.5   |
| Epoxy Crack Injection  | Foot   |        | 627  | 627    |
| Structural Repair of Concrete (Depth equal to or less than 5") | Sq Ft  |        | 531  | 531    |
| Structural Repair of Concrete (Depth greater than 5")          | Sq Ft  |        | 17   | 17     |
| Removal of Existing Precast Concrete Units                     | Sq Ft  |        | 180  | 180    |
| Concrete Structures  | Cu Yd  |        | 17.3 | 17.3   |
| Reinforcement Bars, Epoxy Coated                               | Pound  | 11,780 | 3870 | 15,650 |
| Structure Excavation   | Cu Yd  |        | 10   | 10     |
| Permanent Steel Sheet Piling                                   | Sq Ft  |        | 468  | 468    |
| Stud Shear Connectors  | Each   |        | 19   | 19     |
| Protective Coat  | Sq Yd  | 4341   | 12   | 4353   |
| Concrete Removal   | Cu Yd  | 78.7   | 6.3  | 85.0   |
| Concrete Superstructure  | Cu Yd  | 90.8   |      | 90.8   |
| Bridge Deck Grooving   | Sq Yd  | 4087   |      | 4087   |
| Replace Surface Sensor and Temperature Probe                   | L. Sum | 1      |      | 1      |

|  |             |
|--|-------------|
| ILLINOIS DEPARTMENT OF TRANSPORTATION    |             |
| SHEET TITLE                              |             |
| GENERAL NOTES AND TOTAL BILL OF MATERIAL |             |
| PROJECT                                  | PROJECT NO. |
| I-55 NB OVER KANKAKEE RIVER              | 03095-16    |
| FAI ROUTE 55, SECTION 88(B&B-1)BR        | SCALE       |
| WILL COUNTY                              | DATE        |
| SN 099-0001                              | 6/25/09     |
|  | DRAWN BY    |
|  | TFG/CFC     |
|  | CHECKED BY  |
|  | MCB         |
| DRAWING NO.                              |             |
| COOMBE-BLOXDORF P.C.                     |             |
| Engineers / Land Surveyors               |             |
| Springfield, Illinois                    |             |
| Design Firm License No. 184-002703       |             |
| 2  | OF 19 SHTS  |



**EXISTING CROSS SECTION**  
(Looking South)



**PROPOSED CROSS SECTION**  
(Looking South)

**BILL OF MATERIAL**

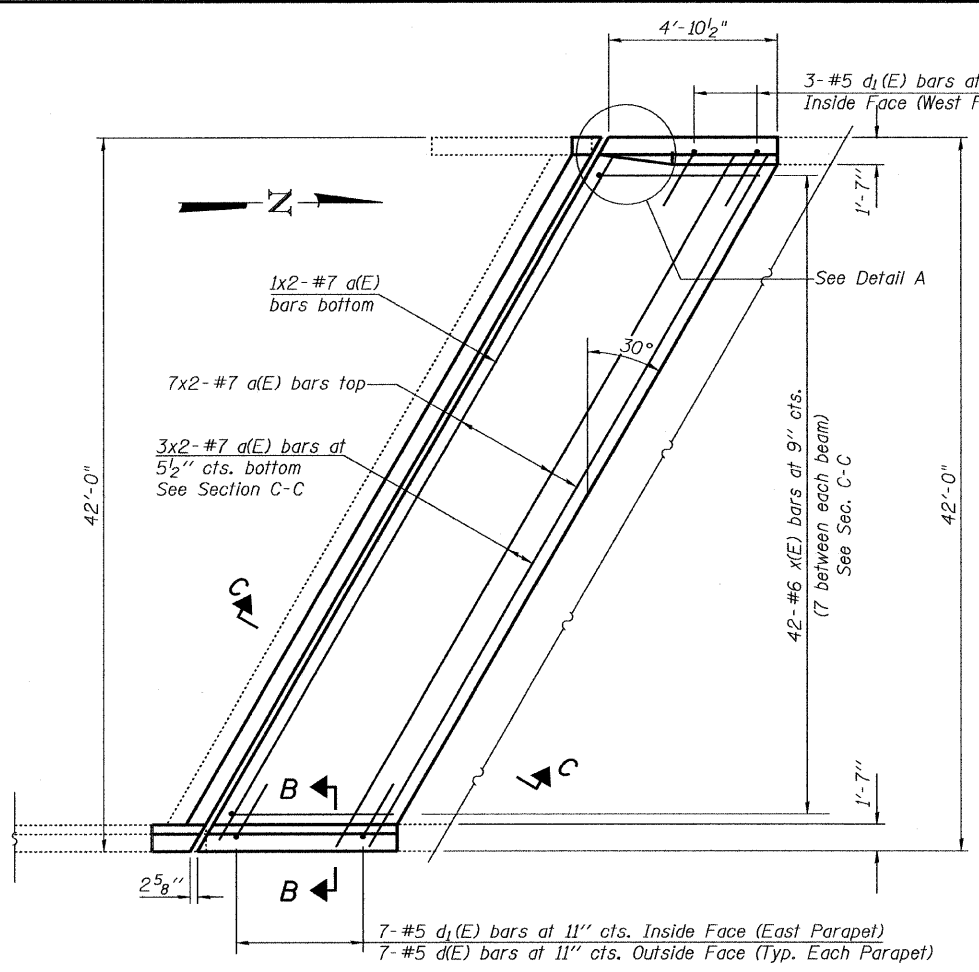
| ITEM                                      | UNIT  | TOTAL |
|---|-------|-------|
| Bridge Deck Latex Concrete Overlay 2 1/4" | Sq Yd | 4162  |
| Bridge Deck Hydro Scarification 1"        | Sq Yd | 4162  |

**NOTE:**

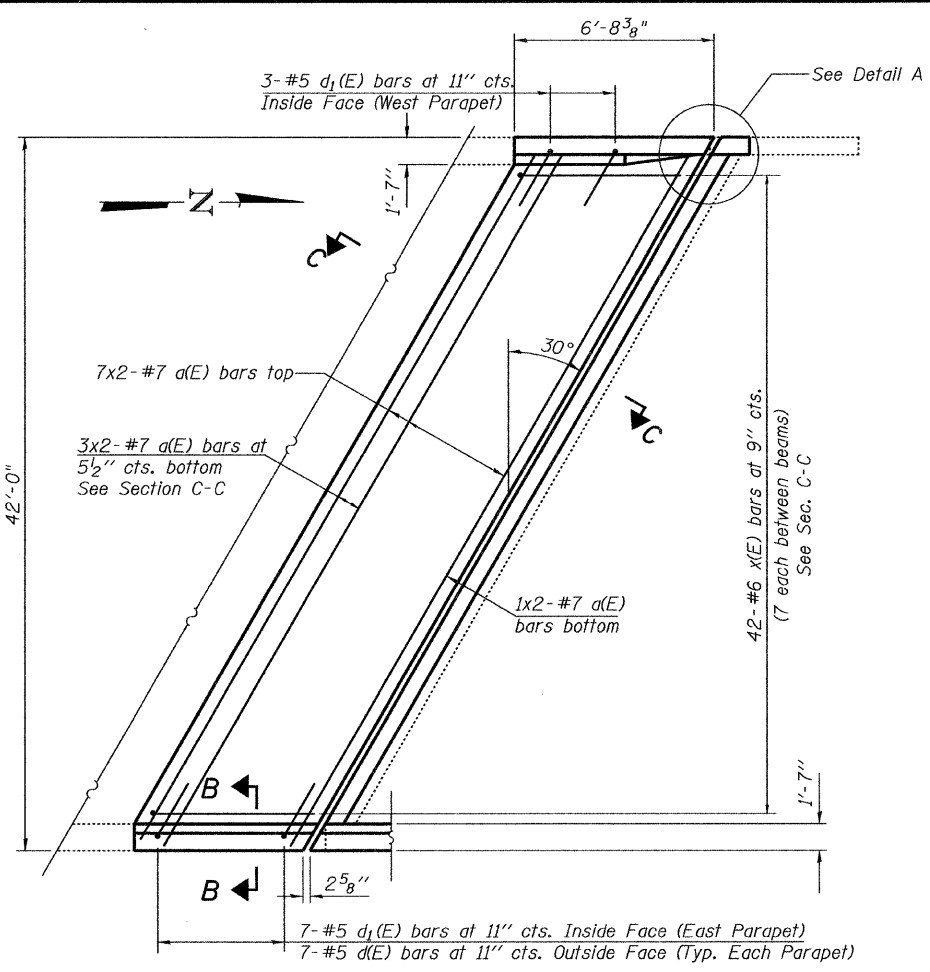
Bridge Deck Hydro Scarification 1" and Bridge Deck Latex Concrete Overlay 2 1/4" extends between Concrete Removal of Deck in Each Unit.

PLOT DATE = 6/25/09  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = JML

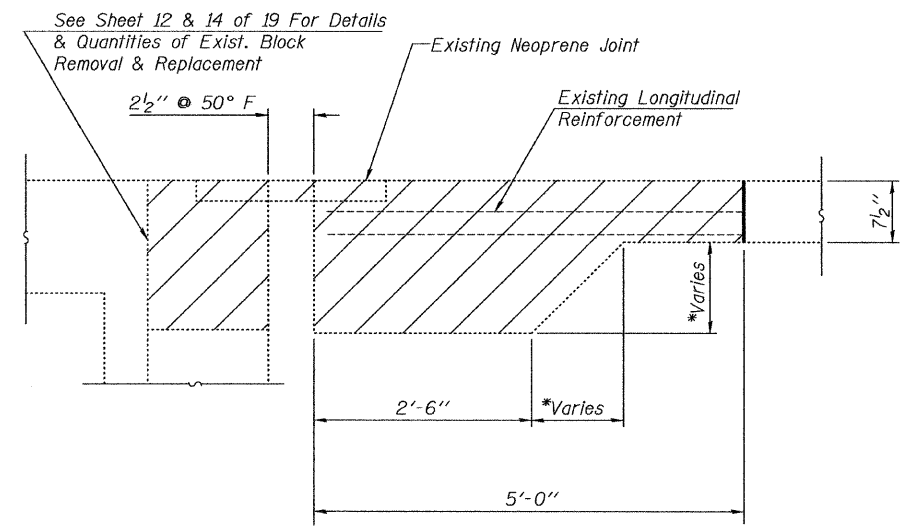
|  |  |
|--|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION  |  |
| SHEET TITLE<br><b>CROSS SECTION</b>  |  |
| PROJECT<br>I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001                | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>06/25/09<br>DRAWN BY<br>TFG/GFC<br>CHECKED BY<br>MCB |
| <b>COOMBE-BLOXDORF P.C.</b><br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 | DRAWING NO.<br><b>3</b><br>OF 19 SHTS  |



**PLAN AT SOUTH ABUTMENT**



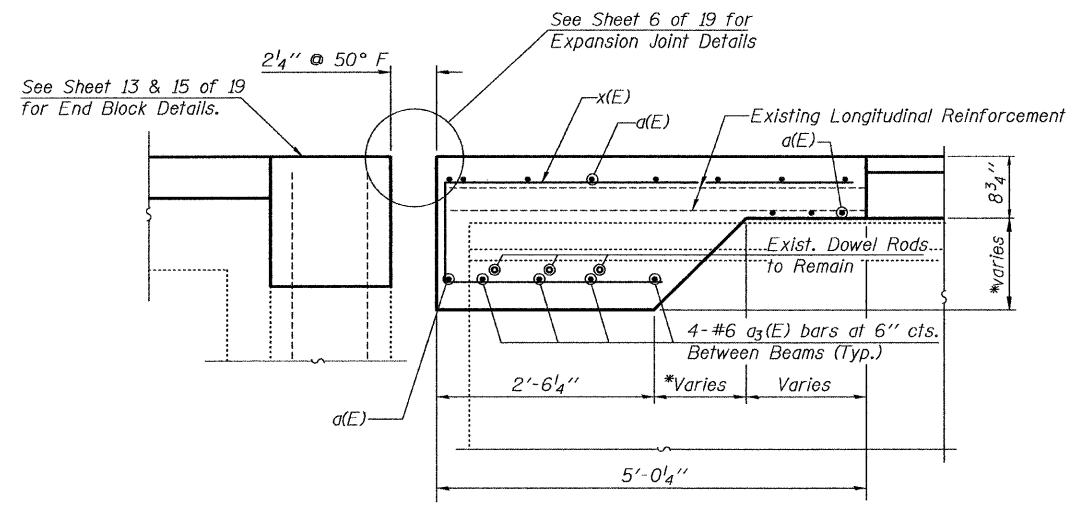
**PLAN AT NORTH ABUTMENT**



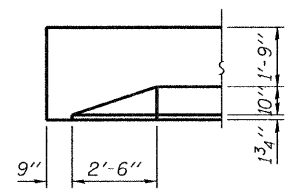
**CONCRETE REMOVAL AT ABUTMENTS**  
(dimensions at right L's)

**MIN. BAR LAP**  
#7 bars = 2'-9"

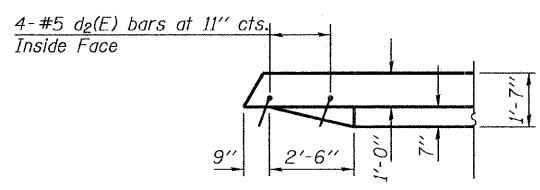
\*1'-1" min. @ low beam to 1'-5 1/8" max. at high beam



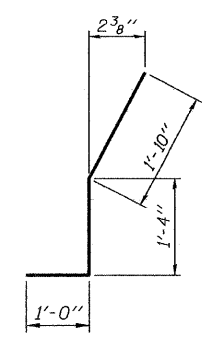
**SECTION C-C**



**VIEW A-A**



**DETAIL A**  
(North Abutment Opposite)



**BAR d2(E)**

**NOTES:**

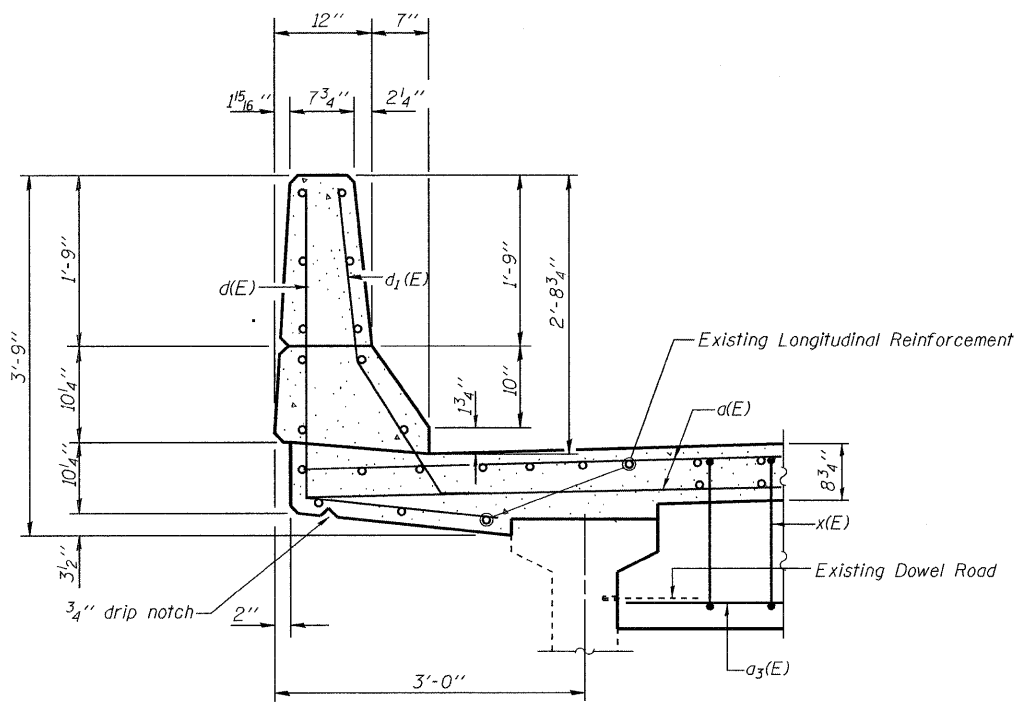
Existing longitudinal reinforcement and dowel rods extending into the removed areas shall be cleaned, straightened and incorporated into the new construction. Cost included with "Concrete Removal".  
 Hatched areas indicate deck to be removed as "Concrete Removal". Removal of existing neoprene joint is included with "Concrete Removal".  
 The parapet within the limits of the deck to be removed is also to be removed as "Concrete Removal".  
 See Sheet 5 of 19 for Section B-B and d(E) and d1(E) bar details.  
 Any longitudinal reinforcement bars extending into the new construction that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
 Existing transverse reinforcement extending into removed area shall be cut.

**BILL OF MATERIAL**  
**2 ABUTMENT JOINTS**

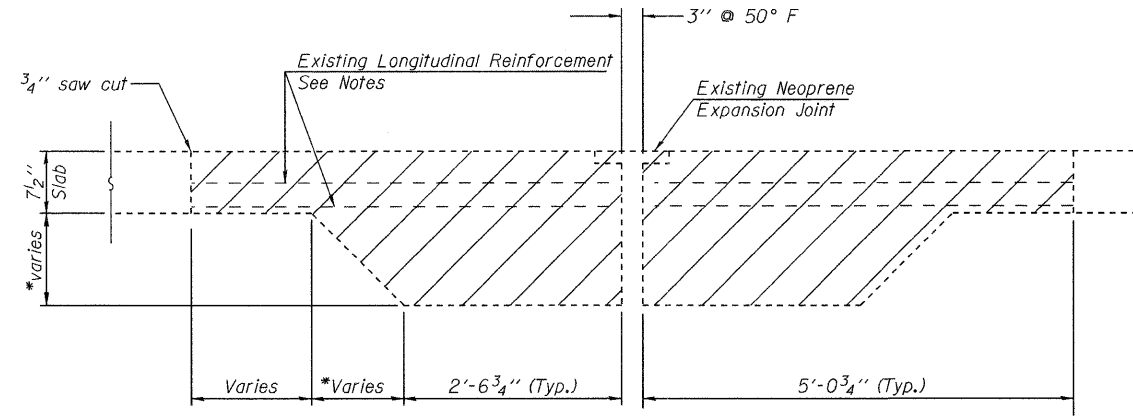
| Bar                              | No. | Size | Length  | Shape |
|----------------------------------|-----|------|---------|-------|
| a(E)                             | 44  | #7   | 25'-3"  | —     |
| a3(E)                            | 48  | #6   | 4'-3"   | —     |
| d(E)                             | 28  | #5   | 5'-0"   | ┘     |
| d1(E)                            | 20  | #5   | 3'-1"   | ┘     |
| d2(E)                            | 8   | #5   | 4'-2"   | ┘     |
| x(E)                             | 84  | #6   | 9'-5"   | ┘     |
| Concrete Removal                 |     |      | Cu. Yd. | 26.0  |
| Concrete Superstructure          |     |      | Cu. Yd. | 34.5  |
| Reinforcement Bars, Epoxy Coated |     |      | Pound   | 4010  |

|   |  |
|---|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION   |  |
| SHEET TITLE<br>EXPANSION JOINT DETAILS ABUTMENTS  |  |
| PROJECT<br>I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001         | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>9/23/09<br>DRAWN BY<br>TFG<br>CHECKED BY<br>MCB<br>DRAWING NO. |
| COOMBE-BLOXDORF P.C.<br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 | 4<br>OF 19 SHTS  |

PLOT DATE = 11/23/2009  
 FILE NAME = ...88-099-0001-sh1-04-expansion-joint-details-abutments.dgn  
 PLOT SCALE = 5x40000 1" = 7' IN.  
 USER NAME = CFC.



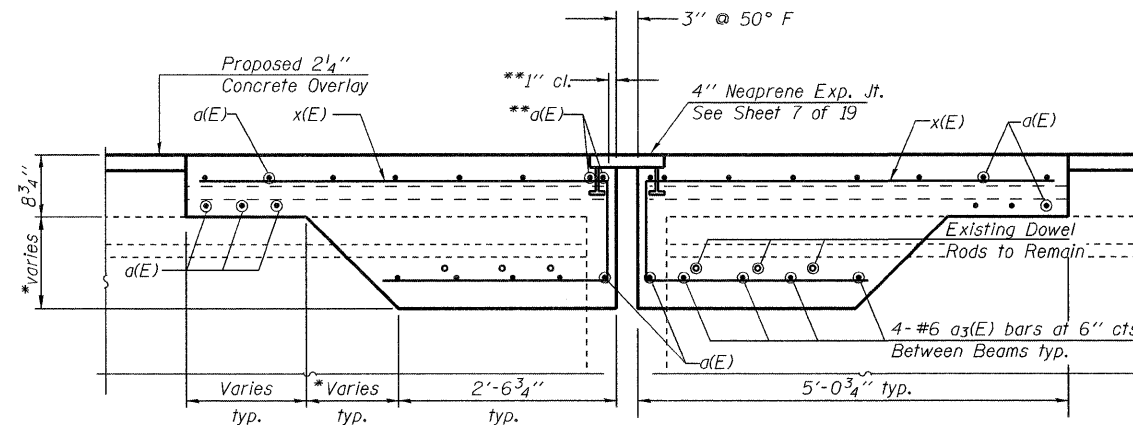
**SECTION B-B**



**CONCRETE REMOVAL AT PIERS 4 & 8**

(dimensions at right L's)

\*1'-1" min. at low beam to  
1'-5 1/8" max. at high beam



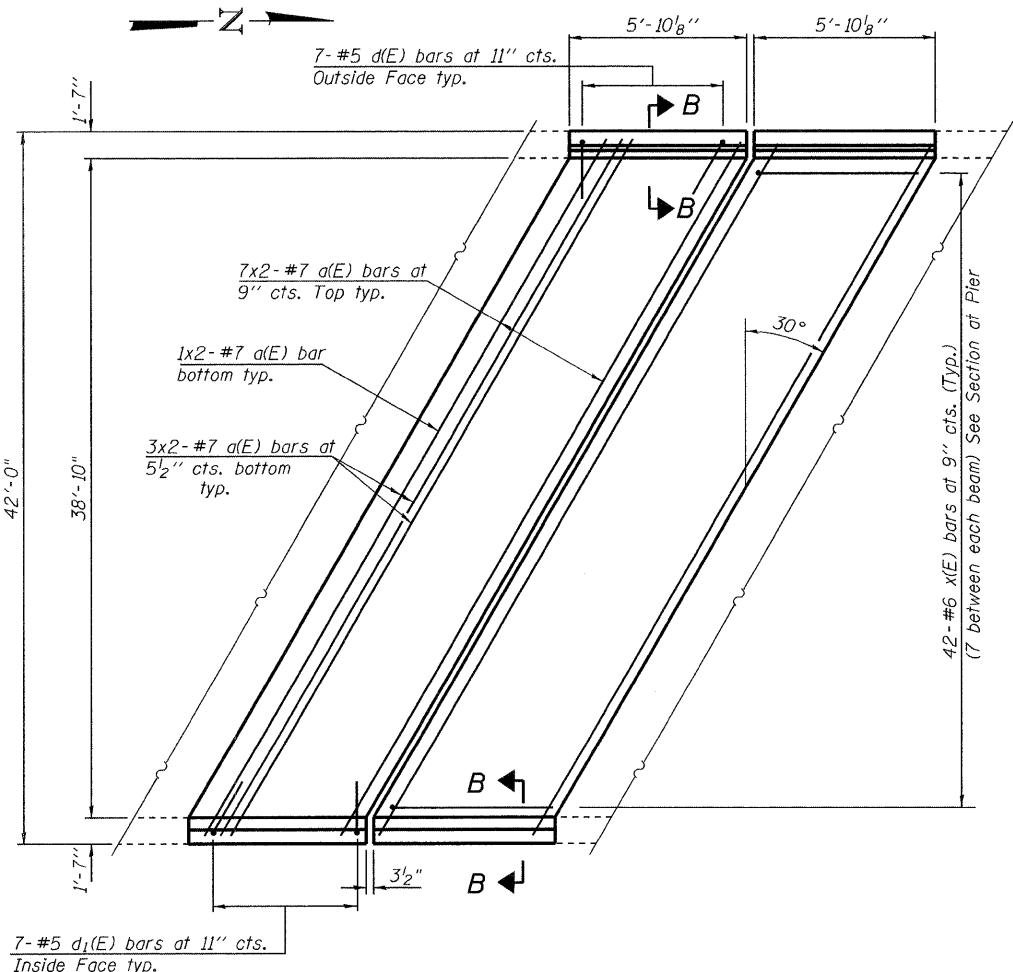
**SECTION AT PIERS 4 & 8**

(dimensions at right L's)

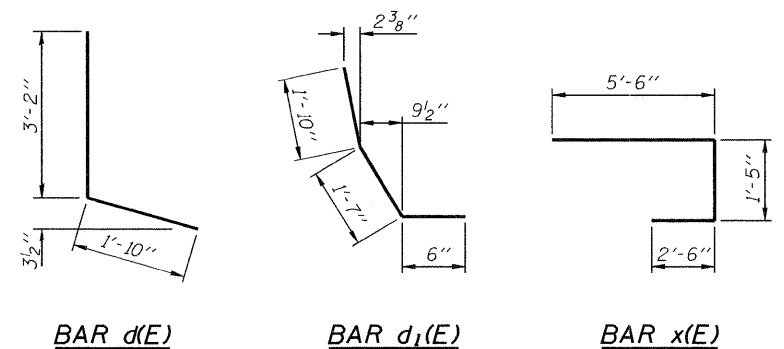
**BILL OF MATERIAL**  
**2 PIERS**

| Bar                              | No. | Size | Length | Shape   |      |
|----------------------------------|-----|------|--------|---------|------|
| a(E)                             | 88  | #7   | 25'-3" | —       |      |
| a3(E)                            | 96  | #6   | 4'-3"  | —       |      |
| d(E)                             | 28  | #5   | 5'-0"  | ┘       |      |
| d1(E)                            | 28  | #5   | 3'-1"  | ┘       |      |
| x(E)                             | 168 | #6   | 9'-5"  | ┘       |      |
| Concrete Removal                 |     |      |        | Cu. Yd. | 52.7 |
| Concrete Superstructures         |     |      |        | Cu. Yd. | 56.3 |
| Reinforcement Bars, Epoxy Coated |     |      |        | Pound   | 7770 |

**MIN. BAR LAP**  
#7 bars = 2'-9"



**PLAN AT PIERS 4 & 8**



\*\*Place a(E) bars in back of anchor bolt as shown if required to maintain 1" cl. (±0-1/8"). Anchor bolts should be tied to a(E) bars, typical each side of joint.

**NOTES:**  
Existing longitudinal reinforcement and dowel rods extending into the removed areas shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal.  
Hatched areas indicate deck to be removed as Concrete Removal. Removal of existing neoprene joint is included with Concrete Removal.  
The parapet within the limits of the deck to be removed is also to be removed as Concrete Removal.  
Any longitudinal reinforcement bars extending into the New Construction that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

|   |   |
|---|---|
| ILLINOIS DEPARTMENT OF TRANSPORTATION   |   |
| SHEET TITLE<br>EXPANSION JOINT DETAILS PIERS 4 & 8  |   |
| PROJECT<br>I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001         | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>6/25/09<br>DRAWN BY<br>TFG/CFC<br>CHECKED BY<br>MCB<br>DRAWING NO.<br>5 |
| COOMBE-BLOXDORF P.C.<br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 |   |
| OF 19 SHTS  |   |

PLOT DATE = 6/25/09  
 PLOT TIME = 10:11:47 AM  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = JML



| Joint Size | "C" at 50°F | "D" at 50°F |
|------------|-------------|-------------|
| 2"         | 2"          | 1 1/2" Min. |
| 2 1/2"     | 2 1/2"      | 1 3/4" Min. |
| 4"         | 3"          | 2 1/2" Min. |

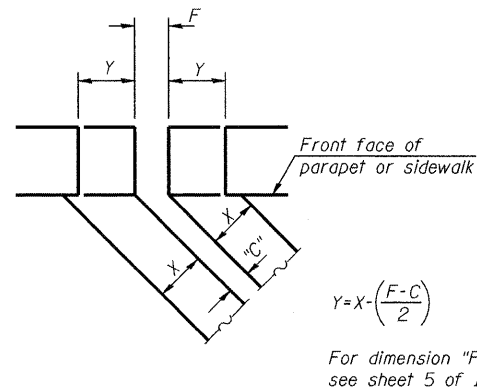
### INSTALLATION NOTES

- Install continuous seal in roadway, parapet, curb, and sidewalk.
- Install anchor blocks as indicated.

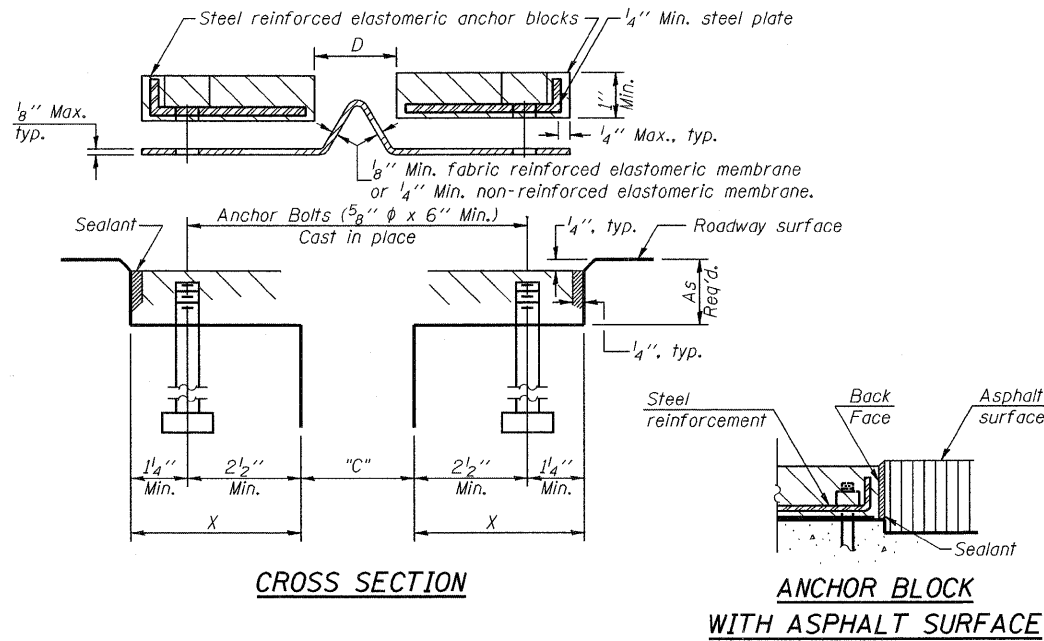
Note A:  
Maximum spacing of anchor bolts shall be 12" centers.

### SKEW LIMITATIONS

The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews. For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 1/2" from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±12" cts.



FORMING BLOCKOUT SKETCH



CROSS SECTION

ANCHOR BLOCK WITH ASPHALT SURFACE

### GENERAL NOTES

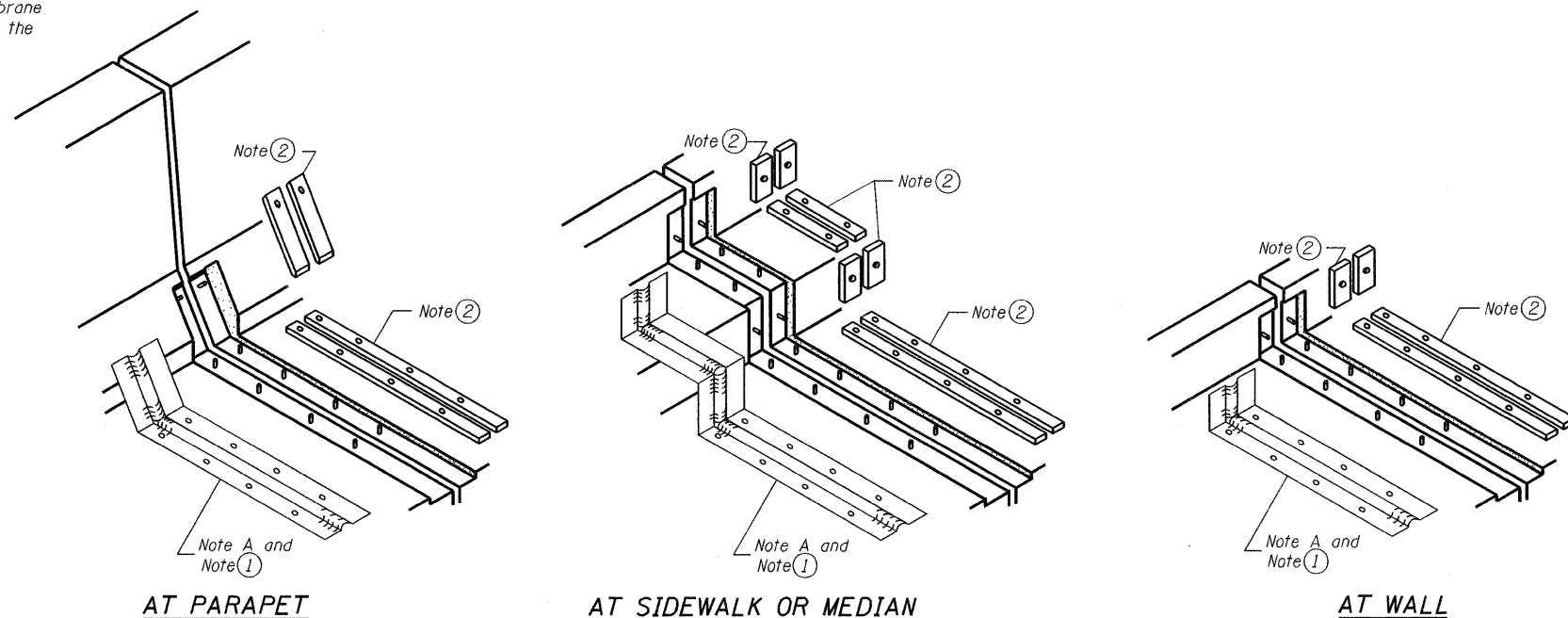
Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane.

The elastomeric membrane shall be premolded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure.

The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed.

Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

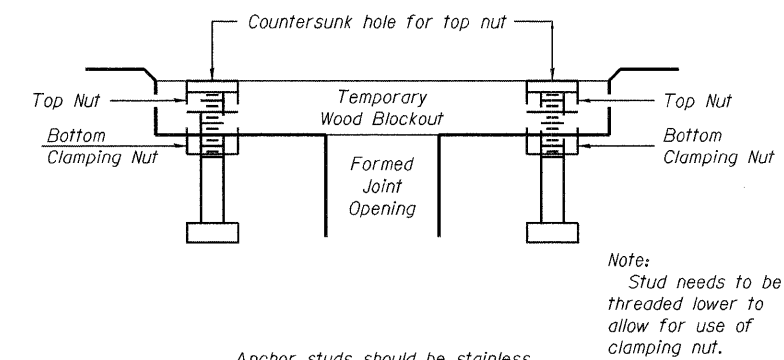
The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted.



AT PARAPET

AT SIDEWALK OR MEDIAN

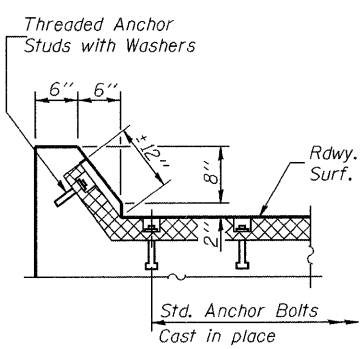
AT WALL



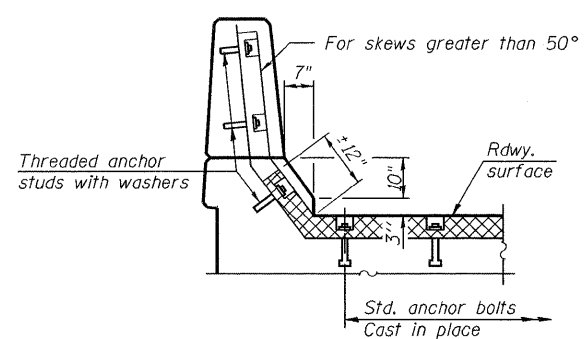
RECOMMENDED BLOCKOUT DETAIL

### BILL OF MATERIAL

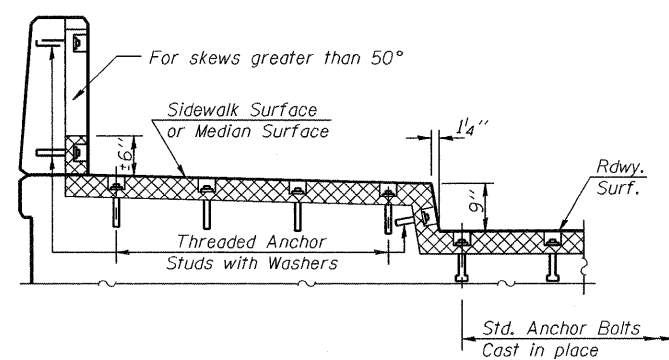
| Item                        | Unit | Total |
|-----------------------------|------|-------|
| Neoprene Expansion Joint 4" | foot | 94    |
|                             |      |       |



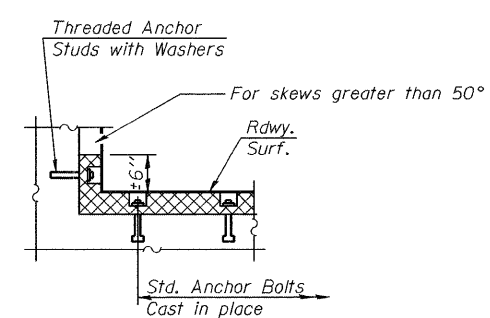
AT CURB



AT PARAPET



AT SIDEWALK OR MEDIAN TYPICAL END TREATMENTS



AT WALL

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE  
CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS

PROJECT  
I-55 NB OVER KANKAKEE RIVER  
FAI ROUTE 55, SECTION 88(B&B-1)BR  
WILL COUNTY  
SN 099-0001

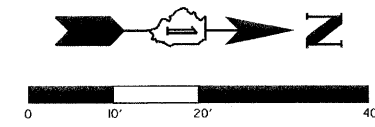
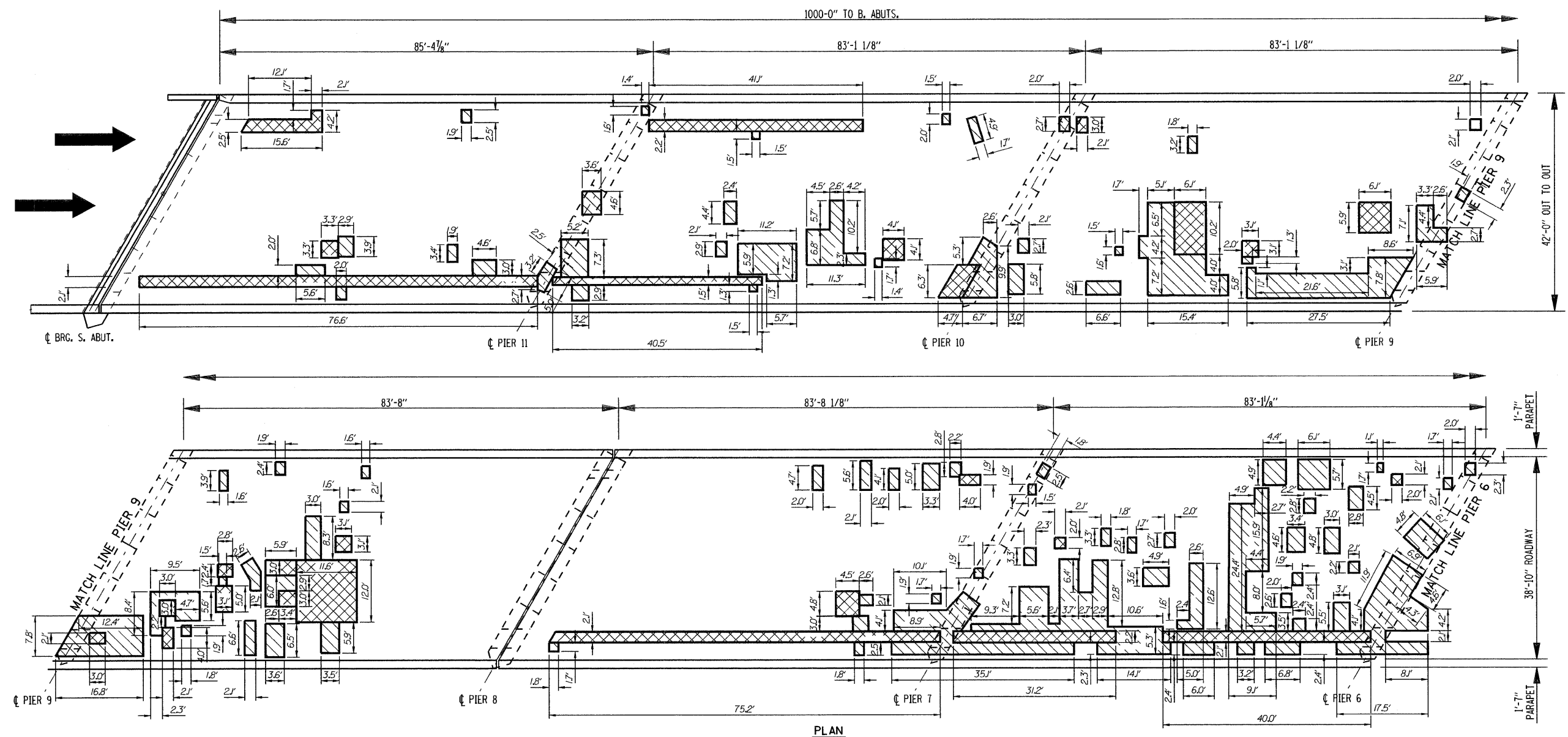
PROJECT NO. 03095-16  
SCALE  
DATE 06/25/09  
DRAWN BY CFC  
CHECKED BY MCB  
DRAWING NO. 7

COOMBE-BLOXDORF P.C.  
Engineers / Land Surveyors  
Springfield, Illinois  
Design Firm License No. 184-002703

OF 19 SHTS

PLOT DATE = 6/25/2009  
 PLOT SCALE = 0.2500  
 USER NAME = JML





- LEGEND**
- DECK SLAB REPAIR PARTIAL
  - DECK SLAB REPAIR (FULL DEPTH, TYPE II)
  - DECK SLAB REPAIR (FULL DEPTH, TYPE I)

**NOTES:**

Deck Drains that are Located In Areas of Full Depth Repair Shall be Replaced as approved by the Engineer. Cost included with Deck Slab Repair (Full Depth, Type II) or Deck Slab Repair (Full Depth, Type I) as Applicable.

The Areas of Deck Slab Repair are Based on Results of an Infrared Deck Survey Conducted on 04/16/09.

Areas of Deck Slab Repair (Partial) are shown for Information only to assist the Contractor in bidding.

Areas of repair that are located within 10 feet of the parapet face should be repaired in maximum lengths of 10' at a time to ensure stability at the parapet.

**BILL OF MATERIAL**

| ITEM                                   | UNIT  | TOTAL |
|--|-------|-------|
| DECK SLAB REPAIR (FULL DEPTH, TYPE I)  | Sq Yd | 1     |
| DECK SLAB REPAIR (FULL DEPTH, TYPE II) | Sq Yd | 124   |

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE

DECK SLAB REPAIR

PROJECT

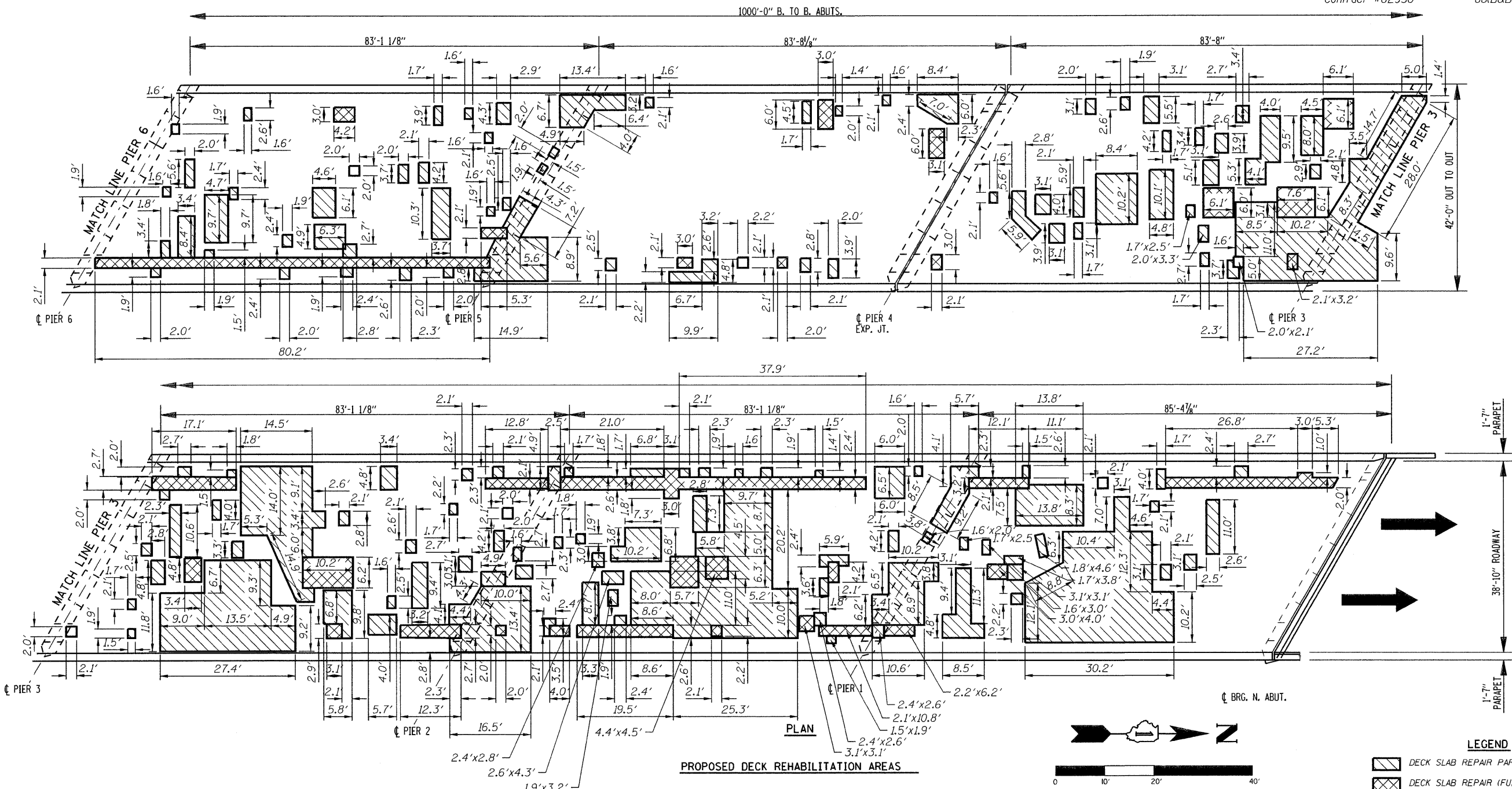
I-55 NB OVER KANKAKEE RIVER  
FAI ROUTE 55, SECTION 88(B&B-1)BR  
WILL COUNTY  
SN 099-0001

PROJECT NO. 03095-16  
SCALE 1" = 20'  
DATE 6/25/09  
DRAWN BY CFC  
CHECKED BY MCB  
DRAWING NO. 8

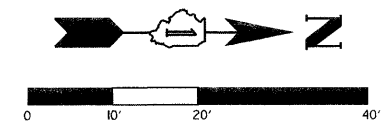
**COOMBE-BLOXDORF P.C.**  
Engineers / Land Surveyors  
Springfield, Illinois  
Design Firm License No. 184-002703

OF 19 SHTS

PLOT DATE = 6/25/2009  
PLOT SCALE = 2411.5294 1" = 71'  
USER NAME = JML



**PROPOSED DECK REHABILITATION AREAS**



**LEGEND**

|  |  |
|--|--|
|  | DECK SLAB REPAIR PARTIAL               |
|  | DECK SLAB REPAIR (FULL DEPTH, TYPE II) |
|  | DECK SLAB REPAIR (FULL DEPTH, TYPE I)  |

**NOTES:**

Deck Drains that are Located in Areas of Full Depth Repair Shall be Replaced as approved by the Engineer. Cost Included with Deck Slab Repair (Full Depth, Type I) or Deck Slab Repair (Full Depth, Type II) as Applicable.

The Areas of Deck Slab Repair are Based on Results of an Infrared Deck Survey Conducted on 04/16/09.

Areas of Deck Slab Repair (Partial) are shown for Information only to assist the Contractor in bidding.

Areas of repair that are located within 10 feet of the parapet face should be repaired in maximum lengths of 10' at a time to ensure stability at the parapet.

**BILL OF MATERIAL**

| ITEM                                   | UNIT  | TOTAL |
|--|-------|-------|
| DECK SLAB REPAIR (FULL DEPTH, TYPE I)  | Sq Yd | 4     |
| DECK SLAB REPAIR (FULL DEPTH, TYPE II) | Sq Yd | 114   |

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE

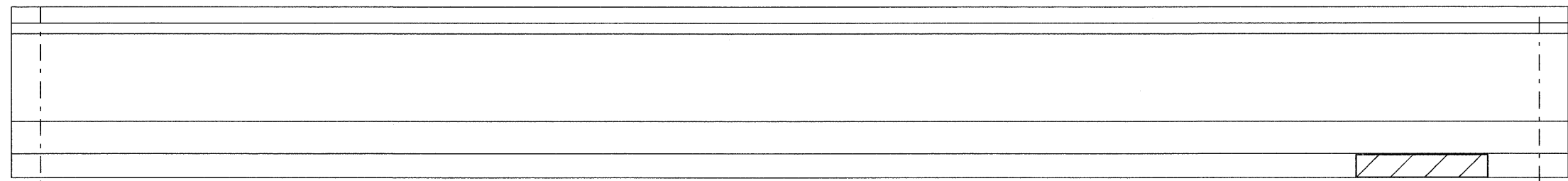
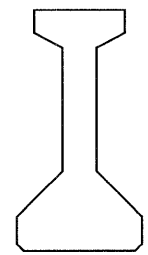
**DECK SLAB REPAIR**

|             |  |             |          |
|-------------|--|-------------|----------|
| PROJECT     | I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001 | PROJECT NO. | 03095-16 |
| SCALE       |  | DATE        | 6/25/09  |
| DRAWN BY    | CFC  | CHECKED BY  | MCB      |
| DRAWING NO. |  |             |          |

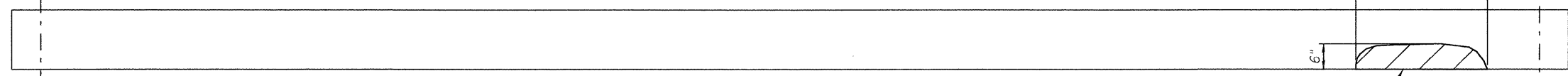
**COOMBE-BLOXDORF P.C.**  
Engineers / Land Surveyors  
Springfield, Illinois  
Design Firm License No. 184-002703

9 OF 19 SHTS

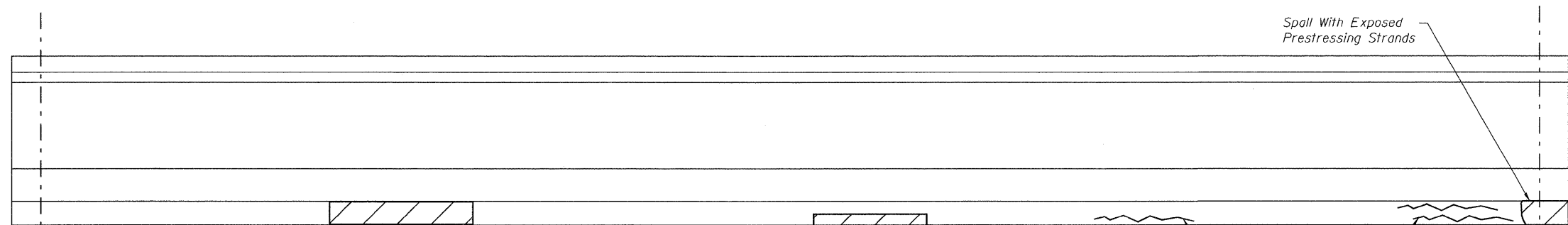
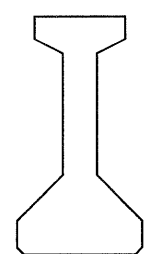
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 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = JML



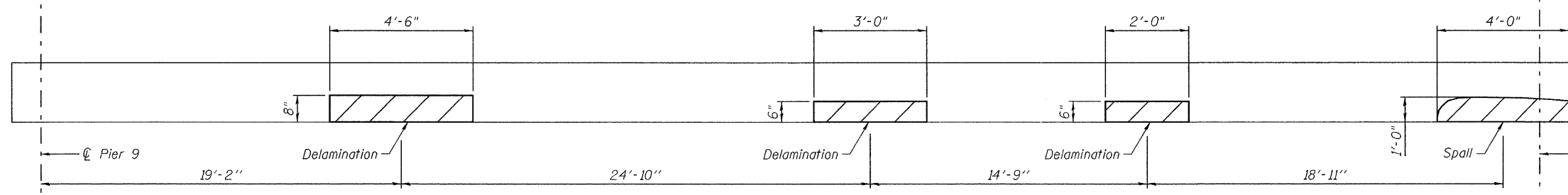
**ELEVATION**  
(Looking West)



**BOTTOM FLANGE  
PLAN VIEW  
BEAM 1, SPAN 8**



**ELEVATION**  
(Looking West)



**BOTTOM FLANGE  
PLAN VIEW  
BEAM 1, SPAN 9**

**LEGEND**

Precast Prestressed Concrete  
I-Beam Repair

**BILL OF MATERIAL**

| ITEM  | UNIT  | TOTAL |
|---|-------|-------|
| Precast Prestressed Concrete<br>I-Beam Repair | Sq Ft | 11.5  |

Note:  
See Sheet 11 of 19 for preloading requirements.

ILLINOIS DEPARTMENT OF TRANSPORTATION

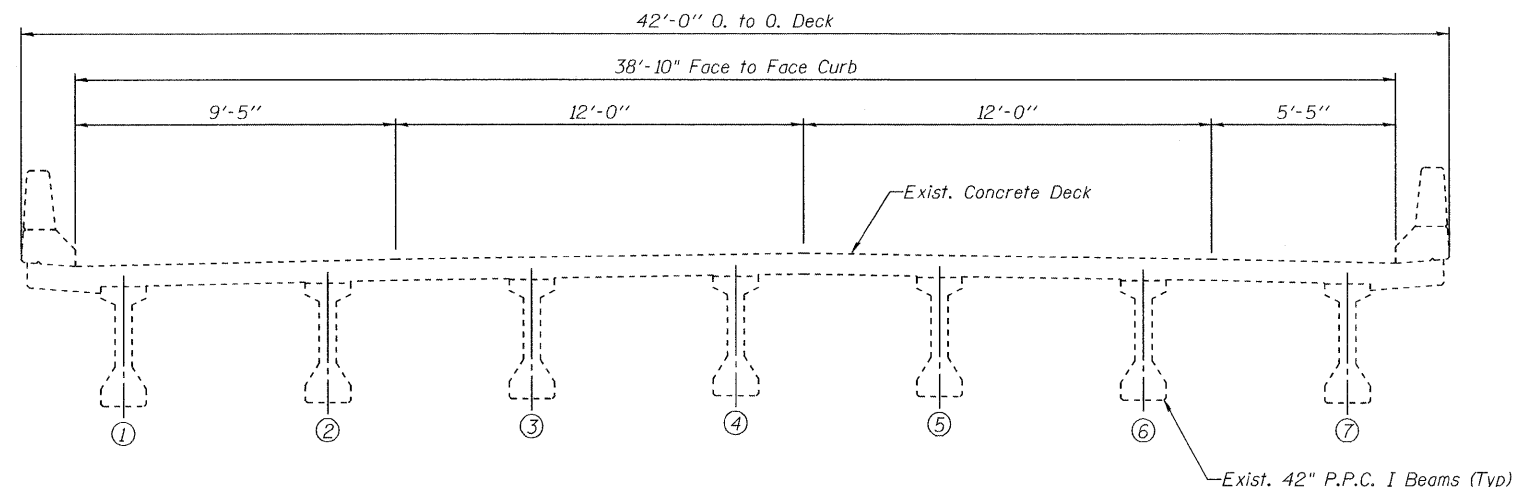
SHEET TITLE  
**BEAM REPAIR DETAILS**

|   |   |
|---|---|
| PROJECT<br>I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001 | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>06/25/09<br>DRAWN BY<br>CFC<br>CHECKED BY<br>MCB/KPS<br>DRAWING NO. |
|---|---|

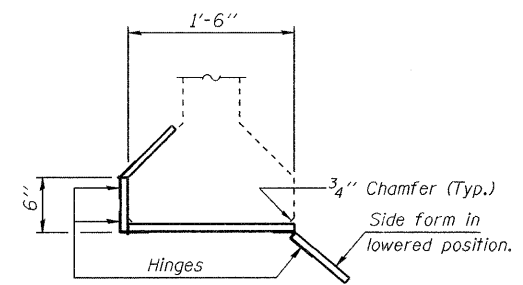
**COOMBE-BLOXDORF P.C.**  
Engineers / Land Surveyors  
Springfield, Illinois  
Design Firm License No. 184-002703

10  
OF 19 SHTS

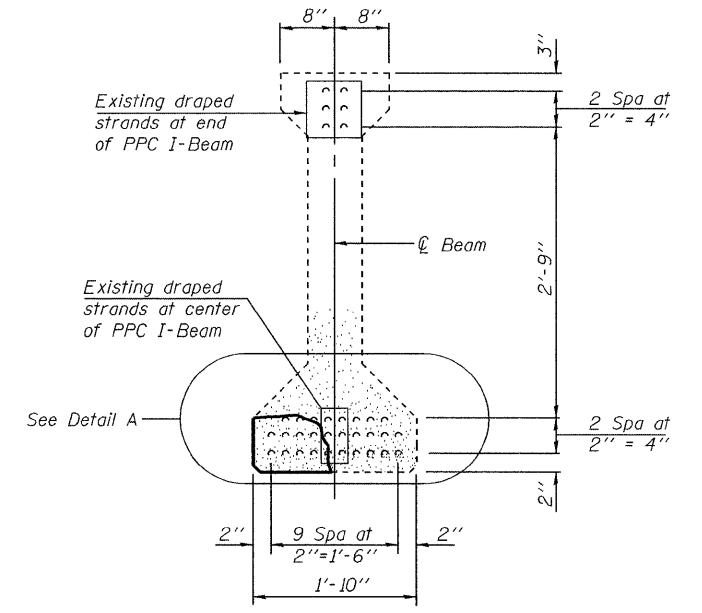
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 USER NAME = JML



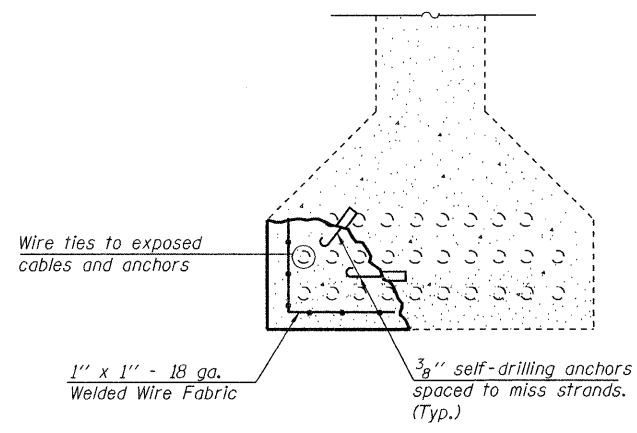
**EXISTING CROSS SECTION**  
(Looking South)



**SUGGESTED FORM DETAIL**



**TYPICAL PATCHING DETAIL**  
Beam 1, Spans 8 and 9



**DETAIL A**

**REPAIR PROCEDURES FOR BEAM 1**  
(SPANS 8 AND 9)

- Beam repairs shall be done before addition of the Bridge Deck Overlay.
- The damaged area of the beam shall be cleaned of all loose and spalled concrete and sealant. All loose material shall be removed to sound concrete until coarse aggregate will break under chipping rather than dislodging. Hand tools shall be used for the removal of concrete adjacent to the prestressing strands. While a 15 pound chipping hammer may be used away from prestressing strands, extreme care shall be taken not to damage the exposed prestressing strands.
- Using the same tools, remove the existing concrete to sound concrete, as described above, along the edges of the damaged area to a depth of 1" min. to 1 1/2" max. The edges shall be saw cut 3/4" deep. The entire area of existing concrete against which new concrete will be placed and any exposed portions of the prestressing strands shall be sandblasted. The concrete shall be sandblasted to expose clean, well bonded aggregate.
- Power driven pins as shown in Detail A shall be placed at 9" alternate centers along damaged length of beam at locations shown in Detail A. Place 1" x 1" x 18 gauge welded wire fabric in repair areas and attach it to the pins or strands with wire ties. The clearance between the finished surface of the new concrete and the welded wire fabric shall be 1" minimum. All beams involved in this work shall be rebuilt to their original dimensions.
- The surface of the existing concrete against which new concrete will be placed shall be prepared as a bonded construction joint according to Article 503.09(b) of the Standard Specifications. Other minor mortar repair, crack sealing or surface sealing of gouges on the beam shall be performed as directed by the Engineer.
- The repair shall be made using a material from the "Approved List of Non-Shrink Grouts" maintained by the Bureau of Materials and Physical Research. The repair material chosen shall be appropriate for the thickness of repair to be made. Coarse aggregate with maximum size of 3/8" shall be added with the amount as specified by the manufacturer. Place the lower form on the bottom of the beam and compact by vibrating (or other approved methods) the mix into the voids.
- Preloading, if specified, and forms shall be kept in place until the repair material has reached an ultimate strength of 5,000 psi. Timing of form removal shall be modified as necessary to meet curing requirements as specified by the manufacturer.

**PRELOADING FOR PPC I-BEAM REPAIRS**  
(Service Moment)

**BEAM 1**

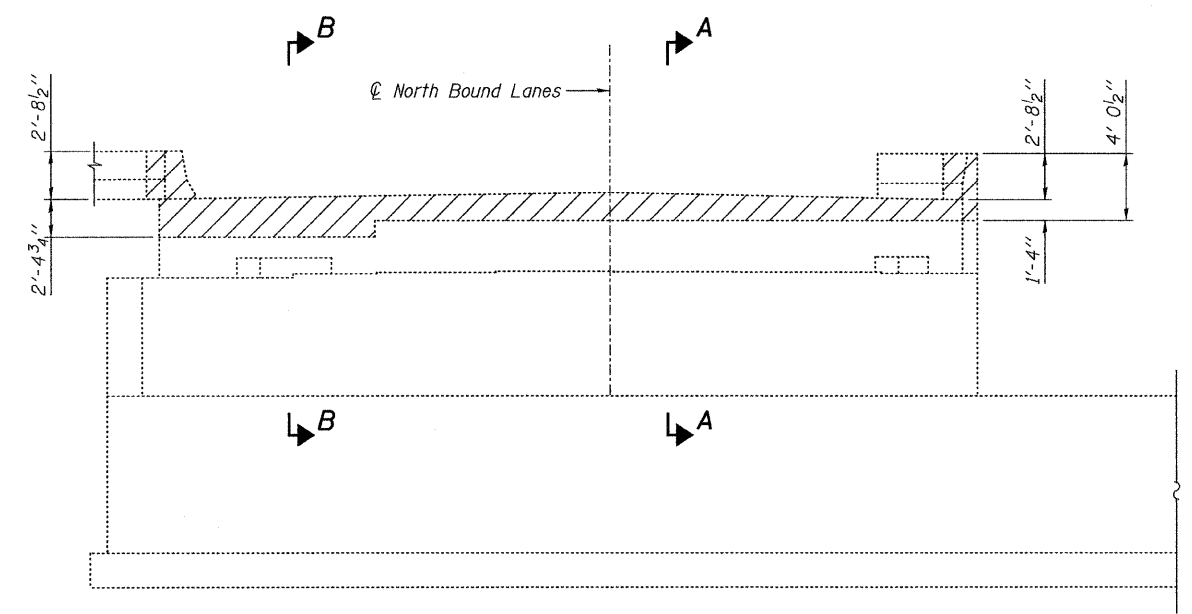
| Span | Location |           | Moment (kip-ft) |
|------|----------|-----------|-----------------|
|      | From     | Distance* |                 |
| 9    | ℄ Pier 9 | 19'-2"    | 320             |
| 9    | ℄ Pier 9 | 44'-0"    | 649             |
| 9    | ℄ Pier 8 | 21'-0"    | 547             |

\*To center of repair area. If actual distance varies by more than 2' from distance shown notify the Engineer to determine Moment required to preload.

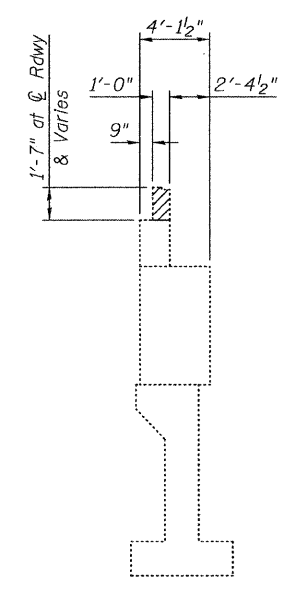
**NOTE**  
The cost of concrete removal, Class PS Concrete, power Driven pins, wire ties, wire mesh, epoxy bonding agent, Epoxy Crack Injection and all other work required to perform repairs on Beam 1 in Spans 8 and 9 shall be included in the unit cost per sq. ft. for Precast Prestressed Concrete I-beam repair

|                                       |                             |             |             |
|---------------------------------------|-----------------------------|-------------|-------------|
| ILLINOIS DEPARTMENT OF TRANSPORTATION |                             |             |             |
| SHEET TITLE                           |                             |             |             |
| BEAM PRELOADING DETAILS               |                             |             |             |
| PROJECT                               | I-55 NB OVER KANKAKEE RIVER | PROJECT NO. | 03095-16    |
| FAI ROUTE 55, SECTION 88(B&B-1)BR     | WILL COUNTY                 | SCALE       | AS SHOWN    |
| SN 099-0001                           |                             | DATE        | 6/25/09     |
|                                       |                             | DRAWN BY    | TFG         |
|                                       |                             | CHECKED BY  | MCB         |
| COOMBE-BLOXDORF P.C.                  |                             |             | DRAWING NO. |
| Engineers / Land Surveyors            |                             |             | 11          |
| Springfield, Illinois                 |                             |             | OF 19 SHTS  |
| Design Firm License No. 184-002703    |                             |             |             |

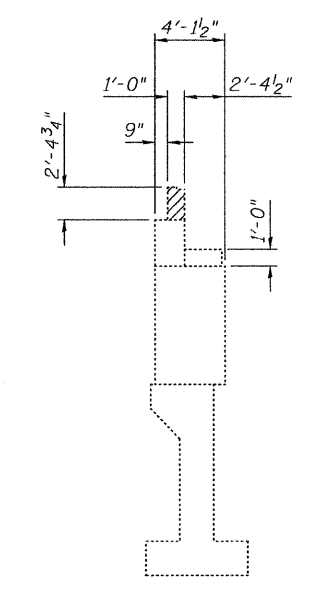
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PLOT SCALE = 0.25000 1" = 1'-0"  
USER NAME = JML



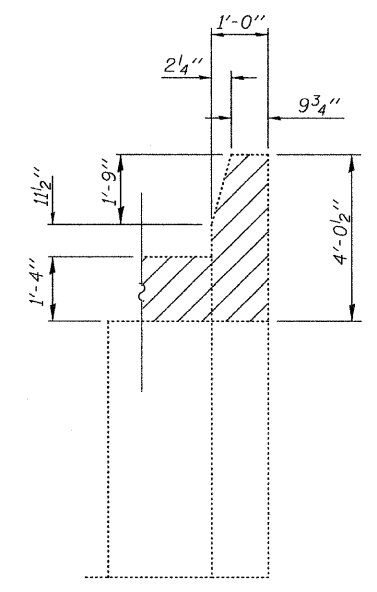
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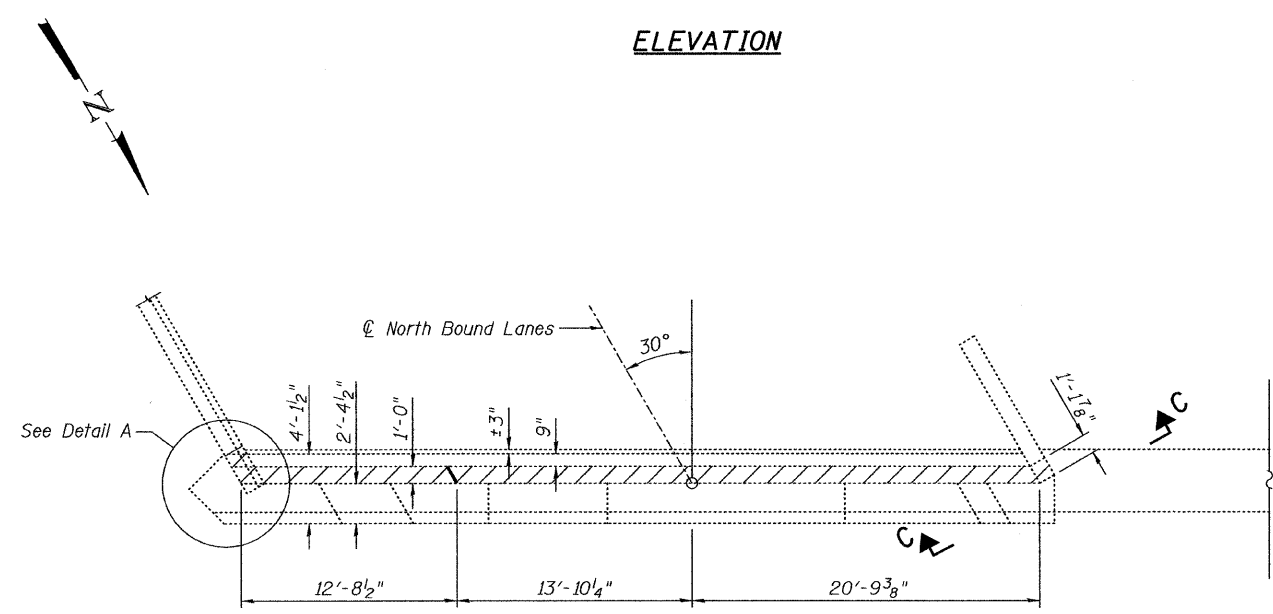
**SECTION A-A**



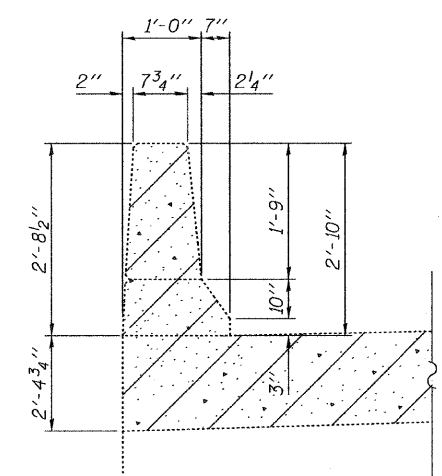
**SECTION B-B**



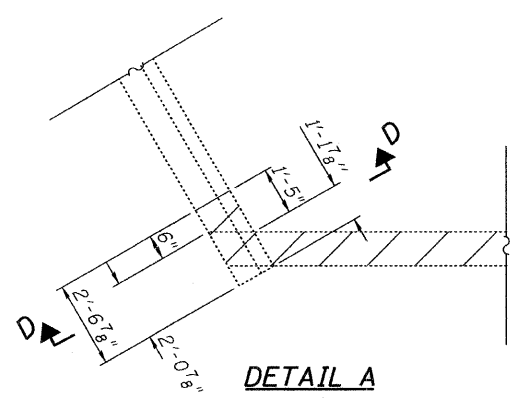
**SECTION C-C**



**PLAN**



**SECTION D-D**



**DETAIL A**

**BILL OF MATERIAL**

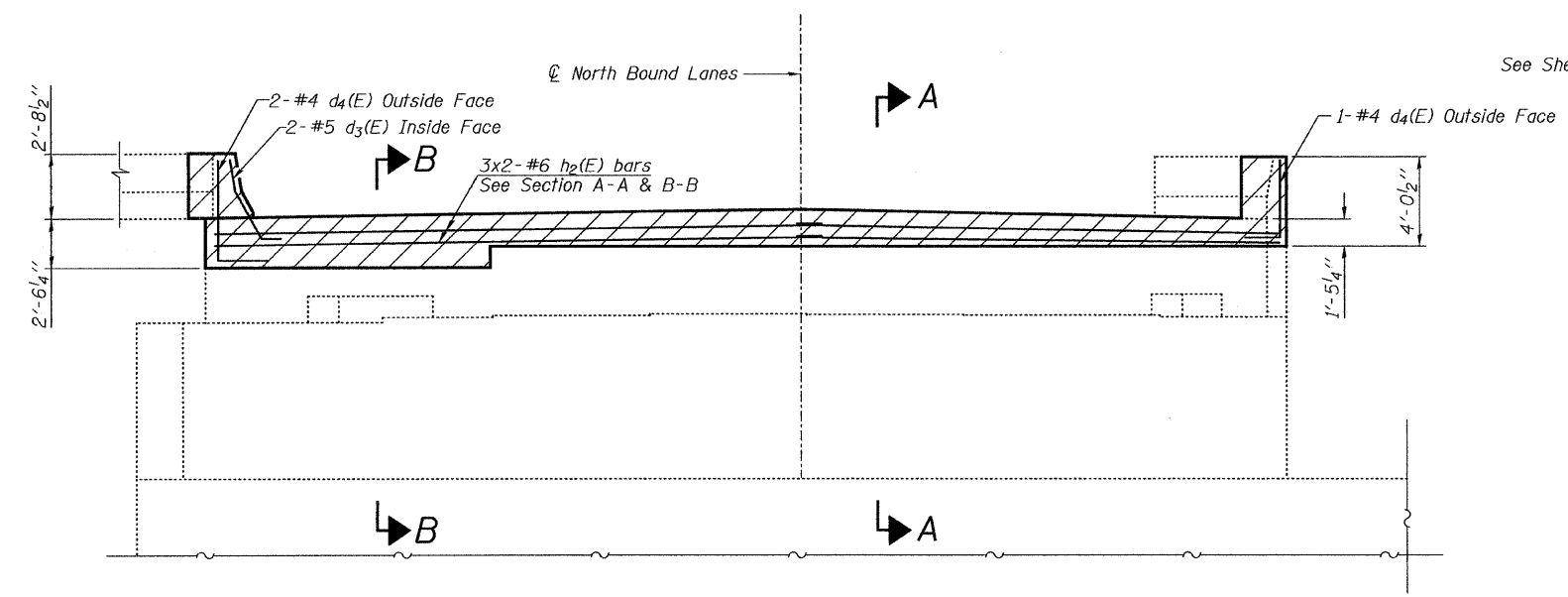
| Item             | Unit  | Total |
|------------------|-------|-------|
| Concrete Removal | Cu Yd | 3.4   |

**NOTES:**

Hatched areas indicate Concrete Removal.  
 Existing vertical reinforcement extending into the New Construction shall be cleaned, straightened and incorporated into the new construction and existing horizontal reinforcement at the wingwalls extending into the new construction shall be cleaned, straightened and incorporated into the new construction. See Sheet 13 of 19. Cost included with Concrete Removal.  
 Any reinforcement bars extending into the new construction that are damaged during Concrete Removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
 Existing reinforcement not extending into areas of new construction shall be cut at the removal line and removed. Exposed portion will be cleaned and coated with a layer of epoxy. Cost included with Concrete Removal.

|   |  |
|---|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION   |  |
| SHEET TITLE<br>SOUTH ABUTMENT CONCRETE REMOVAL  |  |
| PROJECT<br>I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001         | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>9/23/09<br>DRAWN BY<br>TFG/CEC<br>CHECKED BY<br>MCB<br>DRAWING NO. |
| COOMBE-BLOXDORF P.C.<br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 | 12<br>OF 19 SHTS   |

PLOT DATE = 11/23/2009  
 FILE NAME = ...  
 PLOT SCALE = 5/8" = 1' / IN.  
 USER NAME = CFC.

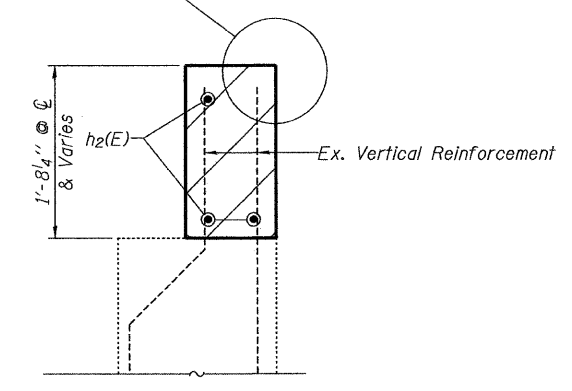


**ELEVATION**

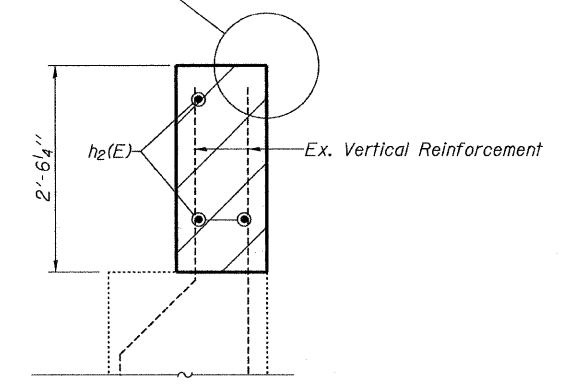
**MIN. BAR LAP**  
#6 bar = 2'-7"

See Sheet 6 of 19 for Exp JT Details

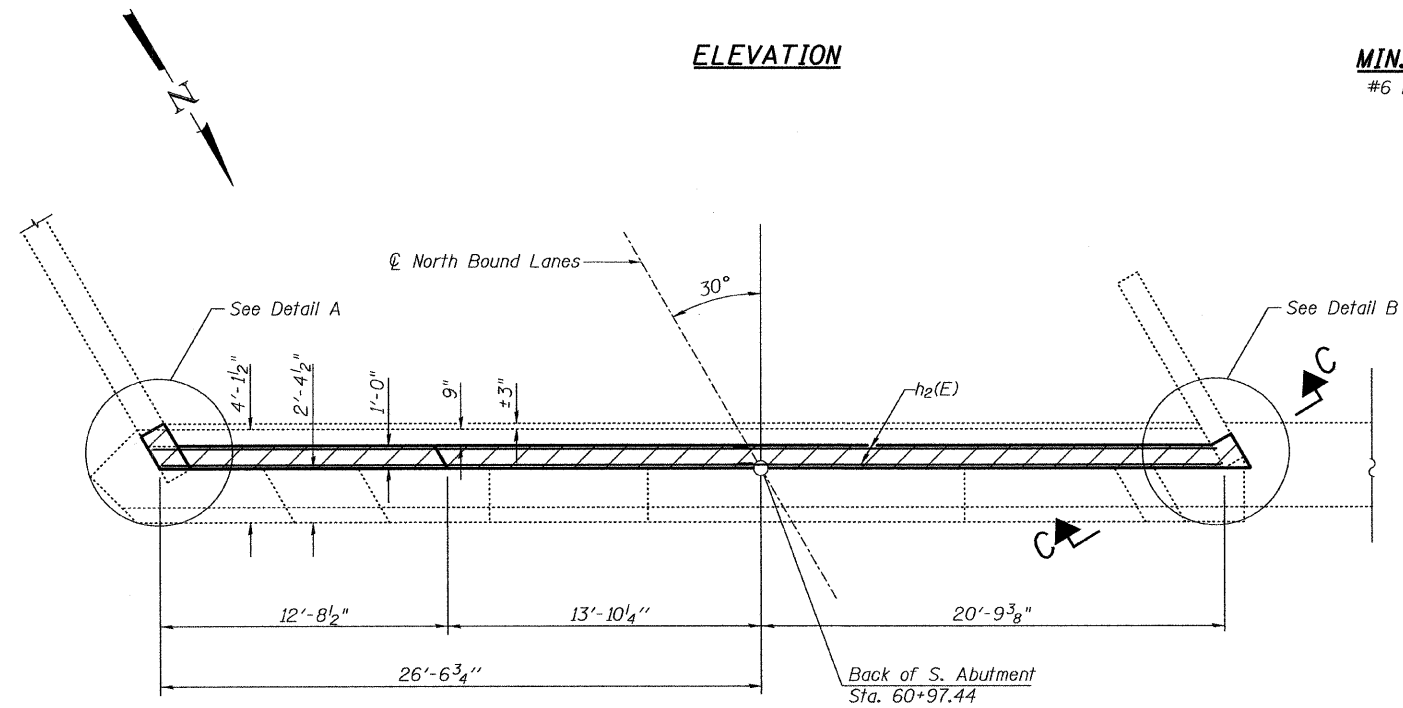
See Sheet 6 of 19 for Exp JT Details



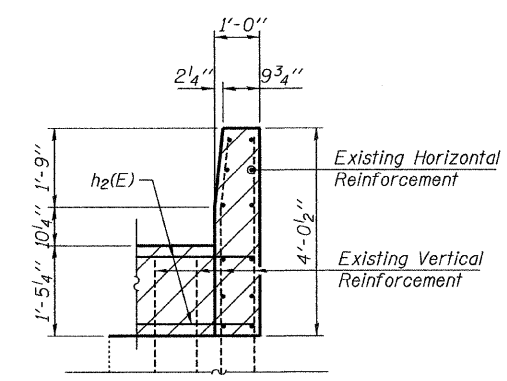
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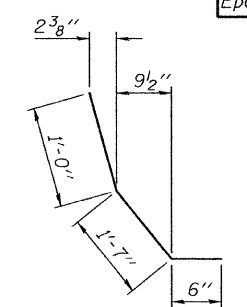
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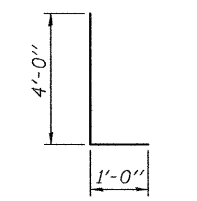
**PLAN**



**SECTION C-C**



**BAR d3(E)**



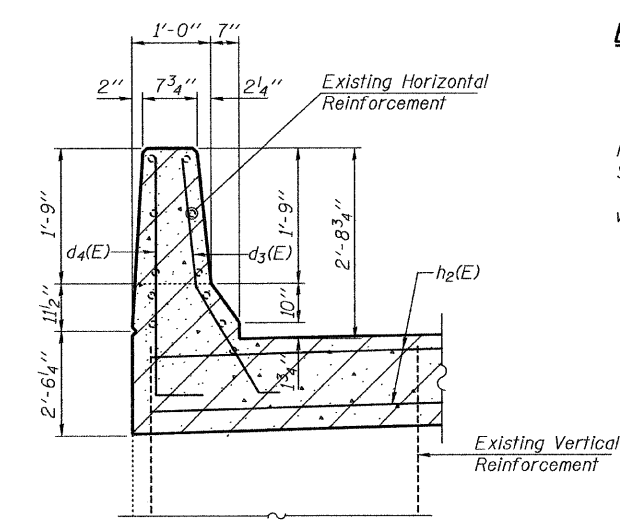
**BAR d4(E)**

**BILL OF MATERIAL**

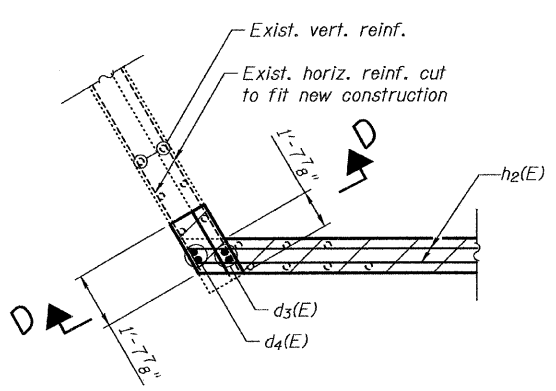
| Bar                              | No. | Size | Length | Shape |
|----------------------------------|-----|------|--------|-------|
| h2(E)                            | 6   | #6   | 25'-5" |       |
| d3(E)                            | 2   | #5   | 3'-1"  |       |
| d4(E)                            | 3   | #4   | 5'-0"  |       |
| Reinforcement Bars, Epoxy Coated |     |      | Pound  | 240   |

**NOTES:**

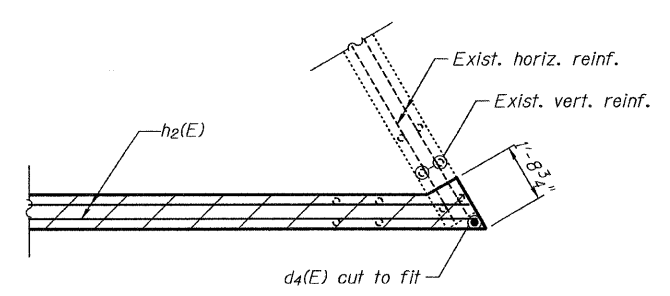
Pour hatched area after proposed deck is in place. Match top of hatched area to top of deck. Quantity included with Concrete Superstructure Sheet 4 of 19.  
Bars indicated thus 3x2-#6 h2(E) indicates 3 lines of bars with 2 lengths per line.



**SECTION D-D**



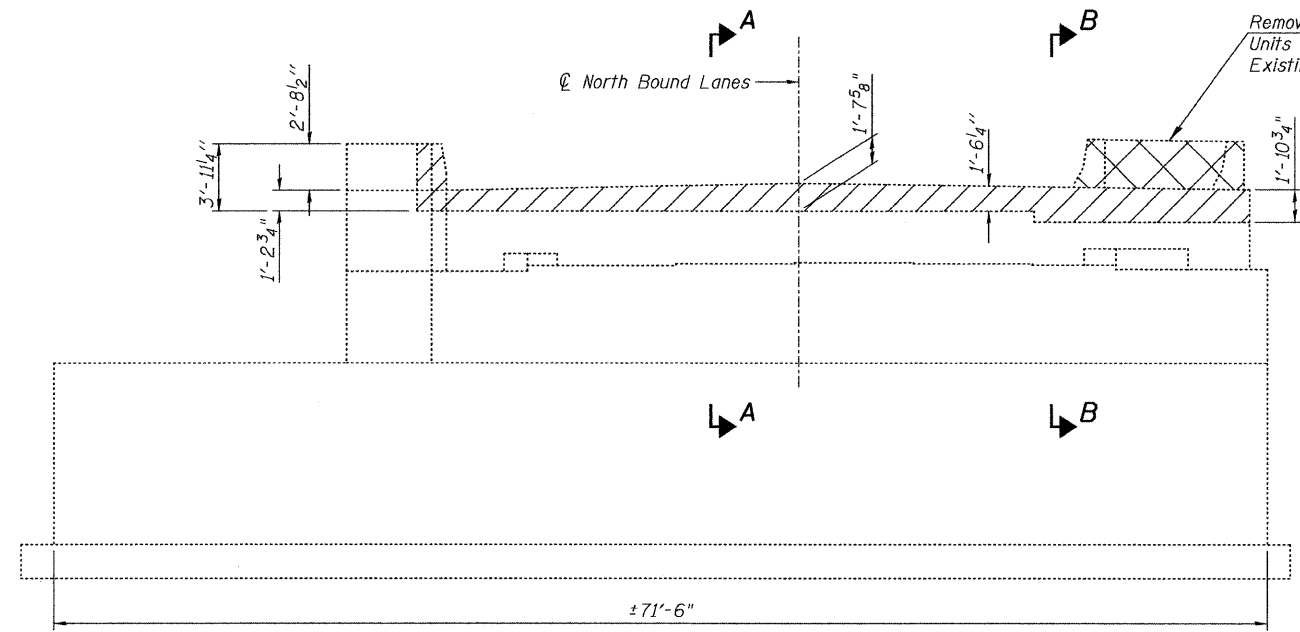
**DETAIL A**



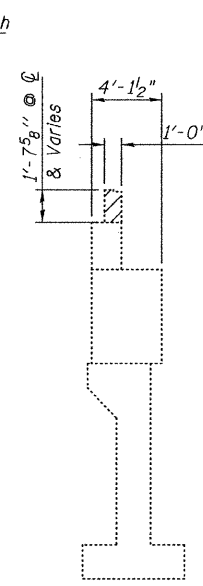
**DETAIL B**

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FILE NAME = ...88-099-000-ent-13-south-abut-detailed.dgn  
PLOT SCALE = 400000 1" = 100'  
USER NAME = DFC

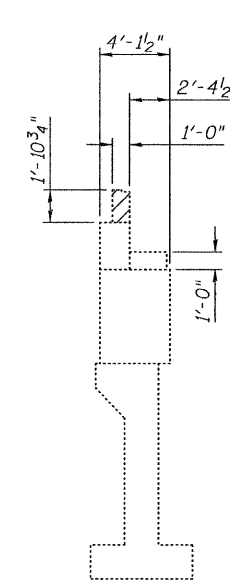
|                                       |             |
|---------------------------------------|-------------|
| ILLINOIS DEPARTMENT OF TRANSPORTATION |             |
| SHEET TITLE                           |             |
| SOUTH ABUTMENT DETAILS                |             |
| PROJECT                               | PROJECT NO. |
| I-55 NB OVER KANKAKEE RIVER           | 03095-16    |
| FAI ROUTE 55, SECTION BB(B&B-1)BR     | DATE        |
| WILL COUNTY                           | 9/23/09     |
| SN 099-0001                           | DRAWN BY    |
|                                       | TFC         |
|                                       | CHECKED BY  |
|                                       | MCB         |
| DRAWING NO.                           |             |
| COOMBE-BLOXDORF P.C.                  | 13          |
| Engineers / Land Surveyors            |             |
| Springfield, Illinois                 |             |
| Design Firm License No. 184-002703    | OF 19 SHTS  |



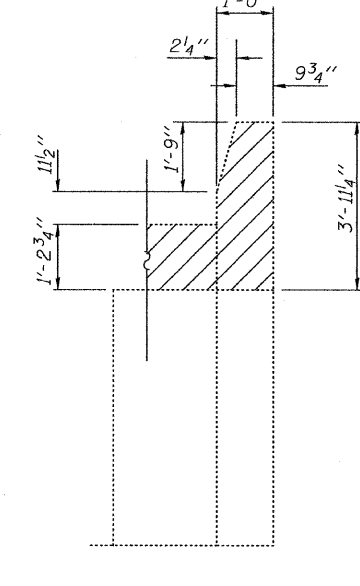
**ELEVATION**



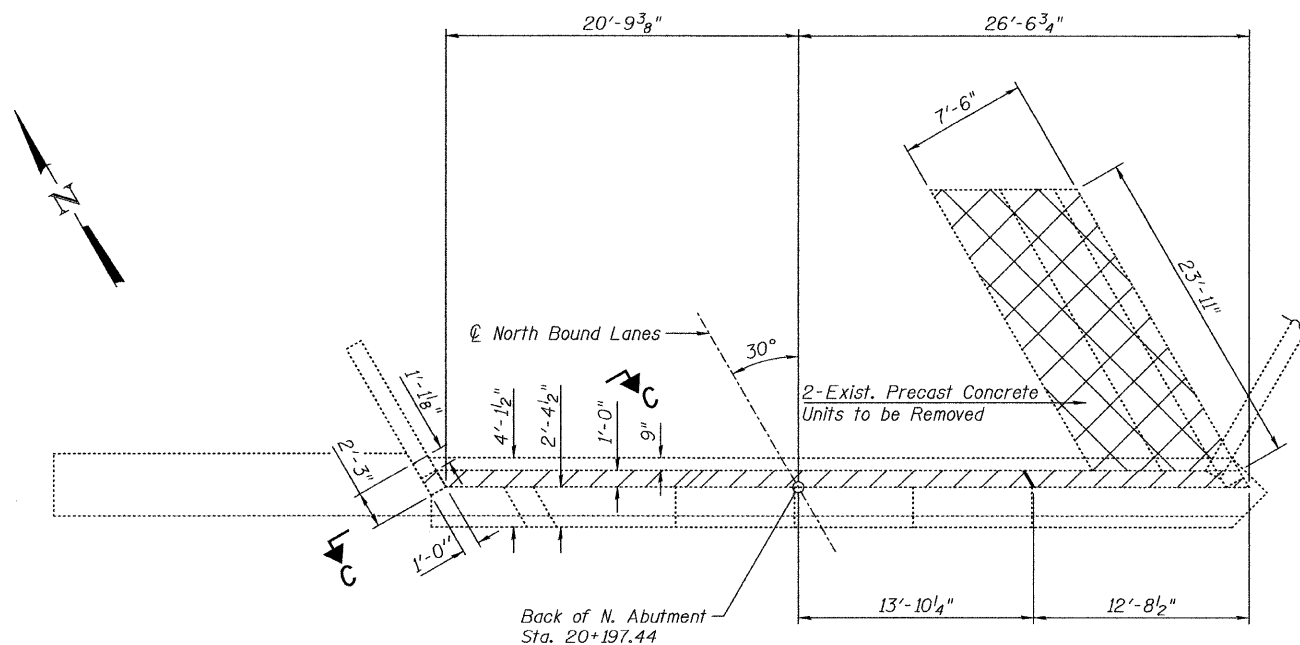
**SECTION A-A**



**SECTION B-B**



**SECTION C-C**



**PLAN**

**BILL OF MATERIAL**

| Item                                       | Unit  | Total |
|--|-------|-------|
| Concrete Removal                           | Cu Yd | 2.9   |
| Removal of Existing Precast Concrete Units | Sq Ft | 180   |

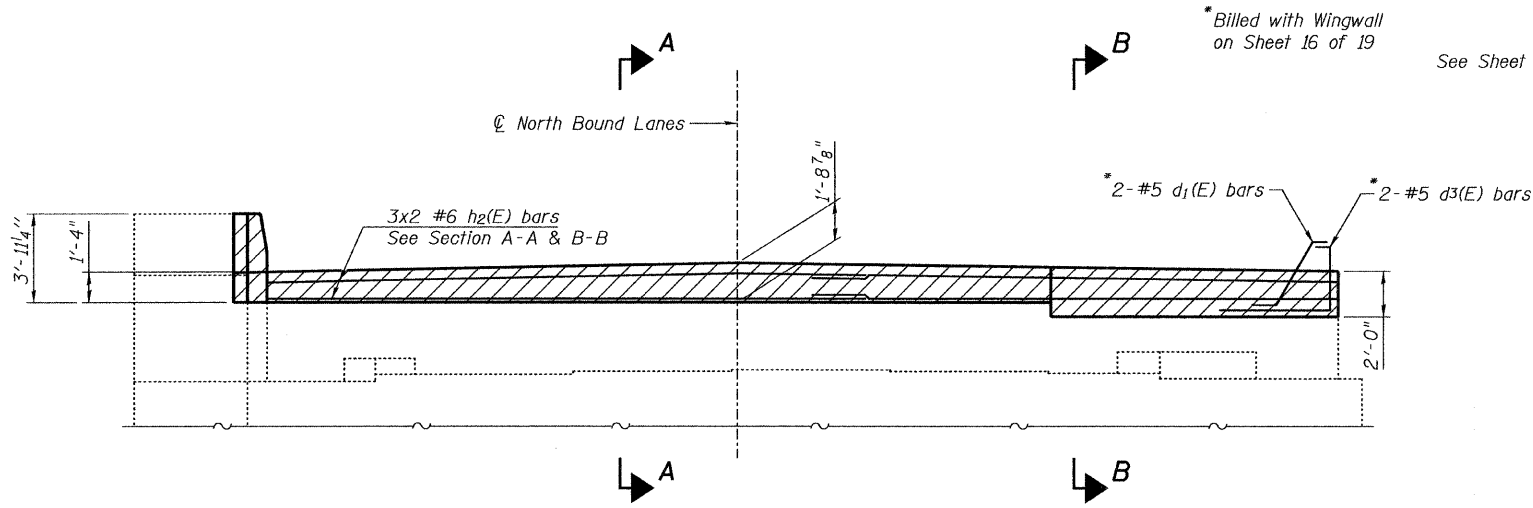
**NOTES:**

Hatched areas indicate Concrete Removal.  
 Cross Hatched areas indicate Removal of Existing Precast Concrete Units.  
 Existing vertical reinforcement extending into the New Construction shall be cleaned, straightened and incorporated into the new construction and existing horizontal reinforcement at the west wingwall extending into the new construction shall be cleaned, straightened and incorporated into the new construction. See Sheet 15 of 19. Cost included with Concrete Removal.  
 Any reinforcement bars extending into the new construction that are damaged during Concrete Removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
 Existing reinforcement not extending into areas of new construction shall be cut at the removal line and removed. Exposed portion will be cleaned and coated with a layer of epoxy. Cost included with Concrete Removal.

PLOT DATE = 11/23/2009  
 FILE NAME = \\snp-099-0001\ent-14-north-abut-concrete-removal.dgn  
 PLOT SCALE = 54,0000 1/1 IN.  
 USER NAME = CFC

|   |  |
|---|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION   |  |
| SHEET TITLE<br>NORTH ABUTMENT CONCRETE REMOVAL  |  |
| PROJECT<br>I-55 SB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001         | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>9/23/09<br>DRAWN BY<br>TFC/CFC<br>CHECKED BY<br>MCB<br>DRAWING NO. |
| COOMBE-BLOXDORF P.C.<br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 | 14<br>OF 19 SHTS   |

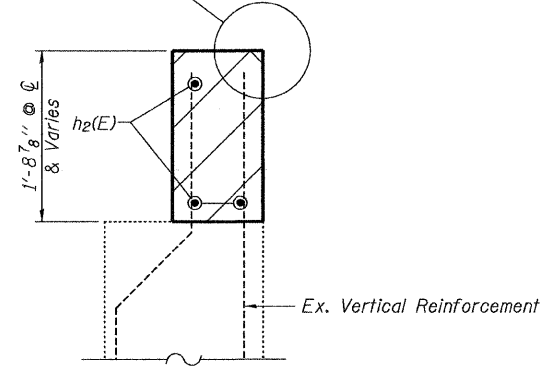




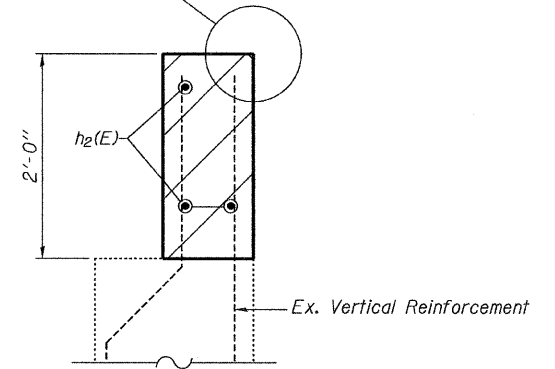
**ELEVATION**

See Sheet 6 of 19 for Exp JT Details

See Sheet 6 of 19 for Exp JT Details



**SECTION A-A**

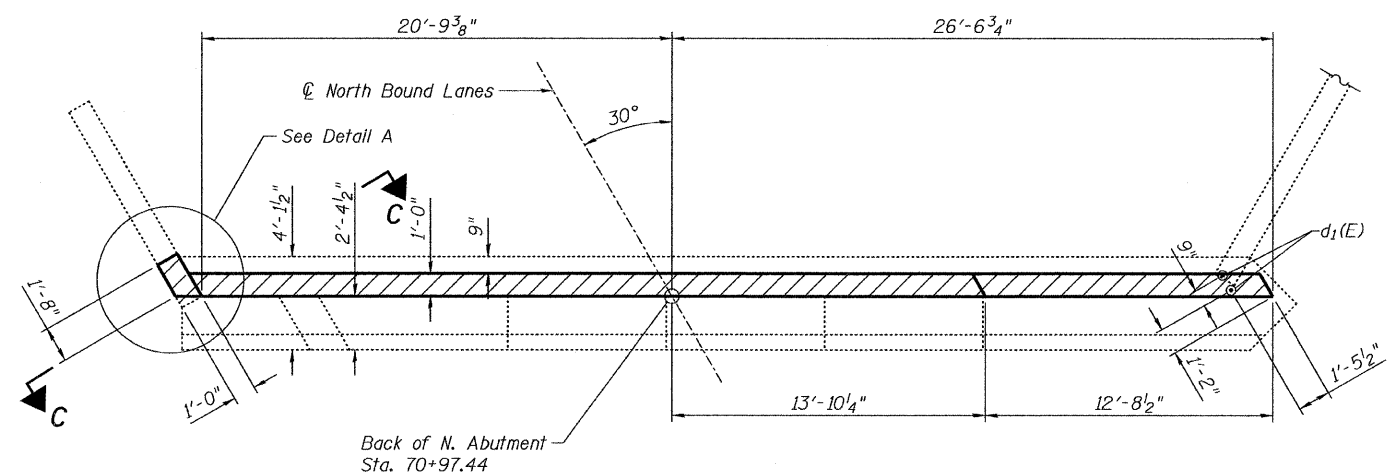


**SECTION B-B**

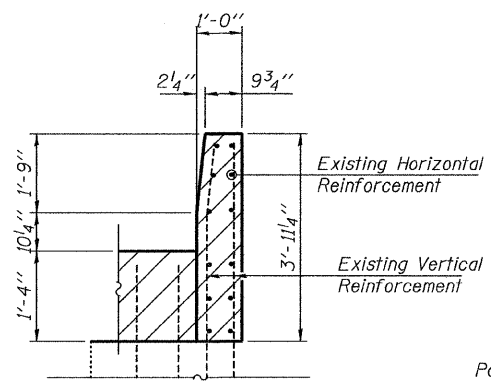
**MIN. BAR LAP**  
#6 bar = 2'-7"

**BILL OF MATERIAL**

| Bar                              | No. | Size | Length | Shape     |
|----------------------------------|-----|------|--------|-----------|
| h2(E)                            | 6   | #6   | 25'-5" |           |
| Reinforcement Bars, Epoxy Coated |     |      |        | Pound 230 |



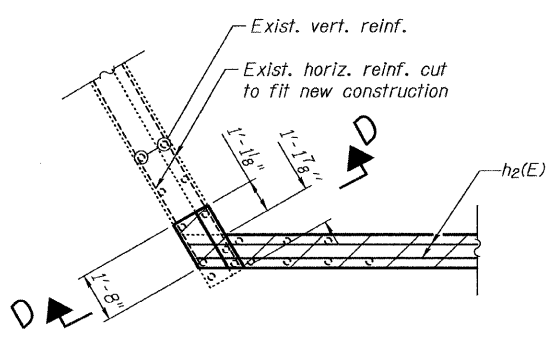
**PLAN**



**SECTION C-C**

**NOTES:**

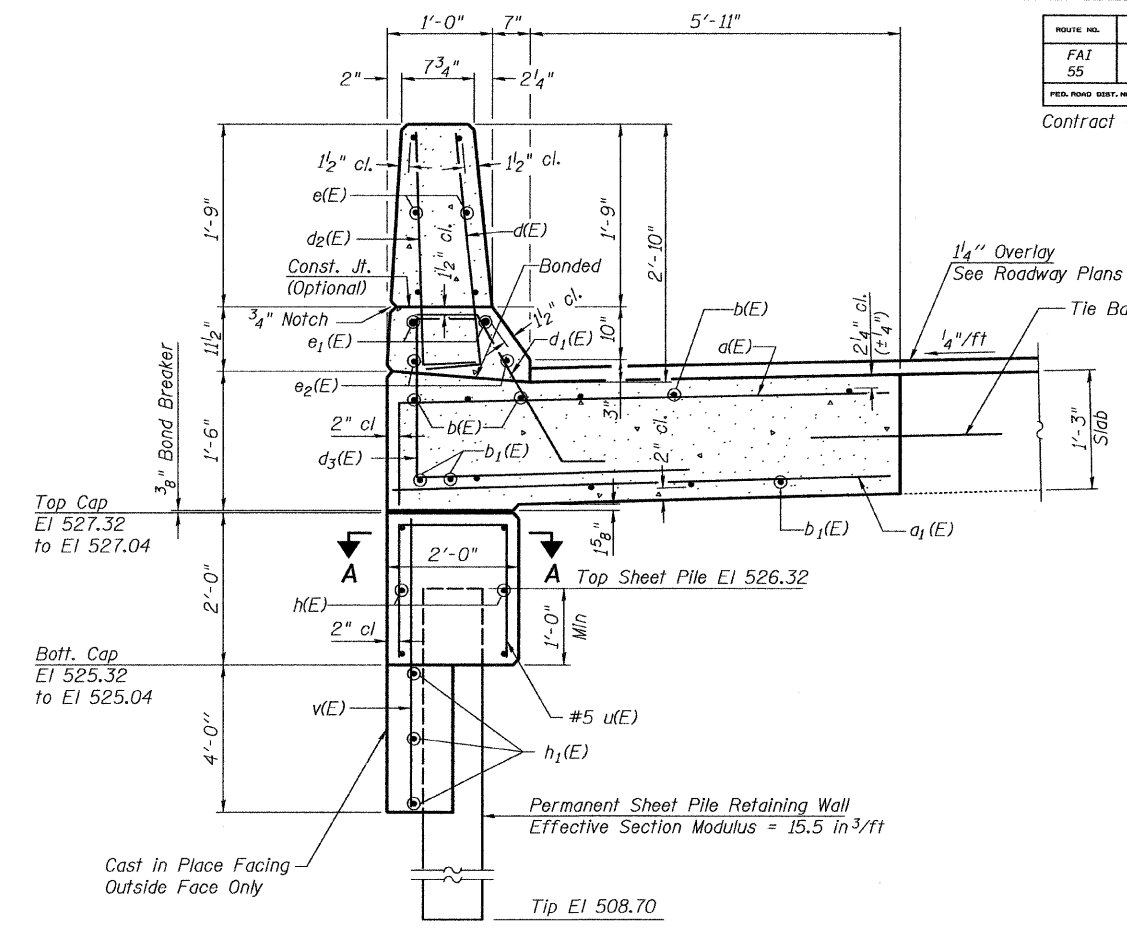
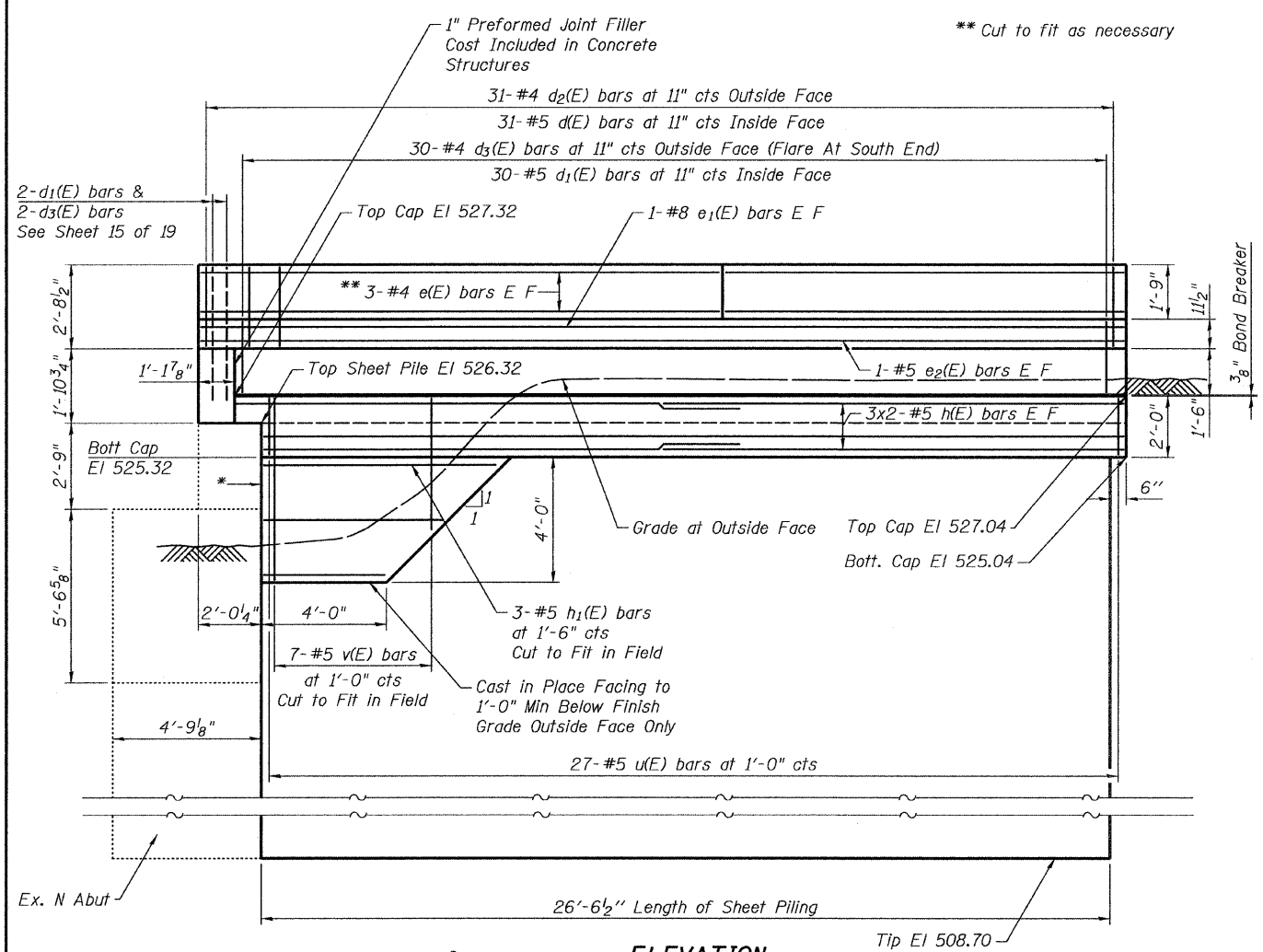
Pour hatched area after proposed deck is in place. Match top of hatched area to top of deck. Quantity included with Concrete Superstructure Sheet 4 of 19.  
Bars Indicated thus 3x2-#6 h2(E) indicates 3 lines of bars with 2 lengths per line.



**DETAIL A**

PLOT DATE = 11/23/2009  
FILE NAME = \\ben-099-0001-ent-15-north-abut-detailed.dgn  
PLOT SCALE = 400.0000 1/1 IN.  
USER NAME = CFC

|                                       |             |
|---------------------------------------|-------------|
| ILLINOIS DEPARTMENT OF TRANSPORTATION |             |
| SHEET TITLE                           |             |
| NORTH ABUTMENT DETAILS                |             |
| PROJECT                               | PROJECT NO. |
| I-55 NB OVER KANKAKEE RIVER           | 03095-16    |
| FAI ROUTE 55, SECTION BB(B&B-1)BR     | DATE        |
| WILL COUNTY                           | 9/23/09     |
| SN 099-0001                           | DRAWN BY    |
|                                       | TFG/CFC     |
|                                       | CHECKED BY  |
|                                       | MCB         |
| DRAWING NO.                           |             |
| COOMBE-BLOXDORF P.C.                  |             |
| Engineers / Land Surveyors            |             |
| Springfield, Illinois                 |             |
| Design Firm License No. 184-002703    |             |
| 15                                    | OF 19 SHTS  |

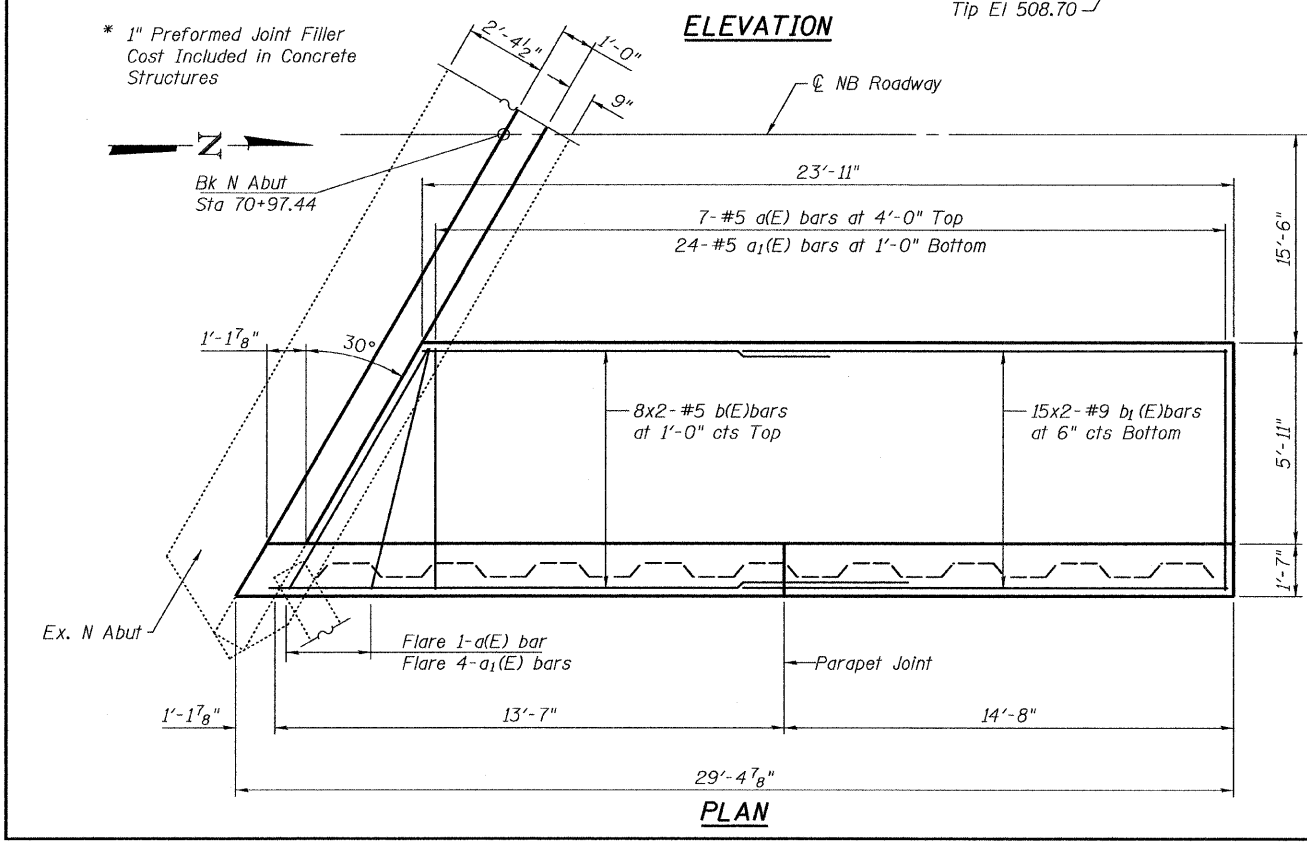


**BILL OF MATERIAL**

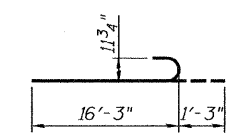
| Bar                              | No. | Size | Length  | Shape |
|----------------------------------|-----|------|---------|-------|
| d(E)                             | 8   | #5   | 8'-1"   | ┌     |
| a1(E)                            | 28  | #5   | 7'-2"   | ┌     |
| b(E)                             | 16  | #5   | 16'-4"  | ┌     |
| b1(E)                            | 30  | #9   | 17'-6"  | ┌     |
| d(E)                             | 31  | #5   | 3'-0"   | ┌     |
| d1(E)                            | 32  | #5   | 2'-8"   | ┌     |
| d2(E)                            | 31  | #4   | 3'-0"   | ┌     |
| d3(E)                            | 32  | #4   | 5'-0"   | ┌     |
| e(E)                             | 12  | #4   | 14'-4"  | ┌     |
| e1(E)                            | 2   | #8   | 28'-0"  | ┌     |
| e2(E)                            | 2   | #5   | 28'-0"  | ┌     |
| h(E)                             | 12  | #5   | 15'-3"  | ┌     |
| h1(E)                            | 3   | #5   | 7'-8"   | ┌     |
| u(E)                             | 27  | #5   | 5'-0"   | ┌     |
| v(E)                             | 7   | #5   | 5'-8"   | ┌     |
| Concrete Structures              |     |      | Cu. Yd. | 17.3  |
| Reinforcement Bars, Epoxy Coated |     |      | Pound   | 3400  |
| Structure Excavation             |     |      | Cu. Yd. | 10    |
| Permanent Steel Sheet Piling     |     |      | Sq. Ft. | 468   |
| Stud Shear Connectors            |     |      | Each    | 19    |
| Protective Coat                  |     |      | Sq. Yd. | 12    |

Bars Indicated Thus 3x2-#5 etc Indicates 3 Lines of Bars with 2 Lengths Per Line.

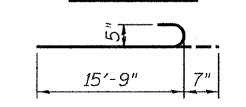
**SECTION THRU PARAPET ABOVE STEEL SHEET PILE WALL**



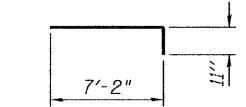
**BAR d(E) & d2(E)**



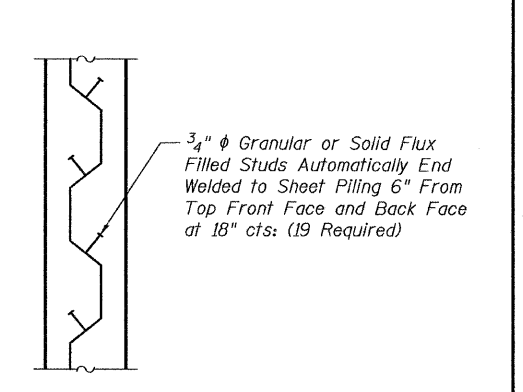
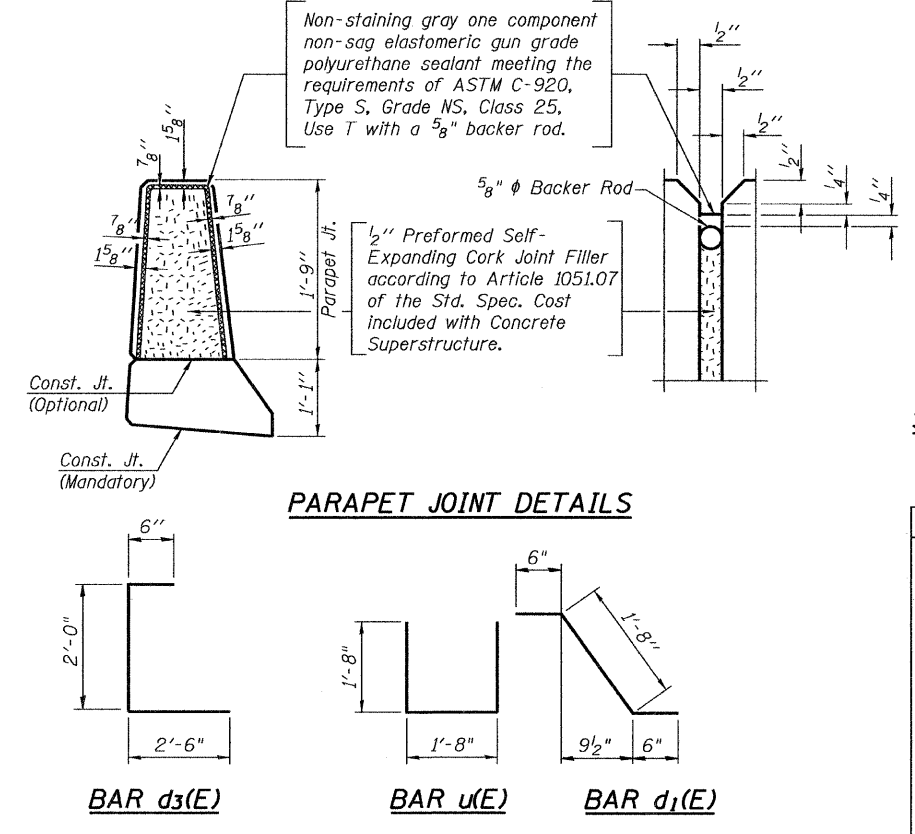
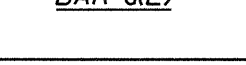
**BAR b1(E)**



**BAR b(E)**



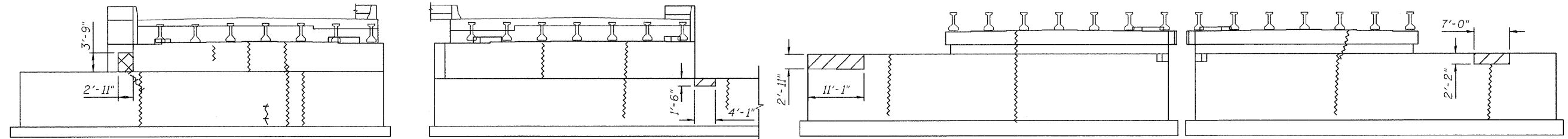
**BAR a(E)**



**SECTION A-A MIN BAR LAPS**

|  |  |
|--|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION  |  |
| SHEET TITLE<br>NORTH ABUTMENT WINGWALL DETAILS   |  |
| PROJECT<br>I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001                | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>9/23/09<br>DRAWN BY<br>TFG/CFC<br>CHECKED BY<br>MCB<br>DRAWING NO. |
| <b>COOMBE-BLOXDORF P.C.</b><br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 | 16<br>OF 19 SHTS   |

PLOT DATE = 11/23/2009  
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USER NAME = CFC

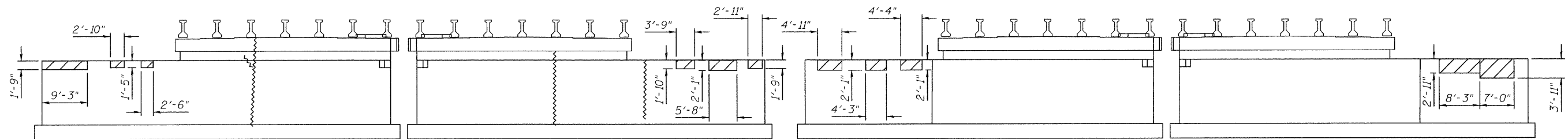


**NORTH ABUTMENT**  
**(LOOKING NORTH)**

**SOUTH ABUTMENT**  
**(LOOKING SOUTH)**

**PIER 1**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**

**PIER 1**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**

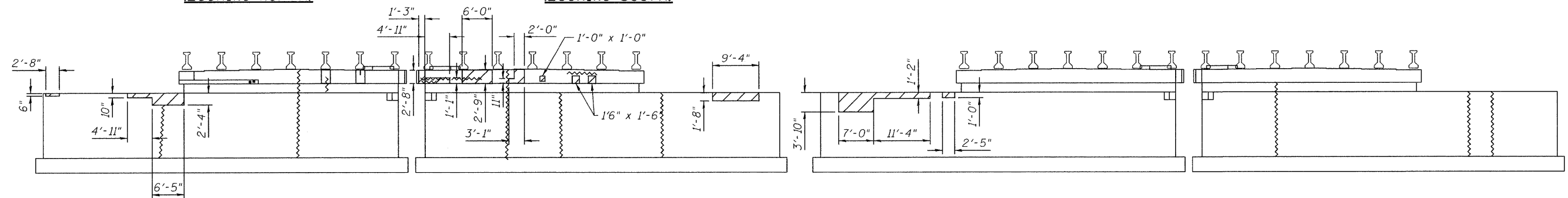


**PIER 2**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**

**PIER 2**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**

**PIER 3**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**

**PIER 3**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**



**PIER 4**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**

**PIER 4**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**

**PIER 5**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**

**PIER 5**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**

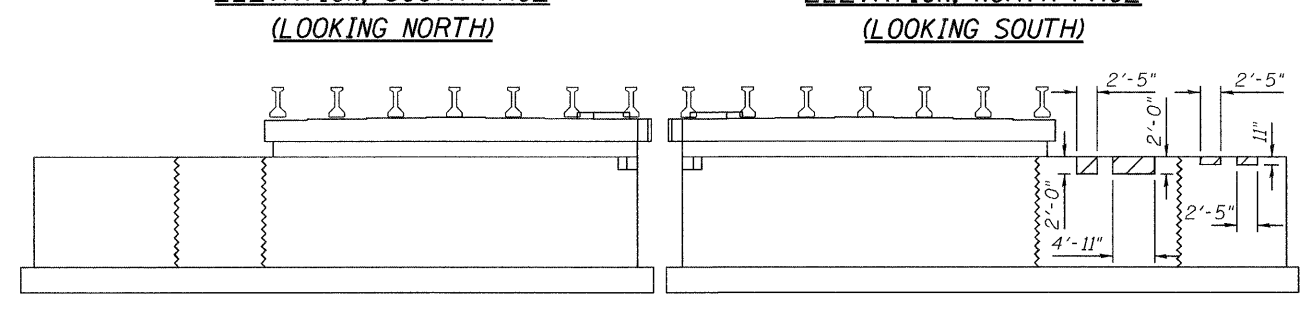
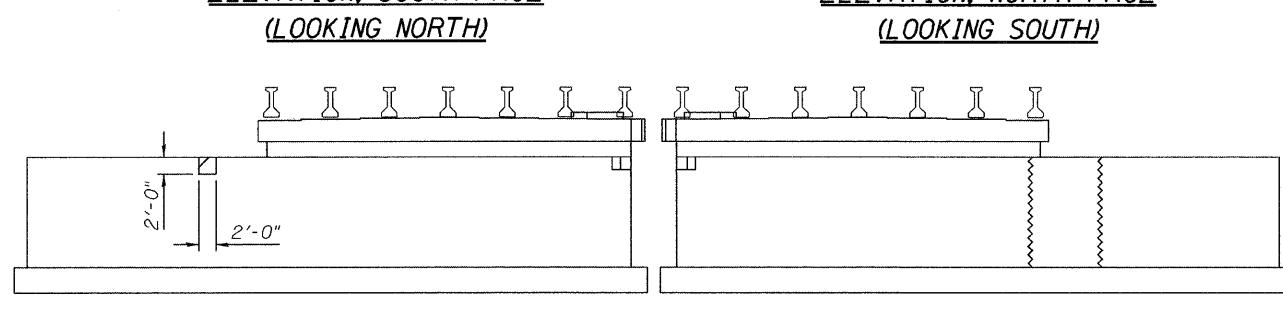
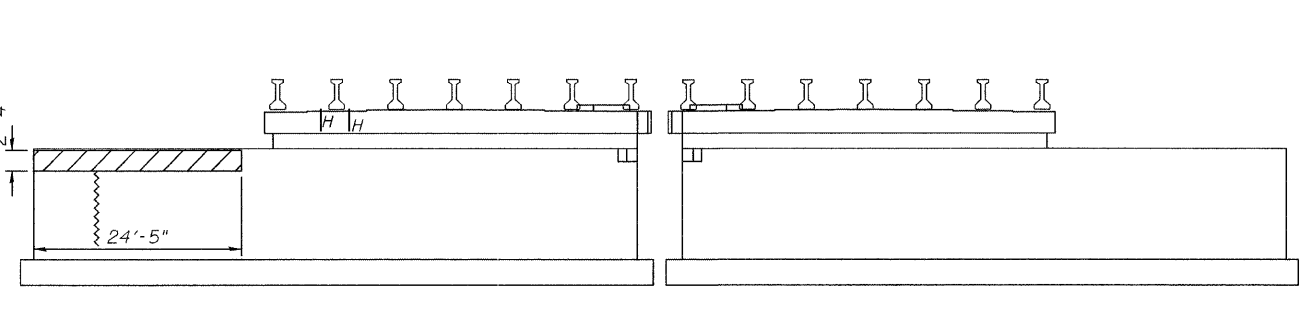
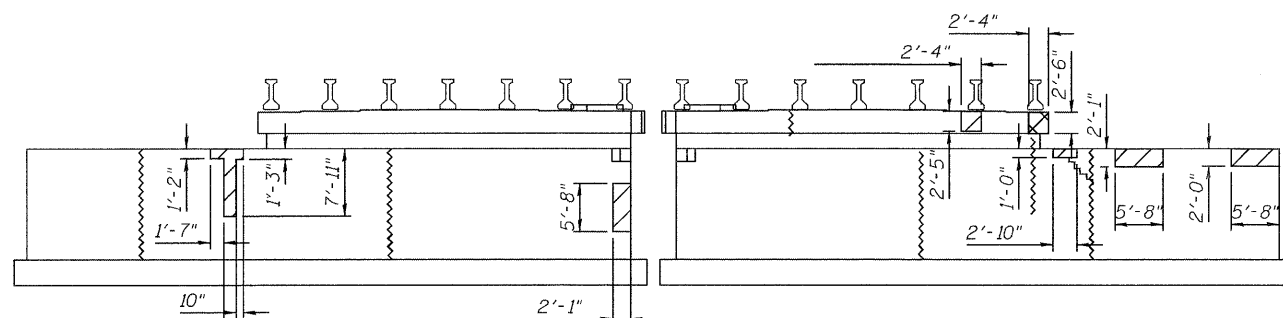
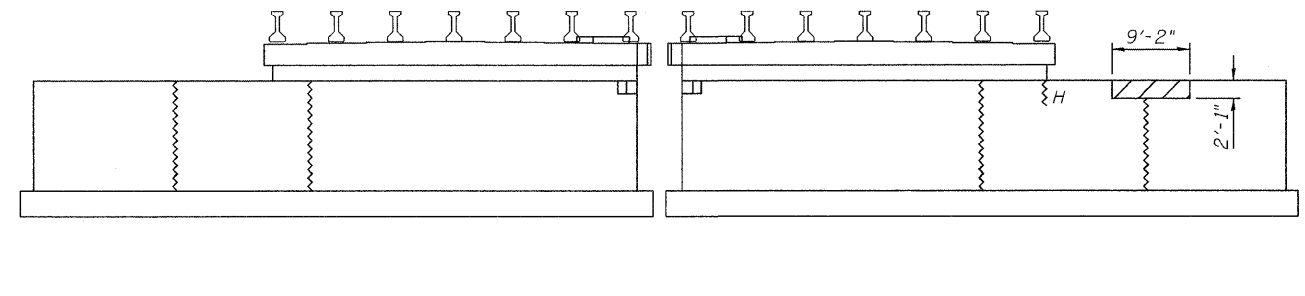
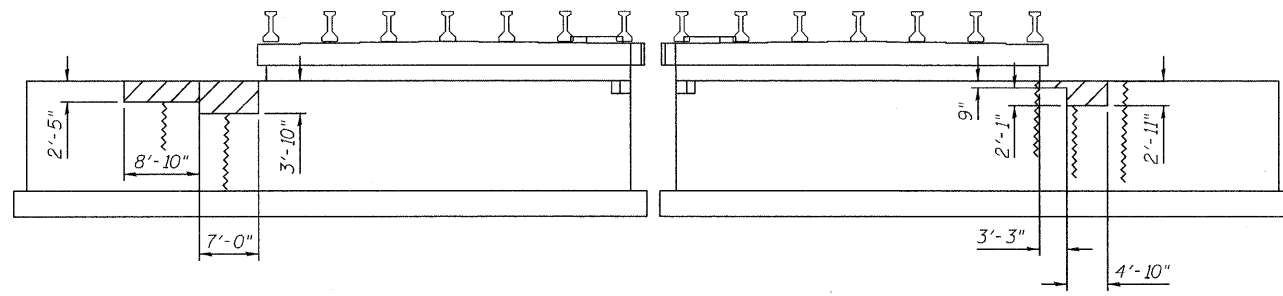
**LEGEND**

- Epoxy Crack Injection
- Structural Repair of Concrete  
(Depth Equal to or Less Than 5")
- Structural Repair of Concrete  
(Depth greater Than 5")

Note:  
Quantities are Included in Bill of Material on Sheet 18 of 19

ILLINOIS DEPARTMENT OF TRANSPORTATION

|                                    |             |
|------------------------------------|-------------|
| SHEET TITLE                        |             |
| ABUTMENT & PIER REPAIR DETAILS     |             |
| PROJECT                            | PROJECT NO. |
| I-55 NB OVER KANKAKEE RIVER        | 03095-16    |
| FAI ROUTE 55, SECTION 88(B&B-1)BR  | SCALE       |
| WILL COUNTY                        | 06/25/09    |
| SN 099-0001                        | DATE        |
|                                    | DRAWN BY    |
|                                    | CFC         |
|                                    | CHECKED BY  |
|                                    | MCB/KPS     |
|                                    | DRAWING NO. |
|                                    | 17          |
| COOMBE-BLOXDORF P.C.               | OF 19 SHTS  |
| Engineers / Land Surveyors         |             |
| Springfield, Illinois              |             |
| Design Firm License No. 184-002703 |             |



- LEGEND**
- Epoxy Crack Injection
  - Structural Repair of Concrete (Depth Equal to or Less Than 5")
  - Structural Repair of Concrete (Depth greater Than 5")

**BILL OF MATERIAL**

| Item   | Unit  | Total |
|--|-------|-------|
| Epoxy Crack Injection  | Foot  | 627   |
| Structural Repair of Concrete (Depth Equal to or Less Than 5") | Sq Ft | 531   |
| Structural Repair of Concrete (Depth Greater Than 5")          | Sq Ft | 17    |

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: PIER REPAIR DETAILS

|   |   |
|---|---|
| PROJECT: I-55 NB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0001 | PROJECT NO.: 03095-16<br>SCALE:<br>DATE: 06/25/09<br>DRAWN BY: CFC<br>CHECKED BY: MCB/KPS<br>DRAWING NO.: |
|---|---|

COOMBE-BLOXDORF P.C.  
Engineers / Land Surveyors  
Springfield, Illinois  
Design Firm License No. 184-002703

18 OF 19 SHTS

PLOT DATE = 6/25/2009  
 FILE NAME = I:\Projects\099-0001\epi-18pier-r-repair-ds1.dgn  
 PLOT SCALE = 2:1 (1/8" = 1'-0")  
 USER NAME = JML



Illinois Department of Transportation  
Division of Highways  
ILLINOIS DEPARTMENT OF TRANSPORTATION

### SOIL BORING LOG

Page 1 of 1

Date 7/6/05

ROUTE FAI 55 DESCRIPTION NB I-55 over Kankakee River LOGGED BY J. Stewart  
SECTION 88(B & B1)BR LOCATION SW SEC. 10, TWP. 33, RNG. 9, 3<sup>rd</sup> PM  
COUNTY WILL DRILLING METHOD CME 750, 3.25 ID H.S.A. HAMMER TYPE CME Automatic

| STRUCT. NO. | STATION | DEPTH | DESCRIPTION  | MOISTURE | UNSATURATED WATER CONTENT | PERCENTAGE | TEST |
|-------------|---------|-------|--|----------|---------------------------|------------|------|
| 099-0001    |         |       | Surface Water Elev. 507.86 ft                              |          |                           |            |      |
|             |         |       | Stream Bed Elev. 508.78 ft                                 |          |                           |            |      |
| B-1         | 21+03   |       | Groundwater Elev.:   |          |                           |            |      |
|             |         |       | First Encounter 505.7 ft                                   |          |                           |            |      |
|             |         |       | Upon Completion  |          |                           |            |      |
|             |         |       | After  |          |                           |            |      |
|             |         |       | Ground Surface Elev. 528.70 ft                             |          |                           |            |      |
|             |         |       | 8" Reinforced Concrete                                     |          |                           |            |      |
|             |         |       | Augured through fine GRAVEL (Fill)                         |          |                           |            |      |
|             |         |       | Very Stiff Gray SILTY CLAY w/ trace fine gravel            |          | 2.5                       |            |      |
|             |         |       | Loose to Medium Dense fine SAND and GRAVEL (Fill)          |          |                           |            |      |
|             |         |       | Soft Gray SILTY CLAY LOAM                                  |          |                           |            |      |
|             |         |       | Soft Brown SILTY CLAY LOAM with occasional shale fragments |          |                           |            |      |
|             |         |       | Grades w/ trace GRAVEL                                     |          |                           |            |      |
|             |         |       | Limestone Cobble @ 17.5 feet                               |          |                           |            |      |
|             |         |       | Medium Stiff to Stiff mottled Gray/Black SILTY CLAY LOAM   |          | 1.0                       |            |      |

The Unconfined Compressive Strength (UCS) Failure Mode is Indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T208) BBS, from 137 (Rev. 8-99)

PLT DATE = 6/25/2009  
FILE NAME = I:\88099-0001-SP1-19-borlogs.dgn  
PLOT SCALE = 0.25000" / 1"  
USER NAME = JML

|                                       |             |
|---------------------------------------|-------------|
| ILLINOIS DEPARTMENT OF TRANSPORTATION |             |
| SHEET TITLE                           |             |
| BORING LOG                            |             |
| PROJECT                               | PROJECT NO. |
| I-55 NB OVER KANKAKEE RIVER           | 03095-16    |
| FAI ROUTE 55, SECTION 88(B&B-1)BR     | SCALE       |
| WILL COUNTY                           | DATE        |
| SN 099-0001                           | 06/25/09    |
|                                       | DRAWN BY    |
|                                       | CFC         |
|                                       | CHECKED BY  |
|                                       | MCB/KPS     |
|                                       | DRAWING NO. |
| COOMBE-BLOXDORF P.C.                  | 19          |
| Engineers / Land Surveyors            | OF 19 SHTS  |
| Springfield, Illinois                 |             |
| Design Firm License No. 184-002703    |             |

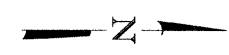
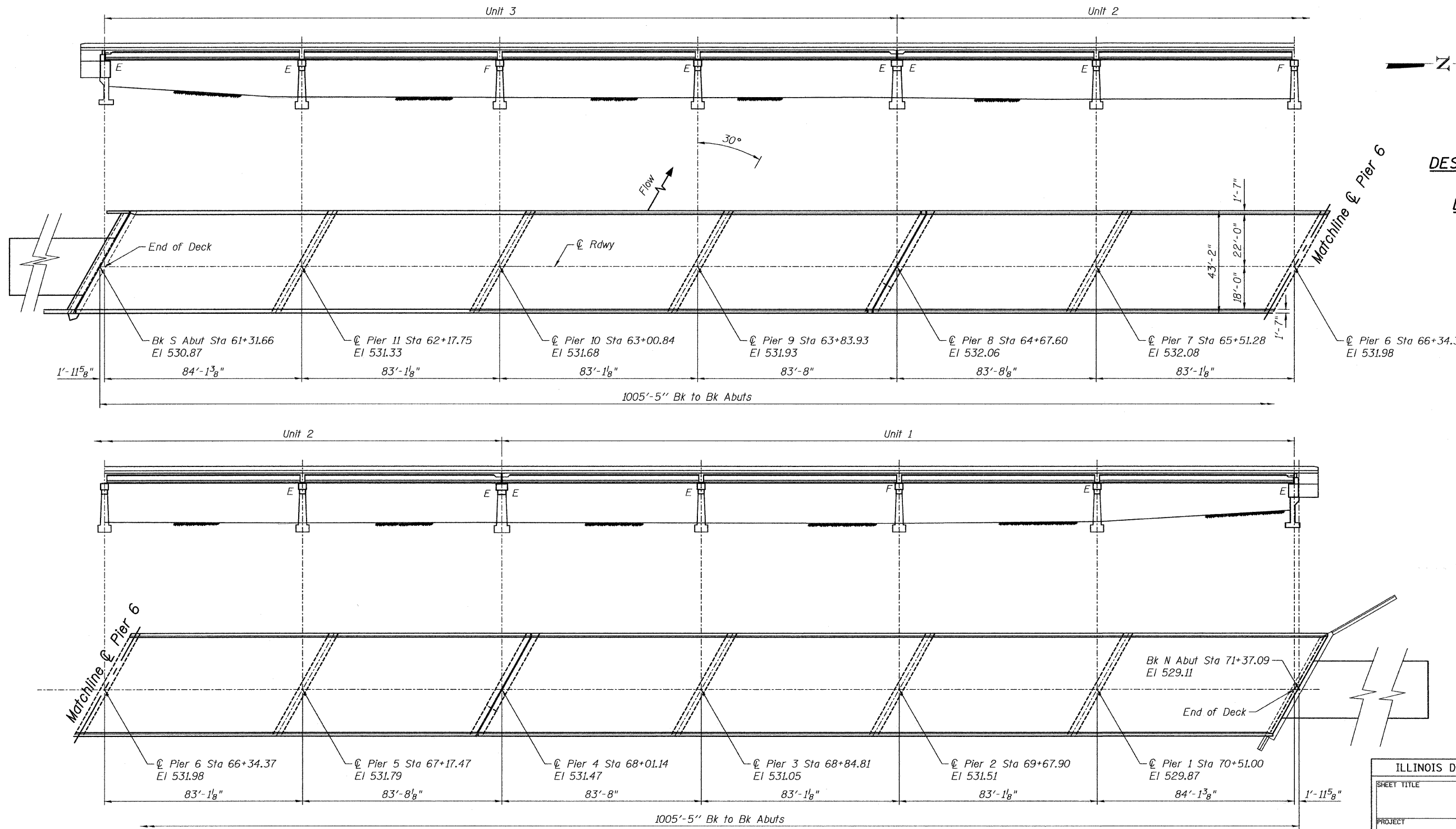
Benchmark: Chiseled "X" on top of west parapet, south end of SN 099-0002, Elev. 533.22

Existing Structure: 12 Span PPC I-Beam Superstructure Consisting of Three (4 Span) Continuous Units on Solid Concrete Piers and Closed Concrete Abutments. The Abutments and Piers Were Built in 1955 and New Caps and Superstructure Were Added in 1986.

Proposed Improvement: Deck Repair, Scarify Deck 1" and Install Bridge Deck Latex concrete Overlay 2 1/4".  
 Replace Neoprene Expansion Joints. Install Concrete Brackets Under Cantilever Portion of Pier Caps at Piers 1 Thru 11.  
 Structural Repair of Concrete and Epoxy Crack Injection at Abutments and Piers.

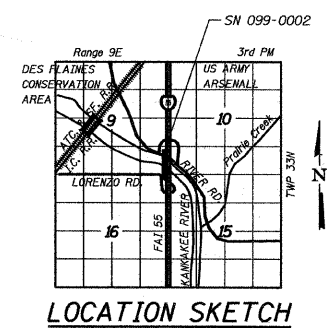
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|-----------------------|----------|-------------------|--------------|-----------|-------------|
| ROUTE NO.             | SECTION  | COUNTY            | TOTAL SHEETS | SHEET NO. | SHEET NO. 1 |
| FAI 55                | #        | Will              | 72           | 48        | 9 SHEETS    |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT- |              |           |             |

Contract #62930 \* 88(B&B-1)BR

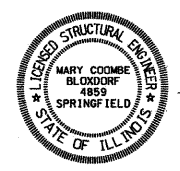


**DESIGN SPECIFICATIONS**  
 2002 AASHTO

**DESIGN STRESSES**  
 FIELD UNITS  
 $f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (reinforcement)



**PLAN & ELEVATION**



*Mary Coombe Bloxdorf*  
 ILLINOIS STRUCTURAL NO. 4859  
 EXPIRES: 11/30/10  
 DATE: 6-25-09

|  |  |
|--|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION  |  |
| SHEET TITLE<br><b>PLAN AND ELEVATION</b>   |  |
| PROJECT<br>I-55 SB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0002                | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>6/25/09<br>DRAWN BY<br>CFC<br>CHECKED BY<br>MCB/KPS<br>DRAWING NO. |
| <b>COOMBE-BLOXDORF P.C.</b><br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 | 1<br>OF 9 SHTS   |

PLOT DATE = 12/01/2009  
 FILE NAME = ...88(B&B-1)plan-el.dgn  
 PLOT SCALE = 214.0000 1/1 IN.  
 USER NAME = CFC

|                       |          |                  |        |       |
|-----------------------|----------|------------------|--------|-------|
| ROUTE NO.             | SECTION  | COUNTY           | SHEETS | SHEET |
| FAI<br>55             | *        | Willi            | 72     | 49    |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT |        |       |

SHEET NO. 2

9 SHEETS

Contract #62930

\* 88(B&B-1)BR

**INDEX OF SHEETS**

- 1 PLAN AND ELEVATION
- 2 GENERAL NOTES AND TOTAL BILL OF MATERIAL
- 3 CROSS SECTION
- 4 CONTINUOUS SEAL NEOPRENE EXPANSION JOINTS
- 5-6 DECK SLAB REPAIR
- 7 CONCRETE BRACKETS AT PIERS
- 8 PIER REPAIRS
- 9 ABUTMENT AND PIER REPAIRS

**GENERAL NOTES**

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

THE CONTRACTOR SHALL ENSURE THAT NO CONCRETE WILL BE ALLOWED TO DROP INTO THE RIVER. CONCRETE SHALL BE CAUGHT ON A FLOATING PLATFORM OR OTHER MEANS APPROVED BY THE ENGINEER. COST INCLUDED WITH DECK SLAB REPAIR (FULL DEPTH, TYPE II).

REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 760 GR 60. SEE SPECIAL PROVISIONS.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

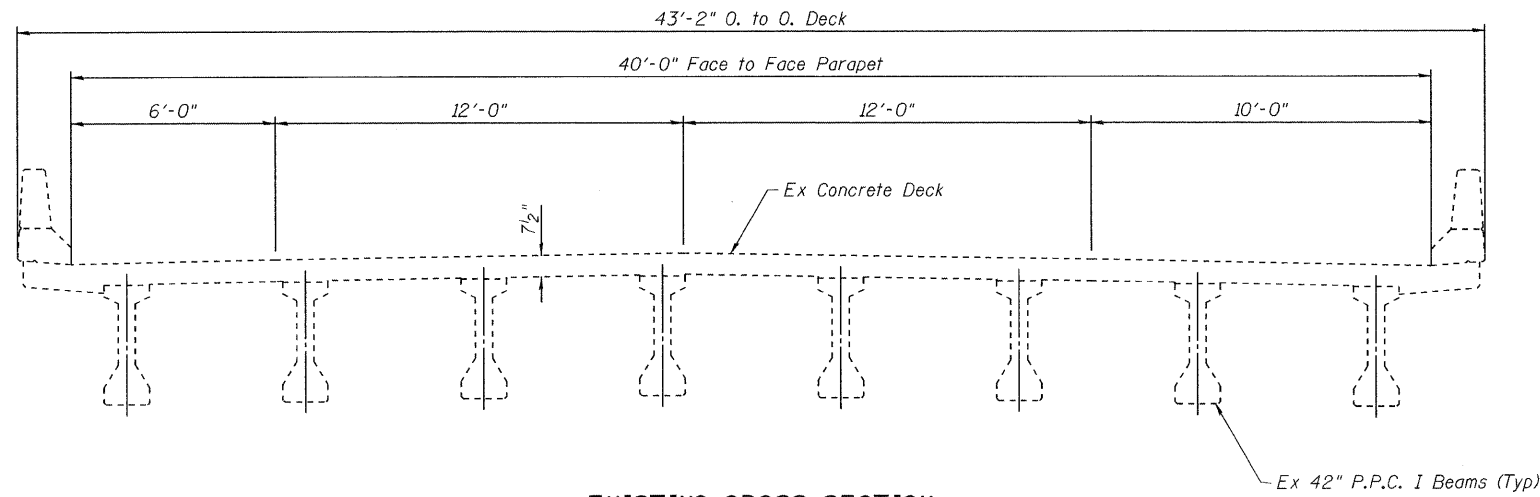
**TOTAL BILL OF MATERIAL**

| ITEM   | UNIT  | SUPER | SUB   | TOTAL |
|--|-------|-------|-------|-------|
| Bridge Deck Latex Concrete Overlay 2 1/4"                      | Sq Yd | 4449  |       | 4449  |
| Bridge Deck Hydro Scarification 1"                             | Sq Yd | 4449  |       | 4449  |
| Neoprene Expansion Joint 2 1/2"                                | Foot  | 97    |       | 97    |
| Neoprene Expansion Joint 4"                                    | Foot  | 97    |       | 97    |
| Epoxy Crack Injection  | Foot  |       | 275   | 275   |
| Structural Repair of Concrete (Depth equal to or less than 5") | Sq Ft |       | 90    | 90    |
| Concrete Removal   | Cu Yd |       | 8.0   | 8.0   |
| Concrete Structures  | Cu Yd |       | 29.5  | 29.5  |
| Reinforcement Bars, Epoxy Coated                               | Pound |       | 3,090 | 3,090 |
| Polymer Concrete   | Cu Ft | 0.5   |       | 0.5   |
| Deck Slab Repair (Full Depth, Type I)                          | Sq Yd | 1     |       | 1     |
| Bridge Deck Grooving   | Sq Yd | 4219  |       | 4219  |
| Protective Coat  | Sq Yd | 4449  |       | 4449  |

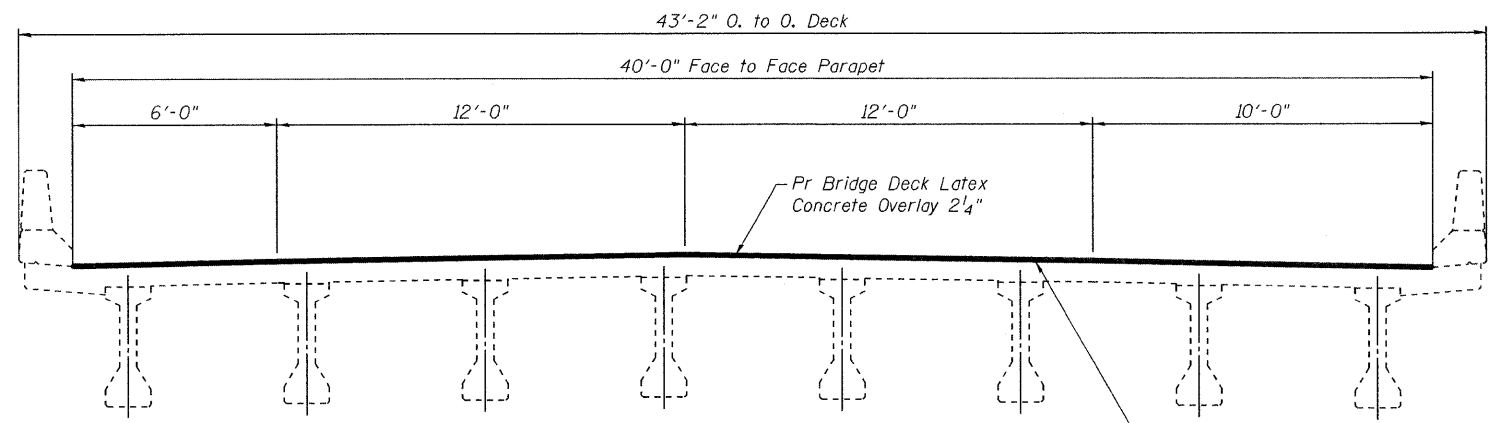
PLOT DATE = 6/25/2009  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = JML

|   |   |
|---|---|
| ILLINOIS DEPARTMENT OF TRANSPORTATION   |   |
| SHEET TITLE<br>GENERAL NOTES AND TOTAL BILL OF MATERIAL   |   |
| PROJECT<br>I-55 SB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0002         | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>6/25/09<br>DRAWN BY<br>TFG/CFC<br>CHECKED BY<br>MCB/KPS |
| COOMBE-BLOXDORF P.C.<br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 | DRAWING NO.<br>2<br>OF 9 SHTS   |

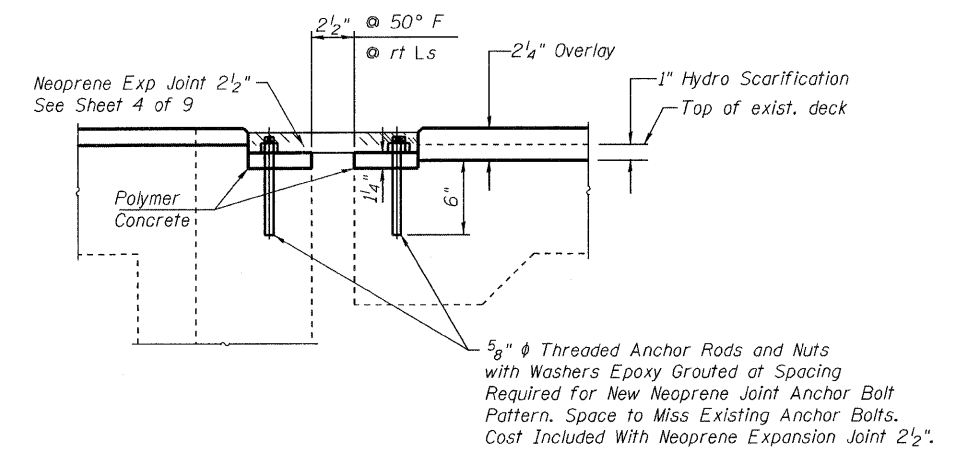




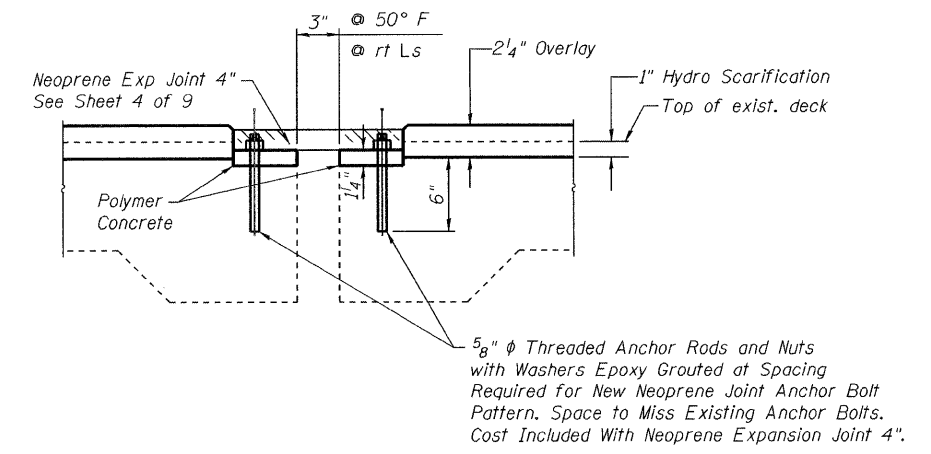
**EXISTING CROSS SECTION**  
(Looking South)



**PROPOSED CROSS SECTION**  
(Looking South)



**SECTION AT ABUTMENTS**  
(Showing New Neoprene Joint)



**SECTION AT PIERS 4 & 8**  
(Showing New Neoprene Joint)

**NOTES:**  
Limits of Concrete Bridge Deck Latex Concrete Overlay 2 1/4" and Bridge Deck Hydro Scarification 1" Extend from End of Deck at S Abutment to End of Deck at N Abutment 333'-9 7/8" Units 1 & 3, ±333'-3" Unit 2.  
Remove Existing Neoprene Joints, Cut Existing Anchor Bolts Flush With Top of Concrete, Grind Smooth and Seal With Epoxy. Cost Included With Neoprene Joints 2 1/2" and 4" as Applicable.

**BILL OF MATERIAL**

| ITEM                                      | UNIT  | TOTAL |
|---|-------|-------|
| Bridge Deck Latex Concrete Overlay 2 1/4" | Sq Yd | 4449  |
| Bridge Deck Hydro Scarification 1"        | Sq Yd | 4449  |
| Neoprene Expansion Joint 2 1/2"           | Foot  | 97    |
| Neoprene Expansion Joint 4"               | Foot  | 97    |

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: CROSS SECTION

PROJECT: I-55 SB OVER KANKAKEE RIVER  
FAI ROUTE 55, SECTION 88(B&B-1)BR  
WILL COUNTY  
SN 099-0002

PROJECT NO.: 03095-16  
SCALE: AS SHOWN  
DATE: 6/25/09  
DRAWN BY: CFC  
CHECKED BY: MCB/KPS  
DRAWING NO.: 3

**COOMBE-BLOXDORF P.C.**  
Engineers / Land Surveyors  
Springfield, Illinois  
Design Firm License No. 184-002703

OF 9 SHTS

PLOT DATE = 6/25/2009  
PLOT SCALE = 1/8" = 1'-0"  
USER NAME = JML

| Joint Size | "C" at 50°F | "D" at 50°F |
|------------|-------------|-------------|
| 2"         | 2"          | 1 1/2" Min. |
| 2 1/2"     | 2 1/2"      | 1 3/4" Min. |
| 4"         | 3"          | 2 1/2" Min. |

### GENERAL NOTES

Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane.

The elastomeric membrane shall be premolded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure.

The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed.

Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted.

### INSTALLATION NOTES

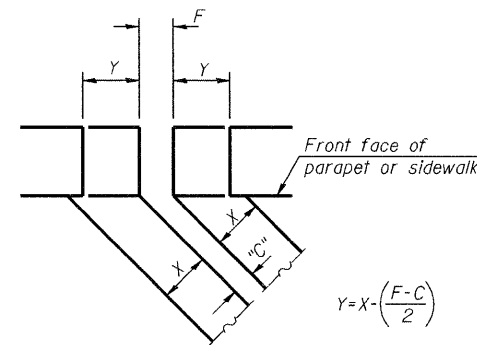
- Install continuous seal in roadway, parapet, curb, and sidewalk.
- Install anchor blocks as indicated.

Note A:  
Maximum spacing of anchor bolts shall be 12" centers.

### SKREW LIMITATIONS

The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews.

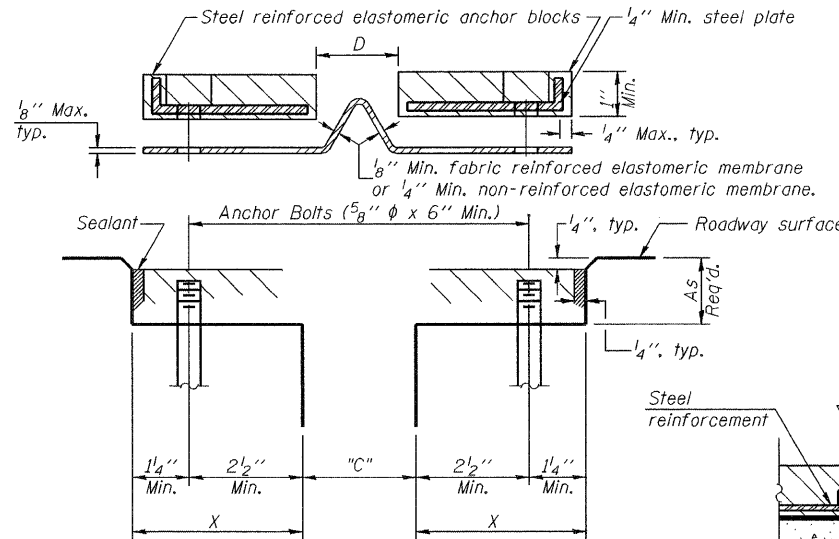
For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 1/2" from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±12" cts.



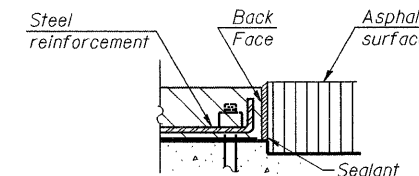
$$Y = X \cdot \left( \frac{F - C}{2} \right)$$

For dimension "F" see sheet 3 of 9

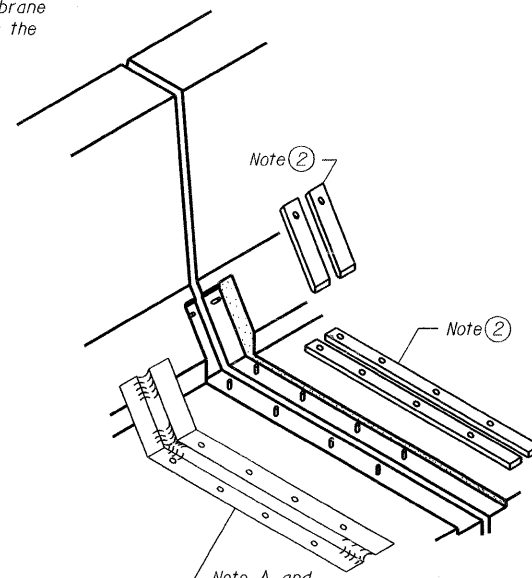
### FORMING BLOCKOUT SKETCH



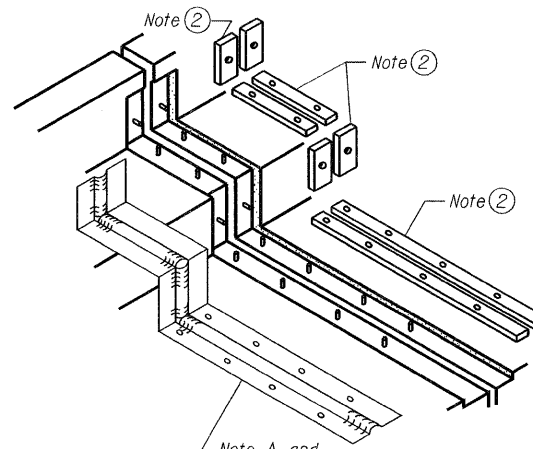
### CROSS SECTION



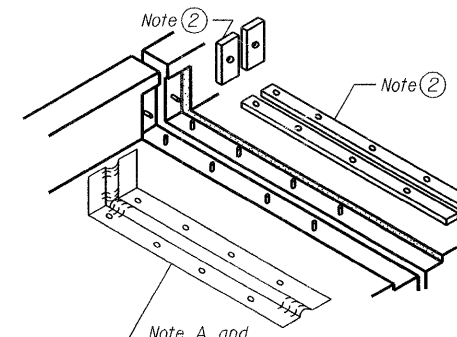
### ANCHOR BLOCK WITH ASPHALT SURFACE



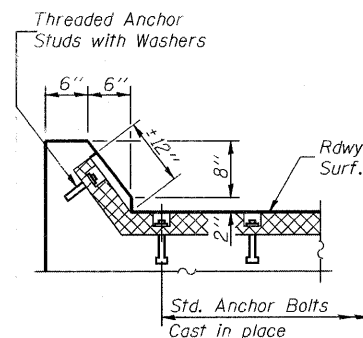
### AT PARAPET



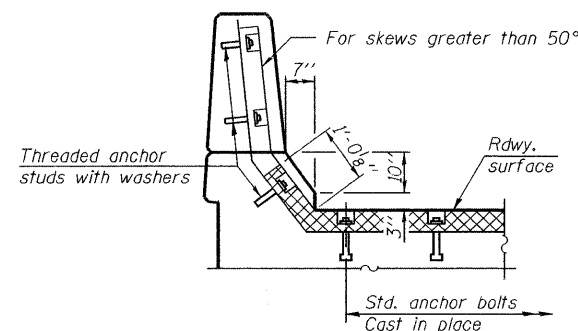
### AT SIDEWALK OR MEDIAN



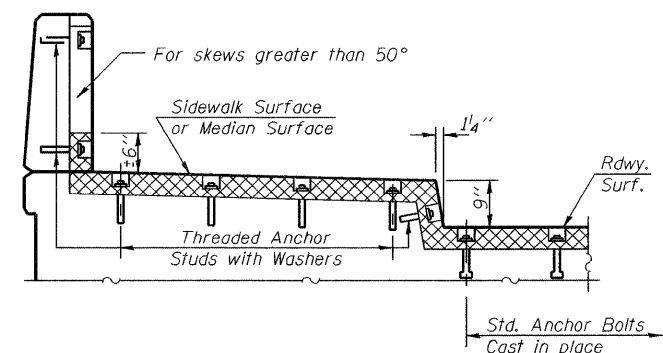
### AT WALL



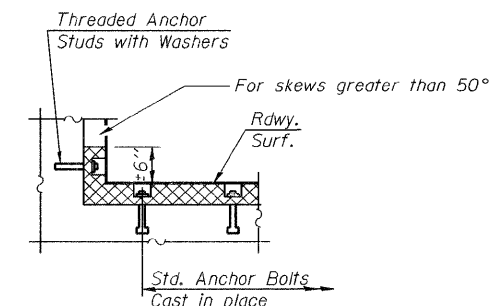
### AT CURB



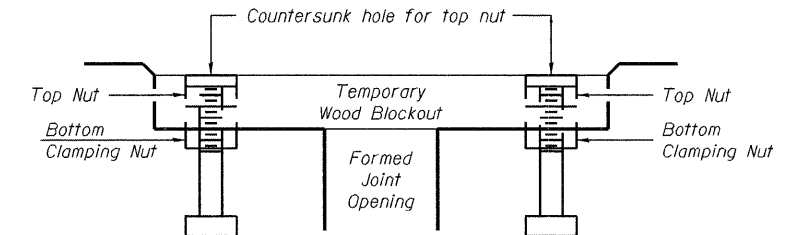
### AT PARAPET



### AT SIDEWALK OR MEDIAN TYPICAL END TREATMENTS



### AT WALL



Note:  
Stud needs to be threaded lower to allow for use of clamping nut.

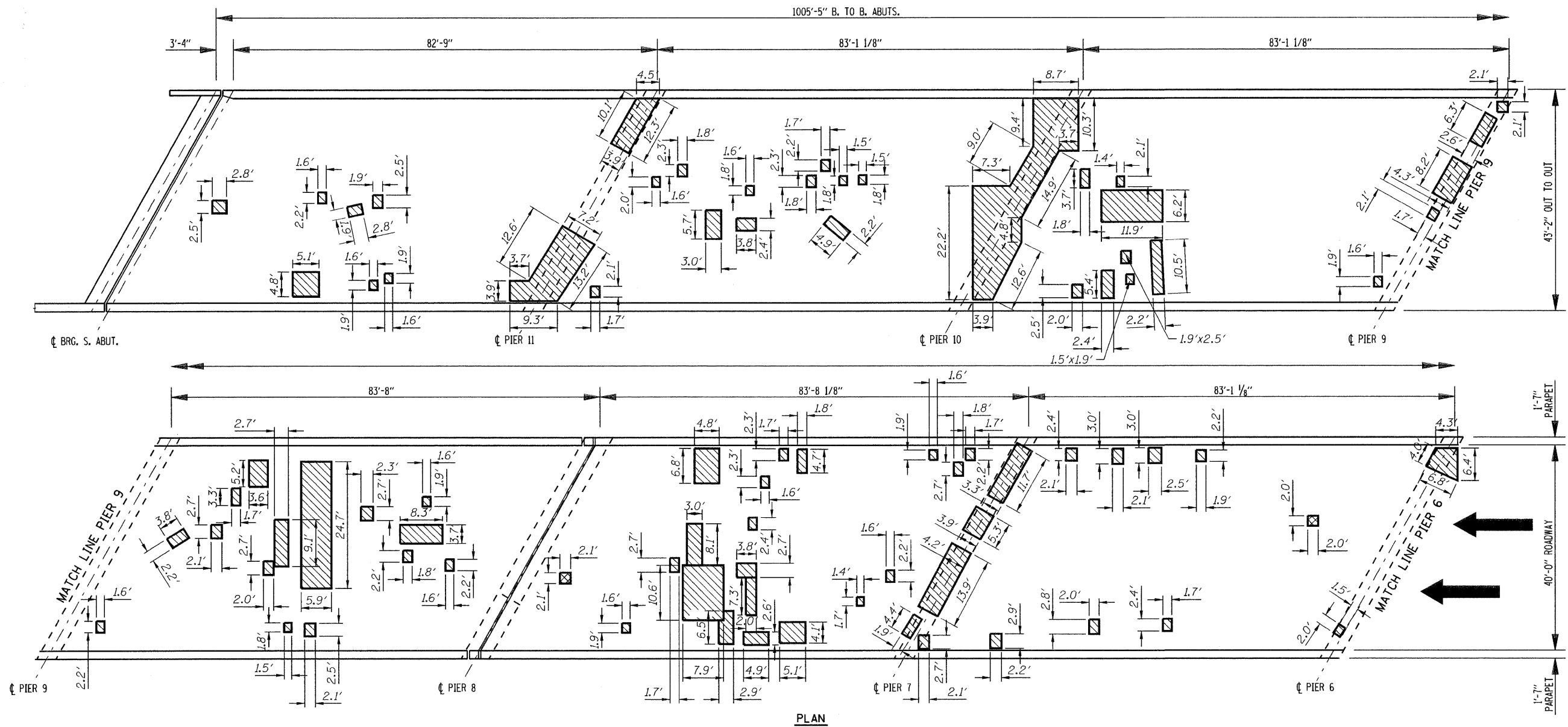
### RECOMMENDED BLOCKOUT DETAIL

### BILL OF MATERIAL

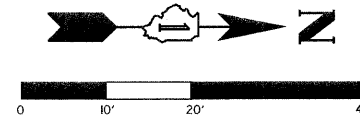
| Item                            | Unit | Total |
|---------------------------------|------|-------|
| Neoprene Expansion Joint 2 1/2" | foot | 97    |
| Neoprene Expansion Joint 4"     | foot | 97    |

### ILLINOIS DEPARTMENT OF TRANSPORTATION

|  |       |             |          |
|--|-------|-------------|----------|
| SHEET TITLE  |       | PROJECT NO. |          |
| CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS   |       | 03095-16    |          |
| PROJECT  | SCALE | DATE        | DRAWN BY |
| I-55 SB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN C99-0002 |       | 06/25/09    | CFC      |
| CHECKED BY   |       | DRAWING NO. |          |
| MCB/KPS  |       |             |          |
| COOMBE-BLOXDORF P.C.<br>Engineers / Land Surveyors<br>Springfield, Illinois                    |       | 4           |          |
| Design Firm License No. 184-002703   |       | OF 9 SHTS   |          |



PROPOSED DECK REHABILITATION AREAS



LEGEND

- DECK SLAB REPAIR PARTIAL
- DECK SLAB REPAIR (FULL DEPTH, TYPE I)

NOTES:

Deck Drains that are Located In Areas of Full Depth Repair Shall be Replaced as approved by the Engineer, Cost Included with Deck Slab Repair (Full Depth, Type I) or Deck Slab Repair (Full Depth, Type II) as Applicable.

The Areas of Deck Slab Repair are Based on Results of an Infrared Deck Survey Conducted on 04/16/09.

Areas of Deck Slab Repair (Partial) shown for Information only to assist the Contractor in Bidding.

Areas of repair that are located within 10 feet of the parapet face should be repaired in maximum lengths of 10' at a time to ensure stability of the parapet.

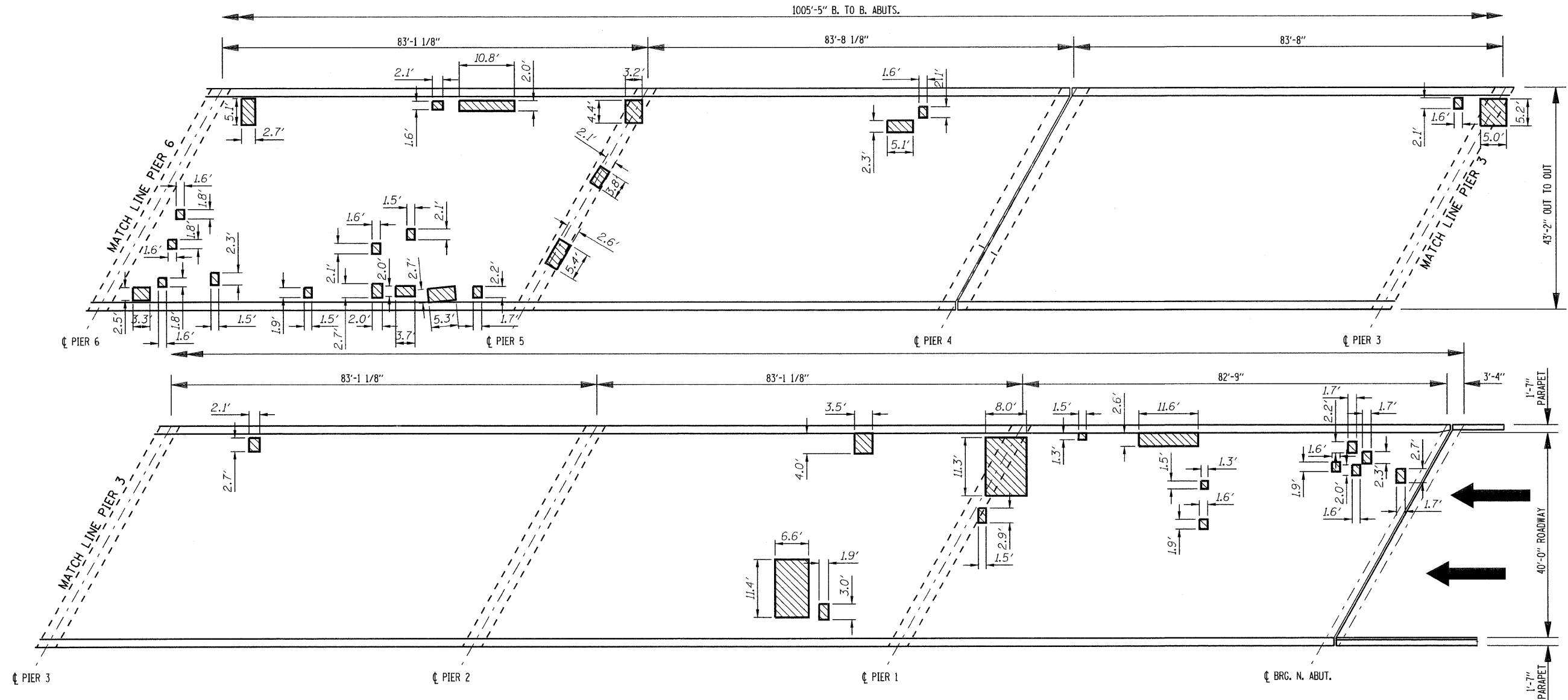
BILL OF MATERIAL

|                                       | UNIT  | TOTAL |
|---------------------------------------|-------|-------|
| DECK SLAB REPAIR (FULL DEPTH, TYPE I) | Sq Yd |       |

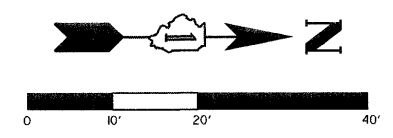
ILLINOIS DEPARTMENT OF TRANSPORTATION

|   |  |                  |          |
|---|--|------------------|----------|
| SHEET TITLE   |  | DECK SLAB REPAIR |          |
| PROJECT   | I-55 SB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0002 | PROJECT NO.      | 03095-16 |
| SCALE   |  | DATE             | 6/25/09  |
| DRAWN BY  | CFC  | CHECKED BY       | MCB/KPS  |
| COOMBE-BLOXDORF P.C.<br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 |  | DRAWING NO.      | 5        |
|   |  | OF 9 SHTS        |          |

PLOT DATE : 6/25/2009  
PLOT SCALE : 1" = 20'-0"  
USER NAME : JML



**PLAN**  
PROPOSED DECK REHABILITATION AREAS

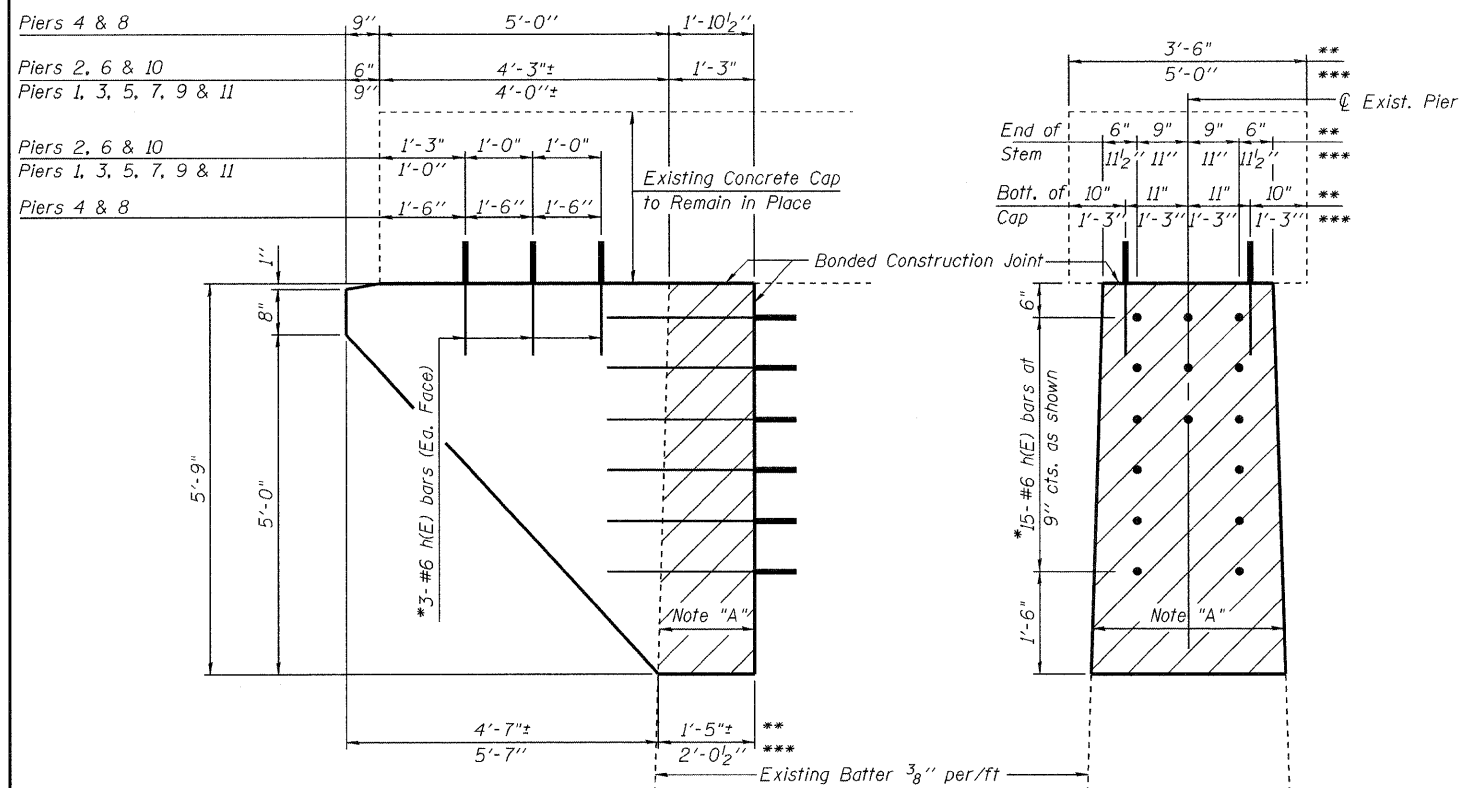


**LEGEND**  
 DECK SLAB REPAIR PARTIAL

**NOTES:**  
 Deck Drains that are Located In Areas of Full Depth Repair Shall be Replaced as approved by the Engineer. Cost Included with Deck Slab Repair (Full Depth, Type I) or Deck Slab Repair (Full Depth, Type II) as Applicable.  
 The Areas of Deck Slab Repair are Based on Results of an Infrared Deck Survey Conducted on 04/16/09.  
 Areas of Deck Slab Repair (Partial) shown for Information only to assist the Contractor in bidding.  
 Areas of repair that are located within 10 feet of the parapet face should be repaired in maximum lengths of 10' at a time to ensure stability at the parapet.

|  |  |
|--|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION  |  |
| SHEET TITLE<br><b>DECK SLAB REPAIR</b>   |  |
| PROJECT<br>I-55 SB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0002                | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>6/25/09<br>DRAWN BY<br>CFC<br>CHECKED BY<br>MCB/KPS<br>DRAWING NO. |
| <b>COOMBE-BLOXDORF P.C.</b><br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 |  |
| 6<br>OF 9 SHTS   |  |

PLOT DATE = 6/25/2009  
 PLOT SCALE = 2411.5294' / 1" / IN.  
 USER NAME = JNL



\*\*Piers 1, 2, 3, 5, 6, 7, 9, 10 & 11  
 \*\*\*Piers 4 & 8

**NOTE:**  
 The Contractor shall take the necessary means to ensure that the concrete between the bottom of the existing cap and the top of the new cantilever support is properly consolidated. The method of consolidation shall be approved by the Engineer.  
 The Contractor shall take precautions that no live loads be allowed on the outside beam during the concrete removal from the west pier nose and until the proposed concrete has cured.

**NOTES:**  
 Existing reinforcement extending into the new construction shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.  
 Any existing reinforcement bars which are intended to be incorporated into the new construction that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
 All exposed edges shall have 2" chamfers.

**BILL OF MATERIAL  
 ONE SUPPORT BRACKET  
 PIERS 4 & 8**

| Bar                              | No. | Size | Length  | Shape |
|----------------------------------|-----|------|---------|-------|
| h(E)                             | 21  | #6   | 3'-0"   | —     |
| h <sub>4</sub> (E)               | 4   | #6   | 7'-3"   | —     |
| h <sub>5</sub> (E)               | 4   | #6   | 9'-0"   | —     |
| h <sub>6</sub> (E)               | 4   | #6   | 9'-6"   | —     |
| s <sub>4</sub> (E)               | 2   | #4   | 6'-1"   | □     |
| s <sub>5</sub> (E)               | 2   | #4   | 7'-1"   | □     |
| s <sub>6</sub> (E)               | 2   | #4   | 7'-11"  | □     |
| s <sub>7</sub> (E)               | 2   | #4   | 8'-11"  | □     |
| s <sub>8</sub> (E)               | 2   | #4   | 9'-9"   | □     |
| u <sub>1</sub> (E)               | 2   | #6   | 7'-4"   | □     |
| Concrete Removal                 |     |      | Cu. Yd. | 1.3   |
| Concrete Structures              |     |      | Cu. Yd. | 4.4   |
| Reinforcement Bars, Epoxy Coated |     |      | Lb.     | 330   |

**BILL OF MATERIAL  
 ONE SUPPORT BRACKET  
 PIERS 1, 2, 3, 5, 6, 7, 9, 10 & 11**

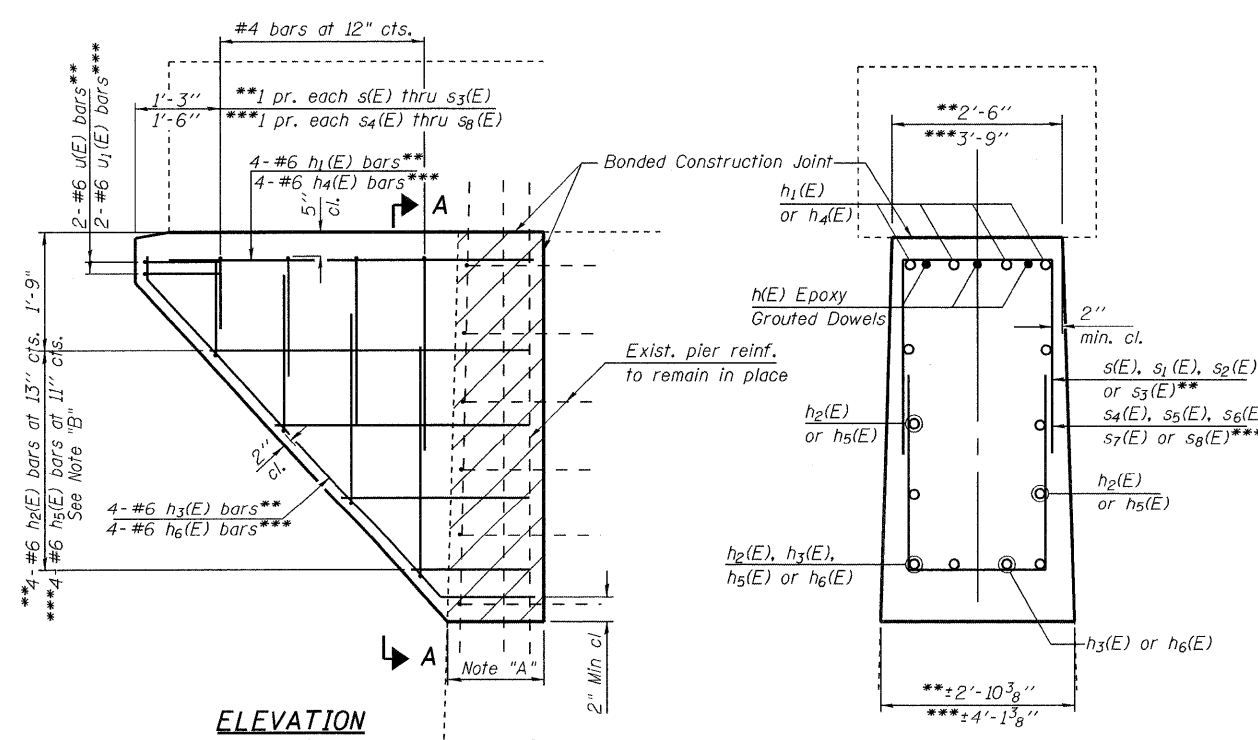
| Bar                              | No. | Size | Length  | Shape |
|----------------------------------|-----|------|---------|-------|
| h(E)                             | 21  | #6   | 3'-0"   | —     |
| h <sub>1</sub> (E)               | 4   | #6   | 5'-8"   | —     |
| h <sub>2</sub> (E)               | 4   | #6   | 6'-10"  | —     |
| h <sub>3</sub> (E)               | 4   | #6   | 8'-2"   | —     |
| s(E)                             | 2   | #4   | 4'-10"  | □     |
| s <sub>1</sub> (E)               | 2   | #4   | 5'-10"  | □     |
| s <sub>2</sub> (E)               | 2   | #4   | 7'-0"   | □     |
| s <sub>3</sub> (E)               | 2   | #4   | 8'-0"   | □     |
| u(E)                             | 2   | #6   | 6'-1"   | □     |
| Concrete Removal                 |     |      | Cu. Yd. | 0.6   |
| Concrete Structures              |     |      | Cu. Yd. | 2.3   |
| Reinforcement Bars, Epoxy Coated |     |      | Lb.     | 270   |

**ELEVATION**  
 Showing Anchor Bars and Dimensions (Looking North)

**END VIEW**  
 Showing Anchor Bars and Dimensions

\*Epoxy grout h(E) bars into 9" drilled holes. See Section 584 of the Standard Specifications.

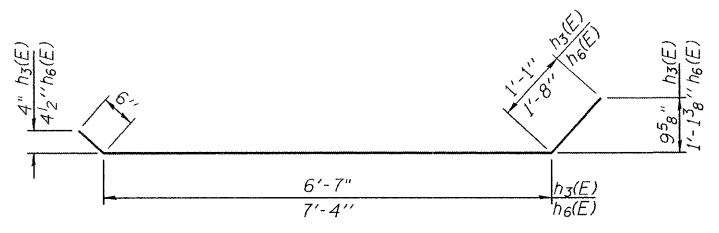
Note "A":  
 Hatched area indicates concrete removal of a portion of the west nose of the existing pier stem. Existing reinforcement in the nose shall be cleaned, straightened and incorporated into the new construction. Cost Included With Concrete Removal.



**ELEVATION**  
 Showing Reinforcement (Looking North)

**SECTION A-A**

Note "B":  
 Order bars full length. Cut to fit and use remainder in opposite face.



**BAR h<sub>3</sub>(E) or h<sub>6</sub>(E)**

**BAR u(E) & u<sub>1</sub>(E)**

**BARS s(E), s<sub>1</sub>(E), s<sub>2</sub>(E), s<sub>3</sub>(E), s<sub>4</sub>(E), s<sub>5</sub>(E), s<sub>6</sub>(E) s<sub>7</sub>(E) & s<sub>8</sub>(E)**

**A & B DIMENSIONS**

| BAR                | A      | B     |
|--------------------|--------|-------|
| s(E)               | 1'-4"  | 2'-2" |
| s <sub>1</sub> (E) | 1'-10" | 2'-2" |
| s <sub>2</sub> (E) | 2'-5"  | 2'-2" |
| s <sub>3</sub> (E) | 2'-11" | 2'-2" |
| s <sub>4</sub> (E) | 1'-4"  | 3'-5" |
| s <sub>5</sub> (E) | 1'-10" | 3'-5" |
| s <sub>6</sub> (E) | 2'-3"  | 3'-5" |
| s <sub>7</sub> (E) | 2'-9"  | 3'-5" |
| s <sub>8</sub> (E) | 3'-2"  | 3'-5" |

**MIN BAR LAP**  
 #4 bars 1'-4"

PLOT DATE: 6/25/09  
 PLOT SCALE: 1/8" = 1'-0"  
 USER NAME: JML

ILLINOIS DEPARTMENT OF TRANSPORTATION

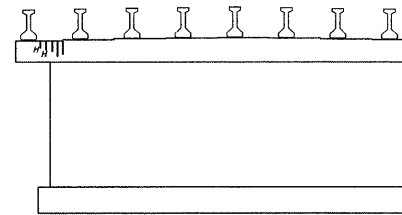
SHEET TITLE: CONCRETE BRACKETS AT PIERS

PROJECT: I-55 SB OVER KANKAKEE RIVER  
 FAI ROUTE 55, SECTION 88(B&B-1)BR  
 WILL COUNTY  
 SN 099-0002

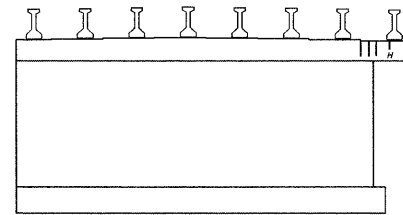
PROJECT NO.: 03095-16  
 SCALE: 1/8" = 1'-0"  
 DATE: 6/25/09  
 DRAWN BY: TFG  
 CHECKED BY: KPS/MCB  
 DRAWING NO.: 7

**COOMBE-BLOXDORF P.C.**  
 Engineers / Land Surveyors  
 Springfield, Illinois  
 Design Firm License No. 184-002703

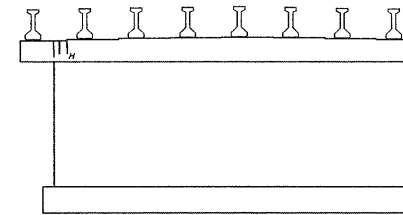
OF 9 SHTS



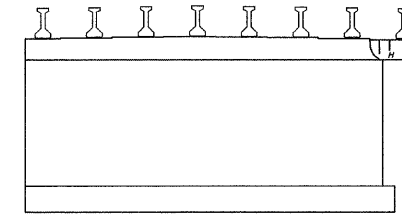
**PIER 1**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**



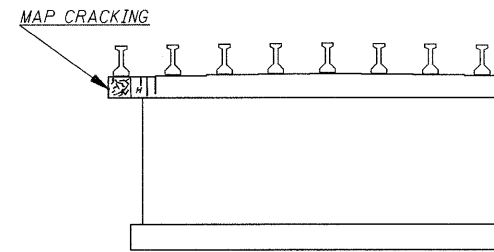
**PIER 1**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**



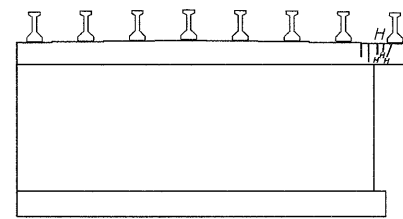
**PIER 2**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**



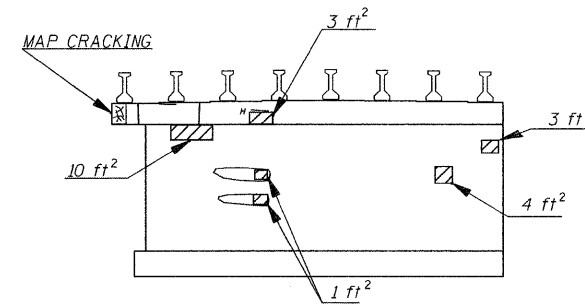
**PIER 2**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**



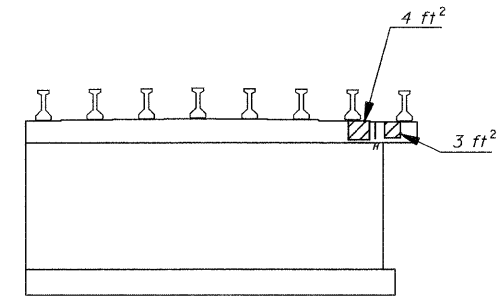
**PIER 3**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**



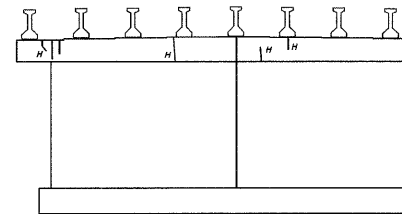
**PIER 3**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**



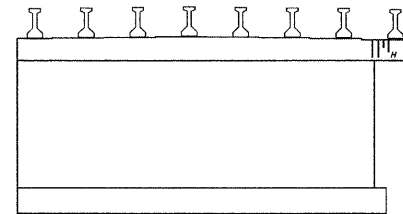
**PIER 4**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**



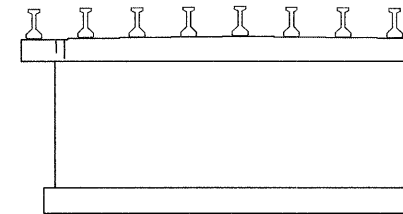
**PIER 4**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**



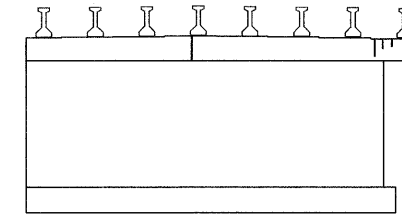
**PIER 5**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**



**PIER 5**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**


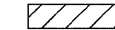


**PIER 6**  
**ELEVATION, SOUTH FACE**  
**(LOOKING NORTH)**



**PIER 6**  
**ELEVATION, NORTH FACE**  
**(LOOKING SOUTH)**

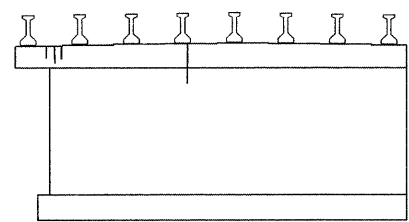
**LEGEND**

-  Epoxy Crack Injection
-  Structural Repair of Concrete  
(Depth Equal to or Less Than 5")

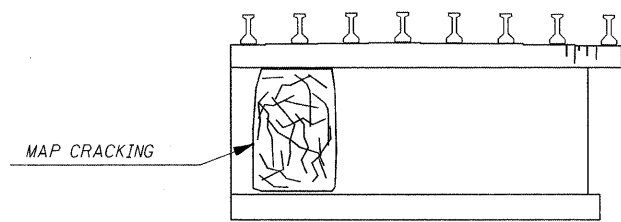
Note:  
Quantities Included in Bill of Material on Sheet 9 of 9

PLOT DATE = 6/25/2009  
 PLOT SCALE = 1/8" = 1'-0"  
 PLOT SCALE = 20115284 Y' / IN.  
 USER NAME = JNL

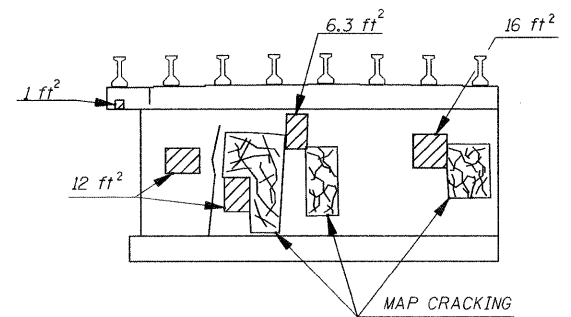
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|--|--|
| ILLINOIS DEPARTMENT OF TRANSPORTATION  |  |
| SHEET TITLE<br><b>PIER REPAIRS</b>   |  |
| PROJECT<br>I-55 SB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0002                | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>06/25/09<br>DRAWN BY<br>CFC<br>CHECKED BY<br>MCB/KPS<br>DRAWING NO.<br>8 |
| <b>COOMBE-BLOXDORF P.C.</b><br>Engineers / Land Surveyors<br>Springfield, Illinois<br>Design Firm License No. 184-002703 |  |
| OF 9 SHTS  |  |



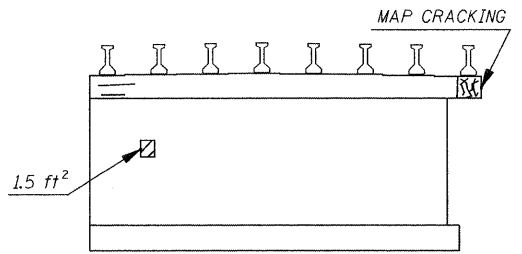
PIER 7  
ELEVATION, SOUTH FACE  
(LOOKING NORTH)



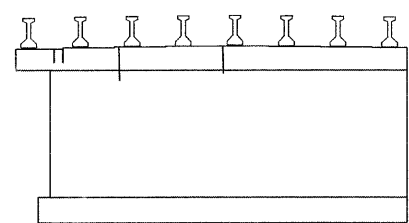
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ELEVATION, NORTH FACE  
(LOOKING SOUTH)



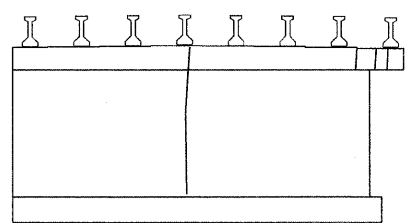
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ELEVATION, SOUTH FACE  
(LOOKING NORTH)



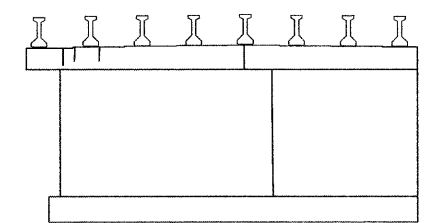
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ELEVATION, NORTH FACE  
(LOOKING SOUTH)



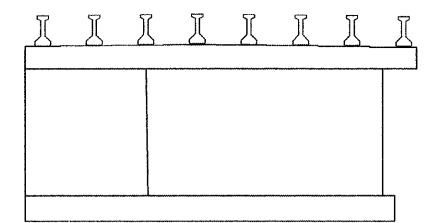
PIER 9  
ELEVATION, SOUTH FACE  
(LOOKING NORTH)



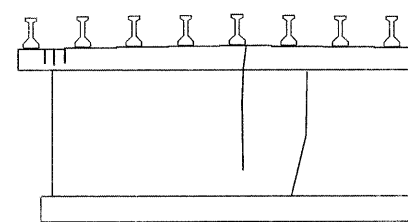
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ELEVATION, NORTH FACE  
(LOOKING SOUTH)



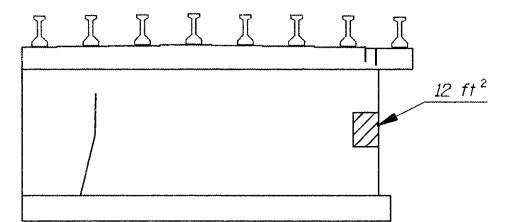
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ELEVATION, SOUTH FACE  
(LOOKING NORTH)



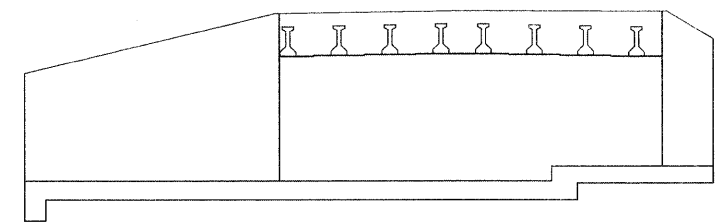
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ELEVATION, NORTH FACE  
(LOOKING SOUTH)



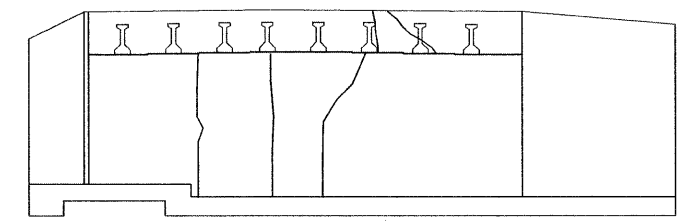
PIER 11  
ELEVATION, SOUTH FACE  
(LOOKING NORTH)



PIER 11  
ELEVATION, NORTH FACE  
(LOOKING SOUTH)



NORTH ABUTMENT  
(LOOKING NORTH)



SOUTH ABUTMENT  
(LOOKING SOUTH)

**LEGEND**

- Epoxy Crack Injection
- Structural Repair of Concrete (Depth Equal to or Less Than 5")

**BILL OF MATERIAL**

| Item   | Unit  | Total |
|--|-------|-------|
| Epoxy Crack Injection  | Foot  | 275   |
| Structural Repair of Concrete (Depth Equal to or Less Than 5") | Sq Ft | 90    |

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE  
ABUTMENT AND PIER REPAIRS

|   |   |
|---|---|
| PROJECT<br>I-55 SB OVER KANKAKEE RIVER<br>FAI ROUTE 55, SECTION 88(B&B-1)BR<br>WILL COUNTY<br>SN 099-0002 | PROJECT NO.<br>03095-16<br>SCALE<br>DATE<br>06/25/09<br>DRAWN BY<br>CFC<br>CHECKED BY<br>MCB/KPS<br>DRAWING NO. |
|---|---|

COOMBE-BLOXDORF P.C.  
Engineers / Land Surveyors  
Springfield, Illinois  
Design Firm License No. 184-002703

9  
OF 9 SHTS

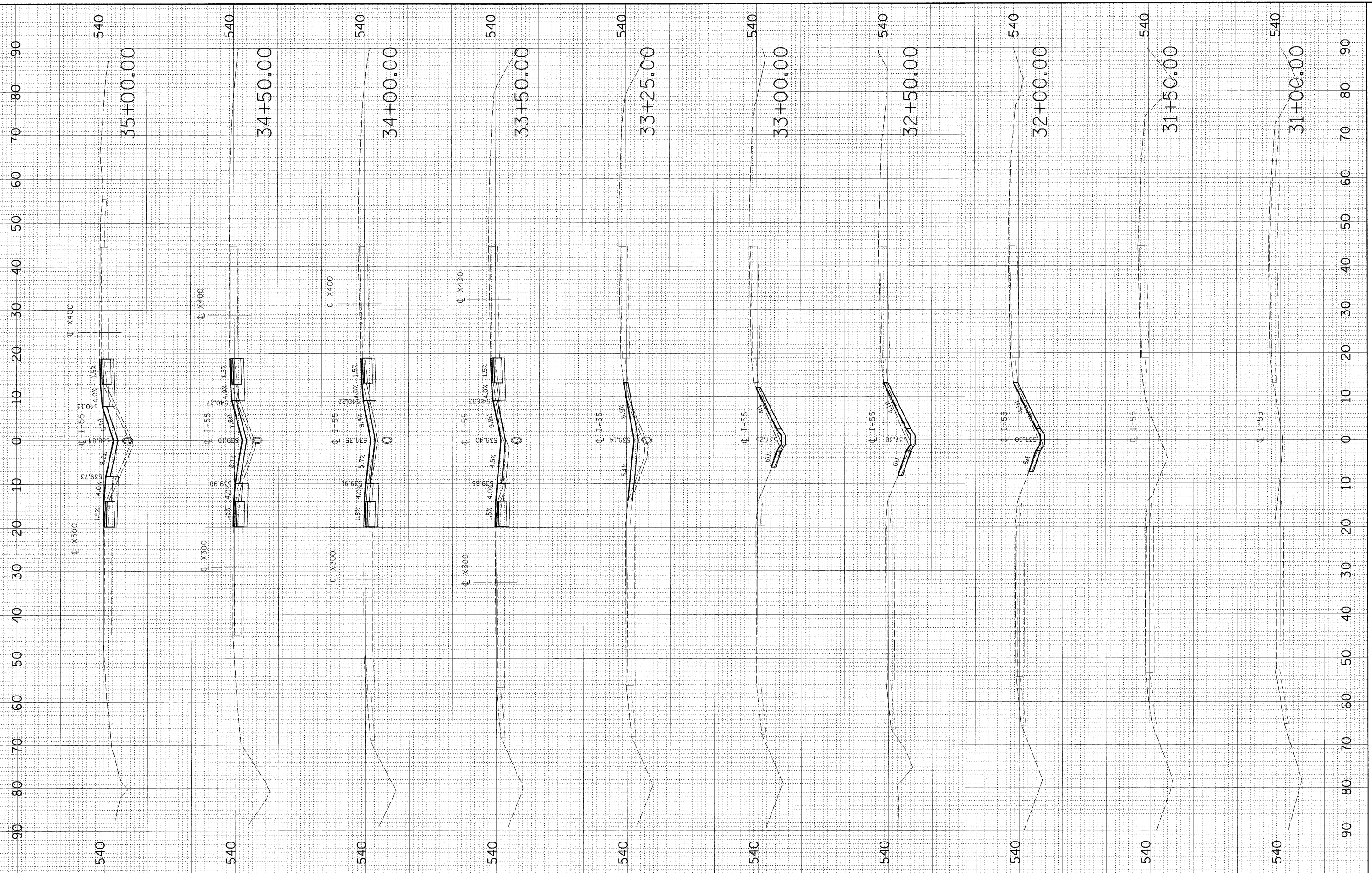
PLOT DATE : 6/25/2009  
 PLOT SCALE : 1/8" = 1'-0"  
 USER NAME : JML



|               |               |    |      |
|---------------|---------------|----|------|
| FINAL SURVEY  | SUPERVIEWED   | BY | DATE |
| NOTE BOOK     | PLOTTED       |    |      |
| AREAS CHECKED | TEMPLATE      |    |      |
| NO.           | AREAS CHECKED |    |      |

|                 |               |    |      |
|-----------------|---------------|----|------|
| ORIGINAL SURVEY | SUPERVIEWED   | BY | DATE |
| NOTE BOOK       | PLOTTED       |    |      |
| AREAS CHECKED   | TEMPLATE      |    |      |
| NO.             | AREAS CHECKED |    |      |

PLOT DATE = 6/25/2009  
 FILE NAME = K:\1102518\Cadd\Geo\55\_P.s.d  
 PLOT SCALE = 1/8"=1'-0"  
 PLOT NAME = XS-SHEET 1  
 MODEL =



**rjngroup**  
 Excellence through Ownership  
 200 West Front Street  
 Wheaton, IL 60187

|          |   |     |         |   |     |
|----------|---|-----|---------|---|-----|
| DESIGNED | - | --- | REVISED | - | --- |
| DRAWN    | - | --- | REVISED | - | --- |
| CHECKED  | - | --- | REVISED | - | --- |
| DATE     | - | --- | REVISED | - | --- |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

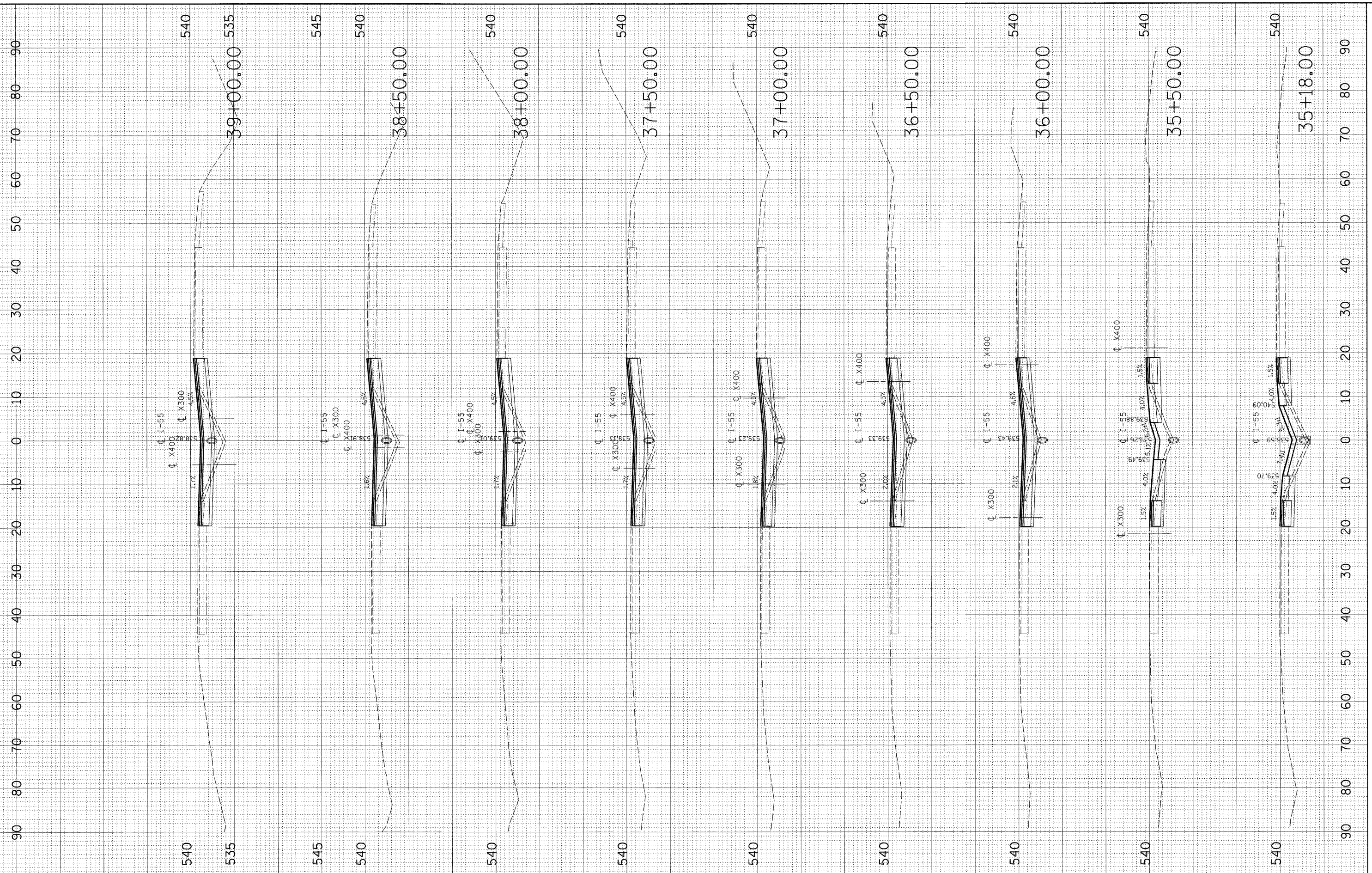
**I-55 OVER KANKAKEE RIVER**  
**BRIDGE REHABILITATION**  
 SCALE: \_\_\_\_\_ SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. 31+00.00 TO STA. 35+00.00

|   |               |        |              |           |
|---|---------------|--------|--------------|-----------|
| F.A. RTE.   | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
| 55  | 88 (B&B-1) BR | WILL   | 72           | 57        |
| CONTRACT NO. 62930                                  |               |        |              |           |
| FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT |               |        |              |           |

BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_  
 PLOTTED: \_\_\_\_\_  
 TEMPLATE: \_\_\_\_\_  
 NOTE BOOK: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_  
 NO. \_\_\_\_\_

BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_  
 PLOTTED: \_\_\_\_\_  
 TEMPLATE: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_  
 NO. \_\_\_\_\_

PLOT DATE = 6/25/2009  
 FILE NAME = K:\1182518\Cadd\Geo\55\_P...  
 USER NAME = KS-SHEET 2  
 MODEL =



|            |     |           |     |
|------------|-----|-----------|-----|
| DESIGNED - | --- | REVISED - | --- |
| DRAWN -    | --- | REVISED - | --- |
| CHECKED -  | --- | REVISED - | --- |
| DATE -     | --- | REVISED - | --- |

|            |     |           |     |
|------------|-----|-----------|-----|
| DESIGNED - | --- | REVISED - | --- |
| DRAWN -    | --- | REVISED - | --- |
| CHECKED -  | --- | REVISED - | --- |
| DATE -     | --- | REVISED - | --- |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-55 OVER KANKAKEE RIVER  
 BRIDGE REHABILITATION**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. 35+18.00 TO STA. 39+00.00

|   |                       |             |                 |              |
|---|-----------------------|-------------|-----------------|--------------|
| F.A. RTE. 55  | SECTION 88 (B&B-1) BR | COUNTY WILL | TOTAL SHEETS 72 | SHEET NO. 58 |
| CONTRACT NO. 62930                                    |                       |             |                 |              |
| FED. ROAD DIST. NO. _____ (ILLINOIS) FED. AID PROJECT |                       |             |                 |              |

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 Excellence through Ownership  
 200 West Front Street  
 Wheaton, IL 60187

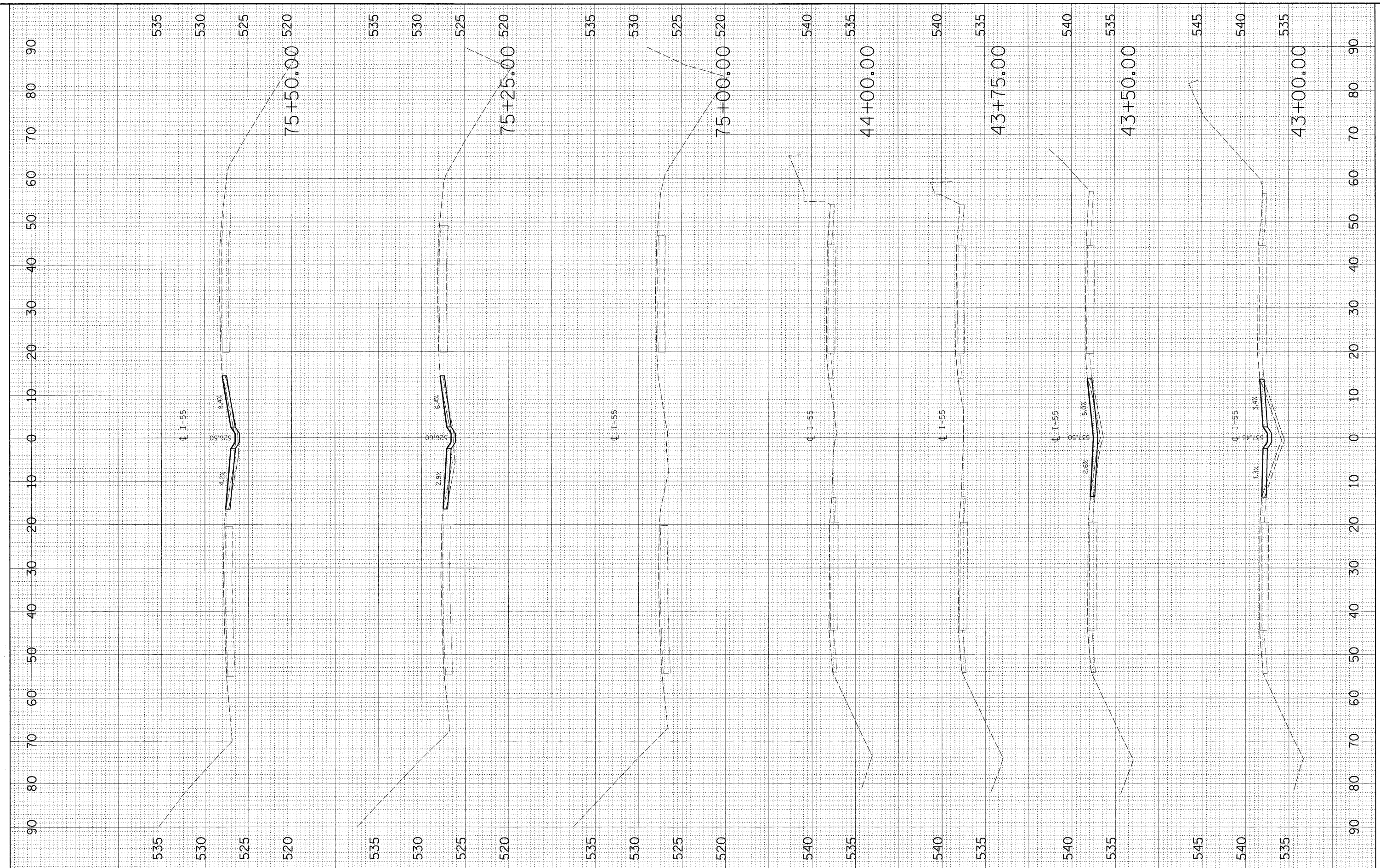




|              |          |    |      |
|--------------|----------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK    | PLOTTED  |    |      |
| AREAS        | TEMPLATE |    |      |
| AREAS        | CHECKED  |    |      |
| NO.          |          |    |      |

|                 |          |    |      |
|-----------------|----------|----|------|
| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK       | PLOTTED  |    |      |
| AREAS           | TEMPLATE |    |      |
| AREAS           | CHECKED  |    |      |
| NO.             |          |    |      |

PLOT DATE = 6/25/2009  
 FILE NAME = K:\1162518\Cadd\Geo\55\_P\3.dwg  
 USER NAME = J.S. SHELL  
 MODEL = XS\_SHEET\_4



200 West Front Street  
 Wheaton, IL 60187

|          |   |     |         |   |     |
|----------|---|-----|---------|---|-----|
| DESIGNED | - | --- | REVISED | - | --- |
| DRAWN    | - | --- | REVISED | - | --- |
| CHECKED  | - | --- | REVISED | - | --- |
| DATE     | - | --- | REVISED | - | --- |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-55 OVER KANKAKEE RIVER  
 BRIDGE REHABILITATION**

**I-55  
 CROSS SECTIONS**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. 43+00.00 TO STA. 75+50.00

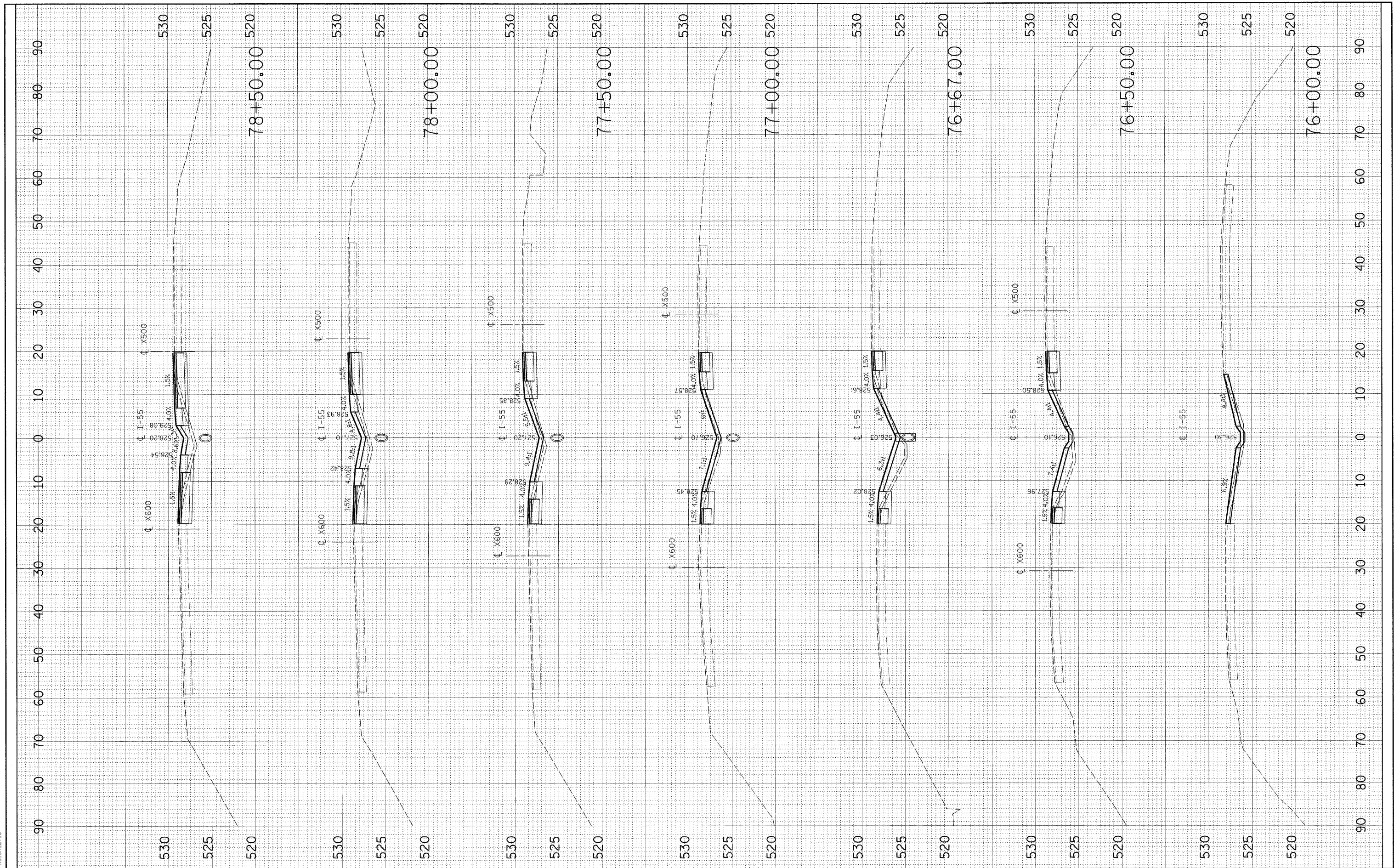
|   |               |        |              |           |
|---|---------------|--------|--------------|-----------|
| F.A. RTE.   | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
| 55  | 88 (B&B-1) BR | WILL   | 72           | 60        |
| CONTRACT NO. 62930                                  |               |        |              |           |
| FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT |               |        |              |           |



|              |               |    |      |
|--------------|---------------|----|------|
| FINAL SURVEY | SURVEYED      | BY | DATE |
| NOTE BOOK    | PLOTTED       |    |      |
| NO.          | AREAS CHECKED |    |      |

|                 |               |    |      |
|-----------------|---------------|----|------|
| ORIGINAL SURVEY | SURVEYED      | BY | DATE |
| NOTE BOOK       | PLOTTED       |    |      |
| NO.             | AREAS CHECKED |    |      |

PLOT DATE = 6/25/2009  
 FILE NAME = I:\11182818\Cadd\Geo\55\_Parallel\NOTE BOOK  
 USER NAME = JLS  
 MODEL = XS-SHEET.5



200 West Front Street  
 Wheaton, IL 60187

|          |   |     |         |   |     |
|----------|---|-----|---------|---|-----|
| DESIGNED | - | --- | REVISED | - | --- |
| DRAWN    | - | --- | REVISED | - | --- |
| CHECKED  | - | --- | REVISED | - | --- |
| DATE     | - | --- | REVISED | - | --- |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

I-55 OVER KANKAKEE RIVER  
 BRIDGE REHABILITATION

I-55  
 CROSS SECTIONS

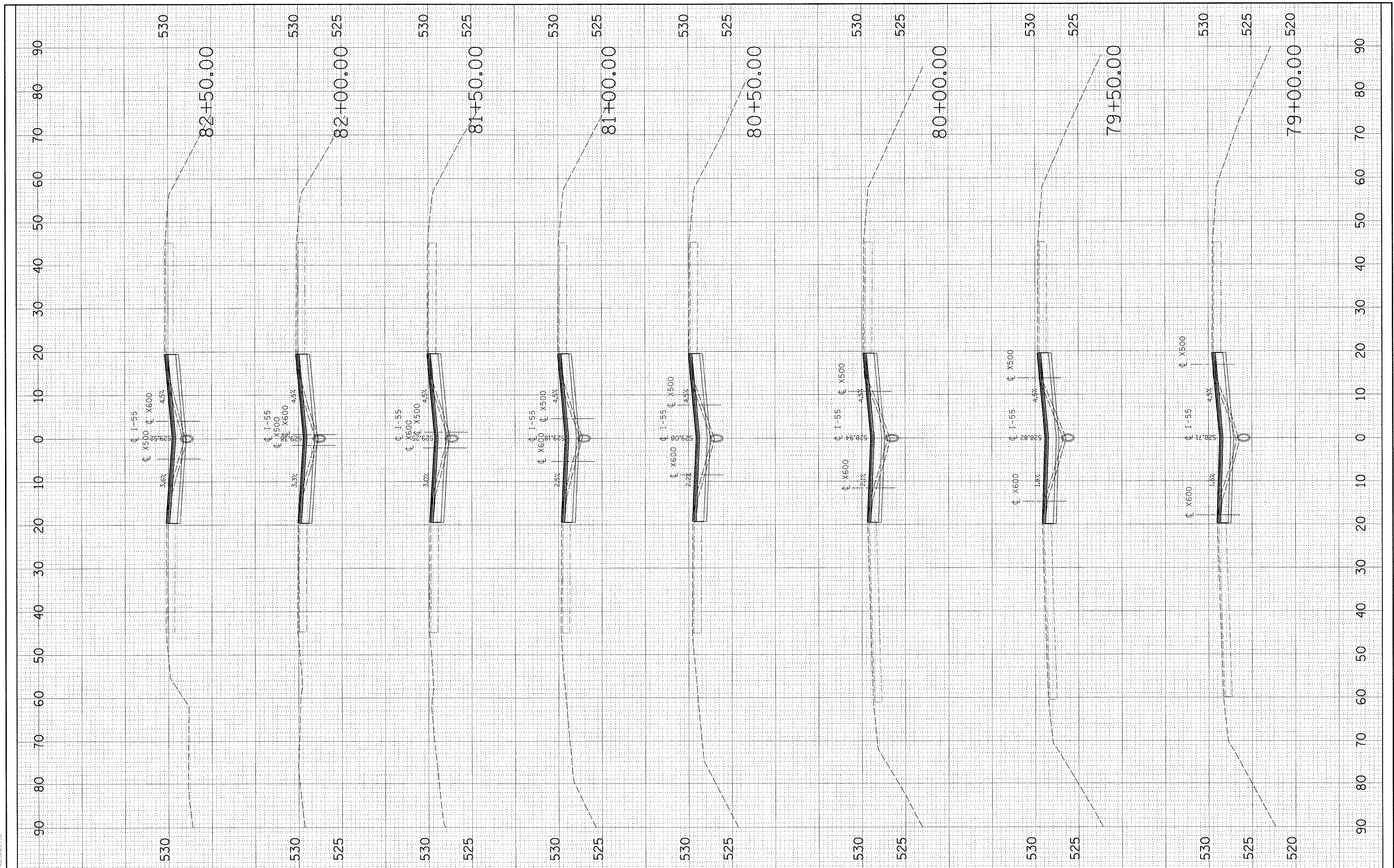
SCALE: \_\_\_\_\_ SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. 76+00.00 TO STA. 78+50.00

|   |               |        |              |           |
|---|---------------|--------|--------------|-----------|
| F.A. RTE.   | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
| 55  | 88 (B&B-1) BR | WILL   | 72           | 61        |
| CONTRACT NO. 62930                                  |               |        |              |           |
| FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT |               |        |              |           |

|               |               |    |      |
|---------------|---------------|----|------|
| FINAL SURVEY  | SURVEYED      | BY | DATE |
| NOTE BOOK     | PLOTTED       |    |      |
| AREAS CHECKED | TEMPLATE      |    |      |
| NO.           | AREAS CHECKED |    |      |

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|-----------------|---------------|----|------|
| ORIGINAL SURVEY | SURVEYED      | BY | DATE |
| NOTE BOOK       | PLOTTED       |    |      |
| AREAS CHECKED   | TEMPLATE      |    |      |
| NO.             | AREAS CHECKED |    |      |

PLOT DATE = 6/25/2009  
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 PLOT NAME = XS BRIDGE  
 MODEL = XS BRIDGE.L6



200 West Front Street  
 Wheaton, IL 60187

|          |   |     |         |   |     |
|----------|---|-----|---------|---|-----|
| DESIGNED | - | --- | REVISED | - | --- |
| DRAWN    | - | --- | REVISED | - | --- |
| CHECKED  | - | --- | REVISED | - | --- |
| DATE     | - | --- | REVISED | - | --- |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

I-55 OVER KANKAKEE RIVER  
 BRIDGE REHABILITATION

I-55  
 CROSS SECTIONS

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. 79+00.00 TO STA. 82+50.00

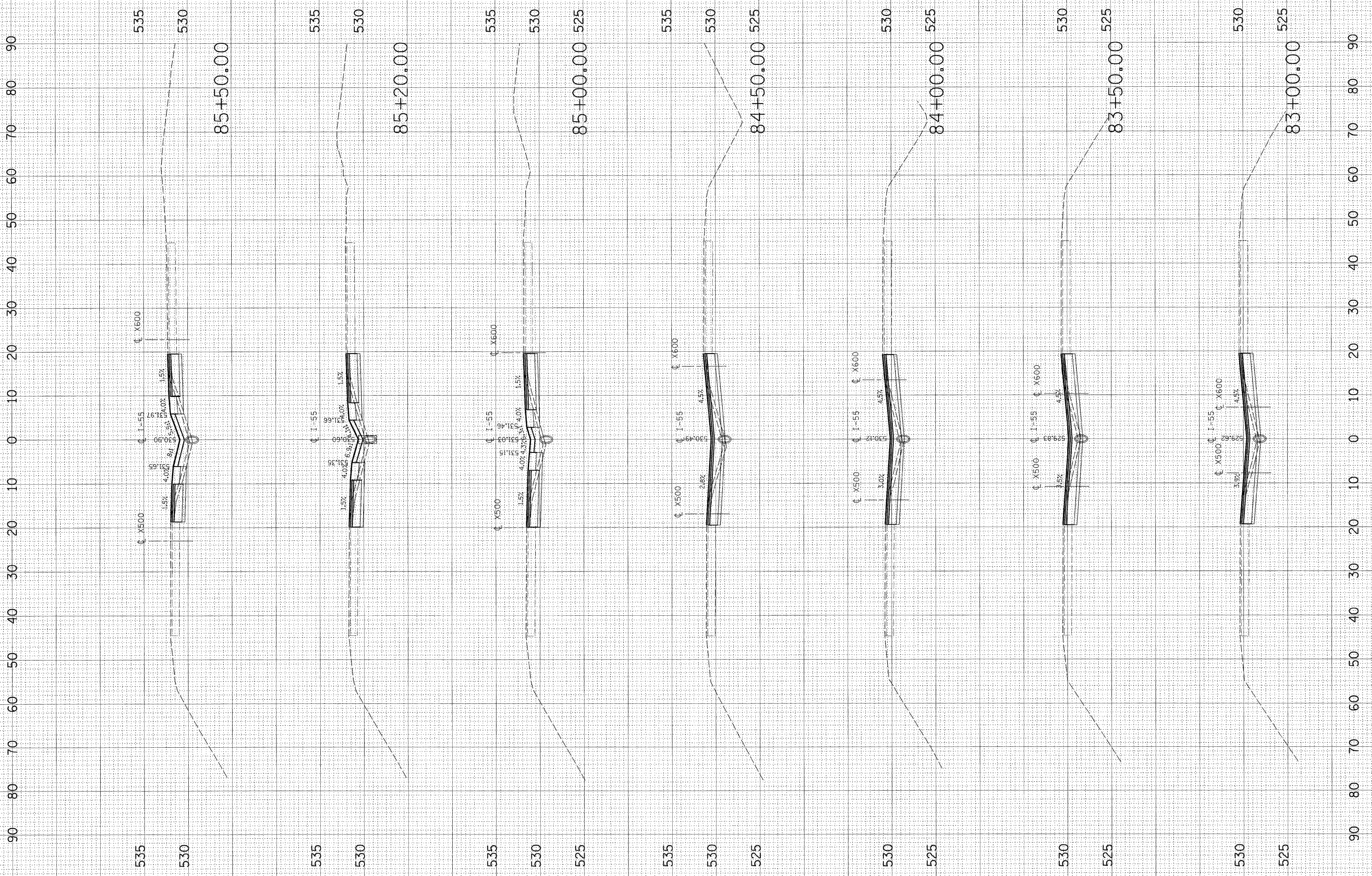
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|---|---------------|--------|--------------|-----------|
| F.A. RTE.   | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
| 55  | 88 (B&B-1) BR | WILL   | 72           | 62        |
| CONTRACT NO. 62930                                  |               |        |              |           |
| FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT |               |        |              |           |



|               |               |    |      |
|---------------|---------------|----|------|
| FINAL SURVEY  | SURVEYED      | BY | DATE |
| NOTE BOOK     | PLOTTED       |    |      |
| AREAS CHECKED | TEMPLATE      |    |      |
| NO.           | AREAS CHECKED |    |      |

|                 |               |    |      |
|-----------------|---------------|----|------|
| ORIGINAL SURVEY | SURVEYED      | BY | DATE |
| NOTE BOOK       | PLOTTED       |    |      |
| AREAS CHECKED   | TEMPLATE      |    |      |
| NO.             | AREAS CHECKED |    |      |

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 PLOT NAME = XS SHEET L7



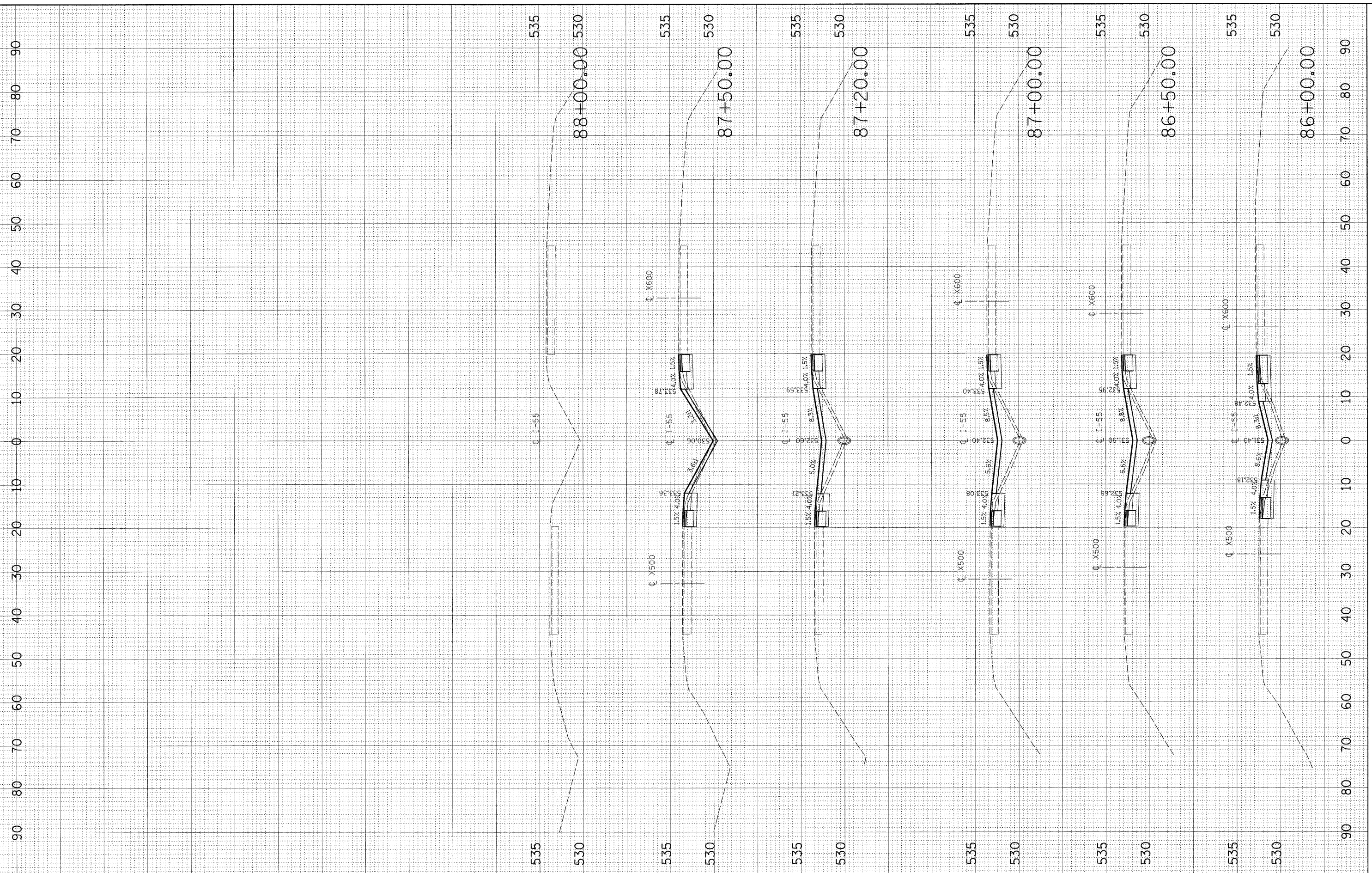
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|----------------------------------|--|------------|-----------|---|---|--------------------------------------|--------------------|---|---------------|-------------------|----------------|
| <br>Excellence through Ownership | 200 West Front Street<br>Wheaton, IL 60187 | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>I-55 OVER KANKAKEE RIVER</b><br><b>BRIDGE REHABILITATION</b> | <b>I-55</b><br><b>CROSS SECTIONS</b> | F.A. RTE. = 55     | SECTION = 88 (B&B-1) BR                         | COUNTY = WILL | TOTAL SHEETS = 72 | SHEET NO. = 63 |
|                                  |  | DRAWN -    | REVISED - |   |   |                                      | CONTRACT NO. 62930 |   |               |                   |                |
| DATE -                           |  | REVISED -  | REVISED - | SCALE: _____ SHEET NO. ___ OF ___ SHEETS                        |   | STA. 83+00.00 TO STA. 85+50.00       |                    | FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT |               |                   |                |



|               |               |    |      |
|---------------|---------------|----|------|
| FINAL SURVEY  | SURVEYED      | BY | DATE |
| NOTE BOOK     | PLOTTED       |    |      |
| AREAS CHECKED | TEMPLATE      |    |      |
| NO.           | AREAS CHECKED |    |      |

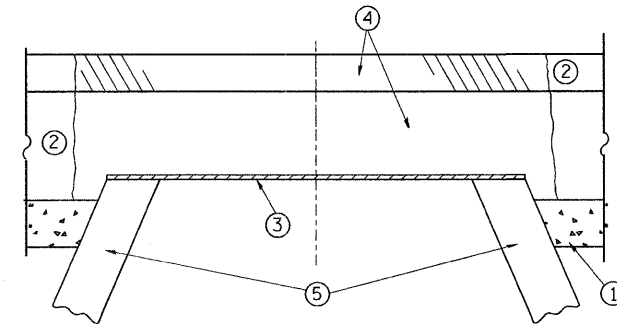
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| ORIGINAL SURVEY | SURVEYED      | BY | DATE |
| NOTE BOOK       | PLOTTED       |    |      |
| AREAS CHECKED   | TEMPLATE      |    |      |
| NO.             | AREAS CHECKED |    |      |

PLOT DATE = 6/25/2009  
 FILE NAME = K:\1182518\Cadd\Geo\55\_P.s.d  
 USER SCALE = #P.LT SCALE\$  
 USER NAME = XS  
 MODEL = XS SHEET 18



|  |                |               |   |   |                                      |                    |                       |             |   |              |
|--|----------------|---------------|---|---|--------------------------------------|--------------------|-----------------------|-------------|---|--------------|
|  | DESIGNED - --- | REVISED - --- | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b>         | <b>I-55 OVER KANKAKEE RIVER</b><br><b>BRIDGE REHABILITATION</b> | <b>I-55</b><br><b>CROSS SECTIONS</b> | F.A. RTE. 55       | SECTION 88 (B&B-1) BR | COUNTY WILL | TOTAL SHEETS 72                               | SHEET NO. 64 |
|  | DRAWN - ---    | REVISED - --- |   |   |                                      | CONTRACT NO. 62930 |                       |             | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |              |
| 200 West Front Street<br>Wheaton, IL 60187 | CHECKED - ---  | REVISED - --- | SCALE: _____ SHEET NO. ___ OF ___ SHEETS STA. 86+00.00 TO STA. 88+00.00 |   |                                      |                    |                       |             |   |              |
| Excellence through Ownership               | DATE - ---     | REVISED - --- |   |   |                                      |                    |                       |             |   |              |

|                       |               |                           |              |           |
|-----------------------|---------------|---------------------------|--------------|-----------|
| F.A. RTE.             | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| 55                    | 88 (B&B-1) BR | WILL                      | 72           | 65        |
| STA.                  |               | TO STA.                   |              |           |
| FED. ROAD DIST. NO. 1 |               | ILLINOIS FED. AID PROJECT |              |           |



**CONSTRUCTION PROCEDURES**

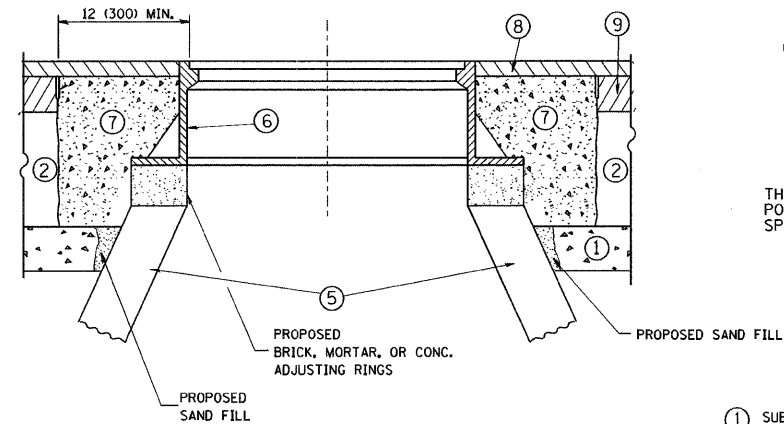
**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.



**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| REVISIONS   |          |
|-------------|----------|
| NAME        | DATE     |
| R. SHAH     | 10/25/94 |
| R. SHAH     | 01/30/95 |
| R. SHAH     | 03/10/95 |
| A. ABBAS    | 03/21/97 |
| R. WIEDEMAN | 05/14/04 |
| R. BORG     | 01/01/07 |
|             |          |
|             |          |
|             |          |

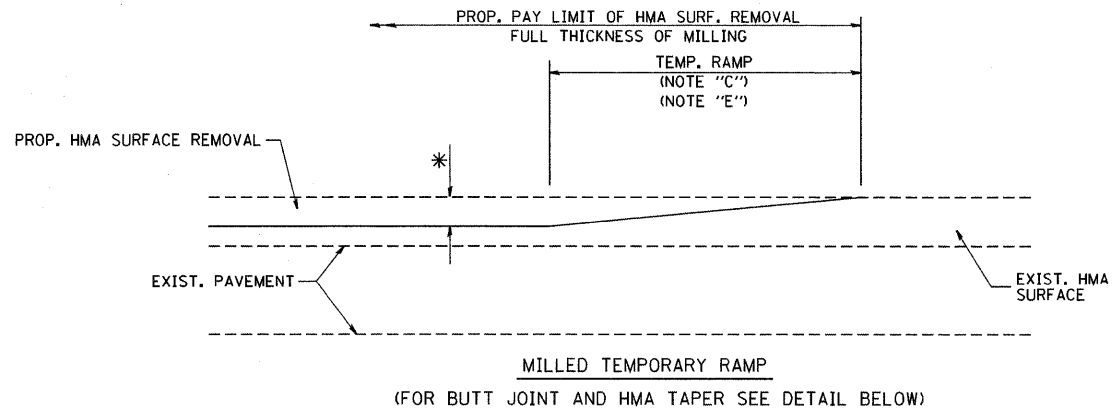
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: VERT. NONE  
HORIZ.

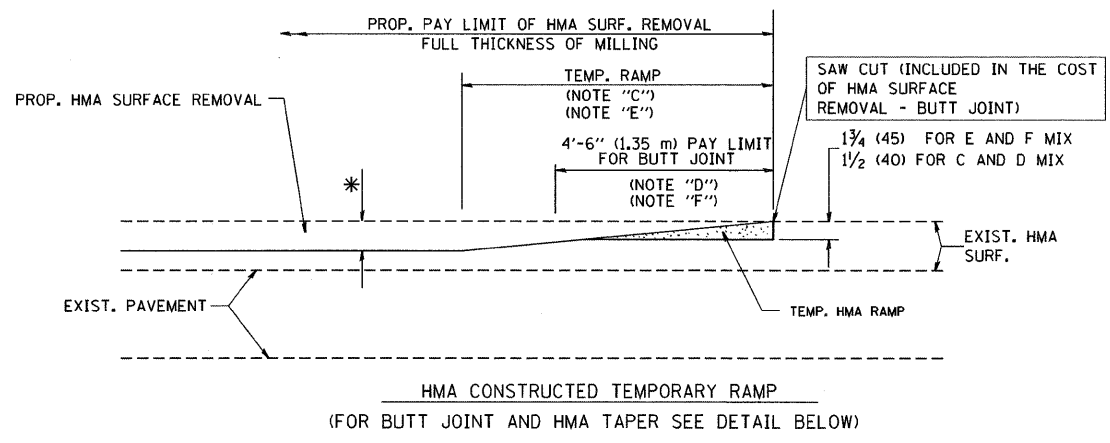
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CHECKED BY

BD600-03 (BD-8)

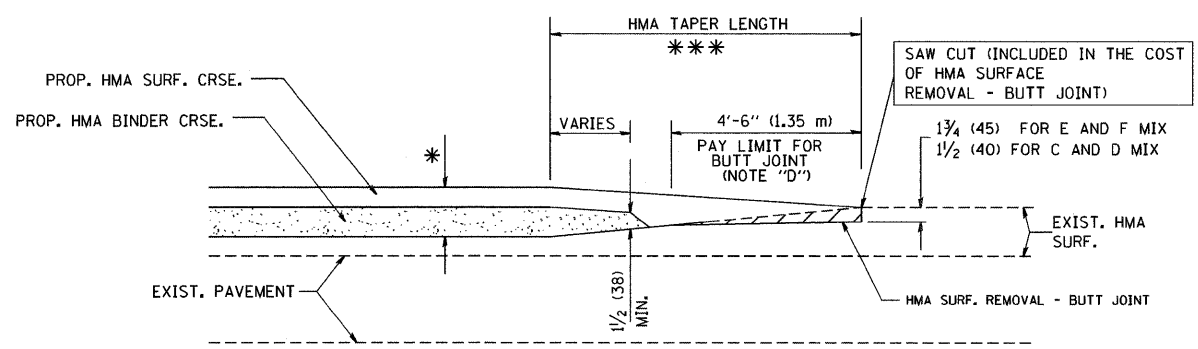
| F.A.I. RTE.           | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------------|---------------------------|--------------|-----------|
| 55                    | 88 (B&B-1) BR | WILL                      | 72           | 66        |
| STA.                  |               | TO STA.                   |              |           |
| FED. ROAD DIST. NO. 1 |               | ILLINOIS FED. AID PROJECT |              |           |



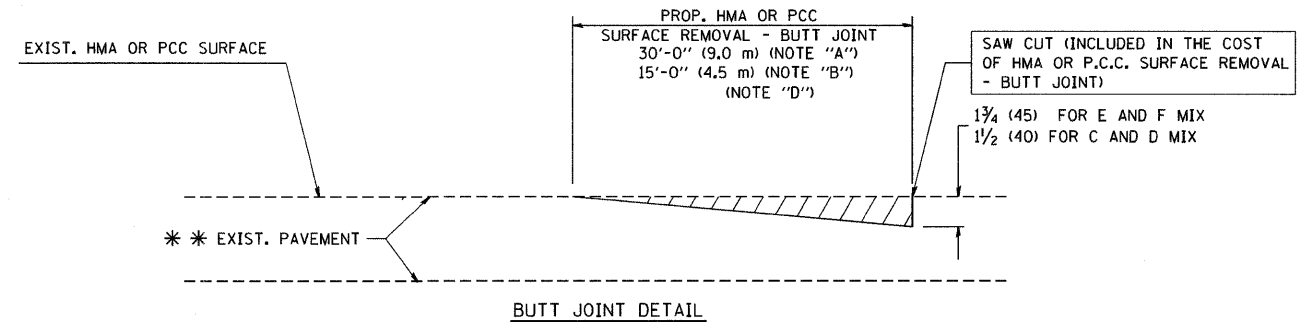
**OPTION 1**



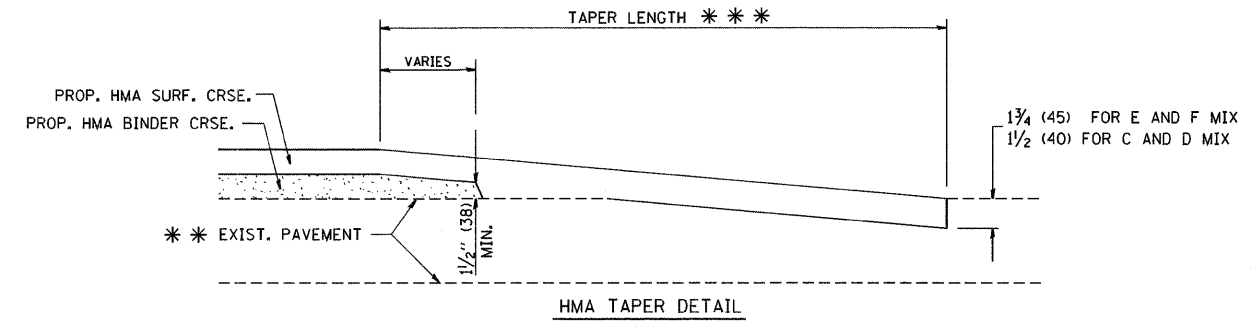
**OPTION 2**  
**TYPICAL TEMPORARY RAMP**



**BUTT JOINT AND HMA TAPER**  
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

| REVISIONS  |          |
|------------|----------|
| NAME       | DATE     |
| M. DE YONG | 6-13-90  |
| M. DE YONG | 7-3-90   |
| M. DE YONG | 3-27-92  |
| R. SHAH    | 09/09/94 |
| R. SHAH    | 10/25/94 |
| A. ABBAS   | 03/21/97 |
| M. GOMEZ   | 04/06/01 |
| R. BORO    | 01/01/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: VERT. NONE  
HORIZ.

DRAWN BY

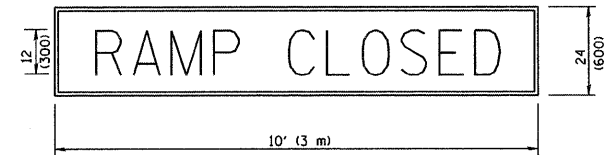
CHECKED BY

BD400-05 (VI-BD32)

PLOT DATE = 3/9/2007  
FILE NAME = K:\dashed\bd32.dgn  
PLOT SCALE = 1/8" = 1'-0"  
USER NAME = bborro01

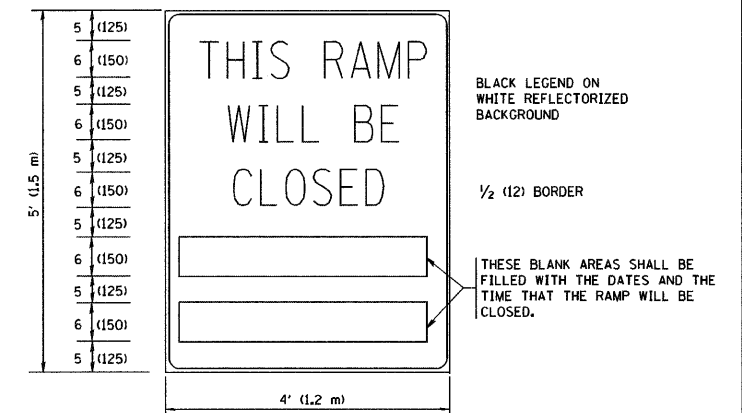
| F.A.I. RTE.           | SECTION       | COUNTY   | TOTAL SHEETS | SHEET NO.        |
|-----------------------|---------------|----------|--------------|------------------|
| 55                    | 88 (B&B-1) BR | WILL     | 72           | 67               |
| STA.                  |               | TO STA.  |              |                  |
| FED. ROAD DIST. NO. 1 |               | ILLINOIS |              | FED. AID PROJECT |

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND  
1 (25) BORDER  
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMP.

RAMP CLOSURE ADVANCE INFORMATION SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

1. CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
2. STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
3. A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
4. ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
5. THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
7. THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY FOUR (24) HOURS IN LENGTH.

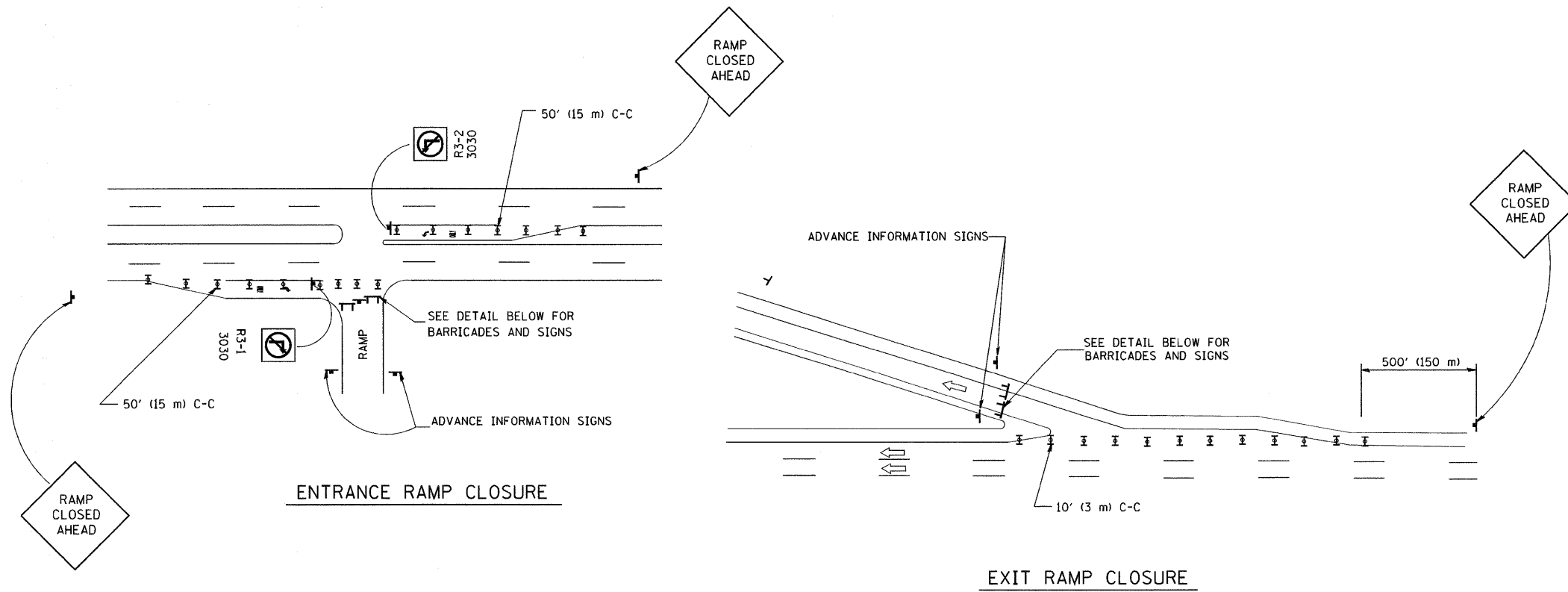
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| REVISIONS                        |       |
|----------------------------------|-------|
| NAME                             | DATE  |
| DWS                              | 2-83  |
| DWS                              | 1/90  |
| DWS                              | 9/94  |
| DWS                              | 12/94 |
| DWS/JAF                          | 12/02 |
| JAF                              | 2/06  |
| SPB                              | 1/07  |
| Revise devices to meet NCHRP 350 | 4/03  |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**FREWAY  
ENTRANCE AND EXIT RAMP  
CLOSURE DETAILS**

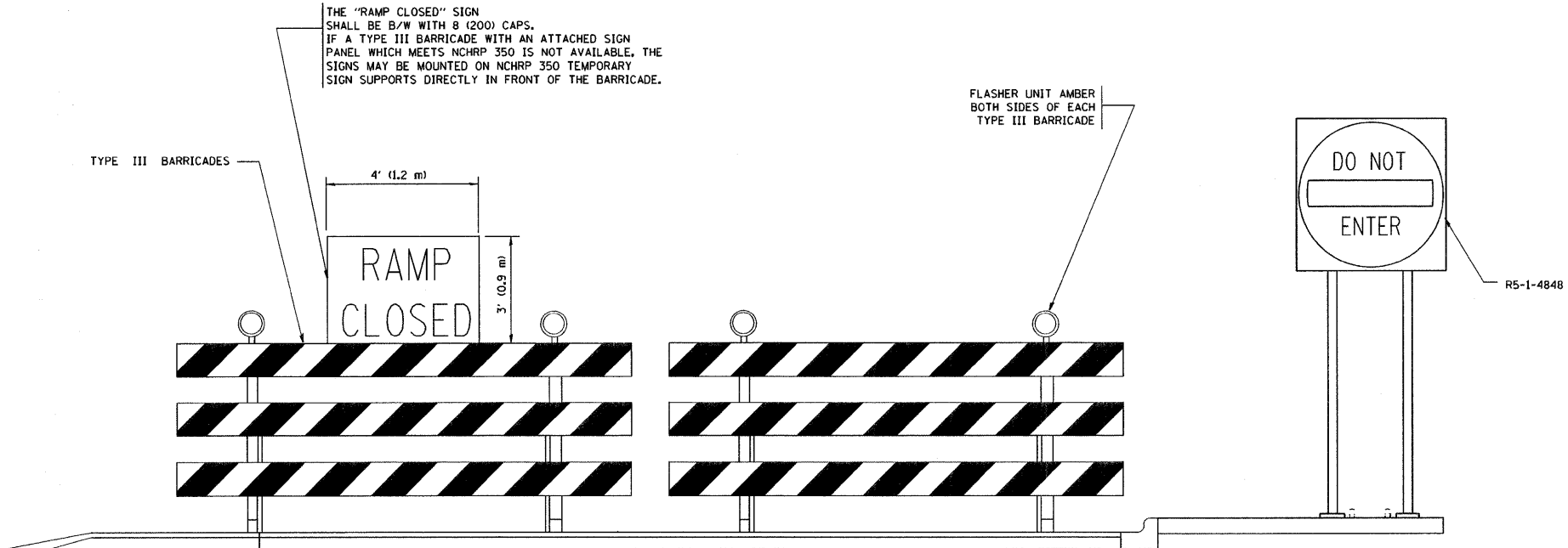
SCALE: NONE

DRAWN BY  
CHECKED BY  
TC-8



THE "RAMP CLOSED" SIGN SHALL BE B/W WITH 8 (200) CAPS. IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.

FLASHER UNIT AMBER BOTH SIDES OF EACH TYPE III BARRICADE



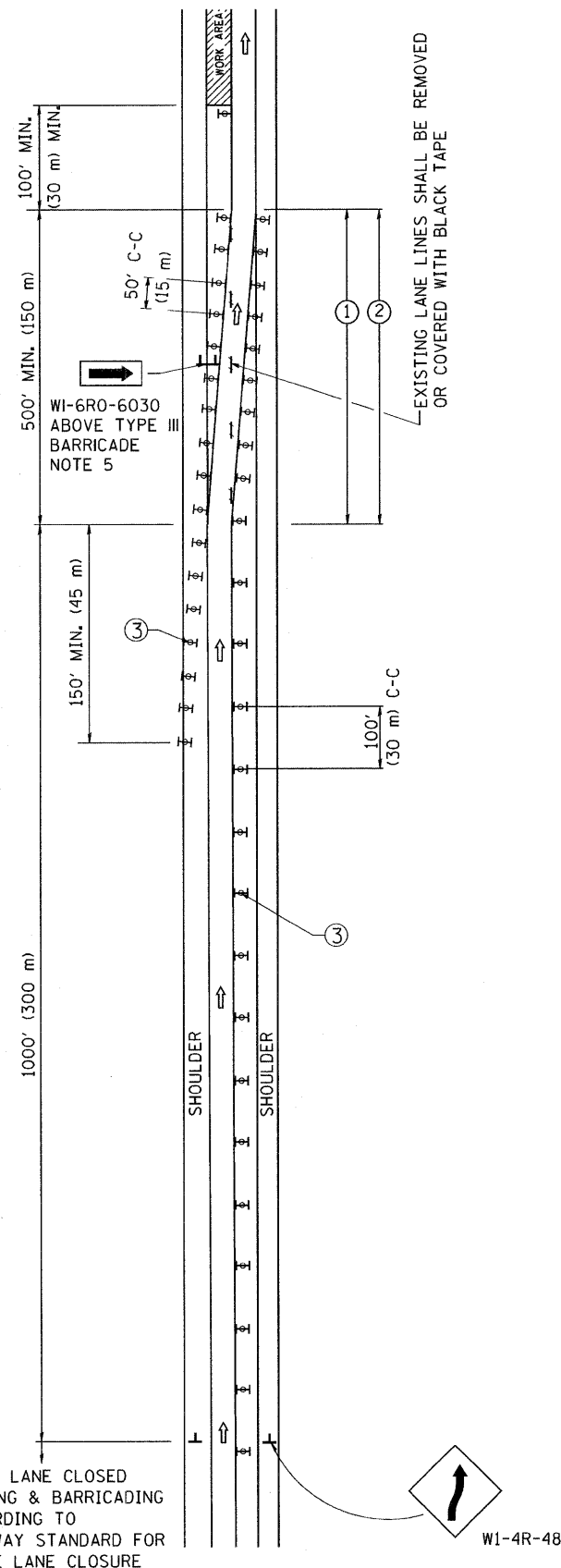
DETAIL FOR REQUIRED BARRICADES & SIGNS

- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
  - ⌈ TYPE III BARRICADE WITH FLASHING LIGHT

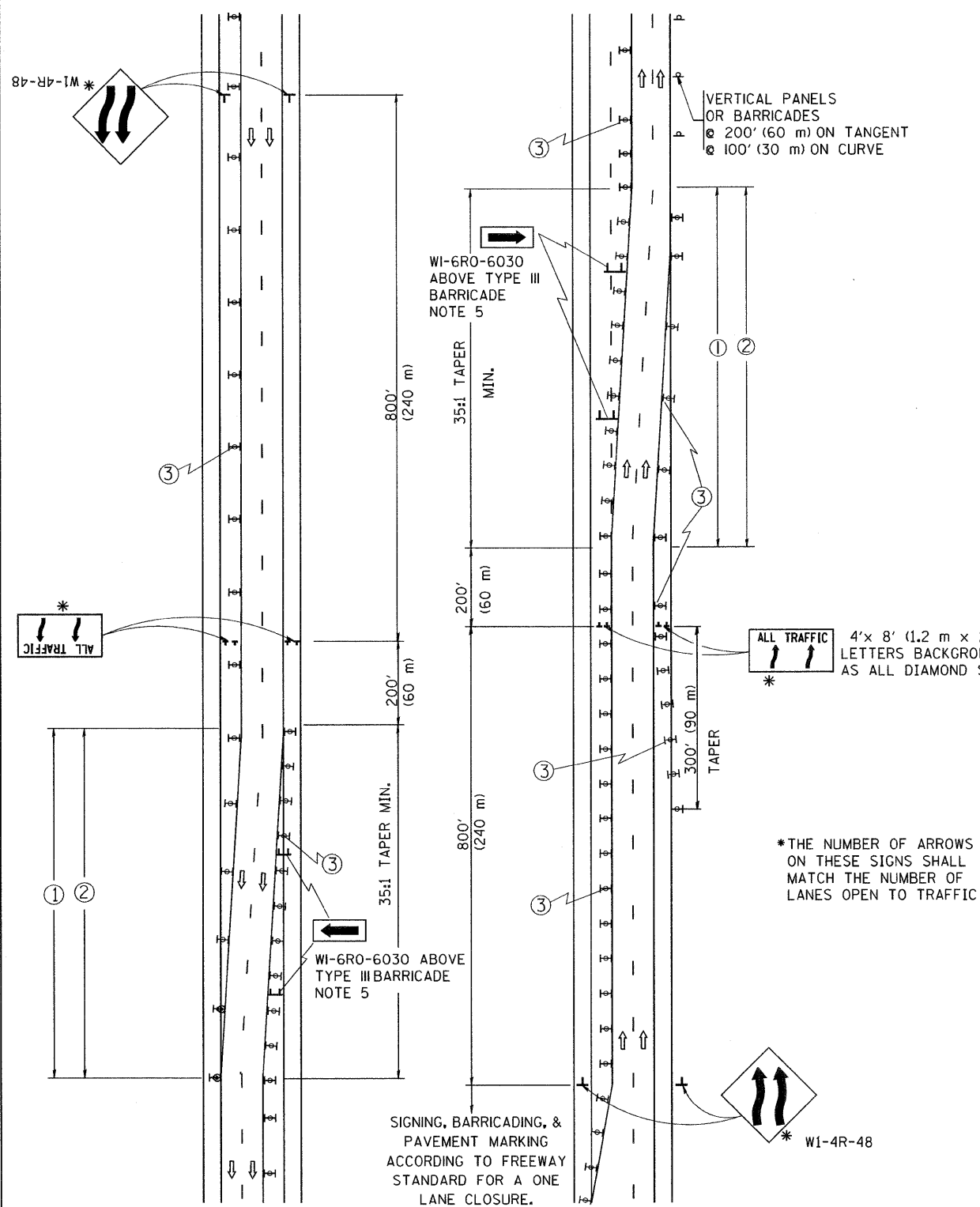
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FILE NAME = K:\data\m\c88.dgn  
PLOT SCALE = 50/800 / IN.  
USER NAME = bauerd

| F.A.I. RTE.           | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------------|---------------------------|--------------|-----------|
| 55                    | 88 (B&B-1) BR | WILL                      | 72           | 68        |
| STA.                  |               | TO STA.                   |              |           |
| FED. ROAD DIST. NO. 1 |               | ILLINOIS FED. AID PROJECT |              |           |

### SINGLE LANE WEAVE



### MULTI-LANE WEAVE



#### GENERAL NOTES

- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED OR COVERED WITH BLACK TAPE. PAVEMENT MARKING REMOVAL OR BLACK TAPE SHALL NOT BE REQUIRED FOR LANE CLOSURES UNDER 24 HOURS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVE LANE LINES SHALL BE 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.
- IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.

#### SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH MONO-DIRECTIONAL STEADY BURNING LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| REVISIONS |          |
|-----------|----------|
| NAME      | DATE     |
| DWS       | 2/87     |
| DWS       | 1/90     |
| DWS       | 12/27/94 |
| DWS       | 11/96    |
| JAF       | 4/03     |
| JAF       | 2/06     |
| SPB       | 1/07     |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL DETAILS  
 FOR FREEWAY  
 SINGLE & MULTI-LANE WEAVE

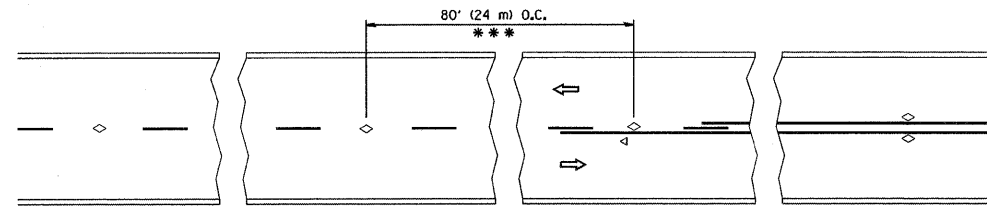
SCALE: NONE

DRAWN BY R.H.

CHECKED BY

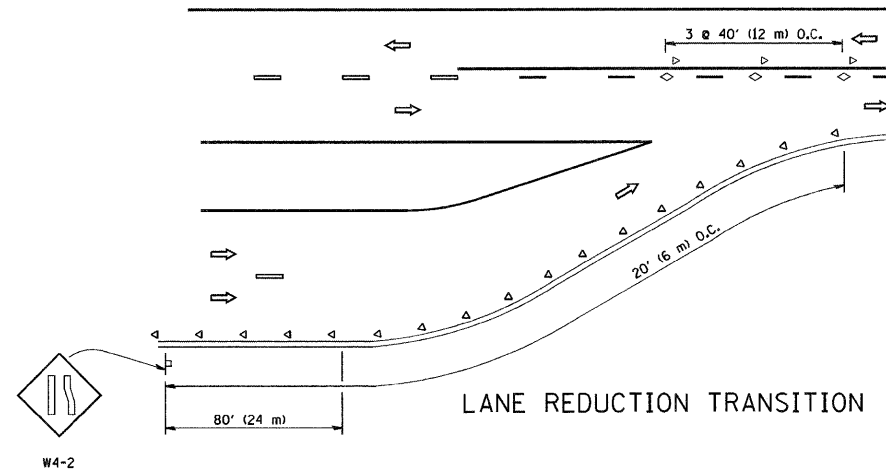
TC-9

| F.A.I. RTE.           | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------------|---------------------------|--------------|-----------|
| 55                    | 88 (B&B-1) BR | WILL                      | 72           | 69        |
| STA.                  |               | TO STA.                   |              |           |
| FED. ROAD DIST. NO. 1 |               | ILLINOIS FED. AID PROJECT |              |           |

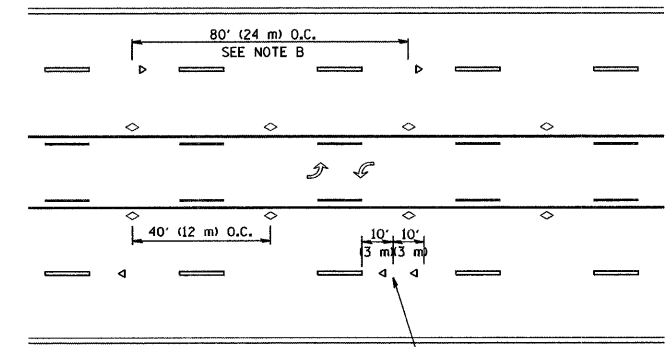


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

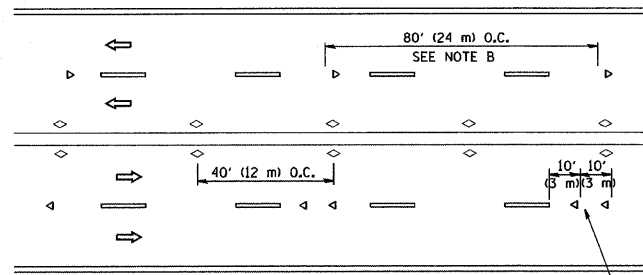
TWO-LANE/TWO-WAY



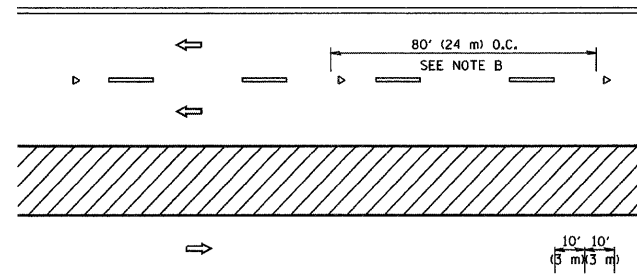
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

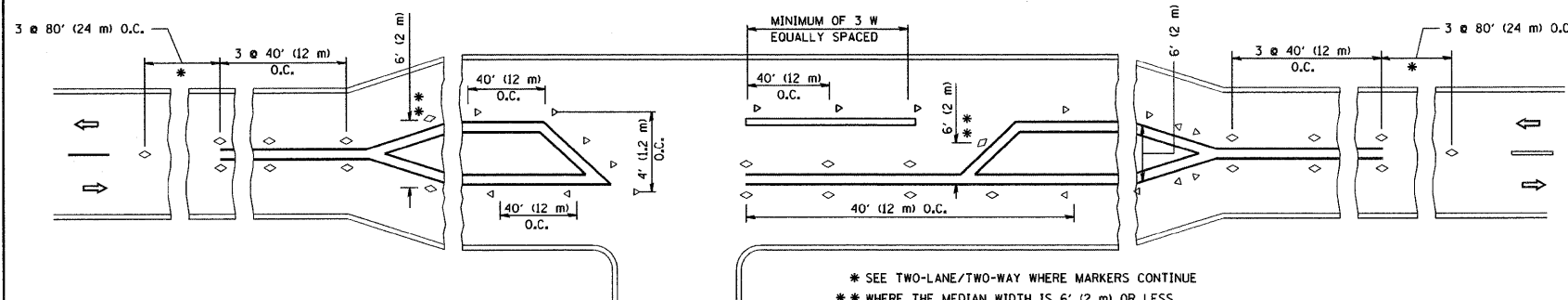
- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| T. RAMMACHER | 09-19-94 |
| T. RAMMACHER | 03-12-99 |
| T. RAMMACHER | 01-06-00 |
|              |          |
|              |          |
|              |          |
|              |          |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT  
 MARKERS (SNOW-PLOW RESISTANT)

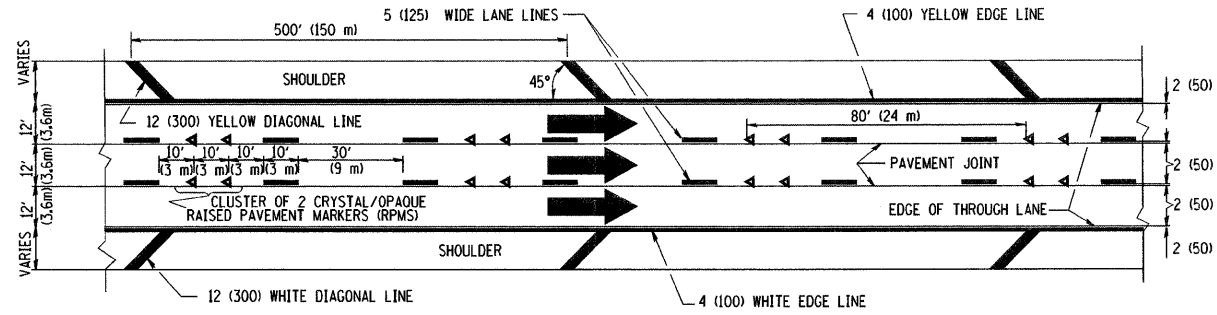
SCALE: NONE

DRAWN BY CADD  
 CHECKED BY  
 TC-11



| F.A.I. RTE.           | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------------|---------------------------|--------------|-----------|
| 55                    | 88 (B&B-1) BR | WILL                      | 72           | 70        |
| STA.                  |               | TO STA.                   |              |           |
| FED. ROAD DIST. NO. 1 |               | ILLINOIS FED. AID PROJECT |              |           |

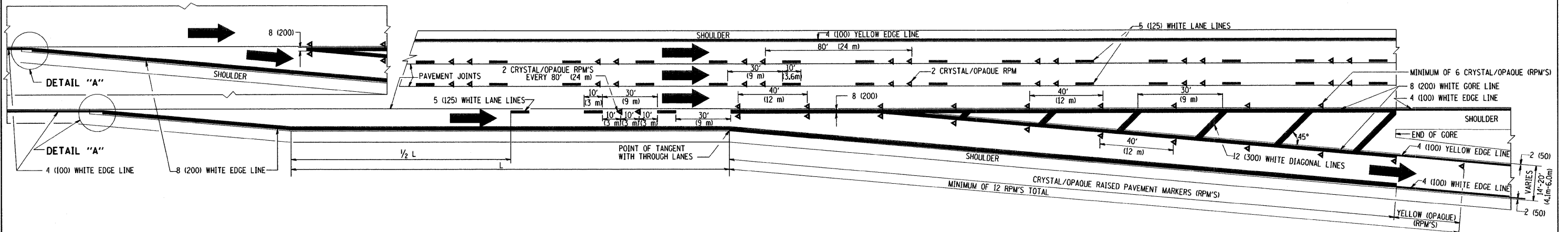
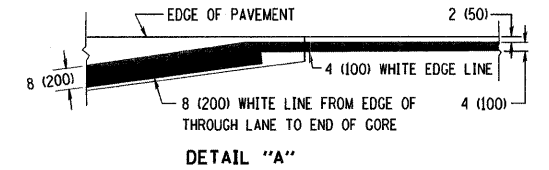
THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH  
THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH



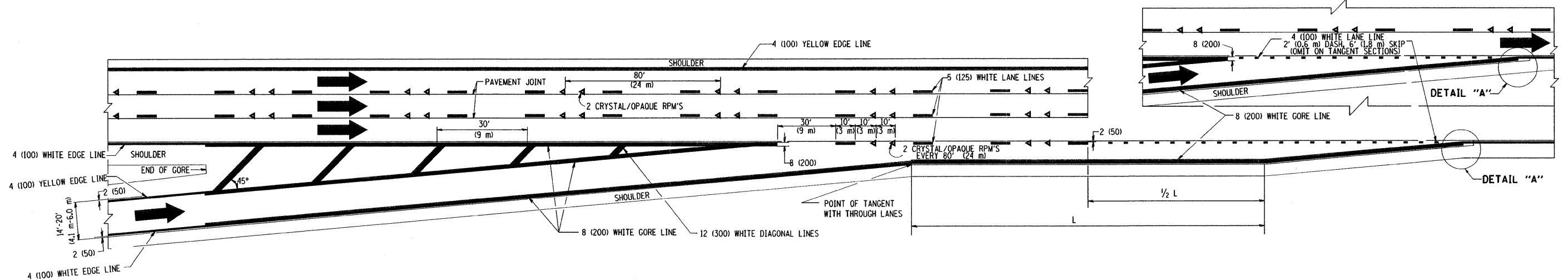
TYPICAL EDGE LINES & LANE LINES

NOTES:

1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC



TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS

| REVISIONS |      |
|-----------|------|
| NAME      | DATE |
| DWS       | 1/90 |
| DWS       | 5/91 |
| AH        | 3/96 |
| DWS       | 7/96 |
| JAF       | 2/06 |
| SPB       | 1/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

MULTI-LANE FREEWAY  
PAVEMENT MARKING  
DETAILS

SCALE: NONE

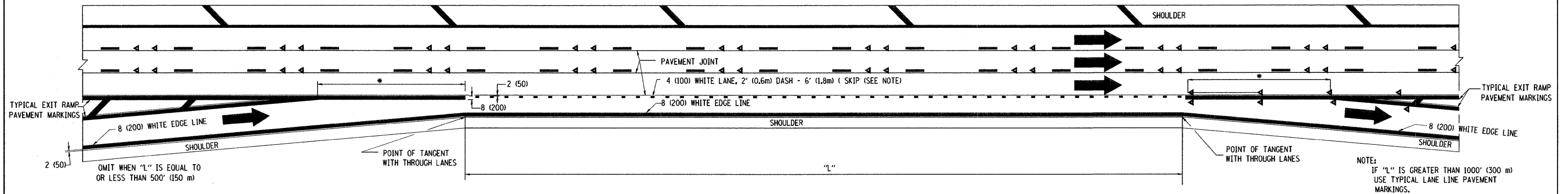
DRAWN BY C.A.D.D.

CHECKED BY

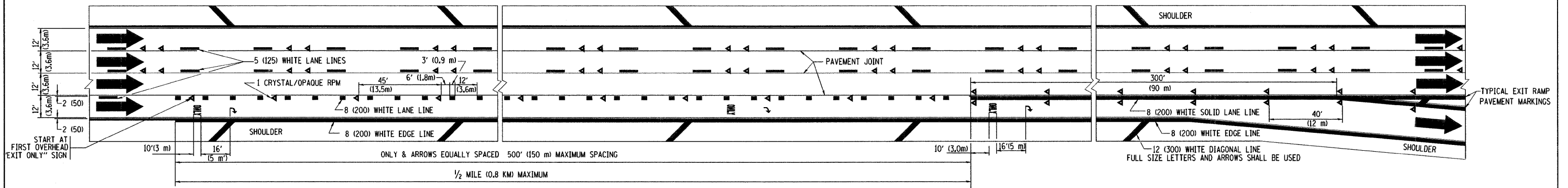
TC12 SHEET 1 OF 2



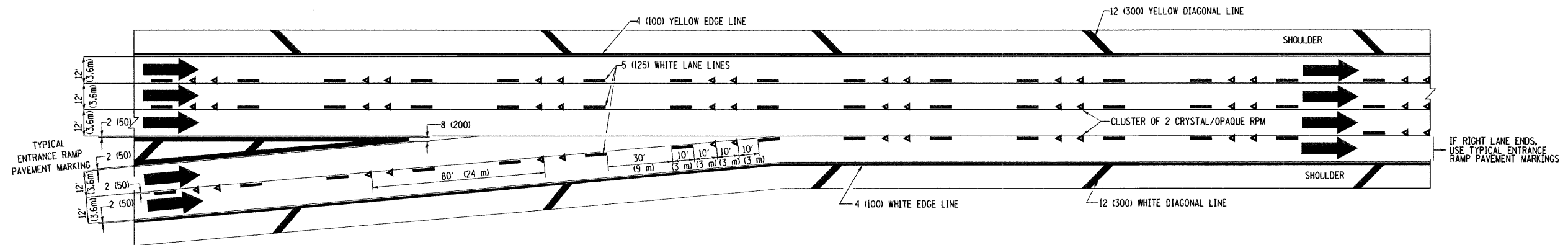
| F.A.I. RTE.           | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------------|---------------------------|--------------|-----------|
| 55                    | 88 (B&B-1) BR | WILL                      | 72           | 71        |
| STA.                  |               | TO STA.                   |              |           |
| FED. ROAD DIST. NO. 1 |               | ILLINOIS FED. AID PROJECT |              |           |



TYPICAL ENTRANCE/EXIT RAMP COMBINATION PAVEMENT MARKINGS



TYPICAL EXIT ONLY LANE PAVEMENT MARKINGS



TYPICAL TWO LANE ENTRANCE RAMP PAVEMENT MARKINGS

PLOT DATE = 3/6/2007  
 FILE NAME = K:\highway\12.dgn  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = bshard

| REVISIONS |      |
|-----------|------|
| NAME      | DATE |
| DWS       | 1/90 |
| DWS       | 5/91 |
| SPB       | 1/07 |
|           |      |
|           |      |
|           |      |
|           |      |
|           |      |
|           |      |

ILLINOIS DEPARTMENT OF TRANSPORTATION

MULTI-LANE FREEWAY  
 PAVEMENT MARKING  
 DETAILS

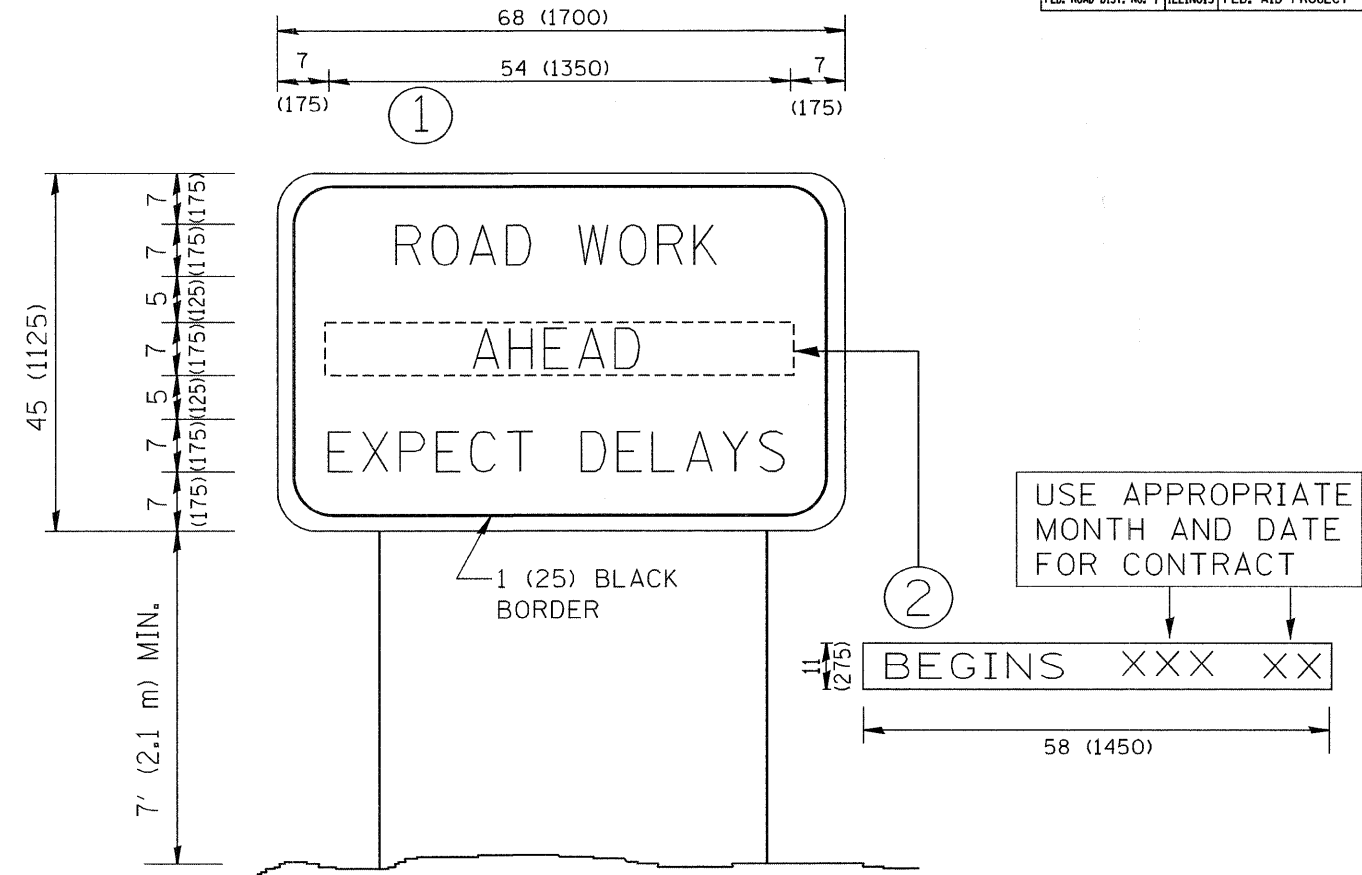
SCALE: NONE

DRAWN BY C.A.D.D.

CHECKED BY

TC12 SHEET 2 OF 2

| F.A.T. RTE.           | SECTION       | COUNTY   | TOTAL SHEETS     | SHEET NO. |
|-----------------------|---------------|----------|------------------|-----------|
| 55                    | 88 (B&B-1) BR | WILL     | 72               | 72        |
| STA.                  |               | TO STA.  |                  |           |
| FED. ROAD DIST. NO. 1 |               | ILLINOIS | FED. AID PROJECT |           |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

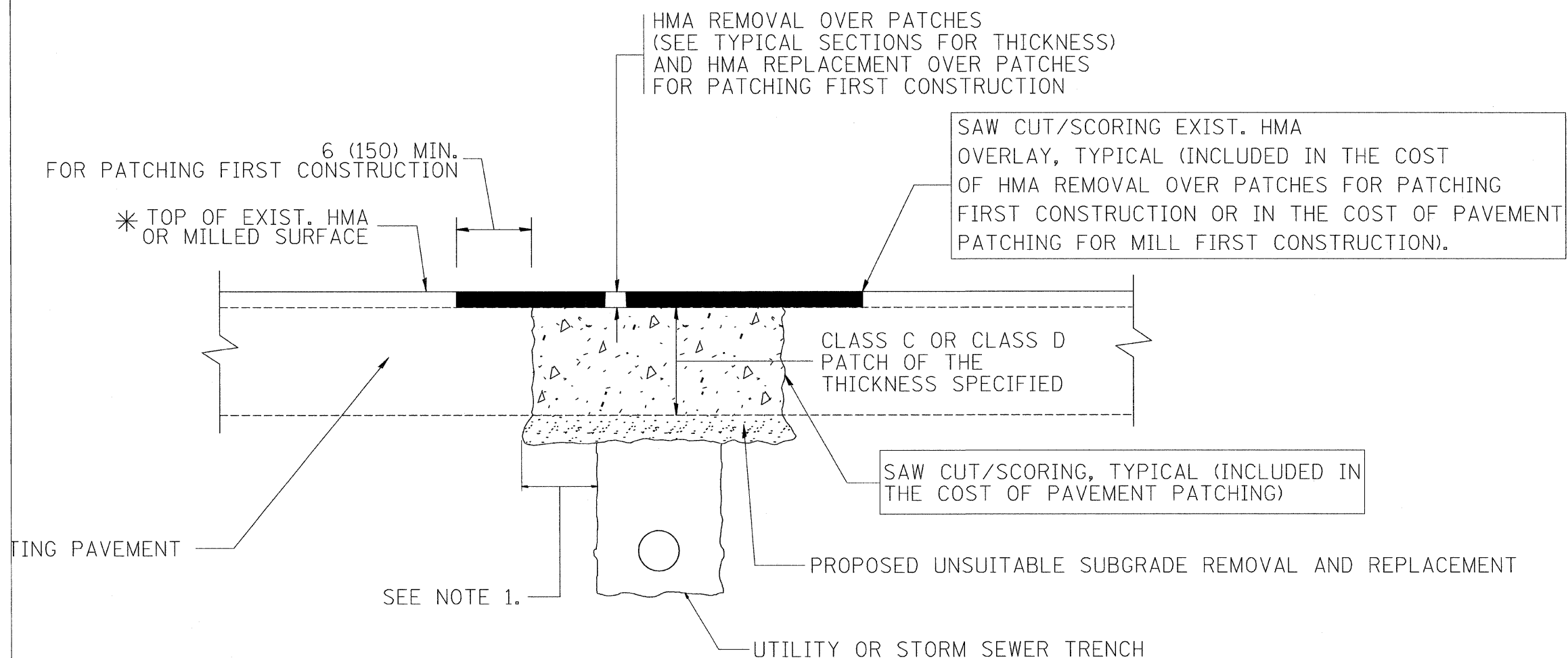
| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| R. MIRS      | 9-15-97  |
| R. MIRS      | 12-11-97 |
| T. RAMMACHER | 2-2-99   |
| C. JUXTUS    | 1-31-07  |
|              |          |
|              |          |
|              |          |
|              |          |
|              |          |
|              |          |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ARTERIAL ROAD  
 INFORMATION SIGN**

SCALE: NONE

DRAWN BY DESIGN  
 CHECKED BY

TC22



CONDITIONS FOR MATERIALS

THE FULL DEPTH PATCH SHALL BE 12 (300) WIDER OF THE TRENCH. MEASUREMENT AND BASIS RECURRING SPECIAL PATCHING WITH HOT-MIX BY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

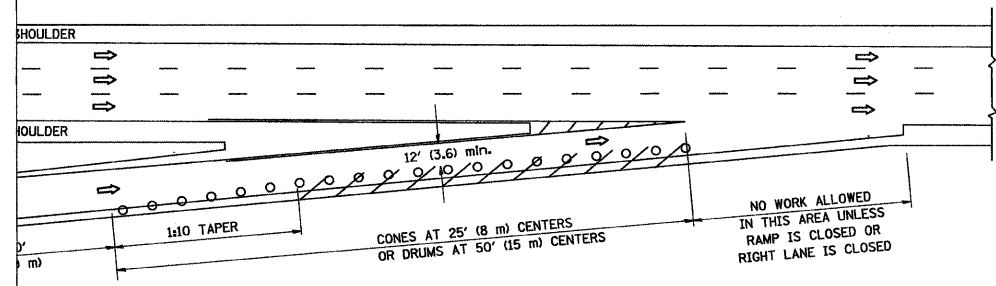
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

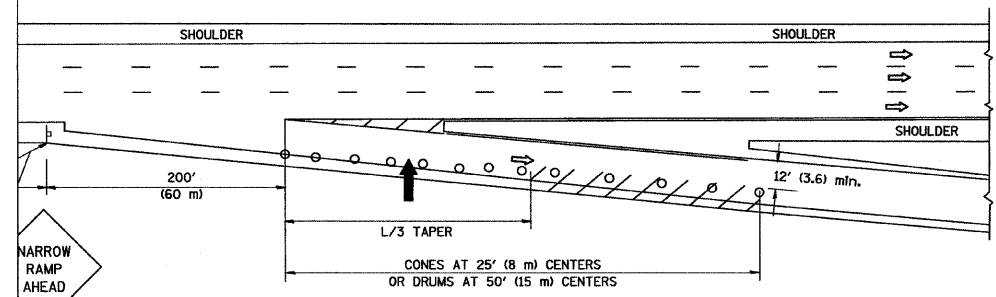
*Added Sheet 12-30-09*

|   |  |   |  |  |  |  |  |  |  |                |                     |             |                 |                    |  |
|---|--|---|--|--|--|--|--|--|--|----------------|---------------------|-------------|-----------------|--------------------|--|
| DESIGNED - R. SHAH<br>DRAWN -<br>CHECKED -<br>DATE - 10-25-94 | REVISED - A. ABBAS 04-27-98<br>REVISED - R. BORO 01-01-07<br>REVISED - R. BORO 09-04-07<br>REVISED - K. ENG 10-27-08 | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> |  |  |  | <b>PAVEMENT PATCHING FOR<br/>HMA SURFACED PAVEMENT</b> |  |  |  | F.A.I. RTE. 55 | SECTION BB(B2B-1)BR | COUNTY WILL | TOTAL SHEETS 72 | SHEET NO. 72A      |  |
|   |  | SCALE: NONE   |  |  |  | SHEET NO. 1 OF 1 SHEETS                                |  |  |  | STA. TO STA.   |                     |             |                 | CONTRACT NO. 62930 |  |
|   |  | FEDERAL ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT      |  |  |  |  |  |  |  |                |                     |             |                 |                    |  |
|   |  | <i>Added Sheet 12-30-09</i>                               |  |  |  |  |  |  |  |                |                     |             |                 |                    |  |

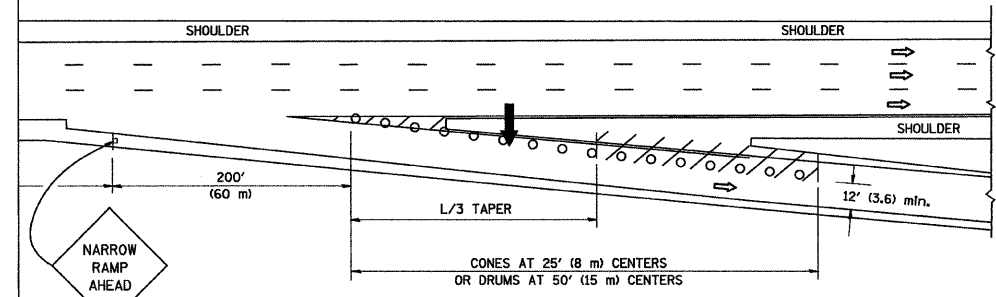
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

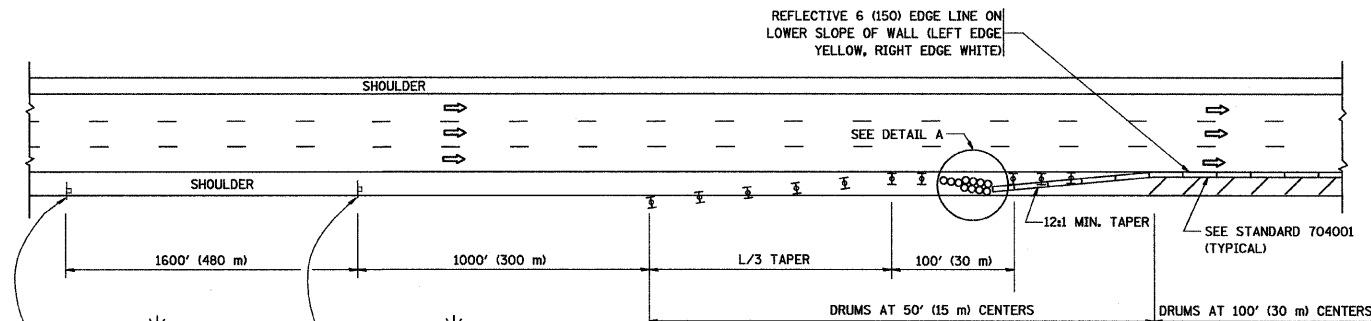
GENERAL NOTES

- THE "L" DISTANCE EQUALS:  
SPEED LIMIT FORMULAS  
45 mph (80 km/h) METRIC ENGLISH  
OR GREATER: L=0.65(W/S) L=(W/S)  
W = WIDTH OF OFFSET IN FEET (METERS)  
S = NORMAL POSTED SPEED MPH (KM/H)
- PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

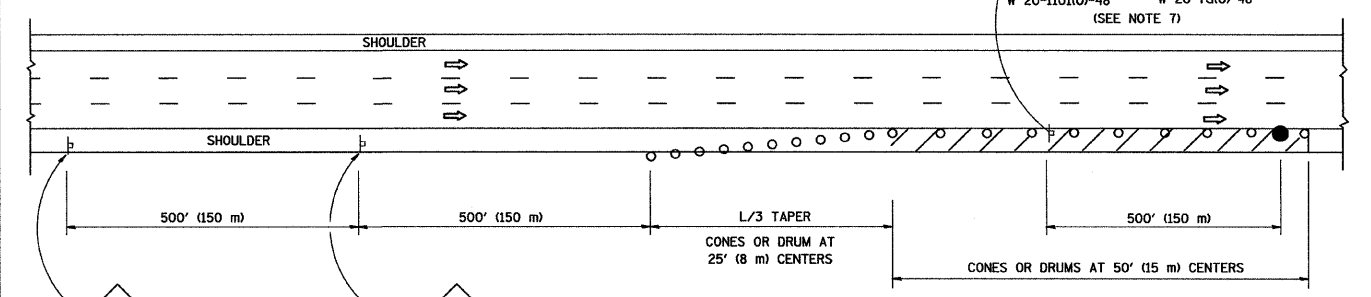
MENT SUPPORT

VERTICAL BARRICADE WITH STEADY LIGHT

SHOULDER CLOSURE DETAILS



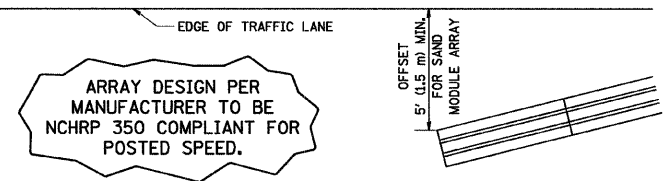
PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:

- VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A" IMPACT ATTENUATOR, TEMPORARY (SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

|            |        |           |              |
|------------|--------|-----------|--------------|
| DESIGNED - | D.W.S. | REVISED - | J.A.F. 12-02 |
| DRAWN -    | D.W.S. | REVISED - | 04-03        |
| CHECKED -  |        | REVISED - | J.A.F. 12-06 |
| DATE -     | 11-96  | REVISED - | S.P.B. 01-07 |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY  
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

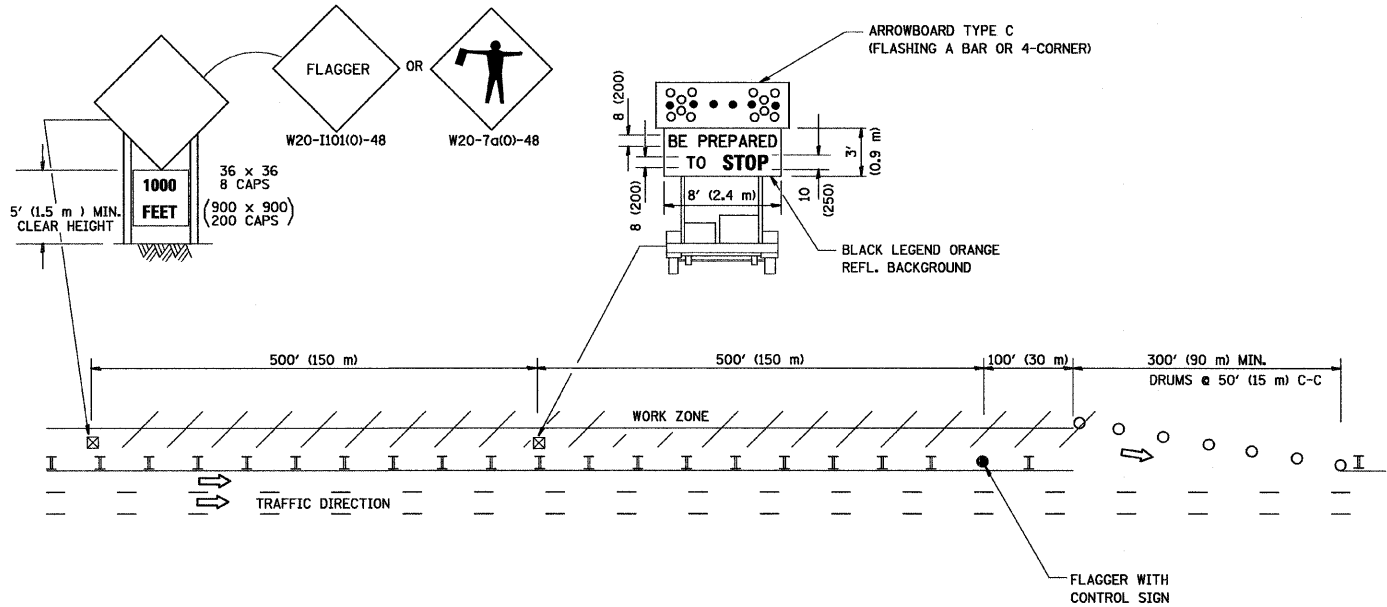
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

ADDED SHEET 12-30-09

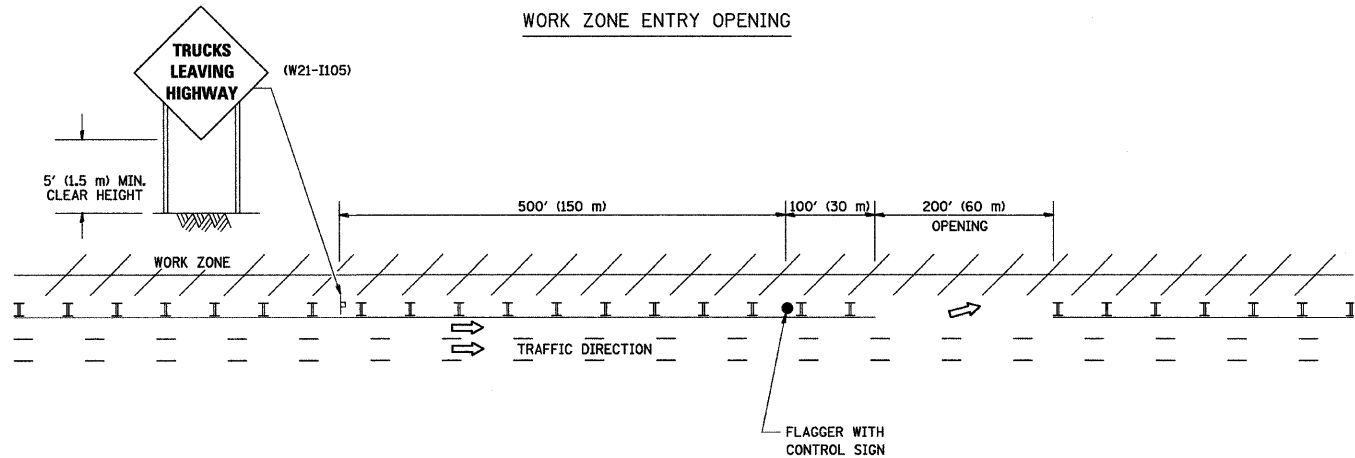
|   |               |        |                    |           |
|---|---------------|--------|--------------------|-----------|
| F.A.I. RTE.                                       | SECTION       | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 55  | 88 (B#B-1) BR | WILL   | 72                 | 72B       |
| TC-17   |               |        | CONTRACT NO. 62930 |           |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT |               |        |                    |           |

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

ADDED SHEET 12-30-09

|            |           |              |
|------------|-----------|--------------|
| DESIGNED - | REVISOR - | D.W.S. 08-98 |
| DRAWN -    | REVISOR - | J.A.F. 04-03 |
| CHECKED -  | REVISOR - | J.A.F. 02-06 |
| DATE -     | REVISOR - | S.P.B. 01-07 |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS  
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |            |        |                    |           |
|---|------------|--------|--------------------|-----------|
| F.A.I. RTE.                                     | SECTION    | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 55  | BB(BB-1)BR | WILL   | 72                 | 72C       |
| TC-18   |            |        | CONTRACT NO. 62930 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |            |        |                    |           |