

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2779	09-00246-00-RS	COOK	25	1
		ILLINOIS	CONTRACT NO. 63293	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

FAU ROUTE 2779 (EAST AVE)  
FROM ROOSEVELT RD TO GARFIELD ST  
SECTION 09-00246-00-RS  
PROJECT HD-9003 (618)  
RESURFACING  
COOK COUNTY  
C-91-682-09

FOR INDEX OF SHEETS, SEE SHEET NO. 2

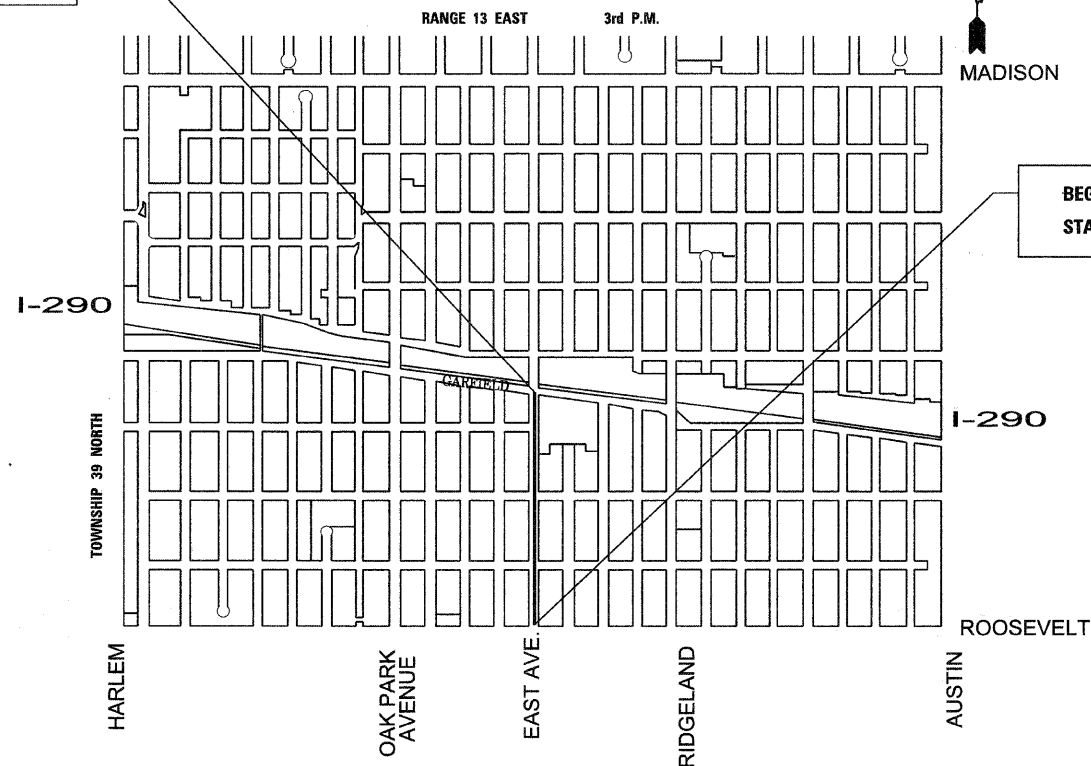
PROJECT LOCATED IN  
VILLAGE OF OAK PARK

ADT EAST AVENUE 4,050 (2006)  
EAST AVENUE POSTED SPEED LIMIT: 30 MPH  
EAST AVENUE CLASSIFICATION: COLLECTOR

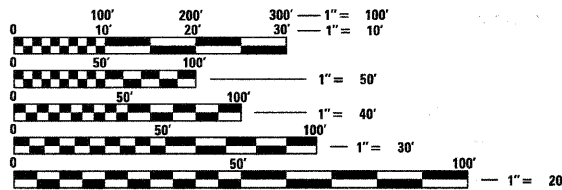


END PROJECT  
STA. 24 + 13.56

**LOCATION MAP**



BEGIN PROJECT  
STA. 1 + 48.41



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

FIELD ENGINEER: MARILIN SOLOMON

PROJECT LENGTH  
GROSS LENGTH = 2265.15 FT. = 0.429 MILE  
NET LENGTH = 2665.15 FT. = 0.429 MILE

CONTRACT NO. 63293

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

Approved DEC 9 20 09  
*James Butch*  
VILLAGE OF OAK PARK, VILLAGE ENGINEER

Passed JANUARY 29 20 10  
*Chris H. Herberholt*  
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

Releasing for Bid Based on Limited Review FEBRUARY 5, 20 10  
*Diana M. O'Keefe*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

*Jim Budrick*

Jim Budrick, P.E.  
Expires: 11-30-11

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OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
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<u>DISTRICT ONE DETAILS</u>	
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20	BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
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25	TC-21 TYPICAL MARKING FOR CLOSING STATE HIGHWAYS

STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
420001-07	PAVEMENT JOINTS
420101-04	24' JOINTED PCC PAVEMENT
420401-08	BRIDGE APPROACH PAVEMENT CONNECTOR
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
701301-03	LANE CLOSURE, 2L 2W SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, SW MOVING OPERATIONS-DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L 2W, UNDIVDED
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES, FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE, IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) FOR LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL CONSIDER INCLUDED IN THE COST OF THE ITEM REMOVED.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC, ARE FROM THE ROADWAY CENTERLINE.
- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE ENGINEER WILL IDENTIFY FINAL PATCH LOCATIONS IN THE FIELD. PAYMENT WILL BE IN ACCORDANCE WITH ARTICLES 109.03 AND SECTION 442.
- THE VERTICAL CONTROL IS REFERENCED TO NAVD 88 DATUM. A FACTOR OF 579.48 WAS SUBTRACTED TO ACHIEVE CHICAGO CITY DATUM. THE HORIZONTAL CONTROL IS REFERENCED TO STATE PLANE ZONE 1201(US FEET).

STORM SEWERS, WATER MAINS AND UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION ADJUSTMENT OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS BASED ON BEST AVAILABLE RECORD DATA. THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE COST OF VARIOUS STORM SEWER ITEMS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ALL UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
- ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENNANCES THAT MUST BE KEPT IN OPERATION.
- THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE VILLAGE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
- ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES FOR PROPOSED AND ADJUSTED STRUCTURES.

SIGNING AND STRIPING

- SEE IDOT STANDART DETAIL 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.

TRAFFIC CONTROL

- SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.

FILE NAME = INX_GenNotes.dgn	USER NAME = default	DESIGNED - RB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EAST AVENUE INDEX &amp; GENERAL NOTES</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN - LB	REVISED -			2779	09-00246-00-RS	COOK	25	2	
		CHECKED - LB	REVISED -			CONTRACT NO. 63293					
		DATE - SEPTEMBER 2009	REVISED -			ILLINOIS FED. AID PROJECT					
						SHEET NO. 2 OF 25 SHEETS		STA.		TO STA.	

# SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES				
ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL	100% FED
				1000-2A
20101200	TREE ROOT PRUNING	EACH	32	32
20800150	TRENCH BACKFILL	CU YD	119	119
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	277	277
25200100	SODDING	SQ YD	277	277
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	4	4
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	4	4
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	4	4
31101100	SUB-BASE GRANULAR MATERIAL, TYPE B	CU YD	156	156
40300100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	766	766
40600300	AGGREGATE (PRIME COAT)	TON	16	16
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	249	249
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	644	644
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	966	966
42001300	PROTECTIVE COAT	SQ YD	1741	1741
42001420	BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)	SQ YD	61	61
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1086	1086
42400800	DETECTABLE WARNINGS	SQ FT	100	100
44000100	PAVEMENT REMOVAL	SQ YD	1136	1136
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	7641	7641
44000600	SIDEWALK REMOVAL	SQ FT	1086	1086
44000700	APPROACH SLAB REMOVAL	SQ YD	130	130
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1026	1026
44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	23	23
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	25	25
50300225	CONCRETE STRUCTURES	CU YD	19	19
50300255	CONCRETE SUPERSTRUCTURE	CU YD	68	68
50300260	BRIDGE DECK GROOVING	SQ YD	205	205
50300300	PROTECTIVE COAT	SQ YD	205	205
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	16482	16482
50800515	BAR SPLICERS	EACH	162	162
550B0320	STORM SEWERS, CLASS B, TYPE 2 8"	FOOT	208	208
55100300	STORM SEWER REMOVAL 8"	FOOT	208	208
60202705	CATCH BASINS, TYPE A, 4'-DIAMETER, WITH SALVAGED FRAME AND GRATE	EACH	12	12

ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL	100% FED
60238500	INLETS, TYPE A, WITH SALVAGED FRAME AND GRATE	EACH	4	4
60250200	CATCH BASINS TO BE ADJUSTED	EACH	16	16
60253000	CATCH BASINS TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1
60255500	MANHOLES TO BE ADJUSTED	EACH	12	12
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	9	9
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	2	2
60266300	VALVE VAULTS TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1
60266600	VALVE BOXES TO BE ADJUSTED	EACH	4	4
60500050	REMOVING CATCH BASINS	EACH	12	12
67100100	MOBILIZATION	L SUM	1	1
70102550	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	EACH	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
* 78003110	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4"	FOOT	520	520
* 78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	496	496
* 78003150	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 12"	FOOT	139	139
* 78003180	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 24"	FOOT	132	132
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	35	35
* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	343	343
* 78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	40	40
K1005881	TREE TRUNK PROTECTION	EACH	10	10
X0323389	STORM SEWER CONNECTION	EACH	17	17
LR420033	PORTLAND CEMENT CONCRETE PAVEMENT, 9" (SPECIAL)	SQ YD	1161	1161
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0017099	DOWEL BAR ASSEMBLY	EACH	23	23
Z0017202	DOWEL BARS 1 1/2"	EACH	23	23
	* DENOTES SPECIALTY ITEM			

CURB AND GUTTER			44001700 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
LOCATION	OFFSET		LIN FT
FROM	TO	LT/RT	
1+52	2+24	LT	72
1+72	1+82	RT	10
2+35	2+58	RT	23
2+61	2+71	LT	10
3+71	3+81	LT	10
3+75	3+98	RT	23
4+36	4+46	RT	10
5+15	5+25	LT	10
5+19	5+40	RT	21
6+99	7+14	RT	15
7+04	7+14	LT	10
7+80	7+89	LT	10
7+81	7+91	RT	10
10+33	10+43	RT	10
10+35	10+45	LT	10
11+45	11+55	RT	10
12+03	12+13	LT	10
12+04	12+14	RT	10
13+67	13+77	RT	10
14+85	16+03	RT	118
15+91	16+01	LT	10
17+25	17+49	LT	24
17+62	17+80	RT	18
18+01	18+12	RT	11
18+24	18+34	LT	10
22+42	23+68	RT	126
21+46	23+78	LT	274
23+68	SE LIMIT	RT	76
24+08	NE LIMIT	RT	15
23+78	SW LIMIT	LT	35
24+17	NW LIMIT	LT	15
CURB AND GUTTER TOTALS			1026

SIDEWALK			42400200	42400800	31101200
LOCATION	OFFSET		PCC SIDEWALK, 5"	DETECTABLE WARNING	SUB-BASE GRANULAR MATERIAL, 2"
FROM	TO	LT/RT	SQ FT	SQ FT	CU YD
1+48	1+57	LT	40		0.25
1+91	1+95	LT	33		0.20
2+33	2+38	RT	20	10	0.12
2+55	2+60	RT	25	10	0.15
2+69	2+71	LT	19		0.12
3+71	3+80	LT	30		0.19
3+95	3+98	RT	20		0.12
4+46	4+48	RT	17		0.10
5+42	5+49	LT	28		0.17
10+32	10+34	LT	17		0.10
12+07	12+14	LT	31		0.19
14+44	15+07	LT	90		0.56
15+11	15+17	RT	26		0.16
15+12	15+70	LT	99		0.61
14+85	14+92	RT	25		0.15
15+11	15+13	RT	19		0.12
15+36	15+39	RT	22		0.14
15+55	15+68	RT	37		0.23
15+80	15+93	RT	37		0.23
17+37	17+45	RT	13		0.08
17+40	17+47	LT	29		0.18
17+55	17+84	RT	45		0.28
17+86	17+94	RT	11		0.07
22+42	22+65	LT	54	20	0.33
23+67	23+97	LT	265	30	1.64
23+40	23+49	RT	34		0.21
23+76	LIMIT	RT		30	0
SIDEWALK TOTALS			1086	100	7

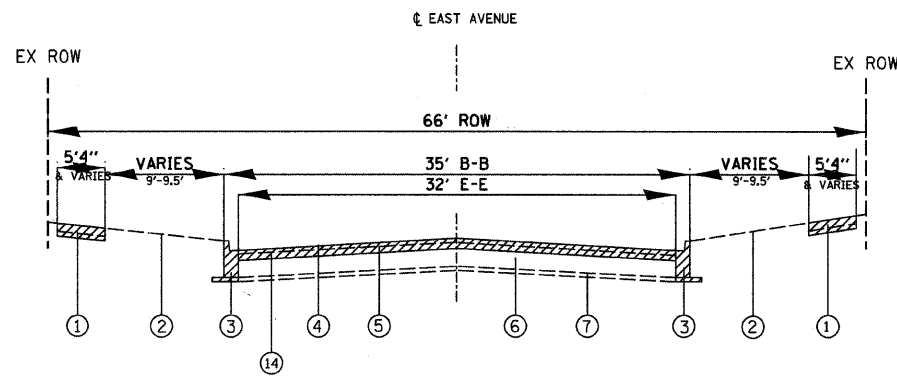
CATCH BASINS		WORK TO BE COMPLETED		
STATION	OFFSET			
1+74	15.9' LT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
1+77	15.9' RT			ADJ
3+81	14.0' LT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
3+80	16.0' RT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
5+22	13.3' LT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
5+35	16.0' RT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
7+09	15.8' LT			ADJ
7+10	15.4' RT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
7+85	15.8' LT			ADJ
7+86	10.0' LT			ADJ
7+87	15.6' RT			ADJ
10+41	10.2' LT			ADJ
10+38	16.0' RT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
12+08	11.0' LT			ADJ
12+09	15.2' RT			ADJ
13+53	15.9' LT			ADJ
13+72	15.9' RT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
14+10	2.1' RT			ADJ
14+13	11.5' RT			ADJ
15+96	15.6' LT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
15+97	15.3' RT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
18+27	15.8' RT			ADJ
18+29	15.6' LT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
19+89	15.9' RT			ADJ
20+06	15.6' RT			ADJ
22+15	15.4' LT	NEW CB	TY A, 4' DIA	ADJUST EXISTING FRAME & LID
22+25	16.1' RT			ADJ
24+09	32.3' RT			RECONSTRUCT W/ NEW T1FOL
24+17	30.4' LT			ADJ

UTILITY STRUCTURE			WORK TO BE COMPLETED
STATION	OFFSET	TYPE	
2+45	0.1' RT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
4+01	0.1' RT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
5+57	0.4' LT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
7+09	7.2' LT	MH	ADJ
7+12	0.1' LT	MH	ADJ
7+46	0.2' LT	MH	ADJ
7+79	0.0' LT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
7+86	10.0' LT	MH	ADJ
11+16	.74' RT	MH	ADJ
12+08	11.0' LT	MH	ADJ
14+10	2.1' RT	MH	ADJ
14+12	11.5' RT	MH	ADJ
14+22	11.4' RT	VB	ADJ
14+28	24.0' RT	VV	ADJ
14+50	12.0' RT	VV	ADJ
15+93	0.4' LT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
17+53	0.6' LT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
19+28	0.4' LT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
20+87	0.6' LT	MH	ADJ
20+82	22.0' LT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
22+44	0.4' LT	MH	ADJ WITH NEW TYPE 1 FRAME CLOSED LID
23+77	5.5' RT	MH	ADJ
23+89	2.3' LT	MH	ADJ
24+05	0.1' RT	MH	ADJ
23+94	8.5' RT	VB	ADJ
23+95	8.5' RT	VB	ADJ
23+95	10.7' RT	VV	RECONSTRUCT WITH NEW TYPE 1 FRAME CLOSED LID
24+10	20.0' LT	VB	ADJ

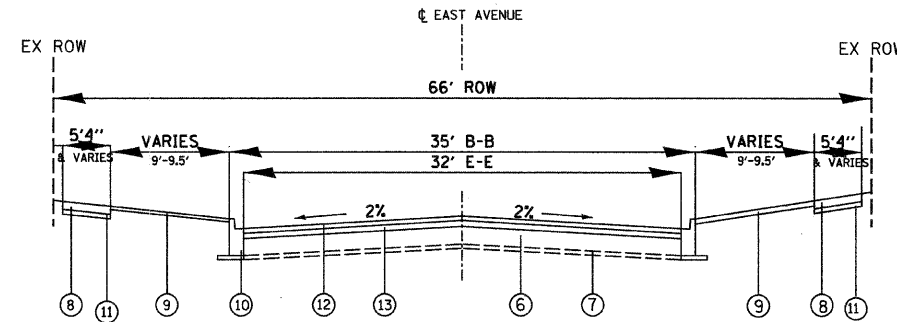
STORM SEWER CONNECTION			X0323389
STATION	OFFSET	PR SS	CONNECT TO
1+74	LEFT	8"	EX 12" COMBINED SEWER
3+80	RIGHT	8"	EX 12" COMBINED SEWER
3+81	LEFT	8"	PR CATCH BASIN
3+81	LEFT	8"	EX 12" COMBINED SEWER
5+22	LEFT	8"	PR CATCH BASIN
5+22	LEFT	8"	EX 12" COMBINED SEWER
5+35	RIGHT	8"	EX 12" COMBINED SEWER
7+12	RIGHT	8"	EX MANHOLE
10+38	RIGHT	8"	EX 12" COMBINED SEWER
10+41	LEFT	8"	EX CATCH BASIN
12+08	LEFT	8"	EX 12" COMBINED SEWER
12+08	RIGHT	8"	EX 12" COMBINED SEWER
13+73	RIGHT	8"	EX 12" COMBINED SEWER
15+93	LEFT	8"	EX 12" COMBINED SEWER
15+93	RIGHT	8"	EX 12" COMBINED SEWER
18+29	LEFT	8"	EX 12" COMBINED SEWER
21+12	LEFT	8"	EX 12" COMBINED SEWER

INLETS		WORK TO BE COMPLETED		
STATION	OFFSET			
3+78	16.4' LT	NEW INLET	TY A	ADJ EXISTING FR & LID
5+20	16.4' LT	NEW INLET	TY A	ADJ EXISTING FR & LID
10+40	16.4' LT	NEW INLET	TY A	ADJ EXISTING FR & LID
12+08	16.4' LT	NEW INLET	TY A	ADJ EXISTING FR & LID

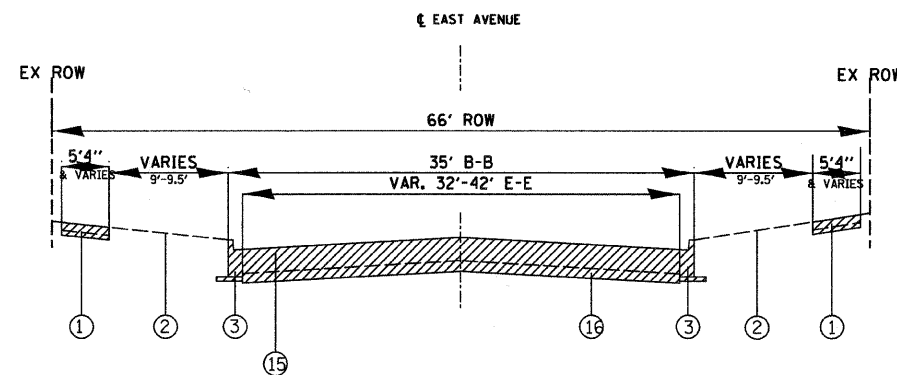
PAVEMENT PATCHING			44201713	44201723
LOCATION		OFFSET	TYPE I	IV
FROM	TO	LT/RT	SQ YD	SQ YD
4+14	4+29	RT	11	
8+14	8+20	RT	5	
8+49	8+57	LT	7	
17+12	17+30	RT		25
TOTAL			23	25



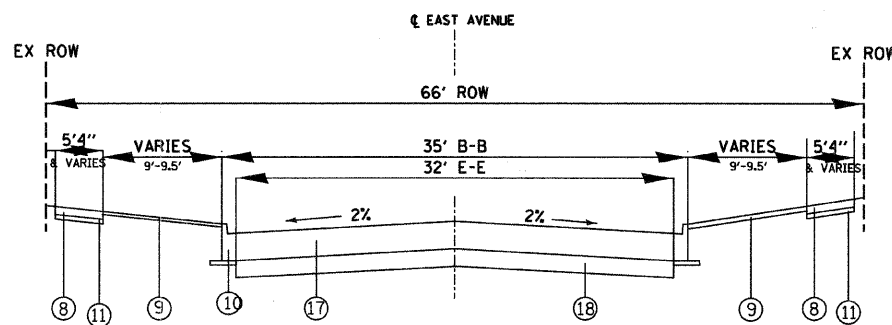
EXISTING TYPICAL SECTION  
STA 1+48.41 TO STA 22+32  
(LOOKING NORTH)



PROPOSED TYPICAL SECTION  
STA 1+48.41 TO STA 22+32  
(LOOKING NORTH)



EXISTING TYPICAL SECTION  
STA 22+42 TO STA 23+78  
(LOOKING NORTH)



PROPOSED TYPICAL SECTION  
STA 22+42 TO STA 23+78  
(LOOKING NORTH)

LEGEND

- ① EX PCC SIDEWALK
- ② EX PARKWAY
- ③ EX B 6.12 CURB AND GUTTER
- ④ EX HMA SURFACE COURSE
- ⑤ EX HMA BINDER COURSE
- ⑥ EX BITUMINOUS AGGREGATE MIXTURE BASE COURSE, 8"
- ⑦ EX SUB-BASE GRANULAR MATERIAL
- ⑧ PROPOSED SIDEWALK REMOVAL AND REPLACEMENT - INTERMITTENT
- ⑨ PROPOSED TOPSOIL FURNISH AND PLACE, 4" AND SODDING
- ⑩ PROPOSED COMBINATION CONCRETE CURB AND GUTTER  
REMOVAL AND REPLACEMENT - INTERMITTENT
- ⑪ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE A, 2"
- ⑫ PROPOSED 1 1/2" HMA SURFACE COURSE, MIX "D", N50
- ⑬ PROPOSED 2 1/4" HMA BINDER COURSE, IL-19.0 N50
- ⑭ HMA SURFACE REMOVAL (VARIABLE DEPTH)
- ⑮ EX PCC PAVEMENT, 9"
- ⑯ EX SUB-BASE
- ⑰ PR HES PCC PAVEMENT, 9"
- ⑱ PR SUBBASE, TY B, 4"

CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm)	4% @ 50 Gyr
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	4% @ 50 Gyr
PATCHING	
CLASS D PATCH (HMA BINDER IL-19.0)	4% @ 70 Gyr

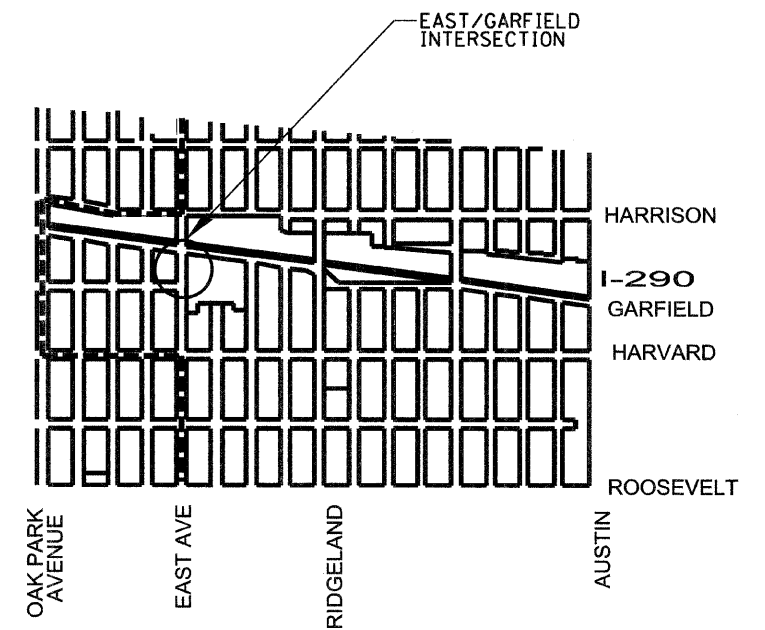
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS FOR "PERCENT OF RAP". SEE DISTRICT ONE SPECIAL PROVISIONS.

BUTT JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAIL SHEET "BUTT JOINT AND HMA TAPER DETAILS" WITH STATION LIMITS AS SHOWN ON THE PLANS

GRANULAR MATERIALS	2.05	TONS / CU YD
BITUMINOUS MAT PRIME COAT	0.10	GAL / SQ YD
AGGREGATE PRIME COAT	0.002	TONS / SQ YD
HMA RESURFACING	112	LBS / SQ YD / IN

DETOUR PLAN



EAST-GARFIELD INTERSECTION

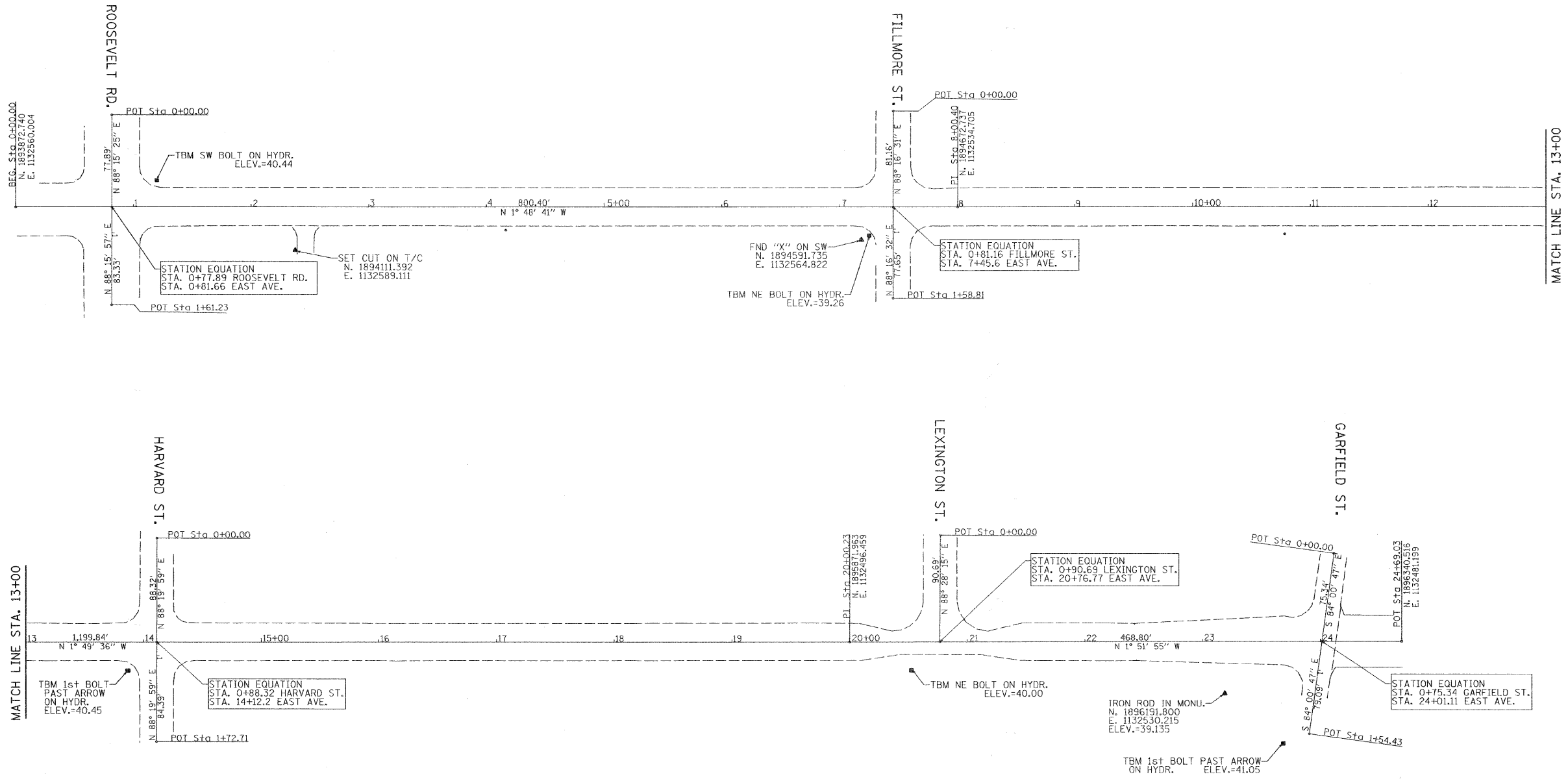
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		DRAWN - LB	REVISED -
		CHECKED - LB	REVISED -
		DATE - SEPTEMBER 2009	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EAST AVENUE  
TYPICAL SECTIONS & DETOUR PLAN

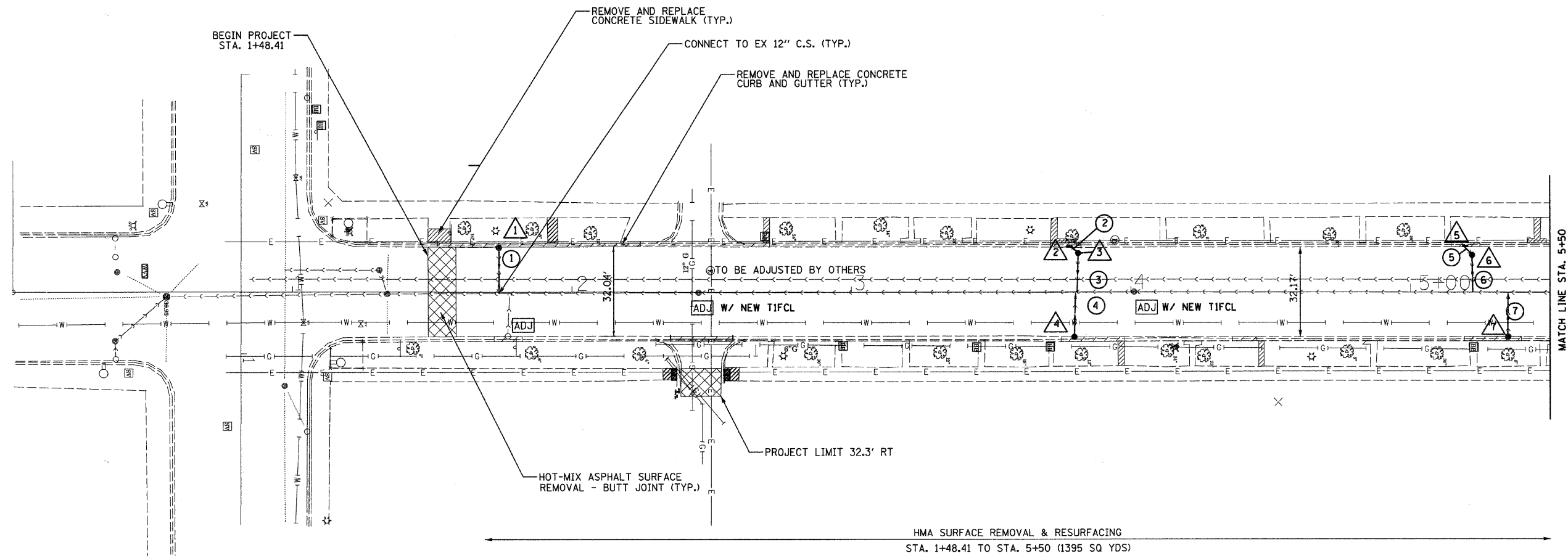
SHEET NO.5 OF 25 SHEETS STA. TO STA.

F.A.U. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 5
CONTRACT NO. 63293				
ILLINOIS FED. AID PROJECT				



FILE NAME = 01-sht-ATB.dgn	USER NAME = default	DESIGNED - DD	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EAST AVENUE ALIGNMENT, TIES &amp; BENCHMARKS</b>			F.A.U. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 6	
	PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET NO. 6 OF 25 SHEETS	STA. 1+48.41 TO STA. 24+15.56	CONTRACT NO. 63293					
	PLOT DATE = 2/1/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								





① PR CB TYPE A, 4' DIA., EX T1 FR, CL  
STA. 1+73.85, 15.86' LT  
RIM = 38.32  
INVERT = 33.95

② PR INL TYPE A, EX T1 FR, CL  
STA. 3+78.06, 16.35' LT  
RIM = 38.12  
INVERT = 35.12

③ PR CB TYPE A, 4' DIA., EX T1 FR, CL  
STA. 3+81.12, 13.97' LT  
RIM = 38.39  
INVERT = 34.40

④ PR CB TYPE A, 4' DIA., EX T1 FR, CL  
STA. 3+79.80, 16.01' RT  
RIM = 38.15  
INVERT = 34.15

⑤ PR INL TYPE A, EX T1 FR, CL  
STA. 5+19.64, 16.44' LT  
RIM = 38.09  
INVERT = 35.20

① 16 LF 8" PVC SDR 26 SS, CL B TY 2 @ 17.7%

② 5 LF 8" PVC SDR 26 SS, CL B TY 2 @ 5.0%

③ 13 LF 8" PVC SDR 26 SS, CL B TY 2 @ 4.1%

④ 15 LF 8" PVC SDR 26 SS, CL B TY 2 @ 2.6%

⑤ 5 LF 8" PVC SDR 26 SS, CL B TY 2 @ 4.0%

⑥ PR CB TYPE A, 4' DIA., T1 FR, CL  
STA. 5+22.04, 13.29' LT  
RIM = 38.40  
INVERT = 34.30

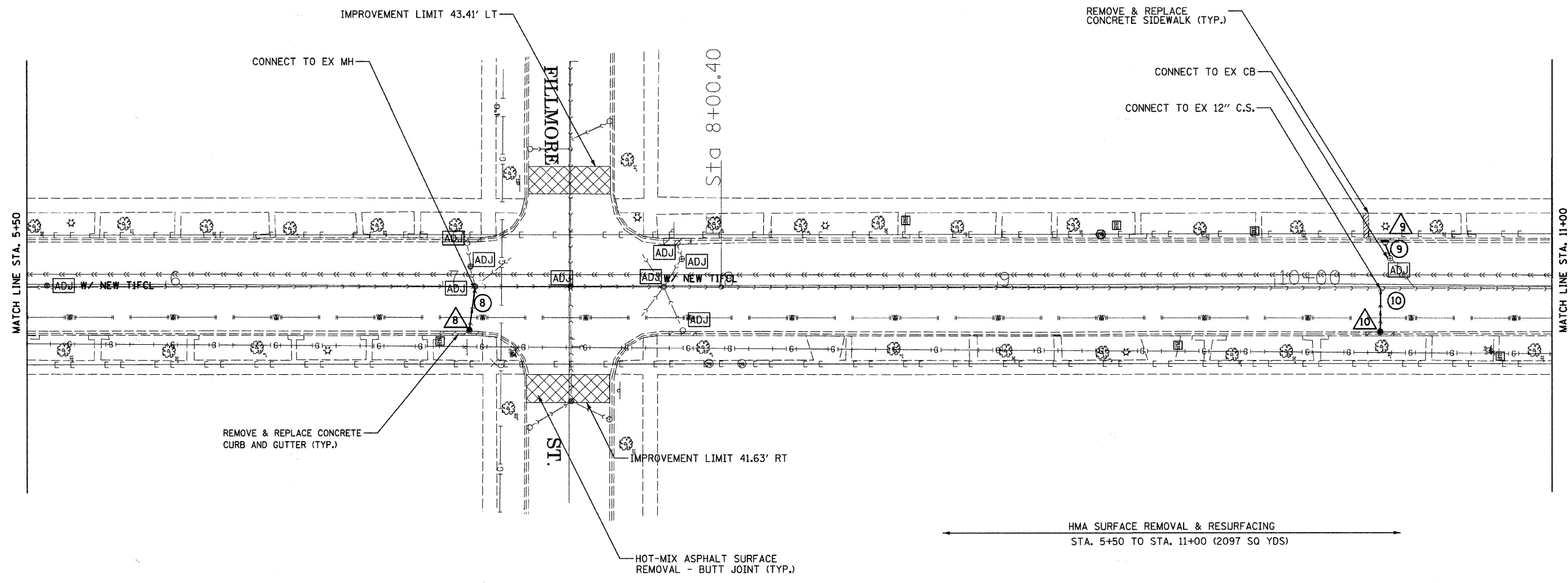
⑦ PR CB TYPE A, 4' DIA., T1 FR, CL  
STA. 5+34.91, 16.14' RT  
RIM = 38.09  
INVERT = 34.09

⑥ 12 LF 8" PVC SDR 26 SS, CL B TY 2 @ 16.4%

⑦ 15 LF 8" PVC SDR 26 SS, CL B TY 2 @ 12.6%

LEGEND	
	PROPOSED BUTT JOINT
	REMOVE & REPLACE CURB & GUTTER
	REMOVE & REPLACE SIDEWALK
	DETECTABLE WARNINGS
	EX MANHOLE
	EX CATCH BASIN
	PR CATCH BASIN
	EX INLET
	PR INLET
	EX COMBINED SEWER
	PROPOSED STORM SEWER
	EX WATER
	EX GAS
	EX ELECTRIC

FILE NAME = 01-shtr-plan1.dgn	USER NAME = default	DESIGNED - RB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EAST AVENUE STATION 1+48.41 TO STATION 5+50</b>			F.A.U. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 7
PLOT SCALE = #SCALE#	CHECKED - LB	REVISED -	REVISED -		SCALE: 1" = 20'	SHEET NO. 7 OF 25 SHEETS	STA. 1+48.41 TO STA. 5+50	CONTRACT NO. 63293				
PLOT DATE = 2/16/2010	DATE - SEPTEMBER 2009	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							



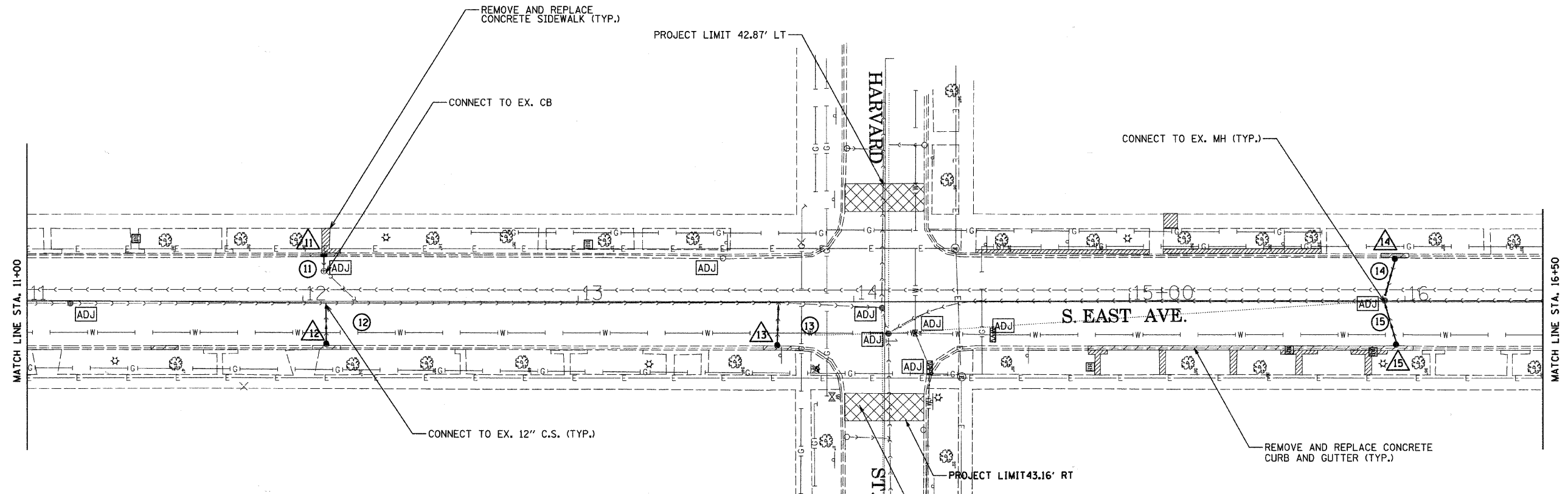
- 8 PR CB TYPE A, 4' DIA., (EXISTING FR & LID) STA. 7+09.55, 14.47 FT RT  
 RIM = 37.79  
 INVERT = 33.79
- 9 PR INLET TYPE A, (EXISTING FR & LID) STA. 10+39.75, 15.91' LT  
 RIM = 37.58  
 INVERT = 34.58
- 10 PR CB TYPE A, 4' DIA., (EXISTING FR & LID) STA. 10+37.95, 15.98' LT  
 RIM = 37.55  
 INVERT = 33.55
- 8 14 LF 8" PVC SDR 26 SS, CL B TY 2 @ 5.6%
- 9 5 LF 8" PVC SDR 26 SS, CL B TY 2, REUSE EX CONNECTION
- 10 15 LF 8" PVC SDR 26 SS, CL B TY 2 @ 14.5%

**LEGEND**

- PROPOSED BUTT JOINT
- REMOVE & REPLACE CURB & GUTTER
- REMOVE & REPLACE SIDEWALK
- DETECTABLE WARNINGS
- EX MANHOLE
- EX CATCH BASIN
- PR CATCH BASIN
- EX INLET
- PR INLET
- EX COMBINED SEWER
- PROPOSED STORM SEWER
- EX WATER
- EX GAS
- EX ELECTRIC

FILE NAME = DI-sht-plan2.dgn	USER NAME = default	DESIGNED - RB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EAST AVENUE STATION 5+50 TO STATION 11+00</b>	F.A.U. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 8	
PLOT SCALE = \$SCALE\$		CHECKED - LB	REVISED -		SCALE: 1" = 20'	SHEET NO. 8 OF 25 SHEETS	STA. 5+50	TO STA. 11+00	CONTRACT NO. 63293		
PLOT DATE = 2/16/2010		DATE - SEPTEMBER 2009	REVISED -		ILLINOIS FED. AID PROJECT						





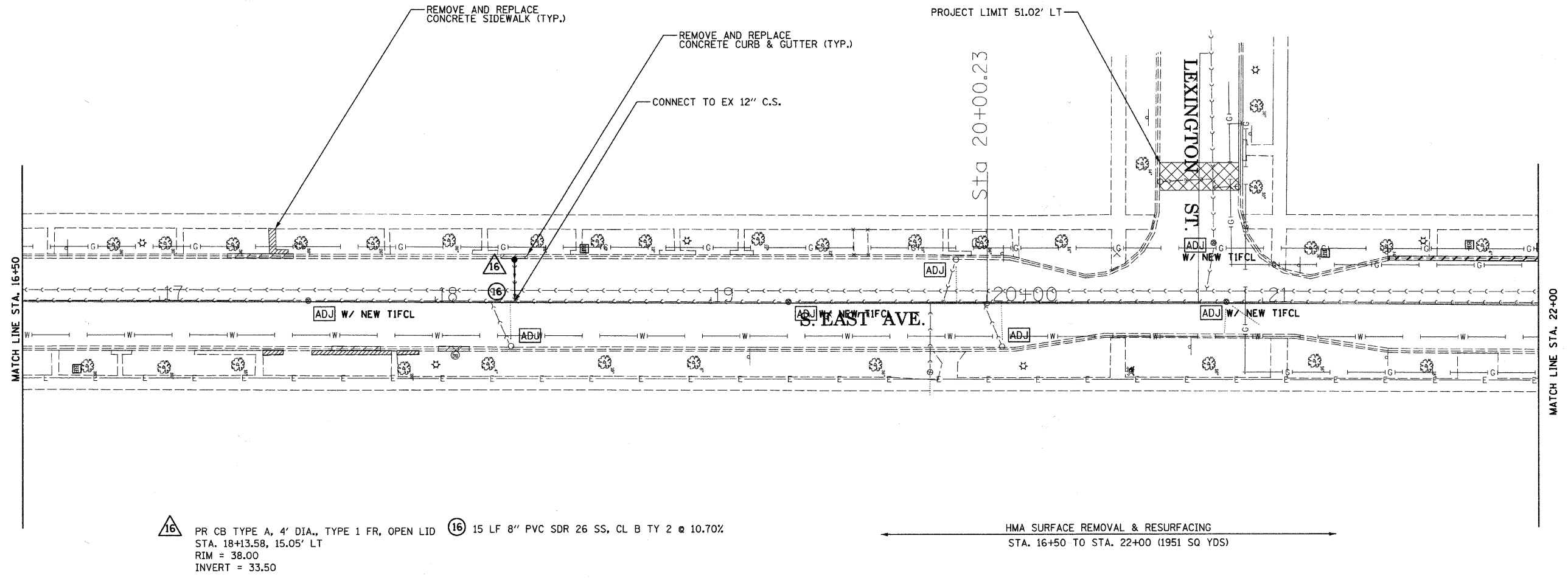
- |  |   |
|--|---|
| <p>11 PR INLET TYPE A, (EXISTING FR &amp; LID)<br/>STA. 12+07.66, 16.40' LT<br/>RIM = 37.55<br/>INVERT = 34.55</p> <p>12 PR CB TYPE A, 4' DIA., (EXISTING FR &amp; LID)<br/>STA. 12+08.57, 15.53' RT<br/>RIM = 37.55<br/>INVERT = 33.55</p> <p>13 PR CB TYPE A, 4' DIA., (EXISTING FR &amp; LID)<br/>STA. 13+72.69, 15.86' RT<br/>RIM = 37.60<br/>INVERT = 33.10</p> <p>14 PR CB TYPE A, 4' DIA., (EXISTING FR &amp; LID)<br/>STA. 15+96.27, 15.80' LT<br/>RIM = 38.00<br/>INVERT = 33.50</p> <p>15 PR CB TYPE A, 4' DIA., (EXISTING FR &amp; LID)<br/>STA. 15+96.72, 15.32' RT<br/>RIM = 38.00<br/>INVERT = 33.50</p> | <p>11 5 LF 8" PVC SDR 26 SS, CL B TY 2 @ 7.6%</p> <p>12 15 LF 8" PVC SDR 26 SS, CL B TY 2 @ 15.6%</p> <p>13 15 LF 8" PVC SDR 26 SS, CL B TY 2 @ 16.5%</p> <p>14 14 LF 8" PVC SDR 26 SS, CL B TY 2 @ 8.9%</p> <p>15 14 LF 8" PVC SDR 26 SS, CL B TY 2 @ 8.9%</p> |
|--|---|

HMA SURFACE REMOVAL & RESURFACING  
STA. 11+00 TO STA. 16+50 (2095 SQ YDS)

**LEGEND**

- PROPOSED BUTT JOINT
- REMOVE & REPLACE CURB & GUTTER
- REMOVE & REPLACE SIDEWALK
- DETECTABLE WARNINGS
- EX MANHOLE
- EX CATCH BASIN
- PR CATCH BASIN
- EX INLET
- PR INLET
- EX COMBINED SEWER
- PROPOSED STORM SEWER
- EX WATER
- EX GAS
- EX ELECTRIC

FILE NAME = DI-shtr-plan3.dgn	USER NAME = default	DESIGNED - RB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EAST AVENUE STATION 11+00 TO STATION 16+50</b>	F.A.U. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 9	
PLOT SCALE = #SCALE#					SCALE: 1" = 20'		SHEET NO. 9 OF SHEETS		STA. 11+00 TO STA. 16+50		ILLINOIS FED. AID PROJECT
PLOT DATE = 2/16/2010					DATE - SEPTEMBER 2009		CONTRACT NO. 63293				
DRAWN - LB											
CHECKED - LB											
REVISED -											

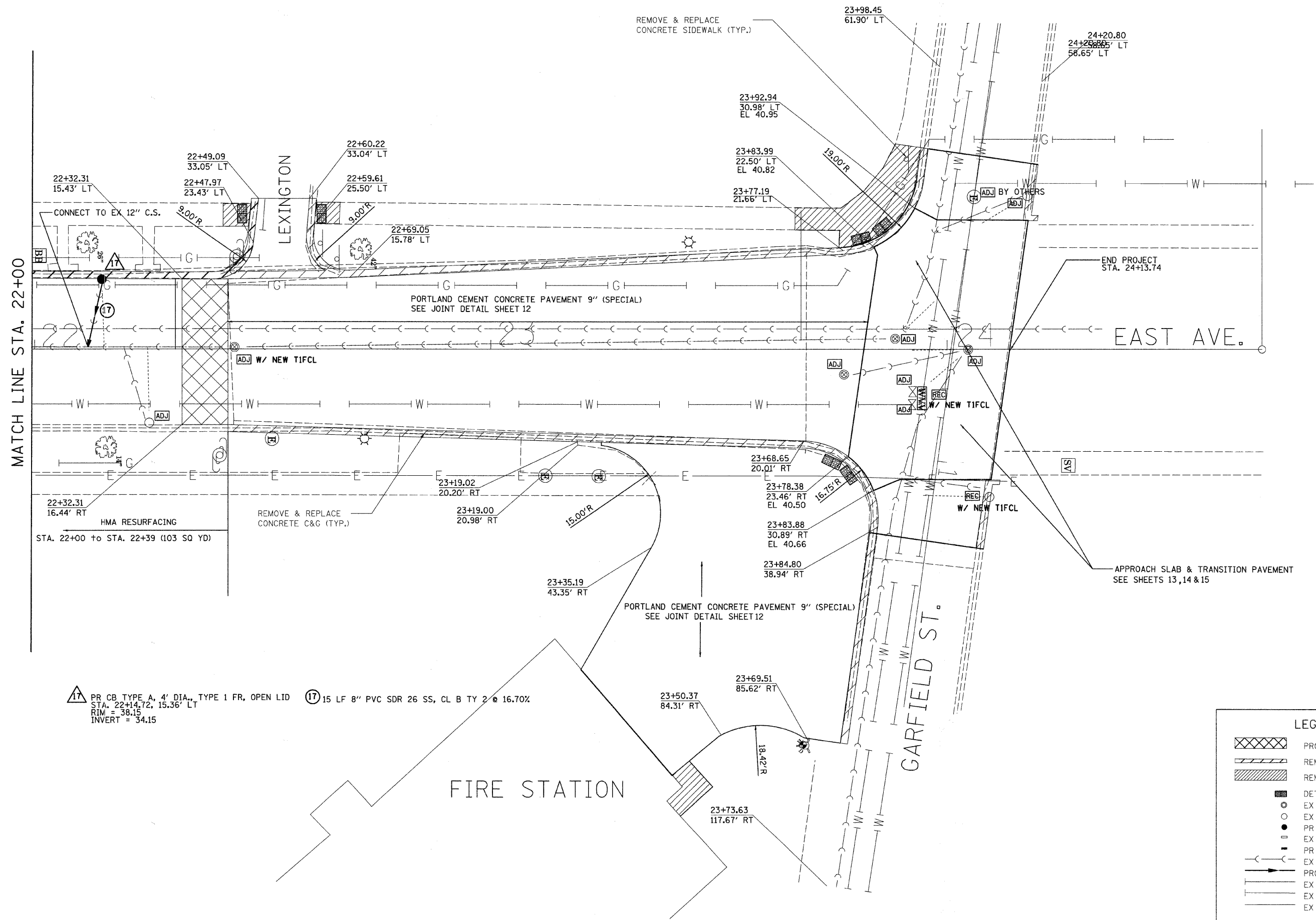


PR CB TYPE A, 4' DIA., TYPE 1 FR, OPEN LID 15 LF 8" PVC SDR 26 SS, CL B TY 2 @ 10.70%  
 STA. 18+13.58, 15.05' LT  
 RIM = 38.00  
 INVERT = 33.50

HMA SURFACE REMOVAL & RESURFACING  
 STA. 16+50 TO STA. 22+00 (1951 SQ YDS)

LEGEND	
	PROPOSED BUTT JOINT
	REMOVE & REPLACE CURB & GUTTER
	REMOVE & REPLACE SIDEWALK
	DETECTABLE WARNINGS
	EX MANHOLE
	EX CATCH BASIN
	PR CATCH BASIN
	EX INLET
	PR INLET
	EX COMBINED SEWER
	PROPOSED STORM SEWER
	EX WATER
	EX GAS
	EX ELECTRIC

FILE NAME = DI-sht-plan4.dgn	USER NAME = default	DESIGNED - RB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EAST AVENUE STATION 16 + 50 TO STATION 22 + 00</b>			F.A.U. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 10
PLOT SCALE = #SCALE#		DRAWN - LB	REVISED -		SCALE: 1" = 20'	SHEET NO. 10 OF 25 SHEETS	STA. 16+50 TO STA. 22+00	CONTRACT NO. 63293				
PLOT DATE = 2/16/2010		CHECKED - LB	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE - SEPTEMBER 2009	REVISED -									



MATCH LINE STA. 22+00

EAST AVE.

GARFIELD ST.

LEXINGTON

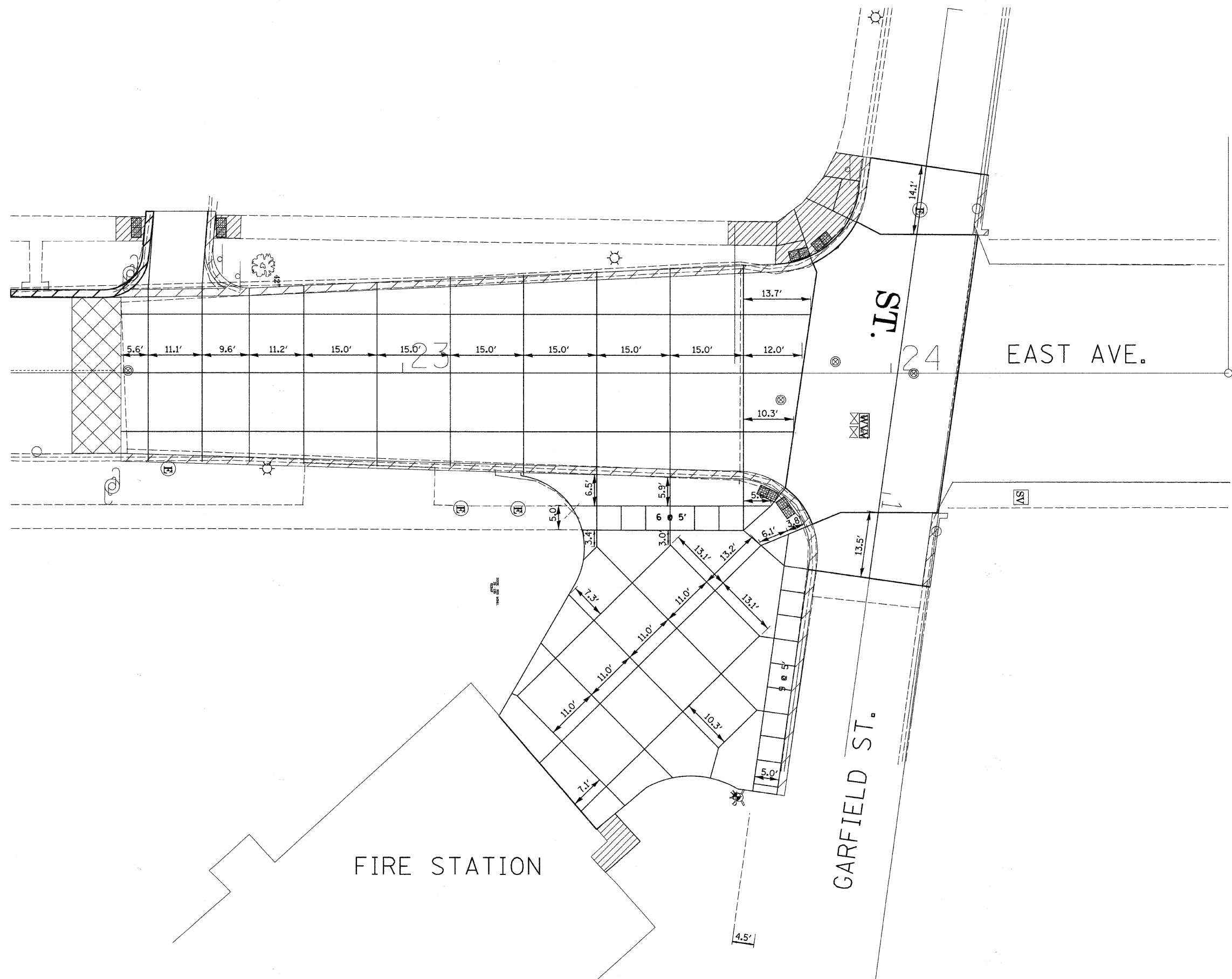
FIRE STATION

END PROJECT STA. 24+13.74

- 17 PR CB TYPE A, 4' DIA., TYPE 1 FR, OPEN LID STA. 22+14.72, 15.36' LT R/W = 38.15 INVERT = 34.15
- 17 15 LF 8" PVC SDR 26 SS, CL B TY 2 @ 16.70%

LEGEND	
	PROPOSED BUTT JOINT
	REMOVE & REPLACE CURB & GUTTER
	REMOVE & REPLACE SIDEWALK
	DETECTABLE WARNINGS
	EX MANHOLE
	EX CATCH BASIN
	PR CATCH BASIN
	EX INLET
	PR INLET
	EX COMBINED SEWER
	PROPOSED STORM SEWER
	EX WATER
	EX GAS
	EX ELECTRIC

FILE NAME = D1-ah-plot5.dgn	USER NAME = default	DESIGNED - RB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EAST AVENUE STATION 22+00 TO 24+13.74</b>			F.A.U. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 11	
	PLOT SCALE = #SCALE#	DRAWN - LB	REVISED -		SCALE: 1" = 10'	SHEET NO. 11 OF	SHEETS	STA. 22+00	TO STA. 24+18	CONTRACT NO. 63293			
	PLOT DATE = 2/16/2010	CHECKED - LB	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE - SEPTEMBER 2009	REVISED -										



FILE NAME =  
01-sht-JointDetail.dgn

USER NAME = default  
PLOT SCALE = \$SCALE\$  
PLOT DATE = 2/16/2010

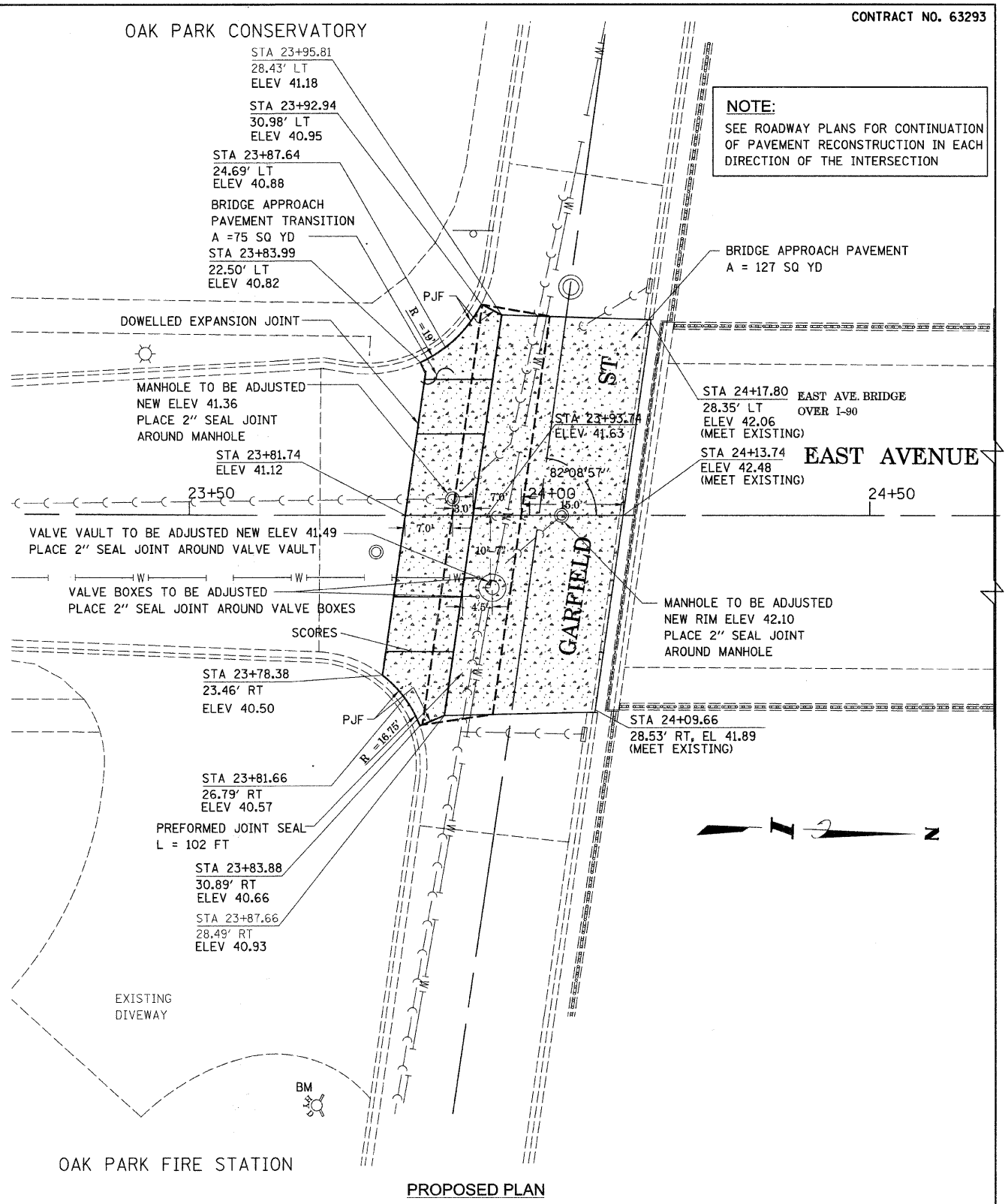
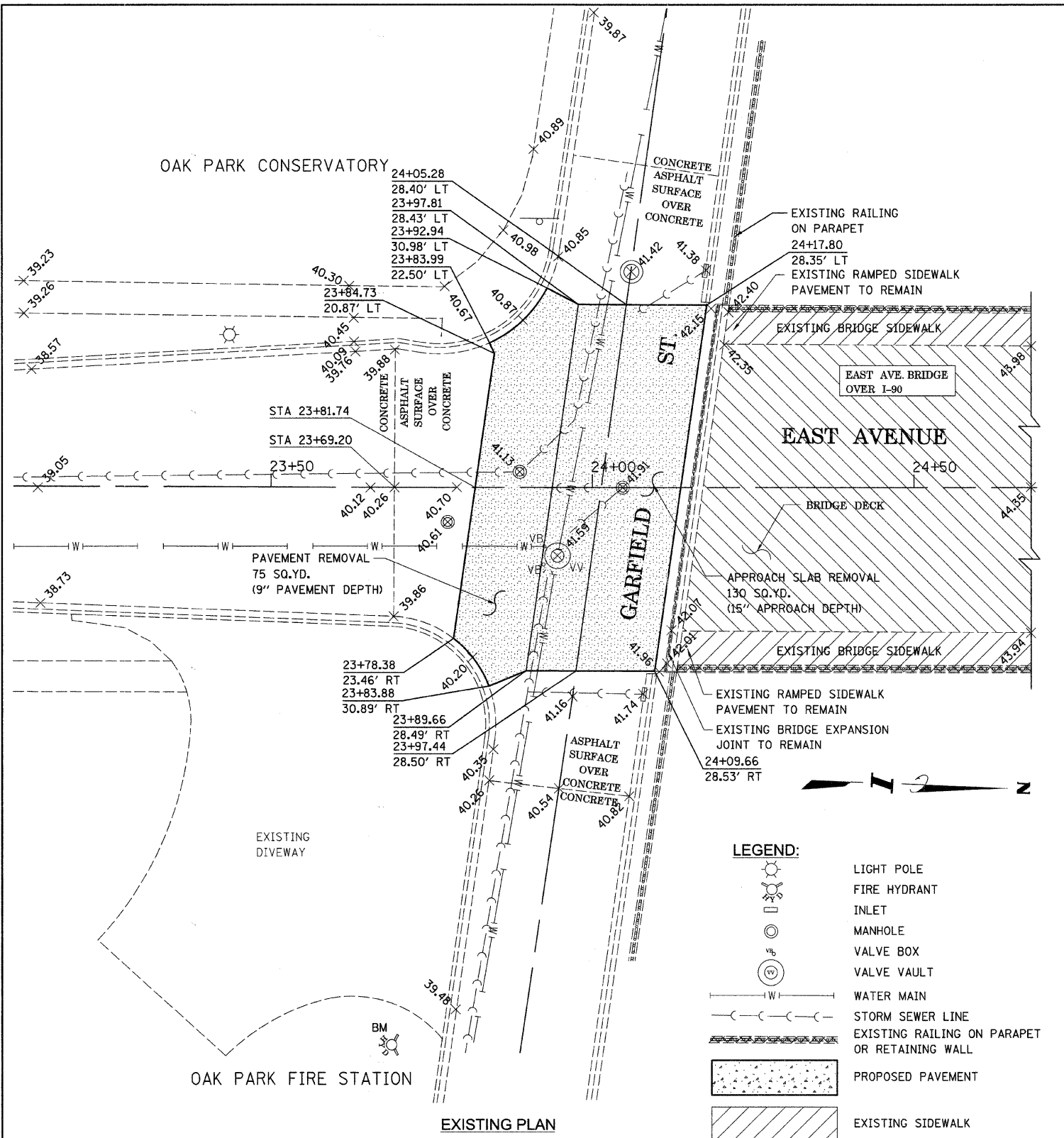
DESIGNED - RB  
DRAWN - LB  
CHECKED - LB  
DATE - SEPTEMBER 2009

REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EAST AVENUE  
JOINT DETAIL**  
SCALE: 1" = 10'  
STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2779	09-00246-00-RS	COOK	25	12
CONTRACT NO. 63293				
ILLINOIS FED. AID PROJECT				



**NOTE:**  
SEE ROADWAY PLANS FOR CONTINUATION  
OF PAVEMENT RECONSTRUCTION IN EACH  
DIRECTION OF THE INTERSECTION

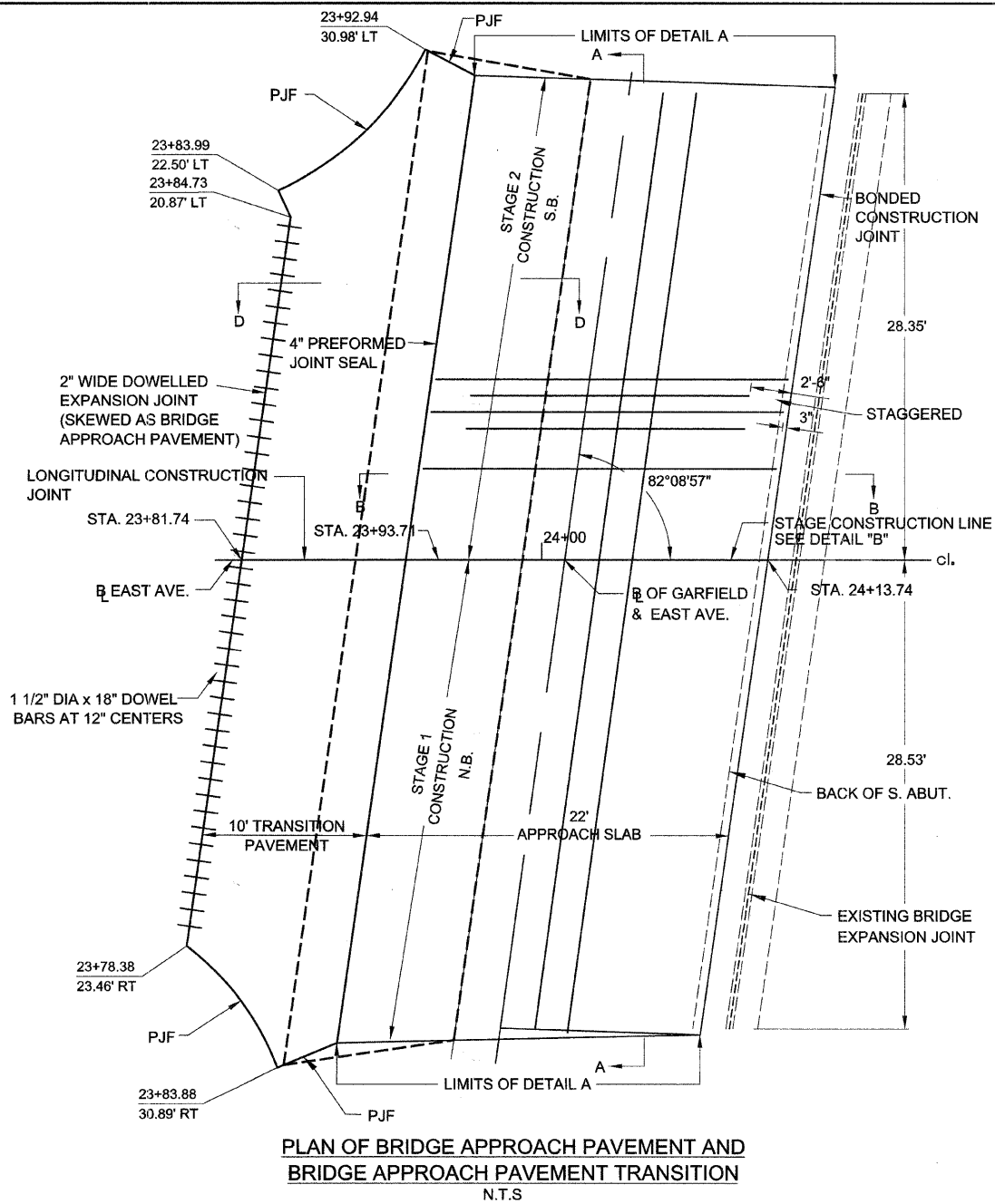
**BENCHMARK:**  
FLANGE BOLT ON FIRE HYDRANT AT SOUTHEAST CORNER  
OF EAST AVENUE AND GARFIELD STREET. ELEV 41.12

FILE NAME = EAST_AVE_REM&PROP.dgn	USER NAME = default	DESIGNED - EAH	REVISED - 01/28/10
		DRAWN - EAH	REVISED -
		CHECKED - OY	REVISED -
		DATE - 01/28/10	REVISED -

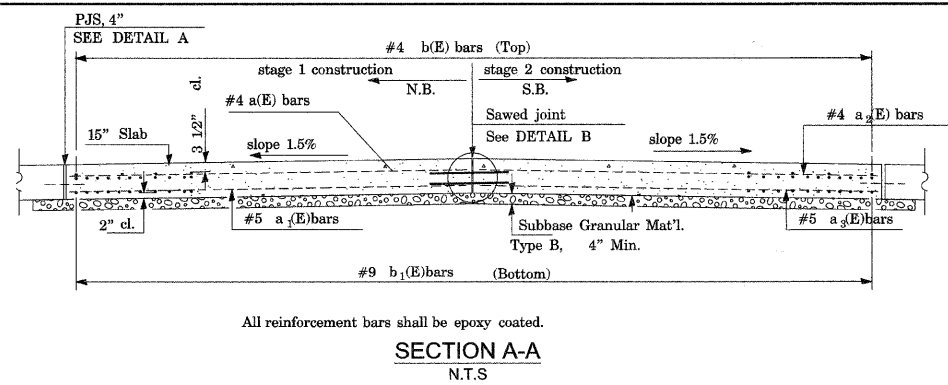
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

EAST AVENUE BRIDGE APPROACH PAVEMENT PLAN			
SCALE: 1" = 10'	SHEET NO. OF SHEETS	STA.	TO STA.

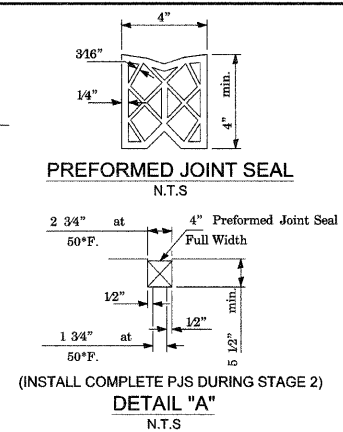
F.A.P. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 13
CONTRACT NO. 63293				
ILLINOIS FED. AID PROJECT				



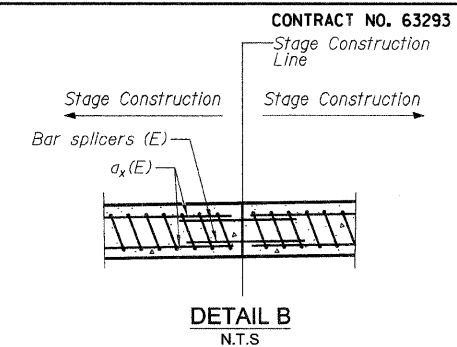
**PLAN OF BRIDGE APPROACH PAVEMENT AND BRIDGE APPROACH PAVEMENT TRANSITION**  
N.T.S



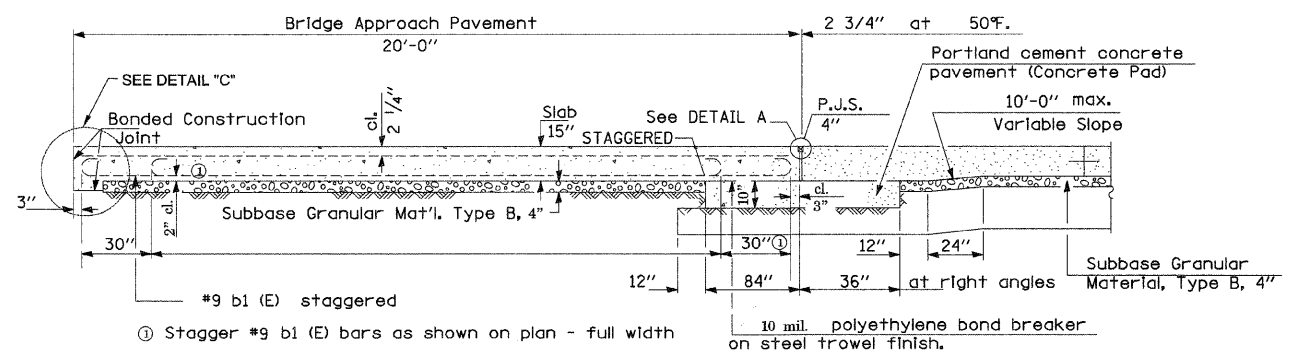
**SECTION A-A**  
N.T.S



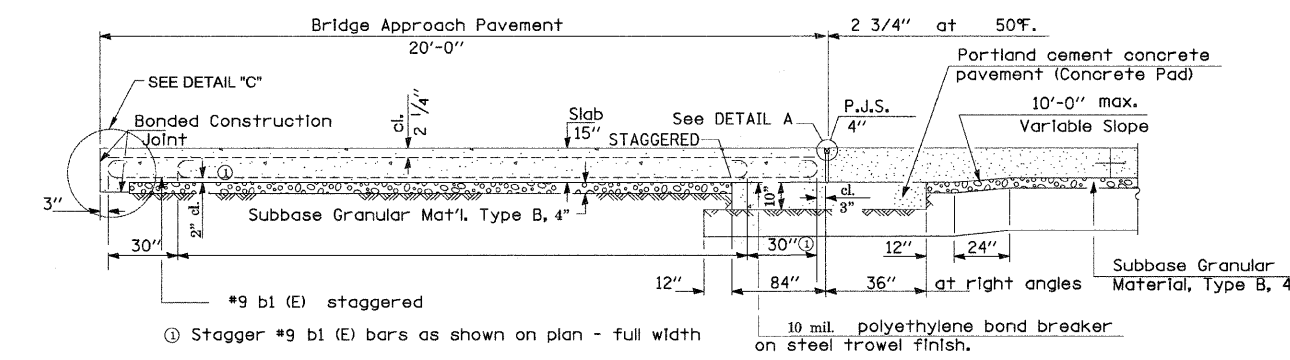
**PREFORMED JOINT SEAL**  
N.T.S  
(INSTALL COMPLETE PJS DURING STAGE 2)  
**DETAIL "A"**  
N.T.S



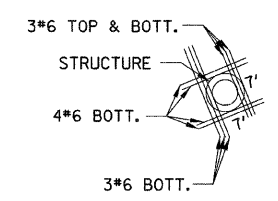
**DETAIL B**  
N.T.S



**SECTION B-B (NORTH BOUND)**  
N.T.S



**SECTION B-B (SOUTH BOUND)**  
N.T.S



**REINFORCEMENT AROUND STRUCTURE (TYP.)**  
N.T.S

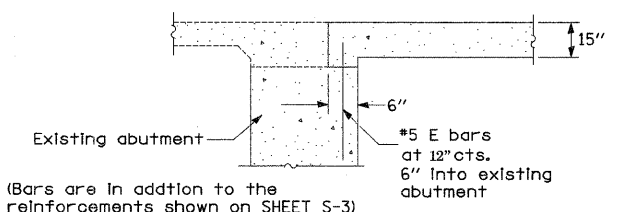
**REINFORCEMENT AROUND STRUCTURE (TYP.)**  
**DETAIL D**  
N.T.S

**GENERAL NOTES**

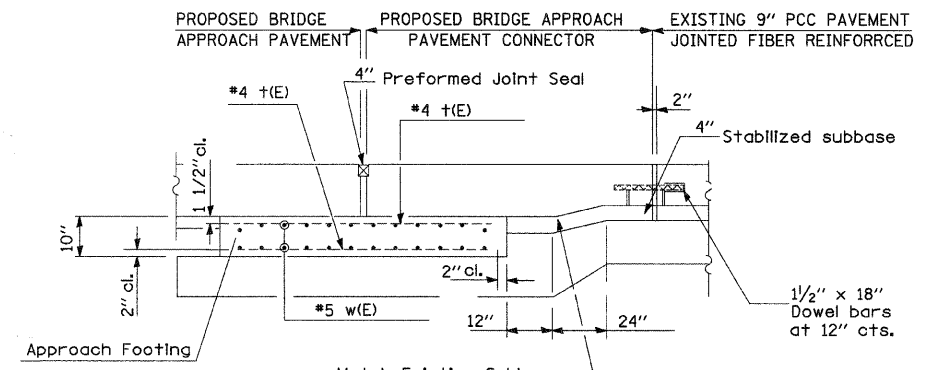
- CUT BARS IN FIELD AROUND MANHOLES, VALVE VAULT AND VALVE BOXES AS REQUIRED.
- PLACE REINFORCEMENT AS DIRECTED BY THE ENGINEER THE SAME PATTERN AND SIZES AS SHOWN IN DETAIL D
- P.J.F SHALL BE 3/4" BITUMINOUS PREFORMED JOINT FILLER
- TILT # 9 b1(E) BARS AS REQUIRED TO MAINTAIN CLEARANCE

**DESIGN STRESSES**

$f_y = 60,000$  p.s.i.  
 $f_c = 3,500$  p.s.i.  
 $n = 8.5$



**DETAIL C**  
N.T.S



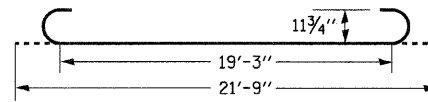
**SECTION D-D - RIGID PAVEMENT**  
N.T.S

FILE NAME = EAST_AVE_DETAILS.dgn	USER NAME = default	DESIGNED - EAH	REvised - 01/28/10	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	EAST AVENUE BRIDGE APPROACH DETAIL 1		F.A.P. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 14	
	PLOT SCALE = #SCALE#	DRAWN - EAH	REvised -		SCALE: 1" = 10'	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 63293				
	PLOT DATE = 2/1/2010	CHECKED - OY	REvised -		ILLINOIS FED. AID PROJECT							
		DATE - 01/28/10	REvised -									

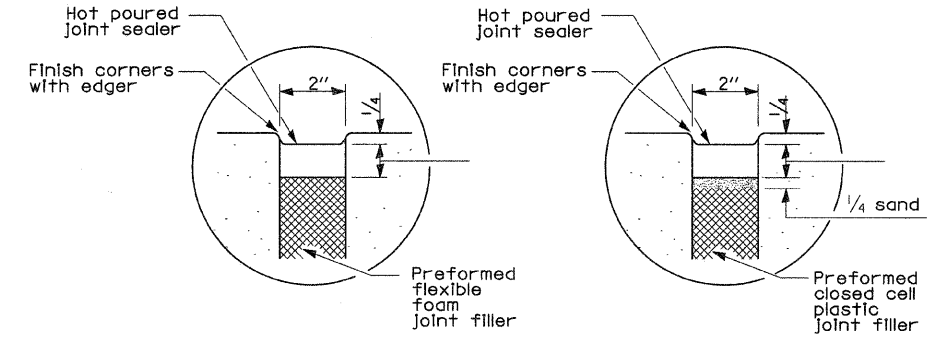


### BILL OF MATERIALS

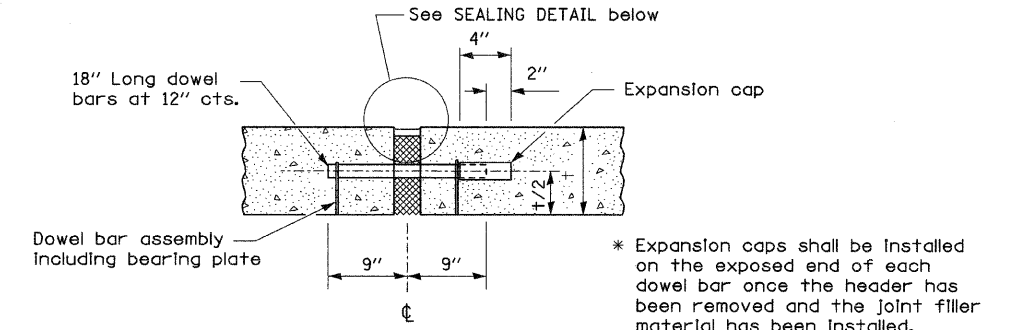
ITEM	SP	DESCRIPTION	QUANTITY	UNITS
42001420		Bridge Approach Pavement Connector (PCC)	61	Sq. Yd.
44000100		Pavement Removal	75	Sq. Yd.
44000700	*	Approach Slab Removal	130	Sq. Yd.
50300225		Concrete Structures	19	Cu. Yd.
50300255		Concrete Superstructure	68	Cu. Yd.
50300260		Bridge Deck Grooving	205	Sq. Yd.
50300300		Protective Coat	205	Sq. Yd.
50800205		Reinforcement Bars, Epoxy Coated	16482	Pound
50800515		Bar Splicers	162	Each
60255500		Manholes To Be Adjusted	2	Each
60265700		Valve Vaults To Be Adjusted	1	Each
60266600		Valve Boxes To Be Adjusted	2	Each
JT301001		Granular Subbase	23	Cu. Yd.
Z0017099	*	Dowel Bar Assembly	23	Each
Z0017202	*	Dowel Bars 1 1/2"	23	Each



BAR-b<sub>1</sub>(E)



SEALING DETAIL



DOWELLED EXPANSION JOINT

Bar	No.	Size	Length N.B.	Shape
a(E)	36	#4	28'-5"	—
a <sub>1</sub> (E)	68	#5	28'-5"	—
b(E)	24	#4	21'-6"	—
b <sub>1</sub> (E)	71	#9	21'-9"	—
t(E)	63	#4	9'-8"	—
w(E)	58	#5	28'-5"	—
Splicers	36	#4		
Splicers	126	#5		

REINFORCEMENT DETAILS  
PHASE 1 : NORTH BOUND

Bar	No.	Size	Length S.B.	Shape
a(E)	36	#4	28'-3"	—
a <sub>1</sub> (E)	68	#5	28'-3"	—
b(E)	24	#4	21'-6"	—
b <sub>1</sub> (E)	71	#9	21'-9"	—
t(E)	63	#4	9'-8"	—
w(E)	58	#5	28'-3"	—

REINFORCEMENT DETAILS  
PHASE 2 : SOUTH BOUND

#### NOTES

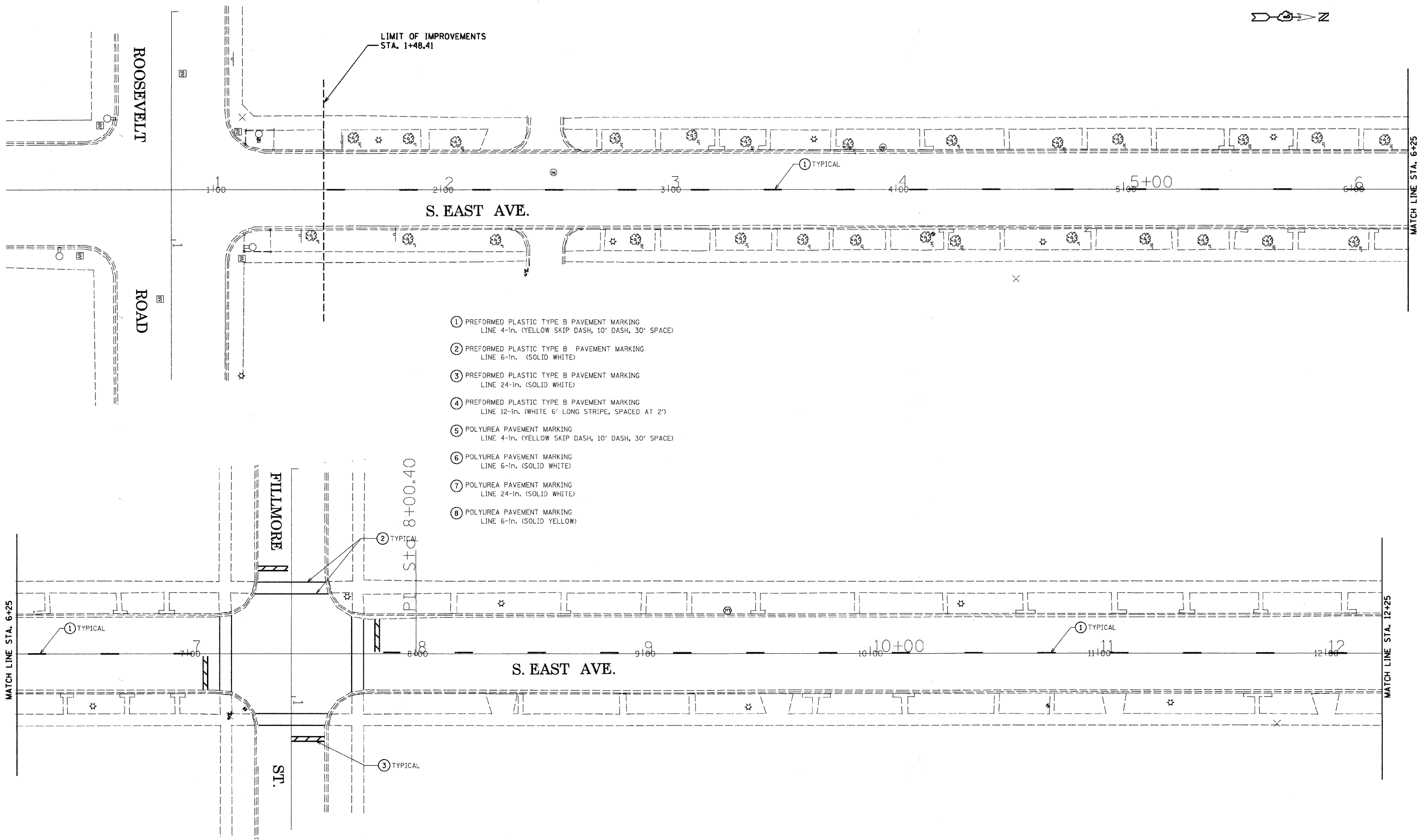
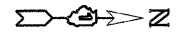
APPROACH SLAB REMOVAL INCLUDES SAW CUTTING APPROACH SLAB AND ANY SUPPORTING MEMBERS CONSISTANT WITH STAGE CONSTRUCTION, REMOVING CONCRETE, SUBBASE, POSSIBLE GRADE BEAM OR SLEEPER SLAB, AND OTHER MATERIAL AS REQUIRED TO CONSTRUCT THE APPROACH SHOWN IN THESE PLANS. FOR APPROACH FOOTING MAXIMUM APPLIED SERVICE BEARING PRESSURE (Q<sub>max</sub>)= 2.0ksf. COST OF EXCAVATION FOR APPROACH FOOTING IS INCLUDED WITH APPROACH SLAB REMOVAL

APPROACH SLAB SHALL BE PAID FOR AS CONCRETE SUPERSTRUCTURE. PCC PAVEMENT CONNECTOR SHALL BE PAID FOR AS BRIDGE APPROACH PAVEMENT CONNECTOR (PCC). 10 MIL. POLYETHYLENE BOND BREAKER ON STEEL TROWEL FINISH COST IS INCLUDED WITH CONCRETE SUPERSTRUCTURE. APPROACH FOOTING CONCRETE SHALL BE PAID FOR AS CONCRETE STRUCTURES. REINFORCEMENT SHALL BE PAID FOR AS REINFORCEMENT BARS, EPOXY COATED.

WHEN REMOVING APPROACH SLAB AND PAVEMENT CONNECTOR, CONTRACTOR MUST PROTECT ALL MANHOLES, VALVE VAULTS, VALVE BOXES AND ANY OTHER STRUCTURE

BRIDGE APPROACH PAVEMENT CONNECTOR (PCC) SHALL BE JOINTED PCC PAVEMENT WITH FIBER  
SEE "EAST AVENUE JOINT DETAIL" - SHEET J1

FILE NAME = EAST_AVE_DETAILS02.dgn	USER NAME = default	DESIGNED - EAH	REVISED - 01/28/10	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	EAST AVENUE BRIDGE APPROACH DETAIL 2			F.A.P. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 15
	PLOT SCALE = \$SCALE\$	DRAWN - EAH	REVISED -		SCALE: 1" = 10'	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 63293	
	PLOT DATE = 2/16/2010	CHECKED - OY	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE - 01/28/10	REVISED -									



- ① PREFORMED PLASTIC TYPE B PAVEMENT MARKING  
LINE 4-in. (YELLOW SKIP DASH, 10' DASH, 30' SPACE)
- ② PREFORMED PLASTIC TYPE B PAVEMENT MARKING  
LINE 6-in. (SOLID WHITE)
- ③ PREFORMED PLASTIC TYPE B PAVEMENT MARKING  
LINE 24-in. (SOLID WHITE)
- ④ PREFORMED PLASTIC TYPE B PAVEMENT MARKING  
LINE 12-in. (WHITE 6' LONG STRIPE, SPACED AT 2')
- ⑤ POLYUREA PAVEMENT MARKING  
LINE 4-in. (YELLOW SKIP DASH, 10' DASH, 30' SPACE)
- ⑥ POLYUREA PAVEMENT MARKING  
LINE 6-in. (SOLID WHITE)
- ⑦ POLYUREA PAVEMENT MARKING  
LINE 24-in. (SOLID WHITE)
- ⑧ POLYUREA PAVEMENT MARKING  
LINE 6-in. (SOLID YELLOW)

FILE NAME =  
01-shht-pavmkt.dgn

USER NAME = default  
PLOT SCALE = \$SCALE\$  
PLOT DATE = 2/1/2010

DESIGNED - RB  
DRAWN - LB  
CHECKED - LB  
DATE - SEPTEMBER 2009

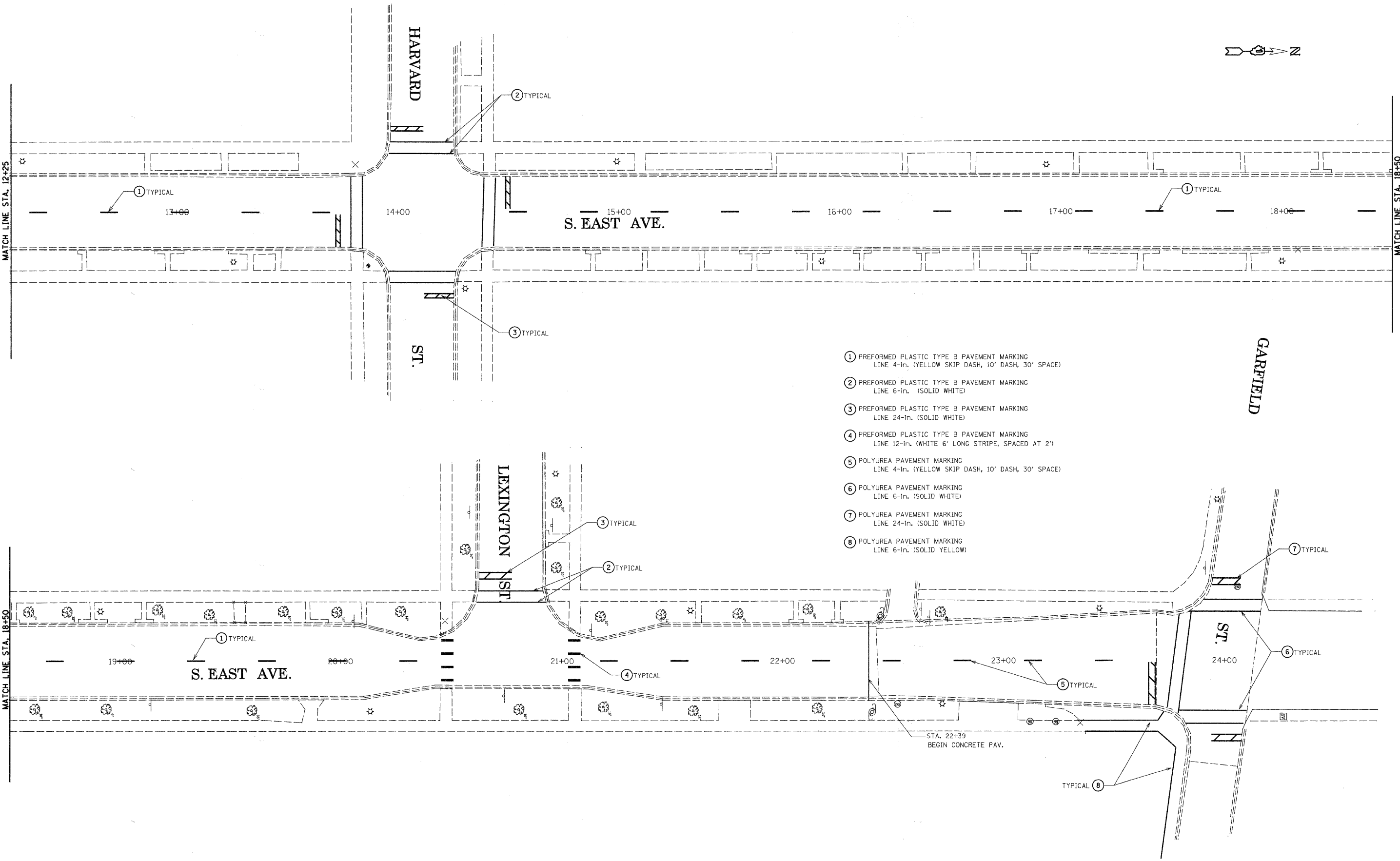
REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EAST AVENUE  
PAVEMENT MARKING**

SCALE: 1" = 20'    SHEET NO. 16 OF 25 SHEETS    STA.    TO STA.

F.A.J. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 16
CONTRACT NO. 63293				ILLINOIS FED. AID PROJECT

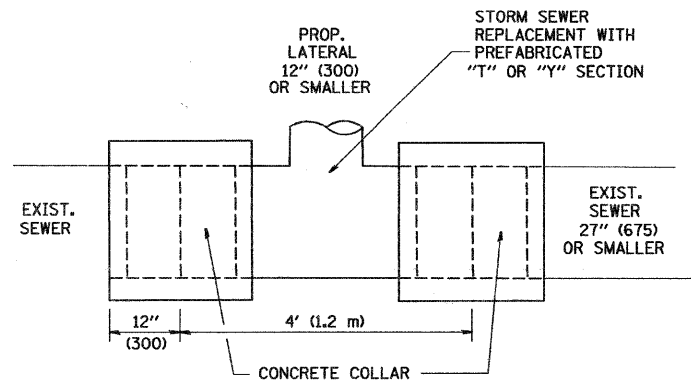


- ① PREFORMED PLASTIC TYPE B PAVEMENT MARKING  
LINE 4-in. (YELLOW SKIP DASH, 10' DASH, 30' SPACE)
- ② PREFORMED PLASTIC TYPE B PAVEMENT MARKING  
LINE 6-in. (SOLID WHITE)
- ③ PREFORMED PLASTIC TYPE B PAVEMENT MARKING  
LINE 24-in. (SOLID WHITE)
- ④ PREFORMED PLASTIC TYPE B PAVEMENT MARKING  
LINE 12-in. (WHITE 6' LONG STRIPE, SPACED AT 2')
- ⑤ POLYUREA PAVEMENT MARKING  
LINE 4-in. (YELLOW SKIP DASH, 10' DASH, 30' SPACE)
- ⑥ POLYUREA PAVEMENT MARKING  
LINE 6-in. (SOLID WHITE)
- ⑦ POLYUREA PAVEMENT MARKING  
LINE 24-in. (SOLID WHITE)
- ⑧ POLYUREA PAVEMENT MARKING  
LINE 6-in. (SOLID YELLOW)

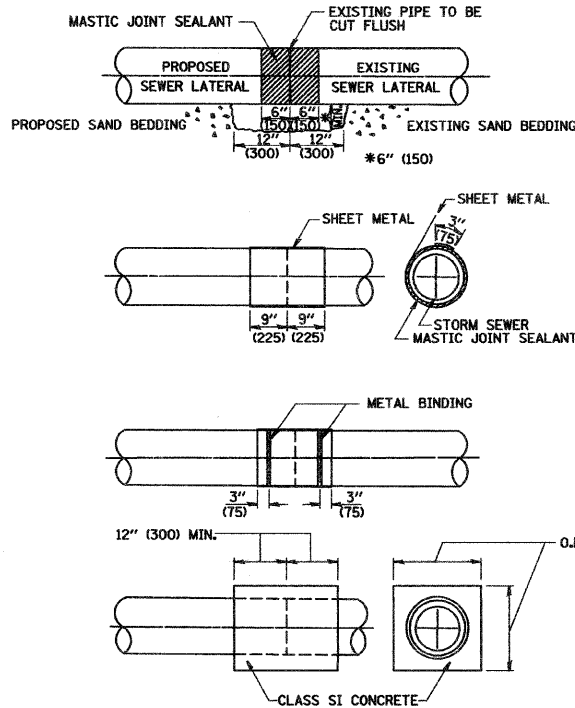
STA. 22+39  
BEGIN CONCRETE PAV.

FILE NAME = D1-shr-pavmk2.dgn	USER NAME = default	DESIGNED - RB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EAST AVENUE PAVEMENT MARKING</b>		F.A.J. RTE. 2779	SECTION 09-00246-00-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 17	
	PLOT SCALE = \$SCALE#	CHECKED - LB	REVISED -		SCALE: 1" = 20'	SHEET NO. 17 OF 25 SHEETS	STA.	TO STA.	CONTRACT NO. 63293			
	PLOT DATE = 2/1/2010	DATE - SEPTEMBER 2009	REVISED -		ILLINOIS FED. AID PROJECT							

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3779	09-00246-00-R8	COOK	25	18
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



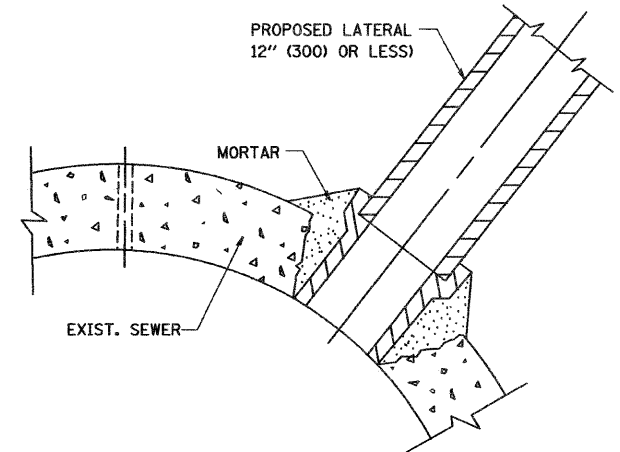
**DETAIL "A"**  
LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



**DETAIL "B"**  
CLASS SI CONCRETE COLLAR

**CONSTRUCTION SEQUENCE**

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



**DETAIL "C"**  
PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

**NOTES**

**MATERIAL**

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

**CONSTRUCTION METHODS**

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
  - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
  - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

**GENERAL**

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

**BASIS OF PAYMENT**

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	07/25/90
M. DE YONG	02/05/92
M. DE YONG	05/08/92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	06/12/96

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER**

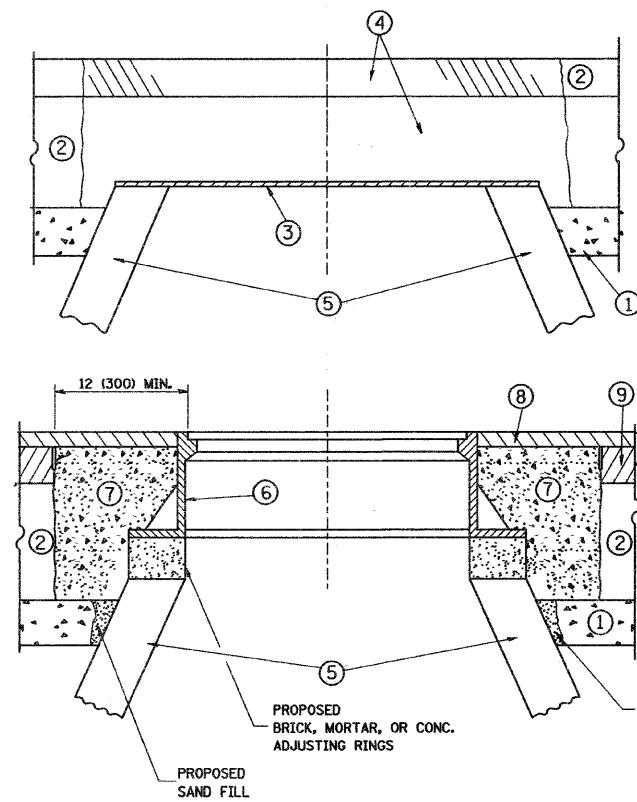
SCALE: VERT. NONE  
HORIZ.

DRAWN BY

CHECKED BY

BD500-01 (BD-7)

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2779	09-00246-00-RB	COOK	25	19
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"  
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

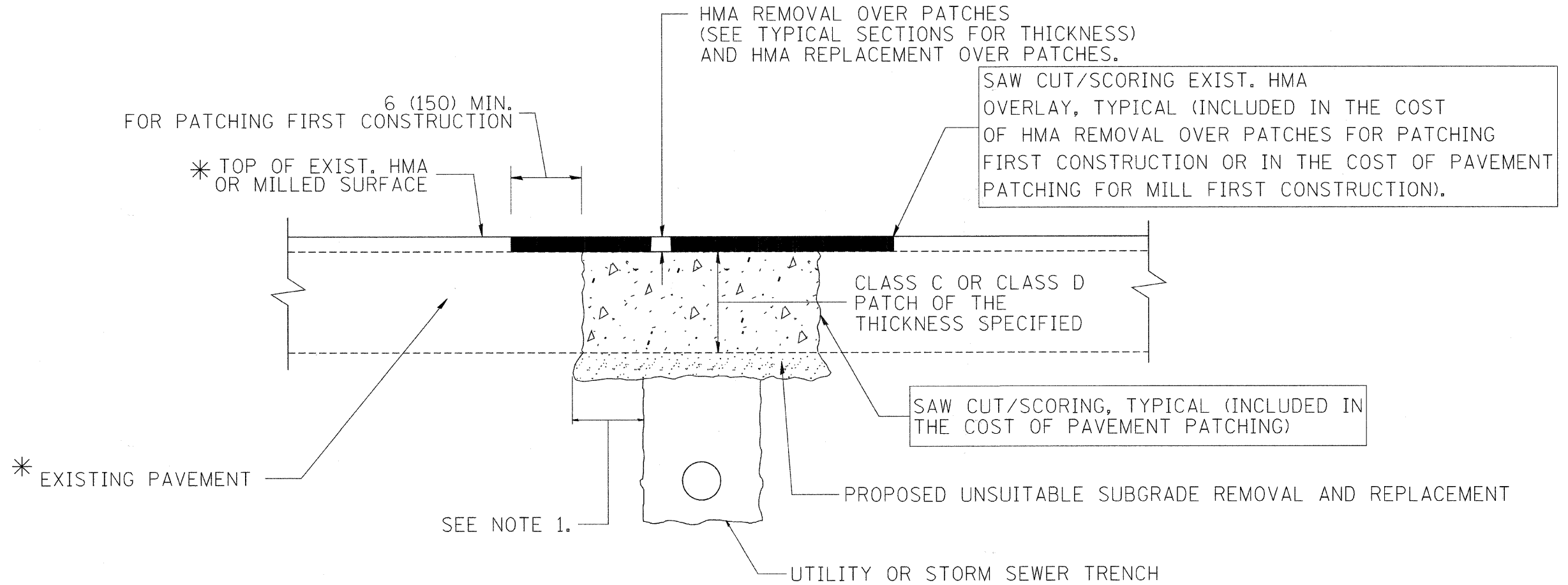
REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: VERT. NONE  
HORIZ. NONE  
DRAWN BY  
CHECKED BY

PLOT DATE = 3/5/2007  
FILE NAME = K:\dashed\bd600-dgn  
USER NAME = bborodt

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2779	09-00246-00-RS	COOK	25	20
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07
R. BORO	09/04/07
K. ENG	10/27/08

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: VERT. NONE  
HORIZ.

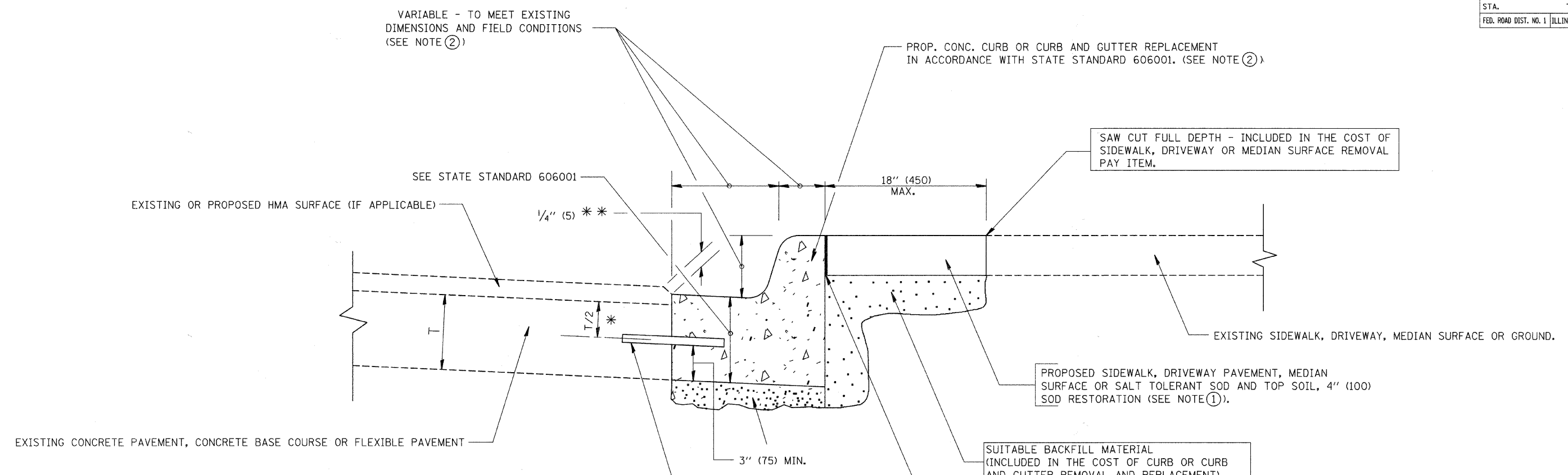
DRAWN BY

CHECKED BY

BD400-04 (BD-22)



F.A. DIST. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2719	09-00246-00-RS	COOK	25	21
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.  
 \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
 SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	12/15/09

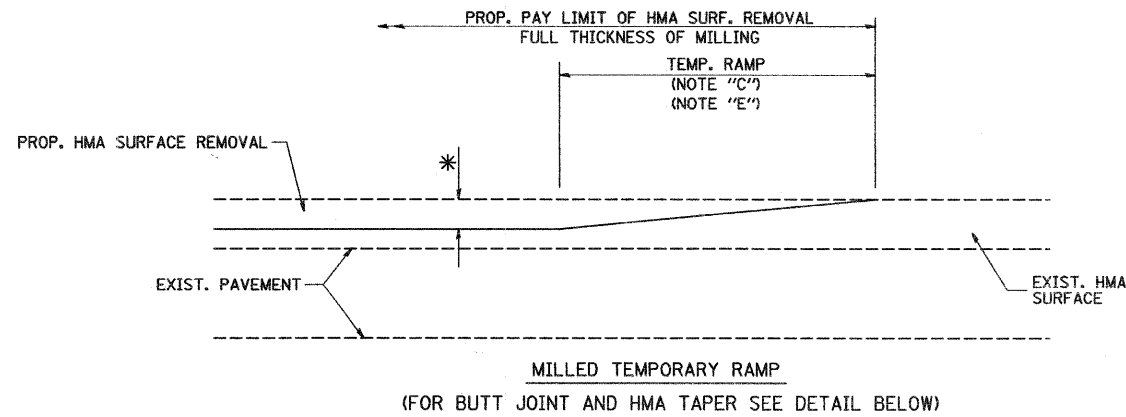
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 CURB OR CURB AND GUTTER  
 REMOVAL AND REPLACEMENT

SCALE: VERT. NONE  
 HORIZ. NONE  
 DRAWN BY  
 CHECKED BY  
 BD600-06 (BD-24)

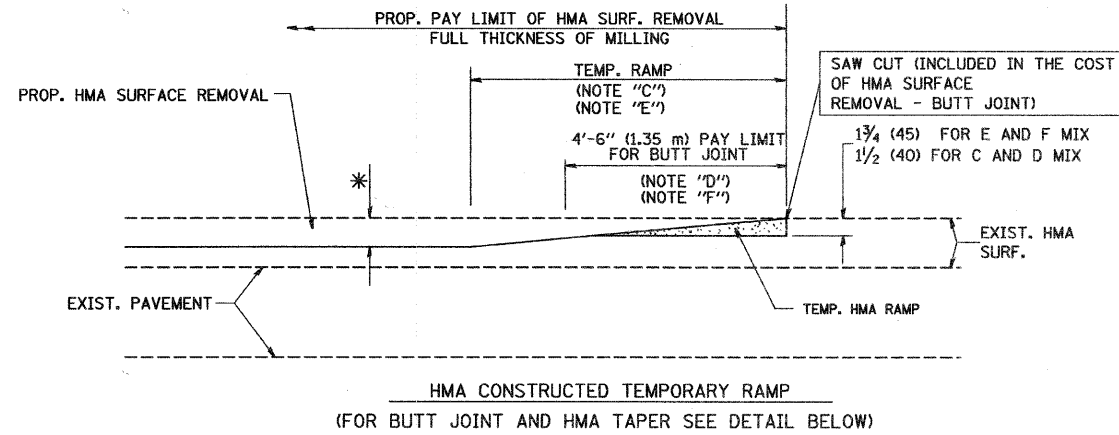
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

PLOT DATE = 12/15/2009  
 PLOT SCALE = 5/8" = 1'-0"  
 USER NAME = dr:valdesign

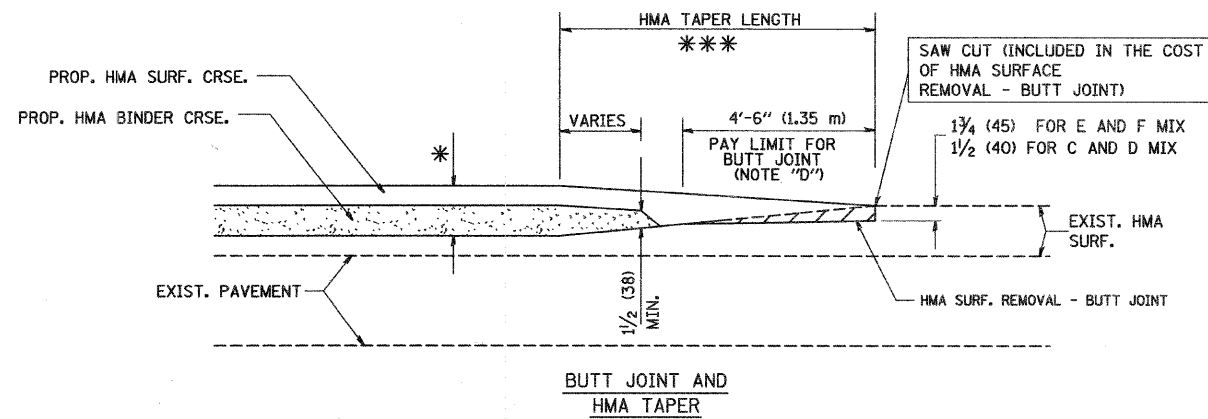
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2779	09-00246-00-98	COOK	25	22
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



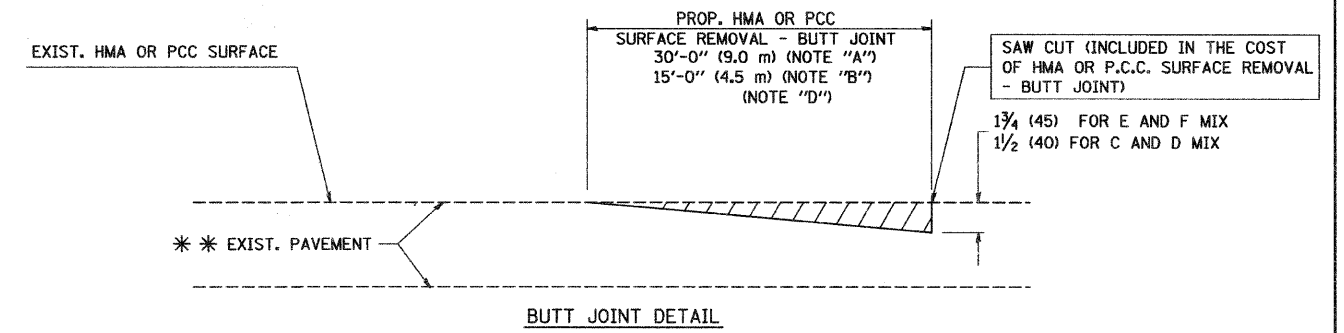
**OPTION 1**



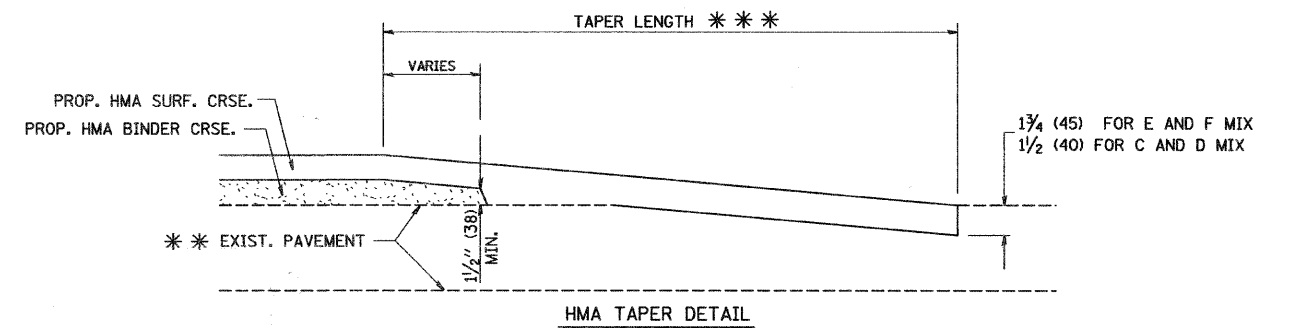
**OPTION 2**  
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND HMA TAPER DETAILS**

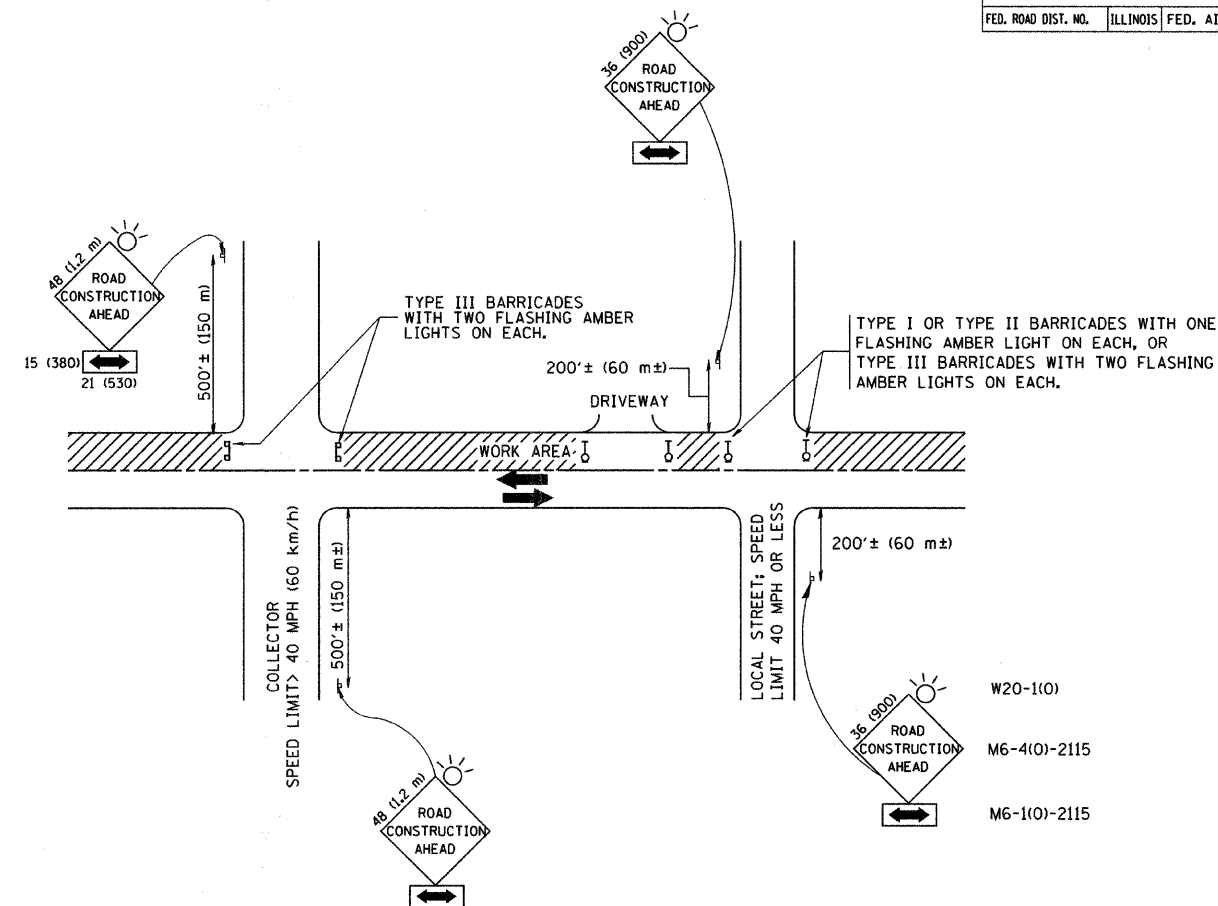
SCALE: VERT. NONE  
HORIZ. NONE

DRAWN BY

CHECKED BY

BD400-05 (VI-BD32)

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2779	09-00246-00-BB	COOK	25	23
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



W20-1(O)  
M6-4(O)-2115  
M6-1(O)-2115

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

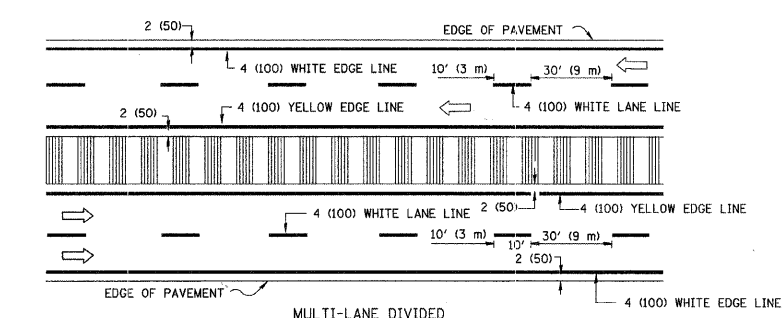
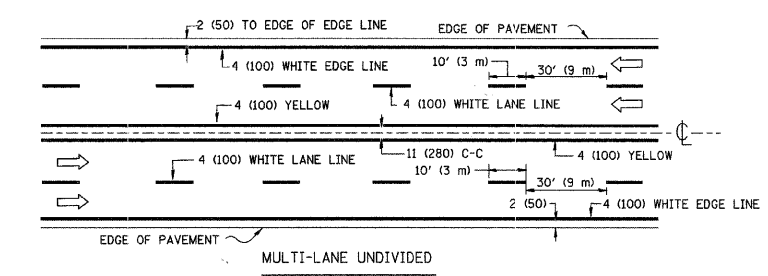
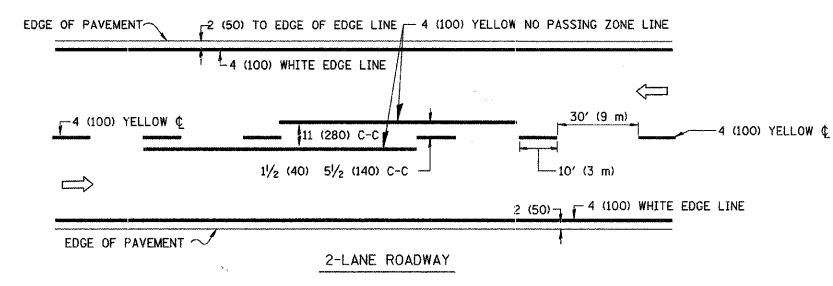
ILLINOIS DEPARTMENT OF TRANSPORTATION  
TRAFFIC CONTROL AND PROTECTION  
FOR  
SIDE ROADS, INTERSECTIONS, AND  
DRIVEWAYS

SCALE: NONE

DRAWN BY  
CHECKED BY

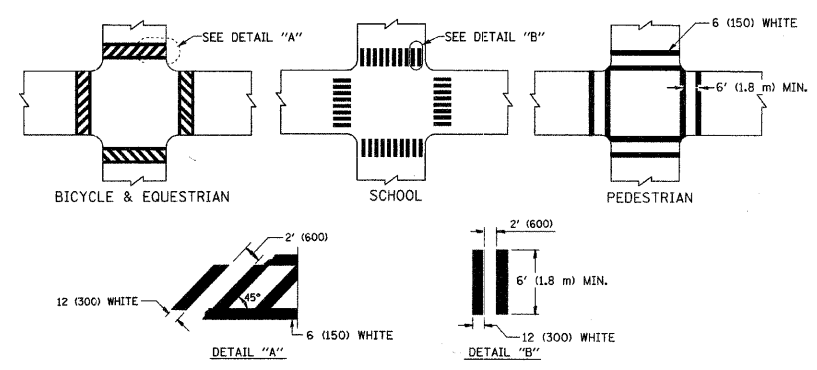
TC-10

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

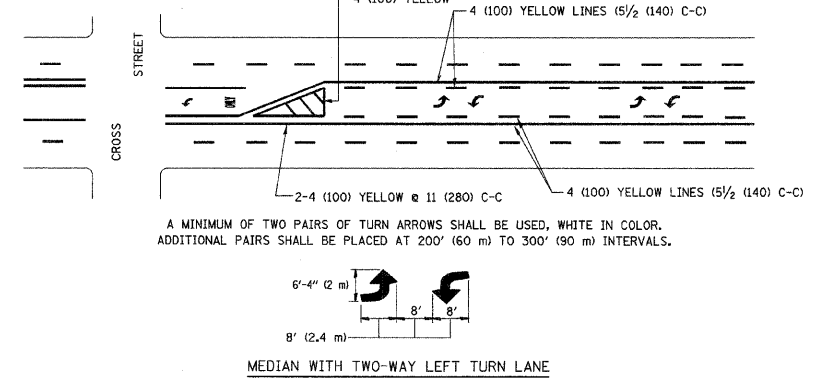
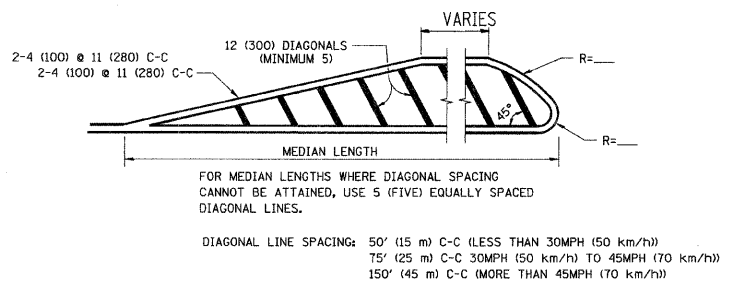
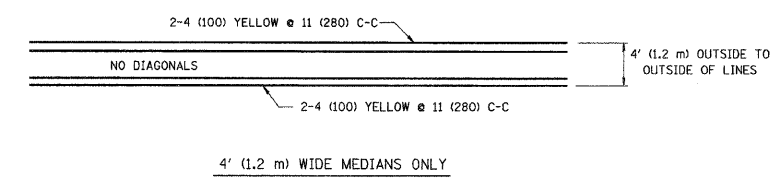


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

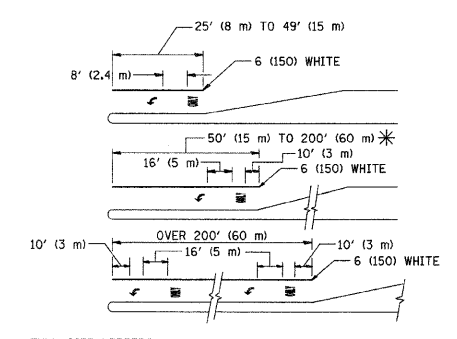
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

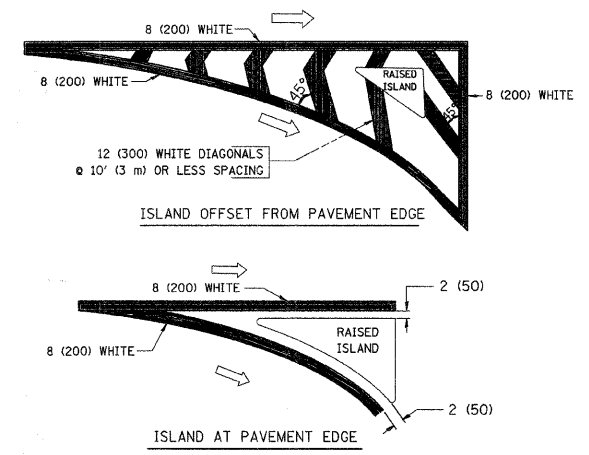


TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
C. JUCIUS	09-09-09


ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DISTRICT ONE  
 TYPICAL PAVEMENT MARKINGS


SCALE: NONE  
 DRAWN BY CADD  
 CHECKED BY  
 TC-13


PLOT DATE = 9/9/2009  
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 USER NAME = drvvelocogn

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2779	09-00246-00-R8	COOK	25	25
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		


**ROUTE MARKERS**


 FOR U.S. ROUTES  
MI-40-2424


 FOR ILLINOIS ROUTES  
MI-50-2424


 R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND


**ARROWS SIGNS**

 M5-1L-2115


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
 M6-1-2115


 M6-2-2115


 M6-3-2115


**CARDINAL DIRECTION & DETOUR SIGNS**

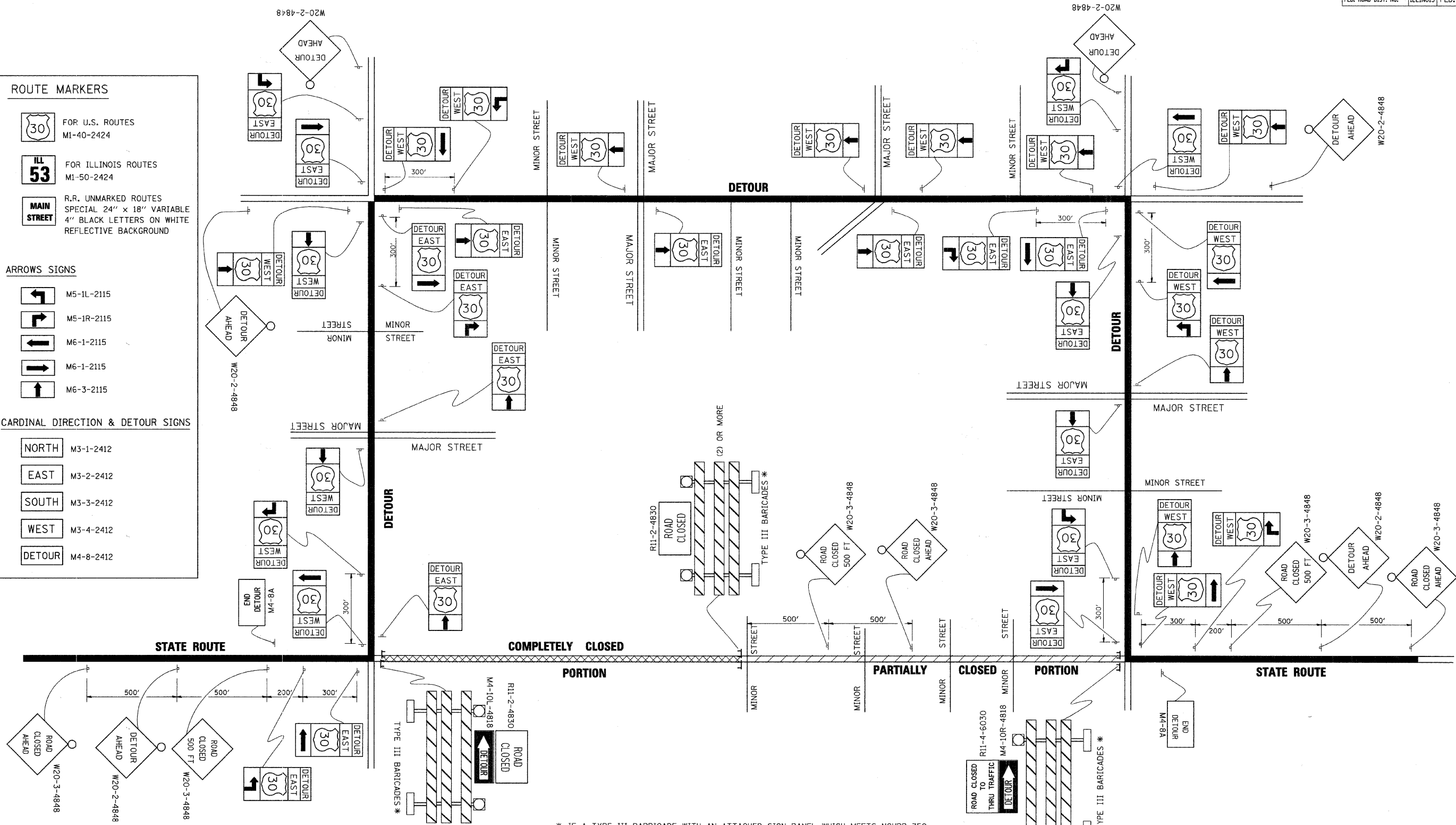
 NORTH M3-1-2412

 EAST M3-2-2412

 SOUTH M3-3-2412

 WEST M3-4-2412

 DETOUR M4-8-2412



\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

REVISIONS	
NAME	DATE
R. BORO	10/18/02
	09/14/09

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DETOUR SIGNING FOR CLOSING STATE HIGHWAYS**

SCALE: NONE

DRAWN BY  
CHECKED BY  
TC-21

PLOT DATE = 9/14/2009  
FILE NAME = c:\pwworking\dwg\1001\DRIVE\KOSON\2010\8364\2c21.dwg  
PLOT SCALE = 4/1/9999 / IN.  
USER NAME = drvtrc09gn