

**MAINTENANCE OF TRAFFIC  
GENERAL NOTES - ALL STAGES**

- THE STAGING PROCEDURES PRESENTED HERIN ARE THE SUGGESTED SEQUENCE OF CONSTRUCTION OPERATIONS. IT IS THE CONTRACTORS OPTION TO SUBMIT AN ALTERNATIVE STAGING PLAN TO THE ENGINEER FOR APPROVAL.
- TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE TRAFFIC CONTROL PLAN AND SECTION 701 OF THE STANDARD SPECIFICATIONS AS AMENDED BY THE SPECIAL PROVISION FOR CONSTRUCTION ZONE TRAFFIC CONTROL LRS-3.
- EXISTING TRAFFIC CONTROL SIGNS AND DEVICES WILL BE REMOVED BY THE CONTRACTOR AFTER THE PROPOSED TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER. ANY SIGNS OR DEVICES LEFT IN PLACE AT THIS TIME ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THIS WORK WILL BE PAID FOR BY THE CONTRACTOR.
- THE FOLLOWING TRAFFIC CONTROL STANDARDS ARE THE MINIMUM REQUIREMENTS FOR THE TRAFFIC CONTROL FOR THIS PROJECT: 701501, 701502, 701602, 701701, 701801, 701901, TC-10, TC-14, TC-22 AND TC-26.
- TEMPORARY CONCRETE BARRIER, DRUMS, AND TEMPORARY PAVEMENT MARKINGS SHALL BE INSTALLED THROUGHOUT THE LIMITS OF THE WORK ZONE AS SHOWN ON THE MAINTENANCE OF TRAFFIC TYPICAL SECTIONS AND PLAN SHEETS.
- THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC ON ROOSEVELT RD. THROUGHOUT THE DURATION OF CONSTRUCTION. TRUCK ACCESS TO BUSINESS SHALL BE MAINTAINED AT ALL TIMES.
- THE CONTRACTOR SHALL BEGIN WORK FROM HARLEM AVE. AND PROGRESS EAST ALONG ROOSEVELT RD. TO AUSTIN BLVD.
- EACH STAGE, INCLUDING THE THREE CORRESPONDING SUB-STAGES, SHALL BE COMPLETED PRIOR TO STARTING THE NEXT STAGE.
- THE CONTRACTOR WILL BE PERMITTED TO CLOSE A QUADRANT OF EACH SIDE STREET AT A TIME.
- THE CONTRACTOR SHALL REMOVE ALL EXISTING PAVEMENT MARKINGS AND SIGNS WHEN IN CONFLICT WITH PROPOSED TEMPORARY PAVEMENT MARKINGS AND SIGNS.
- WHERE THE PROPOSED WORK WILL IMPACT ALL OF THE ENTRANCES TO A PROPERTY THE CONTRACTOR SHALL STAGE THE WORK TO MAINTAIN BI-DIRECTIONAL ACCESS AT ALL TIMES. STAGING SHALL BE COORDINATED WITH THE PROPERTY OWNER AND APPROVED BY THE ENGINEER.
- VERTICAL PANELS, TYPE I BARRICADES OR DRUMS SHALL BE AT THE FAR EDGE OF THE OUTSIDE SHOULDER WHERE SHOWN IN THE PLANS. SPACING SHALL BE AT 25 FOOT CENTERS ON THE TAPER SECTIONS AND AT 25 FOOT CENTERS ON TANGENTS UNLESS OTHERWISE SHOWN.

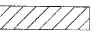


- DRUMS SHALL BE EQUIPPED WITH STEADY BURNING LIGHTS AND PLACED AT 25 FOOT CENTERS ON TAPER SECTIONS AND AT 25 FOOT CENTERS ON TANGENT.
- ALL ADVANCED WARNING SIGNS AND TEMPORARY PAVEMENT MARKINGS SHALL BE IN PLACE AND APPROVED BY THE ENGINEER PRIOR TO THE COMMENCEMENT OF ANY STAGE OR SUBSTAGE OF CONSTRUCTION.
- TRAFFIC CONTROL SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- ARROW BOARDS ARE REQUIRED FOR LANE CLOSURES AS SHOWN ON THE TRAFFIC CONTROL STANDARDS AND PLAN SHEETS. ARROW BOARDS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- WHERE THE HEIGHT DIFFERENCE BETWEEN ADJACENT PAVEMENT SURFACES EXCEEDS 1 INCH THE CONTRACTOR SHALL MILL OR PLACE LEVELING BINDER TO REDUCE THE DROP OFF TO LESS THAN 1 INCH. THIS WILL BE REQUIRED AT LOCATIONS WHERE TRAFFIC MUST CROSS THE DROP OFF SUCH AS ENTRANCES AND INTERSECTIONS, OR AS DIRECTED BY THE ENGINEER. NO MILLING OR LEVELING BINDER SHALL BE PLACED ON THE PROPOSED HMA PAVEMENT SURFACE. IT MAY BE NECESSARY TO DELAY MILLING OF THE EXISTING PAVEMENT AT THESE LOCATIONS UNTIL THE CONTRACTOR IS PREPARED TO PLACE THE PROPOSED HMA PAVEMENT.
- THE FURNISHING, INSTALLATION AND RELOCATION OF ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE STATE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE LUMP SUM COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).

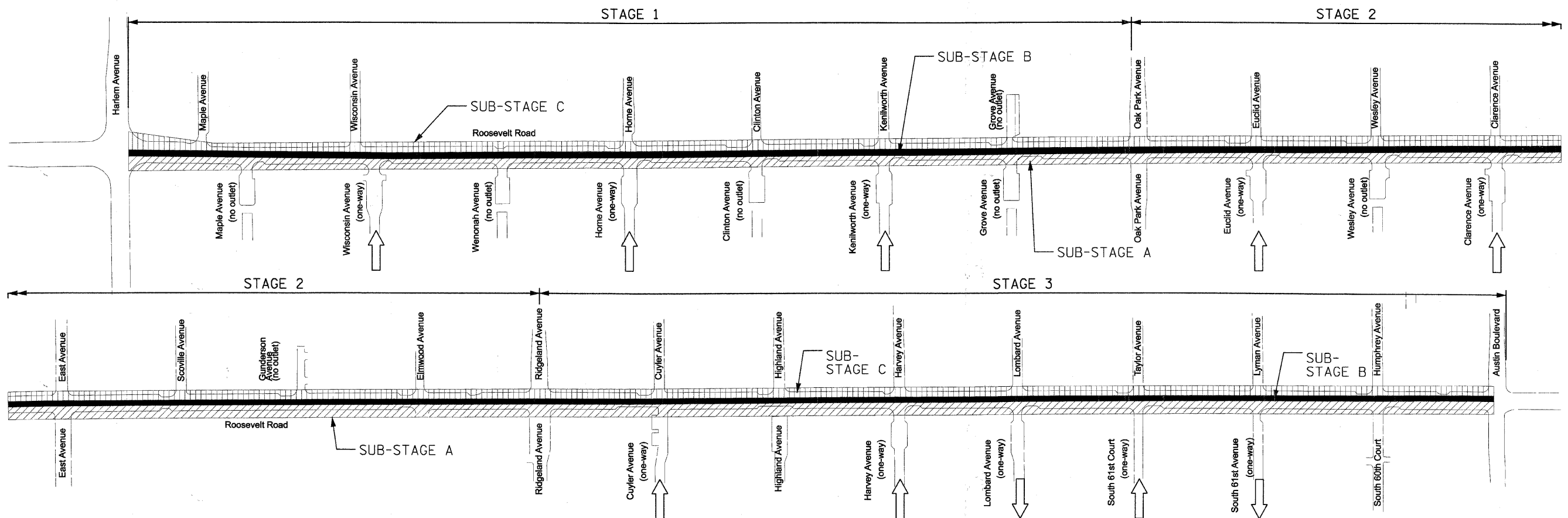
- ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE LUMP SUM COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- ALL SIDE STREETS AND DRIVEWAYS SHALL BE MAINTAINED DURING ALL STAGES OF CONSTRUCTION, WITH THE EXCEPTION OF SHORT TERM CLOSURES FOR HOT-MIX ASPHALT PAVING. ALL CLOSURES SHALL BE COORDINATED WITH THE ENGINEER AND THE PROPERTY OWNER.
- PROPOSED STORM SEWER SHALL BE CONSTRUCTED PRIOR TO PLACING THE PROPOSED HMA PAVEMENT FOR EACH STAGE. STORM SEWER PIPES THAT EXTEND ACROSS SUB-STAGES SHALL BE PLACED TO THE LIMITS POSSIBLE IN EACH SUB-STAGE AND COMPLETED PRIOR TO COMMENCING WITH THE NEXT SUB-STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. SHORT TERM LANE CLOSURES, SUCH AS IDOT STANDARD DRAWINGS 701502 AND 701602, SHALL BE USED TO COMPLETE THE CONNECTION OF THE PROPOSED STORM SEWER PIPES TO THE EXISTING STORM SEWER SYSTEM. ONE 11 FOOT LANE MUST BE MAINTAINED IN EACH DIRECTION AT ALL TIMES EXCEPT AS NOTED IN THE MAINTENANCE OF TRAFFIC PLANS.
- FINAL PAVEMENT SURFACE SHALL BE PLACED CONTINUOUSLY AFTER COMPLETING ALL THREE STAGES. PLACEMENT OF FINAL PAVEMENT SURFACE BY INDIVIDUAL STAGE WILL NOT BE PERMITTED.
- PROPOSED CURB & GUTTER, SIDEWALK, AND LANDSCAPING SHALL BE PLACED PRIOR TO PLACING FINAL PAVEMENT SURFACE TO AVOID DAMAGE TO THE PAVEMENT.
- THE CONTRACTOR SHALL COORDINATE ALL BUS STOP ACCOMMODATIONS WITH THE CTA TO MAINTAIN BUS ACCESS.
- CONTRACTOR SHALL PROTECT PROPOSED MEDIAN ISLANDS AND DECORATIVE CROSSWALKS DURING SUBSEQUENT STAGES OF CONSTRUCTION. SURFACE OF ISLANDS AND DECORATIVE CROSSWALKS SHALL BE PROTECTED FROM CONSTRUCTION EQUIPMENT, IMPACT DAMAGE, AND MARRING BY CONSTRUCTION DEBRIS SUCH AS ASPHALT, CONCRETE, AND LANDSCAPING MATERIALS. METHOD OF PROTECTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO USE. MEDIAN ISLANDS AND DECORATIVE CROSSWALKS SHALL BE STAIN FREE AT THE TIME OF ACCEPTANCE.
- LEFT TURNS FROM ROOSEVELT ROAD SHALL BE RESTRICTED DURING CONSTRUCTION EXCEPT WHERE A DEDICATED LEFT TURN LANE IS PROVIDED AND AT SIGNALIZED INTERSECTIONS.

**MAINTENANCE OF TRAFFIC  
SUGGESTED STAGING PLAN**

- SUB-STAGE A (SOUTH SIDE OF ROOSEVELT ROAD)  
 INSTALL TEMPORARY TRAFFIC CONTROL FOR STAGE  
 CONSTRUCT NEW LIGHT POLE FOUNDATIONS  
 INSTALL STORM SEWER STRUCTURES AND LATERALS  
 REMOVE AND REPLACE EXISTING SIDEWALK, CROSSWALK, AND CURB & GUTTER  
 INSTALL LANDSCAPING  
 MILL EXISTING PAVEMENT AND CONSTRUCT CURB BUMP OUTS  
 PAVE PROPOSED HMA BINDER COURSE  
 INSTALL PROPOSED TRAFFIC CONTROL
- SUB-STAGE B (CENTER OF ROOSEVELT ROAD)  
 INSTALL TEMPORARY TRAFFIC CONTROL FOR STAGE  
 INSTALL/EXTEND STORM SEWER LATERALS  
 MILL EXISTING PAVEMENT  
 PAVE PROPOSED HMA BINDER COURSE  
 INSTALL PROPOSED TRAFFIC CONTROL
- SUB-STAGE C3 (NORTH SIDE OF ROOSEVELT ROAD)  
 INSTALL TEMPORARY TRAFFIC CONTROL FOR STAGE  
 CONSTRUCT NEW LIGHT POLE FOUNDATIONS  
 INSTALL STORM SEWER STRUCTURES AND LATERALS  
 REMOVE AND REPLACE EXISTING SIDEWALK, CROSSWALK, AND CURB & GUTTER  
 INSTALL NEW LANDSCAPING  
 MILL EXISTING PAVEMENT AND CONSTRUCT CURB BUMP OUTS  
 PAVE PROPOSED HMA BINDER COURSE
- PROJECT COMPLETION  
 PAVE PROPOSED HMA SURFACE  
 INSTALL REMAINDER OF PROPOSED TRAFFIC CONTROL  
 REMOVE ALL TEMPORARY TRAFFIC CONTROL AND REOPEN ENTIRE ROADWAY

**MAINTENANCE OF TRAFFIC SCHEMATIC**

-  SUB-STAGE 1
-  SUB-STAGE 2
-  SUB-STAGE 3



FILE NAME = DIRTE38-ght-staging01.dgn	USER NAME = IDOT	DESIGNED - MD	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC GENERAL NOTES</b>			F.A.P. RTE. 347	SECTION 09-00248-00-RS	COUNTY COOK	TOTAL SHEETS 274	SHEET NO. 21	
	PLOT SCALE = 20.0000' / 1"	DRAWN - LF	REVISED -		SCALE: 1" = 20'	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 63432			
	PLOT DATE = 3/11/2010	CHECKED - JB	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE	REVISED -										