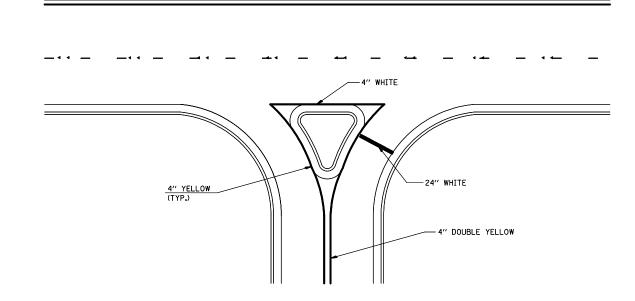
TYPICAL MEDIAN TRANSITIONS

REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNERS

SEE GENERAL NOTE #6

GENERAL NOTES

- 1. WHEN MEDIANS ARE PRESENT, PAVEMENT MARKINGS ARE TO BE PLACED ADJACENT TO MEDIANS.
- 2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
- 3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
- 4. A STRIPING KEY IS AVAILABLE ELSEWHERE AND SHALL BE SHOWN WHERE THE QUANTITIES ARE LISTED.
- 5. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
- 6. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING, (30 MPH USE 15' (<50 km/h USE 4.5 m) 30-45 MPH USE 20' (50-75 km/h USE 6.0 m) >45 MPH USE 30' (>75 km/h USE 9.0 m)

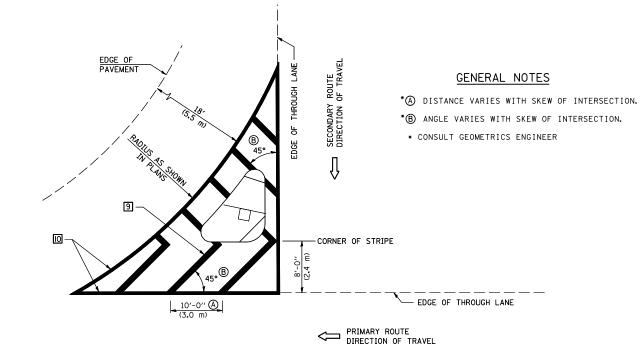


NOTES:

THE ACTUAL MEDIAN CONFIGURATION WILL BE AS SHOWN IN THE PLANS (TAPER OR REVERSE CURVE).

RUMBLE & MOUNTABLE MEDIANS SHALL BE OUTLINED WITH $\fbox{2}$.

RIGHT IN - RIGHT OUT ACCESS



<u>ISLAND</u>

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

/06	REVISIONS REPLACED DETAIL F-5.25	TJB	ILLINOIS DEPARTMENT OF TRANSPORTATION
9/2009	REVISED STRIPING	KJT	DAVENENT MARKING AND
			PAVEMENT MARKING AND
			MARKERS (RURAL &
			URBAN APPLICATIONS)
			ONDAIN AIT LIGATIONS
			DISTRICT 5 DETAIL NO. 7800AAAA

NOTE:

DESIGNER

DATE NAME SCALE NAME

SHEET 3 OF 4