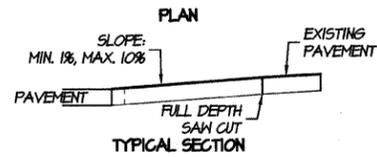
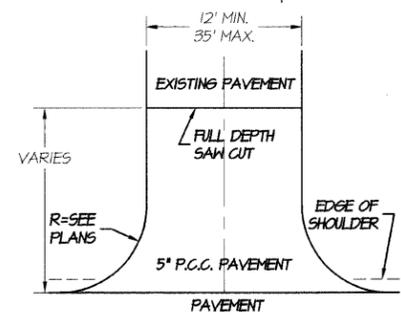
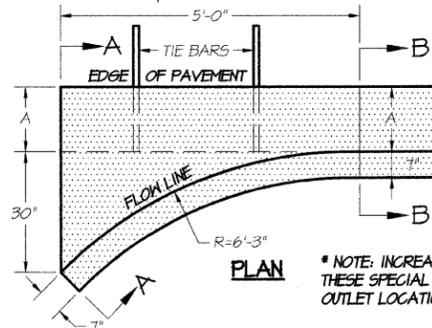


TYPICAL DRIVEWAY ENTRANCE DETAIL A2
SCALE: NO SCALE 2

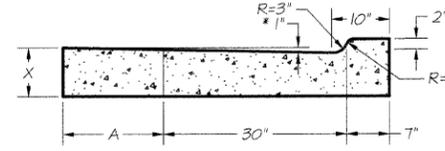
GENERAL NOTES
 1. A LONGITUDINAL CURB EXPANSION JOINT SHALL BE PLACED ONLY WHERE THE PROPOSED P.C. CONCRETE SIDEWALK OR DRIVEWAY PAVEMENT MEETS AN EXISTING CONCRETE ENTRANCE.
 2. THE LONGITUDINAL CURB EXPANSION JOINT SHALL CONFORM TO SECTION 1051 OF THE STANDARD SPECS.
 3. THE ENTRANCE GRADES WILL BE AS SHOWN ON THE STATION CROSS SECTIONS AND AS DIRECTED BY THE ENGINEER.
 4. BEFORE A CHANGE IN THE METHOD OF CONSTRUCTION IS ALLOWED, THE REQUEST SHALL BE SUBMITTED IN WRITING AND MUST BE APPROVED BY THE ENGINEER.



TYPICAL COMMERCIAL ENTRANCE DETAIL C2
SCALE: NO SCALE 2



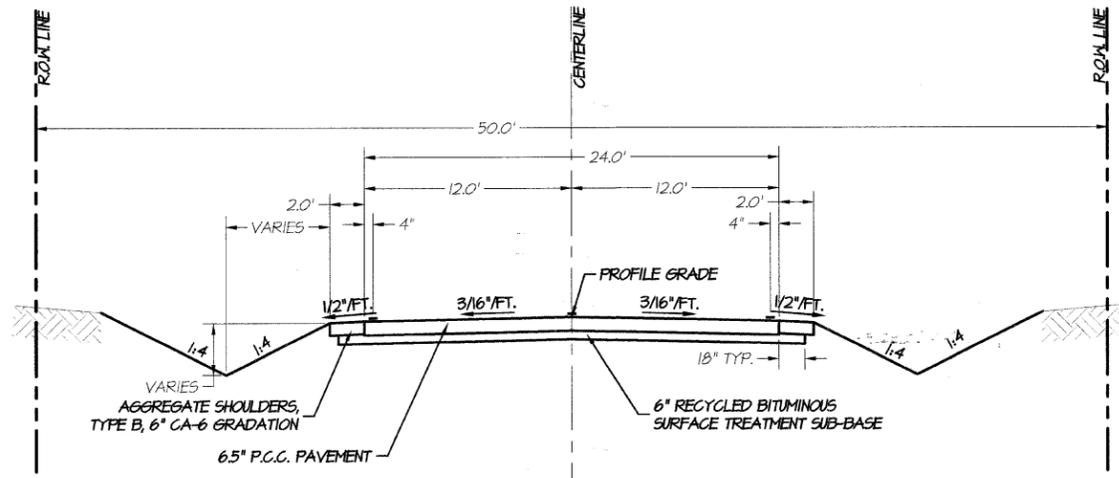
* NOTE: INCREASE TO 2" WHERE IN THE PLANS IT IS SPECIFIED THAT THESE SPECIAL INLETS ARE TO BE CONSTRUCTED AS OUTLETS. ALL OUTLET LOCATIONS WILL BE CONFIRMED BY THE ENGINEER.



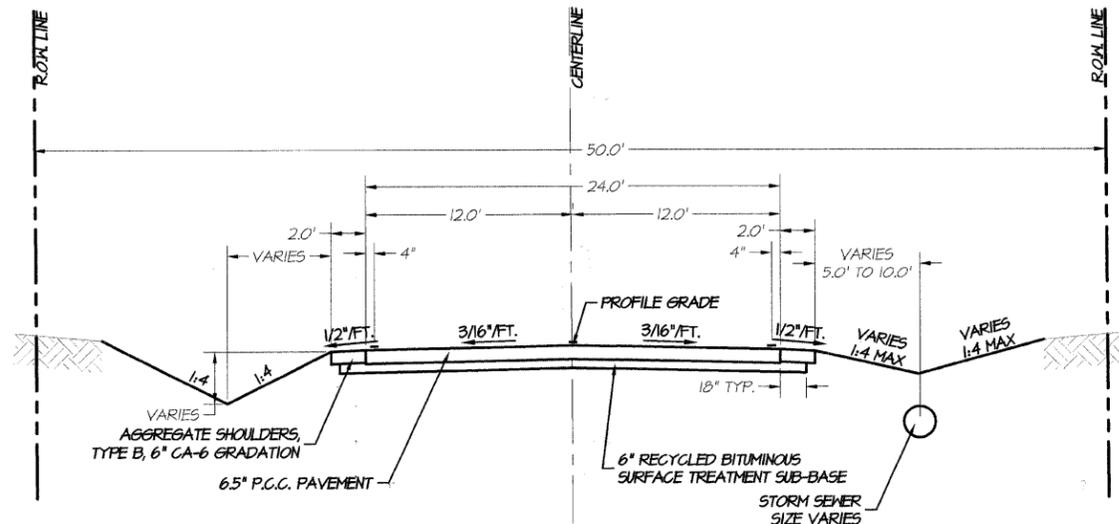
SECTION A-A

COMBINATION CONCRETE CURB AND GUTTER - SPECIAL INLETS & OUTLETS
SCALE: NO SCALE

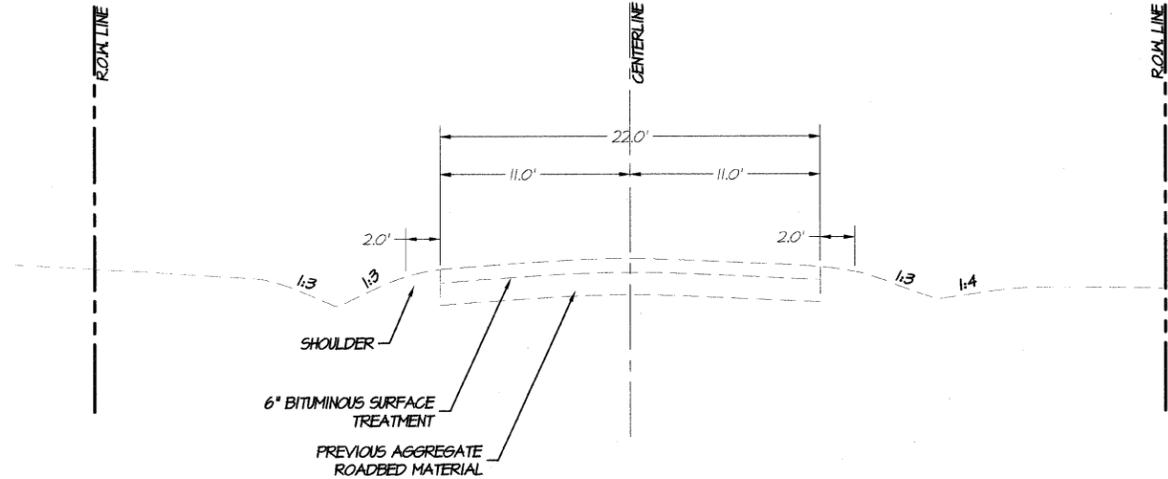
PROPOSED TYPICAL SECTION SOUTH CHESTNUT STREET
 STATION 59+10 TO STATION 61+19.85
 STATION 62+31.85 TO STATION 63+64.25
 STATION 69+47.55 TO 69+65



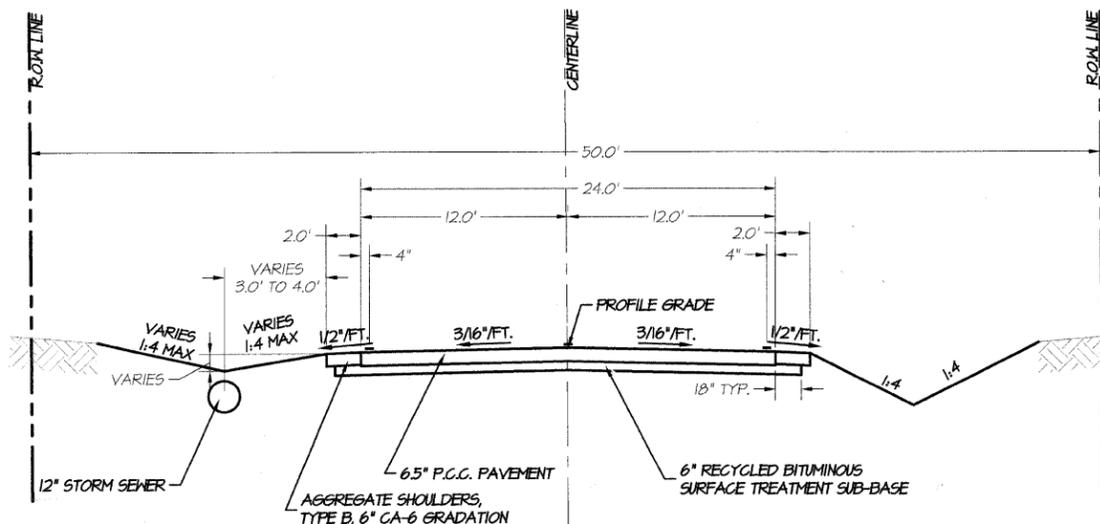
PROPOSED TYPICAL SECTION SOUTH CHESTNUT STREET
 STATION 63+64.25 TO STATION 69+47.55



EXISTING TYPICAL SECTION SOUTH CHESTNUT STREET
 STATION 59+00 TO STATION 70+00



PROPOSED TYPICAL SECTION SOUTH CHESTNUT STREET
 STATION 61+19.85 TO STATION 62+31.85



| | | | | |
|--------------------|----------------|----------|--------------|-----------------|
| ROUTE NO. | SECTION | CITY | TOTAL SHEETS | SHEET NO. |
| F.A.U. 7944 | 96-00046-00-PV | PANA | 17 | 2 |
| FED. ROAD DIST NO. | | ILLINOIS | PROJECT | ARA-M-5053(006) |

GENERAL NOTES:
 1. CLASS 51 CONCRETE SHALL BE USED THROUGHOUT.
 2. TIE BARS SHALL BE NO. 6 AT 24" CENTERS UNLESS OTHERWISE SHOWN. SPECIAL INLETS AND OUTLETS SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.
 3. TIE BARS SHOWN ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL TO CLASS 51 CONCRETE OUTLETS.
 4. WHEN SPECIAL INLET IS CONSTRUCTED ADJACENT TO FLEXIBLE PAVEMENT, THE TIE BARS SHALL BE OMITTED AND ALL CONSTRUCTION JOINTS SHALL BE PROVIDED WITH A DOWEL BAR CONFORMING TO ARTICLE 1006.11(b) (106.11(b)).
 5. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12, B-6.18 OR B6.24 WHICH PRICE SHALL INCLUDE ALL LABOR AND MATERIAL AS SPECIFIED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

| | B-6.12 | B-6.18 | B-6.24 |
|-----------|--------|--------|--------|
| A | 12 | 18 | 24 |
| B | 6 | 6 | 6 |
| X | 9 | 10 | 9 |
| (CU. YDS) | 0.37 | 0.44 | 0.56 |



| NO. | DATE | APPR. | DESCRIPTION |
|-----|----------|-------|-------------------|
| 1 | 01.19.10 | AKL | PER IDOT COMMENTS |

SCALE: 1" = 20'
 DRAWN BY: AKL
 DATE: 2009 AUG 29
 CHECKED BY: AKL
 DATE: 2009 AUG 29

CHESTNUT STREET IMPROVEMENTS
 FROM 7th STREET TO 9th STREET
 PANA, ILLINOIS
 F.A.U. ROUTE 7944 - SECTION 96-00046-00-PV
TYPICAL SECTIONS & DETAILS

BBB ENGINEERS, INC.
 CONSULTING ENGINEERS
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 LICENSE NO. 04/03/2011

PROJECT NO. **911584**
 DATE STAMP **2009 NOV 30**
 SHEET NUMBER **2/17**



Thu, 21 Jan 2010 - 10:37am Last Edited By: Reid Torbeck Drawing Name: P:\2009\911584_Pana\DWG\911584-CoverTypical.dwg