

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	(1213&3198B)RS-4	COOK	23	1
ILLINOIS CONTRACT NO. 60356				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

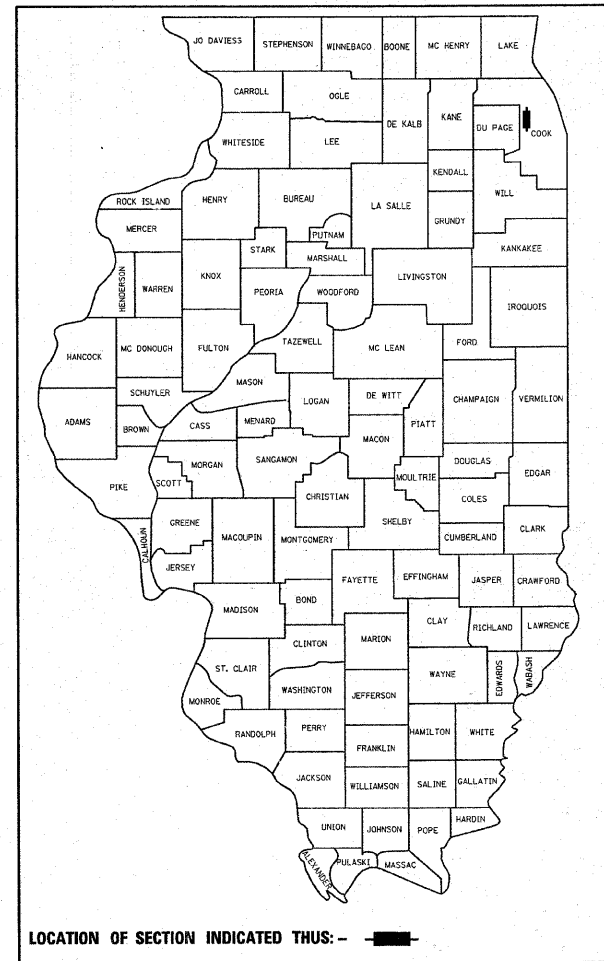
PROPOSED HIGHWAY PLANS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

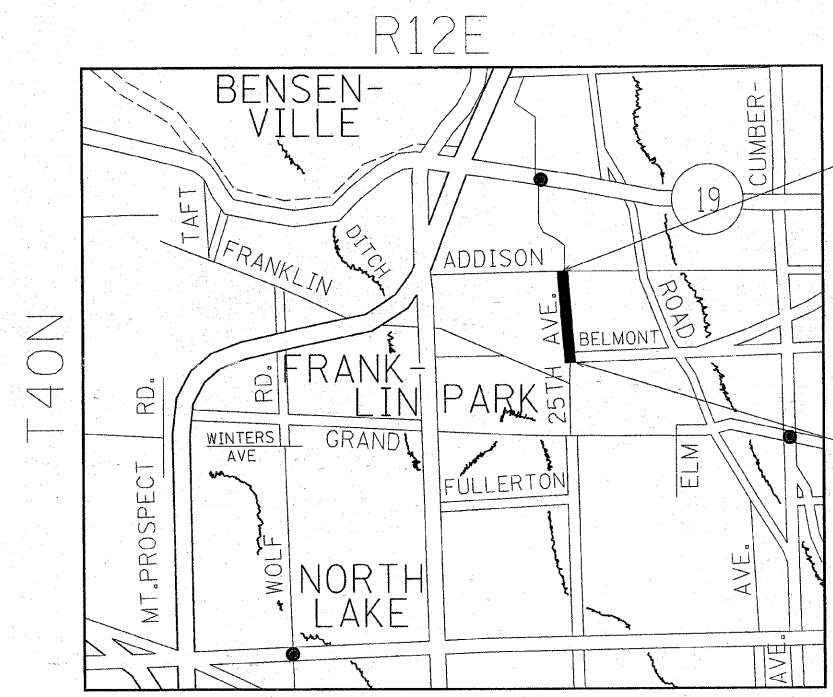
THE PROJECT IS LOCATED IN THE VILLAGE OF FRANKLIN PARK

FAU ROUTE 2714: 25TH AVE. (ROSE ST.)
ADDISON ST. TO BELMONT AVE.
SECTION (1213&3198B)RS-4
PROJECT : ESP-2714(OIO)
RESURFACING (MAINTENANCE)
COOK COUNTY
C-91-243-97

D-91-243-97



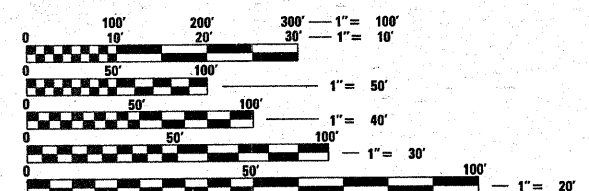
LOCATION OF SECTION INDICATED THUS: —



PROJECT ENDS
STA 29+75

TRAFFIC DATA
ADT (2009) = 11,600
POSTED SPEED LIMIT = 30 MPH

PROJECT BEGINS
STA 00+75



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER JENPAI CHANG 847-705-4432
PROJECT MANAGER KEN ENG

GROSS LENGTH OF THE PROJECT = 2900 FT. = .55 MILES
NET LENGTH OF THE PROJECT = 2900 FT. = .55 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 9, 20 09
Diana M. O'Keefe *or*
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 27, 20 09
Charles J. Ingwall
 ENGINEER OF DESIGN AND ENVIRONMENT
March 27, 20 09
Christine M. Reed
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 60356

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

<u>SHEET NO.</u>	<u>DESCRIPTION</u>	<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
1	TITLE SHEET	000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	420001-07	PAVEMENT JOINTS
3	SUMMARY OF QUANTITIES	442201-03	CLASS C AND D PATCHES
4-6	EXISTING AND PROPOSED TYPICAL SECTIONS	604001-03	FRAMES AND LIDS, TYPE 1
7-8	ROADWAY AND PAVEMENT MARKING PLAN	635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
9-10	DETECTOR LOOPS REPLACEMENT PLANS	635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
11	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING, BD600-03 (BD-8)	701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS GREATER THAN OR EQUAL TO 45MPH
12	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT, BD 400-04 (BD-22)	701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
13	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT, BD600-06 (BD-24)	701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
14	BUTT JOINT AND HMA TAPER, BD 400-05 (BD-32)	701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
15	CATCH BASIN, INLET, AND MANHOLE DETAILS, BD600-13 (BD-47)	701901-01	TRAFFIC CONTROL DEVICES
16	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS, TC-10	780001-02	TYPICAL PAVEMENT MARKINGS
17	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT), TC-11	781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
18	DISTRICT ONE TYPICAL PAVEMENT MARKINGS, TC-13	886001-01	DETECTOR LOOP INSTALLATIONS
19	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC), TC-14	886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS
20	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING, TC-16		
21	SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS, TC-18		
22	ARTERIAL ROAD INFORMATION SIGNING, TC-22		
23	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING, TS-07		

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT (OR ISTHA).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISABILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

THE RESIDENT ENGINEER SHALL CONTACT WALLY CZARNY, AT (773) 685-4342 (OFFICE) OR (847) 715-8414 (CELL) A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR, STEVE BRINK, AT (847) 705-4155 (OFFICE) OR (847) 846-7189 (CELL) A MINIMUM OF 72 HOURS PRIOR TO THE INSTALLATION OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

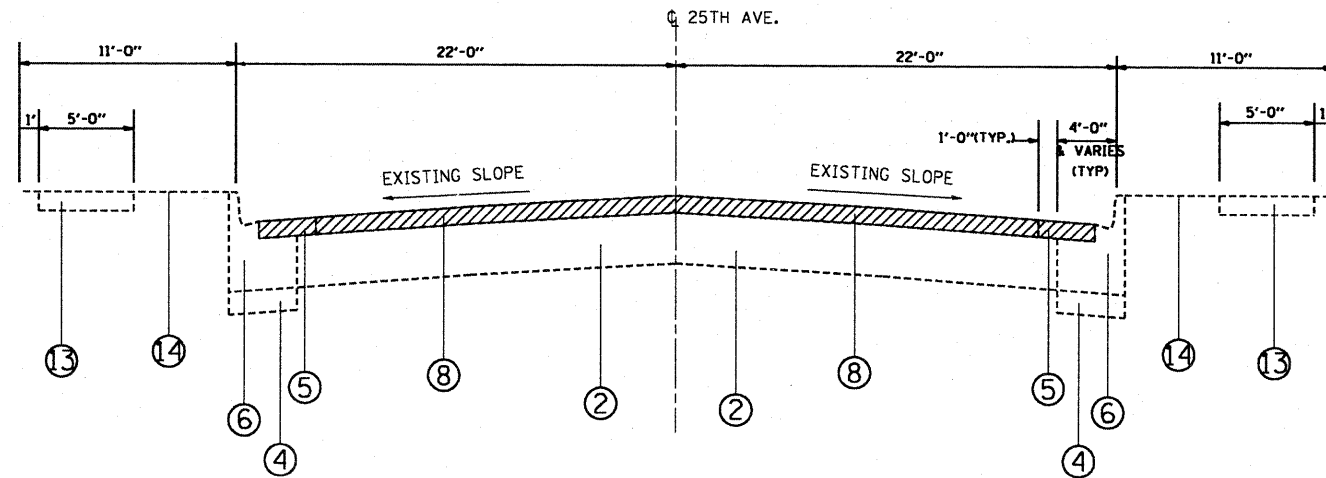
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ct:\pwork\PWIDOT\AQUEELFF\0120745\0124397-sht-plan.dgn		DRAWN -	REVISED -			2714	(1213&3198)RS-4	COOK	23	2	
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60356					
PLOT DATE = 2/11/2009		DATE -	REVISED -			SCALE:	SHEET NO. 2 OF 23 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT	

SUMMARY OF QUANTITIES			URBAN 100% FED.	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			URBAN 100% FED.	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES						CODE NO	ITEM	UNIT	TOTAL QUANTITIES					
				1000									1000				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	7	7					70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	1143	1143				
40600300	AGGREGATE (PRIME COAT)	TON	31	31					70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	820	820				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5	5					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	670	670				
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	368	368				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	221	221					70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	381	381				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	225	225				*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	35	35				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1275	1275				*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1143	1143				
42001300	PROTECTIVE COAT	SO YD	26	26				*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	820	820				
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	2304	2304				*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	670	670				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	12864	12864				*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	368	368				
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	150	150				*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	59	59				
44002209	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 1/4"	SO YD	632	632				*	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	48	48				
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SO YD	583	583				*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	1204	1204				
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SO YD	304	304				X0321374	GRADING AND SHAPING SHOULDERS	FOOT	780	780					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	304	304				X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	507	507					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	304	304				NP	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	35	35				
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	304	304				Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	16	16													
55039700	STORM SEWERS TO BE CLEANED	FOOT	300	300													
60250200	CATCH BASINS TO BE ADJUSTED	EACH	18	18													
60255500	MANHOLES TO BE ADJUSTED	EACH	25	25													
60260100	INLETS TO BE ADJUSTED	EACH	12	12													
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	55	55													
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6													
67100100	MOBILIZATION	L SUM	1	1													
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1													
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1													
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1													
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2248	2248													
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	35	35													

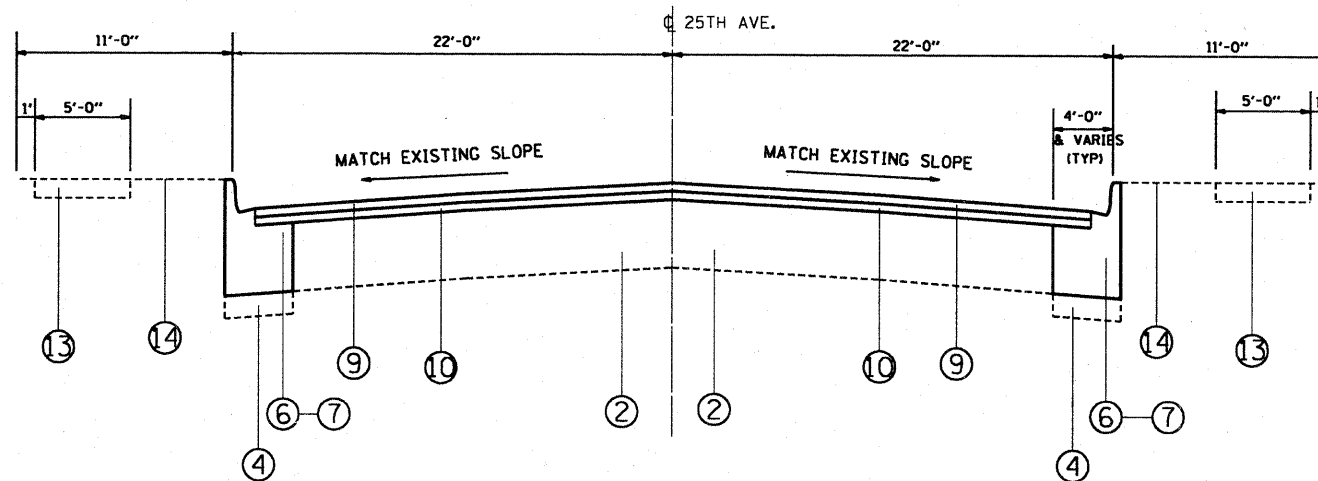
* -Specialty Items
NP=Non-participating

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.						
Plot Scale = 100,000' / IN.	CHECKED -	REVISED -	2714									(1213&3198)RS-4	COOK	23	3							
PLOT DATE = 2/11/2009	DATE -	REVISED -	SCALE:									SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 60356						
			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT																			

LEGEND



EXISTING TYPICAL SECTION
STATION:
00+75 TO 15+97.20



PROPOSED TYPICAL SECTION
STATION:
00+75 TO 15+97.20

- ① EXISTING HOT-MIX ASPHALT RESURFACING, 3/4" (±)
- ② EXISTING PCC BASE COURSE, 9" (±)
- ③ EXISTING AGGREGATE SHOULDER, TYPE B
- ④ SUB BASE GRANULAR MATERIAL, TYPE A, 6"
- ⑤ EXISTING HMA WIDENING
- ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ⑦ PROPOSED COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑬ EXISTING PCC SIDEWALK
- ⑭ EXISTING SODDING
- ⑮ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 1 1/2"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

	MIXTURE TYPE	AC TYPE	AIR VOIDS (%)
ROADWAY	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"	PG 64-22	4% @ 70 GYR
	POLYMERIZED LEVELING BINDER (MM), IL 4.75, N50, 3/4"	SBS/SBR PG 76-28/-22	4% @ 50 GYR
PATCHES	CLASS D PATCHES, (BINDER IL-19.0 MM), 2 1/2"	PG 64-22*	4% @ 70 GYR
	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, (BINDER IL-19.0 MM)	PG 64-22*	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

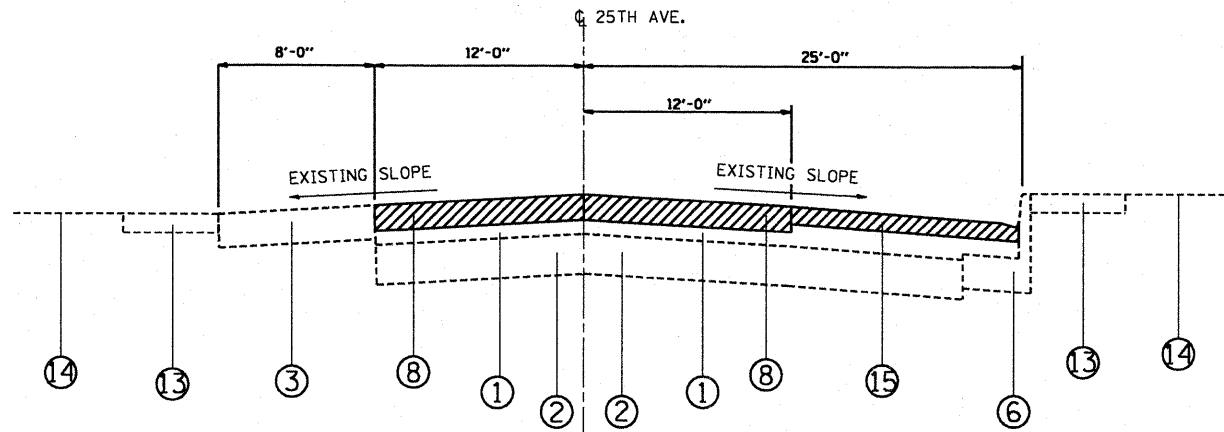
* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

NOTE: PATCH FIRST THEN MILL

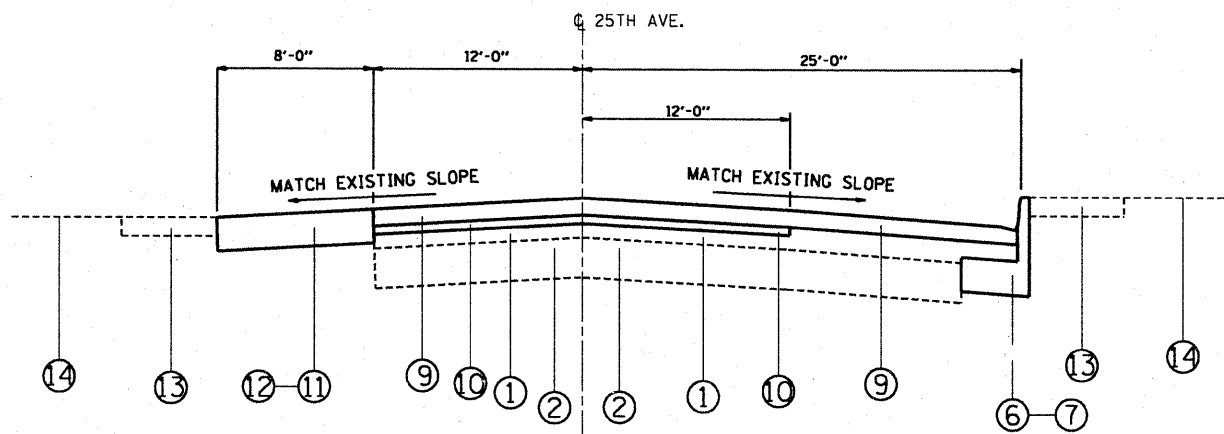
LEGEND

- ① EXISTING HOT-MIX ASPHALT RESURFACING, $\frac{3}{4}$ " (\pm)
- ② EXISTING PCC BASE COURSE, 9" (\pm)
- ③ EXISTING AGGREGATE SHOULDER, TYPE B
- ④ SUB BASE GRANULAR MATERIAL, TYPE A, 6"
- ⑤ EXISTING HMA WIDENING
- ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER
- ⑦ PROPOSED COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - $2\frac{1}{4}$ "
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, $1\frac{1}{2}$ "
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, $\frac{3}{4}$ "
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑬ EXISTING PCC SIDEWALK
- ⑭ EXISTING SODDING
- ⑮ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - $1\frac{1}{2}$ "

NOTE:
HMA OVERLAYS GUTTER AT VARIOUS SECTIONS



EXISTING TYPICAL SECTION
STATION:
15+97.20 TO 25+22



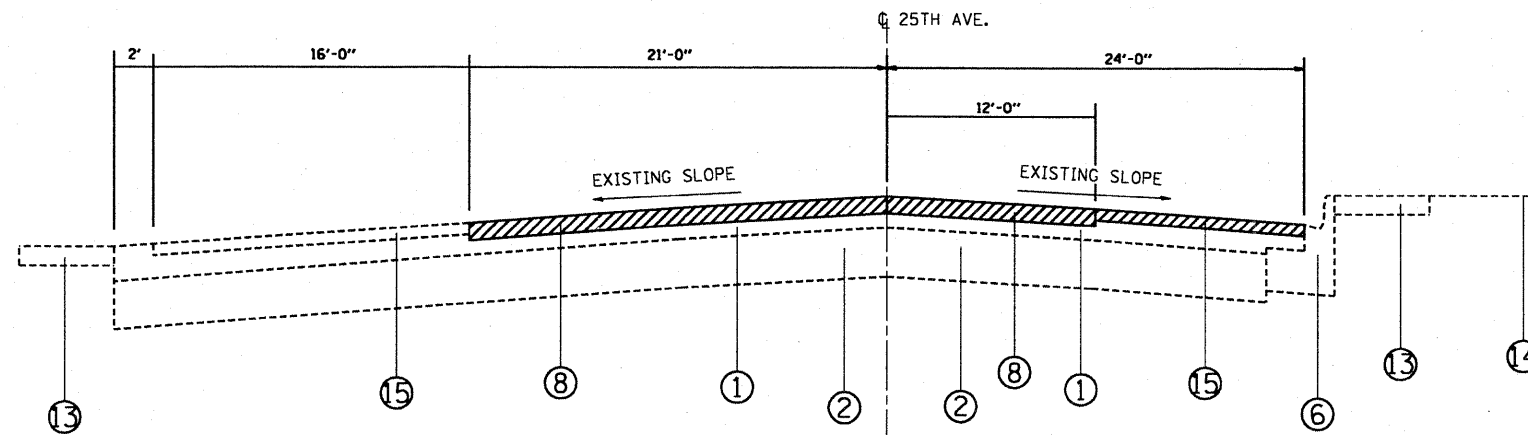
PROPOSED TYPICAL SECTION
STATION:
15+97.20 TO 25+22

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ct:\pwork\PWIDOT\AQEELFF\0120745\0124397-shit-plan.dgn		DRAWN -	REVISED -		2714	(1213&3198B)RS-4	COOK	23	5		
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PLOT DATE = 2/11/2009		DATE -	REVISED -		ILLINOIS FED. AID PROJECT						

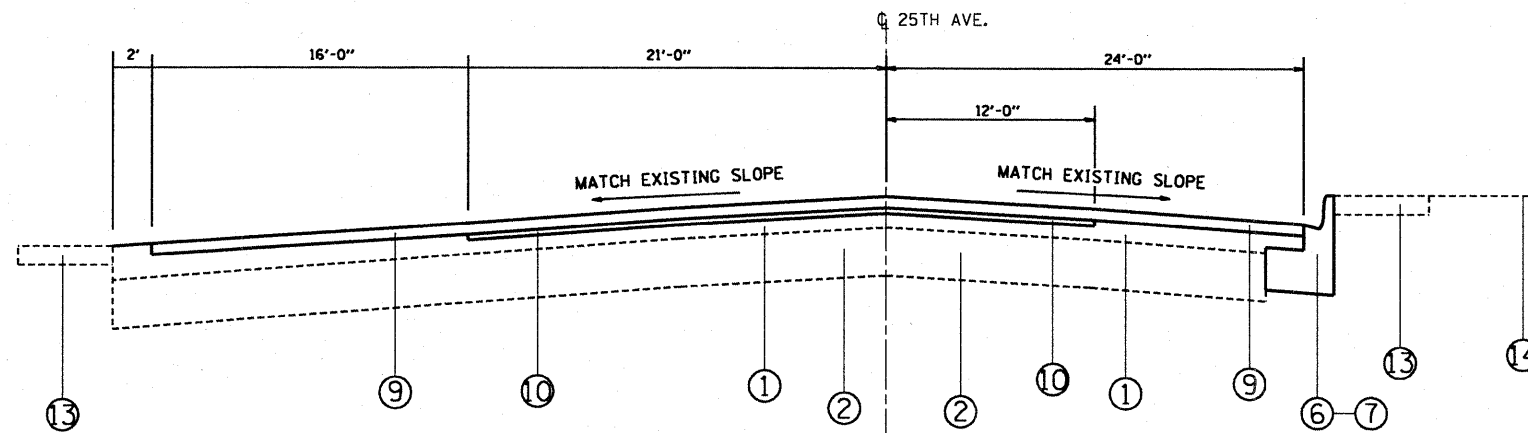
LEGEND

- ① EXISTING HOT-MIX ASPHALT RESURFACING, 3/4" (±)
- ② EXISTING PCC BASE COURSE, 9" (±)
- ③ EXISTING AGGREGATE SHOULDER, TYPE B
- ④ SUB BASE GRANULAR MATERIAL, TYPE A, 6"
- ⑤ EXISTING HMA WIDENING
- ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER
- ⑦ PROPOSED COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑬ EXISTING PCC SIDEWALK
- ⑭ EXISTING SODDING
- ⑮ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 1 1/2"

NOTE:
HMA OVERLAYS GUTER AT VARIOUS SECTIONS

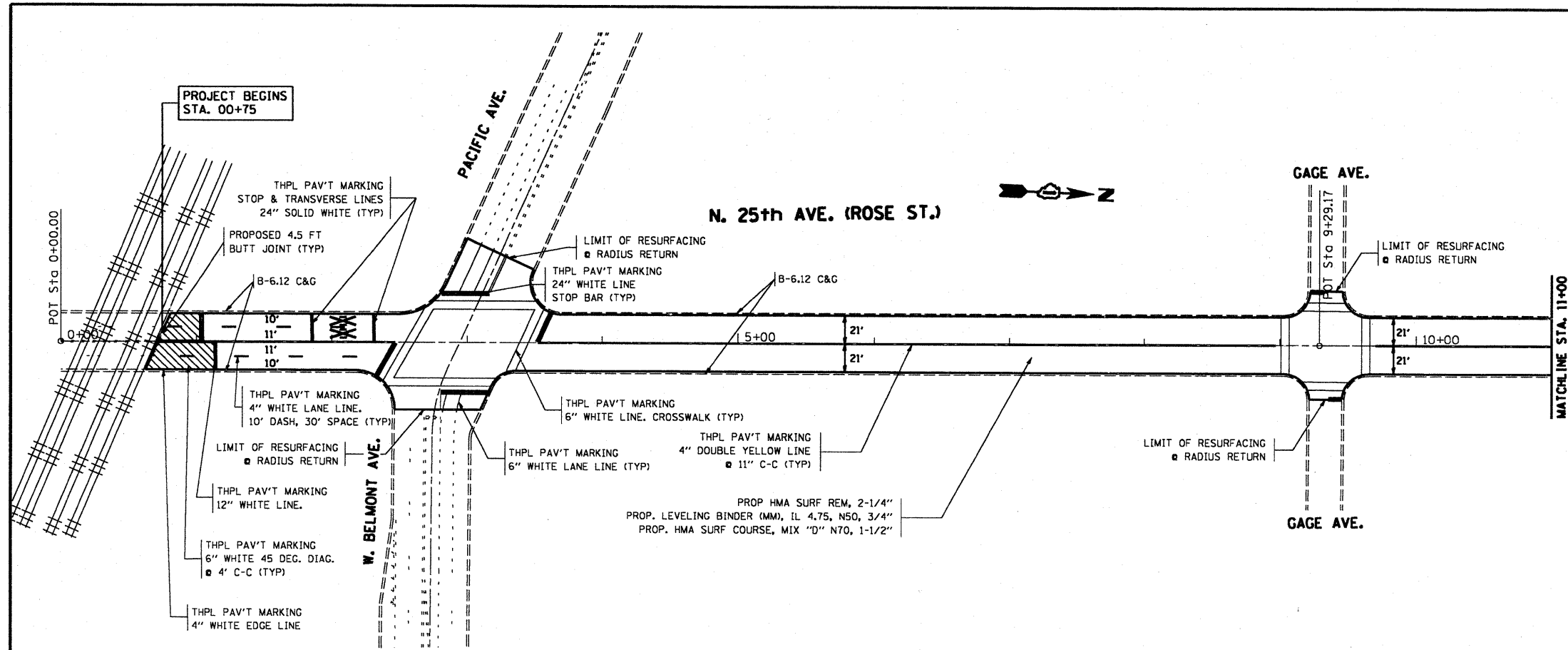


EXISTING TYPICAL SECTION
STATION:
25+22 TO 29+75



PROPOSED TYPICAL SECTION
STATION:
25+22 TO 29+75

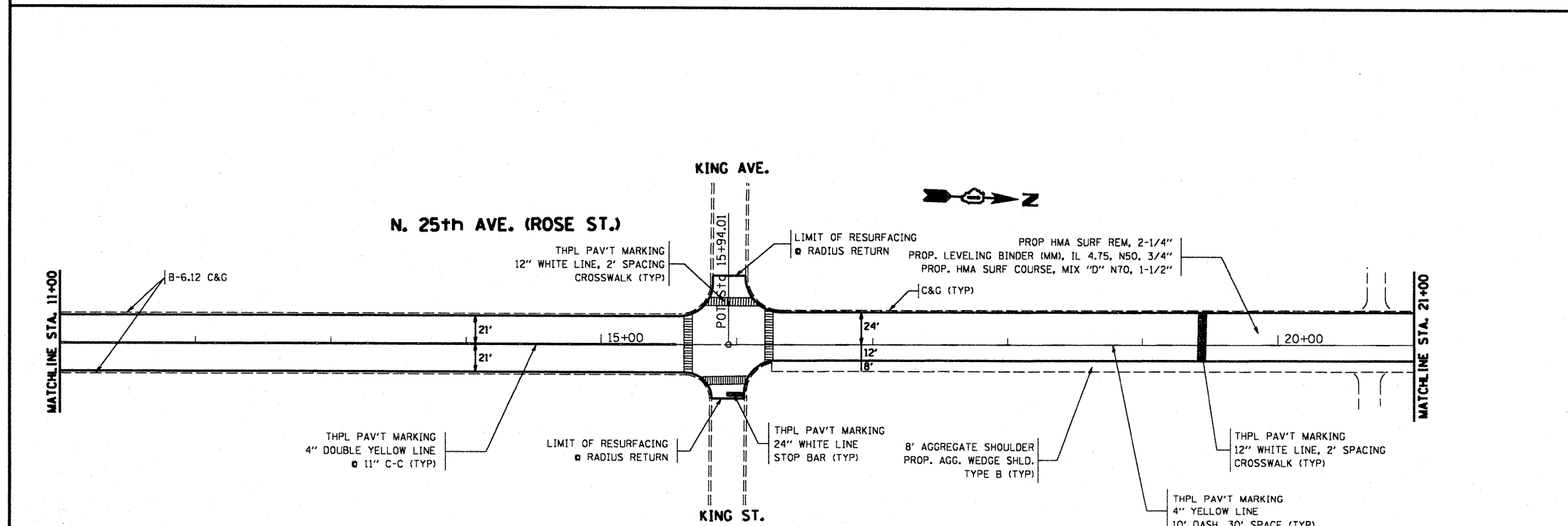
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ci:\pw_work\PWIDOT\AQUEELFF\0120745\0124397-shs-plan.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 6 OF 23 SHEETS	STA. 25+22 TO STA. 29+75	2714	(1213&3198)RS-4	COOK	23	6
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -								CONTRACT NO. 60356	
PLOT DATE = 2/11/2009		DATE -	REVISED -								ILLINOIS FED. AID PROJECT	



NOTE:
FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PROOF RESISTANT) AND DISTRICT TYPICAL PAVEMENT MARKINGS

NOTE: SIDESTREETS
-RESURFACING LIMITS ARE TO RADIUS RETURNS WITH 4.5' BUTT JOINTS

- PROPOSED HMA SURFACE REMOVAL, 2/4"
-PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, NSO, 3/4"
-PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N70, 1/2"

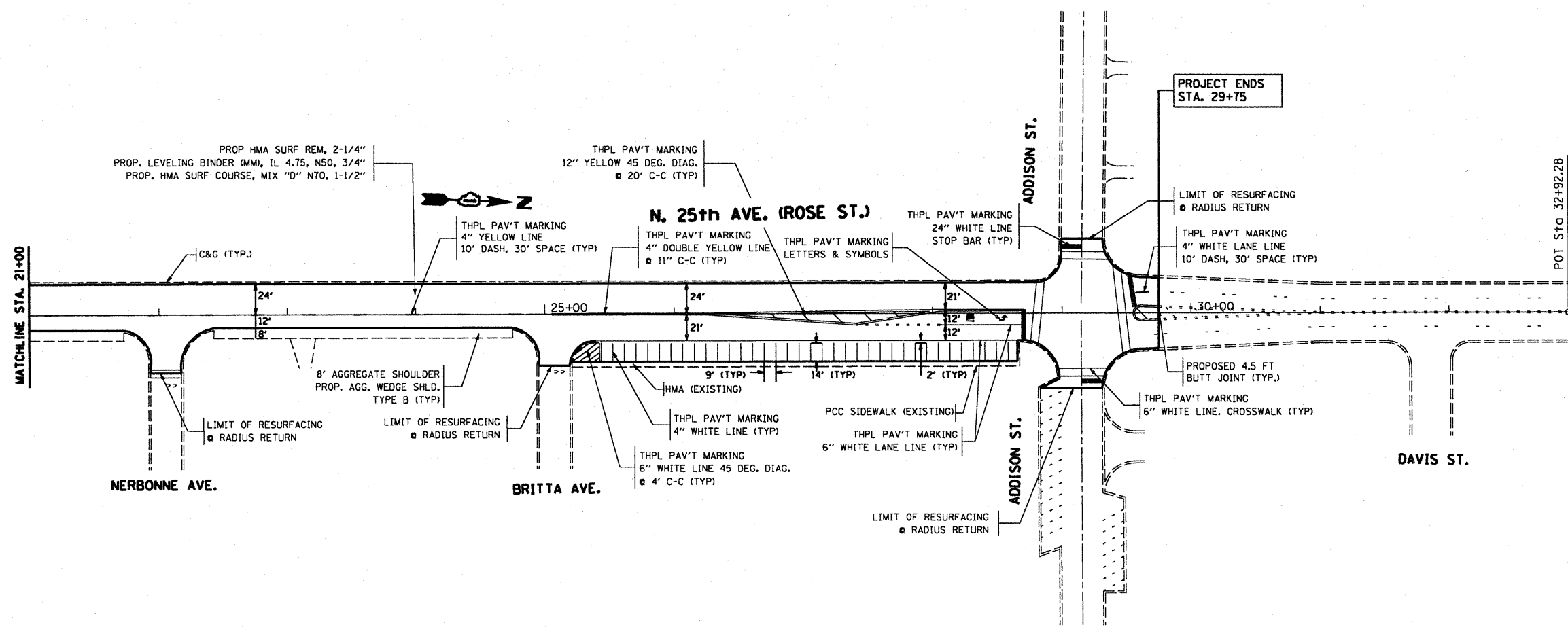


NOTE:
FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PROOF RESISTANT) AND DISTRICT TYPICAL PAVEMENT MARKINGS

NOTE: SIDESTREETS
-RESURFACING LIMITS ARE TO RADIUS RETURNS WITH 4.5' BUTT JOINTS

- PROPOSED HMA SURFACE REMOVAL, 2/4"
-PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, NSO, 3/4"
-PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N70, 1/2"

FILE NAME =	USER NAME = aqeueiff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	25TH AVENUE ROADWAY PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 2/11/2009	CHECKED -	REVISED -			CONTRACT NO. 60356					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 1"=50'	SHEET NO. 7 OF 23 SHEETS	STA. 0+00	TO STA. 21+00			



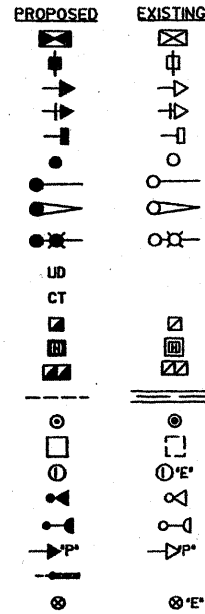
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NOTE: SIDEStreETS
 -RESURFACING LIMITS ARE TO RADIUS RETURNS WITH 4.5' BUTT JOINTS
 - PROPOSED HMA SURFACE REMOVAL, 2 1/4"
 -PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD, IL-4.75, NSO, 3/4")
 -PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", NTO, 1 1/2"

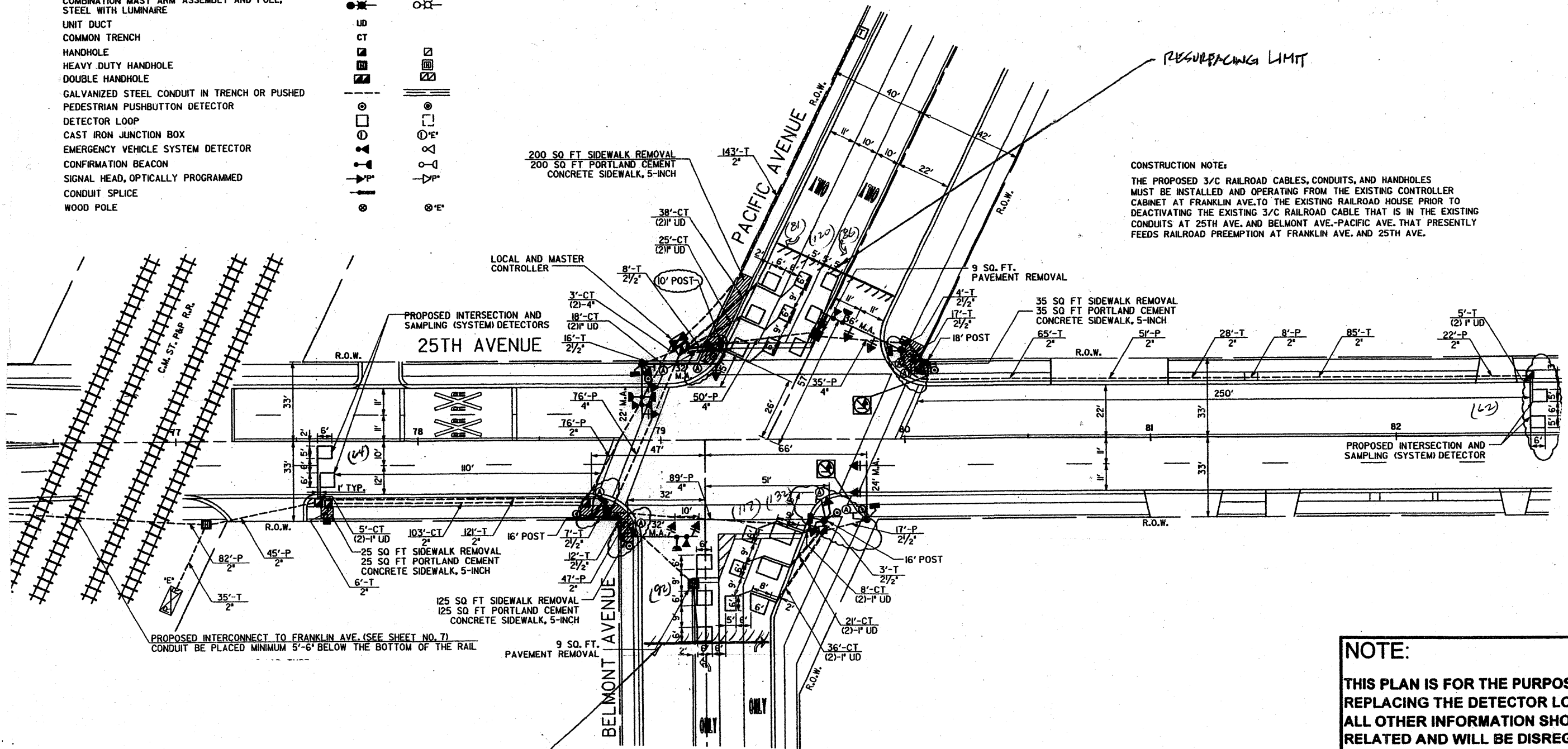
FILE NAME =	USER NAME = aqeueiff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	25TH AVENUE ROADWAY PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pwork\pwork\DOT\ADUEELFF\0128745\0124397-shit-plan.dgn	PLOT SCALE = 100.0000' / IN.	DRAWN -	REVISED -			2714	(1213&3198B)RS-4	COOK	23	8
PLOT DATE = 2/11/2009	DATE -	CHECKED -	REVISED -			CONTRACT NO. 60356			ILLINOIS FED. AID PROJECT	
						SCALE: 1"=50'	SHEET NO. 8	OF 23 SHEETS	STA. 21+00	TO STA. 32+92

TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE
- UNIT DUCT
- COMMON TRENCH
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP
- CAST IRON JUNCTION BOX
- EMERGENCY VEHICLE SYSTEM DETECTOR
- CONFIRMATION BEACON
- SIGNAL HEAD, OPTICALLY PROGRAMMED
- CONDUIT SPLICE
- WOOD POLE



SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
27N (12131200) 85-1	COOK	23	9
STA. TO STA.			
FED. ROAD DIST NO. 7	ILLINOIS	FED. AID PROJECT	



CONSTRUCTION NOTE:
 THE PROPOSED 3/C RAILROAD CABLES, CONDUITS, AND HANDHOLES MUST BE INSTALLED AND OPERATING FROM THE EXISTING CONTROLLER CABINET AT FRANKLIN AVE. TO THE EXISTING RAILROAD HOUSE PRIOR TO DEACTIVATING THE EXISTING 3/C RAILROAD CABLE THAT IS IN THE EXISTING CONDUITS AT 25TH AVE. AND BELMONT AVE.-PACIFIC AVE. THAT PRESENTLY FEEDS RAILROAD PREEMPTION AT FRANKLIN AVE. AND 25TH AVE.

PROPOSED INTERCONNECT TO FRANKLIN AVE. (SEE SHEET NO. 7)
 CONDUIT BE PLACED MINIMUM 5'-6" BELOW THE BOTTOM OF THE RAIL

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

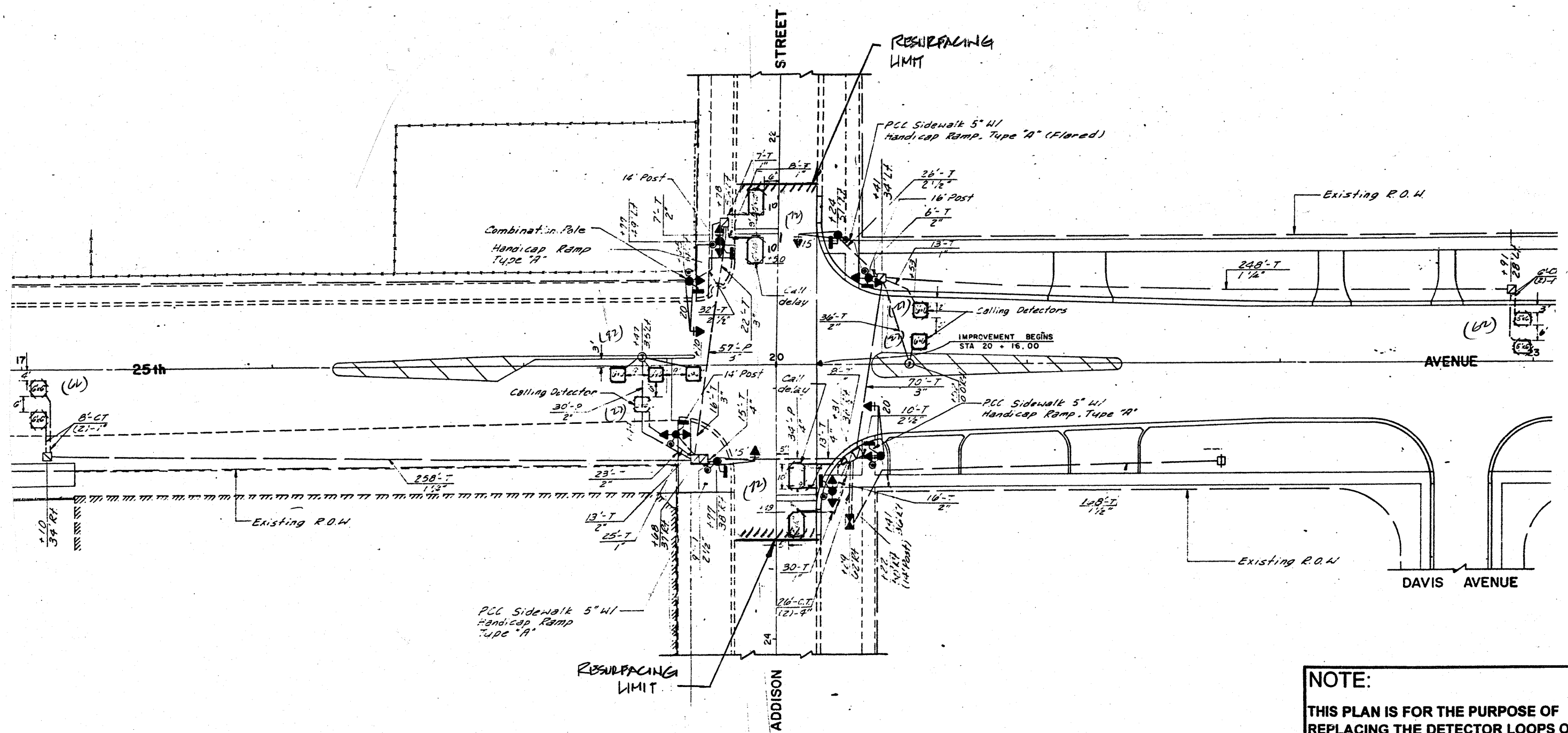
REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
80600600	754	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 25TH AVENUE @ PACIFIC/BELMONT
 SCALE: NONE
 DATE: FEB. 2009
 DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: DAD

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
214	COOK	23	10
STA.	TO STA.		
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	



REPLACE ALL DETECTOR LOOPS AS SHOWN

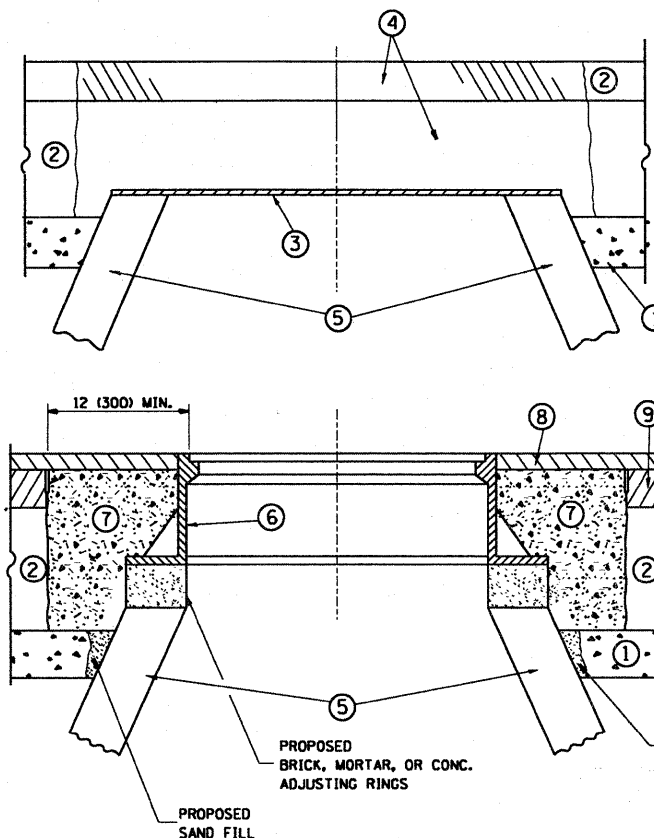
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
80500600	450	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
25TH AVENUE @ ADDISON ST.
SCALE: NONE
DATE: FEB. 2009
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

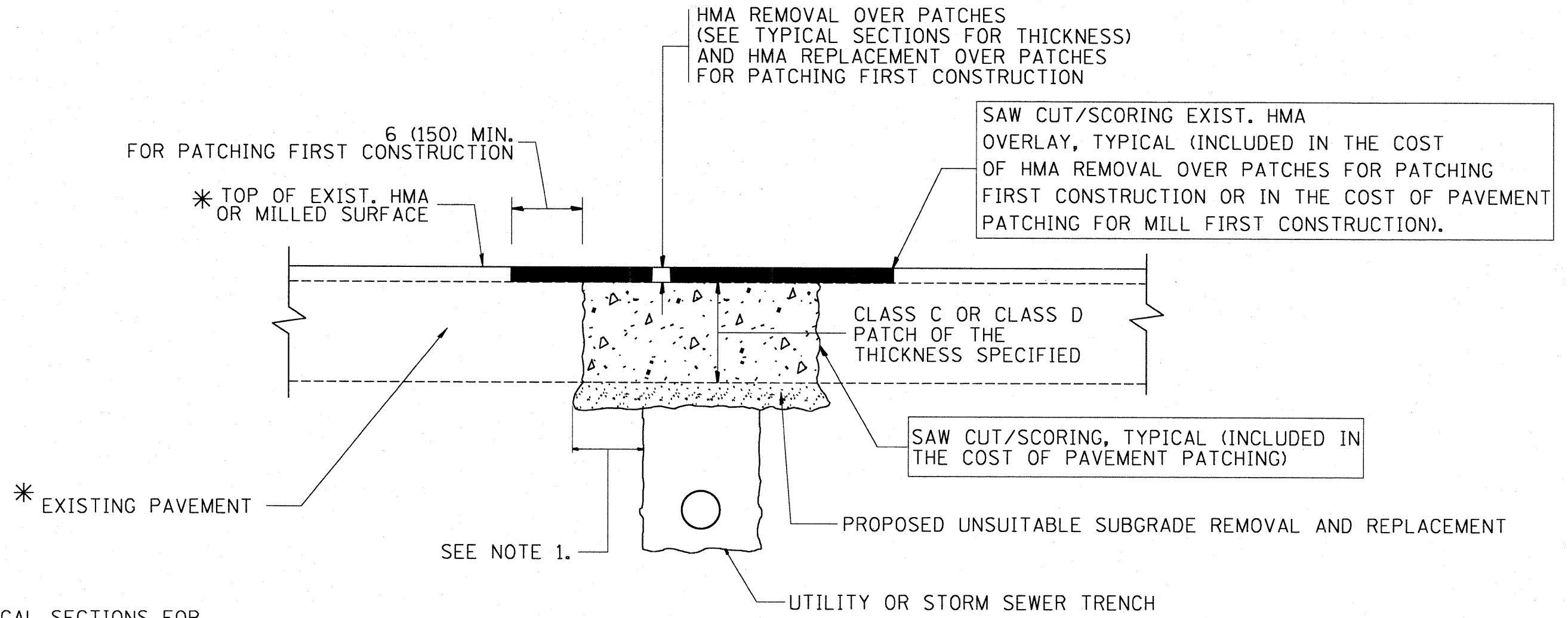
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = aqeeiff	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
ca\pw\work\PWIDDT\AQEEIFF\0120745\01sStd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000 "/ IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 2/11/2009	DATE - 10-25-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FRAMES AND LIDS ADJUSTMENT WITH MILLING		2714	(1213&3198)RS-4	COOK	23	11
SCALE: NONE		SHEET NO. 11 OF 23 SHEETS		STA.	TO STA.	
		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		CONTRACT NO. 60356		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

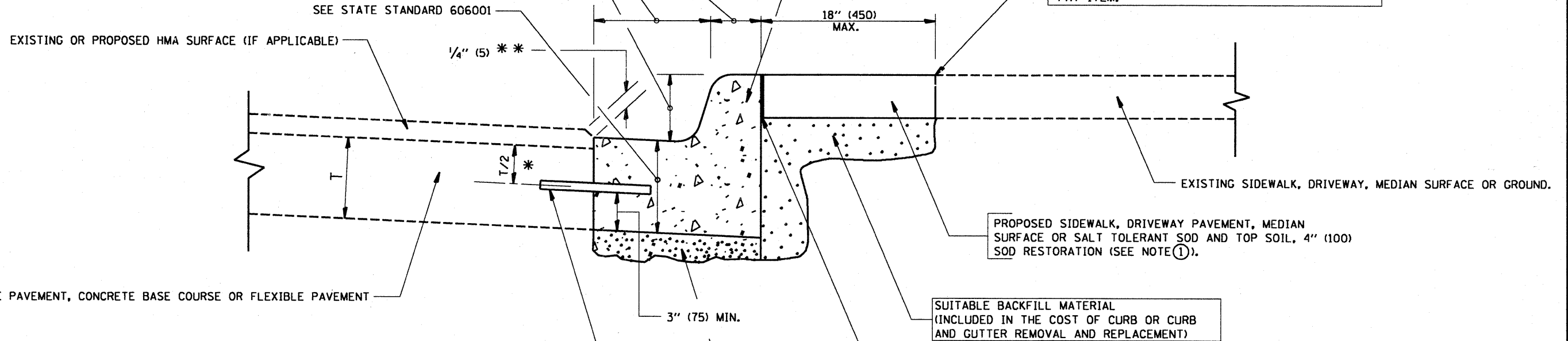
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = aqeelff	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at\pw_work\PW\DOT\AQEELFF\0120745\DotStd.dgn	DRAWN -	REVISED - R. BORO 01-01-07	REVISED - R. BORO 09-04-07			2714	(1213&3198)RS-4	COOK	23	12
PLOT SCALE = 100.0000 "/ IN.	CHECKED -	REVISED - R. BORO 09-04-07	REVISED - K. ENG 10-27-08			BD400-04 (BD-22)		CONTRACT NO. 60356		
PLOT DATE = 2/11/2009	DATE - 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE			SHEET NO. 12 OF 23 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

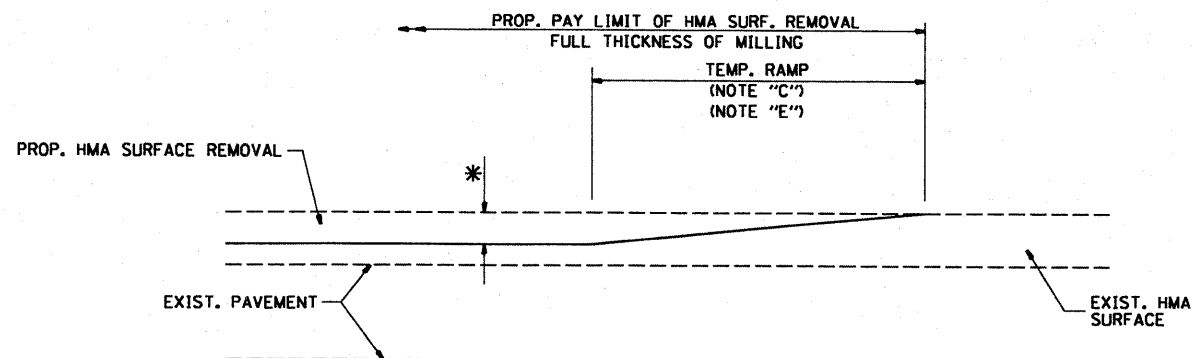
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

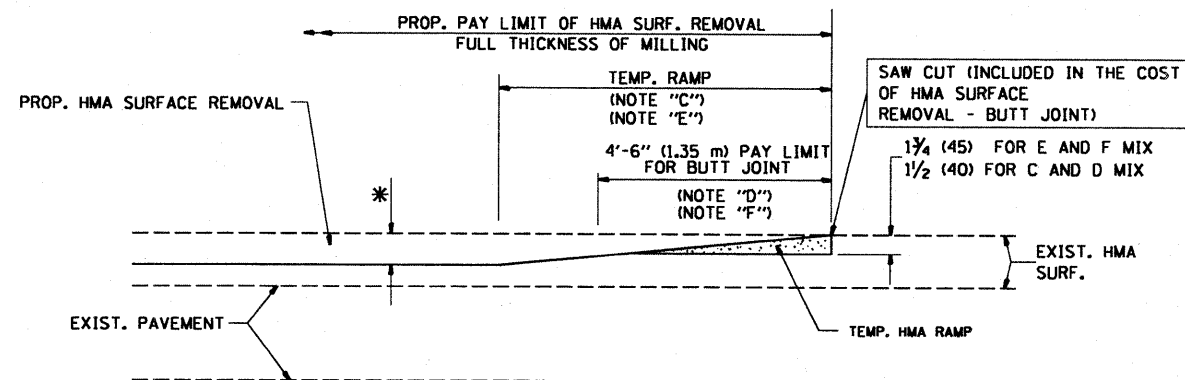
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = aqeelfff	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\PWIDOT\AQEELFF\0120745\01s	Std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97			2714	(1213&3198B)RS-4	COOK	23	13	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01				BD600-06 (BD-24) CONTRACT NO. 60356					
PLOT DATE = 2/11/2009	DATE - 03-11-94	REVISED - R. BORO 01-01-07				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
				SCALE: NONE	SHEET NO. 13 OF 23 SHEETS	STA.	TO STA.				



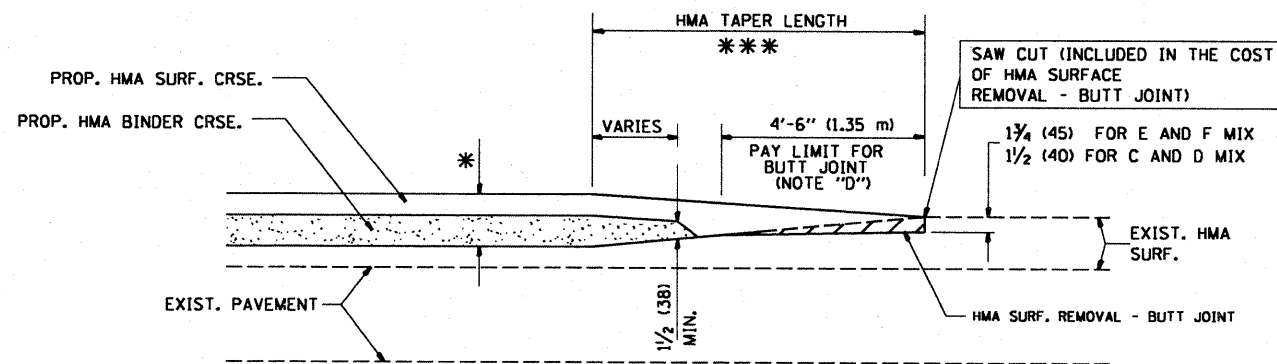
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

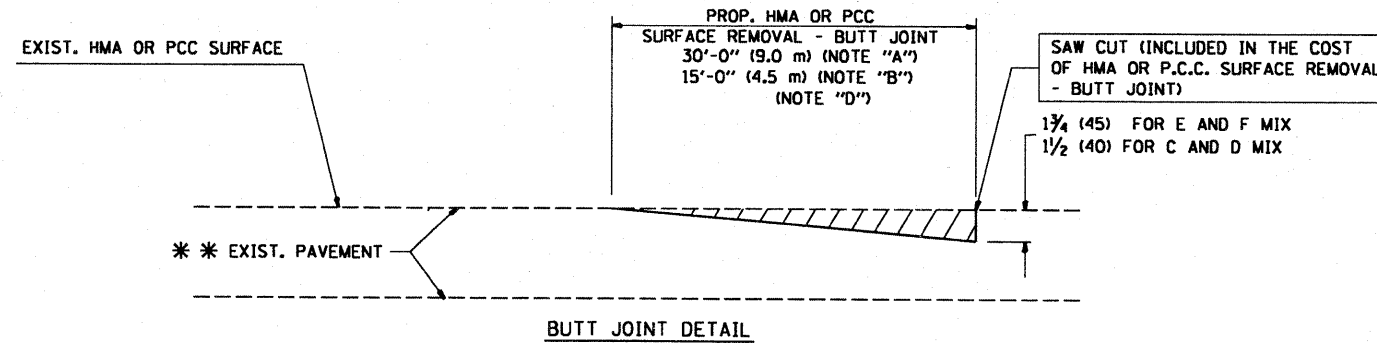


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

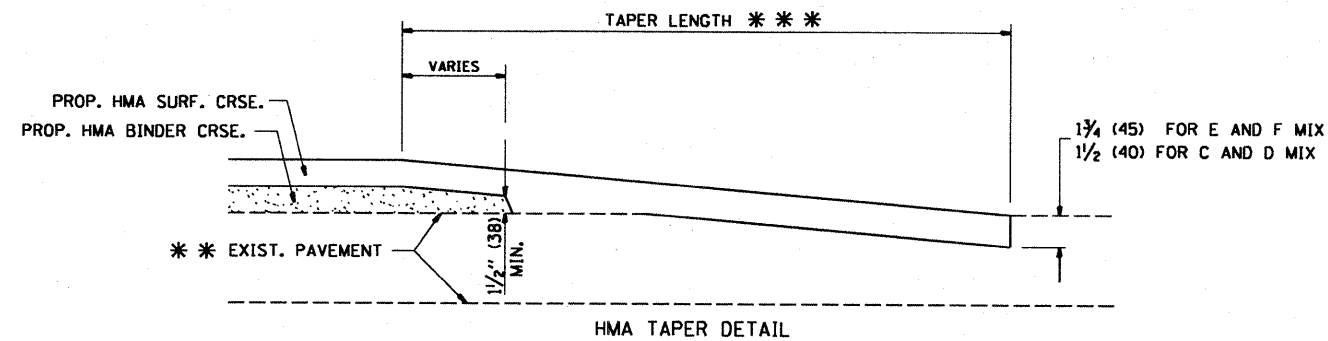
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF "HMA SURFACE REMOVAL - BUTT JOINT"
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

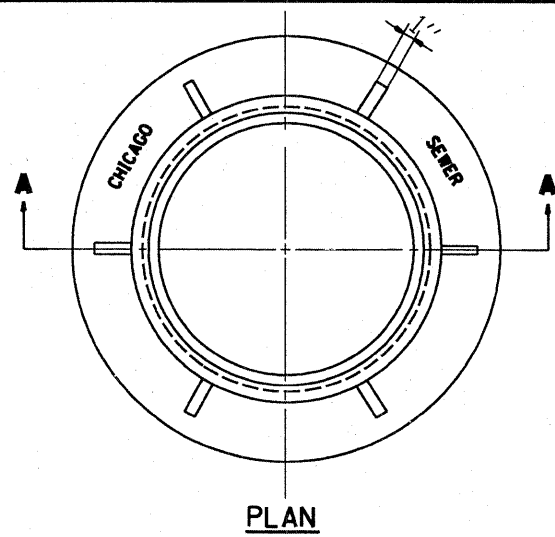
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = aqee1ff	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
c:\pwork\pwork\ADUEELFF\0120745\Std.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
		CHECKED -	REVISED - M. GOMEZ 04-06-01
		DATE - 06-13-90	REVISED - R. BORO 01-01-07

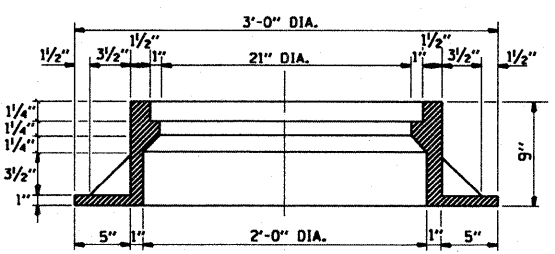
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 14 OF 23 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	(1213&3198)RS-4	COOK	23	14
BD400-05 BD32		CONTRACT NO. 60356		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



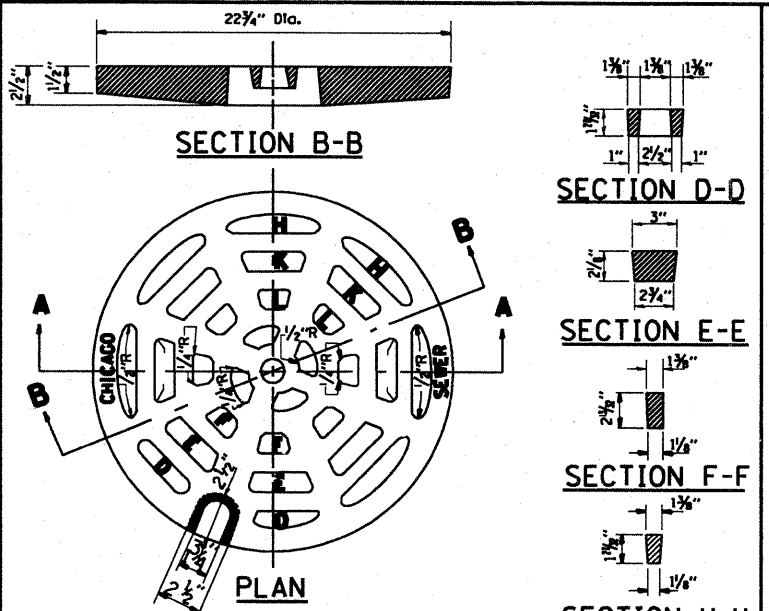
PLAN



SECTION A-A

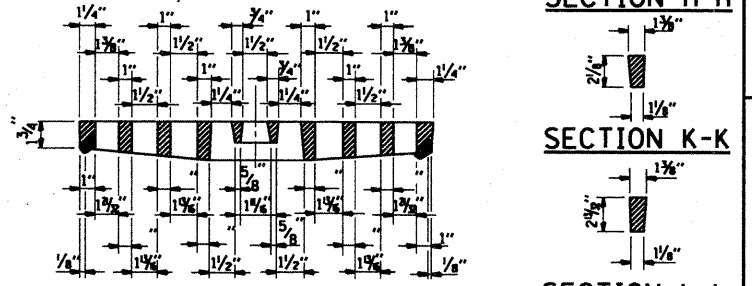
NOTE: METAL PLATES MUST BE FURNISHED FOR PERFORATED LIDS ON MANHOLES

CHICAGO STANDARD MANHOLE FRAME
SCALE: 1/2"=1'-0"
MATERIAL: CAST IRON



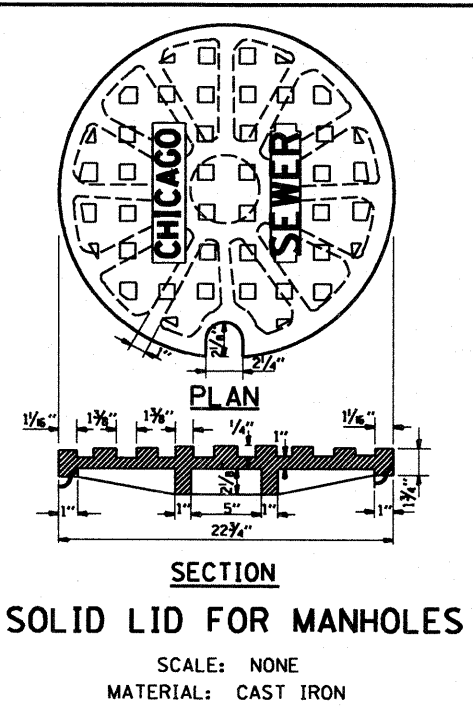
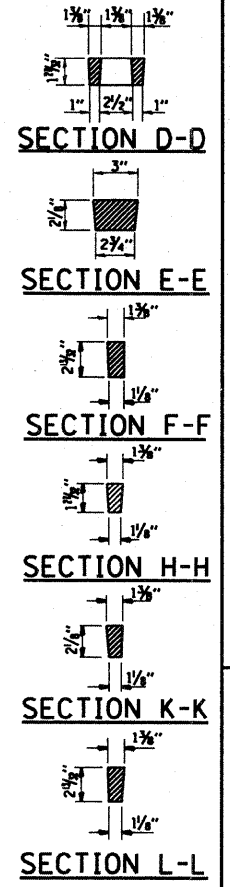
SECTION B-B

PLAN



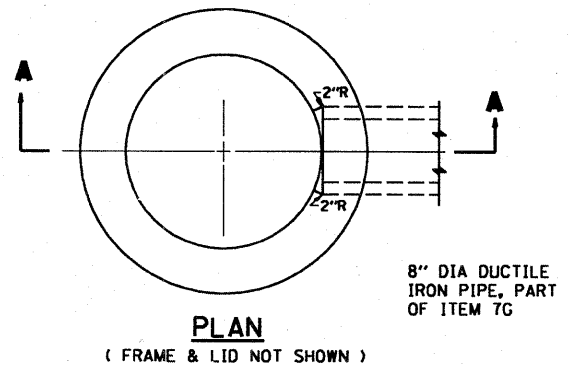
SECTION A-A

PERFORATED LID FOR CATCH BASINS & MANHOLES
SCALE: 2"=1'-0"
MATERIAL: CAST IRON



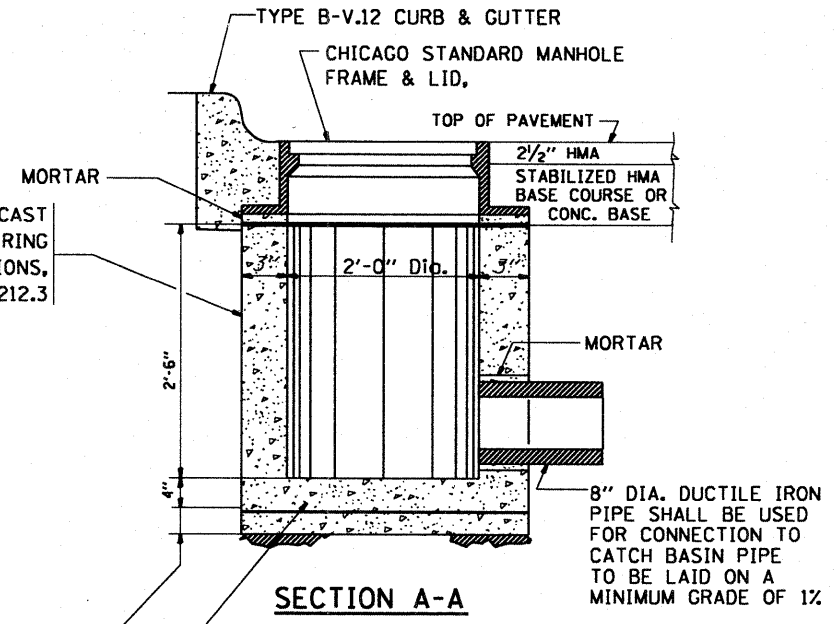
SECTION

SOLID LID FOR MANHOLES
SCALE: NONE
MATERIAL: CAST IRON



PLAN

(FRAME & LID NOT SHOWN)



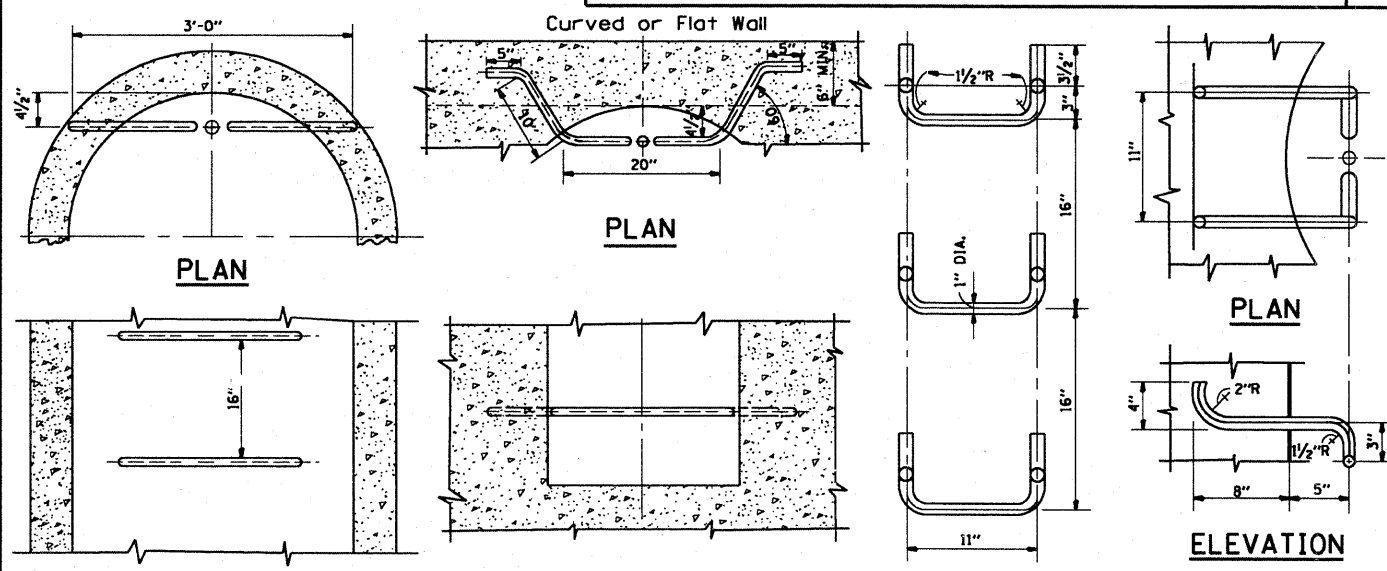
SECTION A-A

6" MINIMUM GRANULAR EMBEDMENT UNDER ALL INLETS. FURNISHING AND INSTALLING GRANULAR EMBEDMENT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 12

STANDARD INLETS
SCALE: 1"=1'-0"
ITEM 12

THIS INLET DETAIL IS SOMETIMES REFERRED TO AS "CHICAGO STANDARD INLET, TYPE A"

NOTE: INLETS SHALL NOT BE CONSTRUCTED UNLESS IT IS IMPOSSIBLE TO CONSTRUCT A CATCH BASIN. THE CONTRACTOR SHALL HAVE THE DEPARTMENT OF SEWERS APPROVAL BEFORE CONSTRUCTING INLETS.



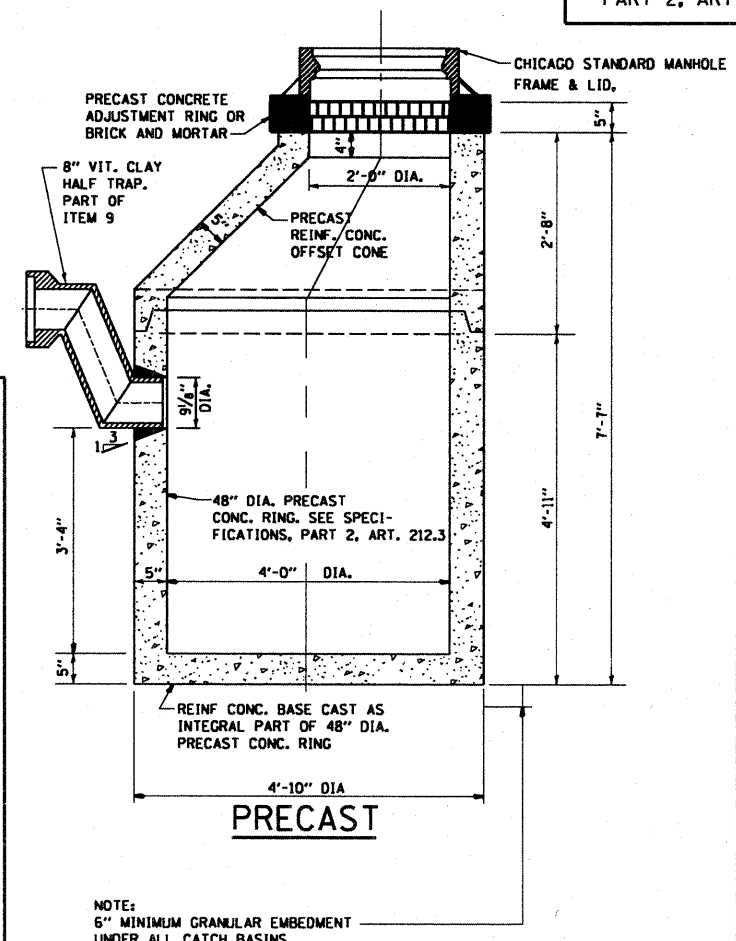
ELEVATION TYPE X
SCALE: 1"=1'-0"

ELEVATION TYPE Y
SCALE: 1"=1'-0"

SPACING HANDHOLD-TYPE Z RUNG
Scale: 1/2"=1'-0"

STANDARD LADDER RUNGS

ALL LADDER RUNGS SHALL BE ALUMINUM OR GALVANIZED WROUGHT IRON AS SPECIFIED IN THE SPECIFICATIONS, PART 2, ARTICLE 214.2. RUNGS SHALL BE 1" DIAMETER OR OF A SHAPE HAVING AN EQUIVALENT CROSS-SECTIONAL AREA.



PRECAST

NOTE: 6" MINIMUM GRANULAR EMBEDMENT UNDER ALL CATCH BASINS

STANDARD CATCH BASINS
SCALE: 3/4"=1'-0"
ITEM 9

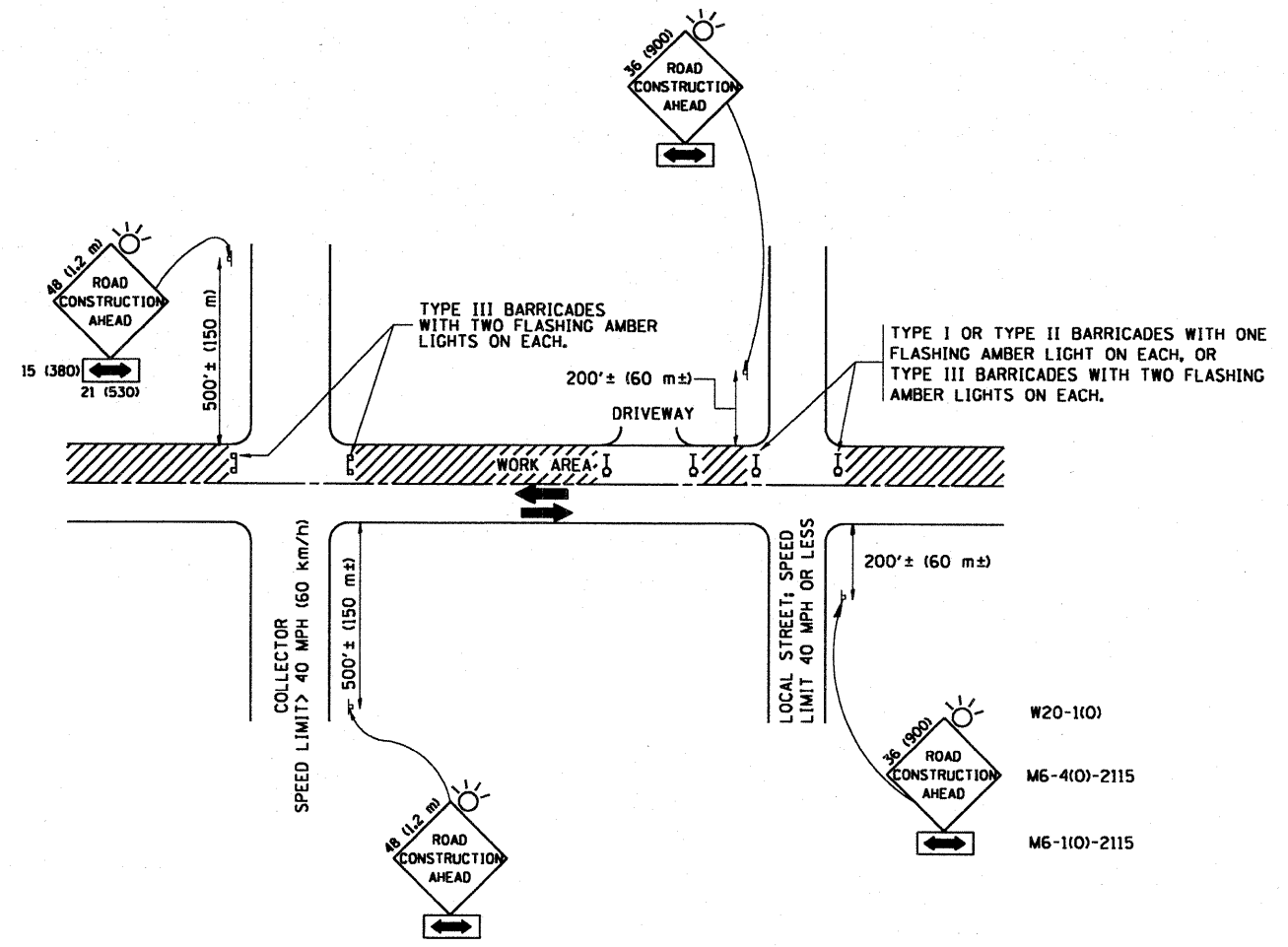
FILE NAME =	USER NAME = aequellf	DESIGNED - M. GOMEZ	REVISED -
as:\pwwork\pwwd\T\AQUEELFF\20120745\015\Std.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE - 01-25-01	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
CATCH BASIN, INLET AND MANHOLE DETAILS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	(1213&3198BRS-4	COOK	23	15
	BD600-13 (BD47)			CONTRACT NO. 60356

SCALE: NONE SHEET NO. 15 OF 23 SHEETS STA. TO STA.



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

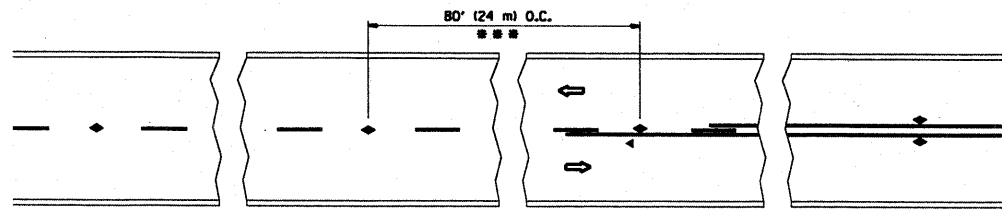
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = aqeelff	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
ca:\pwwork\pwwid\AQUEELFF\d0120745\019Std.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

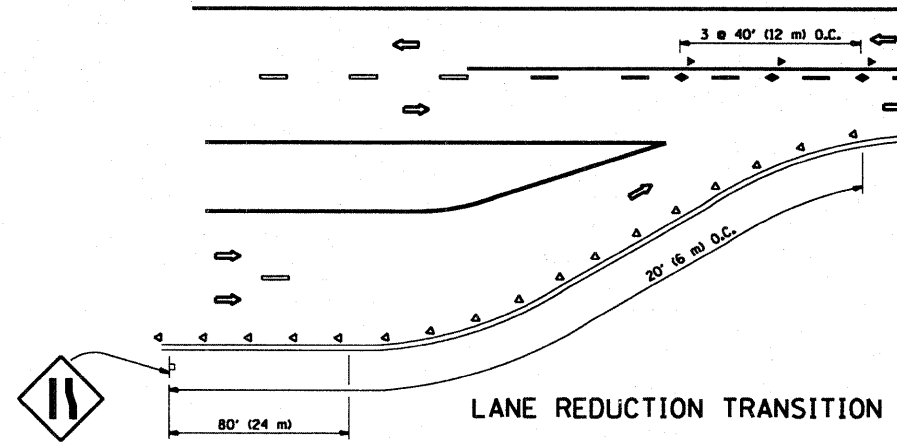
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 16 OF 3 SHEETS	2714	(1213&3198B)RS-4	COOK	23	16
STA.	TO STA.	TC-10		CONTRACT NO. 60356		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

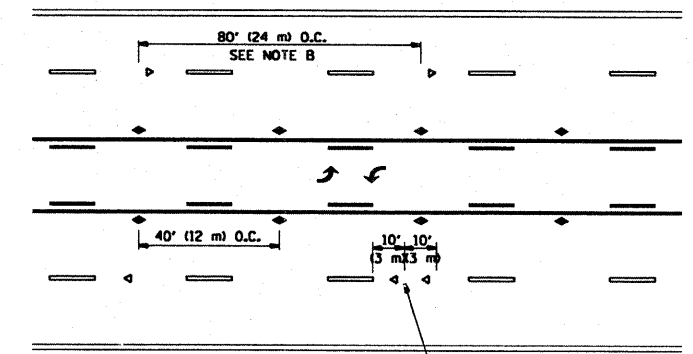


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

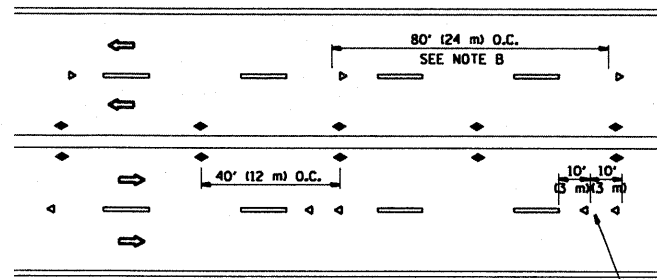


LANE REDUCTION TRANSITION



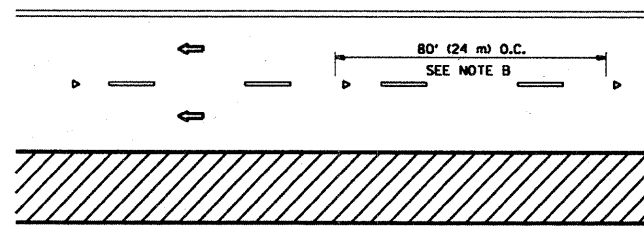
SEE NOTE A

TWO-WAY LEFT TURN



SEE NOTE A

MULTI-LANE/UNDIVIDED



SEE NOTE A

MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

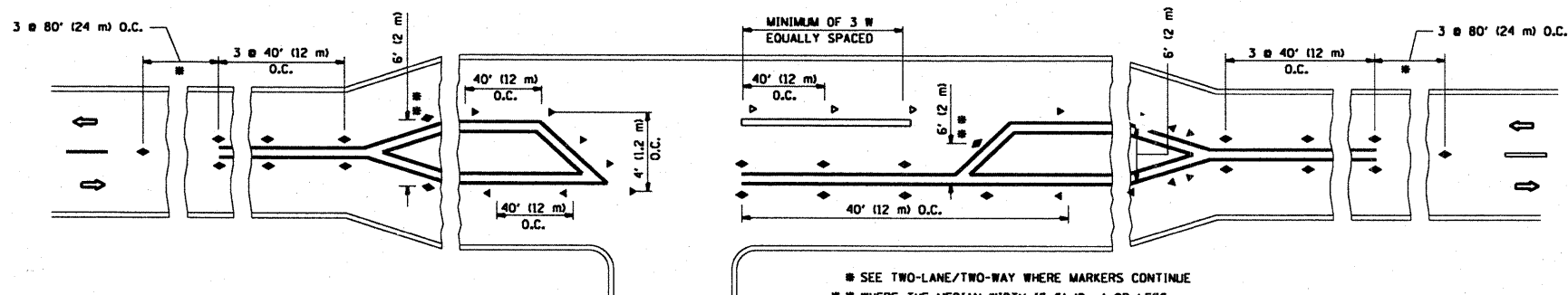
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

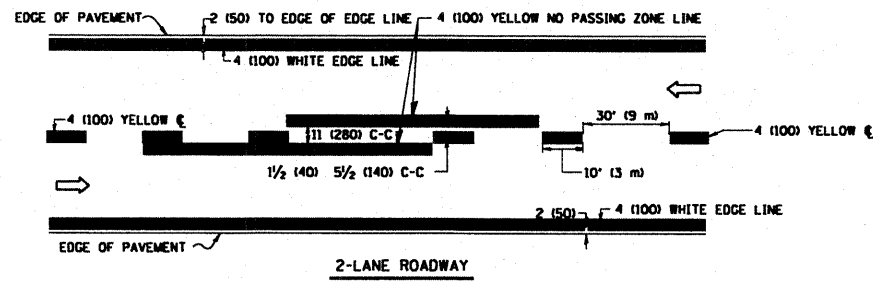
All dimensions are in Inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = equeelff	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
ca:\pw_work\PWIDOT\AQUEELFF\d0120745\Dis\Std.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 100.0000 "/ IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 2/11/2009	DATE -	REVISED -

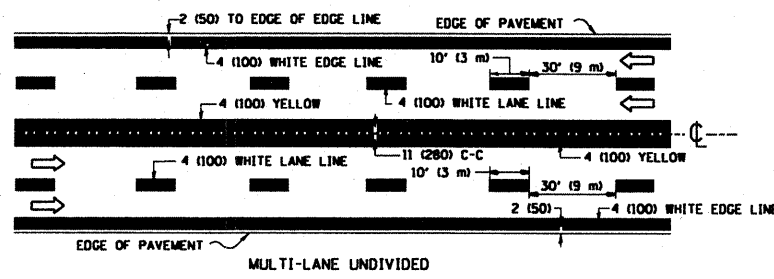
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

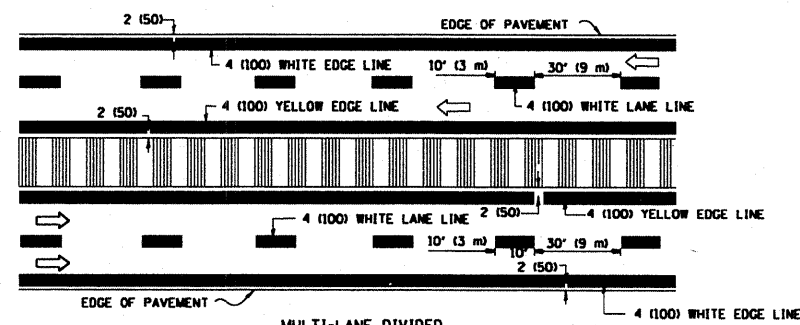
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	(1213&3198BIRS-4	COOK	23	17
TC-11			CONTRACT NO. 60356	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



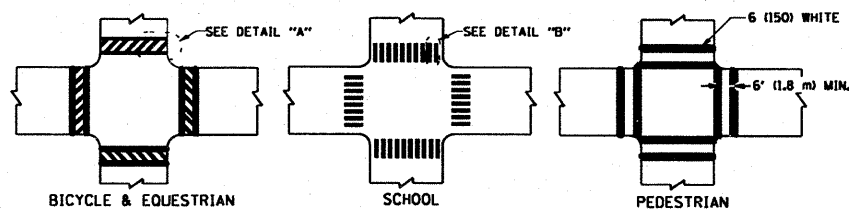
MULTI-LANE UNDIVIDED



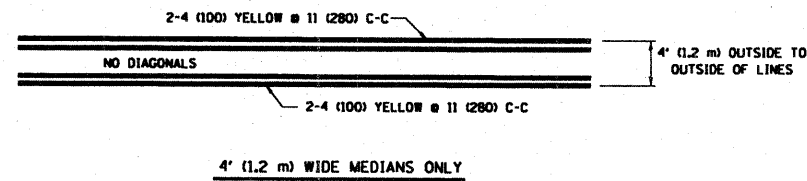
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

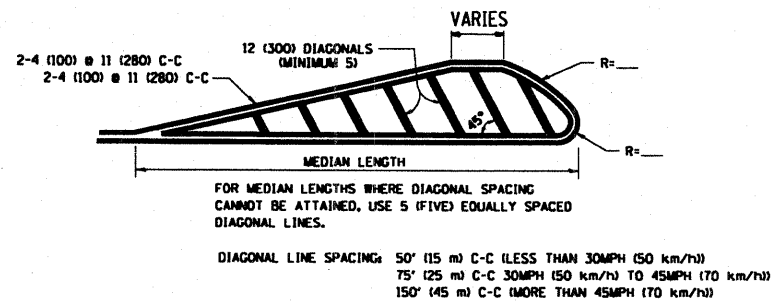
TYPICAL LANE AND EDGE LINE MARKING



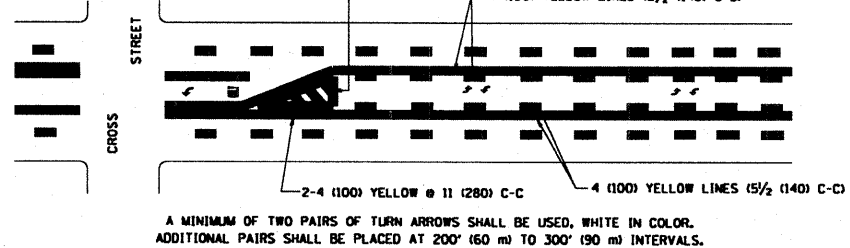
TYPICAL CROSSWALK MARKING



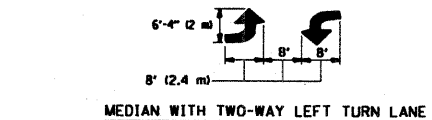
4' (1.2 m) WIDE MEDIANS ONLY



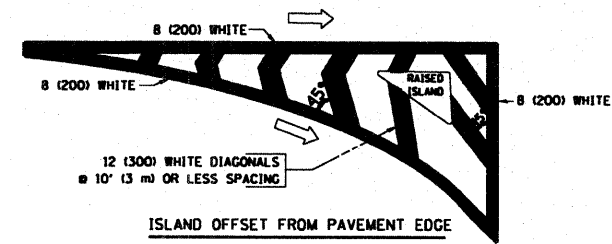
MEDIANS OVER 4' (1.2 m) WIDE



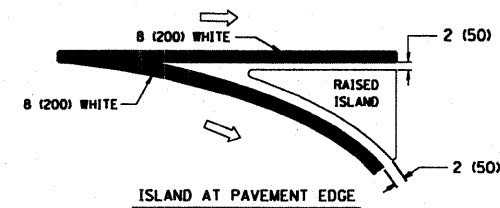
TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (22.5 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

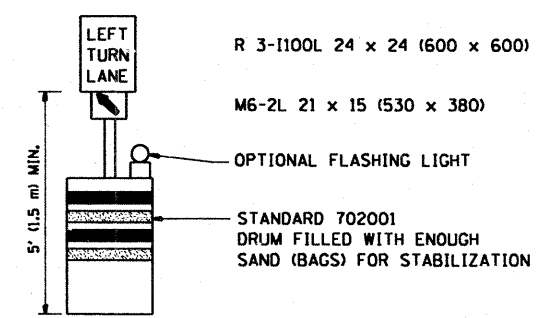
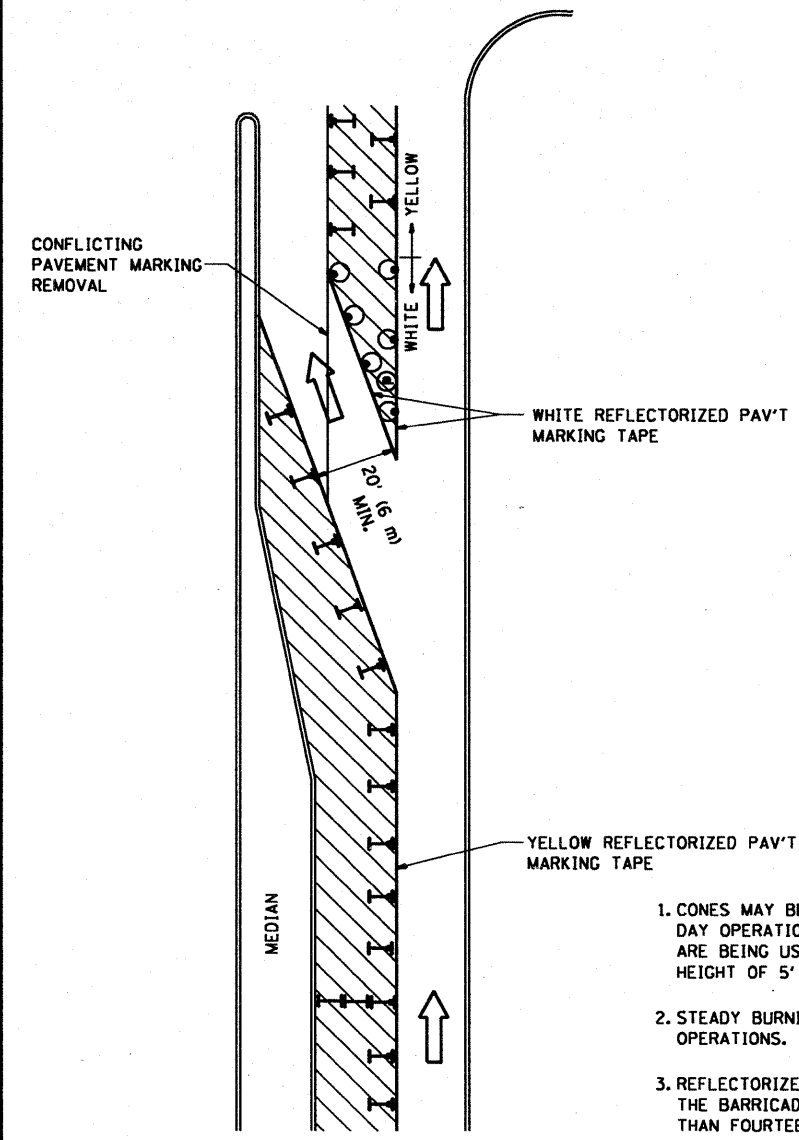
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = equaelff	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
DRAWN -	REVISOR - A. HOUSEH 10-09-96	CHECKED -	REVISOR - A. HOUSEH 10-17-96
DATE - 03-19-90	REVISOR - T. RAMMACHER 01-06-00		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 18 OF 23 SHEETS	STA. TO STA.	

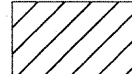





F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	(1213&3198B)RS-4	COOK	23	18
TC-13			CONTRACT NO. 60356	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

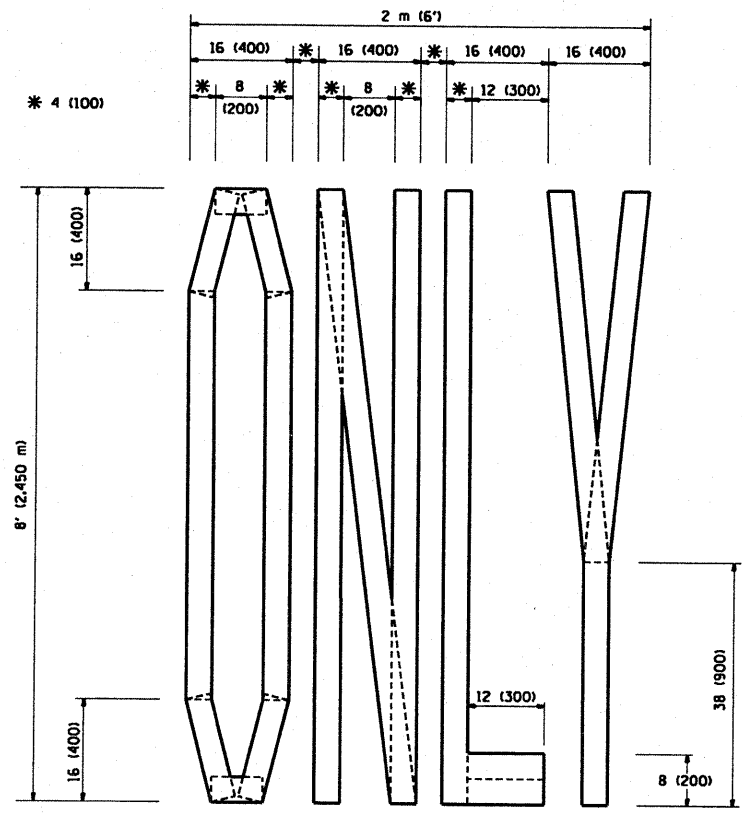
All dimensions are in inches (millimeters) unless otherwise shown.

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ca\p\work\PWIDOT\AQEELFF\0120745\DisStd.dgn		DRAWN -	REVISED - A. HOUSEH 11-07-95
	PLOT SCALE = 100.0000 "/ IN.	CHECKED -	REVISED - A. HOUSEH 10-12-96
	PLOT DATE = 2/11/2009	DATE -	REVISED - T. RAMMACHER 01-06-00

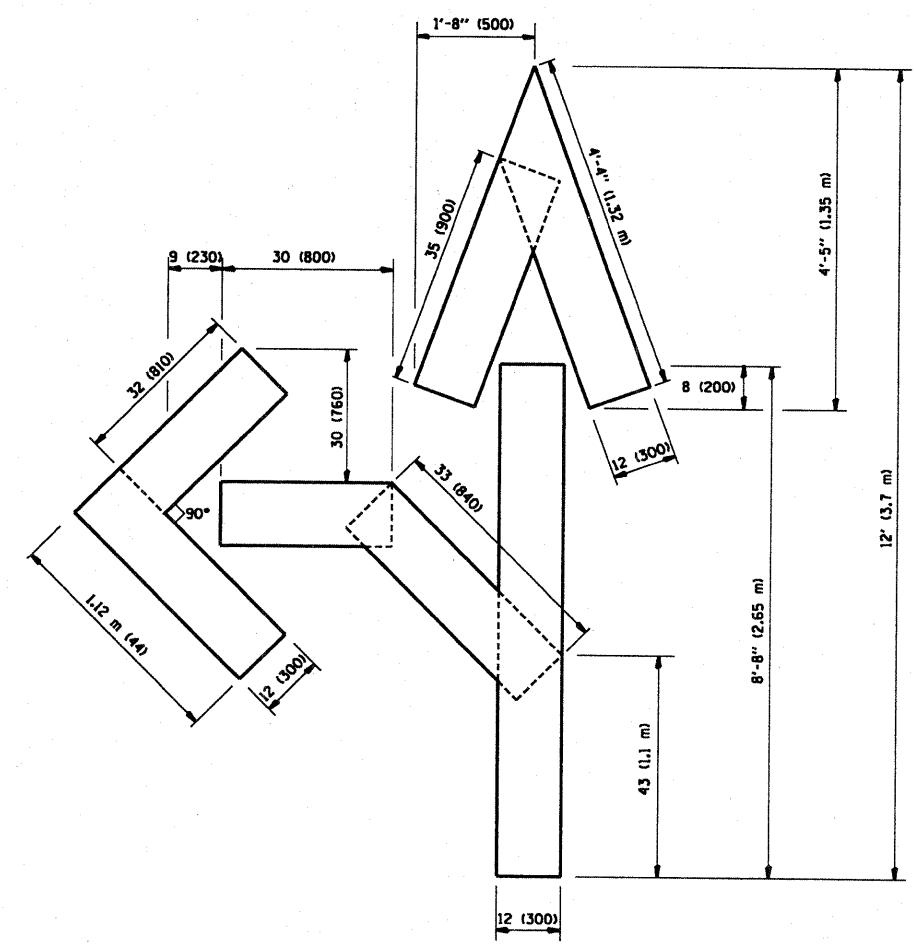
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 19 OF 23 SHEETS	STA.	TO STA.

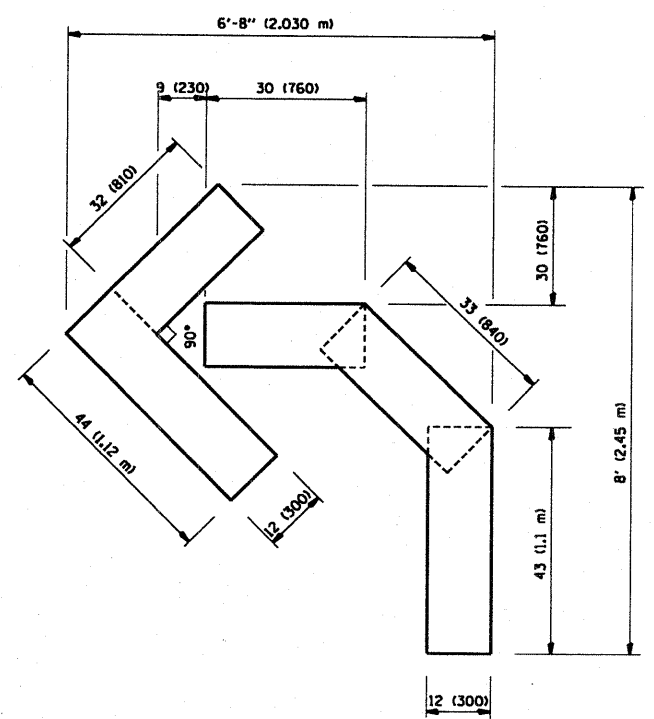
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	(121)383198BRS-4	COOK	23	19
TC-14			CONTRACT NO. 60356	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



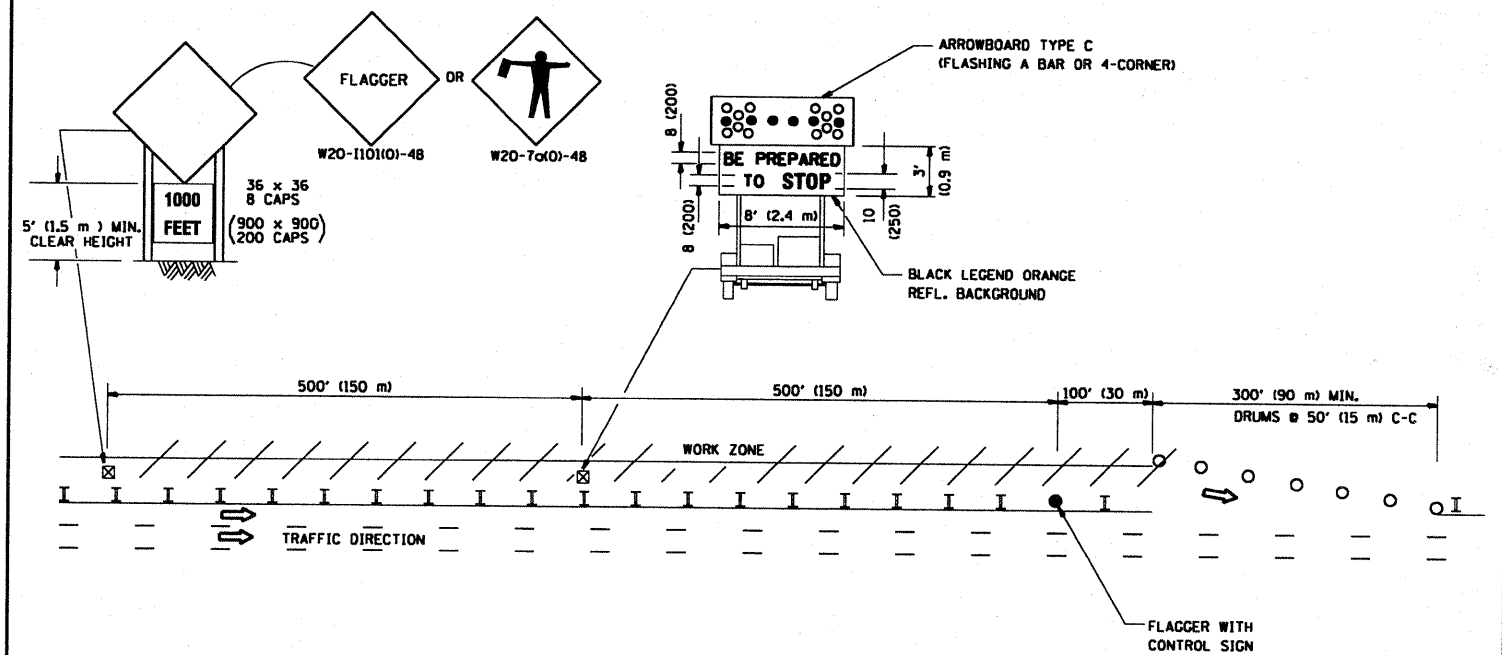
QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

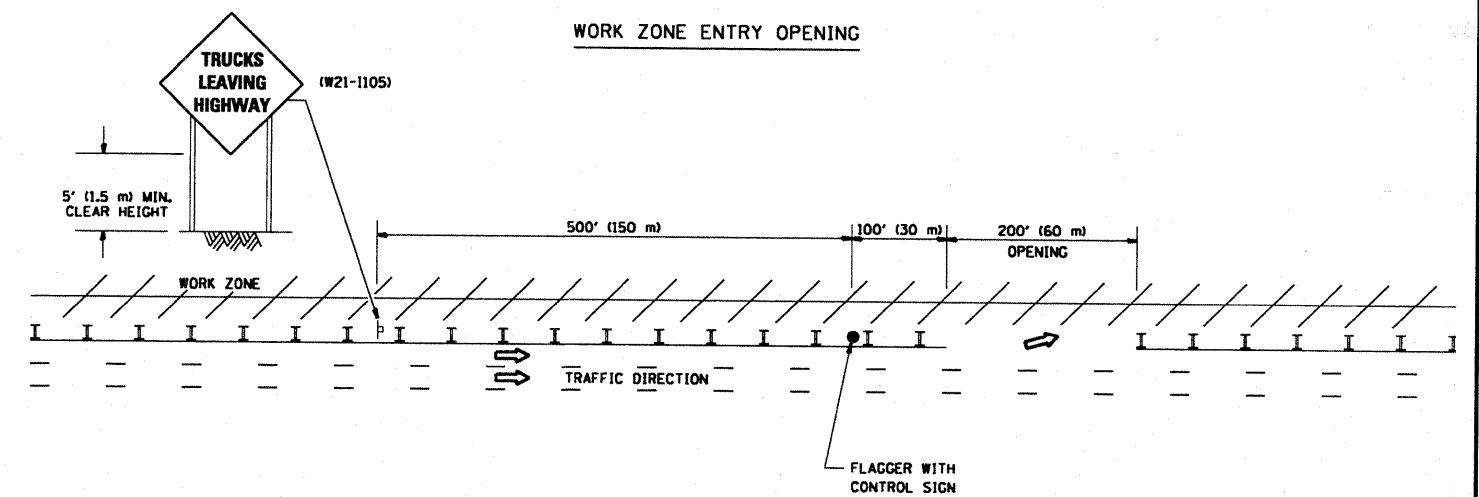
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ar\pwwork\pwwid\AQEELFF\08120745\DistStd.dgn		DRAWN -	REVISED - T. RAMMACHER 11-04-97		2714	(1213&3198)RS-4	COOK	23	20		
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED - T. RAMMACHER 03-02-98		SCALE: NONE	SHEET NO. 20 OF 23 SHEETS	STA.	TO STA.	TC-16	CONTRACT NO. 60356	
PLOT DATE = 2/11/2009		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

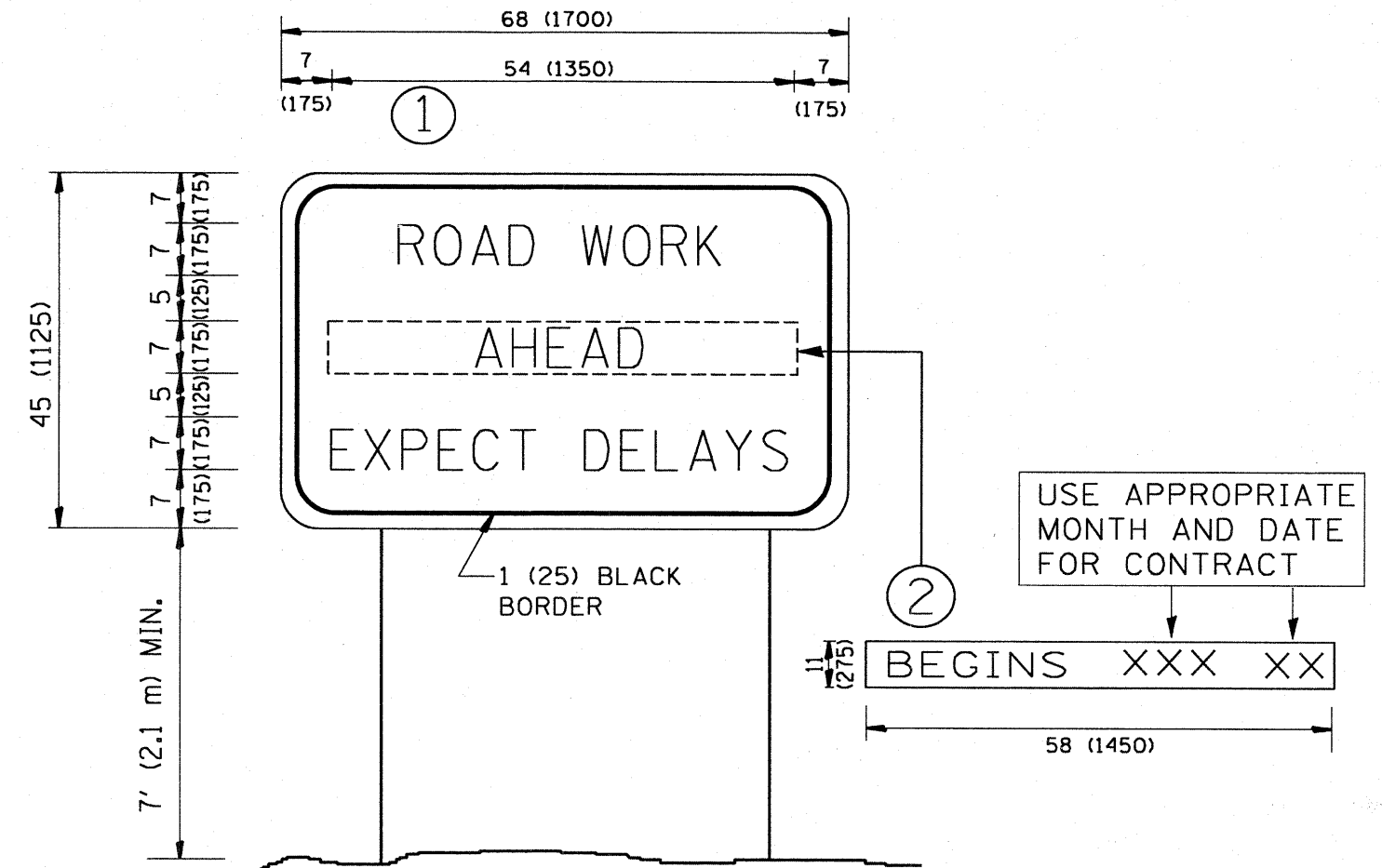
FILE NAME =	USER NAME = aquesiff	DESIGNED -	REVISED - D.W.S. 08-98
at\pw_work\PWIDOT\AQUEELFF\0120745\01	Std.dgn	DRAWN -	REVISED - J.A.F. 04-03
	PLT SCALE = 100.0000 / IN.	CHECKED -	REVISED - J.A.F. 02-06
	PLDT DATE = 2/11/2009	DATE -	REVISED - S.P.B. 01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 21 OF 23 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	(12)3&3198BIRS-4	COOK	23	21
TC-18			CONTRACT NO. 60356	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = aquee1ff	DESIGNED -	REVISED - R. MIRS 09-15-97
c:\pwork\PW100T\AQUEELFF\0120745\0120745.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
		CHECKED -	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE

SHEET NO. 22 OF 23 SHEETS

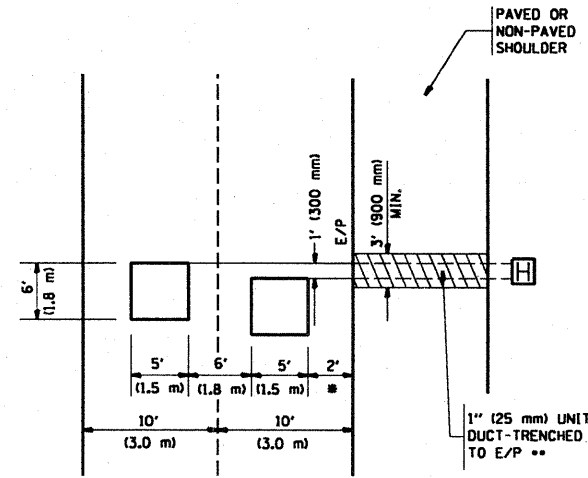
STA.

TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	(1213&3198B)RS-4	COOK	23	22
TC-22		CONTRACT NO. 60356		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

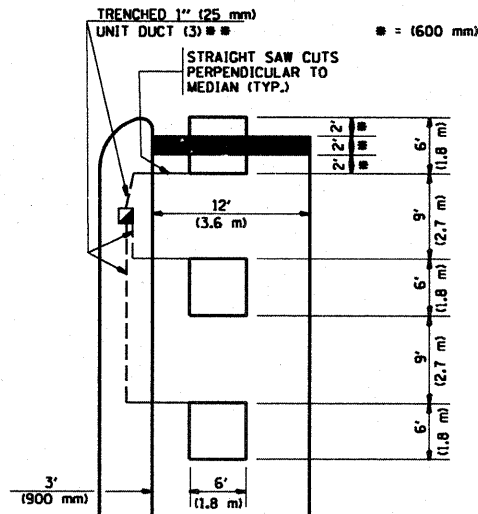


• = (600 mm)

• UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

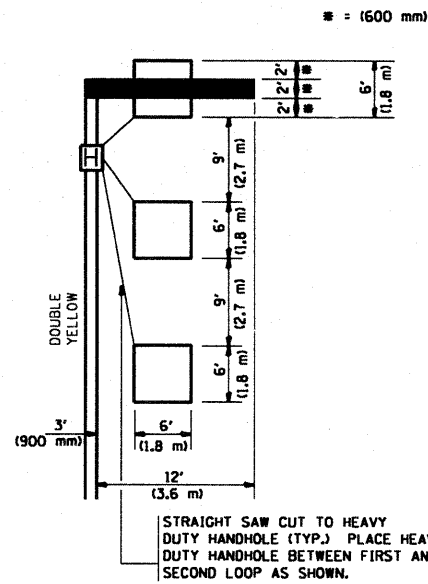
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



• UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

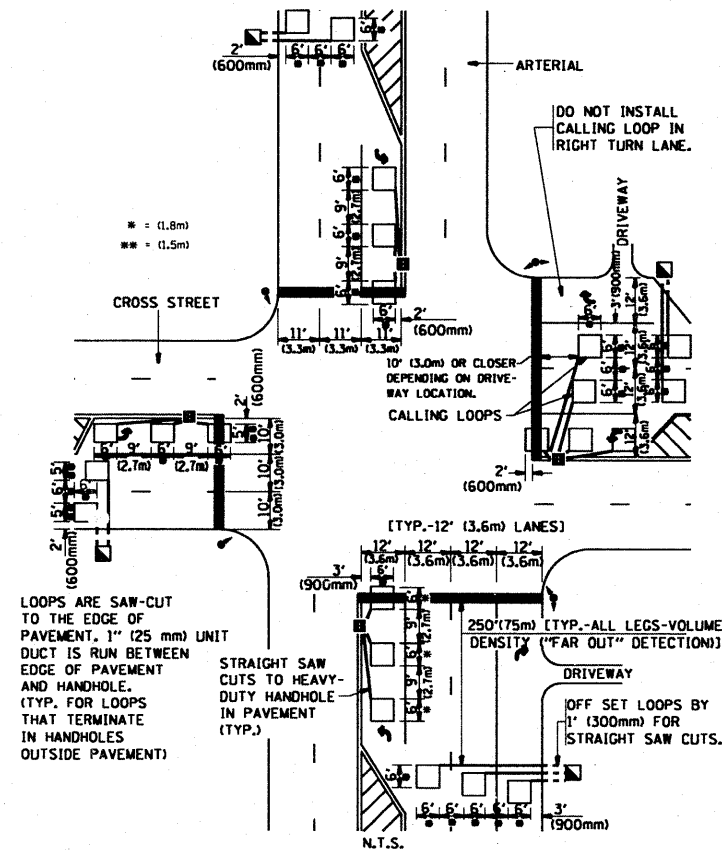
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



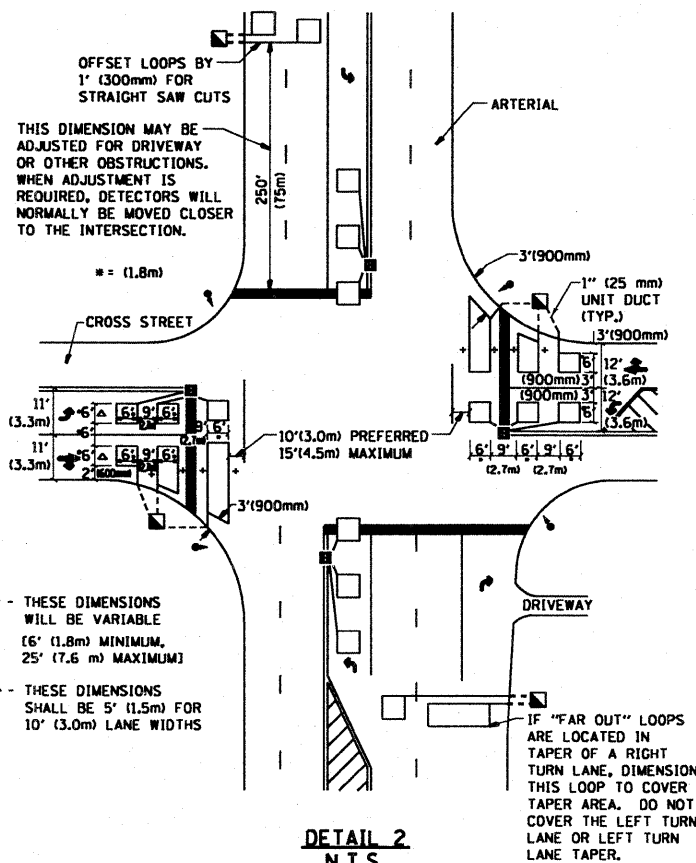
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = aqeeliff	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
α:\pwork\PW100T\AQEELIFF\0120745\0125Std.dgn		DRAWN -	REVISED -		2714	(12)3&3198BRS-4	COOK	23	23			
PLOT SCALE = 1/8" = 1' / IN.		CHECKED - R.K.F.	REVISED -		SCALE: NONE		SHEET NO. 23 OF 23 SHEETS		STA. TO STA.		CONTRACT NO. 60356	
PLOT DATE = 2/11/2009		DATE -	REVISED -		SHEET NO. 23 OF 23 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			