

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	13 D-R-1-I	WILL	16	1
FED. ROAD DIST. NO.		ILLINOIS	CONTRACT NO. 60E89	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

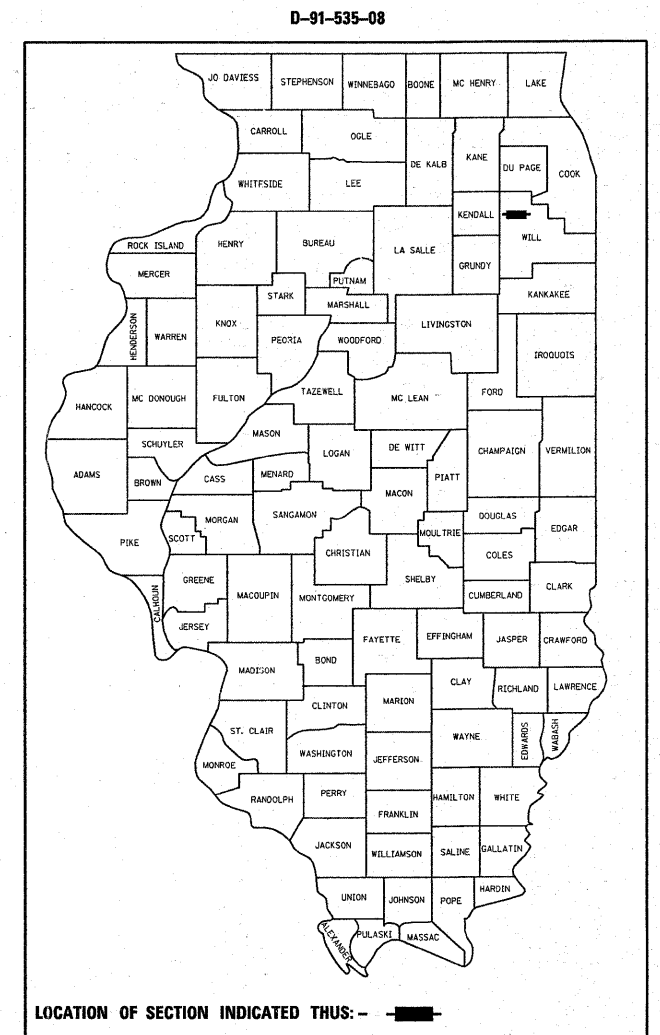
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 607: U.S. 30 (CASS STREET)
OVER EJ&E RR
SECTION 13 D-R-1-I
BRIDGE JOINT REPAIR
WILL COUNTY
C-91-535-08

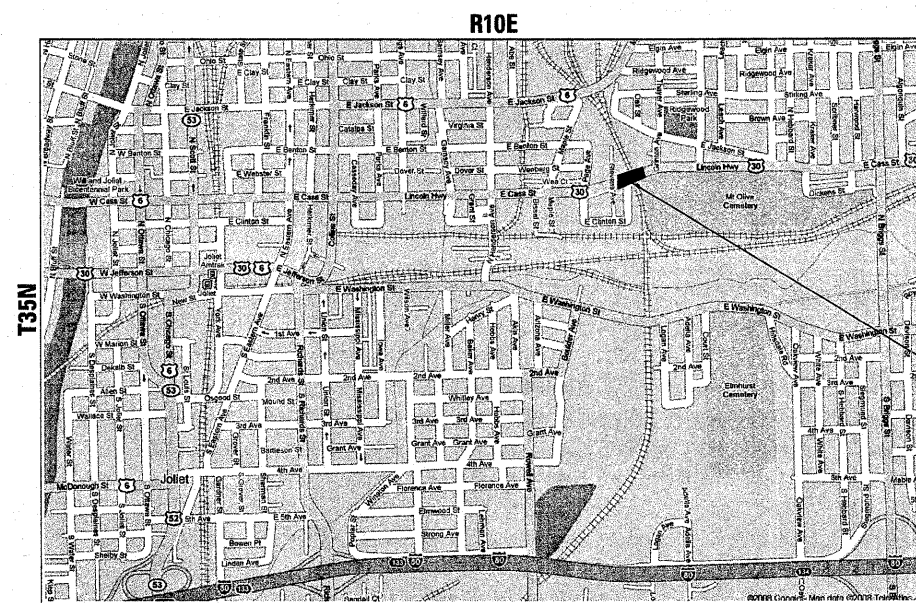
FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT IS LOCATED IN THE
CITY OF JOLIET



TRAFFIC DATA
2007 ADT = 13,600
SPEED LIMIT = 35 MPH

LOCATION OF IMPROVEMENT
SN 099-0264



JOLIET TOWNSHIP

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER ROBERT BORO (847) 705-4178
PROJECT MANAGER KEN ENG

CONTRACT NO. 60E89

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 19, 2009

Diana M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 27, 2009
Charles G. Ongersoll ENGINEER OF DESIGN AND ENVIRONMENT

March 27, 2009
Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

- 1 TITLE SHEET
- 2 INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES AND BRIDGE NOTES
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- 6-14 BRIDGE PLANS
- 15 TRAFFIC CONTROL AND PROTECTION FOR SIDEROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
- 16 DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)

STATE STANDARDS

- 701426-~~03~~ LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS \geq 45 MPH
- ~~701601-06~~ MULTILANE, LANE CLOSURE, 1W OR 2W, WITH NONTRAVERSABLE MEDIAN
- 701606-~~06~~ MULTILANE, URBAN LANE CLOSURE, 2W WITH MOUNTABLE MEDIAN
- 701801-~~04~~ LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
- 701901-~~01~~ TRAFFIC CONTROL DEVICES
- 704001-~~05~~ TEMPORARY CONCRETE BARRIER

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 or 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF JOLIET.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

THE RESIDENT ENGINEER SHALL CONTACT MS. CORA MITHIS AREA TRAFFIC FIELD ENGINEER AT (815) 485-6475 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.

BEFORE BEGINING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKINGS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470. A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINING OF WORK.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISITING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

CONCRETE SUPERSTRUCTURE SHALL HAVE A SEVEN DAY MINIMUM CURE.

BRIDGE NOTES

REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 706 GR 60 (IL MODIFIED). SEE SPECIAL PROVISIONS.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

ALL STRUCTURAL STEEL SHALL CONFORM TO AASHTO CLASSIFICATION M-270 Gr. 36, UNLESS OTHERWISE NOTED.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.

THE DECK SURFACE SHALL HAVE ITS FINAL FINISH TINED ACCORDING TO ARTICLE 420.09(a) (1) OF THE STANDARD SPECIFICATIONS. COST INCLUDED WITH CONCRETE SUPERSTRUCTURES.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

THE CONTRACTOR SHALL USE EXTREME CARE DURING CONCRETE REMOVAL SO AS NOT TO DAMAGE THE PPC I-BEAM.

JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STD. SPECS. WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50° F.

FILE NAME =	USER NAME = timokosmv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 30 OVER E.J. & E. R.R. INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES AND BRIDGE NOTES S.N. 099-0264	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pw_work\VPWIDOT\TINAKOSM\10101671\099-0264.dgn	DRAWN -	REVISED -	607			13 D-R-1-I	WILL	16	2	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60E89							
PLOT DATE = 2/19/2009	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO. OF SHEETS	STA. TO STA.				

URBAN 100% STATE

CONTRACT NO. 60E89

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	SFTY-2A				
45000130	RELIEF JOINT	FOOT	53	53				
50102400	CONCRETE REMOVAL	CU YD	32.8	32.8				
50300255	CONCRETE SUPERSTRUCTURE	CU YD	32.8	32.8				
50300300	PROTECTIVE COAT	SQ YD	5431	5431				
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	6180	6180				
50800515	BAR SPLICERS	EACH	36	36				
52000110	PREFORMED JOINT STRIP SEAL	FOOT	109	109				
52000340	NEOPRENE EXPANSION JOINT 4"	FOOT	109	109				
70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	11168	11168				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	3719	3719				
70400100	TEMPORARY CONCRETE BARRIER	FOOT	300	300				
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	275	275				
78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	1200	1200				
78100300	REPLACEMENT REFLECTORS	EACH	50	50				
78300100	PAVEMENT MARKING REMOVAL	SQ FT	388	388				
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	18	18				
Z0010605	CLEANING DRAINAGE SYSTEM	L SUM	1	1				
Z0030240	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 2	EACH	4	4				
Z0030340	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 2	EACH	4	4				
67000400	ENGINEER'S FIELD OFFICE TYPE A	CAL MO	3	3				
X0222467	TEMPORARY INFORMATION SIGNING FOR LANE CLOSURE	SQ FT	48	48				
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
67100100	MOBILIZATION	L SUM	1	1				

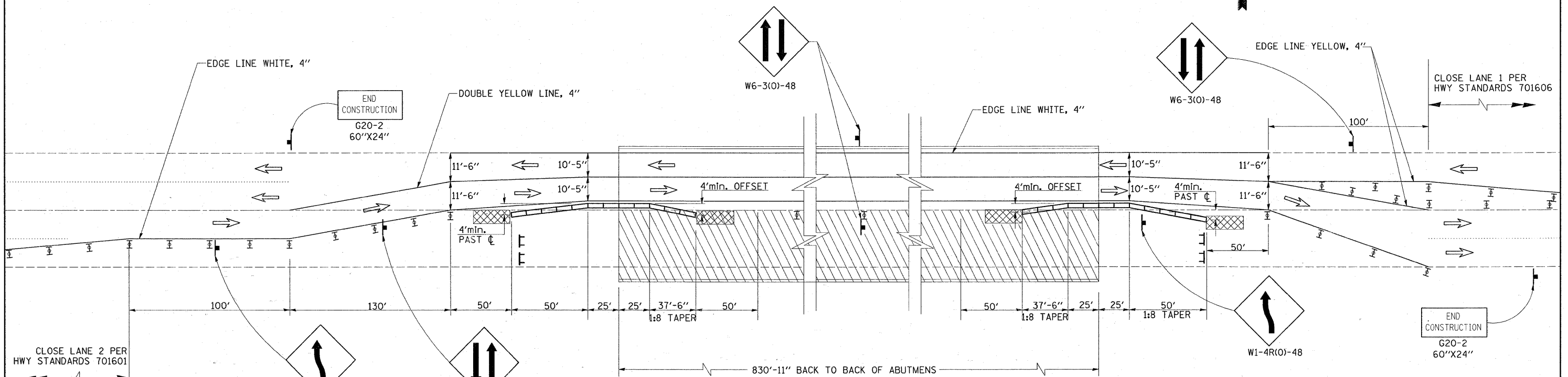
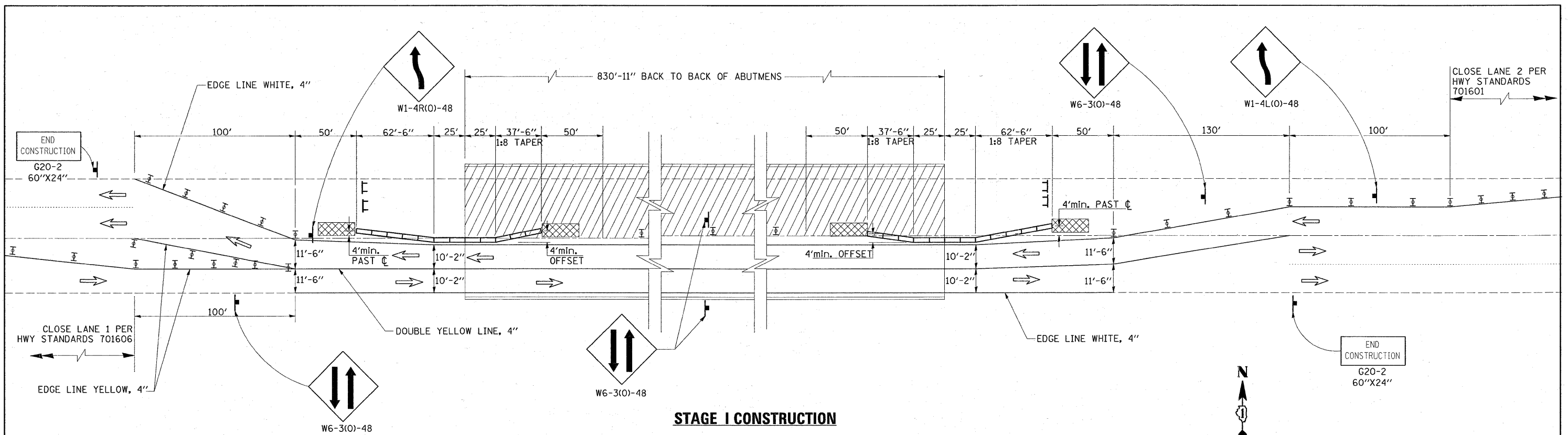
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES						

2/19/2009 2:58:40 PM C:\P\WORK\2009\11\11\AKG\SUB\0101011009-0261.dwg

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

Rev.



- LEGEND**
- 1 TYPE III BARRICADE WITH FLASHING LIGHTS
 - ▨ WORK ZONE
 - ← TRAFFIC FLOW
 - ▤ TEMPORARY CONCRETE BARRIER
 - ▩ TEMPORARY IMPACT ATTENUATOR
 - ↓ SIGN
 - TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT AT 50' CTS IN TANGENT AT 25' CTS IN TAPER AND GORE

FILE NAME =	USER NAME = tniakosmv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 30 OVER E.J. & E.R.R. TRAFFIC CONTROL PLANS S.N. 099-0264	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw_work\PIWIDOT\TINIAKOSMV\d01671\099-0264.dgn	PLOT SCALE = 60.0000' / IN.	DRAWN -	REVISED -			607	13 D-R-1-I	WILL	16	5
PLOT DATE = 2/19/2009	DATE -	CHECKED -	REVISED -			SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.		CONTRACT NO. 60E89		
						ILLINOIS FED. AID PROJECT				

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	13 D-R-1-I	WILL	16	6
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

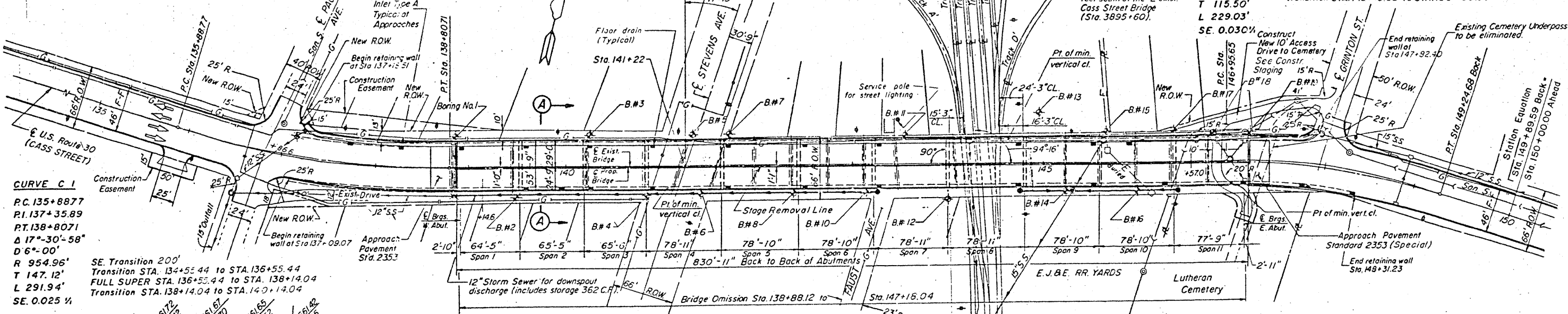
CONTRACT NO. 60E89

Description of Existing Bridge: The existing Cass Street viaduct (U.S. Route 30) over E.J.B.E. R.R. was built in 1925 as 13 span reinforced concrete T-beam and deck plate girder superstructure to carry double track loading from the Chicago and Joliet Electric Railway and two outside traffic lanes. The railway was abandoned prior to resurfacing in 1941. In 1952 the structure was widened and lengthened by adding three span continuous R.C. deck girder to the east. The substructure is supported on solid & conc. column piers, and column supported abutments with retaining walls on spread footings. The back to back abutment length is 833.52' and 51' 6" out to out deck.
 Structure No. 099-0104 Salvage: None Maintain two way traffic during stage construction.

CURVE C 2

PC. 146+95.65
 PI. 148+11.15
 PT. 149+24.68
 Δ 18°-19'-20"
 D 8°-00'-00"
 R 716.20'
 T 115.50'
 L 229.03'
 SE. 0.030%

SE. Transition 225'
 Transition STA. 145+45.65 to STA. 147+70.65
 FULL SUPER STA. 147+70.65 to STA. 148+49.68
 Transition STA. 148+49.68 to STA. 150+85.09



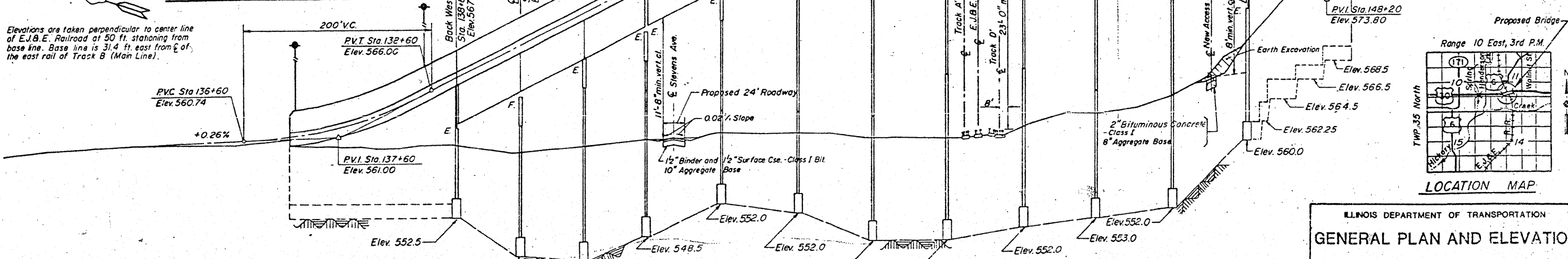
CURVE C 1

PC. 135+88.77
 PI. 137+35.89
 PT. 138+80.71
 Δ 17°-30'-58"
 D 6°-00"
 R 954.96'
 T 147.12'
 L 291.94'
 SE. 0.025%

SE. Transition 200'
 Transition STA. 134+52.44 to STA. 136+55.44
 FULL SUPER STA. 136+55.44 to STA. 138+14.04
 Transition STA. 138+14.04 to STA. 140+14.04

Track	Station	Elevation	Station	Elevation	Station	Elevation	Station	Elevation	Station	Elevation			
Track D' (Spur)	561	561.97	561.92	561.87	561.82	561.77	561.72	561.67	561.62	561.57			
	Track D	561	561.97	561.92	561.87	561.82	561.77	561.72	561.67	561.62	561.57		
		Track C	561	561.87	561.82	561.77	561.72	561.67	561.62	561.57	561.52	561.47	
			Track B (Main Line)	561	561.87	561.82	561.77	561.72	561.67	561.62	561.57	561.52	561.47
				Track A	561	561.87	561.82	561.77	561.72	561.67	561.62	561.57	561.52
Track A' (Spur)					561	561.87	561.82	561.77	561.72	561.67	561.62	561.57	561.52

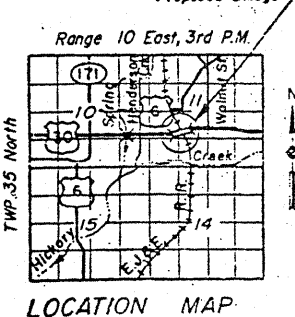
PROFILES OF TOP OF E.J.B.E. TRACKS



Notes.

Benchmark: N.E. corner concrete base of stop light at S.W. corner U.S. Rte. 30 and Walnut St. U.S.G.S Elevation 582.42

The information shown on this drawing concerning type and location of utilities is not guaranteed to be accurate or all inclusive. The contractor is responsible for making his own determinations as to the type and location of underground and other utilities as may be necessary to avoid damage thereto.

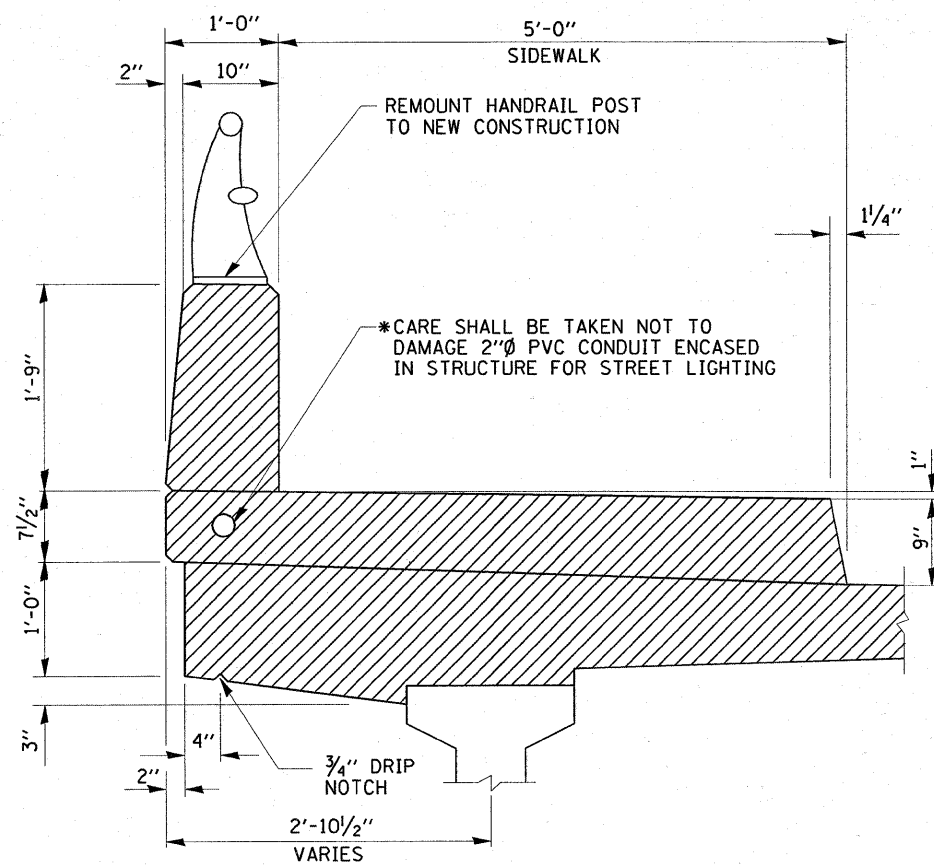


FOR INFORMATION ONLY

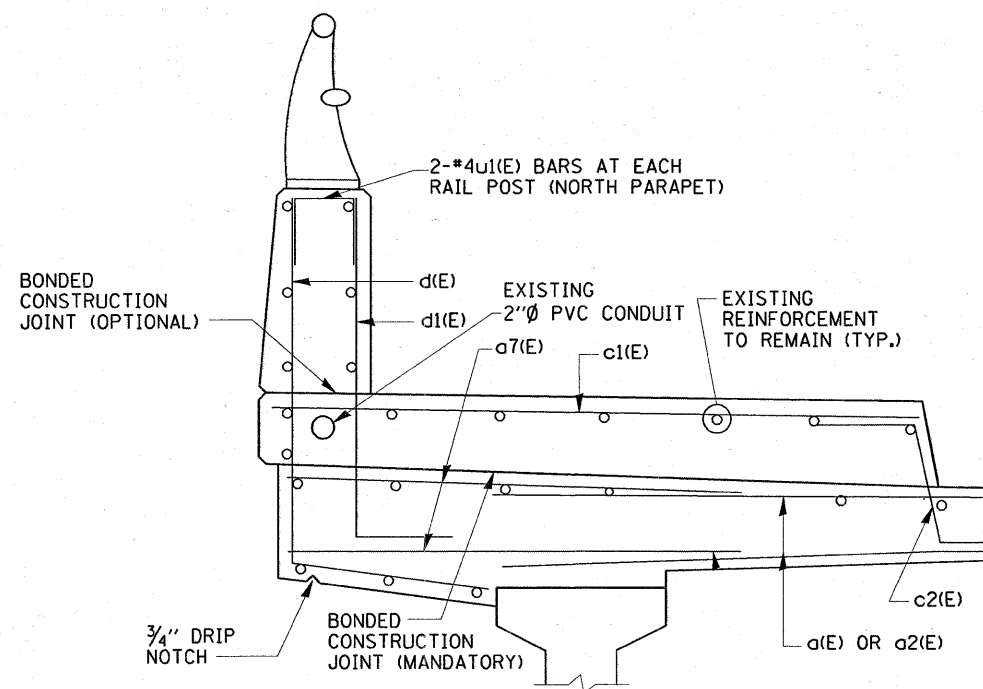
ELEVATION
 Note: Vertical & horizontal scale not the same.

SHEET NO. 1 OF 9 SHEETS

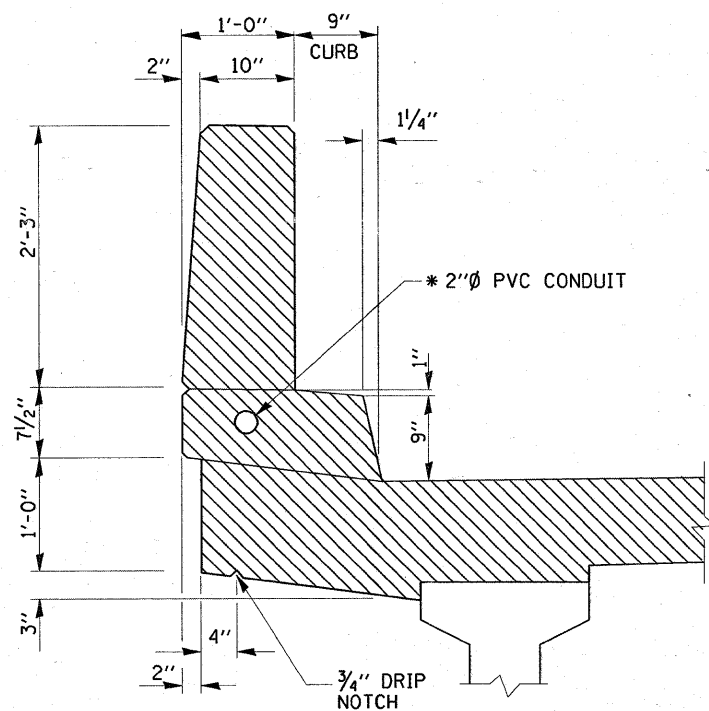
ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL PLAN AND ELEVATION
 CASS STREET (U.S. ROUTE 30) OVER E.J. & E.R.R.
 F.A. ROUTE 607 SECTION 13D-R-1(82)
 WILL COUNTY



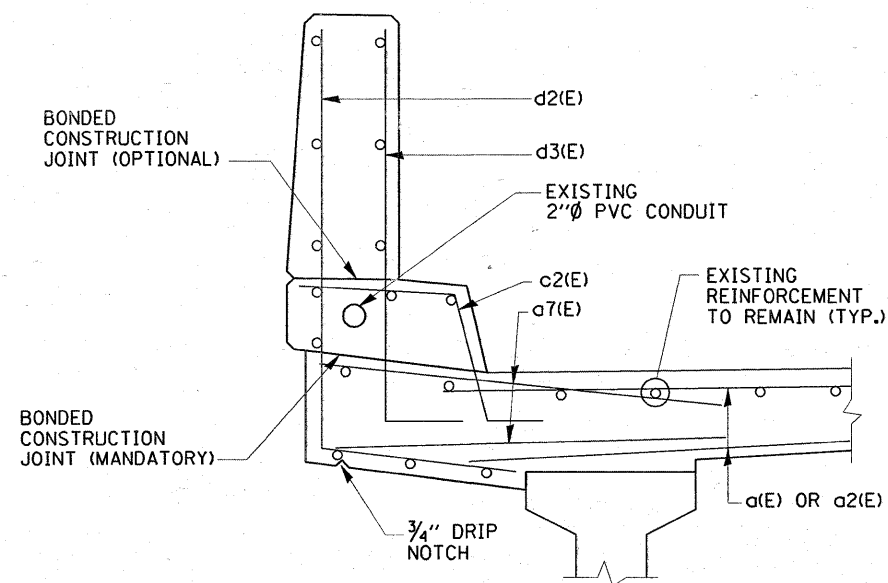
SECTION A-A (EXISTING)
 (HATCHED AREA INDICATES CONCRETE REMOVAL)



SECTION A-A (PROPOSED)
 PROPOSED DIMENSIONS SAME AS EXISTING

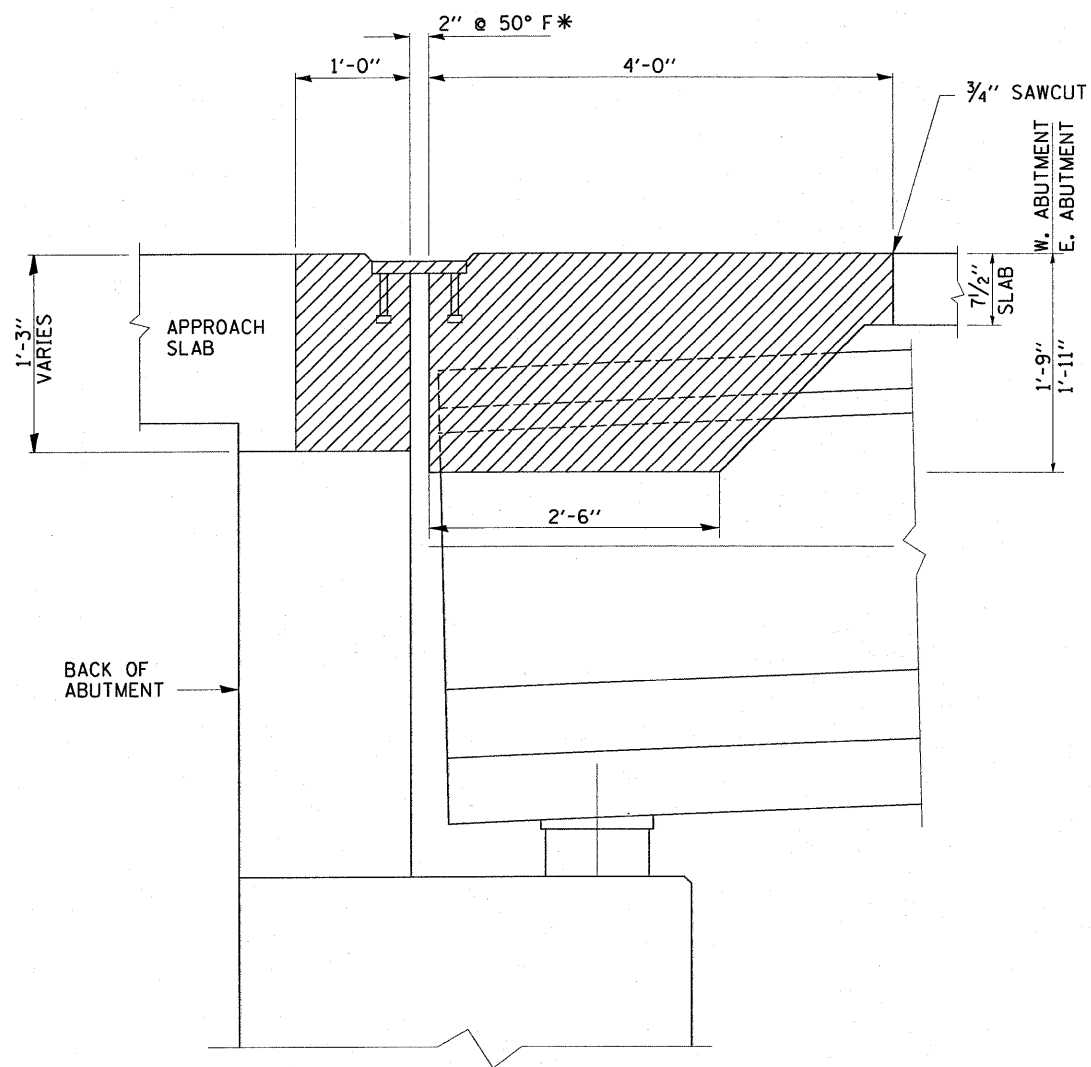


SECTION B-B (EXISTING)
 (HATCHED AREA INDICATES CONCRETE REMOVAL)

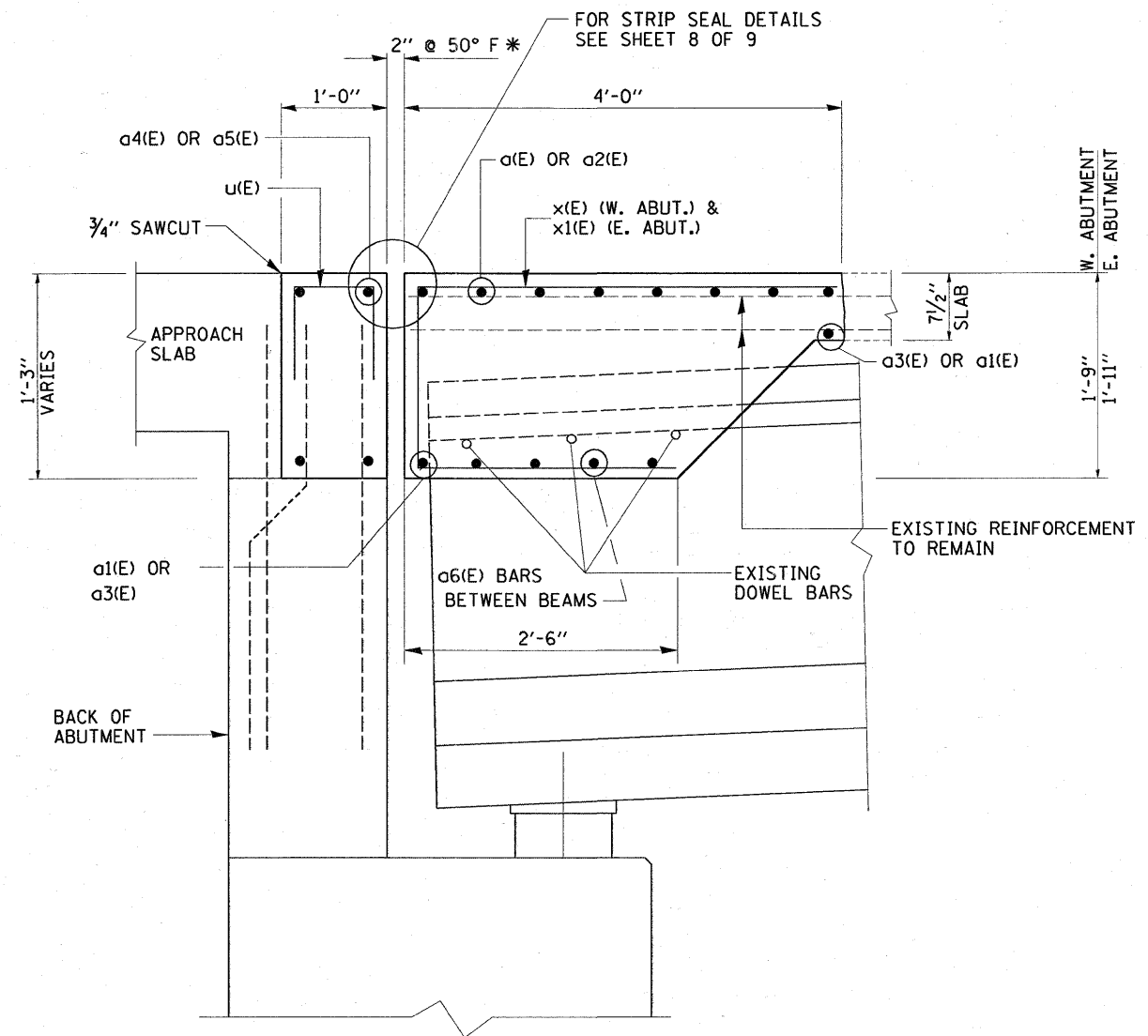


SECTION B-B (PROPOSED)
 PROPOSED DIMENSIONS SAME AS EXISTING

FILE NAME =	USER NAME = tunkosmv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 30 OVER E.J. & E.R.R. SECTIONS AT WEST AND EAST ABUTMENTS S.N. 099-0264		F.A.P. RTE. 607	SECTION 13 D-R-1-I	COUNTY WILL	TOTAL SHEETS 16	SHEET NO. 9	
ca\pw_work\PWIDOT\TINAKOSMV\d0101671\099-0264.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 4 OF 9 SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60E89		
		CHECKED -	REVISED -									
		DATE -	REVISED -									



SECTION C-C (EXISTING)
(HATCHED AREA INDICATES CONCRETE REMOVAL)

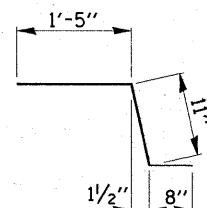


SECTION C-C (PROPOSED)

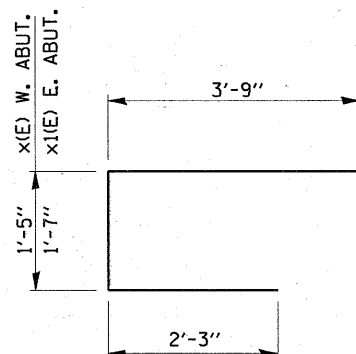
NOTES:

FOR BONDED CONSTRUCTION JOINT SEE SPECIAL PROVISION REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED. EXISTING REINFORCEMENT BARS AND DOWEL RODS SHOWN ARE TO BE CLEANED AND INCORPORATED INTO NEW CONSTRUCTION.

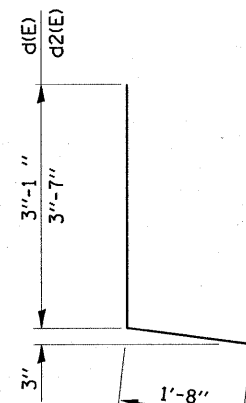
* DIMENSIONS ARE BASED ON A ROLLED RAIL STRIP SEAL JOINT. IF THE CONTRACTOR ELECTS TO USE THE WELDED RAIL STRIP SEAL JOINT, DECK DIMENSIONS MAY REQUIRE ADJUSTMENTS TO SATISFY THE DETAILS ON BASE SHEET EJ-SSJ.



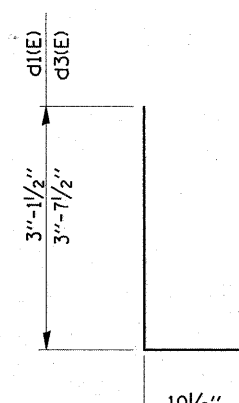
BAR c2(E)



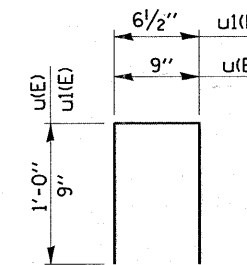
BAR x(E) & x1(E)



BAR d(E) & d2(E)



BAR d1(E) & d3(E)



BAR u(E) & u1(E)

FILE NAME =
c:\pwwork\pww\d04\TINIAKOSMV\d0181671\099-0264.dgn

USER NAME = tiniakosmv
DESIGNED -
DRAWN -
CHECKED -
PLOT DATE = 2/19/2009

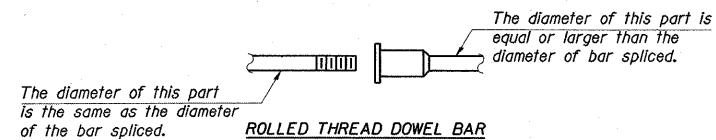
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DATE -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. 30 OVER E.J. & E. R.R.
EXPANSION JOINS AT WEST AND EAST ABUTMENTS
S.N. 099-0264**

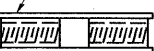
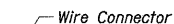
F.A.P. RTE. 607	SECTION 13 D-R-1-I	COUNTY WILL	TOTAL SHEETS 16	SHEET NO. 10
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60E89	



ROLLED THREAD DOWEL BAR



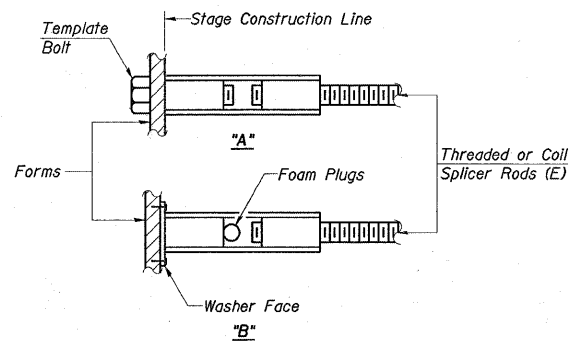
ONE PIECE



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

***Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



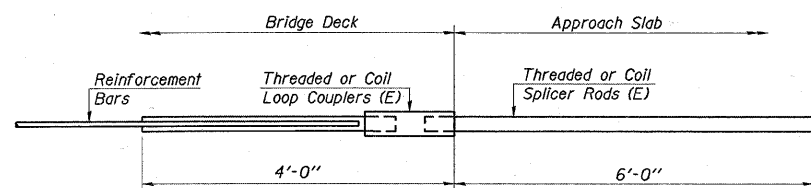
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

NOTES
 Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

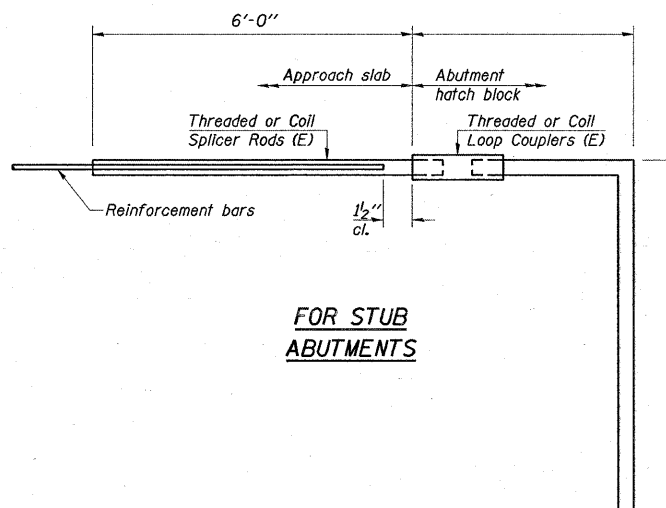
- ① Minimum Capacity = $1.25 \times f_y \times A_s$
 (Tension in kips)
 - ② Minimum *Pull-out Strength = $0.66 \times f_y \times A_s$
 (Tension in kips)
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_s = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



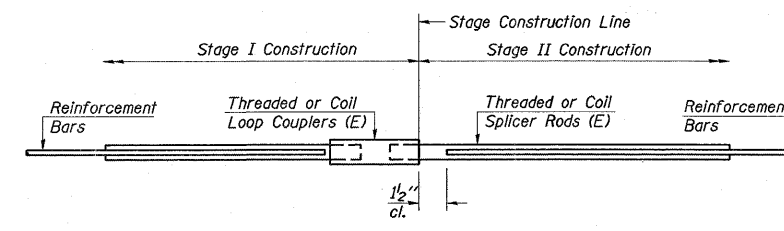
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



FOR STUB ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



STANDARD

Bar Size	No. Assemblies Required	Location
#6	20	Joints Abutment
#7	16	Joints Abutment
Total	36	

BSD-1 10-1-08

Joint Size	"C" at 50°F	"D" at 50°F
2"	2"	1½" Min.
2½"	2½"	1¾" Min.
4"	3"	2½" Min.

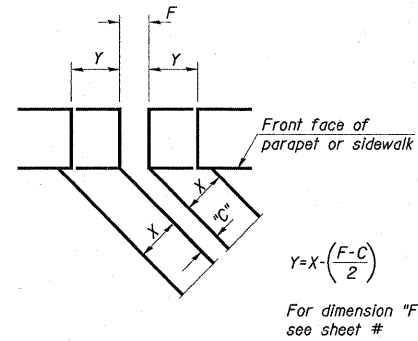
INSTALLATION NOTES

- Install continuous seal in roadway, parapet, curb, and sidewalk.
- Install anchor blocks as indicated.

Note A:
Maximum spacing of anchor bolts shall be 12" centers.

SKEW LIMITATIONS

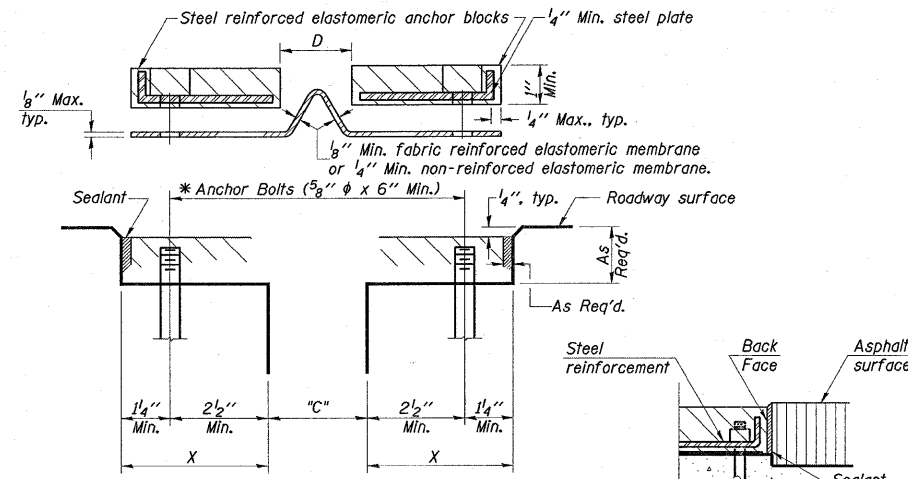
The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews. For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 1½" from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±12" cts.



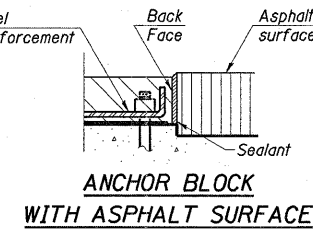
$$Y = X \cdot \left(\frac{F - C}{2} \right)$$

For dimension "F" see sheet #

FORMING BLOCKOUT SKETCH



CROSS SECTION



ANCHOR BLOCK WITH ASPHALT SURFACE

GENERAL NOTES

Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane.

The elastomeric membrane shall be premolded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure.

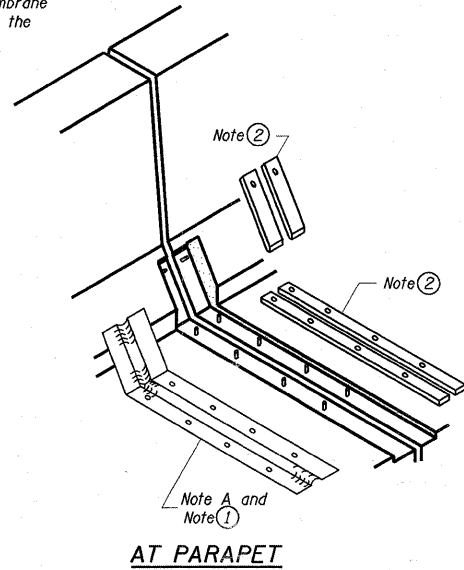
The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed.

The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted.

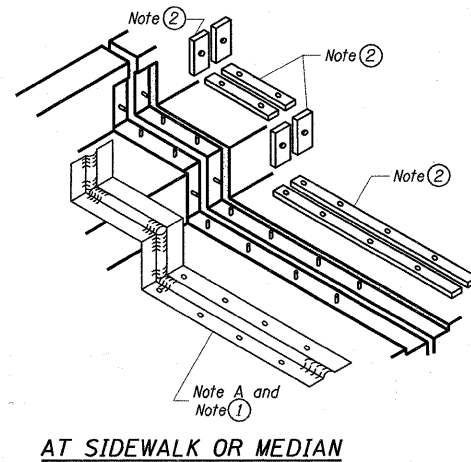
All dimensions are in inches except as noted.

* Epoxy grout 5/8" diameter threaded studs according to Article 584 of the Standard Specifications. Space to miss existing studs.

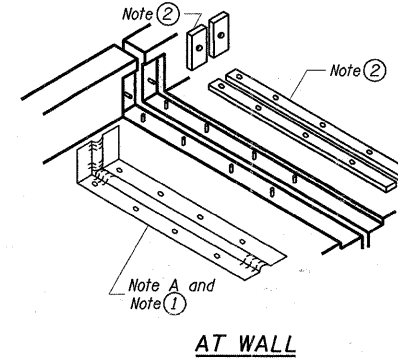
Existing Anchor Bolts to be cut Flush with Concrete.



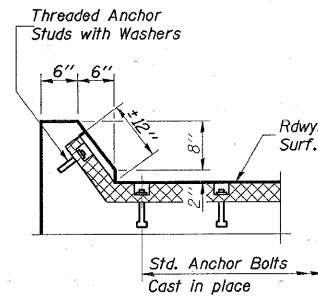
AT PARAPET



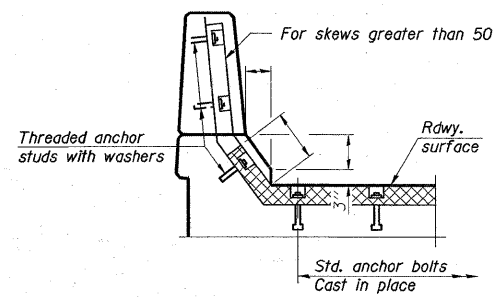
AT SIDEWALK OR MEDIAN



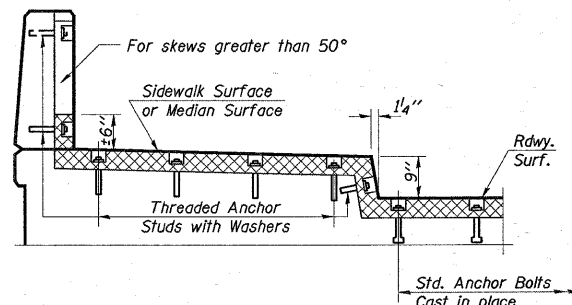
AT WALL



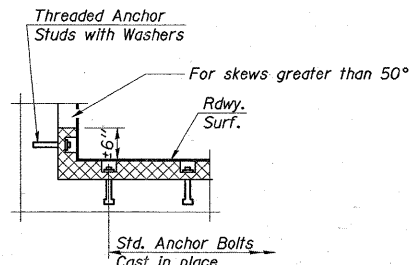
AT CURB



AT PARAPET



AT SIDEWALK OR MEDIAN
TYPICAL END TREATMENTS



AT WALL

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PLDT DATE = 2/19/2009

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

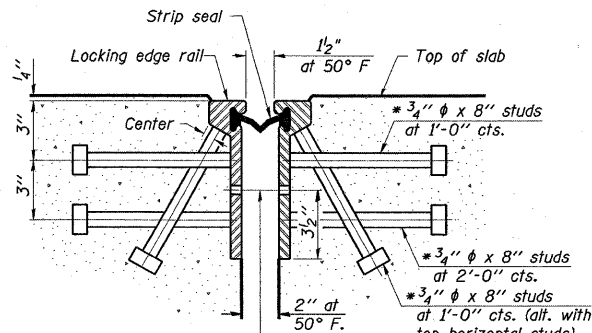
U.S. 30 OVER E.J. & E.R.R.
CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS
S.N. 099-0264

SCALE: SHEET NO. 7 OF 9 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	13 D-R-1-I	WILL	16	12
				CONTRACT NO. 60E89

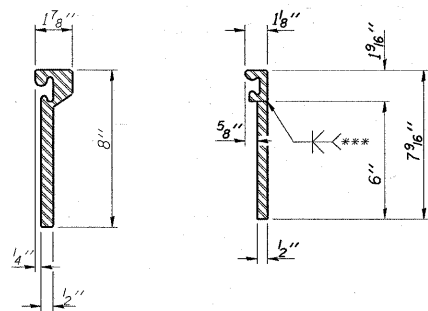
(ILLINOIS) FED. AID PROJECT

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

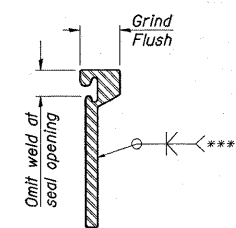


7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU ROLLED RAIL JOINT



ROLLED EXTRUDED RAIL WELDED RAIL

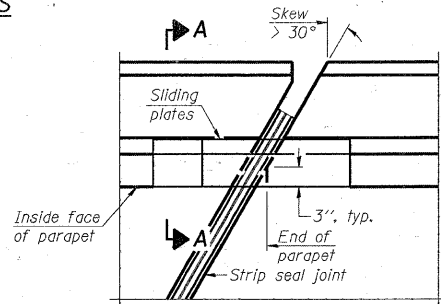


***Back gouge not required if complete joint penetration is verified by mock-up.

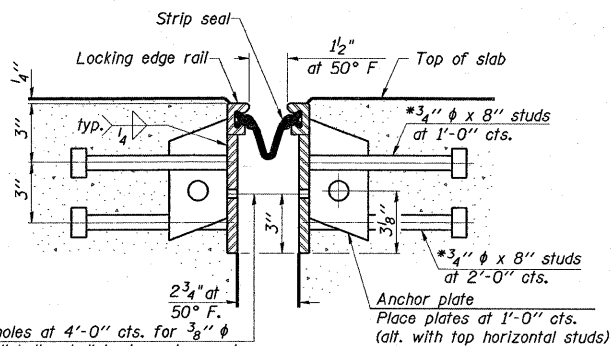
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

LOCKING EDGE RAILS

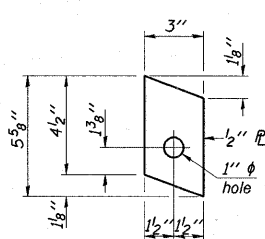


PLAN

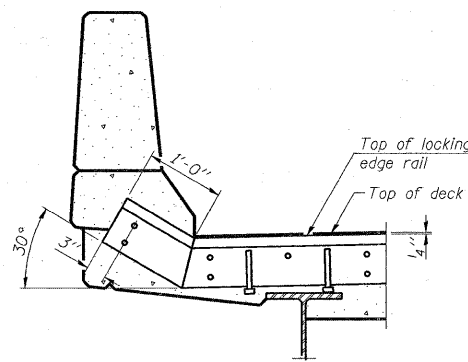


7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

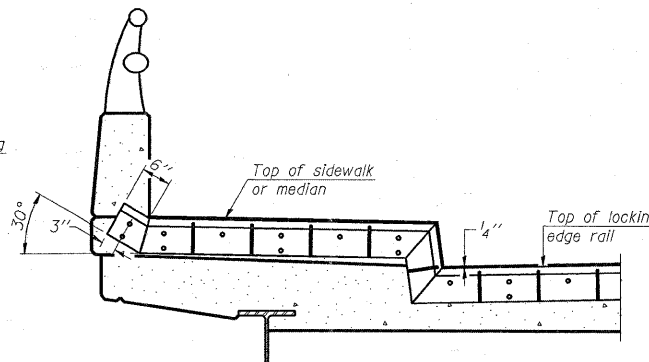
SECTION THRU WELDED RAIL JOINT



ANCHOR PLATE (for welded rail)



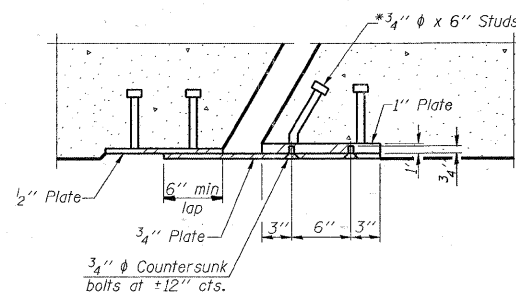
AT PARAPET



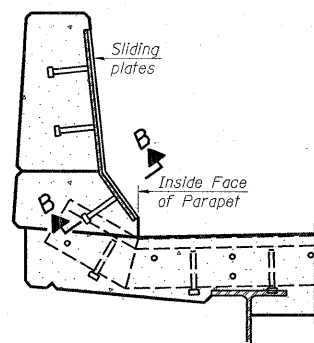
AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

TYPICAL END TREATMENTS



SECTION B-B



SECTION A-A

POINT BLOCK DETAILS (for skews > 30°)

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	109

EJ-SSJ 10-1-08

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DESIGNED -	REVISD -
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DATE -	REVISD -

DESIGNED -	REVISD -
DRAWN -	REVISD -
CHECKED -	REVISD -
DATE -	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

U.S. 30 OVER E.J. & E.R.R.
PREFORMED JOINT STRIP SEAL
S.N. 099-0264

SCALE: NONE

SHEET NO. 8 OF 9 SHEETS

STA.

TO STA.

F.A.P. RTE. 607

SECTION 13 D-R-1-I

COUNTY WILL

TOTAL SHEETS 16

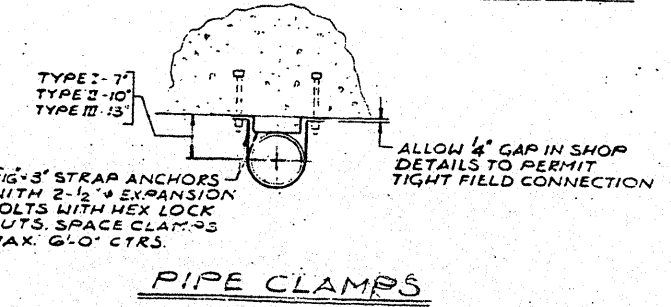
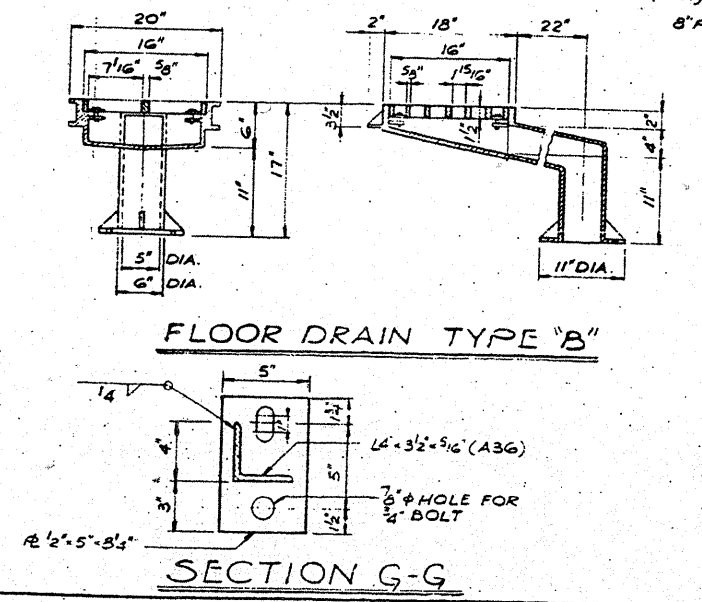
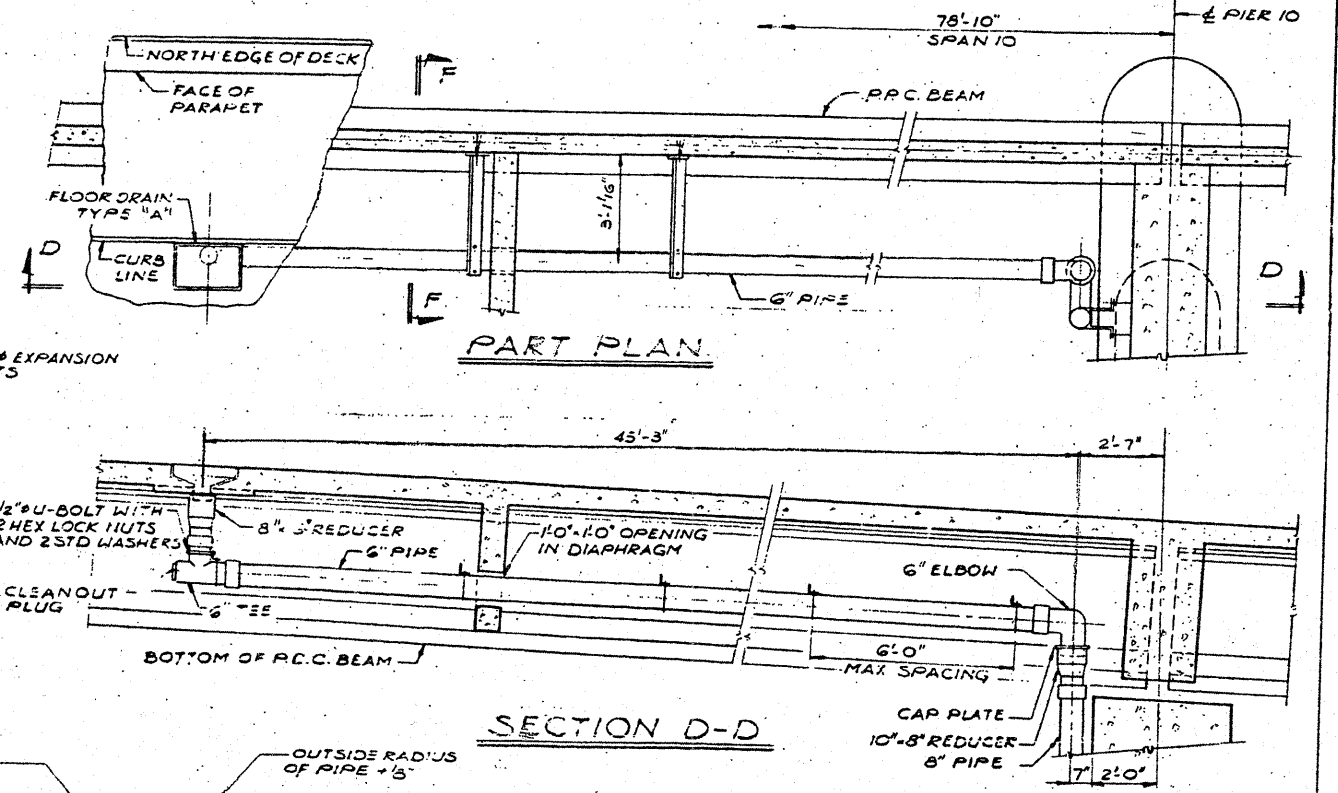
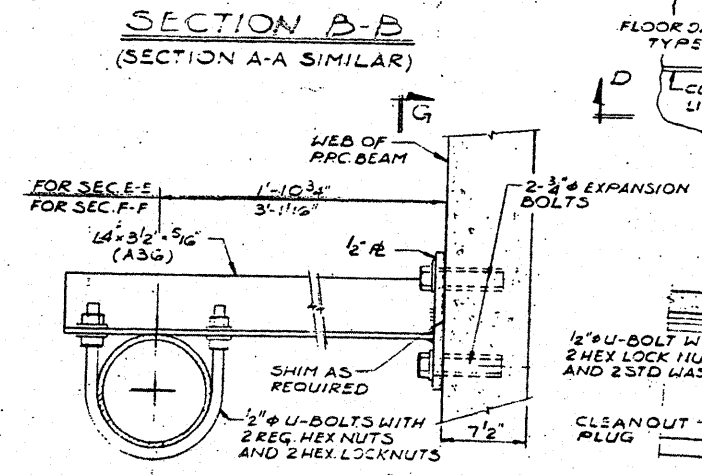
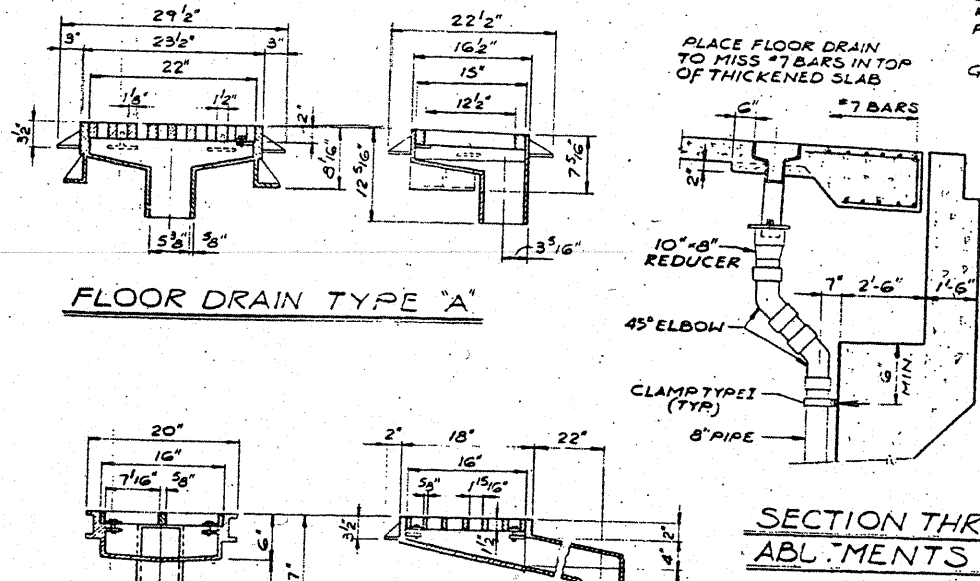
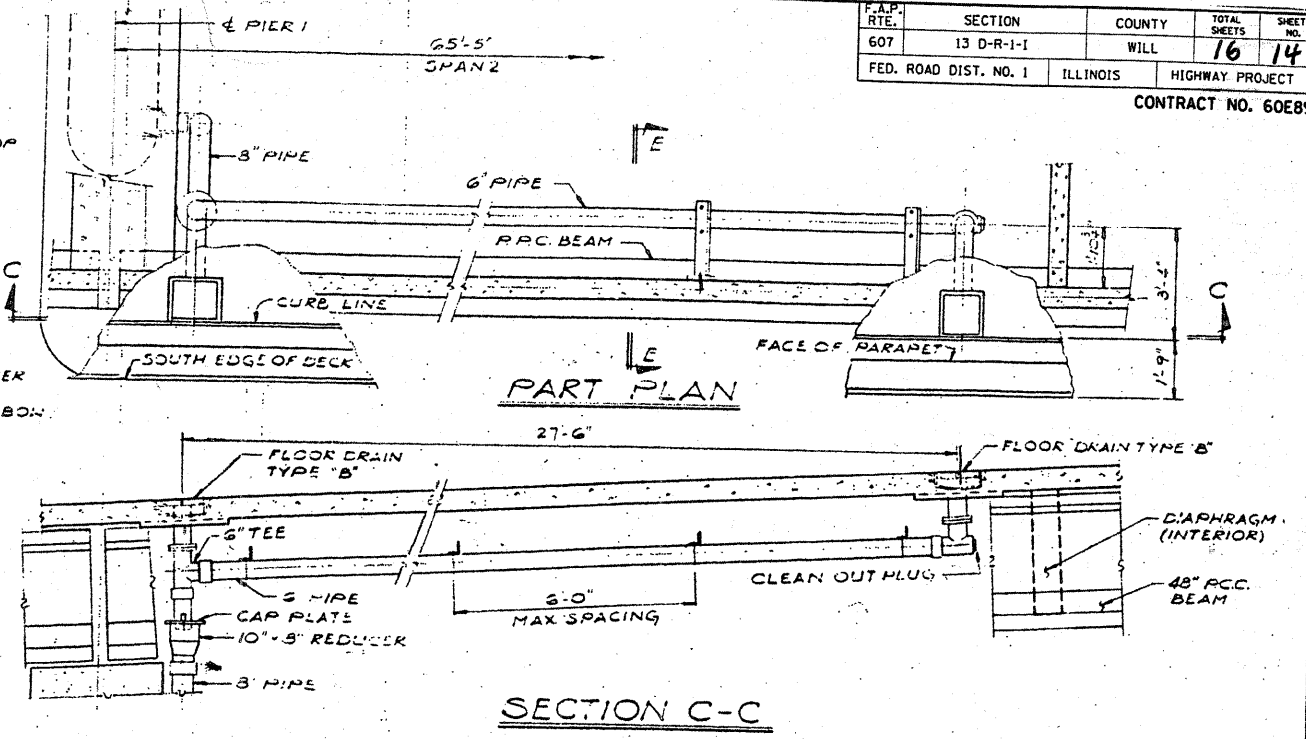
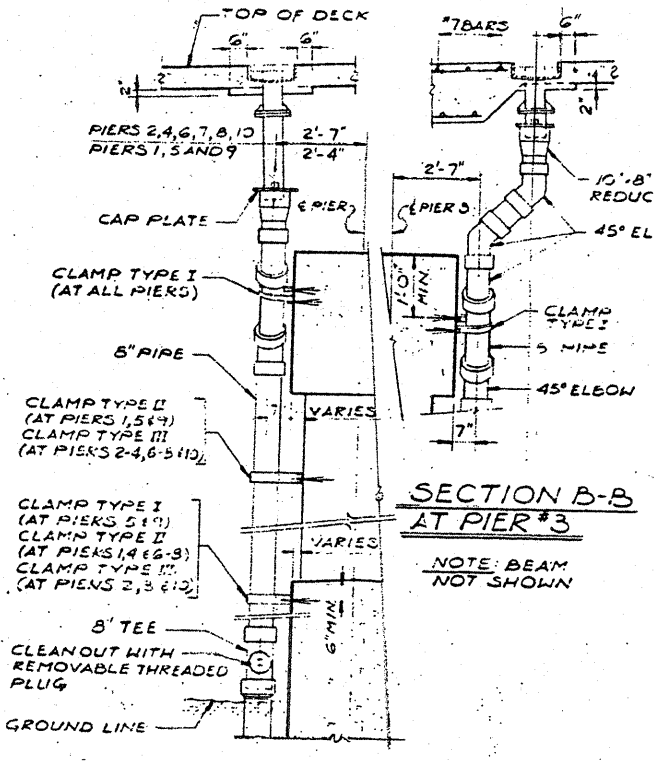
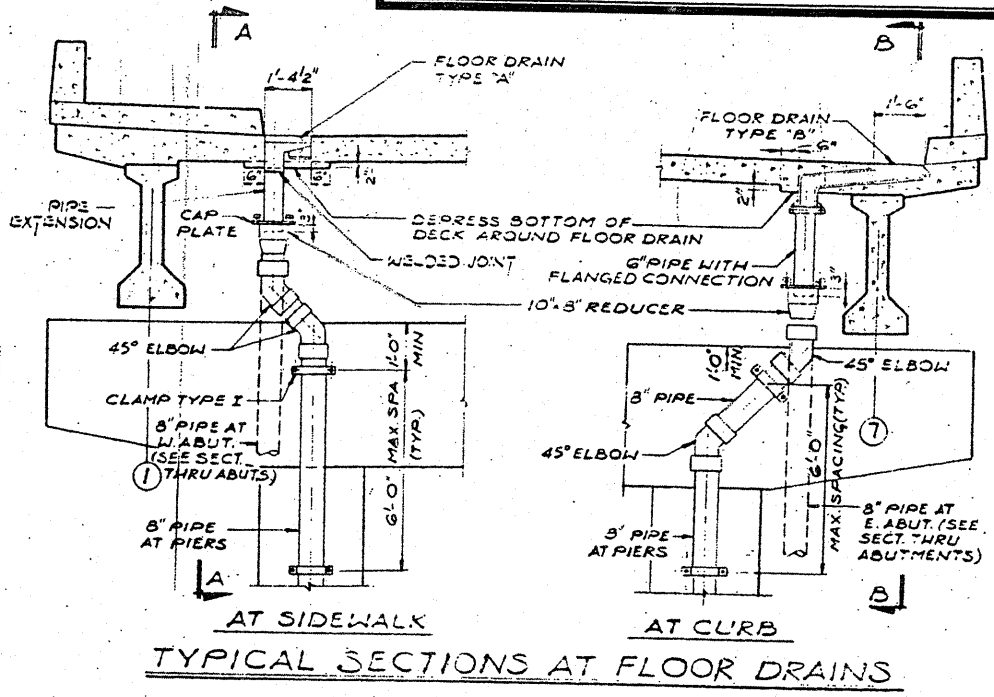
SHEET NO. 13

CONTRACT NO. 60E89

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

FOR INFORMATION ONLY

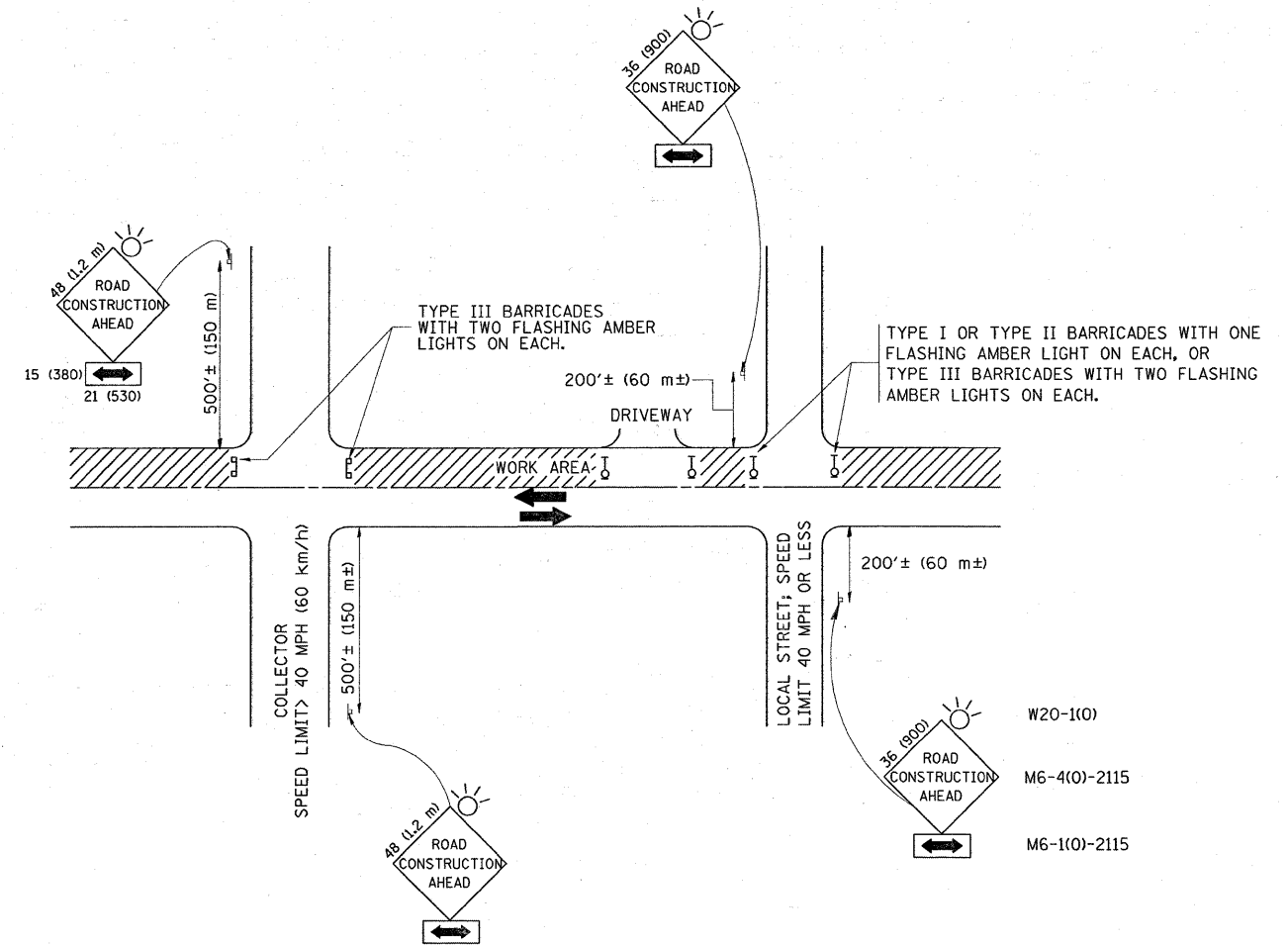
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607	13 D-R-1-1	WILL	16	14
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	
CONTRACT NO. 60E89				



REVISIONS	
4	
3	
2	
1	

SHEET NO. 9 OF 9 SHEETS

ILLINOIS DEPARTMENT OF TRANSPORTATION
DECK DRAINAGE
CASS STREET (U.S. ROUTE 30) OVER E.J. & E.R.R.
F.A. ROUTE 607 SECTION 13D-R-1(82)
WILL COUNTY



W20-1(K)
M6-4(O)-2115
M6-1(O)-2115

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

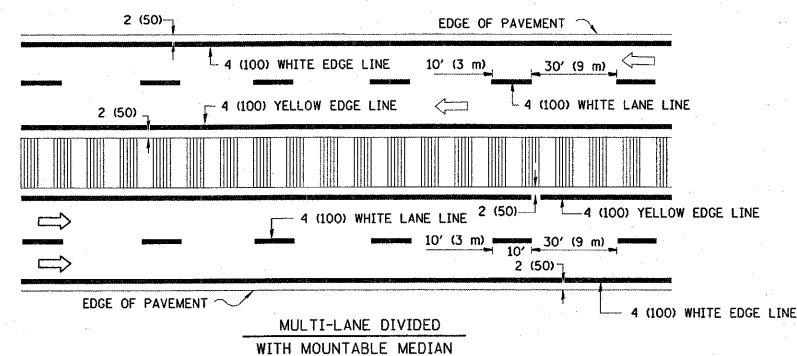
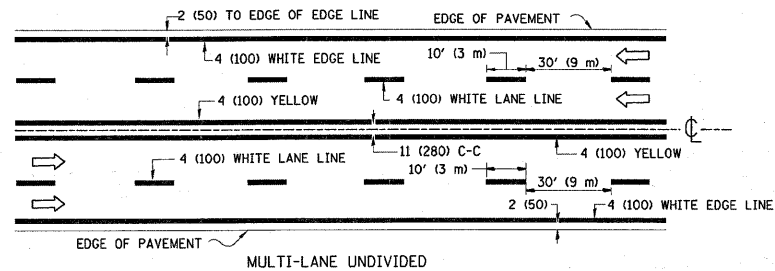
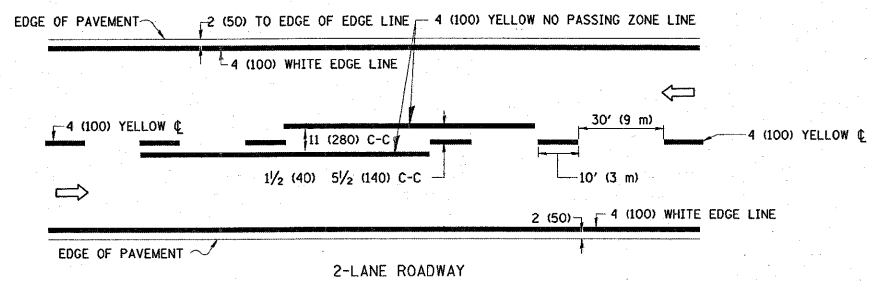
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	PLDT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLDT DATE = 2/19/2009	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

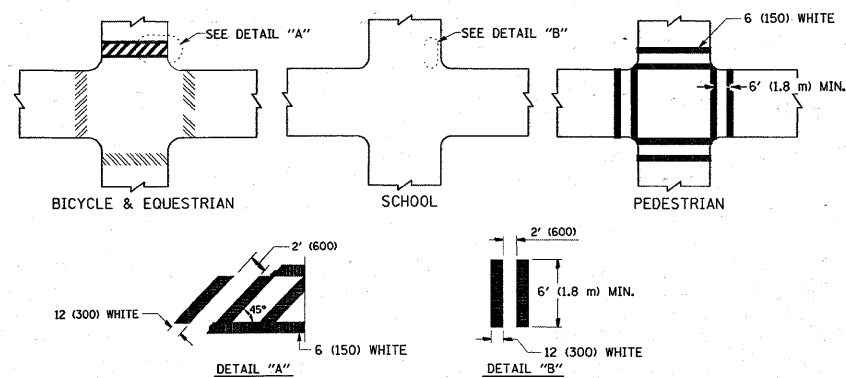
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60E89	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

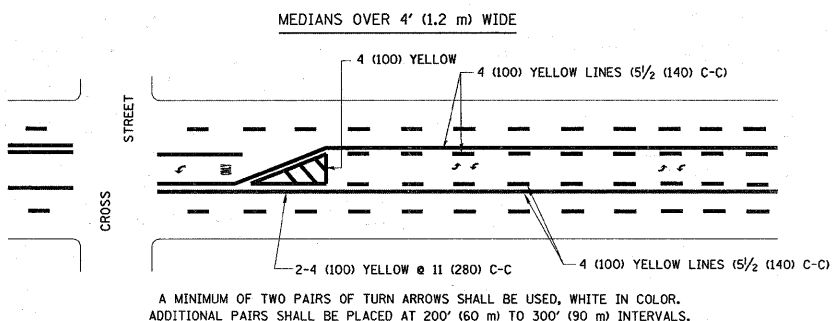
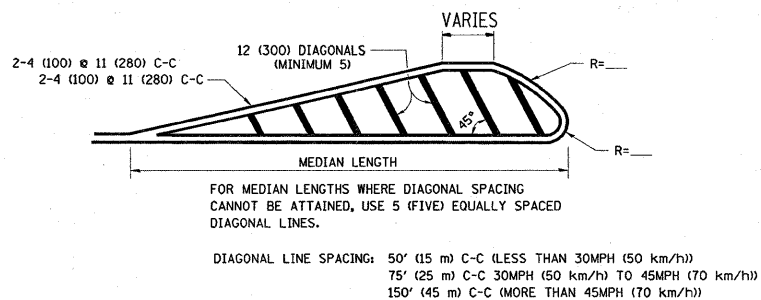
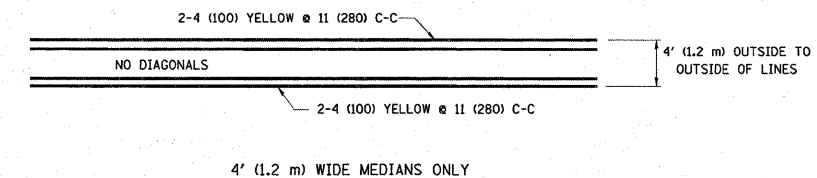


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

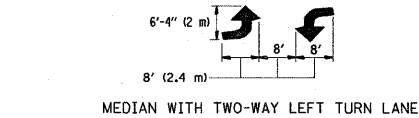
TYPICAL LANE AND EDGE LINE MARKING



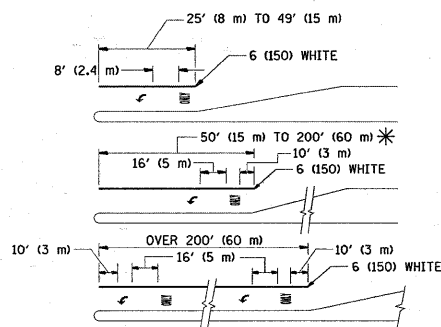
TYPICAL CROSSWALK MARKING



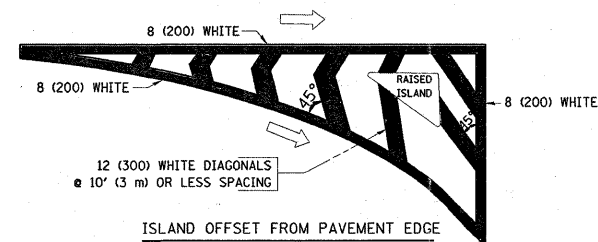
TYPICAL PAINTED MEDIAN MARKING



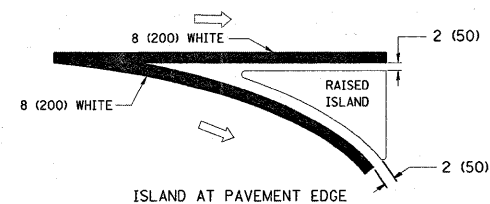
TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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		CHECKED -	REVISED - A. HOUSEH 10-17-96
		DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.P. RTE. 607	SECTION 13 D-R-1-I	COUNTY WILL	TOTAL SHEETS 16	SHEET NO. 16
			TC-13		CONTRACT NO.		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT							