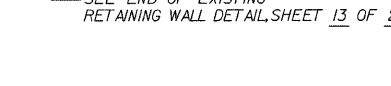
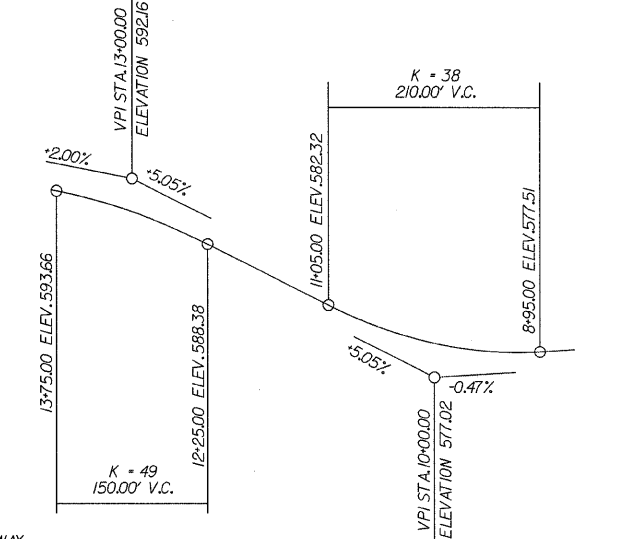
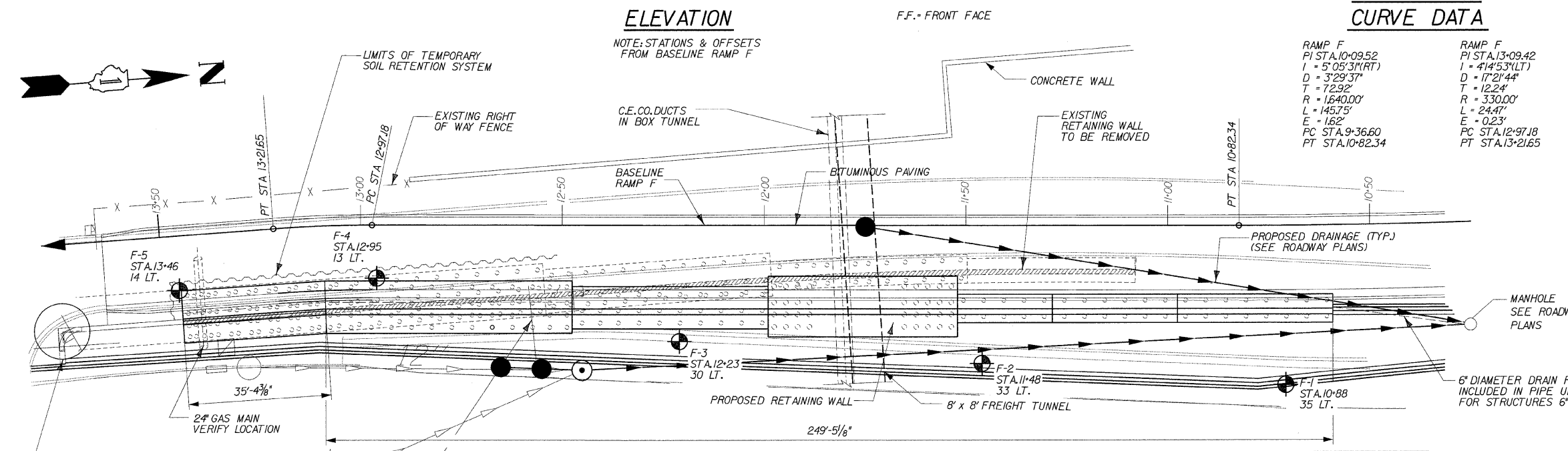


BENCH MARK
 BM 903 - N.E. ANCHOR BOLT OF HIGH MAST LIGHT POLE LOCATED FROM MAINLINE STATION EBL AT STA. 28+10 - 96 FT. LEFT: ELEV. + 583.59

EXISTING STRUCTURE NO. 016-W807
 1-90/94 RAMP F - JACKSON BLVD. EXIT RAMP BUILT IN 1957, CONSISTS OF CAST-IN-PLACE CONCRETE RETAINING WALL SUPPORTED ON CONCRETE PILING.

PROPOSED IMPROVEMENTS
 EXISTING STRUCTURE TO BE PARTIALLY REMOVED AND REPLACED WITH A CAST-IN-PLACE CONCRETE T-TYPE RETAINING WALL SUPPORTED ON 14" DIAMETER METAL SHELL PILES. NO SALVAGE.



- GENERAL NOTES**
- REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 706 GR 60. SEE SPECIAL PROVISIONS.
 - REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
 - PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING STRUCTURES HAVE BEEN TAKEN FROM FIELD MEASUREMENTS AND ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE FOR THE WORK.
 - REMOVAL OF THE EXISTING RAMP F RETAINING WALL, INCLUDING EXISTING PILE TOPS AND BACKFILLING WITH GRANULAR MATERIAL TO EL. 571.92, TO BE INCLUDED IN REMOVAL OF EXISTING STRUCTURES.
 - THE CONTRACTOR SHALL DRIVE TWO METAL SHELL PILES 14" DIAMETER TEST PILES IN A PERMANENT LOCATION NEAR BORINGS F-5 AND F-3 AS INDICATED ON THE PLANS AND AS DIRECTED BY THE ENGINEER BEFORE ORDERING THE REMAINDER OF PILES.
 - A CANTILEVERED SHEET PILING DESIGN DOES NOT APPEAR FEASIBLE AND ADDITIONAL MEMBERS OR OTHER RETENTION SYSTEMS MAY BE NECESSARY. THE CONTRACTOR SHALL SUBMIT A TEMPORARY SOIL RETENTION SYSTEM DESIGN INCLUDING PLAN DETAILS AND CALCULATIONS FOR REVIEW AND ACCEPTANCE BY THE ENGINEER.
 - PRIOR TO INSTALLING THE PILING, THE CONTRACTOR SHALL VERIFY THE LOCATION AND DIMENSIONS SHOWN IN THE PLANS FOR THE GAS LINE AND BOX TUNNELS. FOR THE GAS LINE AND THE C.E. DUCT, PILING SHALL BE INSTALLED WITH A MINIMUM OF TWO (2) FEET CLEARANCE. FOR THE FREIGHT TUNNEL, THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF CHICAGO DEPARTMENT OF ENGINEERING, J.J. MADIA AT 312-744-3920 FOR APPROVED INSTALLATION TECHNIQUES. AT A MINIMUM THE PILES WITHIN TEN (10) FEET OF THE FREIGHT TUNNEL SHALL BE PRE-DRILLED. COST OF PREDRILLING PILES AND ANY PERMITTING REQUIREMENTS FOR THE CITY OF CHICAGO SHALL BE INCLUDED IN THE UNIT PRICE BID FOR DRIVING PILES. ANY DAMAGE TO THE TUNNELS OR GAS LINE AS A RESULT OF THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED TO "LIKE NEW" CONDITIONS, BY THE CONTRACTOR, AT NO COST TO THE DEPARTMENT. "LIKE NEW" CONDITIONS OF THE REPAIRS MUST BE ACCEPTED BY THE ENGINEER.
 - ALL CONSTRUCTION JOINTS SHALL BE BONDED.

TOTAL BILL OF MATERIAL

| ITEM | UNIT | QUANTITY |
|--|-------|----------|
| CONCRETE STRUCTURES | CU YD | 527.0 |
| TEMPORARY SOIL RETENTION SYSTEM | SQ FT | 2,300 |
| CONCRETE REMOVAL | CU YD | 360 |
| REINFORCEMENT BARS | POUND | 40,780 |
| REINFORCEMENT BARS, EPOXY COATED | POUND | 10,100 |
| STRUCTURE EXCAVATION | CU YD | 480 |
| FURNISHING METAL SHELL PILES 14" x 0.250" | FOOT | 14,325 |
| DRIVING PILES | FOOT | 14,325 |
| TEST PILE METAL SHELLS | EACH | 2 |
| GEOCOMPOSITE WALL DRAIN | SQ YD | 344 |
| RUSTICATION FINISH | SQ FT | 2,685 |
| INSTALL DECORATIVE STEEL GUARDRAIL | FOOT | 284 |
| INSTALL DECORATIVE STEEL FASCIA | FOOT | 258 |
| EMBEDDED PLATE, GUARDRAIL UPRIGHT, EXPANSION JOINT | EACH | 10 |
| EMBEDDED PLATE, GUARDRAIL UPRIGHT | EACH | 40 |
| REPAIR OF FASCIA PANELS, EXIT RAMP F | LS | 1 |

DESIGN SPECIFICATIONS
 2002 AASHTO SPECIFICATIONS FOR HIGHWAY BRIDGES

DESIGN STRESSES
 LOADINGS
 EQUIVALENT FLUID SOIL PRESSURE = 40 pcf
 TRAFFIC SURCHARGE PRESSURE = 240 pcf
 F'C = 3,500 PSI (CONCRETE)
 F_y = 60,000 PSI (REINFORCEMENT)

APPROVED
 FOR STRUCTURAL ADEQUACY ONLY

Ralph E. Anderson
 ENGINEER OF BRIDGES AND STRUCTURES

DLZ ILLINOIS, L.L.C.
 ARLINGTON HEIGHTS, ILLINOIS

Signed: *John R. Dambrowski*
 Date: 02-25-2009
 Expiration Date: 11/30/10

DESIGNED DPW
CHECKED JMM
DRAWN PH
CHECKED JMM

RETAINING WALL RAMP F
GENERAL PLAN & ELEVATION
 1-90/94 KENNEDY EXPRESSWAY
 (HUBBARDS CAVE TO I-290)
 SECT. (202.6 - 2P, ETC, 1415 & 1517) R-7
 STATION 13+45 LT 22' TO 10+59 LT 22'
 COOK COUNTY
 STRUCTURE NUMBER 016-W807

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