

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------------------|--------|--------------|-----------|
| 1332 | (3071,3071A&3072)RS-3 | COOK | 38 | 1 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS
FAU ROUTE 1332 (OAKTON STREET)
S. DES PLAINES RIVER RD TO US 14 (CALDWELL AVE)
SECTION (3071, 3071A & 3072) RS-3
PROJECT: ESP-1332 (015)
RESURFACING (3P)
COOK COUNTY
C-91-217-04

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**PROJECT LOCATED IN
 THE CITY OF PARK RIDGE
 AND THE VILLAGE OF NILES**

D-91-217-04

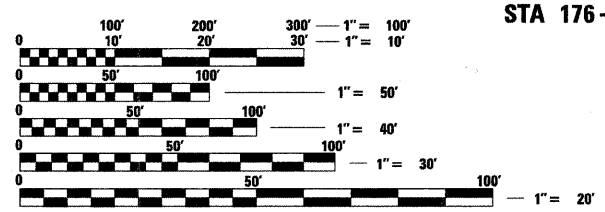


TRAFFIC DATA

2007 ADT = 28,400 VEHICLES PER DAY
 SPEED LIMIT = 35 MPH

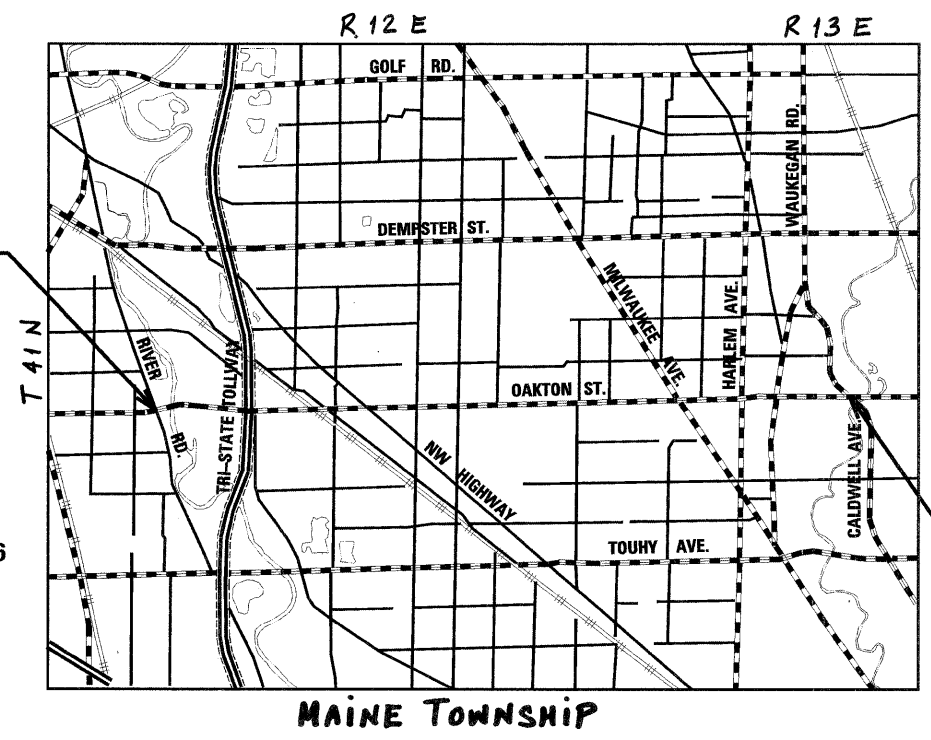
**PROJECT BEGINS
 STA 6+12**

OMISSIONS
 STA 10+10 TO STA 12+73
 STA 34+80 TO STA 37+57
 STA 59+58 TO STA 105+56
 STA 176+23 TO STA 178+16



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811



**PROJECT ENDS
 STA 231+76**

GROSS LENGTH OF PROJECT = 22,564 FT = 4.27 MILES
 NET LENGTH OF PROJECT = 17,233 FT = 3.27 MILES

LOCATION MAP
 NOT TO SCALE

PROJECT ENGINEER: MICHELLE AQUINO (847) 705-4606
PROJECT MANAGER: LONG TRAN (847) 705-4232

CONTRACT NO. 62755

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 6, 20 09
Diana M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 27, 20 09
Charles G. Ingersoll ENGINEER OF DESIGN AND ENVIRONMENT
March 27, 20 09
Christina M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

rjngroup
 Excellence through Ownership
 200 West Front Street
 Wheaton, IL 60187

STATE OF ILLINOIS
 BRANDON L. BUZZELL
 56615
 NAPERVILLE
 PROFESSIONAL ENGINEER
 2/2/09
 Expires 11-30-09

**PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS**

GENERAL NOTES

INDEX OF SHEETS

| SHEET NO. | DESCRIPTION |
|-----------|---|
| 1 | TITLE SHEET |
| 2 | INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES |
| 3 | SUMMARY OF QUANTITIES |
| 4 | TYPICAL SECTIONS |
| 5-10 | PLAN AND PAVEMENT MARKING DETAILS |
| 11-21 | DETECTOR LOOP PLANS |
| 22 | FRAMES AND LIDS ADJUSTMENT WITH MILLING; AND FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING |
| 23 | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT |
| 24 | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT |
| 25 | BUTT JOINTS AND HMA TAPER |
| 26 | SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS |
| 27 | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS |
| 28 | RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) |
| 29 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS |
| 30 | TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) |
| 31 | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING |
| 32 | TEMPORARY INFORMATION SIGNING |
| 33 | SIGNING AND PAVEMENT MARKINGS TREATMENT FOR RAILROAD CROSSINGS |
| 34-37 | STANDARD TRAFFIC SIGNAL DESIGN DETAILS (4 SHEETS) |
| 38 | DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING |

STATE STANDARDS

| STANDARD NO. | DESCRIPTION |
|--------------|---|
| 000001-05 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 424001-05 | CURB RAMPS FOR SIDEWALKS |
| 442201-03 | CLASS C AND D PATCHES |
| 604001-03 | FRAME AND LIDS TYPE 1 |
| 606001-04 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 701301-03 | LANE CLOSURE 2L, 2W SHORT TIME OPERATIONS |
| 701606-06 | URBAN LANE CLOSURE MULTILANE 2W WITH MOUNTABLE MEDIAN |
| 701701-06 | URBAN LANE CLOSURE MULTILANE INTERSECTION |
| 701901-01 | TRAFFIC CONTROL DEVICES |
| 780001-02 | TYPICAL PAVEMENT MARKINGS |

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800)892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED.)
2. ALL PAVEMENT MARKING SHALL BE PLACED THROUGHOUT THE PROJECT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.
3. ALL HMA PAVEMENT PATCHING SHALL BE CLASS D.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
6. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
7. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
8. DRAINAGE ADJUSTMENT, CLEANING OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
9. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
10. THE RESIDENT ENGINEER SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
11. THE ENGINEER SHALL CONTACT MS. PATRICE HARRIS, THE TRAFFIC FIELD ENGINEER, @ 708-597-9800 TWO (2) WEEKS PRIOR TO THE START OF THIS PROJECT SO THAT EXACT STATIONING OF NO PASSING ZONES AND OTHER PERMANENT PAVEMENT MARKINGS MAY BE ESTABLISHED.
12. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
13. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
14. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
15. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL FOR TYPICAL APPLICATION OF RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) SHOWN IN THE PLANS.
16. WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
17. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)
18. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
19. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
20. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.

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 PLOT SCALE : 1:1000
 USER NAME : J.T.T.
 MODEL : Default



200 West Front Street
Wheaton, IL 60187

| | |
|-----------------|-----------|
| DESIGNED - B.B. | REVISED - |
| DRAWN - J.T.T. | REVISED - |
| CHECKED - M.K. | REVISED - |
| DATE - 1/30/09 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE)
INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-------------------------|--------|--------------|-----------|
| 1332 | (3071,3071A & 3072)RS-3 | COOK | 38 | 2 |
| CONTRACT NO. 62755 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

| SUMMARY OF QUANTITIES | | | URBAN 100% FED. | |
|-----------------------|--|--------|--------------------|------------------------|
| CODE NO. | ITEM DESCRIPTION | UNIT | TOTAL QUANTITY | CONSTRUCTION TYPE CODE |
| 40600200 | BITUMINOUS MATERIALS (PRIME COAT) | TON | 46.4 | 46.4 |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 232.5 | 232.5 |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 11.6 | 11.6 |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 2 | 2 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SO YD | 1162 | 1162 |
| 40601005 | HOT-MIX ASPHALT REPLACEMENT OVER PATCHES | TON | 3500 | 3500 |
| 40603595 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 | TON | 11362 | 11362 |
| 42001300 | PROTECTIVE COAT | SO YD | 150 | 150 |
| 44000159 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2" | SO YD | 115936 | 115936 |
| 44001700 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 2000 | 2000 |
| 44002222 | HOT-MIX ASPHALT REMOVAL OVER PATCHES, 5 1/2" | SO YD | 13900 | 13900 |
| 44201749 | CLASS D PATCHES, TYPE I, 9 INCH | SO YD | 1700 | 1700 |
| 44201753 | CLASS D PATCHES, TYPE II, 9 INCH | SO YD | 5200 | 5200 |
| 44201757 | CLASS D PATCHES, TYPE III, 9 INCH | SO YD | 2200 | 2200 |
| 44201759 | CLASS D PATCHES, TYPE IV, 9 INCH | SO YD | 2500 | 2500 |
| NP 55039700 | STORM SEWERS TO BE CLEANED | FOOT | 1200 | 1200 |
| 60252800 | CATCH BASINS TO BE RECONSTRUCTED | EACH | 7 | 7 |
| 60257900 | MANHOLES TO BE RECONSTRUCTED | EACH | 7 | 7 |
| 60300310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | EACH | 253 | 253 |
| 60406000 | FRAMES AND LIDS, TYPE 1, OPEN LID | EACH | 12 | 12 |
| 60406100 | FRAMES AND LIDS, TYPE 1, CLOSED LID | EACH | 12 | 12 |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 8 | 8 |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 |
| 70102625 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701606 | L SUM | 1 | 1 |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1 | 1 |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 70 | 70 |
| ** 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 12 | 12 |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 4704 | 4704 |
| 70300210 | TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS | SO FT | 2964.8 | 2964.8 |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 119144 | 119144 |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 18660 | 18660 |
| 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 3000 | 3000 |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 7406 | 7406 |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 2610 | 2610 |

| SUMMARY OF QUANTITIES | | | URBAN 100% FED. | |
|-----------------------|--|-------|--------------------|------------------------|
| CODE NO. | ITEM DESCRIPTION | UNIT | TOTAL QUANTITY | CONSTRUCTION TYPE CODE |
| • 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SO FT | 1482.4 | 1482.4 |
| • 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 59572 | 59572 |
| • 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 9330 | 9330 |
| • 78000500 | THERMOPLASTIC PAVEMENT MARKING - LINE 8" | FOOT | 1500 | 1500 |
| • 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 3703 | 3703 |
| • 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 1305 | 1305 |
| • 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 1839 | 1839 |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 1839 | 1839 |
| • 88600600 | DETECTOR LOOP REPLACEMENT | FOOT | 3307 | 3307 |
| X0322256 | TEMPORARY INFORMATION SIGNING | SO FT | 308.4 | 308.4 |
| X4067107 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON | 4876 | 4876 |
| NP Z0018500 | DRAINAGE STRUCTURES TO BE CLEANED | EACH | 253 | 253 |
| Z0048665 | RAILROAD PROTECTIVE LIABILITY INSURANCE | L SUM | 1 | 1 |
| • Z0074100 | TRAINERS | HOUR | 1,000 | 1,000 |

• SPECIALTY ITEM
 ** INCLUDES QUANTITY FOR FOUR (4) MESSAGE SIGNS, 3 MONTHS EACH, LOCATION TO BE DETERMINED BY THE ENGINEER.
 NP = Non-participating
 © 1980

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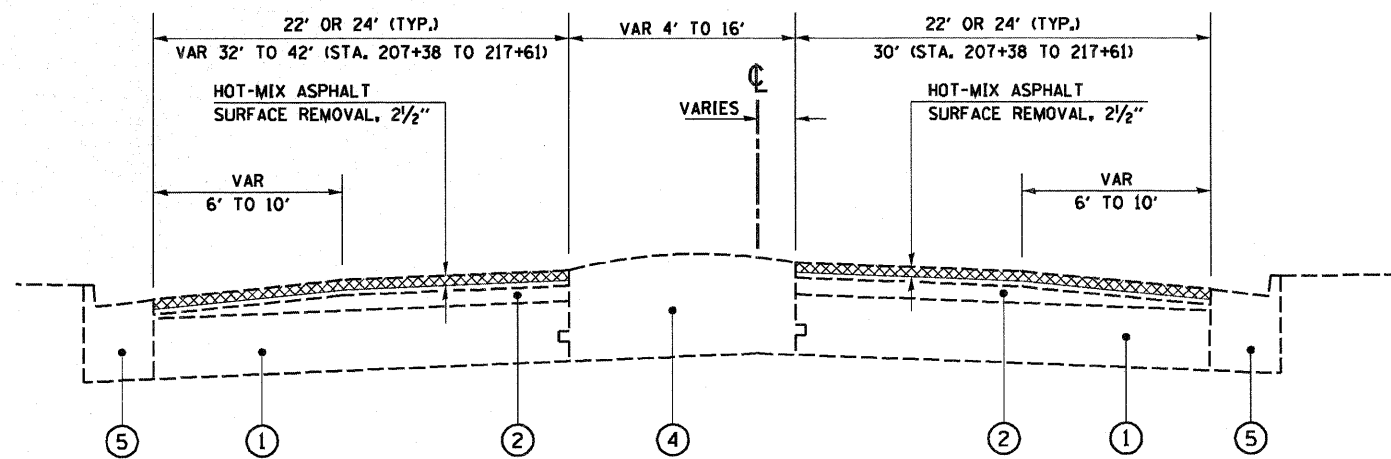
200 West Front Street
 Wheaton, IL 60187

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| DESIGNED - B.B. | REVISED - |
| DRAWN - J.T.T. | REVISED - |
| CHECKED - M.K. | REVISED - |
| DATE - 1/30/09 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

| | |
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| OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE) | |
| SUMMARY OF QUANTITIES | |
| SCALE: | SHEET NO. OF SHEETS STA. TO STA. |

| | | | | |
|---|---------------------------------|-------------|-----------------|-------------|
| F.A.U. RTE. 1332 | SECTION (S071,3071A & 3072RS-3) | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 3 |
| CONTRACT NO. 62755 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



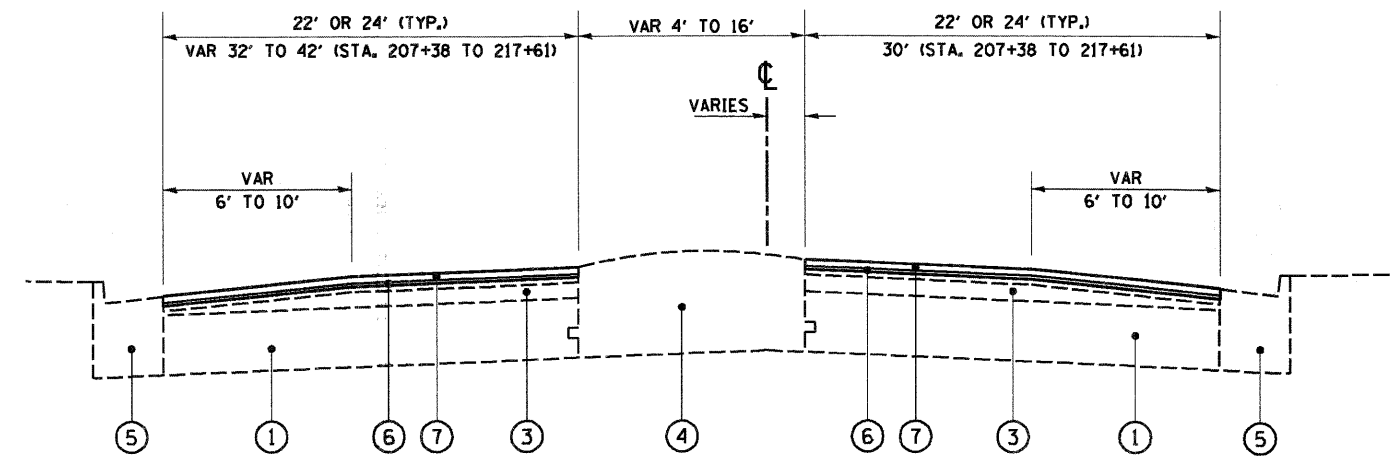
EXISTING TYPICAL SECTION - OAKTON STREET

STA. 55+56 TO STA. 59+58
 STA. 108+13 TO STA. 110+91
 STA. 198+20 TO STA. 199+23
 STA. 207+38 TO STA. 231+76

HOT-MIX ASPHALT MIXTURE

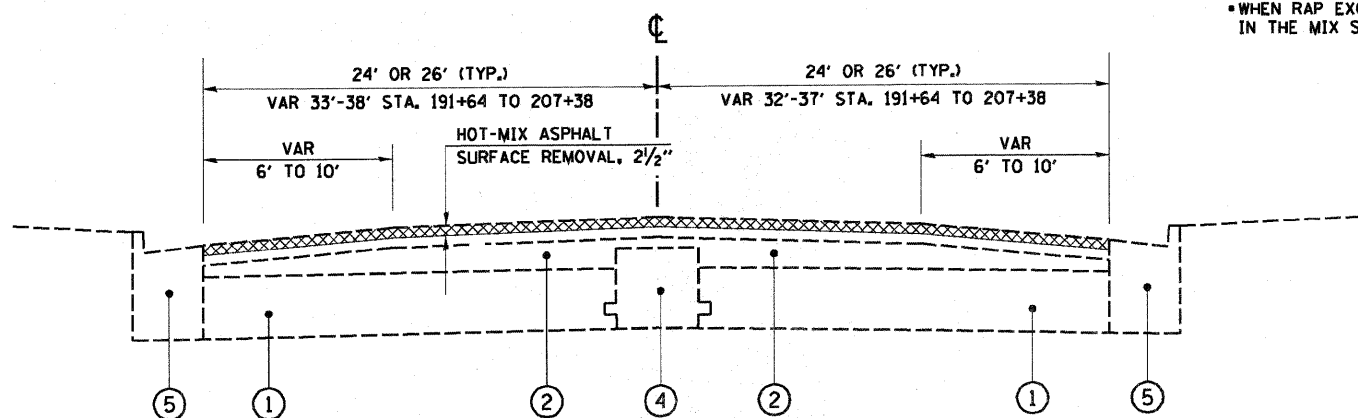
| MIXTURE | AC TYPE | AIR VOIDS (%) |
|--|----------------------|---------------|
| POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm) | SBS/SBR PG 70-22 | 4% @ 90 GYR. |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | SBS/SBR PG 76-28/-22 | 4% @ 50 GYR. |
| HMA REPLACEMENT OVER PATCHES (HMA BINDER, IL-19 mm) | PG 64-22* | 4% @ 70 GYR. |
| CLASS D PATCH (HMA BINDER, IL-19 mm) | PG 64-22* | 4% @ 70 GYR. |

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN
 *WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.



PROPOSED TYPICAL SECTION - OAKTON STREET

STA. 55+56 TO STA. 59+58
 STA. 108+13 TO STA. 110+91
 STA. 198+20 TO STA. 199+23
 STA. 207+38 TO STA. 231+76

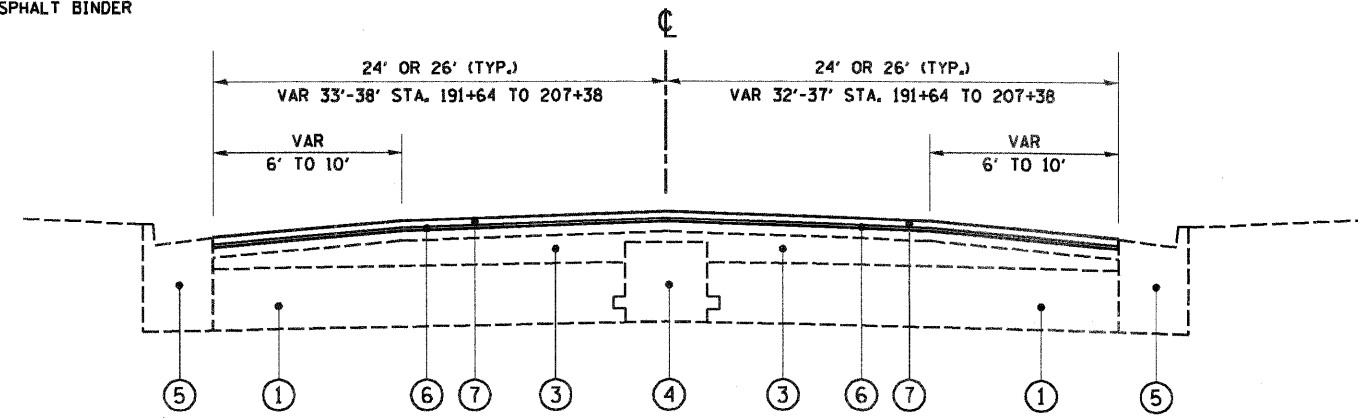


EXISTING TYPICAL SECTION - OAKTON STREET

STA. 6+12 TO STA. 34+80
 STA. 37+57 TO STA. 55+56
 STA. 105+56 TO STA. 108+13
 STA. 110+91 TO STA. 176+23
 STA. 178+16 TO STA. 198+20
 STA. 199+23 TO STA. 207+38

LEGEND

- ① EXISTING PCC BASE COURSE, 9"
- ② EXISTING HOT-MIX ASPHALT PAVEMENT (DEPTH VARIES ±3" TO 5 1/2") BEFORE SURFACE REMOVAL
- ③ EXISTING HOT-MIX ASPHALT PAVEMENT (DEPTH VARIES ±1 1/2" TO 3") AFTER SURFACE REMOVAL
- ④ EXISTING CONCRETE MEDIAN
- ⑤ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑥ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑦ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"



PROPOSED TYPICAL SECTION - OAKTON STREET

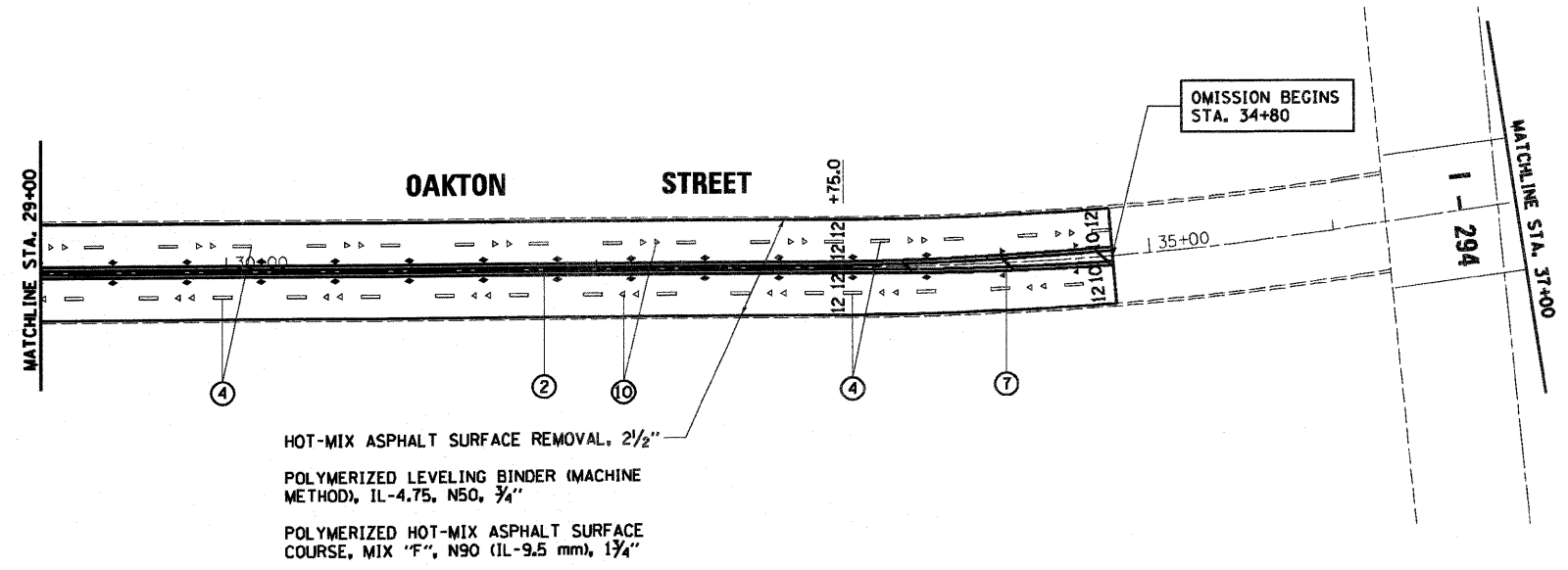
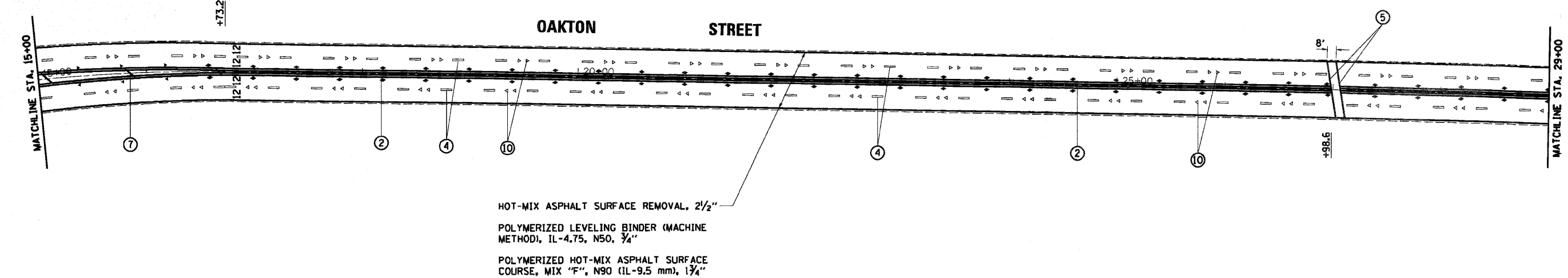
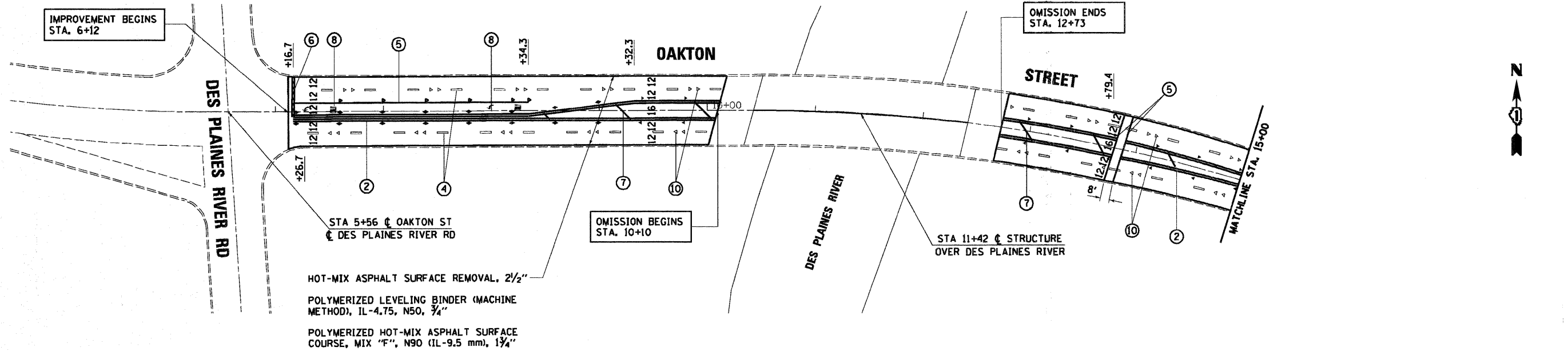
STA. 6+12 TO STA. 34+80
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 STA. 105+56 TO STA. 108+13
 STA. 110+91 TO STA. 176+23
 STA. 178+16 TO STA. 198+20
 STA. 199+23 TO STA. 207+38

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| DRAWN - J.T.T. | REVISED - |
| CHECKED - M.K. | REVISED - |
| DATE - 1/30/09 | REVISED - |

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| OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE) | | | |
| TYPICAL SECTIONS | | | |
| SCALE: | SHEET NO. | OF SHEETS | STA. TO STA. |

| | | | | |
|---|---------------------------------|-------------|-----------------|-------------|
| F.A.J. RTE. 1332 | SECTION (3071,3071A & 3072)RS-3 | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 4 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 mm), 1 3/4"

HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 mm), 1 3/4"

HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 mm), 1 3/4"

THERMOPLASTIC PAVEMENT MARKING LEGEND

- ① 4" SINGLE SOLID YELLOW LINE
 - ② 4" DOUBLE SOLID YELLOW LINE (11" C-C)
 - ③ 6" DOTTED WHITE LINE (2' LINE, 6' SPACE)
 - ④ 4" WHITE LANE LINE (10' LINE, 30' SPACE)
 - ⑤ 6" SOLID WHITE LINE
 - ⑥ 24" SOLID WHITE STOP BAR
 - ⑦ 12" SOLID YELLOW DIAGONAL LINES
 - ⑧ LETTERS AND SYMBOLS
 - ⑨ SCHOOL CROSSWALK WHITE 6 FOOT WIDE LINES (12" LINE, 24" SPACE)
 - ⑩ RAISED REFLECTIVE PAVEMENT MARKERS
- ▷ ONE-WAY CRYSTAL ▷ ONE-WAY AMBER ◀ TWO-WAY AMBER

PLOT DATE = 2/1/2009
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 NCIDEL = Default



200 West Front Street
 Wheaton, IL 60187

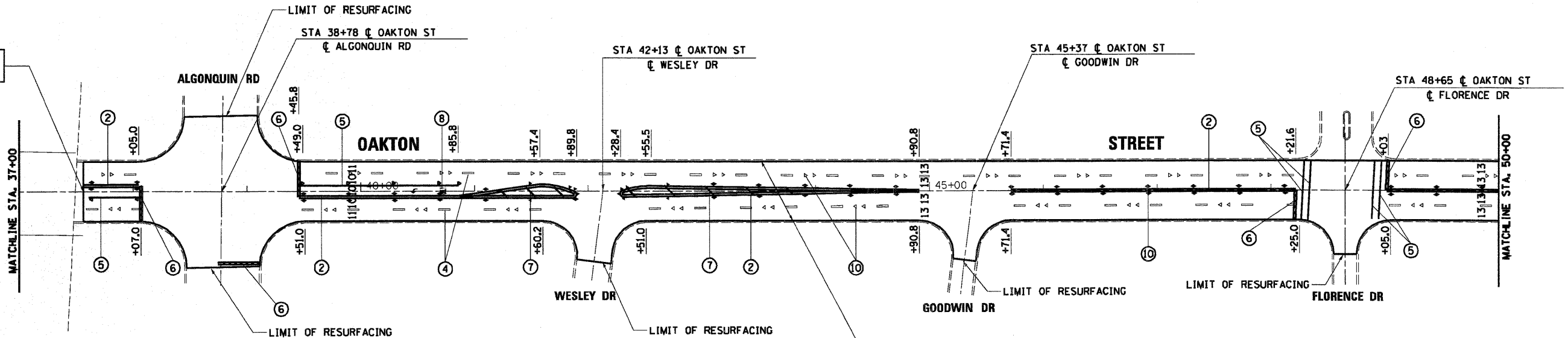
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| DATE - 1/30/09 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

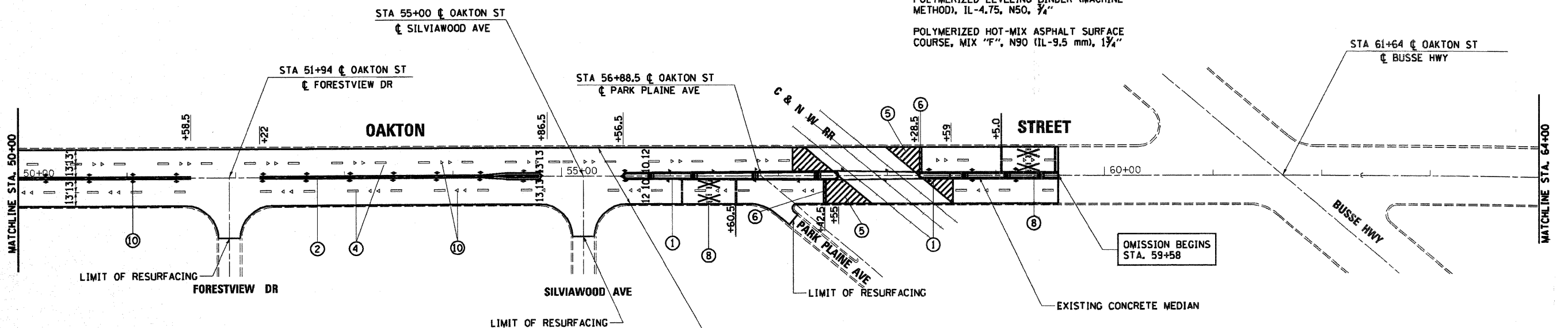
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|---|-----------|-----------|-------------------------|
| OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE) ROADWAY AND PAVEMENT MARKING PLAN | | | |
| SCALE: | SHEET NO. | OF SHEETS | STA. 6+12 TO STA. 37+00 |

| | | | | |
|---|---------------------------------|-------------|-----------------|-------------|
| F.A.U. RTE. 1332 | SECTION (3071,3071A & 3072)R5-3 | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 5 |
| CONTRACT NO. 62755 | | | | |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | | |

OMISSION ENDS STA. 37+57



HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 mm), 1 3/4"



HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 mm), 1 3/4"

THERMOPLASTIC PAVEMENT MARKING LEGEND

- ① 4" SINGLE SOLID YELLOW LINE
- ② 4" DOUBLE SOLID YELLOW LINE (11" C-C)
- ③ 6" DOTTED WHITE LINE (2' LINE, 6' SPACE)
- ④ 4" WHITE LANE LINE (10' LINE, 30' SPACE)
- ⑤ 6" SOLID WHITE LINE
- ⑥ 24" SOLID WHITE STOP BAR
- ⑦ 12" SOLID YELLOW DIAGONAL LINES
- ⑧ LETTERS AND SYMBOLS
- ⑨ SCHOOL CROSSWALK WHITE 6 FOOT WIDE LINES (12" LINE, 24" SPACE)
- ⑩ RAISED REFLECTIVE PAVEMENT MARKERS

▷ ONE-WAY CRYSTAL ▷ ONE-WAY AMBER ◄ TWO-WAY AMBER

PLOT DATE = 2/9/2009
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 PLOT DEVICE = rjnplotter
 MODEL = Default



200 West Front Street
 Wheaton, IL 60187

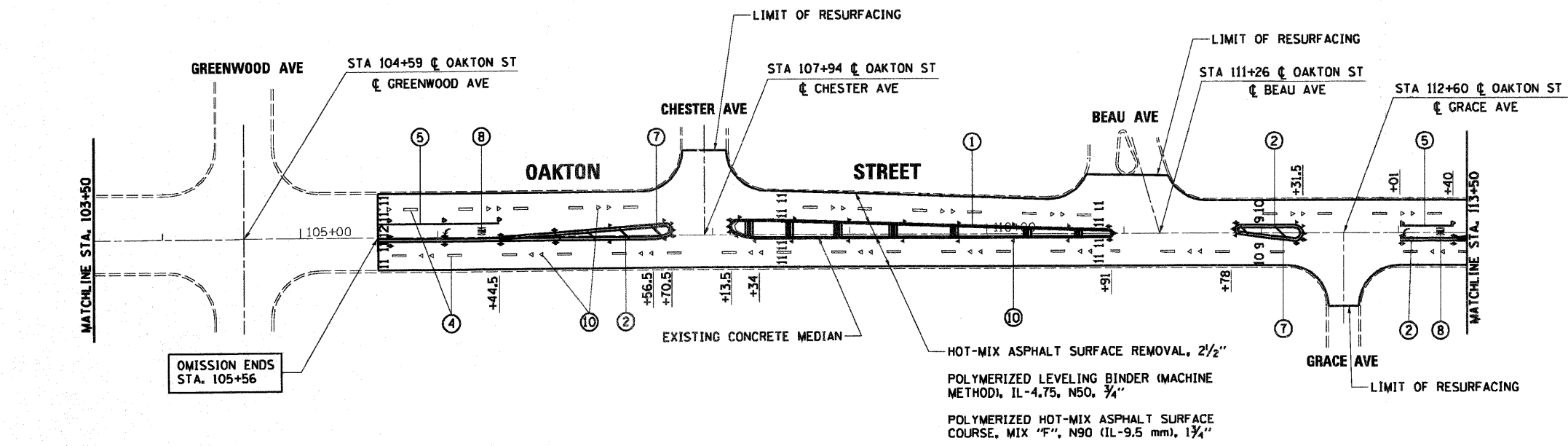
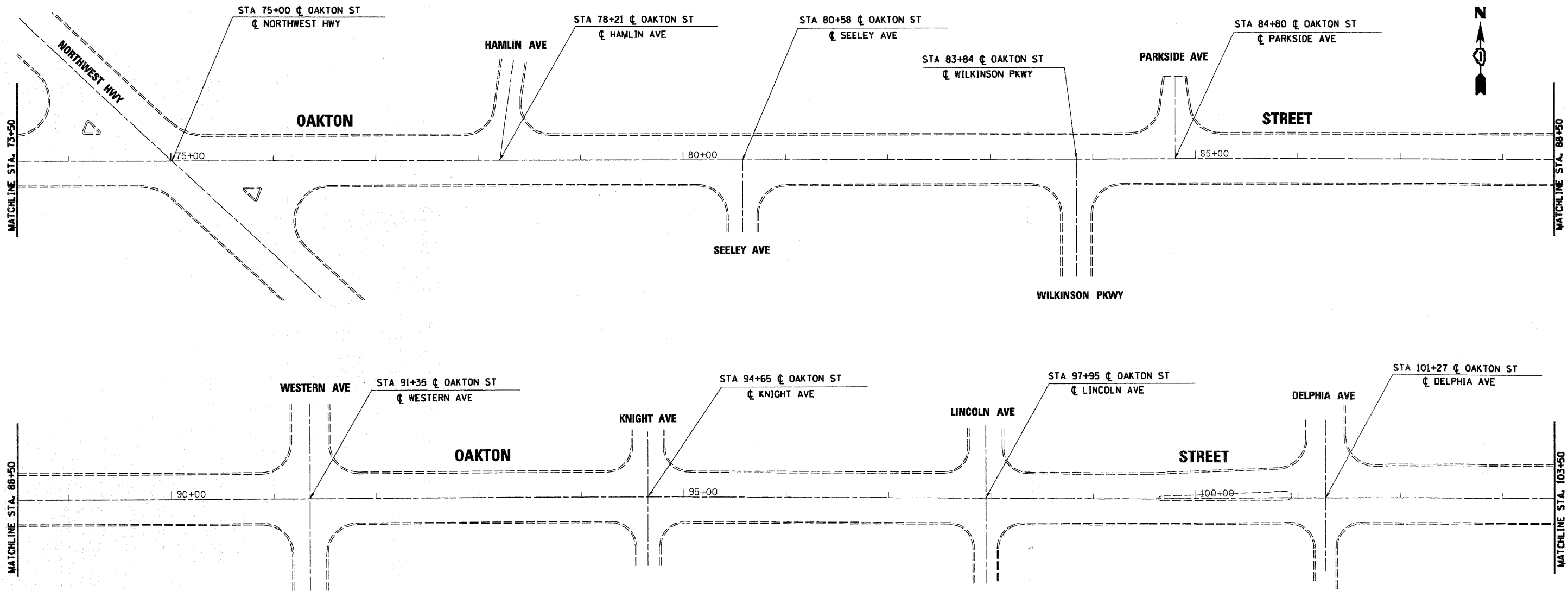
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|-----------------|-----------|
| DESIGNED - B.B. | REVISED - |
| DRAWN - J.T.T. | REVISED - |
| CHECKED - M.K. | REVISED - |
| DATE - 1/30/09 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE)
 ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: SHEET NO. OF SHEETS STA. 37+00 TO STA. 73+50

| | | | | |
|---|---------------------------------|-------------|-----------------|-------------|
| F.A.J. RTE. 1332 | SECTION (3071,3071A & 3072)RS-3 | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 6 |
| CONTRACT NO. 62755 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



THERMOPLASTIC PAVEMENT MARKING LEGEND

- ① 4" SINGLE SOLID YELLOW LINE
 - ② 4" DOUBLE SOLID YELLOW LINE (11" C-C)
 - ③ 6" DOTTED WHITE LINE (2' LINE, 6' SPACE)
 - ④ 4" WHITE LANE LINE (10' LINE, 30' SPACE)
 - ⑤ 6" SOLID WHITE LINE
 - ⑥ 24" SOLID WHITE STOP BAR
 - ⑦ 12" SOLID YELLOW DIAGONAL LINES
 - ⑧ LETTERS AND SYMBOLS
 - ⑨ SCHOOL CROSSWALK WHITE 6 FOOT WIDE LINES (12" LINE, 24" SPACE)
 - ⑩ RAISED REFLECTIVE PAVEMENT MARKERS
- ▷ ONE-WAY CRYSTAL ▷ ONE-WAY AMBER ◈ TWO-WAY AMBER

PLOT DATE = 2/1/2009
 FILE NAME = K:\11182534\Road\design\182518p_06.dgn
 USER NAME = JTP
 MODEL = Default



200 West Front Street
Wheaton, IL 60187

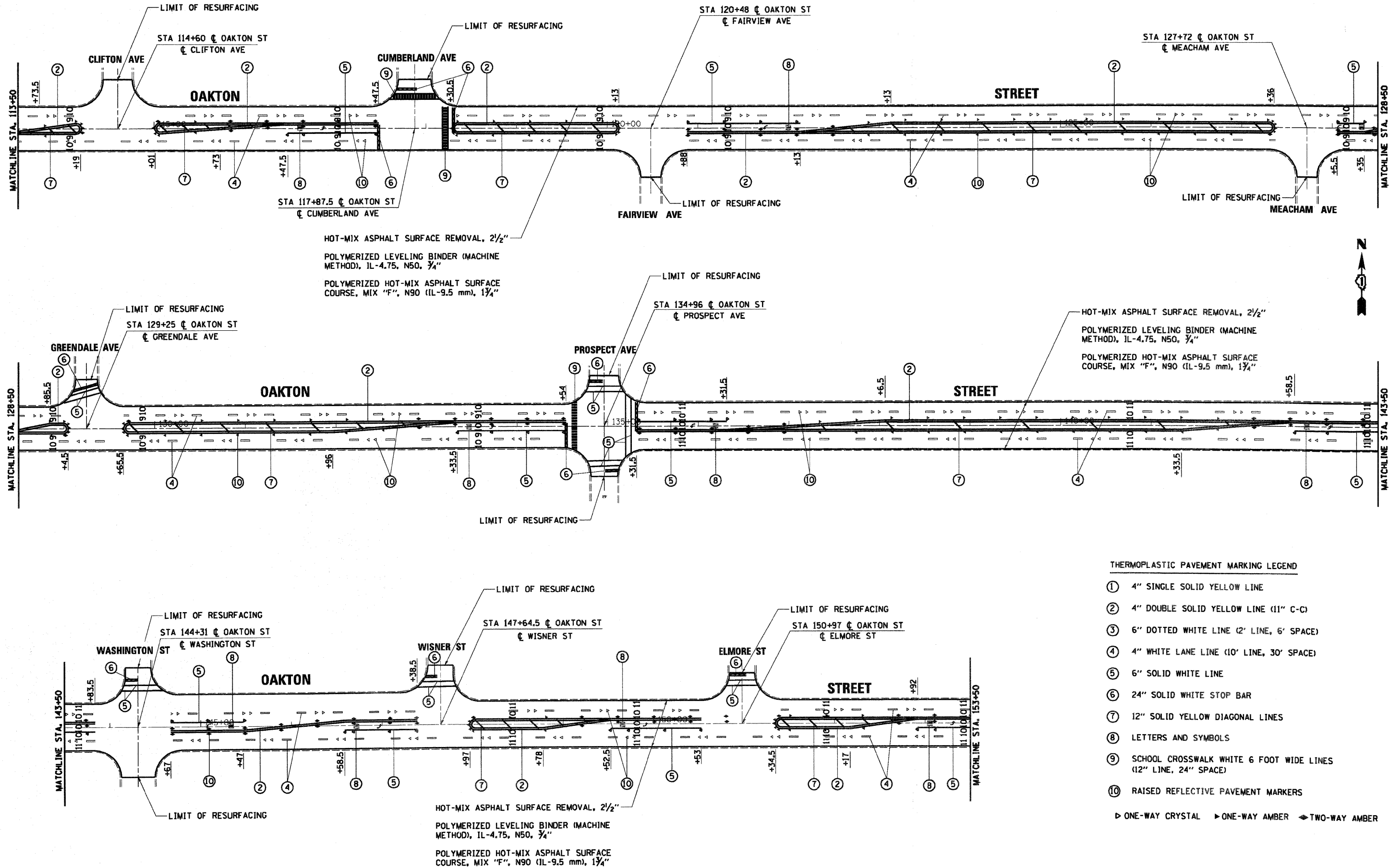
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|------------|---------|-----------|--|
| DESIGNED - | B.B. | REVISED - | |
| DRAWN - | J.T.T. | REVISED - | |
| CHECKED - | M.J.K. | REVISED - | |
| DATE - | 1/30/09 | REVISED - | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE)
ROADWAY AND PAVEMENT MARKING PLAN**

| | | | | | |
|--------|-----------|----|--------|------------|----------------|
| SCALE: | SHEET NO. | OF | SHEETS | STA. 73+50 | TO STA. 113+50 |
|--------|-----------|----|--------|------------|----------------|

| | | | | |
|---|-------------------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1332 | (3071.3071A & 3072RS-3) | COOK | 38 | 7 |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | | |
| CONTRACT NO. 62755 | | | | |



THERMOPLASTIC PAVEMENT MARKING LEGEND

- ① 4" SINGLE SOLID YELLOW LINE
 - ② 4" DOUBLE SOLID YELLOW LINE (11" C-C)
 - ③ 6" DOTTED WHITE LINE (2' LINE, 6' SPACE)
 - ④ 4" WHITE LANE LINE (10' LINE, 30' SPACE)
 - ⑤ 6" SOLID WHITE LINE
 - ⑥ 24" SOLID WHITE STOP BAR
 - ⑦ 12" SOLID YELLOW DIAGONAL LINES
 - ⑧ LETTERS AND SYMBOLS
 - ⑨ SCHOOL CROSSWALK WHITE 6 FOOT WIDE LINES (12" LINE, 24" SPACE)
 - ⑩ RAISED REFLECTIVE PAVEMENT MARKERS
- ▷ ONE-WAY CRYSTAL ▷ ONE-WAY AMBER ◄ TWO-WAY AMBER

PLOT DATE = 2/9/2009
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 USER NAME = J.T.T.
 MODEL = Default



200 West Front Street
Wheaton, IL 60187

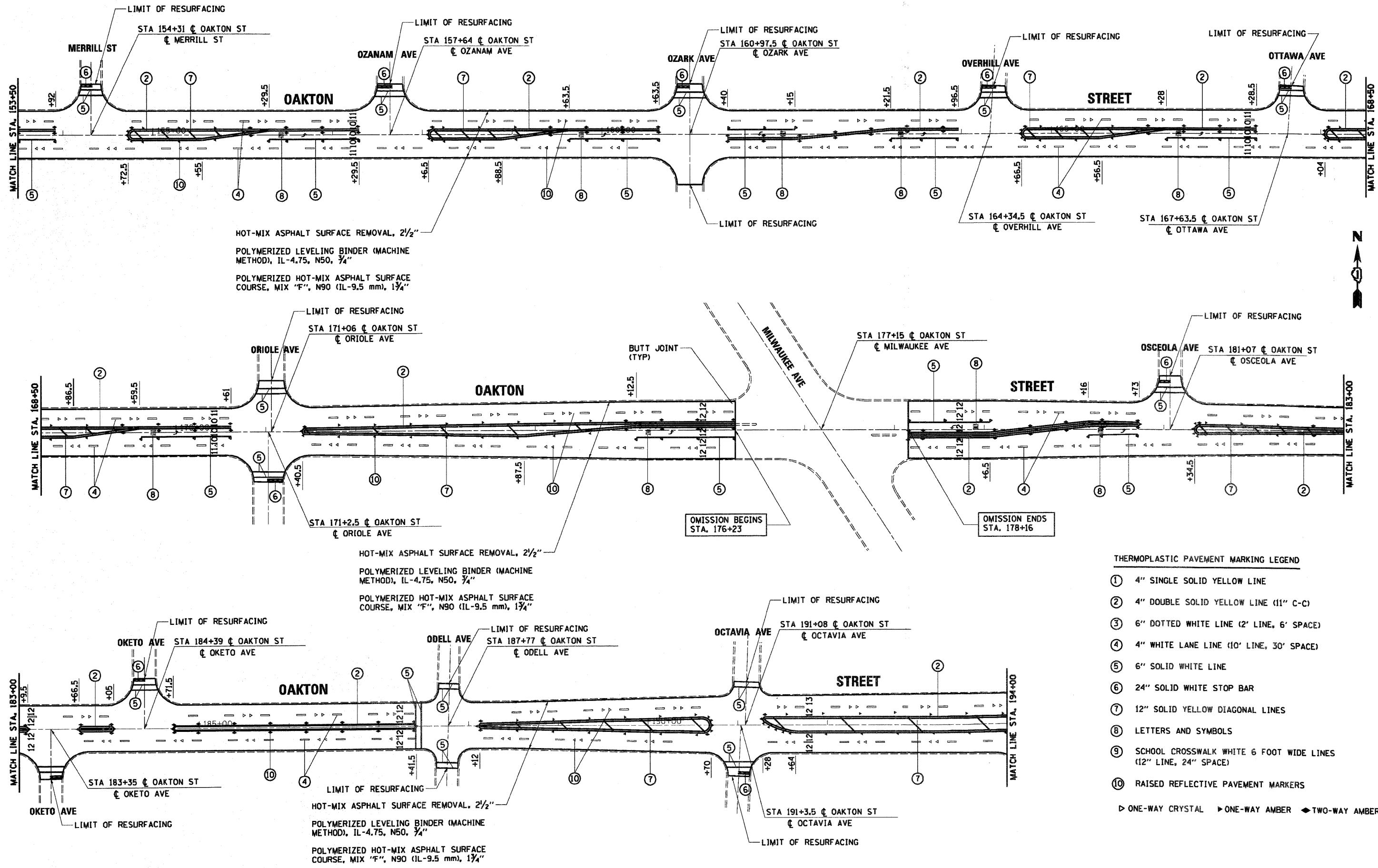
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|-----------------|-----------|
| DESIGNED - B.B. | REVISED - |
| DRAWN - J.T.T. | REVISED - |
| CHECKED - M.K. | REVISED - |
| DATE - 1/30/09 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE)
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: SHEET NO. OF SHEETS STA. 113+50 TO STA. 153+50

| | | | | |
|---|--------------------------------|-------------|-----------------|-------------|
| F.A.U. RTE. 1332 | SECTION (307L3071A & 3072RS-3) | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 8 |
| CONTRACT NO. 62755 | | | | |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | | |



HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 mm), 1 3/4"

HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 mm), 1 3/4"

HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 mm), 1 3/4"

- THERMOPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" SINGLE SOLID YELLOW LINE
 - ② 4" DOUBLE SOLID YELLOW LINE (11" C-C)
 - ③ 6" DOTTED WHITE LINE (2' LINE, 6' SPACE)
 - ④ 4" WHITE LANE LINE (10' LINE, 30' SPACE)
 - ⑤ 6" SOLID WHITE LINE
 - ⑥ 24" SOLID WHITE STOP BAR
 - ⑦ 12" SOLID YELLOW DIAGONAL LINES
 - ⑧ LETTERS AND SYMBOLS
 - ⑨ SCHOOL CROSSWALK WHITE 6 FOOT WIDE LINES (12" LINE, 24" SPACE)
 - ⑩ RAISED REFLECTIVE PAVEMENT MARKERS
- ▷ ONE-WAY CRYSTAL ▷ ONE-WAY AMBER ◄ TWO-WAY AMBER

PLOT DATE = 2/9/2009
 FILE NAME = K:\1182534\Road\Design\182518p_06.dgn
 USER NAME = JTracy
 MODEL = Default



200 West Front Street
 Wheaton, IL 60187

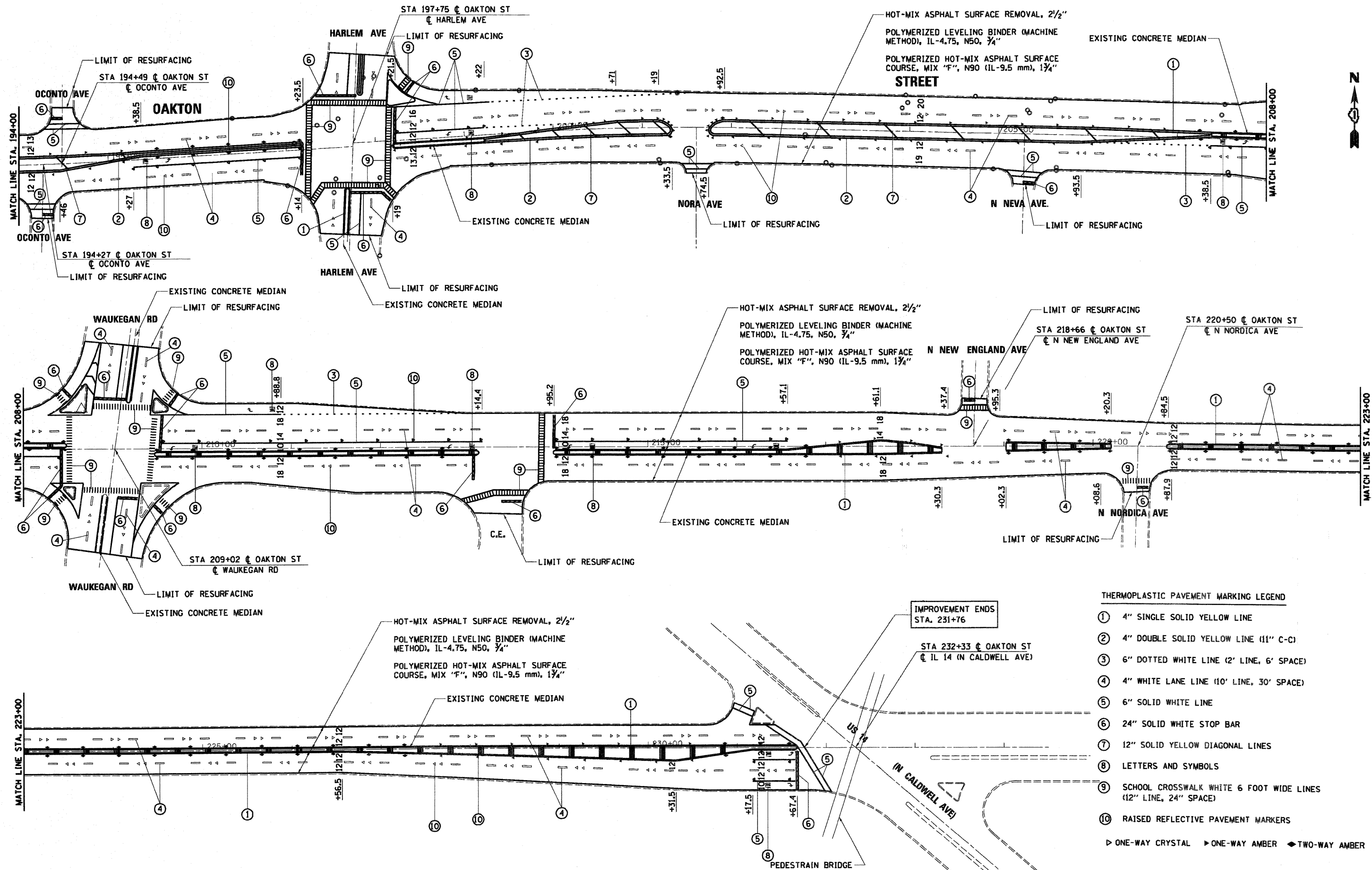
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|----------|-----------|---------|---|
| DESIGNED | - B.B. | REVISED | - |
| DRAWN | - J.T.T. | REVISED | - |
| CHECKED | - M.K. | REVISED | - |
| DATE | - 1/30/09 | REVISED | - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE)
 ROADWAY AND PAVEMENT MARKING PLAN

| | | | | | |
|--------|-----------|----|--------|-------------|----------------|
| SCALE: | SHEET NO. | OF | SHEETS | STA. 153+50 | TO STA. 194+00 |
|--------|-----------|----|--------|-------------|----------------|

| | | | | |
|---|-------------------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1332 | (3071,3071A & 3072)RS-3 | COOK | 38 | 9 |
| CONTRACT NO. 62755 | | | | |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | | |



- THERMOPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" SINGLE SOLID YELLOW LINE
 - ② 4" DOUBLE SOLID YELLOW LINE (11" C-C)
 - ③ 6" DOTTED WHITE LINE (2' LINE, 6' SPACE)
 - ④ 4" WHITE LANE LINE (10' LINE, 30' SPACE)
 - ⑤ 6" SOLID WHITE LINE
 - ⑥ 24" SOLID WHITE STOP BAR
 - ⑦ 12" SOLID YELLOW DIAGONAL LINES
 - ⑧ LETTERS AND SYMBOLS
 - ⑨ SCHOOL CROSSWALK WHITE 6 FOOT WIDE LINES (12" LINE, 24" SPACE)
 - ⑩ RAISED REFLECTIVE PAVEMENT MARKERS
- ▷ ONE-WAY CRYSTAL ▷ ONE-WAY AMBER ◀ TWO-WAY AMBER

PLOT DATE = 2/19/2009
 FILE NAME = I:\1182534\Road\Design\182510p_06.dgn
 USER = JTF
 MODEL = Default



200 West Front Street
Wheaton, IL 60187

| | |
|-----------------|-----------|
| DESIGNED - B.B. | REVISED - |
| DRAWN - J.T.T. | REVISED - |
| CHECKED - M.K. | REVISED - |
| DATE - 1/30/09 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

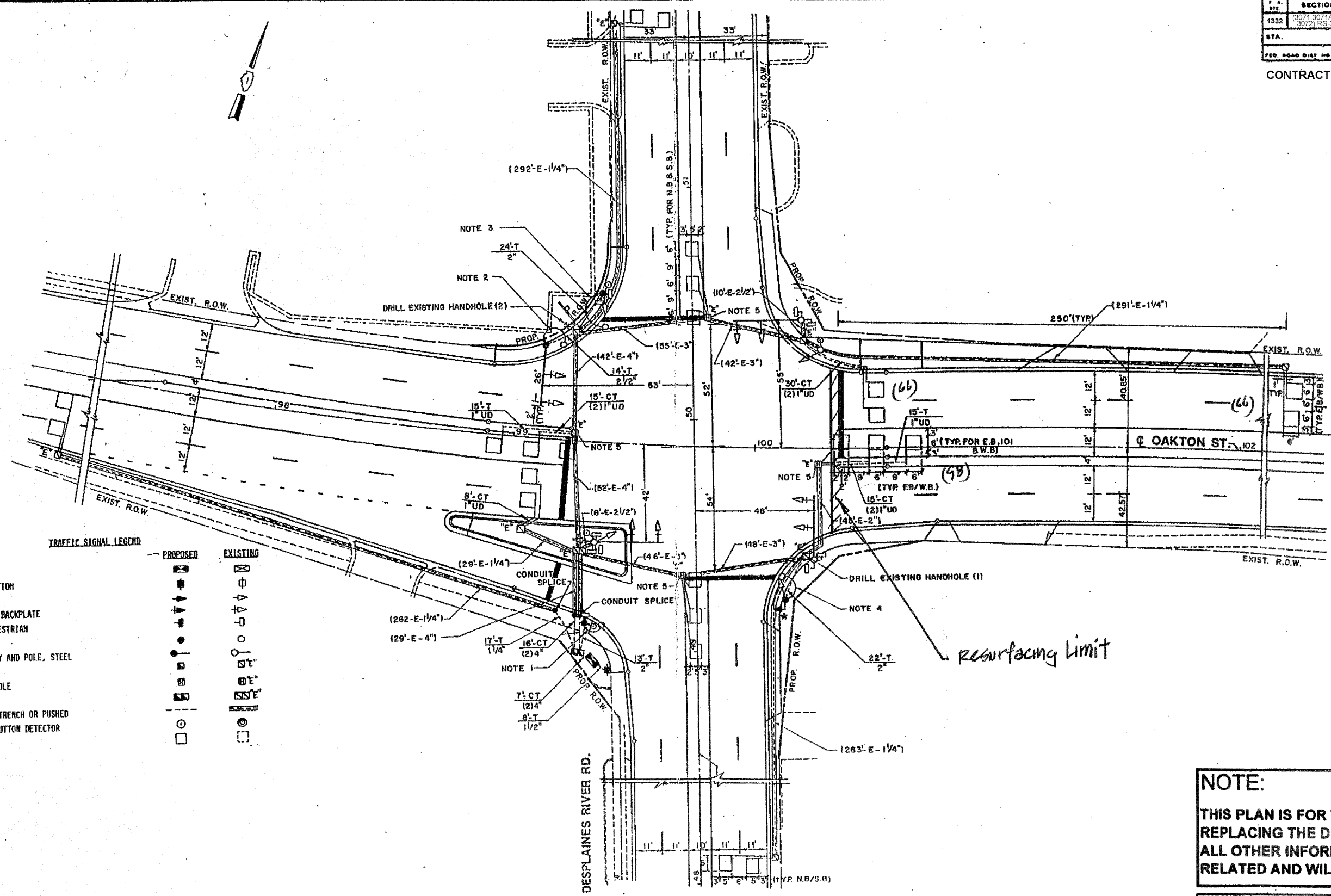
**OAKTON STREET (DES PLAINES RIVER ROAD TO N CALDWELL AVE)
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: SHEET NO. OF SHEETS STA. 194+00 TO STA. 231+76

| | | | | |
|---|---------------------------------|-------------|-----------------|--------------|
| F.A.U. RTE. 1332 | SECTION (307L307IA & 3072IRS-3) | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 10 |
| CONTRACT NO. 62755 | | | | |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | | |

| P.A. SITE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------------------------|----------|--------------|------------------|
| 1332 | (3071, 3071A & 3072) RS-3 | Cook | 38 | 11 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | | FED. AID PROJECT |

CONTRACT NO. 62755



TRAFFIC SIGNAL LEGEND

| PROPOSED | EXISTING |
|----------|----------|
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REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600600 | 130 | Foot | Detector Loop Replacement |

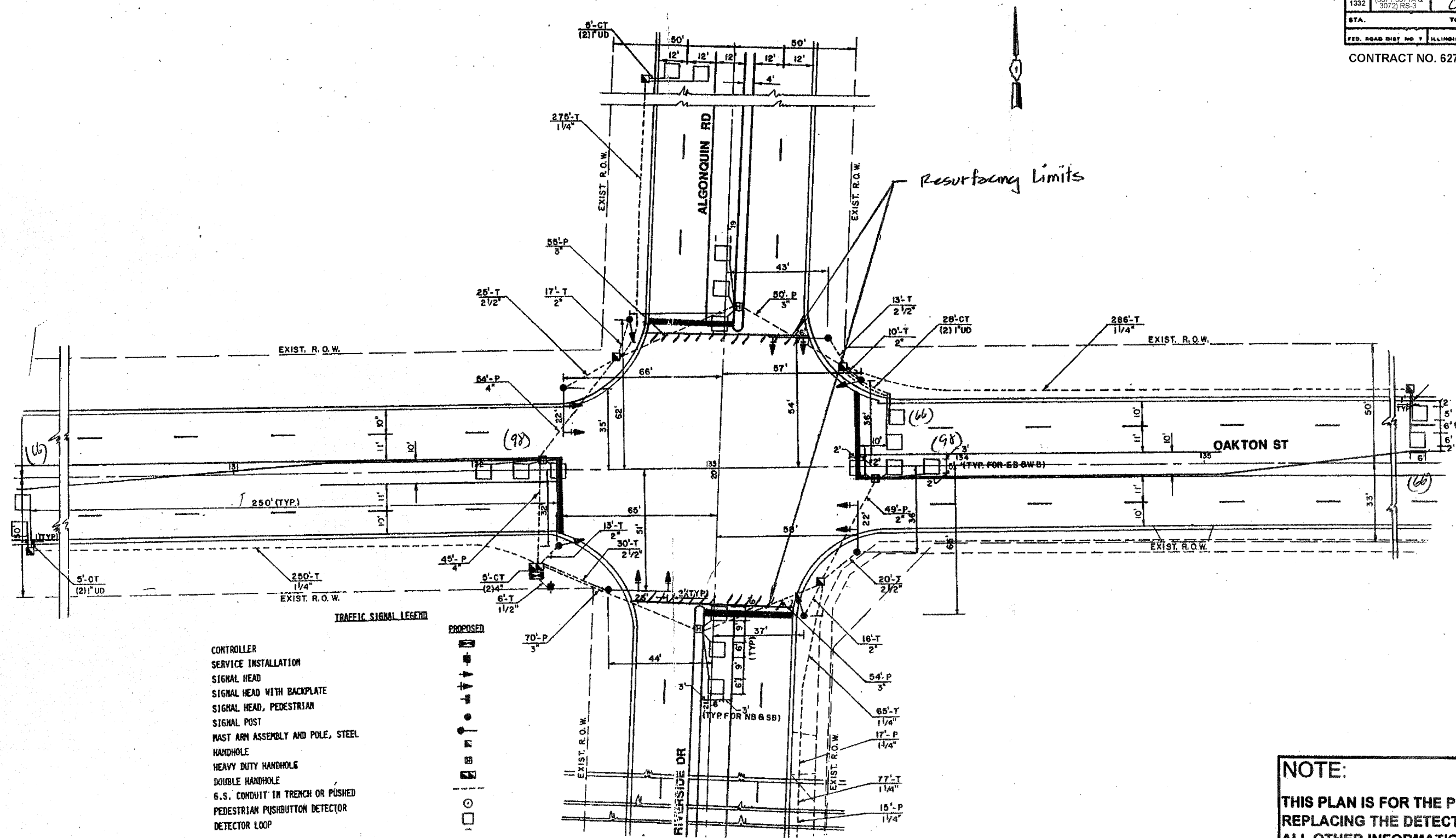
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
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NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
OAKTON ST. @ DES PLAINES R. RD.
SCALE: NONE
DATE: JAN 09
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

| F.A. SITE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------------|---------------------------|----------|--------------|------------------|
| 1332 | (3071, 3071A & 3072) RS-3 | COOK | 38 | 12 |
| STA. | | TO STA. | | |
| FED. ROAD DIST NO. 7 | | ILLINOIS | | FED. AID PROJECT |

CONTRACT NO. 62755



- TRAFFIC SIGNAL LEGEND**
- PROPOSED**
- CONTROLLER
 - SERVICE INSTALLATION
 - SIGNAL HEAD
 - SIGNAL HEAD WITH BACKPLATE
 - SIGNAL HEAD, PEDESTRIAN
 - SIGNAL POST
 - MAST ARM ASSEMBLY AND POLE, STEEL
 - HANDHOLE
 - HEAVY DUTY HANDHOLE
 - DOUBLE HANDHOLE
 - G.S. CONDUIT IN TRENCH OR PUSHED
 - PEDESTRIAN PUSHBUTTON DETECTOR
 - DETECTOR LOOP

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

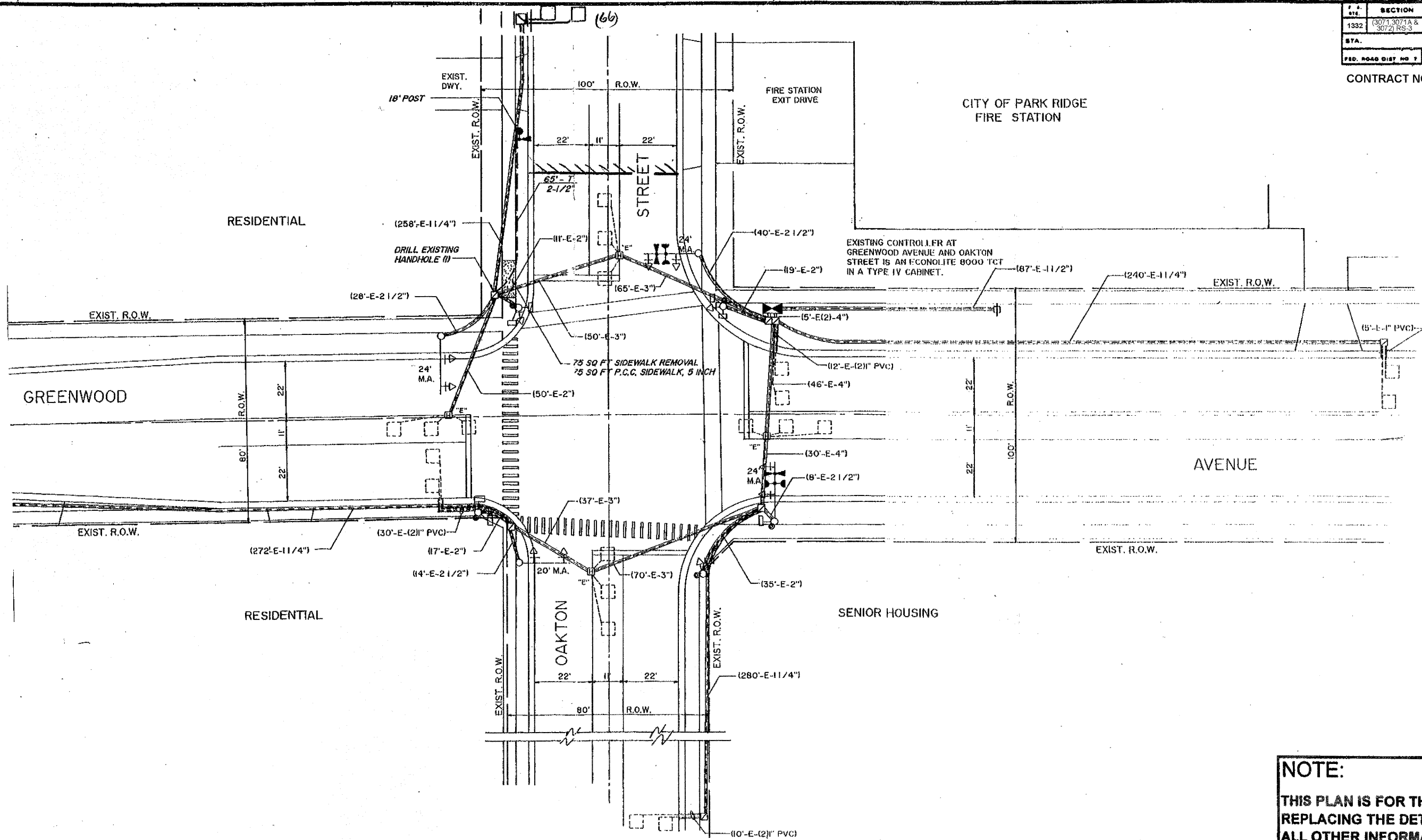
| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600600 | 394 | Foot | Detector Loop Replacement |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
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| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 OAKTON ST. @ ALGONQUIN RD.
 SCALE: NONE
 DATE: JAN, 2009
 DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: DAD

| SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------------|--------|--------------------------------|-----------|
| 1332 (3071, 3071A & 3072) RS-3 | Cook | 38 | 13 |
| STA. TO STA. | | FED. ROAD DIST. NO. 7 ILLINOIS | |
| | | FED. AID PROJECT | |

CONTRACT NO. 62755



REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600600 | 66 | Foot | Detector Loop Replacement |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
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NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

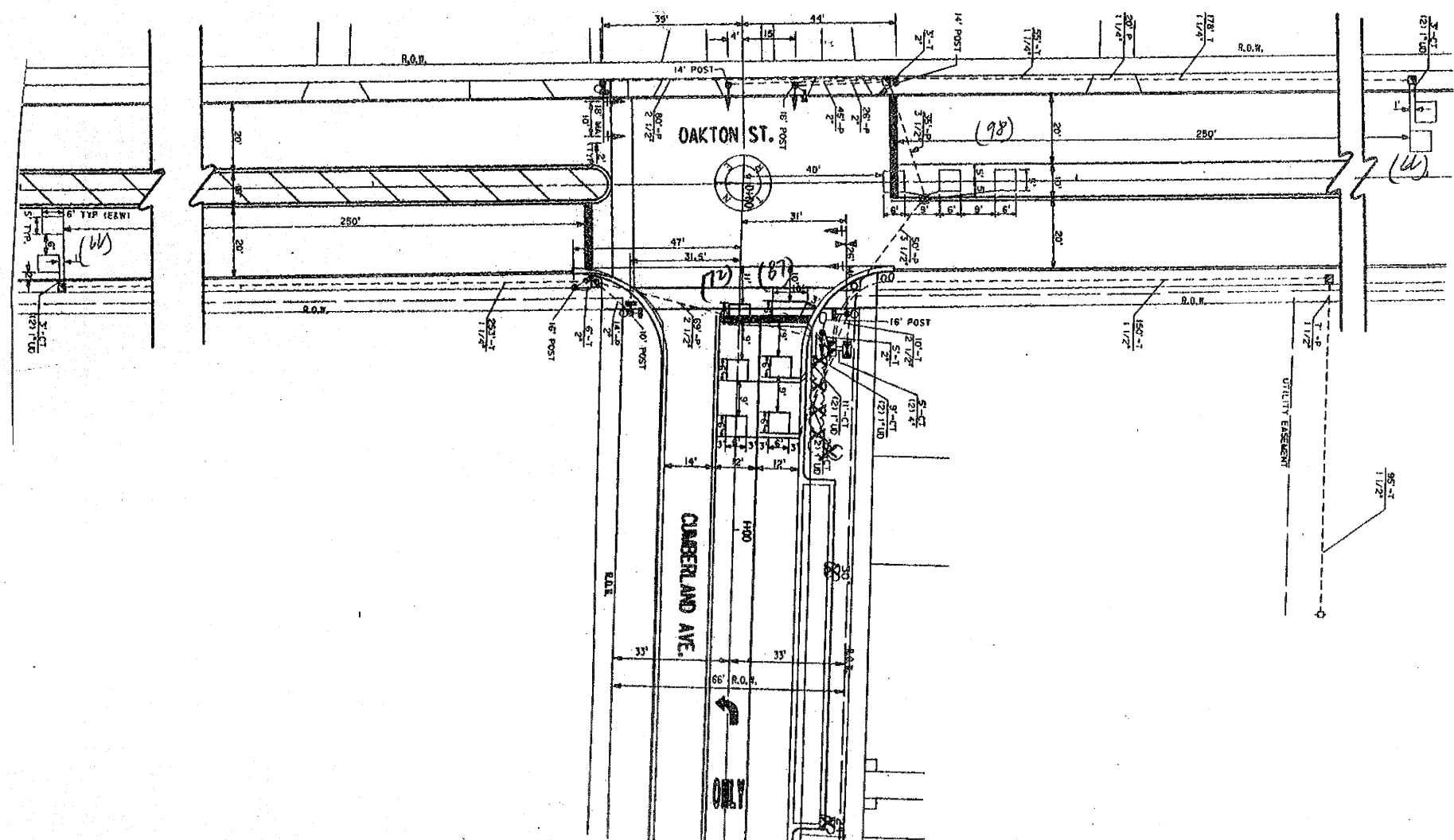
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 OAKTON ST. @ GREENWOOD AVE.
 SCALE: NONE
 DATE: JAN. 09
 DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: DAD

| SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------------|----------|------------------|-----------|
| 1332 (3071, 3071A & 3072) RS-3 | Cook | 38 | 14 |
| STA. | TO STA. | | |
| FED. ROAD DIST. NO. 3 | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 62755

TRAFFIC SIGNAL LEGEND

| | PROPOSED | EXISTING |
|-----------------------------------|----------|----------|
| CONTROLLER | | |
| SERVICE INSTALLATION | | |
| SIGNAL HEAD | | |
| SIGNAL HEAD WITH BACKPLATE | | |
| SIGNAL HEAD, PEDESTRIAN | | |
| SIGNAL POST | | |
| MAST ARM ASSEMBLY AND POLE, STEEL | | |
| HANDHOLE | | |
| HEAVY DUTY HANDHOLE | | |
| DOUBLE HANDHOLE | | |
| G.S. CONDUIT IN TRENCH OR PUSHED | | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | |
| DETECTOR LOOP | | |



REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600600 | 389 | Foot | Detector Loop Replacement |

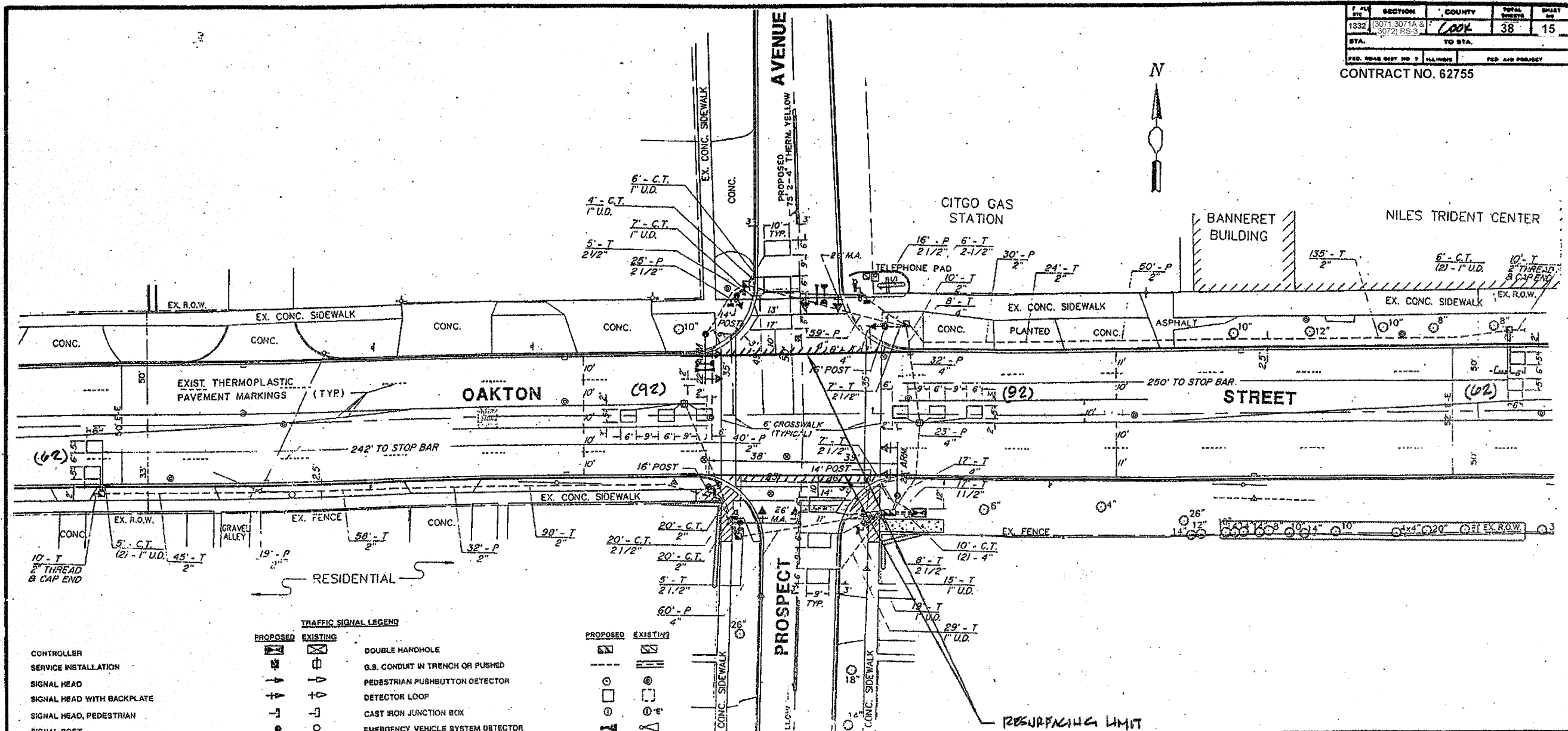
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
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| | |

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 OAKTON ST. @ CUMBERLAND AV.
 SCALE: NONE
 DATE: JAN, 09
 DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: DAD

| | | | | |
|-----------------------|---------------------------|---------|------------------|-----------|
| F. FILE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1332 | (3071, 3071A & 3072) RS-3 | COOK | 38 | 15 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. 7 | | MILEAGE | FED. AID PROJECT | |

CONTRACT NO. 62755



TRAFFIC SIGNAL LEGEND

| PROPOSED | EXISTING | DESCRIPTION |
|----------|----------|--|
| [Symbol] | [Symbol] | DOUBLE HANDHOLE |
| [Symbol] | [Symbol] | G.S. CONDUIT IN TRENCH OR PUSHED |
| [Symbol] | [Symbol] | PEDESTRIAN PUSHBUTTON DETECTOR |
| [Symbol] | [Symbol] | DETECTOR LOOP |
| [Symbol] | [Symbol] | CAST IRON JUNCTION BOX |
| [Symbol] | [Symbol] | EMERGENCY VEHICLE SYSTEM DETECTOR |
| [Symbol] | [Symbol] | CONFIRMATION BEACON |
| [Symbol] | [Symbol] | SIGNAL HEAD OPTICALLY PROGRAMMED |
| [Symbol] | [Symbol] | CONDUIT SPLICE |
| [Symbol] | [Symbol] | WOOD POLE |
| [Symbol] | [Symbol] | RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II |
| [Symbol] | [Symbol] | VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE |
| [Symbol] | [Symbol] | RAILROAD CONTROL CABINET |

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 86600600 | 303 | Foot | Detector Loop Replacement |

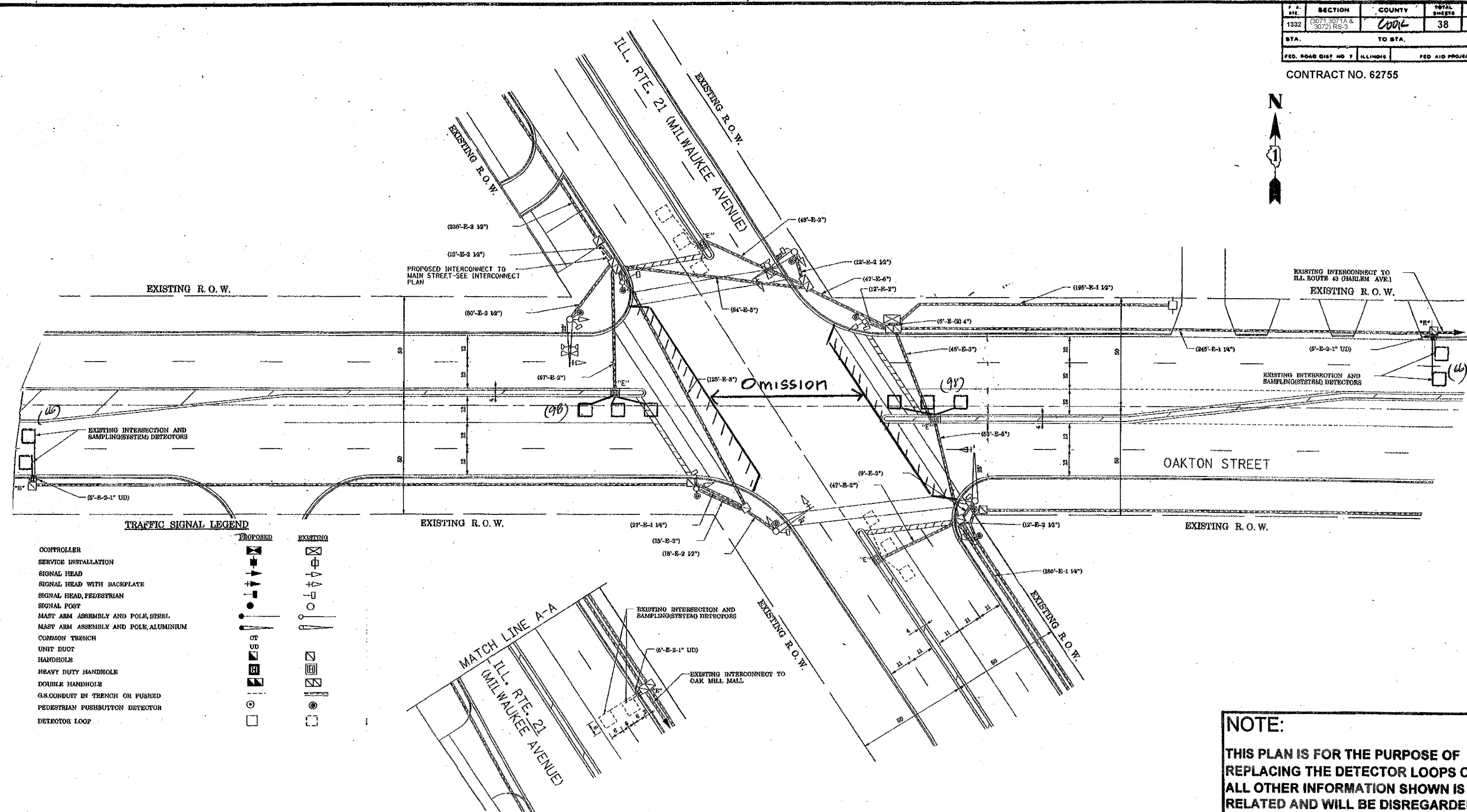
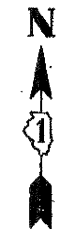
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
OAKTON STREET @ PROSPECT AVE.
SCALE: 1" = 20'
DATE: MAR. 04
DRAWN BY: _____
DESIGNED BY: _____
CHECKED BY: _____

| F.P. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------------------------|----------|--------------|------------------|
| 1332 | (3071, 3071A & 3072) RS-3 | COOK | 38 | 16 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | | FED. AID PROJECT |

CONTRACT NO. 62755



TRAFFIC SIGNAL LEGEND

- | | |
|--------------------------------------|--------------------------------------|
| PROPOSED | EXISTING |
| CONTROLLER | CONTROLLER |
| SERVICE INSTALLATION | SERVICE INSTALLATION |
| SIGNAL HEAD | SIGNAL HEAD |
| SIGNAL HEAD WITH BACKPLATE | SIGNAL HEAD WITH BACKPLATE |
| SIGNAL HEAD, PEDESTRIAN | SIGNAL HEAD, PEDESTRIAN |
| SIGNAL POST | SIGNAL POST |
| MAST ARM ASSEMBLY AND POLE, STEEL | MAST ARM ASSEMBLY AND POLE, STEEL |
| MAST ARM ASSEMBLY AND POLE, ALUMINUM | MAST ARM ASSEMBLY AND POLE, ALUMINUM |
| COMMON TRENCH | COMMON TRENCH |
| UNIT DUCT | UNIT DUCT |
| HANDHOLE | HANDHOLE |
| HEAVY DUTY HANDHOLE | HEAVY DUTY HANDHOLE |
| DOUBLE HANDHOLE | DOUBLE HANDHOLE |
| G.S.CONDUIT IN TRENCH OR PUSHED | G.S.CONDUIT IN TRENCH OR PUSHED |
| PEDESTRIAN PUSHBUTTON DETECTOR | PEDESTRIAN PUSHBUTTON DETECTOR |
| DETECTOR LOOP | DETECTOR LOOP |

REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600600 | 328 | Foot | Detector Loop Replacement |

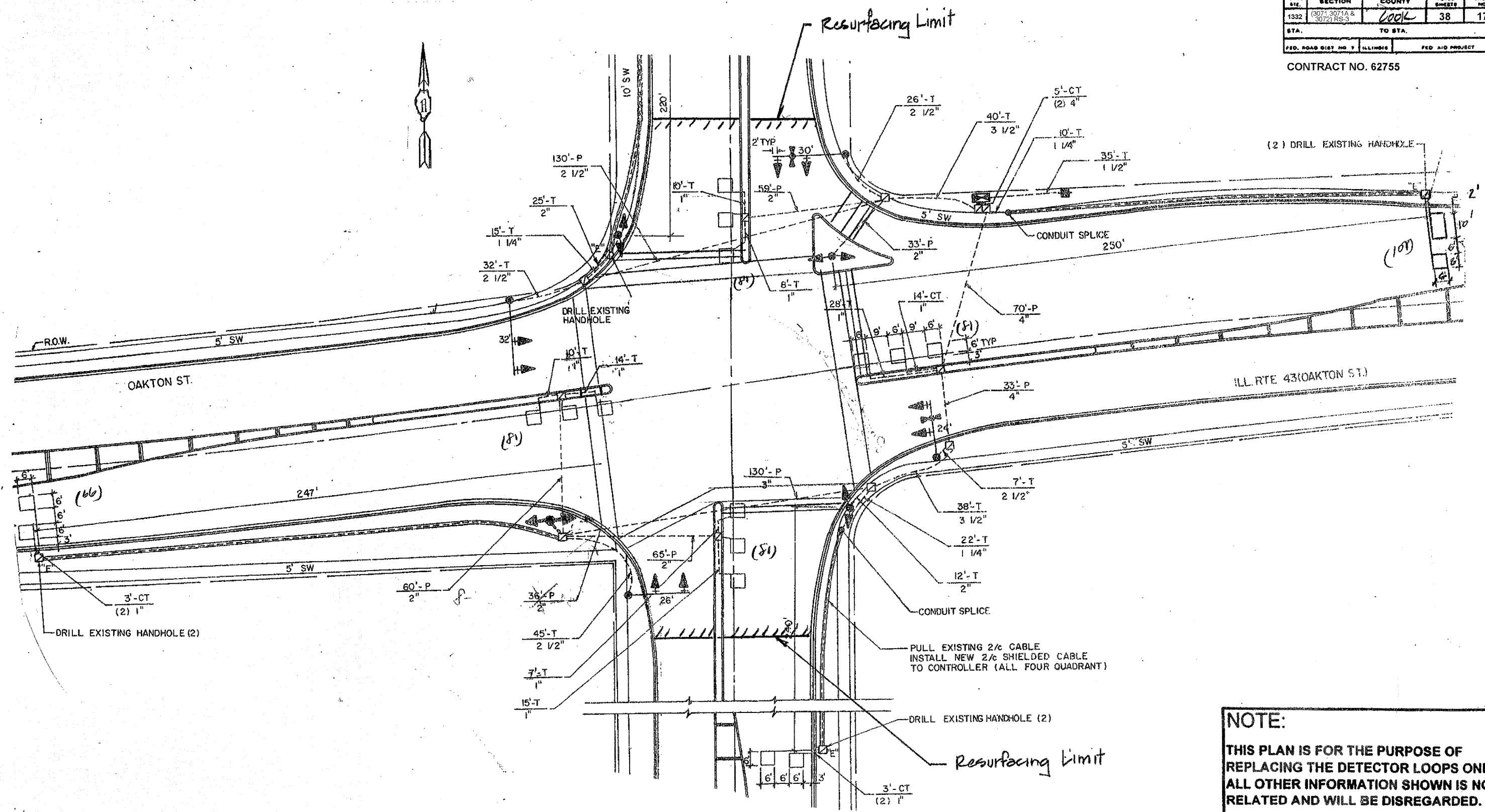
NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
OAKTON ST. @ Ill. 21 (Milw. Av.)
SCALE: NONE
DATE: JAN 09
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

| | | | |
|--------------------------------|----------|------------------|-----------|
| SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1382 (3071, 3071A & 3072) RS-S | Cook | 38 | 17 |
| STA. | TO STA. | | |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 62755



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600600 | 498 | Foot | Detector Loop Replacement |

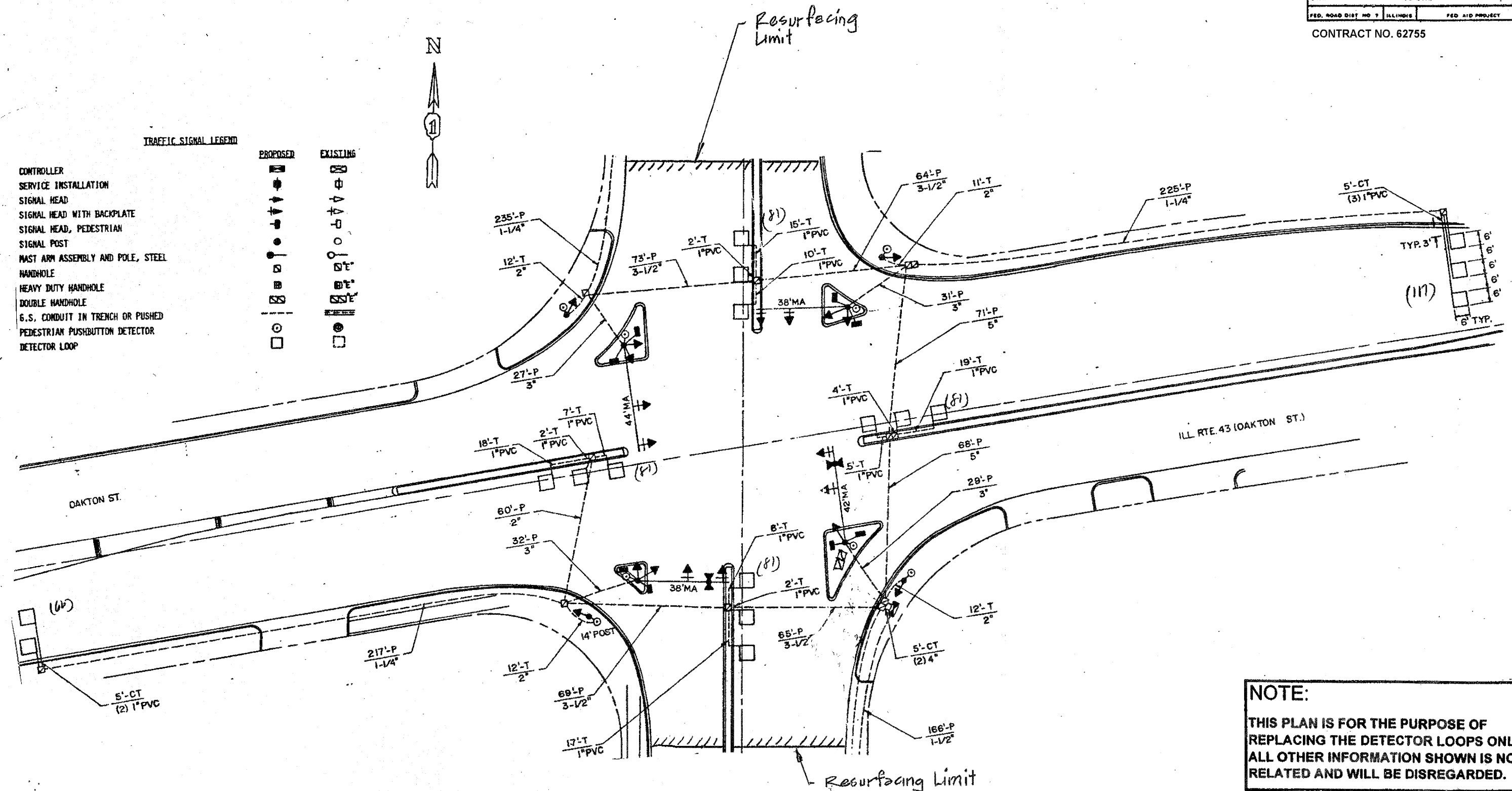
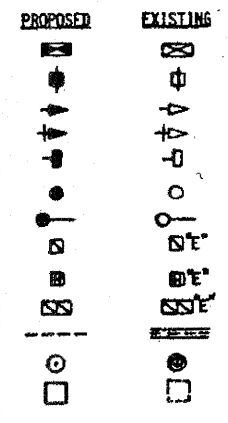
| REVISIONS | |
|-----------|------|
| NAME | DATE |
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NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
OAKTON ST. (IN 43) @ HARLEM AVE.
SCALE: NONE
DATE: JAN. 2009
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- G.S. CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

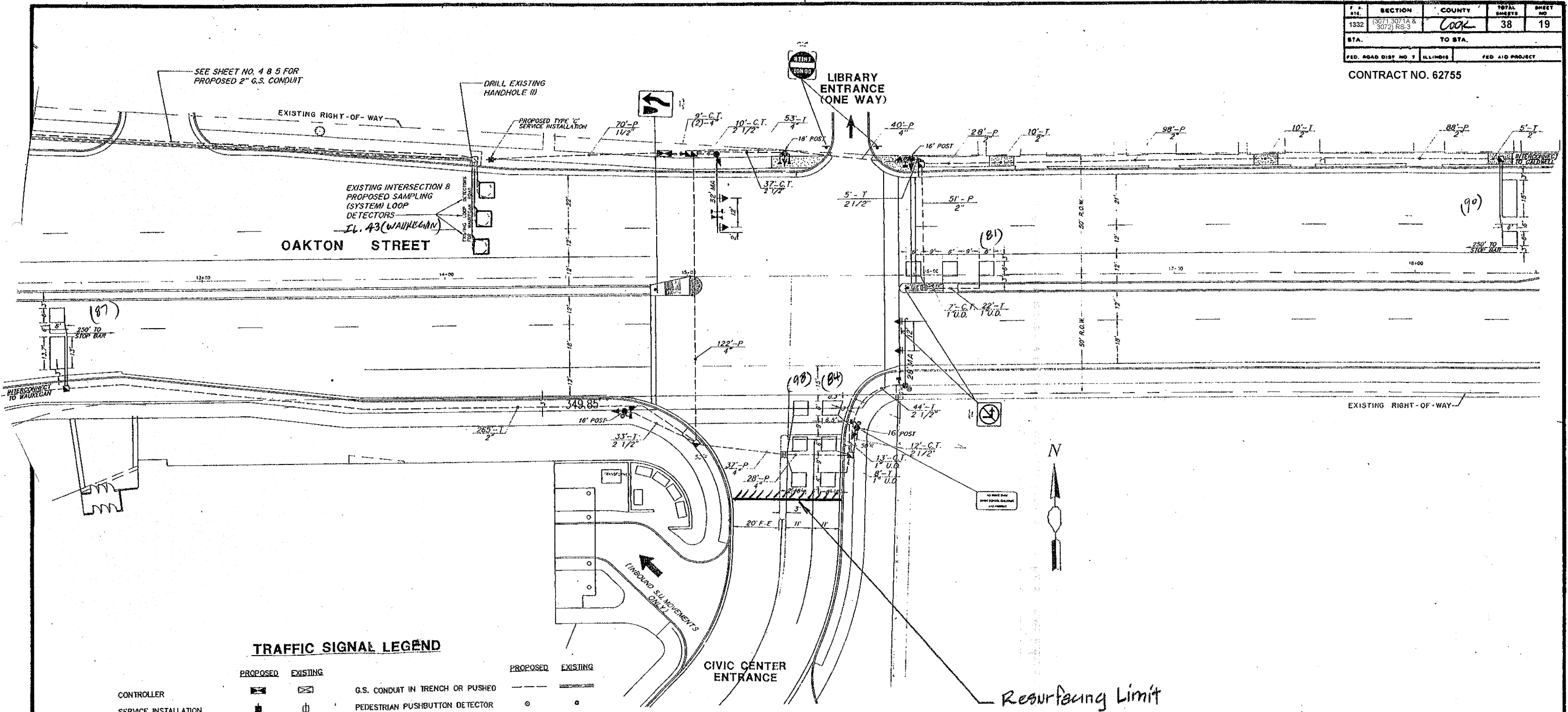
| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600800 | 507 | Foot | Detector Loop Replacement |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
OAKTON ST. (ILL. 43) @ WALKERMAN RD.
SCALE: NONE
DATE: JAN. 2009
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

| SECTION | COUNTY | TOTAL SHEETS | SHEET NO |
|--------------------------------|--------|--------------|----------|
| 1332 (3071, 3071A & 3072) RS-3 | COOK | 38 | 19 |
| CONTRACT NO. 62755 | | | |



TRAFFIC SIGNAL LEGEND

| | PROPOSED | EXISTING | | PROPOSED | EXISTING |
|----------------------------|----------|----------|-----------------------------------|----------|----------|
| CONTROLLER | ☒ | ☒ | G.S. CONDUIT IN TRENCH OR PUSHED | --- | --- |
| SERVICE INSTALLATION | ⊕ | ⊕ | PEDESTRIAN PUSHBUTTON DETECTOR | ○ | ○ |
| SIGNAL HEAD | ▶ | ▶ | DETECTOR LOOP | □ | □ |
| SIGNAL HEAD WITH BACKPLATE | ▶ | ▶ | EMERGENCY VEHICLE SYSTEM DETECTOR | ▲ | ▲ |
| SIGNAL HEAD PEDESTRIAN | ▶ | ▶ | CONFIRMATION BEACON | ⊥ | ⊥ |
| SIGNAL HEAD POST | ● | ○ | HANDHOLE | ⊠ | ⊠ |
| MAST ARM ASSEMBLY & POLE | ● | ○ | DOUBLE HANDHOLE | ⊠ | ⊠ |
| HEAVY DUTY HANDHOLE | ⊠ | ⊠ | SIDEWALK REMOVAL & REPLACEMENT | ▨ | ▨ |

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600600 | 440 | Foot | Detector Loop Replacement |

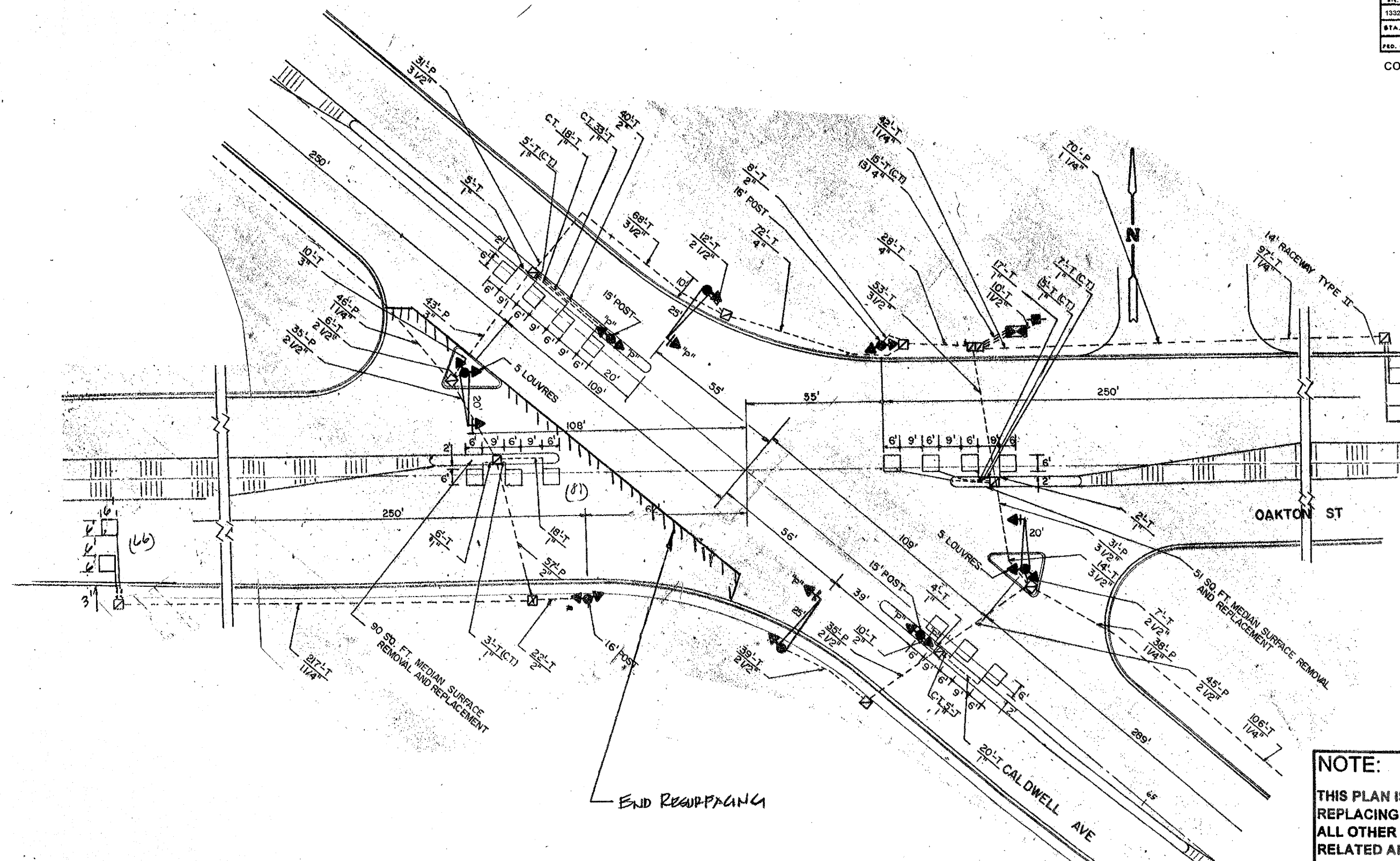
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
OAKTON ST. @ NILES CIVIC CTR.
SCALE: NONE
DATE: JAN 09
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

| P. & S. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------------------------|----------|------------------|-----------|
| 1332 | (3071, 3071A & 3072) RS-3 | COOK | 38 | 20 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 62755



NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

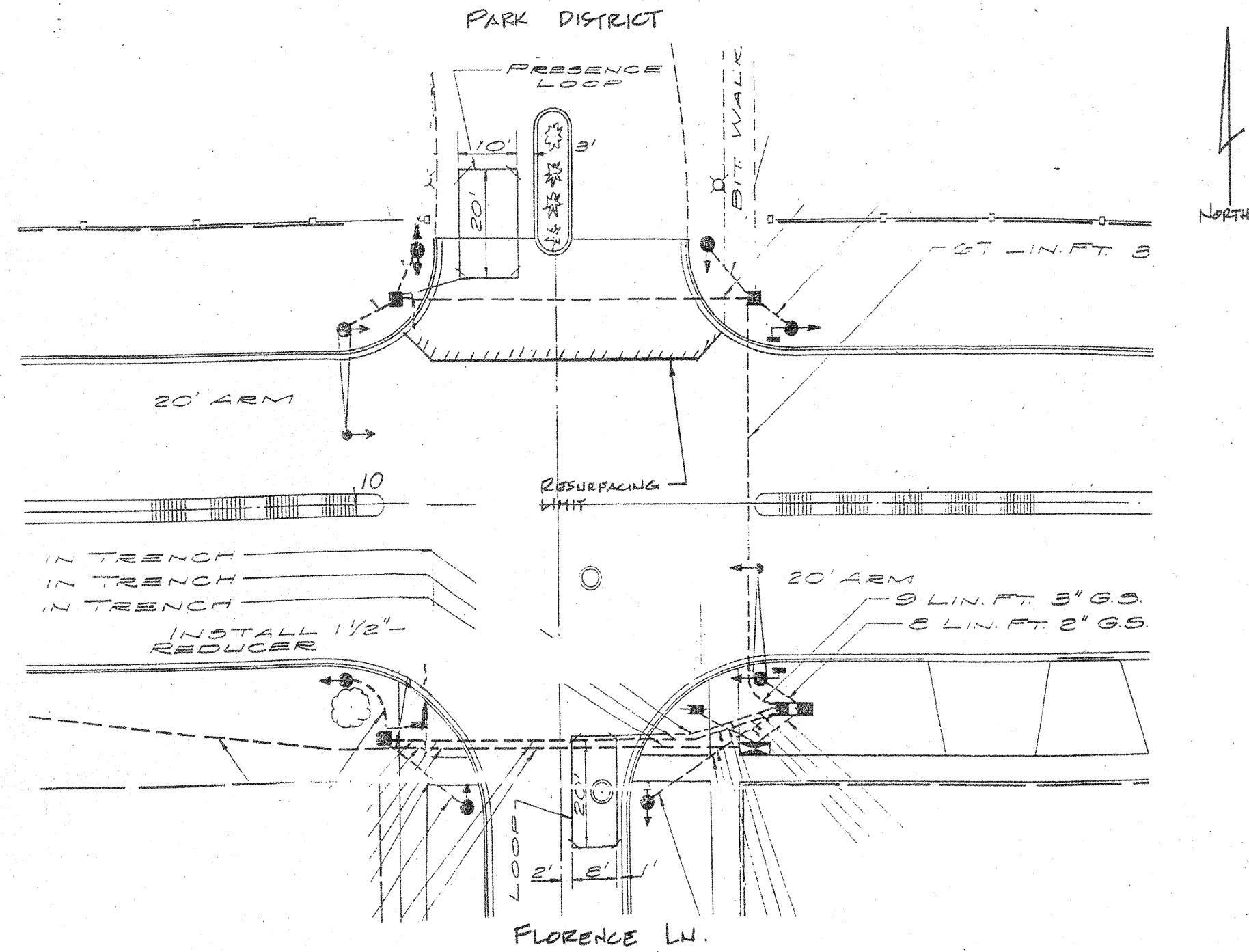
| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 80600600 | 147 | Foot | Detector Loop Replacement |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 OAKTON ST. @ U.S. RTE. 14 (CALDWELL)
 SCALE: NONE
 DATE: JAN. 2009
 DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: DAD

| S. & S. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|---------|--------------|-----------|
| 1392 | * | COOK | 38 | 21 |
| STA. | | TO STA. | | |

FED. ROAD DIST. NO. 3 ILLINOIS
 FED. AID PROJECT
 * (3071, 3071A & 3072) RS-3
 62755



FOR INFORMATION ONLY

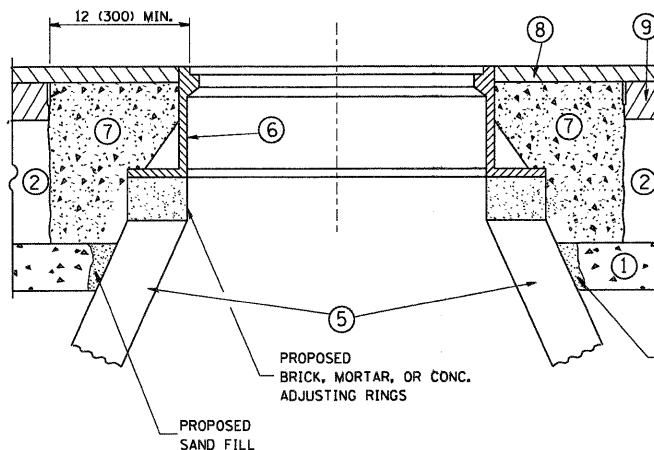
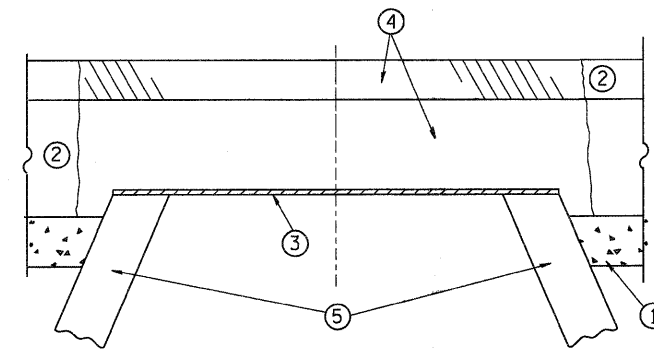
NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

| CODE NO. | QUANTITY | UNIT | ITEM |
|----------|----------|------|---------------------------|
| 86600800 | -0- | Foot | Detector Loop Replacement |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 OAKTON ST. @ FLORENCE LN.
 SCALE: 1" = 20'
 DATE: MAR. 04
 DRAWN BY: _____
 DESIGNED BY: _____
 CHECKED BY: _____



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

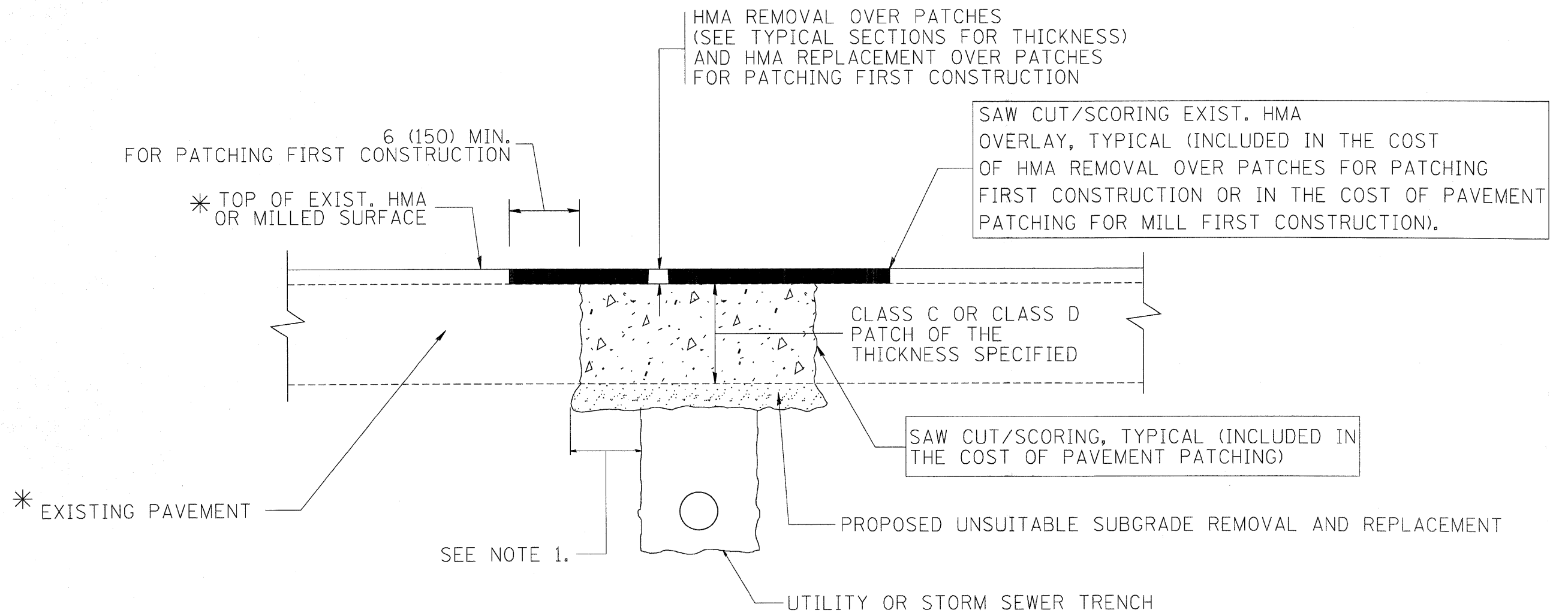
BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | | | | | | | | | |
|--|-----------------------------|--------------------------------|-----------------------------|---|--|-------------------------|---|----------------------------------|-------------|--------------------|--------------|
| FILE NAME = W:\d\stata\22x34\bd08.dgn | USER NAME = gaglianobt | DESIGNED - R. SHAH | REVISED - R. SHAH 03-10-95 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING | | F.A.U. RTE. 1332 | SECTION (3071,3071A & 3072) RS-3 | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 22 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - A. ABBAS 03-21-97 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | BD600-03 (BD-8) | | CONTRACT NO. 62755 | |
| PLOT DATE = 1/4/2008 | DATE - 10-25-94 | REVISED - R. WIEDEMAN 05-14-04 | REVISED - R. BORO 01-01-07 | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| | | | | | | | | | | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

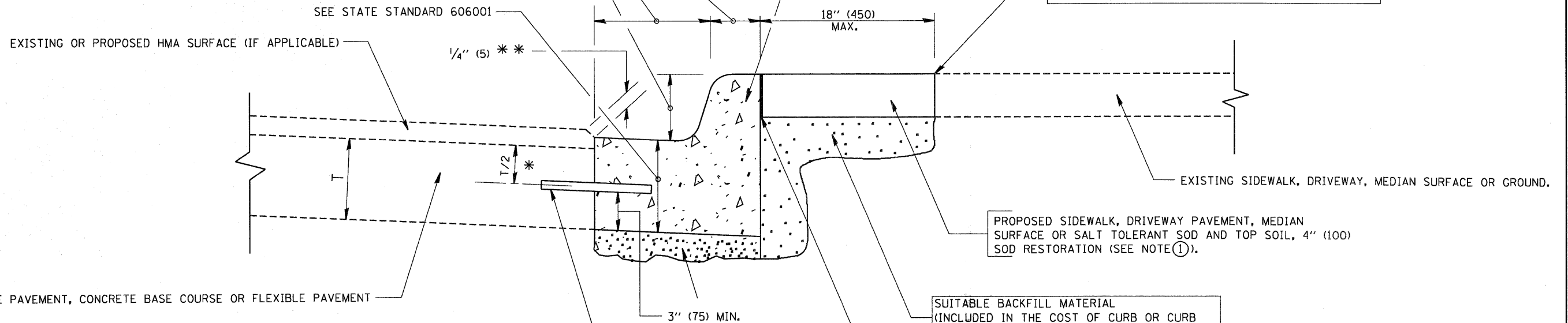
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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|--|----------------------------|----------------------------|-----------------------------|---|--|-------------------------|-------------------------------------|---|--------------------------|--------|-----------------|--------------|
| FILE NAME = c:\projects\dist\td22x34\bd22.dgn | USER NAME = beuerd1 | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT | | | F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - R. BORO 01-01-07 | | | | | 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 23 |
| PLOT DATE = 10/27/2008 | DATE - 10-25-94 | REVISED - R. BORO 09-04-07 | REVISED - K. ENG 10-27-08 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | | |
| | | | | | | | BD400-04 (BD-22) CONTRACT NO. 62755 | | | | | |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

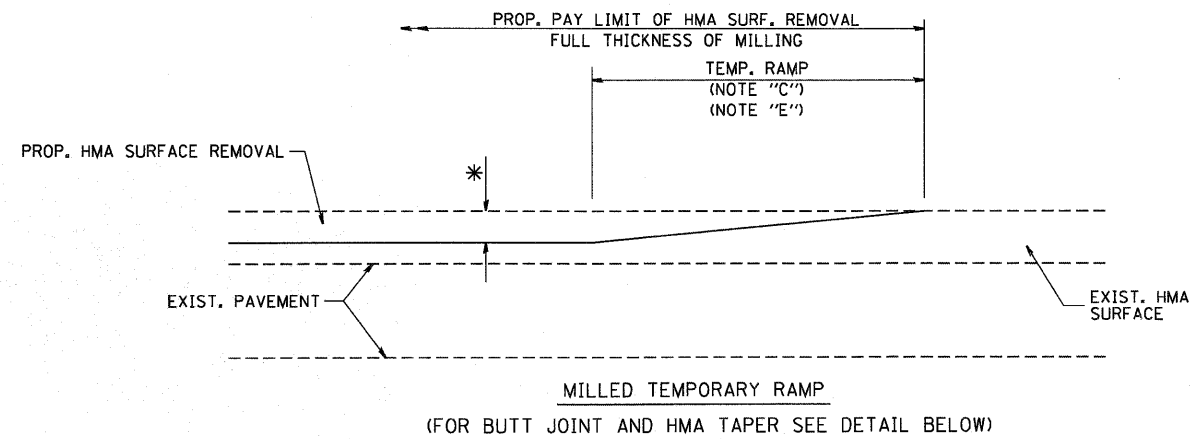
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

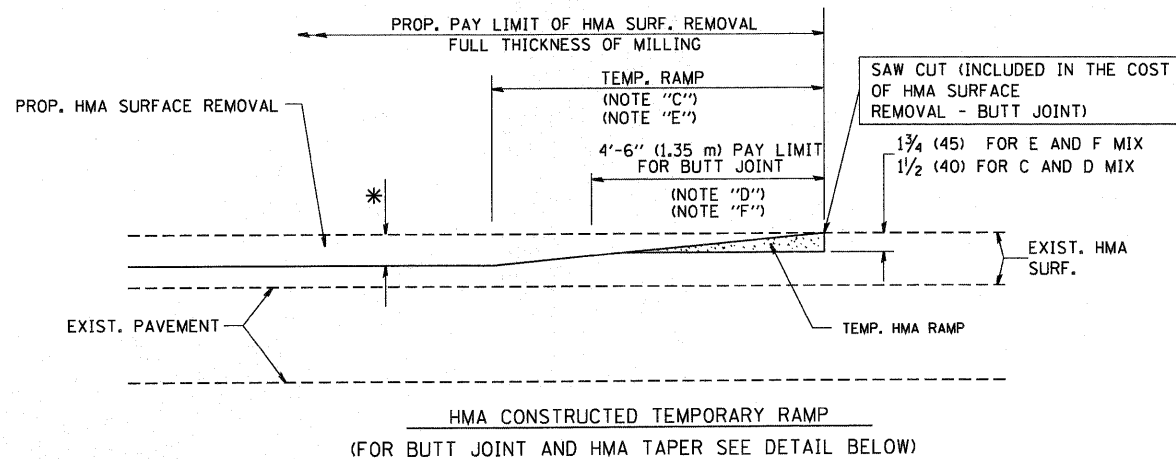
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | | |
|--|------------------------|----------------------------|-----------------------------|---|--|-------------------------|------|----------------|---|--------------------------|---------------------------|--------------|----|
| FILE NAME = W:\diststd\22x34\bd24.dgn | USER NAME = gaglionobt | DESIGNED - A. HOUSEH | REVISED - R. SHAH 10-03-96 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | | DRAWN - | REVISED - A. ABBAS 03-21-97 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 24 |
| | | PLOT SCALE = 5/8" = 1' IN. | REVISED - M. GOMEZ 01-22-01 | | | | | | BD600-06 (BD-24) | | CONTRACT NO. 62755 | | |
| | | PLOT DATE = 1/4/2008 | REVISED - R. BORO 01-01-07 | | | | | | FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | |

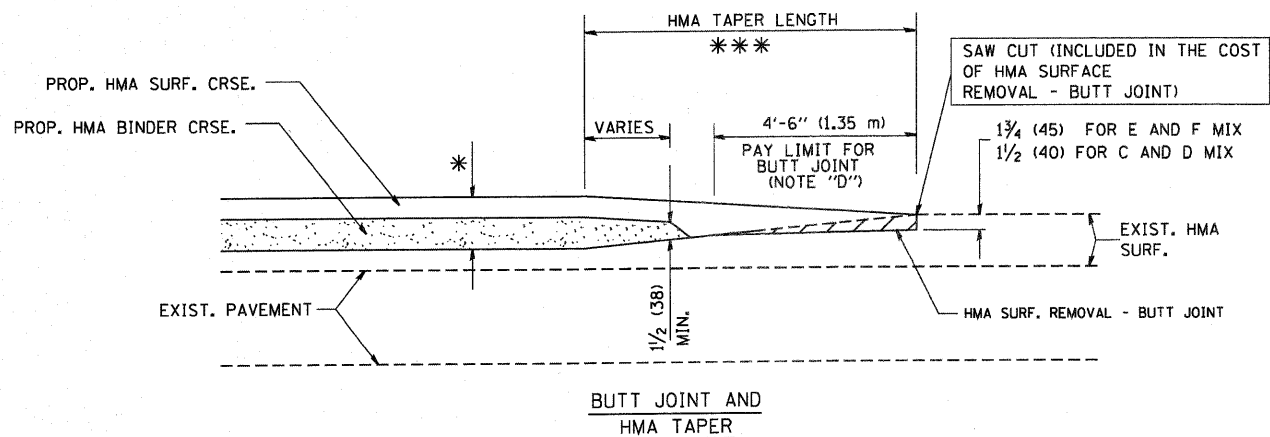


OPTION 1

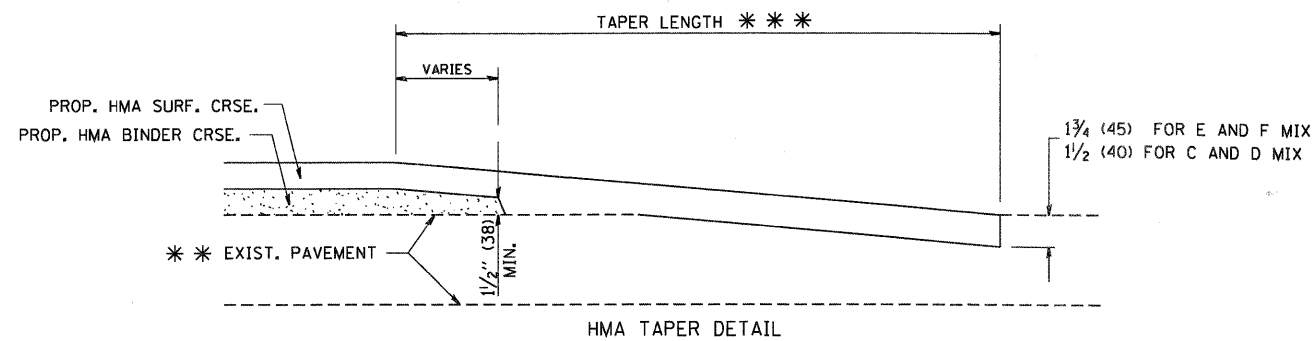
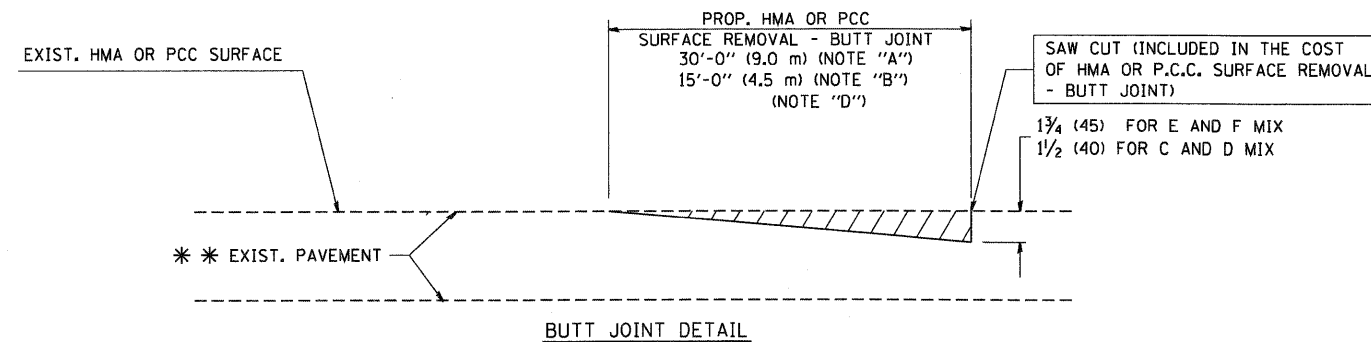


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd32.dgn

USER NAME = gaglianobt
 PLOT SCALE = 50.0000' / IN.
 PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
 DRAWN -
 CHECKED -
 DATE - 06-13-90

REVISED - R. SHAH 10-25-94
 REVISED - A. ABBAS 03-21-97
 REVISED - M. GOMEZ 04-06-01
 REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

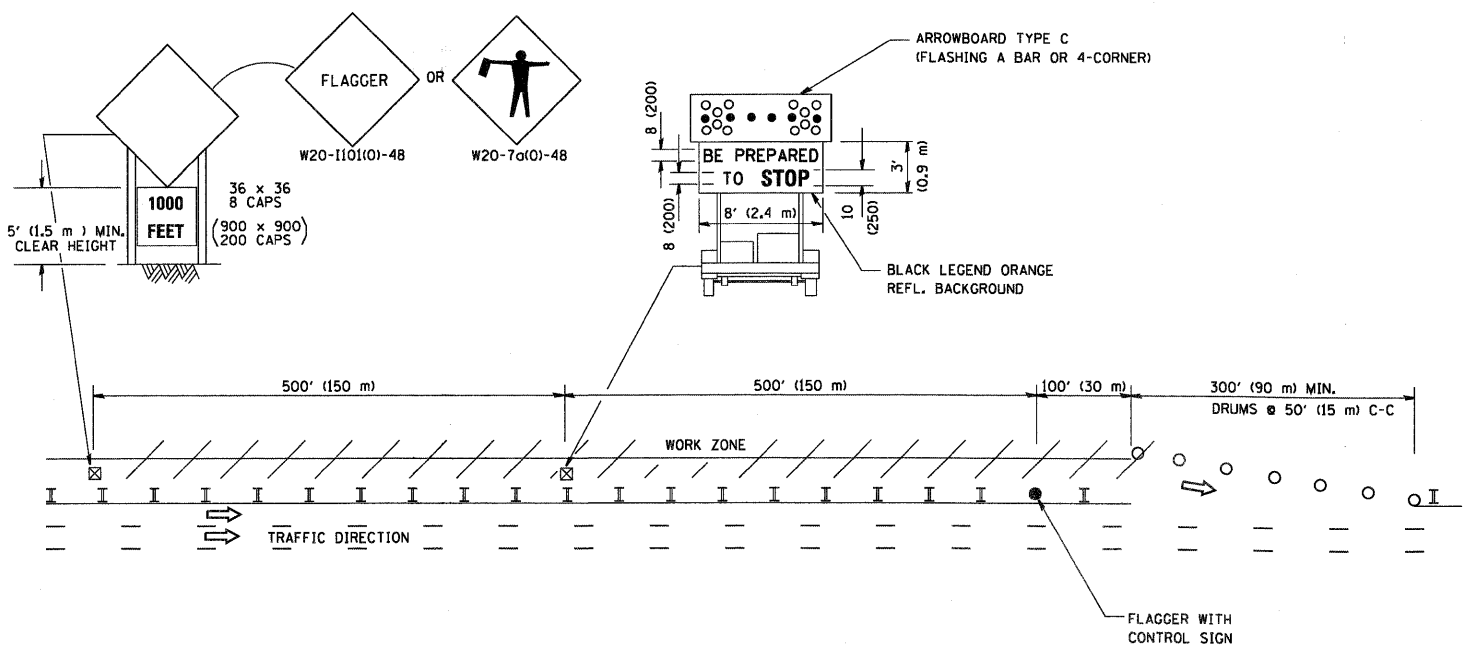
**BUTT JOINT AND
 HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

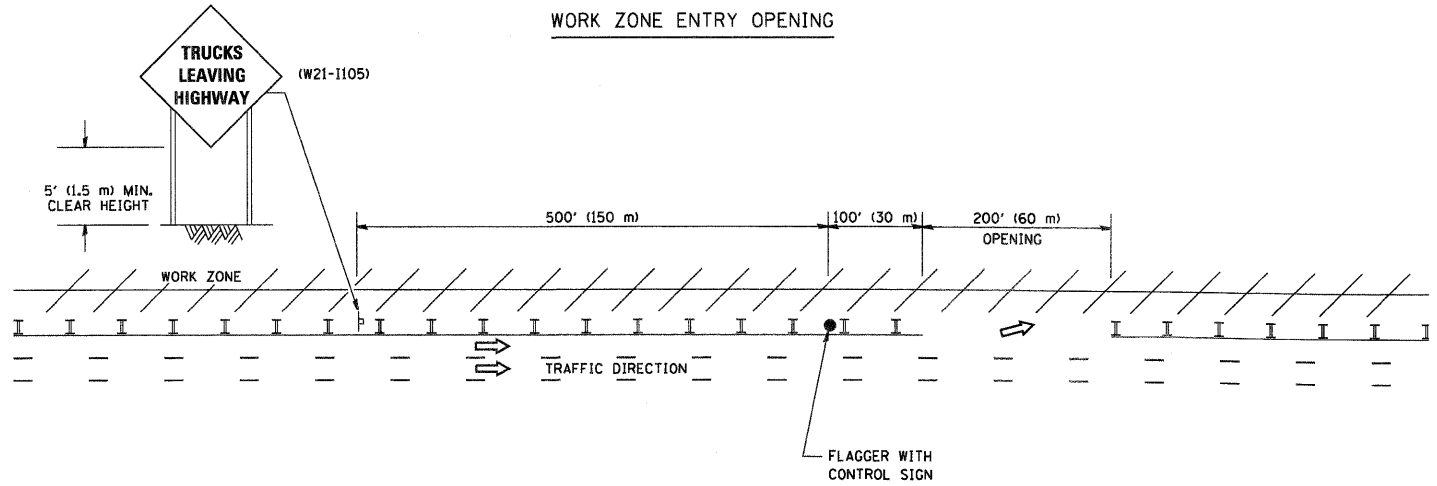
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|--------------------------|--------|--------------------|-----------|
| 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 25 |
| BD400-05 BD32 | | | CONTRACT NO. 62755 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING

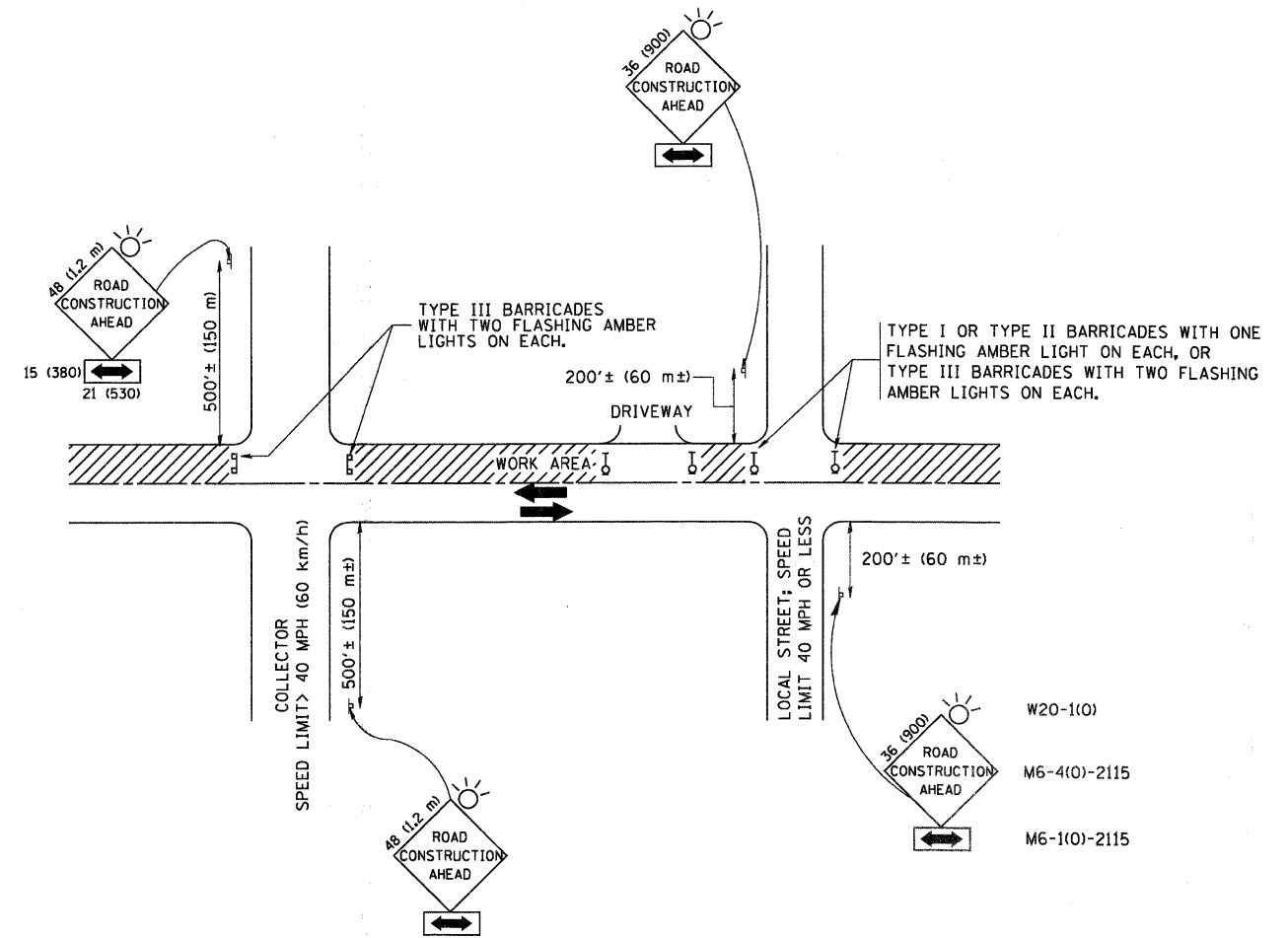


NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | | | | | | | | | | |
|--|----------------------------|------------------------|------------------------|---|--|--------------------------|---|----------------|---------|--------------------|-----------------|--------------|
| FILE NAME = W:\diststd\22x34\to18.dgn | USER NAME = gaglianobt | DESIGNED - | REVISED - D.W.S. 08-98 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - J.A.F. 04-03 | | 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 26 | | | |
| PLOT DATE = 1/4/2008 | DATE - | REVISED - J.A.F. 02-06 | REVISED - S.P.B. 01-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | TC-18 | | CONTRACT NO. 62755 | | |
| | | | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

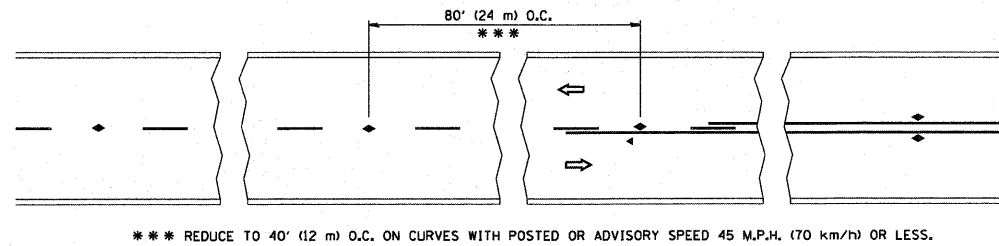
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

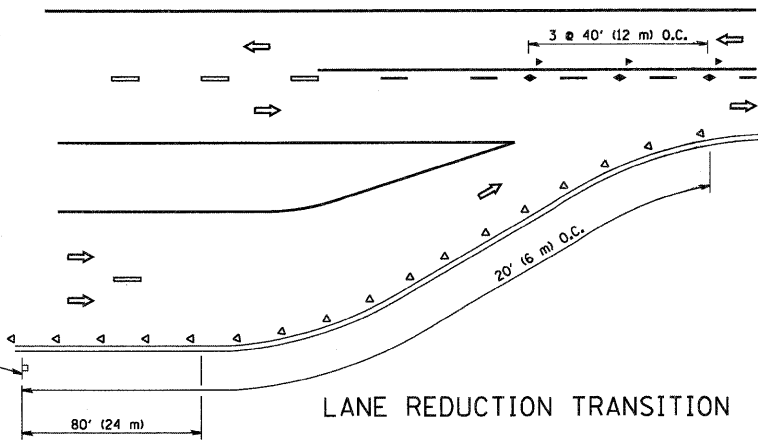
All dimensions are in millimeters (inches) unless otherwise shown.

| | | | | | | | | | | | | |
|--|-----------------------------|---------------------------------|------------------------------|---|--|--------------------------|---------|---|---------|--------------------|-----------------|--------------|
| FILE NAME = W:\d\statd\22x34\to18.dgn | USER NAME = geglanoht | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 50,000 ' / IN. | CHECKED - | REVISED - A. HOUSEH 03-06-96 | | 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 27 | | | |
| PLOT DATE = 1/4/2008 | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-00 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | TC-10 | | CONTRACT NO. 62755 | | |
| | | | | | | | | FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | |

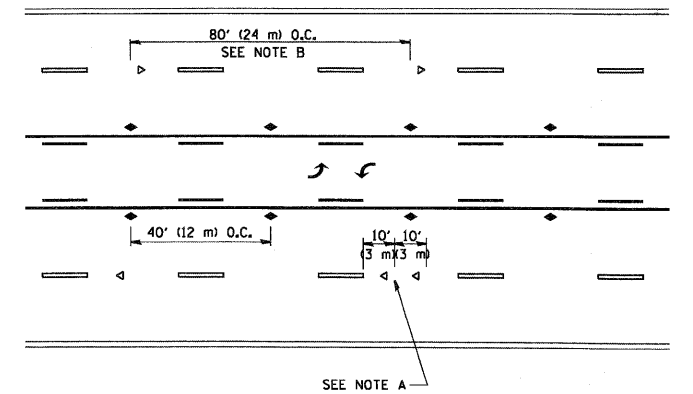


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

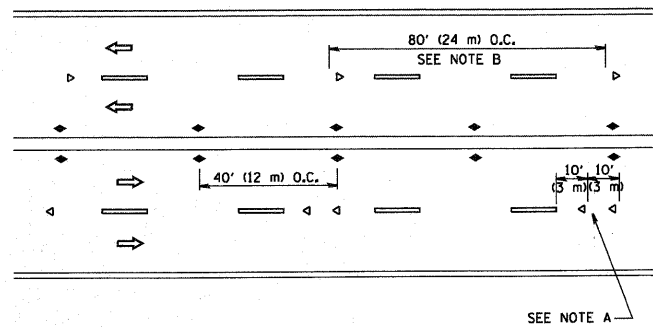
TWO-LANE/TWO-WAY



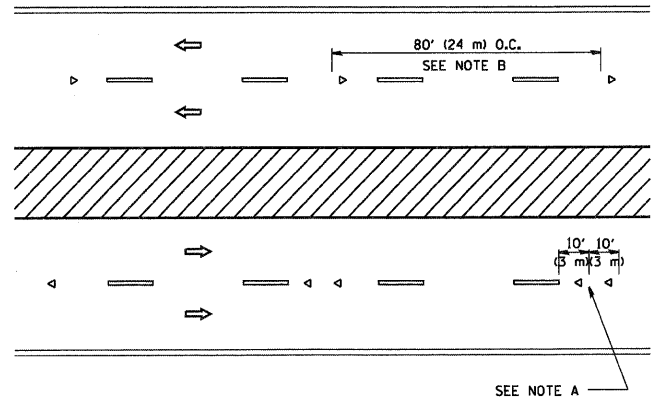
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

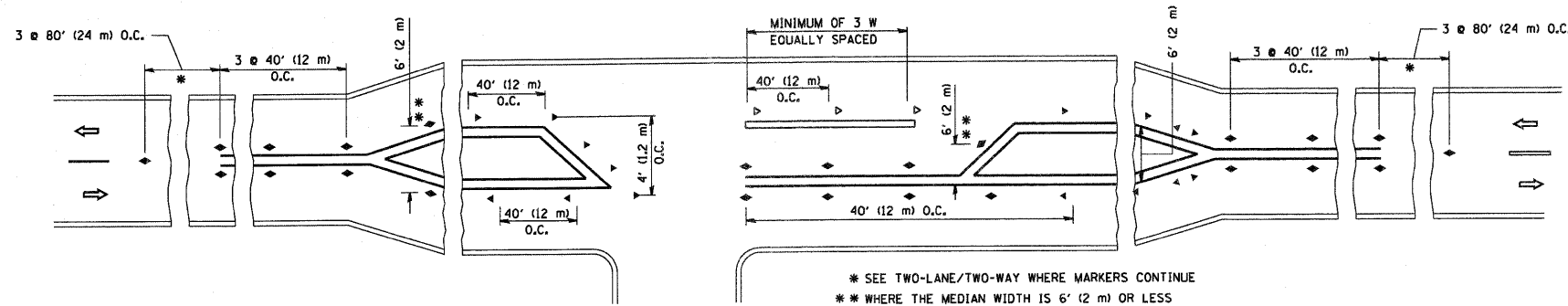
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

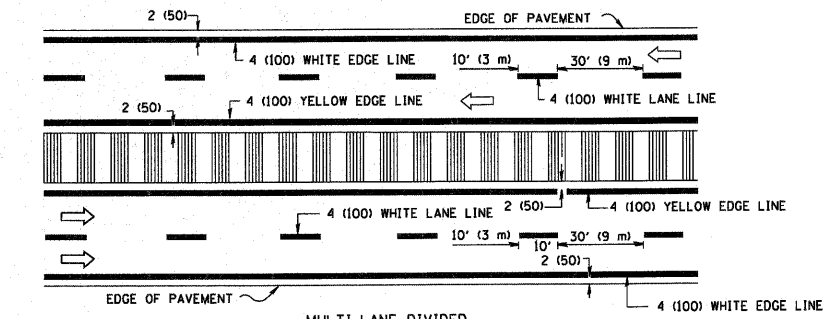
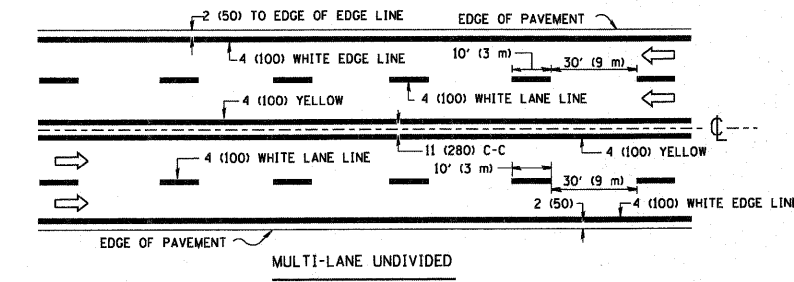
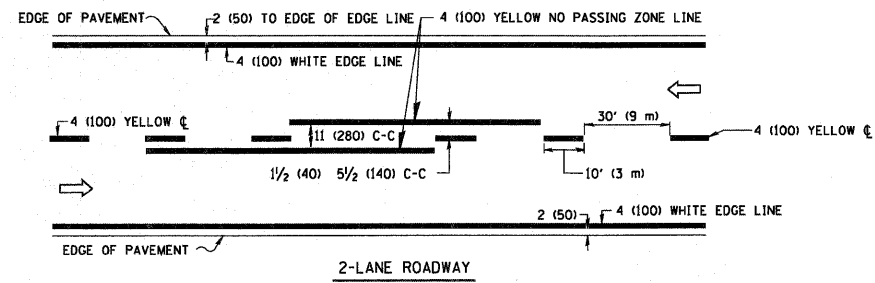


LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

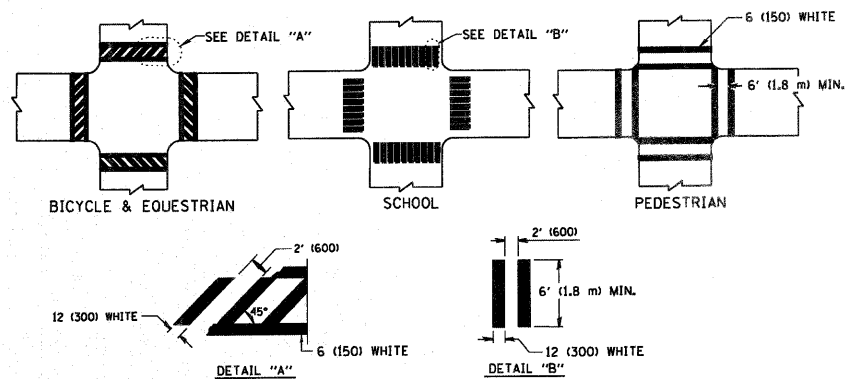
All dimensions are in Inches (millimeters) unless otherwise shown.

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| | PLOT SCALE = 58,000' / IN. | DRAWN - | REVISED - T. RAMMACHER 03-12-99 | | 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 28 | | | |
| PLOT DATE = 1/4/2008 | CHECKED - | REVISED - T. RAMMACHER 01-06-00 | REVISED - | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | TC-11 | | CONTRACT NO. 62755 | | |
| | DATE - | REVISED - | | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

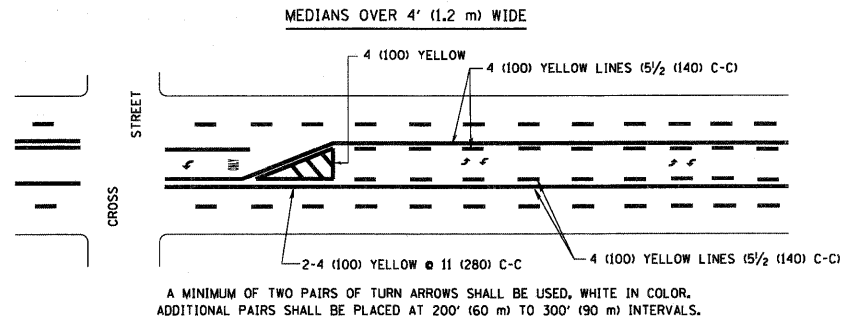
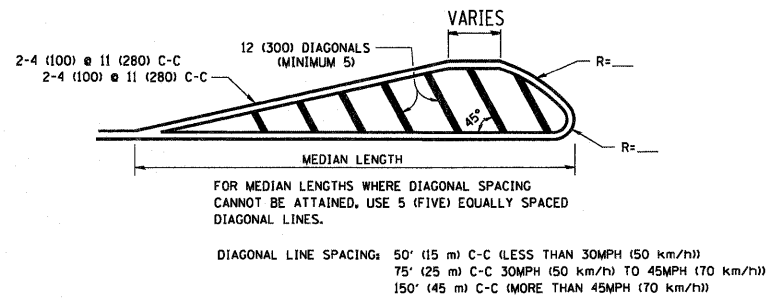
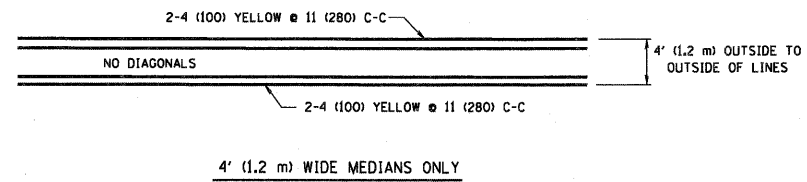


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

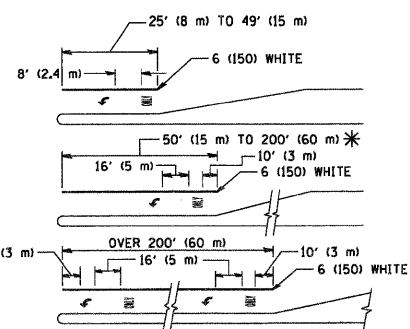
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



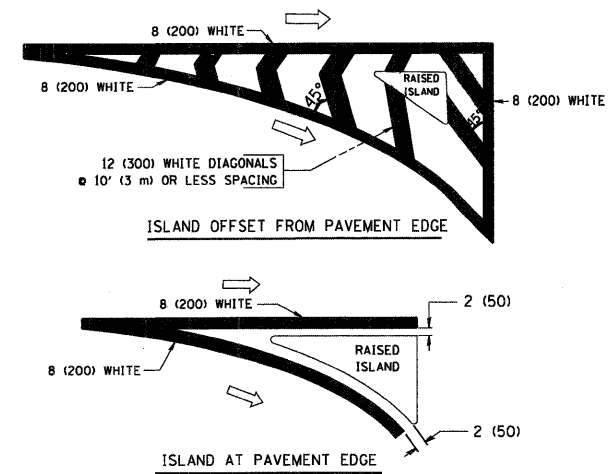
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

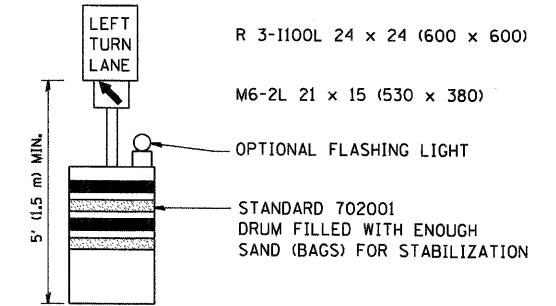
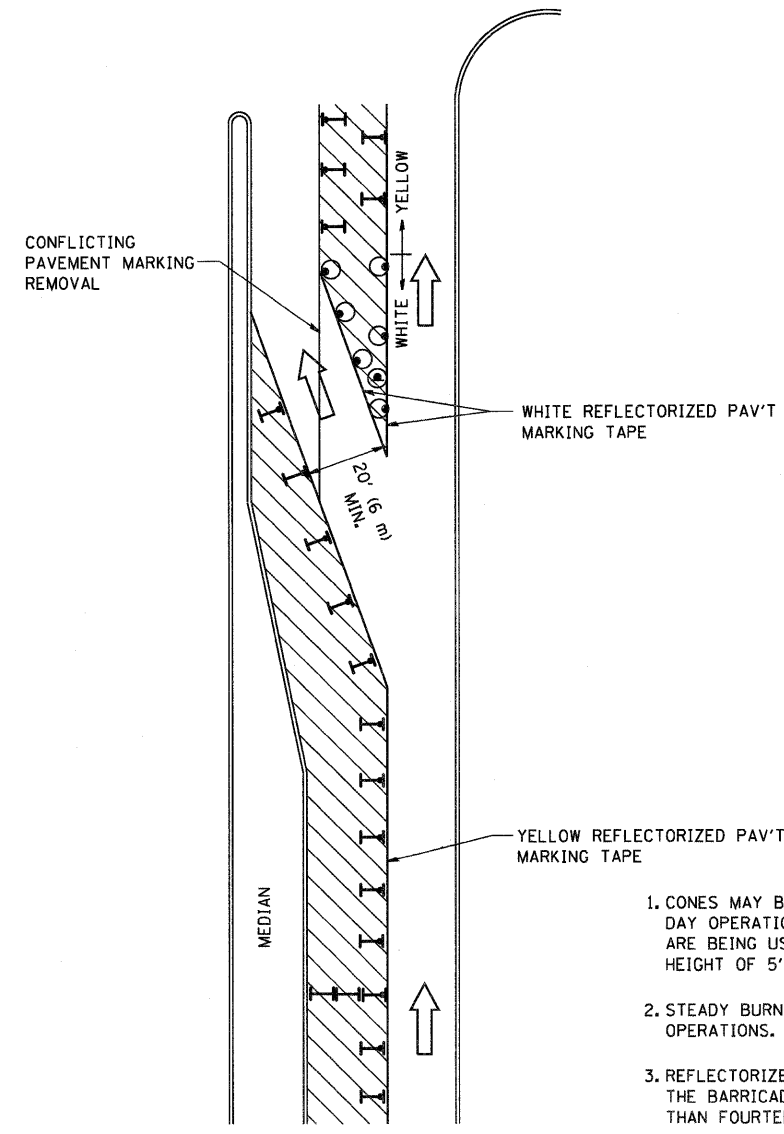


TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

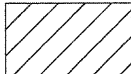





All dimensions are in inches (millimeters) unless otherwise shown.



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

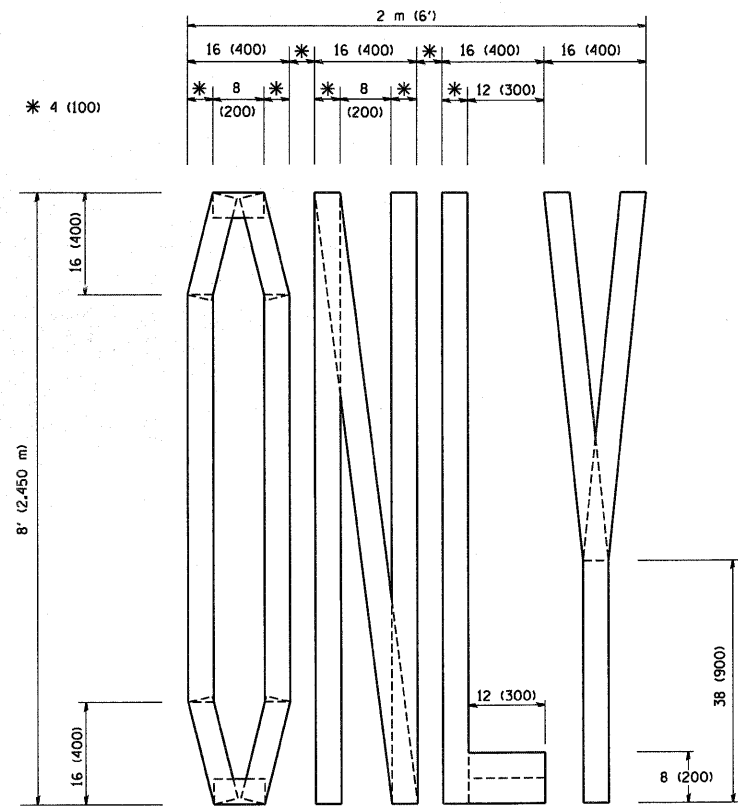
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| W:\diststd\22x34\to14.dgn | | DRAWN - | REVISED - A. HOUSEH 11-07-95 |
| | | CHECKED - | REVISED - A. HOUSEH 10-12-96 |
| | | DATE - | REVISED - T. RAMMACHER 01-06-00 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

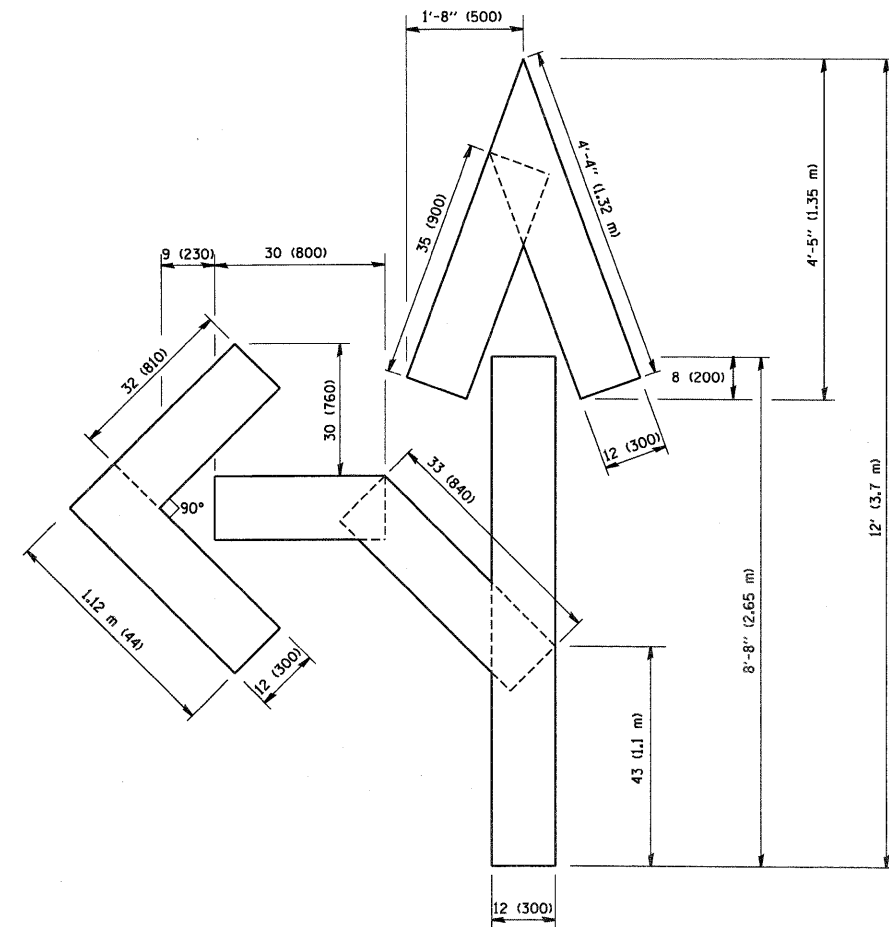
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

| | | |
|-------------|-------------------------|--------------|
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. |
|-------------|-------------------------|--------------|

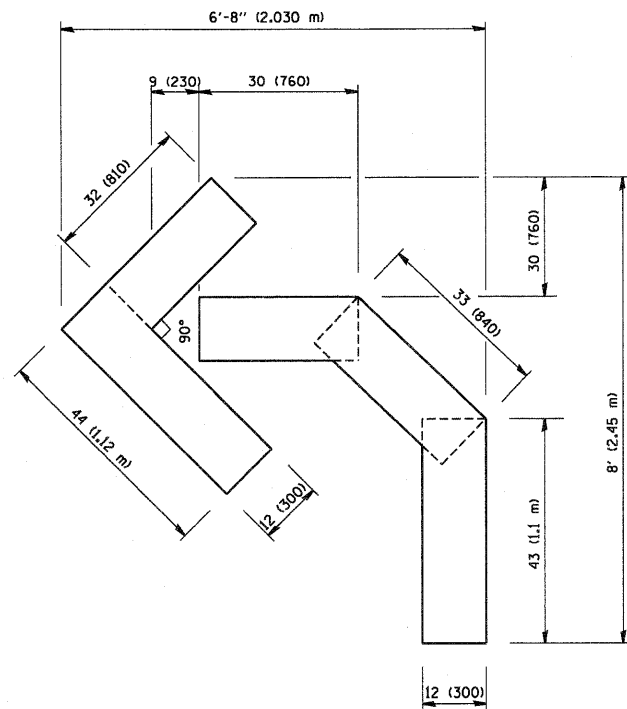
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 30 |
| TC-14 | | | CONTRACT NO. 62755 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

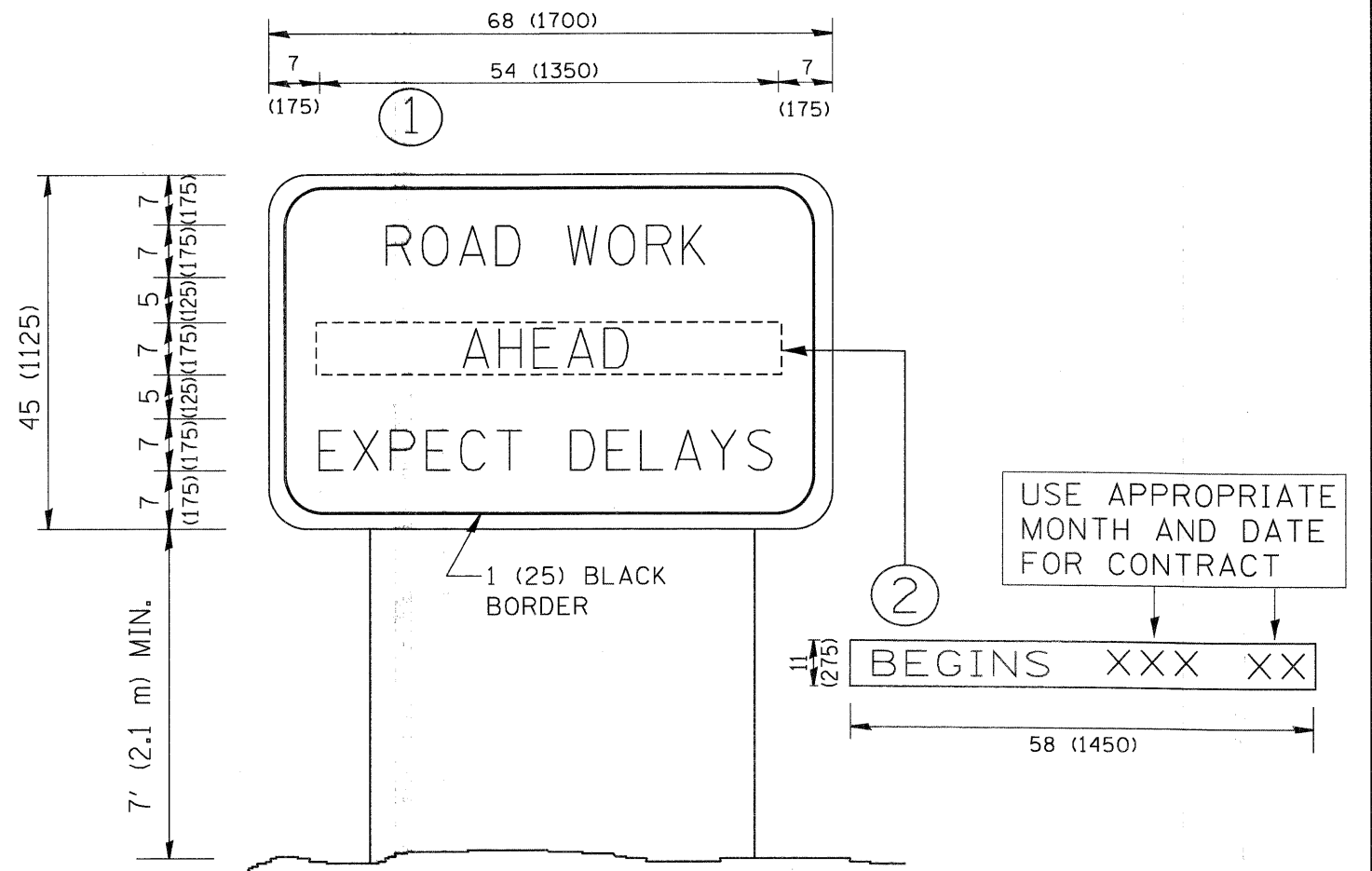
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| | | DRAWN - | REVISED -T. RAMMACHER 11-04-97 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED -T. RAMMACHER 03-02-98 |
| | PLOT DATE = 1/4/2008 | DATE - 09-18-94 | REVISED -E. GOMEZ 08-28-00 |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|-------------------------------------|--------------------|--------------------|-----------------|
| F.A.U. RTE. 1332 | SECTION (3071,3071A & 3072) RS-3 | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 31 |
| TC-16 | | CONTRACT NO. 62755 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



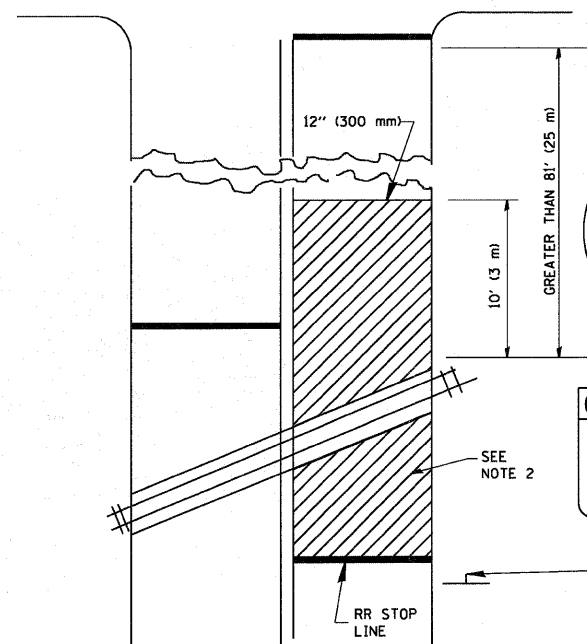
NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

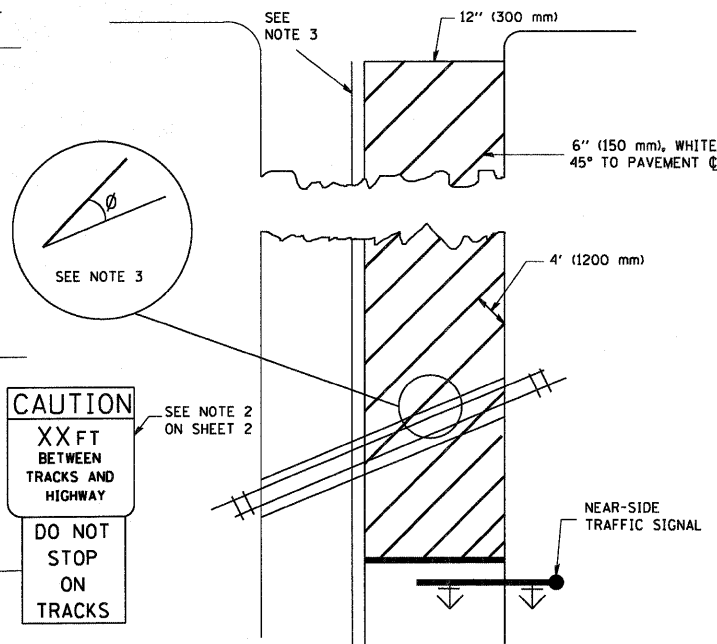
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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| FILE NAME = W:\diststd\22x34\to22.dgn | USER NAME = gaglanobt | DESIGNED - | REVISED - R. MIRS 09-15-97 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | ARTERIAL ROAD INFORMATION SIGN | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - R. MIRS 12-11-97 | | | | | 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 32 |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 | | | | | TC-22 | | | CONTRACT NO. 62755 | |
| | PLOT DATE = 1/4/2008 | DATE - | REVISED - C. JUCIUS 01-31-07 | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| | | | | SCALE: NONE | | | SHEET NO. 1 OF 1 SHEETS | | STA. TO STA. | | | |

WITH INTERSECTION TRAFFIC SIGNALS



WITH NEAR-SIDE TRAFFIC SIGNALS

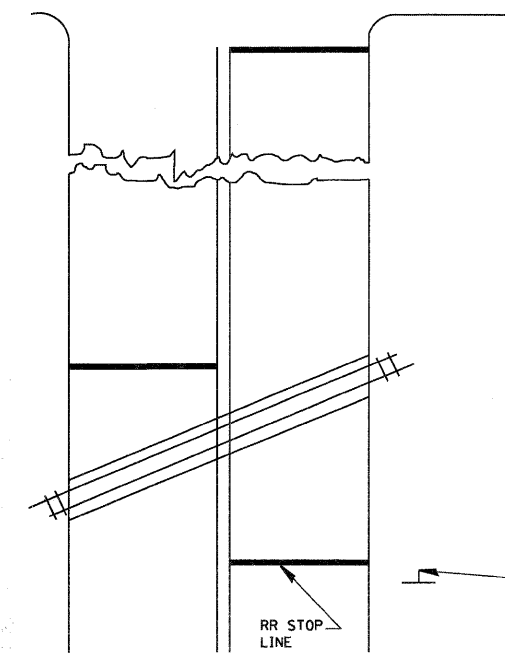


CAUTION
XX FT
BETWEEN
TRACKS AND
HIGHWAY
DO NOT
STOP
ON
TRACKS

- NOTES:
1. PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
 2. WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
 3. WHERE THE ANGLE BETWEEN THE DIAGONAL STRIPES AND THE TRACK (θ) WOULD BE LESS THAN APPROXIMATELY 20°, THE STRIPES SHOULD BE SLOPED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

WITH NONSIGNALIZED INTERSECTION

81' (25 m) OR LESS TO CLOSEST RAIL



(W10 - 1100)
CAUTION
XX FT
BETWEEN
TRACKS AND
HIGHWAY
DO NOT
STOP
ON
TRACKS

- NOTE 1:
1. DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
 2. THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

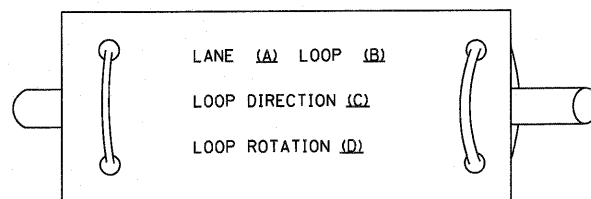
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | | | | |
|--|------------------------|----------------------------|--------------------|---|---|-------------|------|----------------|---------|---------|---|--------------------------|------|----|----|
| FILE NAME = W:\distatd\22x34\to23.dgn | USER NAME = gegljanobt | DESIGNED - | REVISED - 01-01-07 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS | | | F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | | |
| | | DRAWN - | REVISED - | | SCALE: NONE | SHEET NO. 1 | OF 1 | SHEETS | STA. | TO STA. | 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 33 |
| | | CHECKED - | REVISED - | | | | | | | | | TC-23 | | | |
| | | DATE - | REVISED - | | | | | | | | | CONTRACT NO. 62755 | | | |
| | | PLOT SCALE = 50,000' / IN. | | | | | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| | | PLOT DATE = 1/4/2008 | | | | | | | | | | | | | |

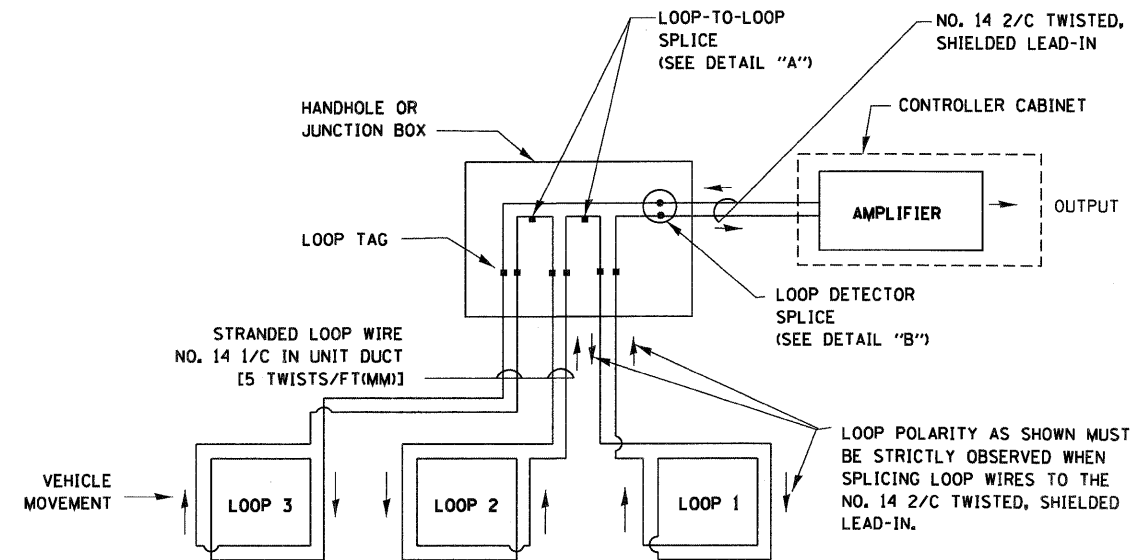
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

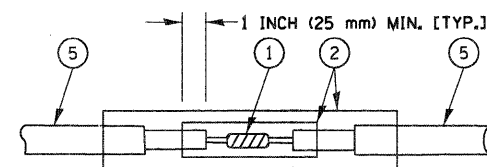


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

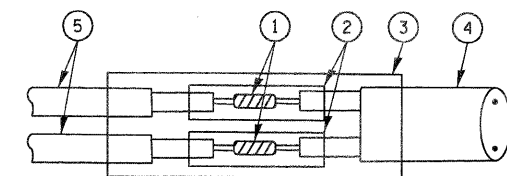


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

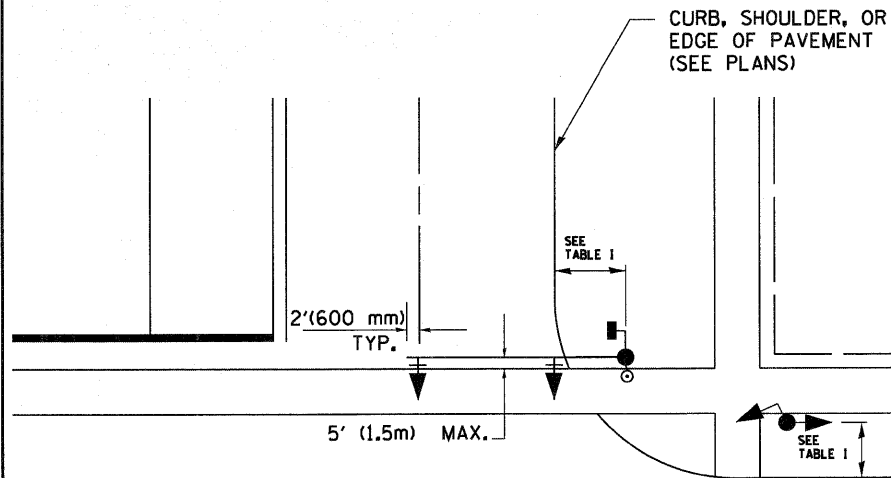
LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

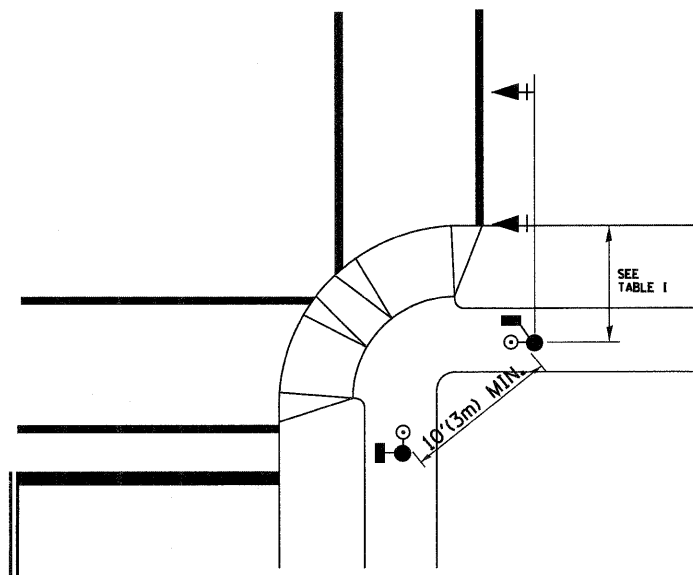
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|---------------------------|------------------------|-------------------|---------------------------------|---|--|--------------|--------------------------|---------------------------|--------------|---|
| FILE NAME = | USER NAME = gegljanobt | DESIGNED - D.A.D. | REVISED - 11-12-01 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| W:\diststd\22x34\ts05.dgn | | DRAWN - R.W.P. | REVISED - BUR. TRAFFIC 01-01-02 | | | 1332 | (3071,3071A & 3072) RS-3 | COOK | 38 | 34 |
| | | CHECKED - D.A.Z. | REVISED - | | | TS-05 | | CONTRACT NO. 62755 | | |
| | | DATE - 05-30-00 | REVISED - | | | SCALE: NONE | SHEET NO. 1 OF 4 SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.

AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.

PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

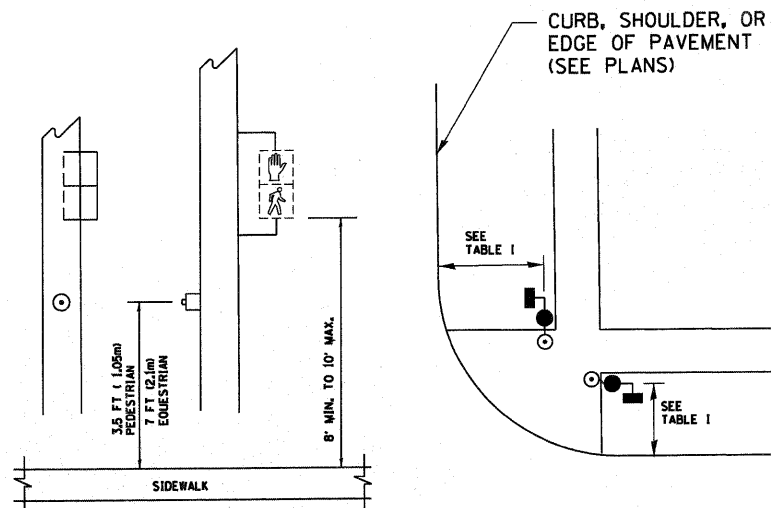
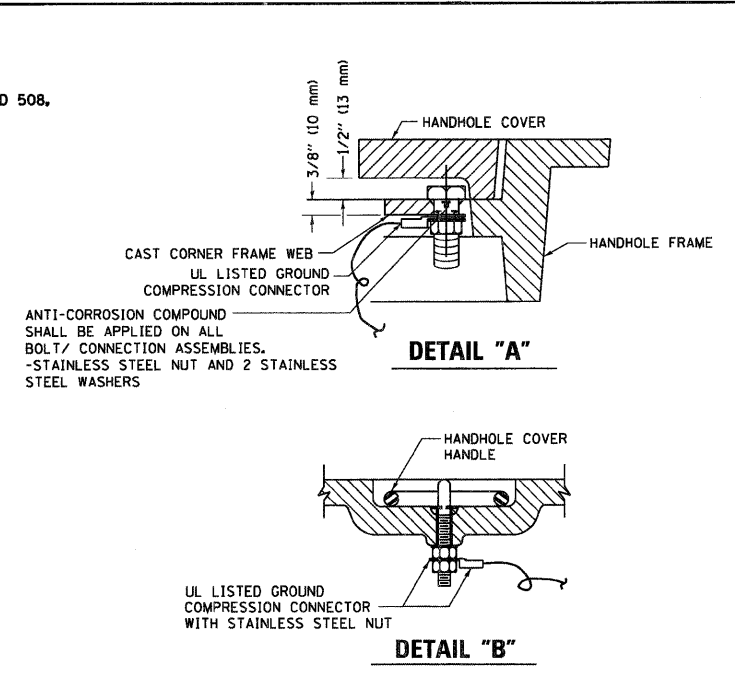
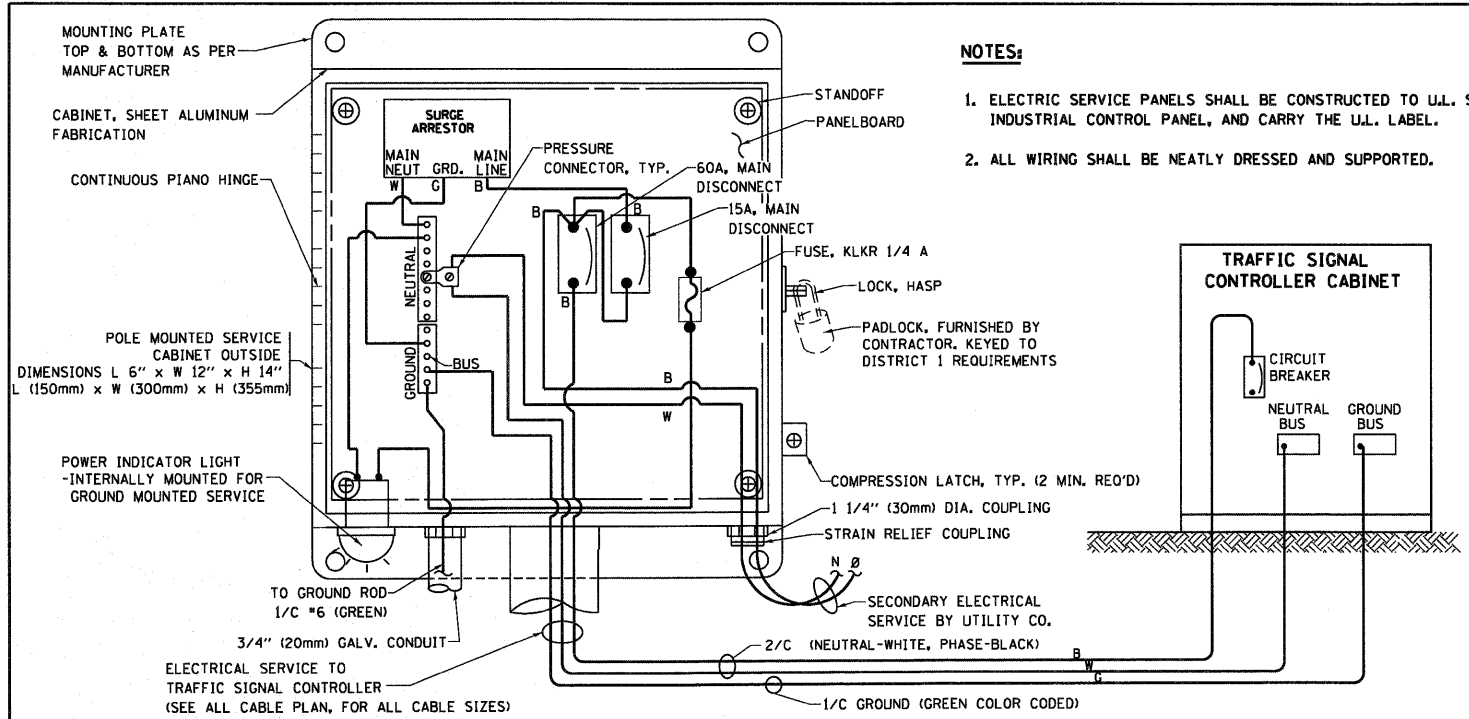


TABLE I

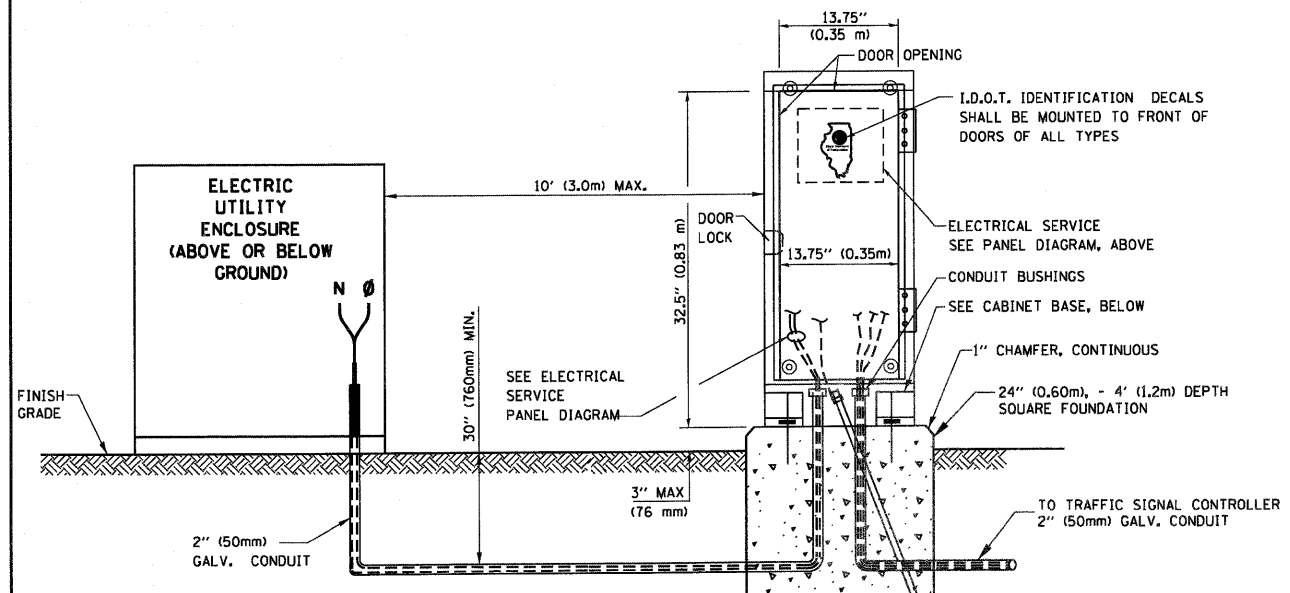
| TRAFFIC SIGNAL EQUIPMENT | COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB) | SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT) |
|------------------------------|---|---|
| TRAFFIC SIGNAL MAST ARM POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| TRAFFIC SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| PEDESTRIAN SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| PEDESTRIAN PUSHBUTTON | SEE NOTE 1 | SEE NOTE 1 |



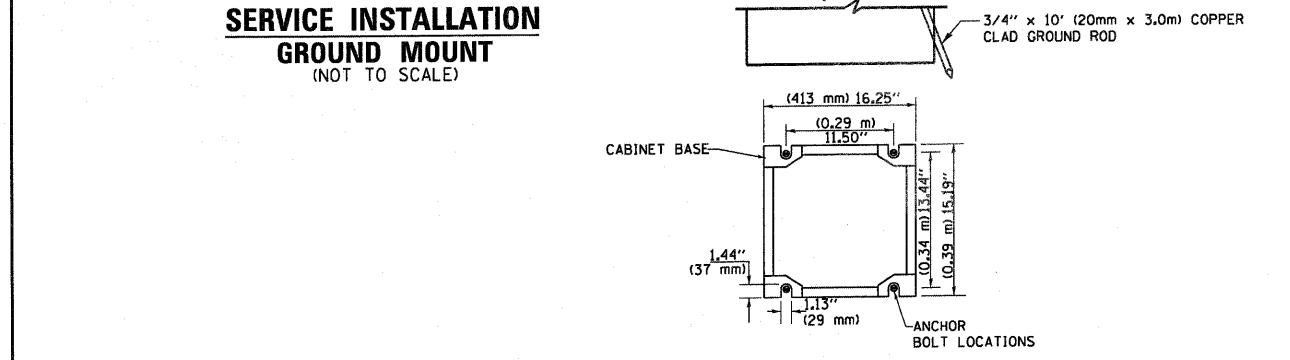
- NOTES:**
- GROUNDING SYSTEM**
- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD, ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
 - THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
 - ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
 - THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

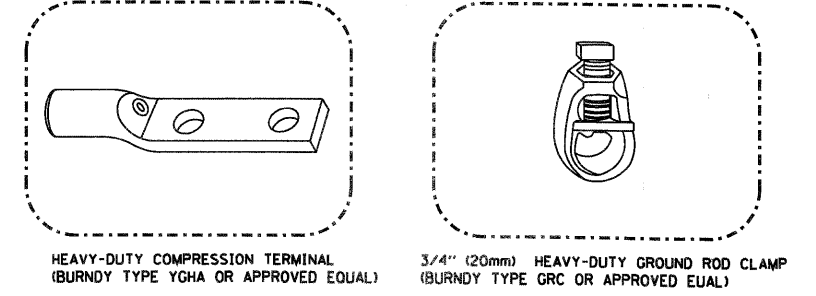
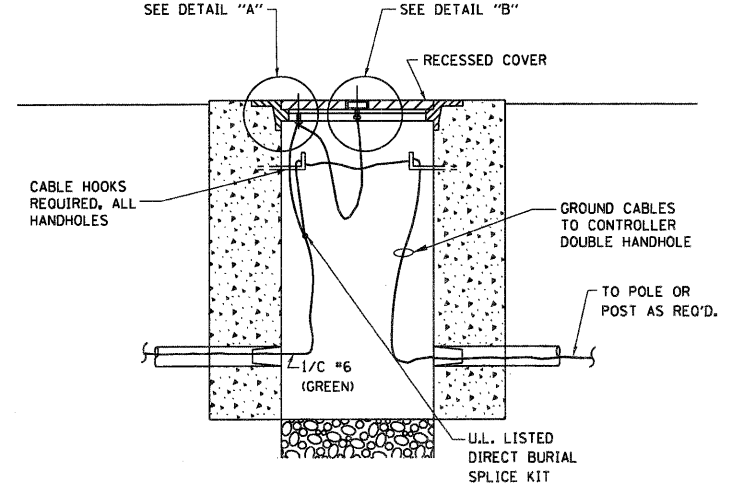
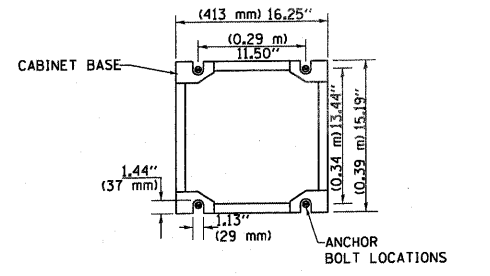
SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)



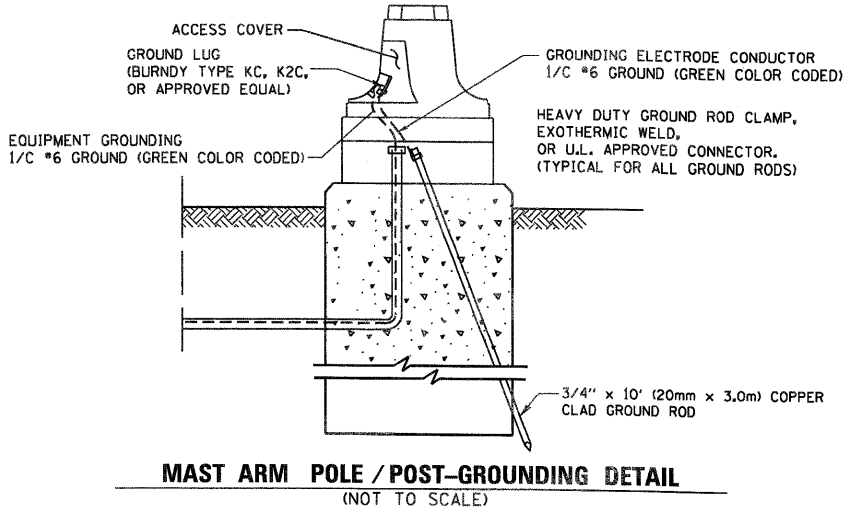
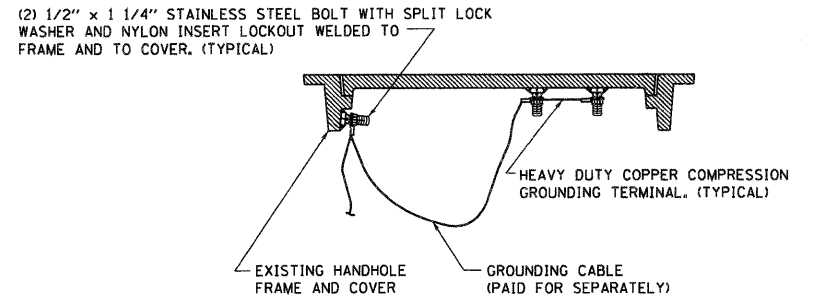
SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)



CABINET - BASE BOLT PATTERN (NOT TO SCALE)



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

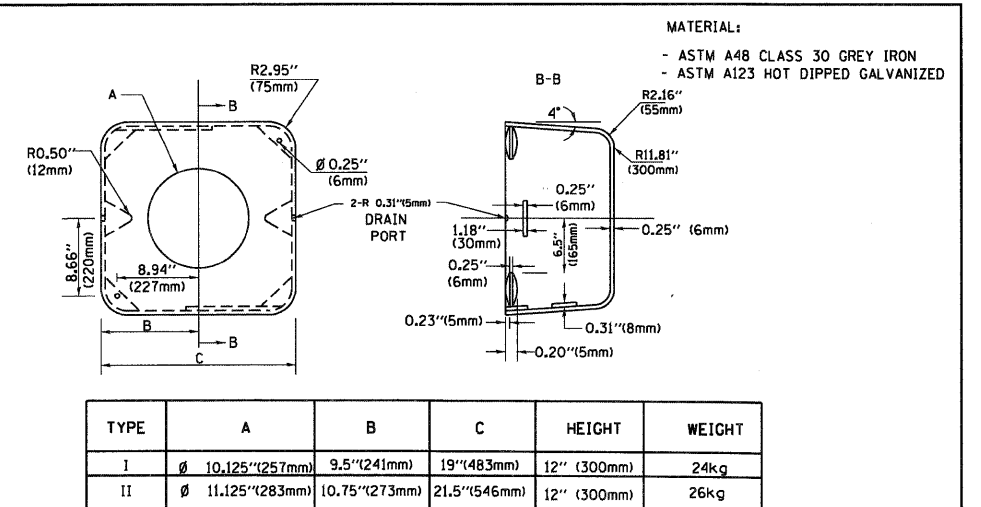
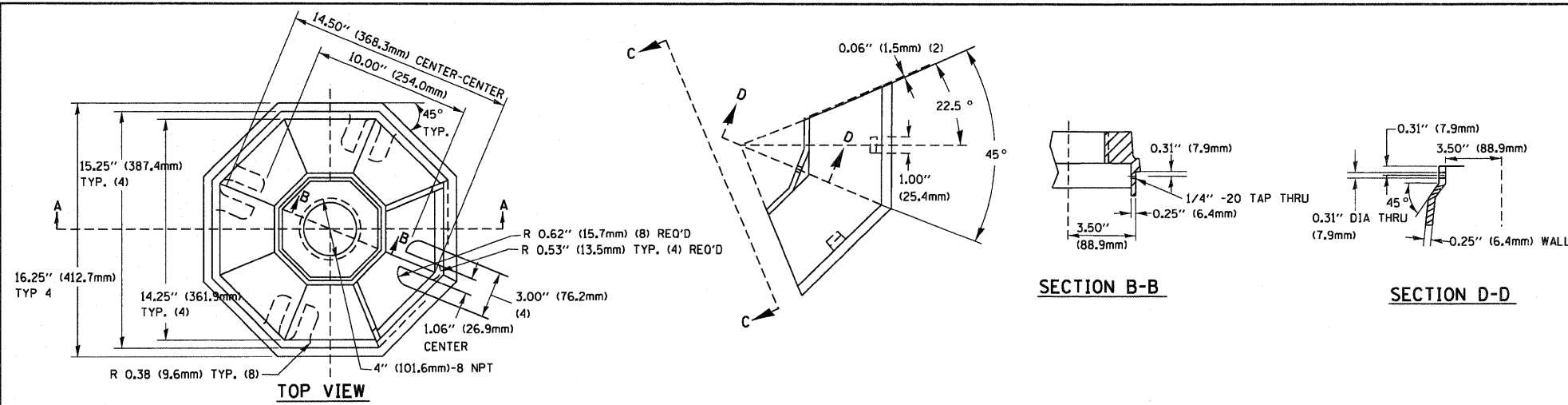


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| | | DRAWN - R.W.P. | REVISED - BUR, TRAFFIC 01-01-02 |
| | PLOT SCALE = 50.0000 "/ IN. | CHECKED - D.A.Z. | REVISED - |
| | PLOT DATE = 1/4/2008 | DATE - 05-30-00 | REVISED - |

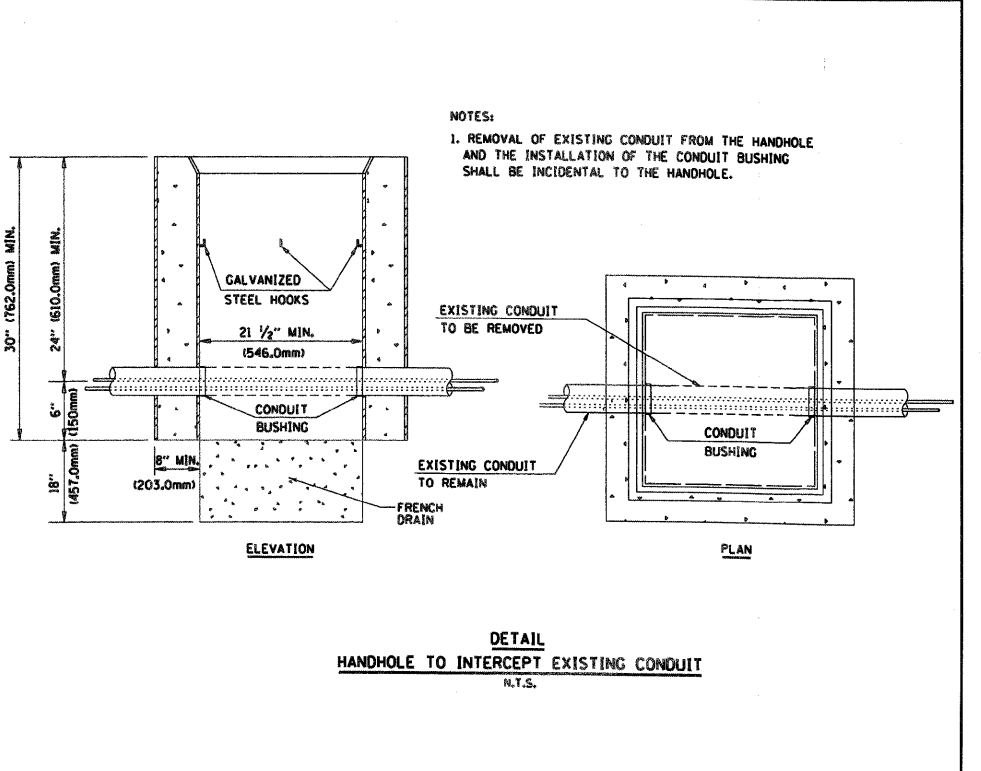
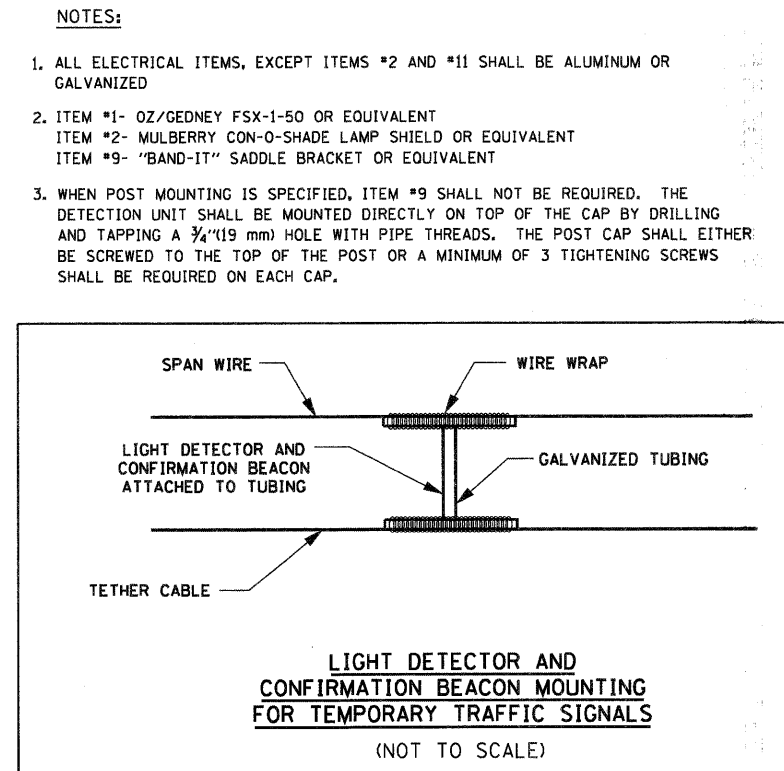
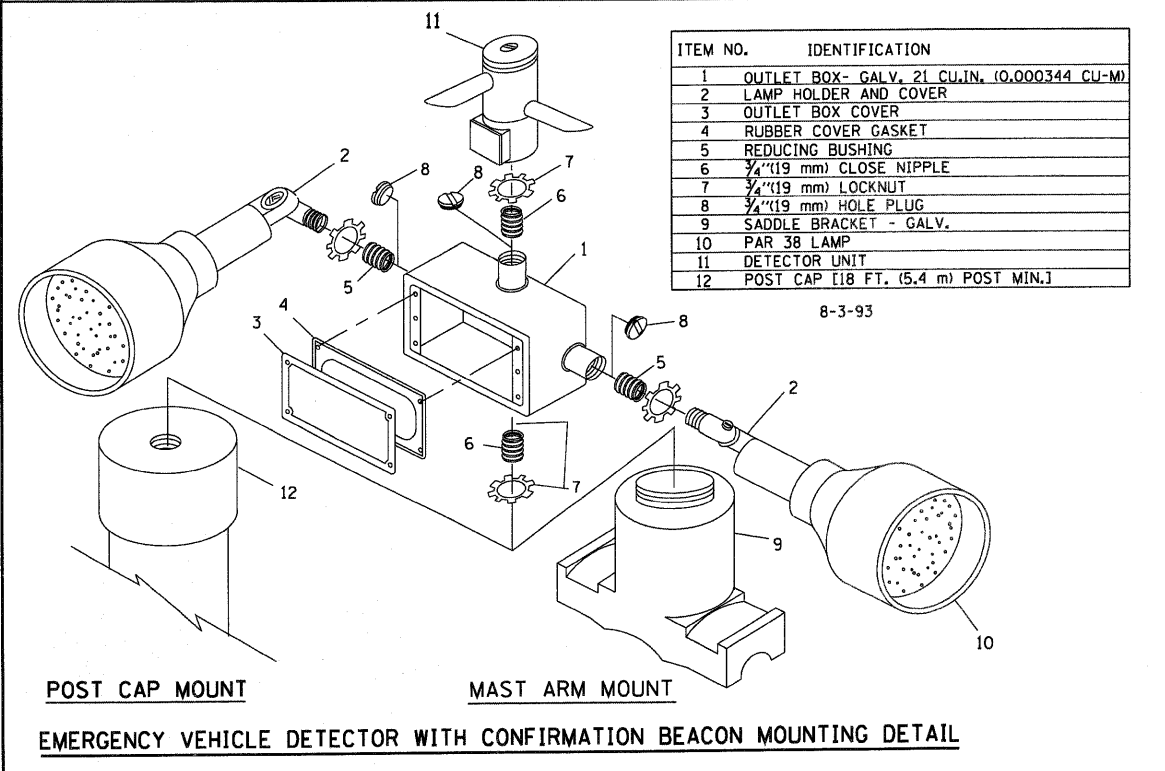
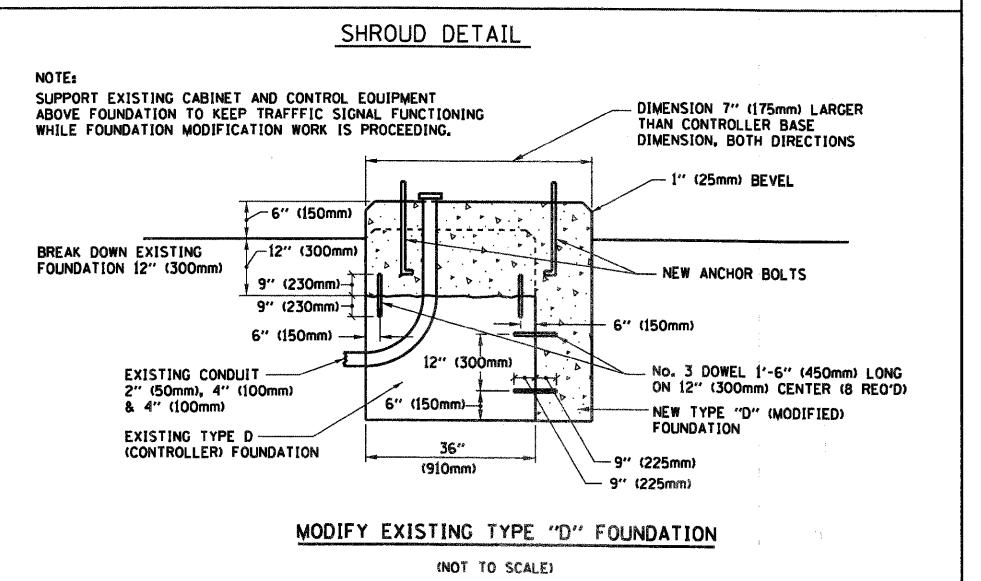
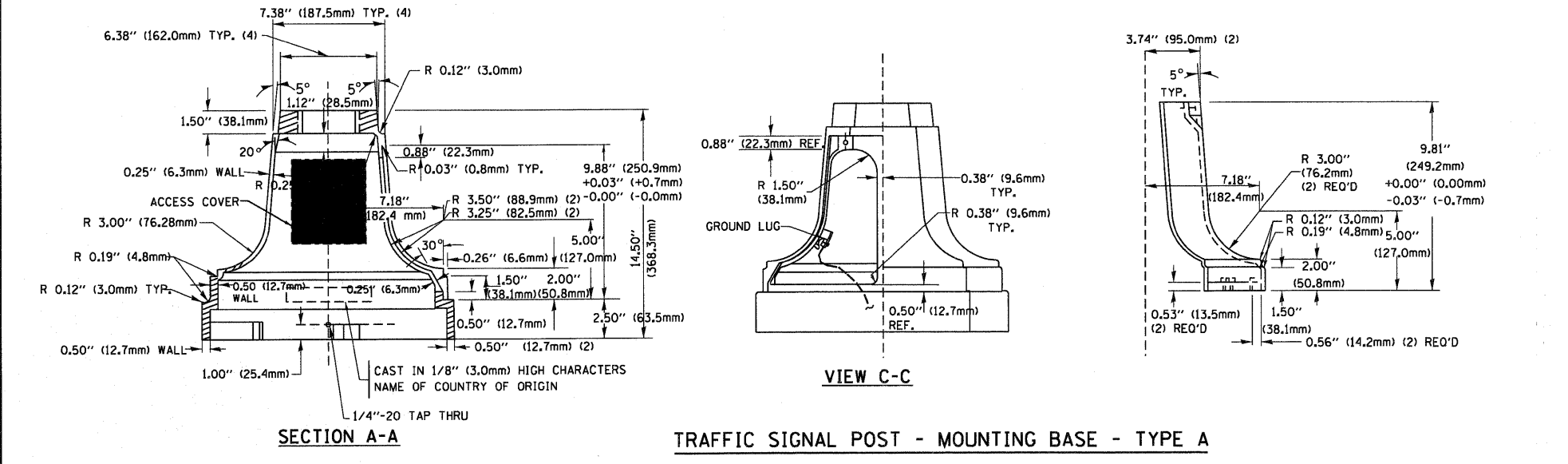
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

| | | | |
|---|-------------------------|--------------|--|
| DISTRICT ONE | | | |
| STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | | |
| SCALE: NONE | SHEET NO. 3 OF 4 SHEETS | STA. TO STA. | |

| | | | | |
|---|----------------------------------|-------------|--------------------|--------------|
| F.A.U. RTE. 1332 | SECTION (3071,3071A & 3072) RS-3 | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 36 |
| TS-05 | | | CONTRACT NO. 62755 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

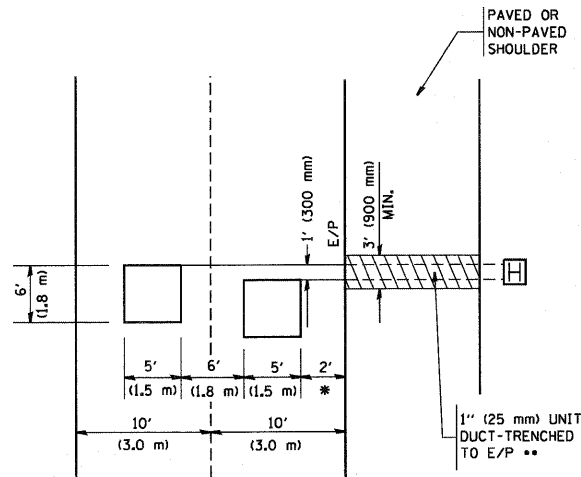


| TYPE | A | B | C | HEIGHT | WEIGHT |
|------|-------------------|----------------|---------------|--------------|--------|
| I | Ø 10.125\"(257mm) | 9.5\"(241mm) | 19\"(483mm) | 12\" (300mm) | 24kg |
| II | Ø 11.125\"(283mm) | 10.75\"(273mm) | 21.5\"(546mm) | 12\" (300mm) | 26kg |



LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

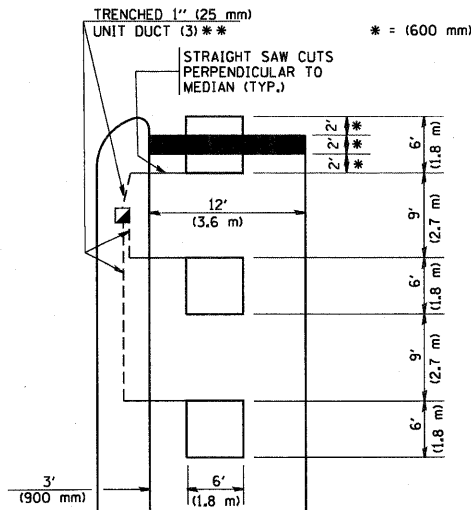


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

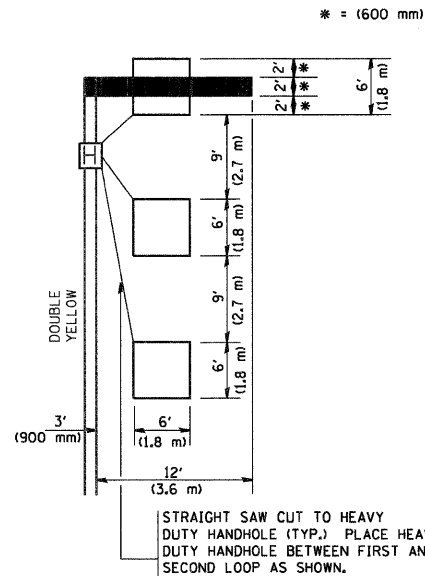
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

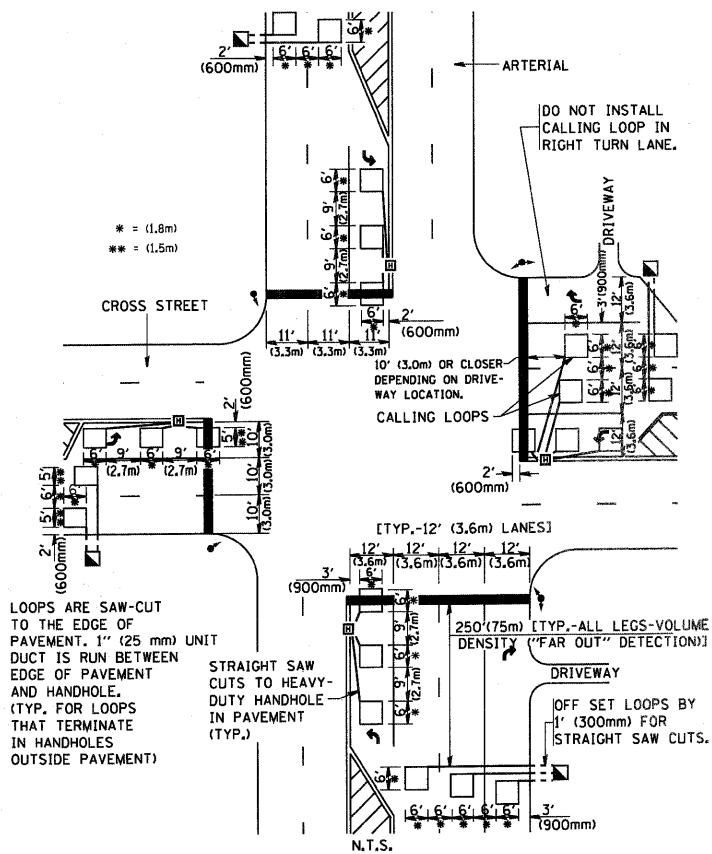
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



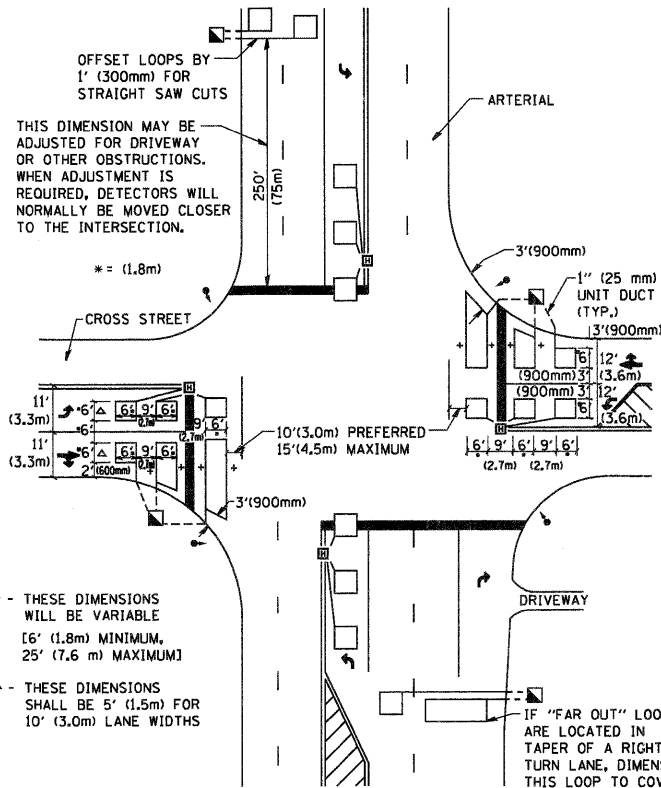
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSS STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn

USER NAME = gaglianobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|----------------------------------|-------------|--------------------|--------------|
| F.A.U. RTE. 1332 | SECTION (3071,3071A & 3072) RS-3 | COUNTY COOK | TOTAL SHEETS 38 | SHEET NO. 38 |
| TS-07 | | | CONTRACT NO. 82755 | |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | |