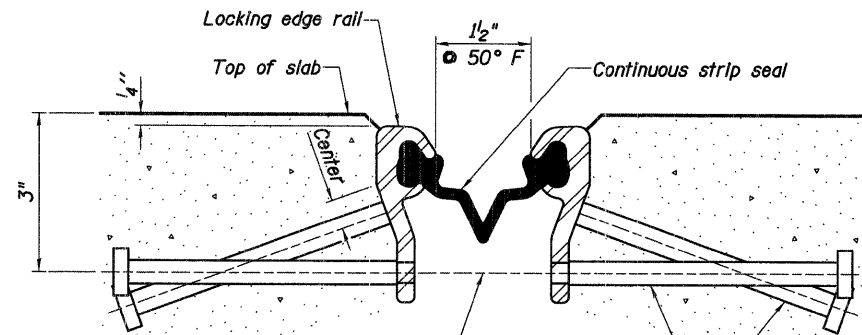


F.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
361	06-00214-20-BR	KANE	320	188
STA. 511+80.00		TO STA. 609+14.92		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		
SHEET NO. S83 OF S108				

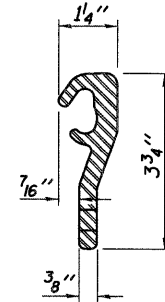


7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

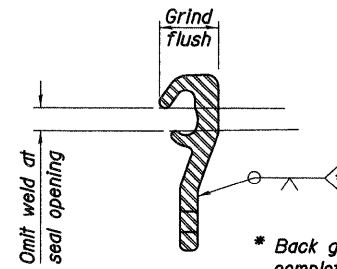
Place 1/2" φ x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.

**SECTION THRU STRIP SEAL JOINT**

(Piers 1, 3, 7, 9, 11, & Intersection between MUP Ramp and MUP Bridge)



**LOCKING EDGE RAIL**



**LOCKING EDGE RAIL SPLICE**

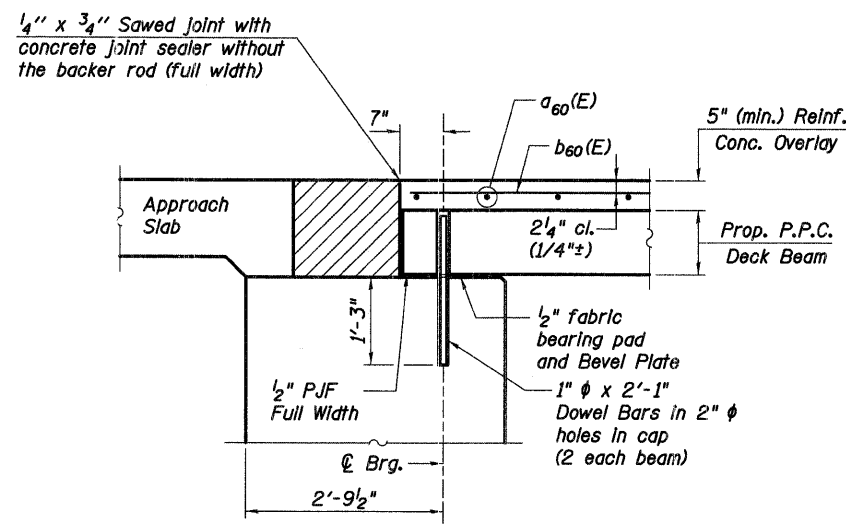
\* Back gouge not required if complete joint penetration is verified by mockup

**Notes:**

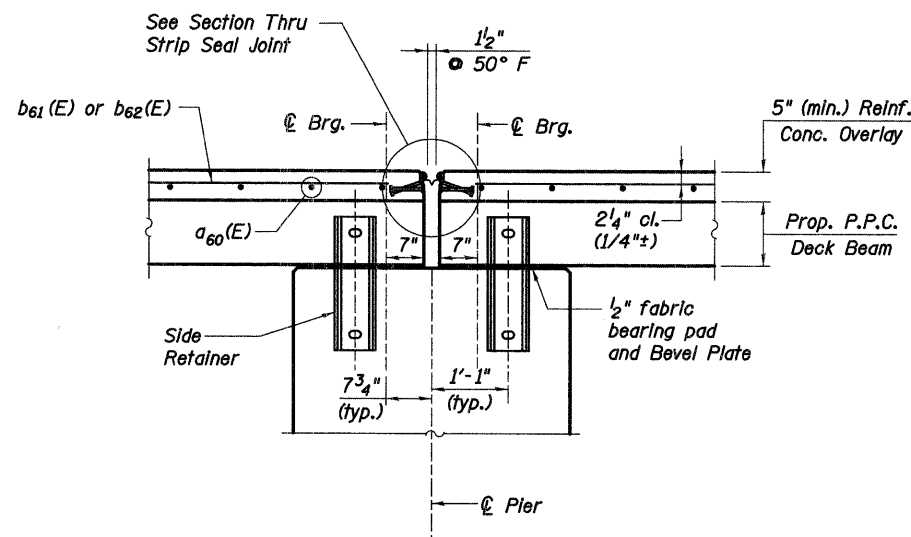
- The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
- The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.
- The manufacturer's recommended installation methods shall be followed.
- The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
- All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
- The inside of the locking edge rail groove shall be free of weld residue
- Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure min. 24 hrs. prior to grouting the shear keys.
- All horizontal dimensions are at right angles to beam ends.
- Hatched area to be poured after concrete wearing surface is in place.
- See sheet S82 of S108 for bearing pad, side retainer, bevel plate, and dowel bar details.

**BILL OF MATERIAL**

Preformed Joint Strip Seal	Foot	97
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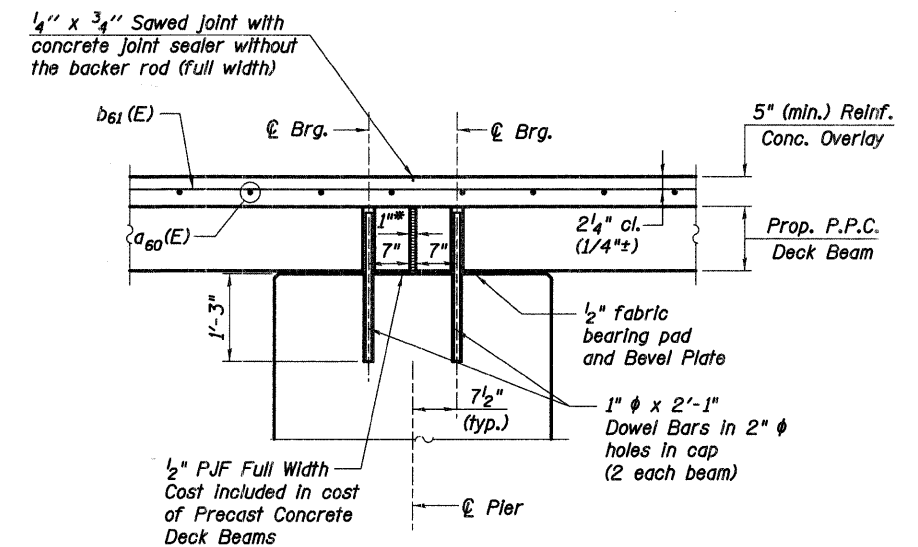


**SECTION THRU ABUTMENT**



**SECTION THRU EXPANSION PIER**

(Piers 1, 3, 7, 9, & 11)



**SECTION THRU FIXED PIER**

(Piers 2, 4, 5, 6, 8, & 10)

\*1" Jt. shall be filled with non-shrink grout. 1" dimension may vary to accommodate tolerance in beam lengths.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**MUP RAMP - JOINT DETAILS**  
 MULTI-USE PATH BRIDGE OVER THE FOX RIVER  
 STRUCTURE NUMBER 045-3164  
 KANE COUNTY FAP 361 SECTION 06-00214-20-BR  
 STATION 572+37.71 DESIGNED: DFM DRAWN: SGW  
 DATE: JANUARY 16, 2009 CHECKED: KPZ CHECKED: KPZ

**Baker**  
 Baker Engineering, Inc.