

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

CONSTRUCTION PROCEDURES

STAGE I (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2" THICK HOT-MIX MATERIAL APPROVED BY THE ENGINEER.

STAGE II (AFTER PAVEMENT MILLING)

- A) REMOVE THE HOT-MIX MATERIAL AND CRUSHED STONE; REMOVE METAL PLATE
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

LOCATION OF STRUCTURES

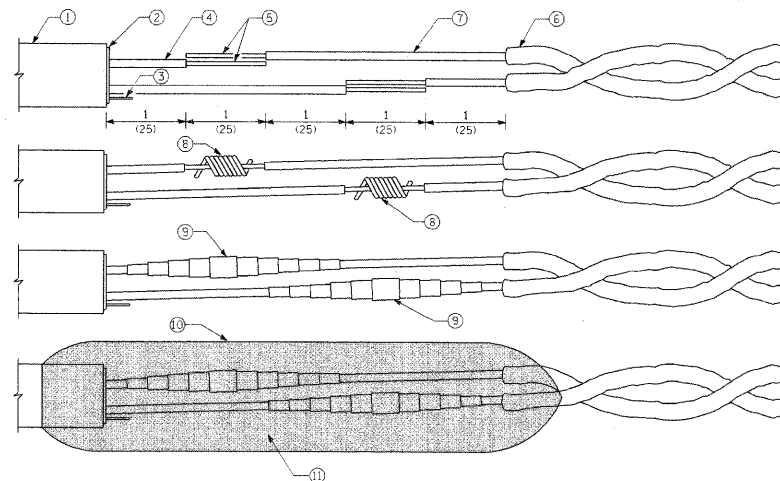
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

STRUCTURE TO BE ADJUSTED.

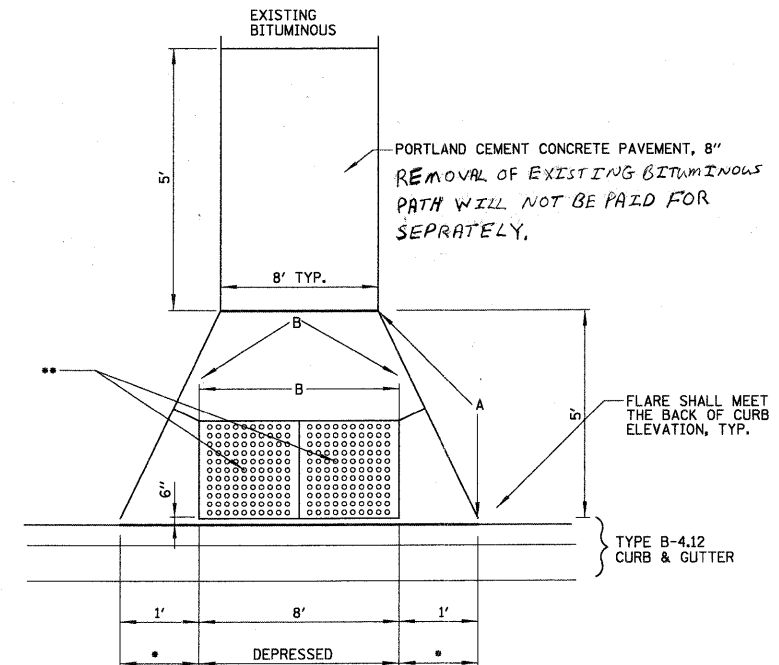
NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.



- 1 = Lead-in cable (single pair or multipair)
- 2 = Lead-in cable shield
- 3 = Lead-in cable shield drain-wire
- 4 = Lead-in cable insulated conductor
- 5 = Bare conductor
- 6 = Loop wire in tube
- 7 = Loop wire insulated conductor
- 8 = Twisted and resin soldered conductor
- 9 = Electrical tape insulated splice
- 10 = Rigid mold
- 11 = Waterproof and dielectric resin

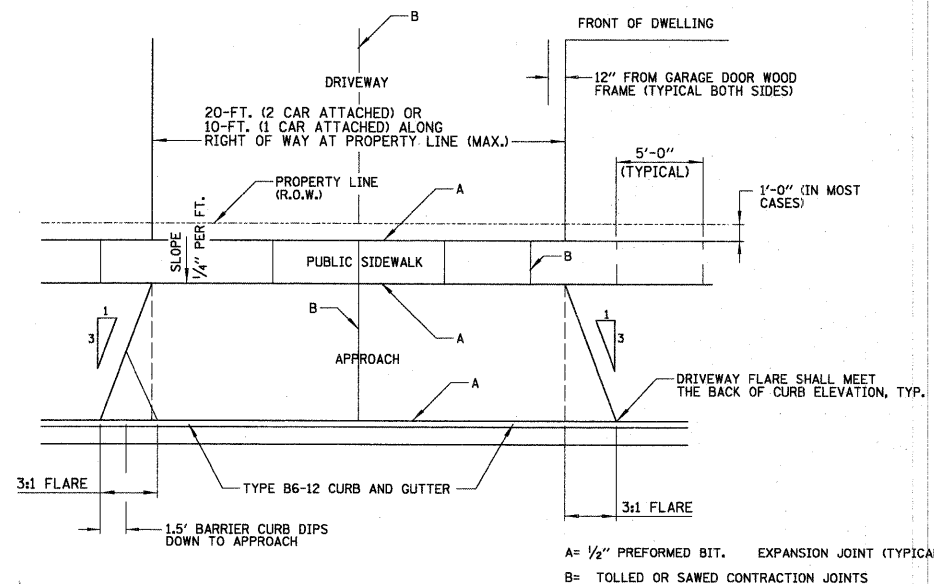
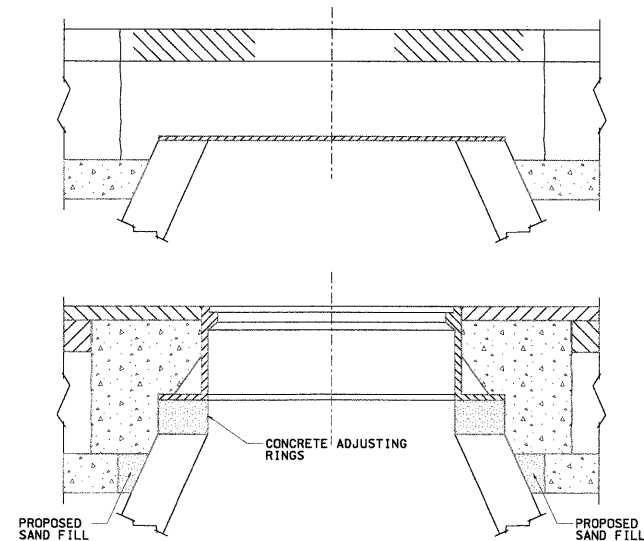
LOOP WIRE AND LEAD-IN CABLE SPLICE



- 1' TRANSITION FROM DEPRESSED CURB AND GUTTER TO FULL HEIGHT CURB AND GUTTER
- META PANEL APPROVED EQUAL DETECTABLE WARNING PLATES (SEE SPECIAL PROVISION), COLOR SELECTED BY VILLAGE
- A = 1/2" PREFORMED EXPANSION JOINT (TYP.)
- B = TOOLED OR SAWED CONTRACTION JOINT

ADA RAMP AT BIKE PATH

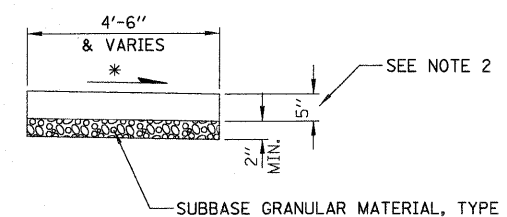
NOT TO SCALE



DRIVEWAY WITH A TYPE B-6.12 CURB AND GUTTER

GENERAL NOTES:

1. DRIVEWAY SHALL HAVE A MIN. SLOPE OF 2% AND MAX. SLOPE OF 6%.
2. APPROACH SHALL HAVE A MIN. SLOPE OF 2% AND MAX. OF 6%.
3. ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED, (95% PROCTOR)
4. PUBLIC SIDEWALK SHALL BE 6" THICK P.C. CONCRETE AT DRIVEWAY. (NO WIRE MESH)
5. MINIMUM THICKNESS FOR APPROACH (NO WIRE MESH) THIS WILL BE PAID FOR BY THE FOLLOWING ITEMS:
 - A. 7" THK. P.C. CONCRETE ON 2" AGGREGATE BASE COURSE TYPE B OR
 - B. 3" THK. HOT-MIX ASPHALT SURFACE, MIX "C" N50 ON 6" AGGREGATE BASE COURSE TYPE B



- CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS
- 1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCIDENTAL TO THE P.C.C. SIDEWALK 5 INCH, REMOVE AND REPLACE
- 2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFIELD WITHIN 24 HOURS.
- 3. IDOT CL 1A SEED MIX, EROSION CONTROL BLANKET AND TOPSOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF SIDEWALK REMOVAL AND REPLACEMENT.

P.C.C. SIDEWALK DETAIL

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IDOT CL 1A SEED MIX, EROSION CONTROL BLANKET AND TOPSOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF SIDEWALK REMOVAL AND REPLACEMENT.

PUBLIC SIDEWALK THICKNESS SHALL BE 6" AT RESIDENTIAL DRIVEWAYS AND 8" AT COMMERCIAL/INDUSTRIAL DRIVEWAYS.

FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONSTRUCTION DETAILS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\Huntley\070103\070103.00011\Civil\det.	70103.00011-01.sht	DRAWN - MAG	REVISED -			0035	08-00033-00-RS	McHENRY	19	14	
PLOT SCALE = 5'		CHECKED - LMF	REVISED -			CONTRACT NO. 63141					
PLOT DATE = 2/18/2009		DATE - 2/18/2009	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: NTS	SHEET NO. OF SHEETS		STA. TO STA.			