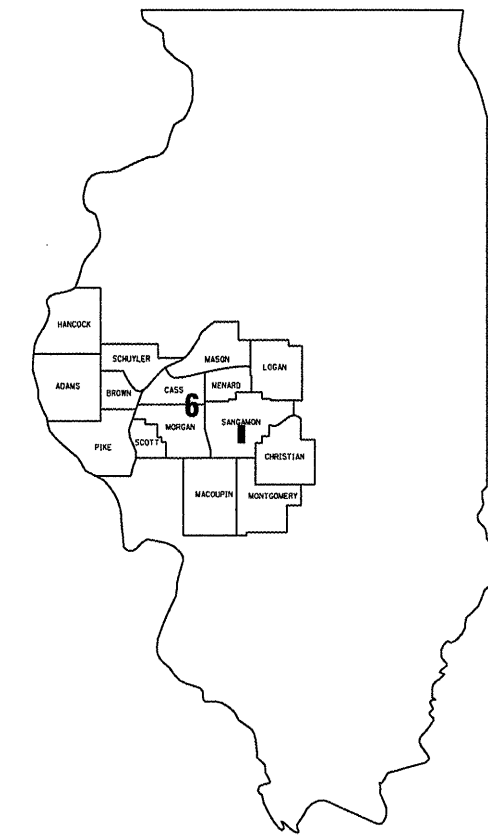


FOR INDEX OF SHEETS AND HIGHWAY
STANDARDS, SEE SHEET 2.

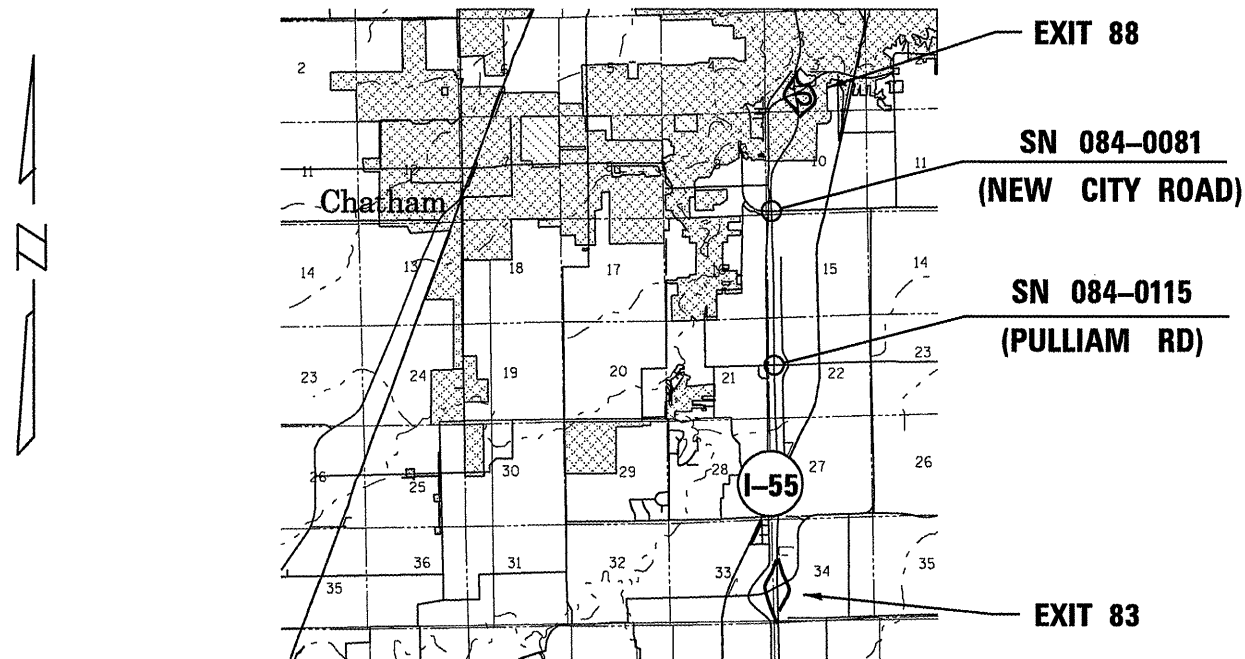
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

CONTRACT: 72C65
SHEET 1 OF 13

**PROPOSED
HIGHWAY PLANS**
FAI 55 (I-55)
SECTION D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY
C96-052-09 / D96-052-09



LOCATION OF SECTION INDICATED THUS: **█**



NET LENGTH OF SEC = 1.5 MILE

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

BRIDGE INSPECTION ENGINEER: DAVE COPENBARGER
PHONE: (217) 785-5306

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 27 20 09

Rege Z. Duckert
DEPUTY DIRECTOR OF HIGHWAYS, REGION FOUR ENGINEER

March 27 20 09

Charles J. Ingersoll
ENGINEER OF DESIGN AND ENVIRONMENT

March 27 20 09

Christine M. Reed
DIRECTOR, DIVISION OF HIGHWAYS

CONTRACT NO. 72C65

INDEX OF SHEETS:

- 1 COVER SHEET
- 2 INDEX, HIGHWAY STANDARDS, & GENERAL NOTES
- 3 SUMMARY OF QUANTITIES
- 4-5 TRAFFIC CONTROL DETAILS
- 6-9 EXISTING PLANS, SN 084-0081
- 10-13 EXISTING PLANS, SN 084-0115


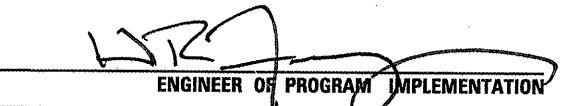
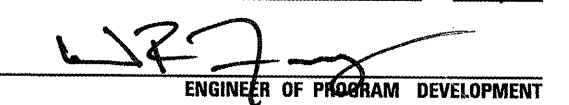
STANDARDS

- 701001-02
- 701006-03
- 701101-02
- 701106-02
- 701316-04
- 701400-03
- 701401-05
- 701446-01
- 701901-01

GENERAL NOTES:

CLEANING AND PAINTING OF THE EXISTING STRUCTURAL STEEL SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS FOR "CLEANING AND PAINTING EXISTING STEEL STRUCTURES". ALL EXISTING STEEL SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING - SSPC-SP10. ALL EXISTING STEEL SHALL BE PAINTED ACCORDING TO THE REQUIREMENTS OF PAINT SYSTEM 1 - OZ/E/U. THE COLOR OF THE FINAL FINISH COAT FOR EXTERIOR AND BOTTOM FLANGE OF THE FASCIA BEAMS SHALL BE INTERSTATE GREEN, MUNSELL NO 7.5G 4/8. THE COLOR OF THE FINAL FINISH COAT FOR ALL INTERIOR STEEL SURFACES SHALL BE GRAY, MUNSELL NO 5B 7/1. THE SSPC-QP-1 AND SSPC-QP2 PAINTING CONTRACTOR CERTIFICATIONS WILL BE REQUIRED FOR THESE BRIDGES. THE USE OF AIR MONITORS WILL BE REQUIRED AT STRUCTURE NUMBER 1.

STRUCTURE NO 1 = 084-0081, STRUCTURE NO 2 = 084-0115

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS DISTRICT 6	
EXAMINED	Jan 16 20 09
 ENGINEER OF OPERATIONS	
EXAMINED	Jan 24 20 09
 ENGINEER OF PROGRAM IMPLEMENTATION	
EXAMINED	Jan 22 20 09
 ENGINEER OF PROGRAM DEVELOPMENT	

INDEX, STANDARDS, & NOTES
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

Rev.

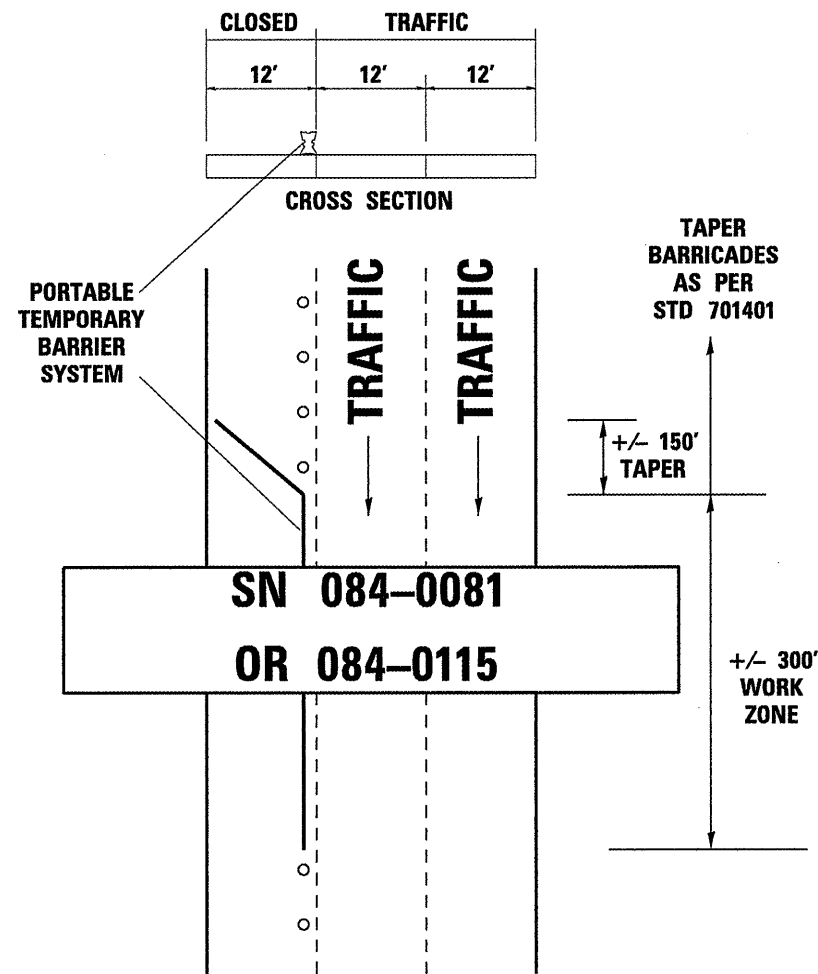
S U M M A R Y O F Q U A N T I T I E S

CODE NO.	ITEM	UNIT	100% STATE
			SFTY-2A
			TOTAL QTY
50600600	CLEANING AND PAINTING STEEL BRIDGE, NO. 1	L SUM	1
50600700	CLEANING AND PAINTING STEEL BRIDGE, NO. 2	L SUM	1
50606401	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES, NO. 1	L SUM	1
50606402	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES, NO. 2	L SUM	1
67100100	MOBILIZATION	L SUM	1
70100100	TRAFFIC CONTROL AND PROTECTION, STANDARD 701316	EACH	2
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1
70100815	TRAFFIC CONTROL AND PROTECTION, STANDARD 701446	L SUM	1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	2
70106800	CHANGEABLE MESSAGE SIGNS	CAL MO	5
X0324401	PORTABLE TEMPORARY BARRIER SYSTEM, TEST LEVEL 3	FOOT	600
X7040092	RELOCATE PORTABLE TEMPORARY BARRIER SYSTEM, TEST LEVEL 3	L SUM	1

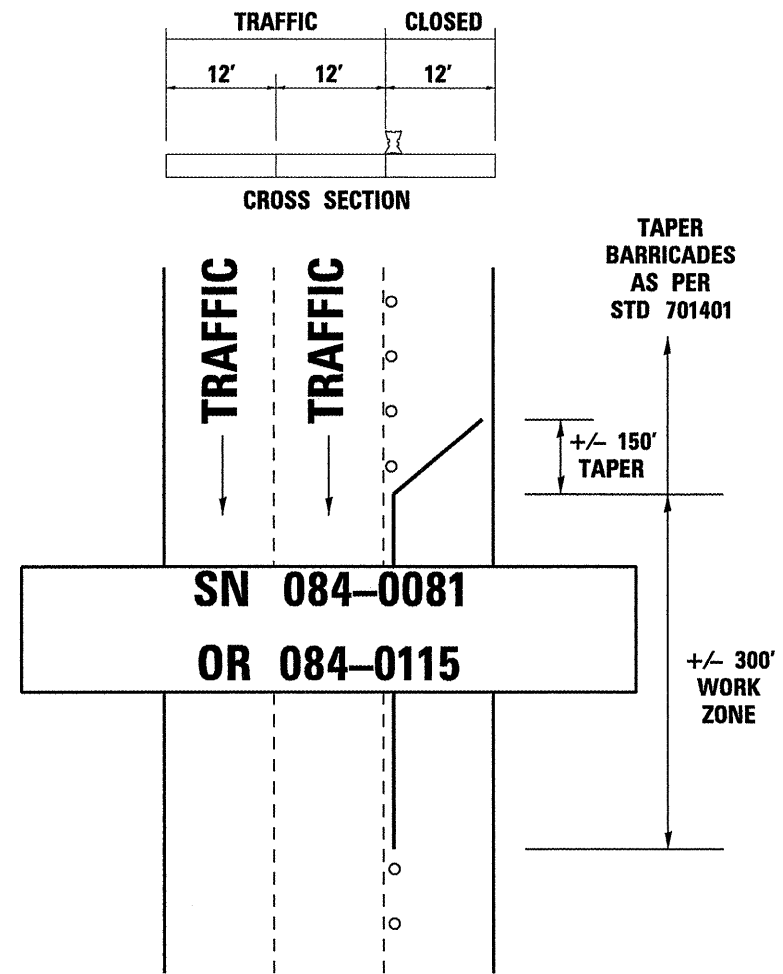
QUANTITIES
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

Rev.

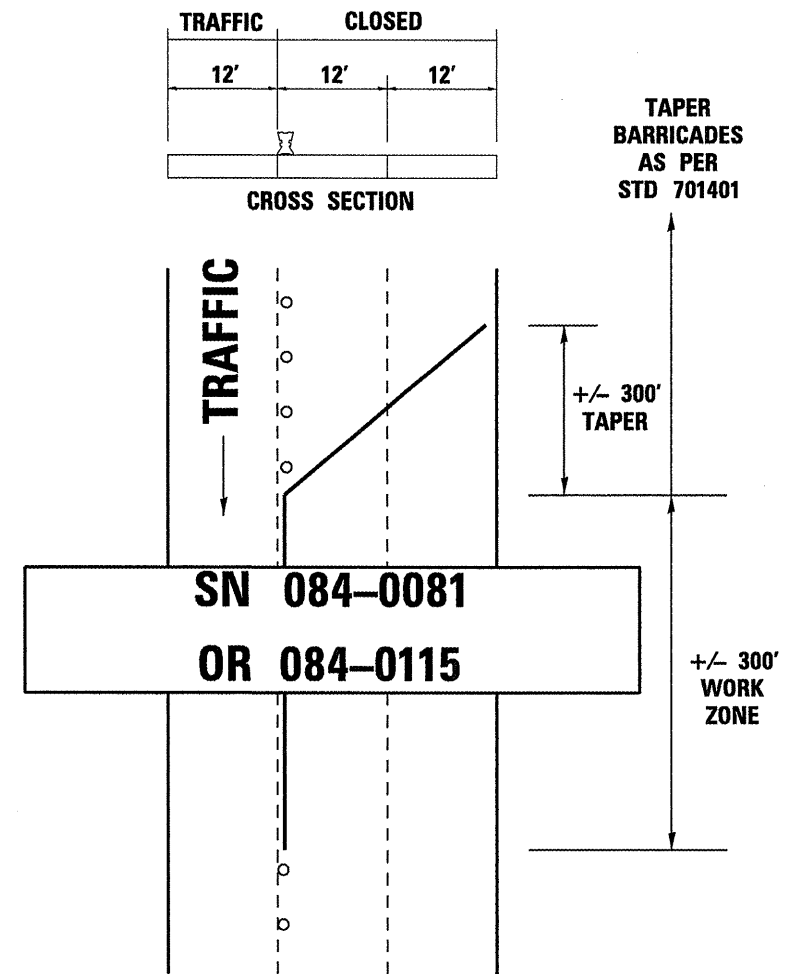
LANE CLOSURE 1



LANE CLOSURE 2



LANE CLOSURE 3



NOTES:

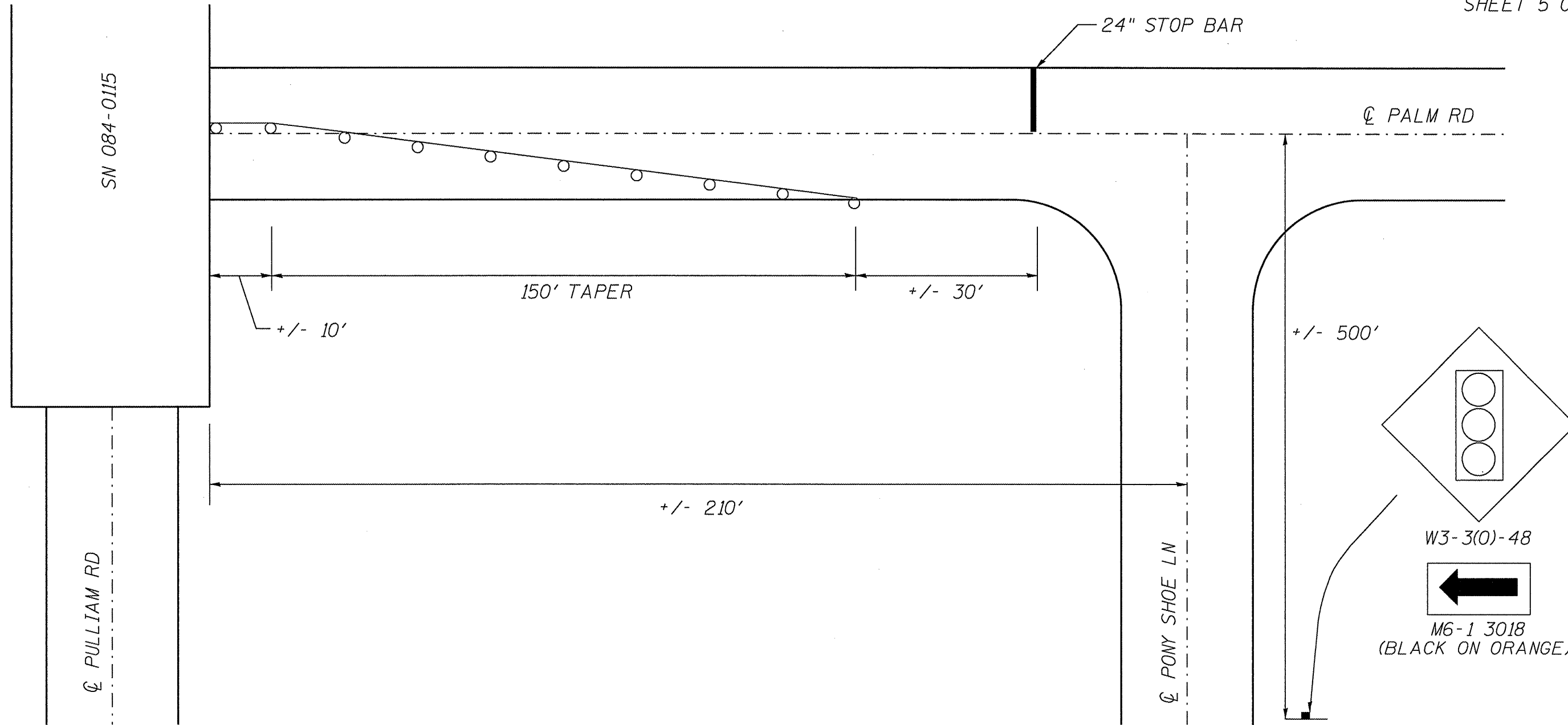
ONLY SOUTHBOUND LANES ARE SHOWN. NORTHBOUND LANES SHALL FOLLOW THE SAME PLAN.

THE CONTRACTOR MAY ELECT TO CHANGE THE ORDER OF THE LANE CLOSURES WITH APPROVAL FROM THE ENGINEER, BUT THE CENTER LANE SHALL ONLY BE CLOSED IN CONJUNCTION WITH THE ADJACENT PASSING LANE.

ALL BARRICADES, BARRICADE LIGHTING, SIGNING, ETC. SHALL BE PAID FOR UNDER TRAFFIC CONTROL AND PROTECTION, STANDARD 701401. THE PORTABLE TEMPORARY BARRIER SYSTEM SHALL BE PAID FOR SEPARATELY BY THE FOOT.

TANGENT LENGTHS OF THE PORTABLE TEMPORARY BARRIER SYSTEM MAY BE SHORTENED WHEN DEEMED SAFE TO DO SO BY THE ENGINEER. BARRIER SYSTEM LENGTHS MAY ONLY BE LENGTHENED WITH PRIOR APPROVAL FROM THE ENGINEER.

I-55 LANE CLOSURE DETAIL
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY



NOTES:

THE CONTRACTOR SHALL ENSURE THAT THE TRAFFIC SIGNALS ARE VISIBLE FROM PONY SHOE LANE.

THIS DETAIL APPLIES TO THE SOUTH PORTION OF THE LANE CLOSURE FOR SN 084-0115 ONLY. ALL OTHER AREAS SHALL CONFORM TO STANDARD 701316. ALL TRAFFIC CONTROL ITEMS DETAILED IN STANDARD 701316 THAT ARE NOT DETAILED ON THIS SHEET SHALL STILL BE PLACED IN THIS LOCATION AS PER THE STANDARD. THE CONTRACTOR SHALL INCLUDE ALL ADDITIONAL WORK AND MATERIAL FOR ITEMS ON THIS SHEET IN THE BID PRICE FOR TRAFFIC CONTROL & PROTECTION, STANDARD 701316

THIS DETAIL SHALL BE TYPICAL FOR BOTH THE SOUTHBOUND AND NORTHBOUND CLOSURES AT THIS LOCATION.

SN 084-0115 CLOSURE DETAIL
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

084-0081

DATE	3/18	BY	CS
DATE	5/12	BY	CS

GENERAL NOTES

Coarse aggregate to be used in parapet handrail and abutment wingwalls must be free of chert, flint, limonite, lignite and soft sandstone.
The concrete floor slab shall be finished in accordance with Article 519 of the Standard Specifications.
Permanent forms will not be permitted in forming the concrete floor.
All reinforcement bars shall be lapped 24 diameters unless otherwise shown.
Slope wall shall be reinforced with welded wire fabric, 6"x6" mesh, weighing 50 lbs per 100 sq ft.
For item Precast Prestressed Concrete Beam, see supplemental specifications effective January 3, 1966.
All structural steel shall conform to A.S.T.M. Designation A-36.
All welding shall conform to the current specifications for Welded Highway and Railway Bridges of the American Welding Society and Special Provisions.
Field welding of construction accessories to the bottom flanges, or for a distance of 1/6 of the span length each way from pier supports on the top flanges of beams or girders, will not be permitted. Field welding in other areas will be permitted only when approved by the Engineer.
Rivets 3/4", Open Holes 1 1/4", unless otherwise noted.
Anchor bolts shall be set before fastening cross frames over supports.
Stud shear connectors on the beam flanges shall be placed in the field after the steel has been erected and the deck forms are in place.
Flange shear connectors are included in the quantity of structural steel.
Estimated Weight - 1930 lbs.
Number required - 3140
Except as otherwise provided, all structural steel shall receive one shop coat of paint and two field coats of paint. See special provisions for paint.
Exposed surfaces of the expansion devices, which are inaccessible after erection, shall receive two shop coats of paint. All other surfaces shall be given one shop coat of paint. Anchor studs shall not be painted.
Expansion devices are included in the quantity of structural steel. Est. weight - 2880 lbs.
Excavation for portions of structures in the embankment shall not be classified.
The Contractor shall drive two (2) steel test piles, one each at the West Abut. and of the Pier 2 such in a permanent location as directed by the Engineer before ordering the remainder of the piles.
Steel piles at Approach Bent shall be driven in holes precored thru the embankment in accordance with Article 60.9 (c) of the Standard Specifications.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Class A Excav for Structure	Cu Yds		240	240
Class X Concrete	Cu Yds	442 B	306.5	748.5
Structural Steel	Lbs	31100		31100
Aluminum Handrail	Lin Ft	690		690
Pipe Culverts (Type-A) 36"	Lin Ft			320
Reinforcement Bars	Lbs	106390	33000	139390
Steel Piles (B.P.36)	Lin Ft		2663	2663
Test Piles, Steel (B.P.36)	Lbs			2
Name Plates	ea			1
Slope Wall (A)	Sq Yds			202
Bridge Seat Sealant	L.S.		1	1
Precast Prestressed Conc. P-Beam 36"	Lin Ft	380		380
Protective Coat	Sq Yds	1670		1670

* Applied at Abutments only.

STA. 224+0.10
BUILT 19 BY
STATE OF ILLINOIS
FAI. RT. 55 SEC. 34-4-34B
FA PROJ. I-55-306
LOADING H5 20

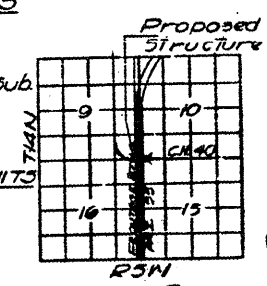
NAME PLATE
Sec Std 2113-1

DESIGN STRESSES
FIELD UNITS

$f_c = 1200$ psi - Deck Slab
 $f_c = 1400$ psi - Curb, Parapet, Sub.
 $f_s = 20,000$ psi - Reinf.
 $f_s = 20,000$ psi - Struct.
 $v_c = 75$ psi - Ftgs.

PRECAST PRESTRESSED UNITS

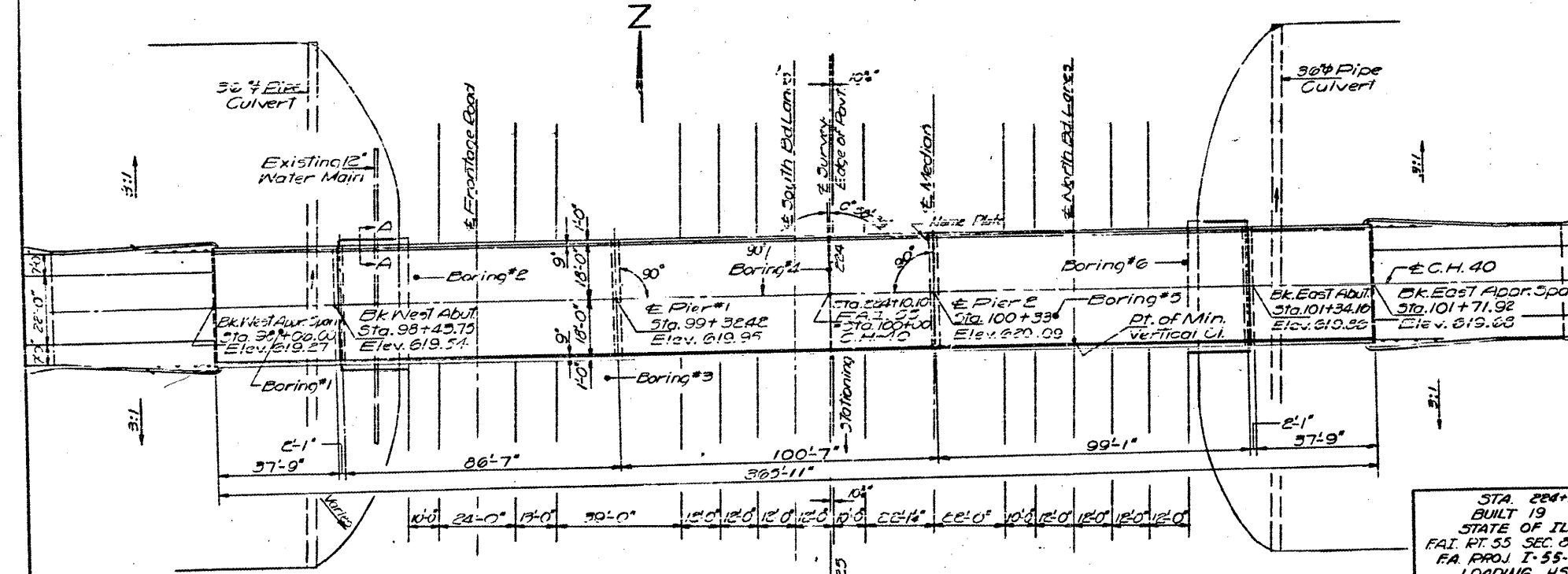
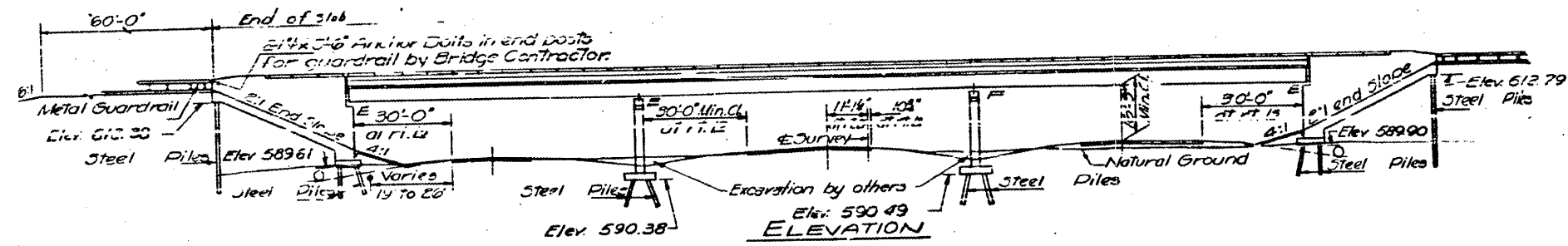
$f_c = 5000$ psi.
 $f_t = 4000$ psi.
 $f_s = 248,000$ psi - Strands
 $f_t = 173,600$ psi - Strands
Allowable Fut. N.S. 25"/ft
Allowable Deflection 1/200 Composite
LOADING H5 20-44



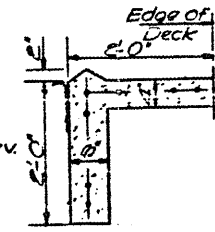
084-00

B.M. #12 DR Spike in Root of 40' Cherry Tree.
121.160' of Station 225+20 Elev. 597.71

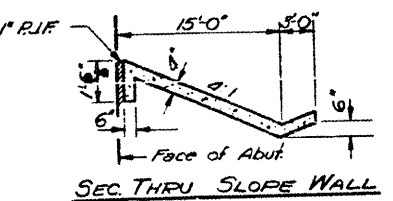
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS



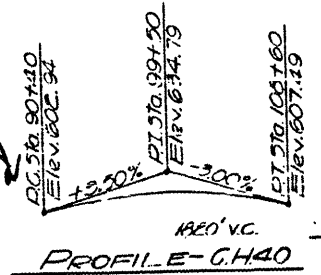
PLAN



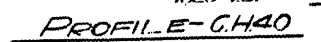
SEC. A-A



SEC. THRU SLOPE WALL



PROFILE-FAI. 55



PROFILE-C.H.40

DESIGNED P.A. STANISLAW
CHECKED J.W. CLARK
DRAWN J.M. HENNINGER
CHECKED J.W. CLARK

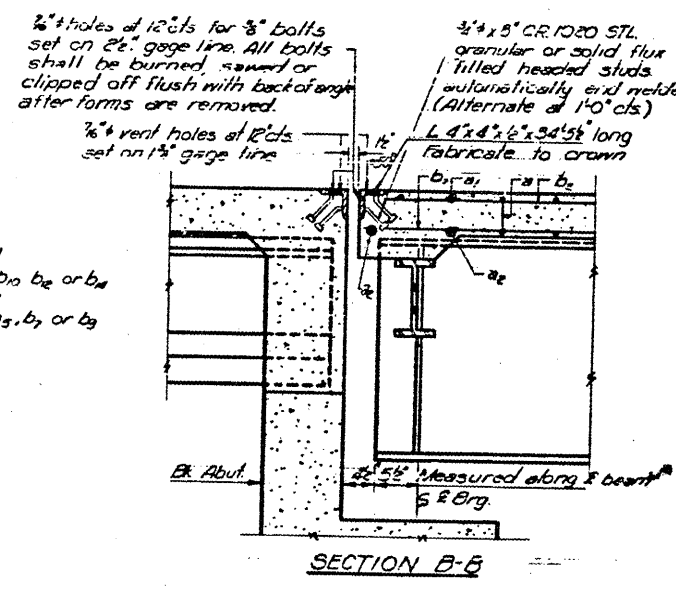
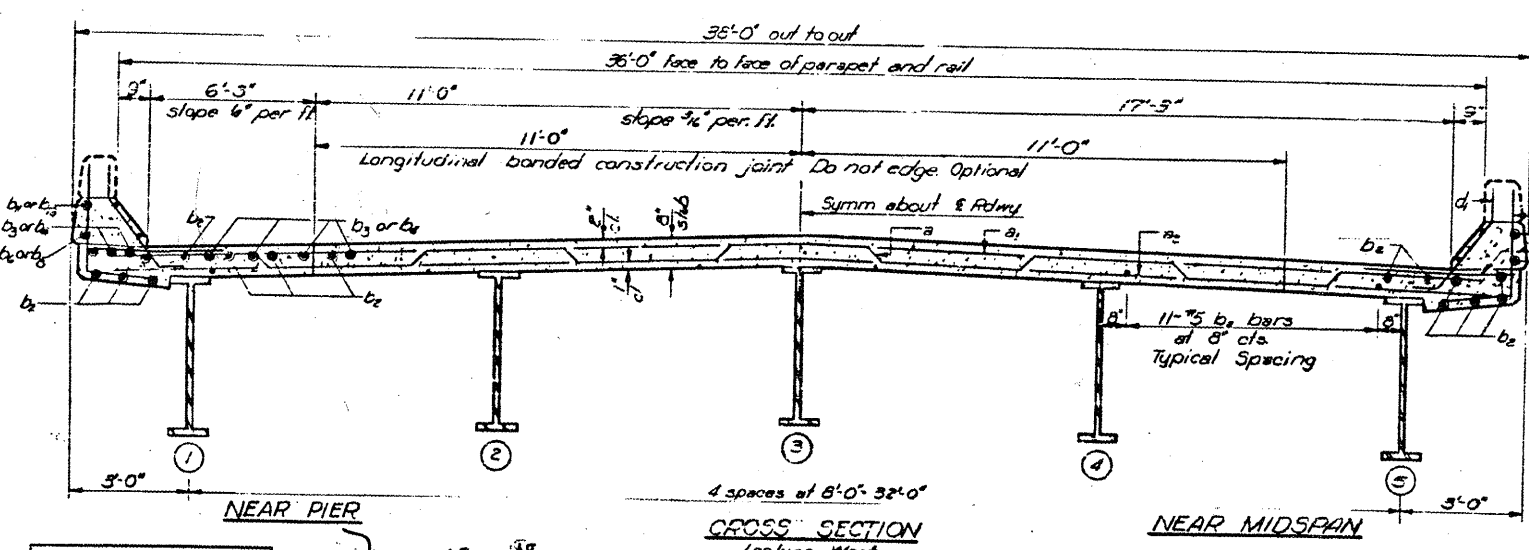
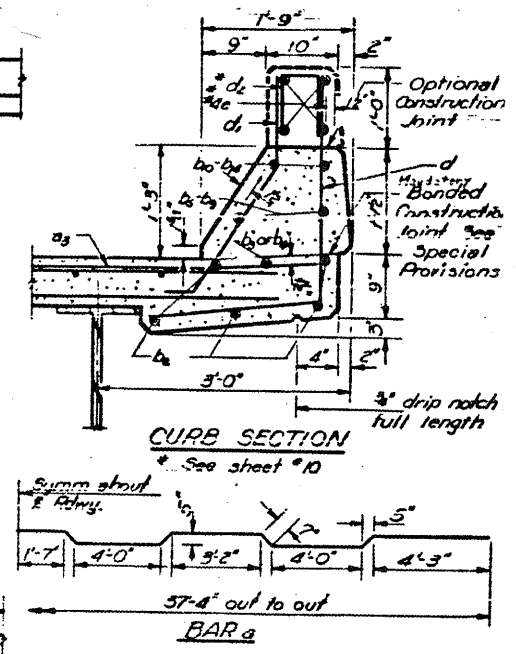
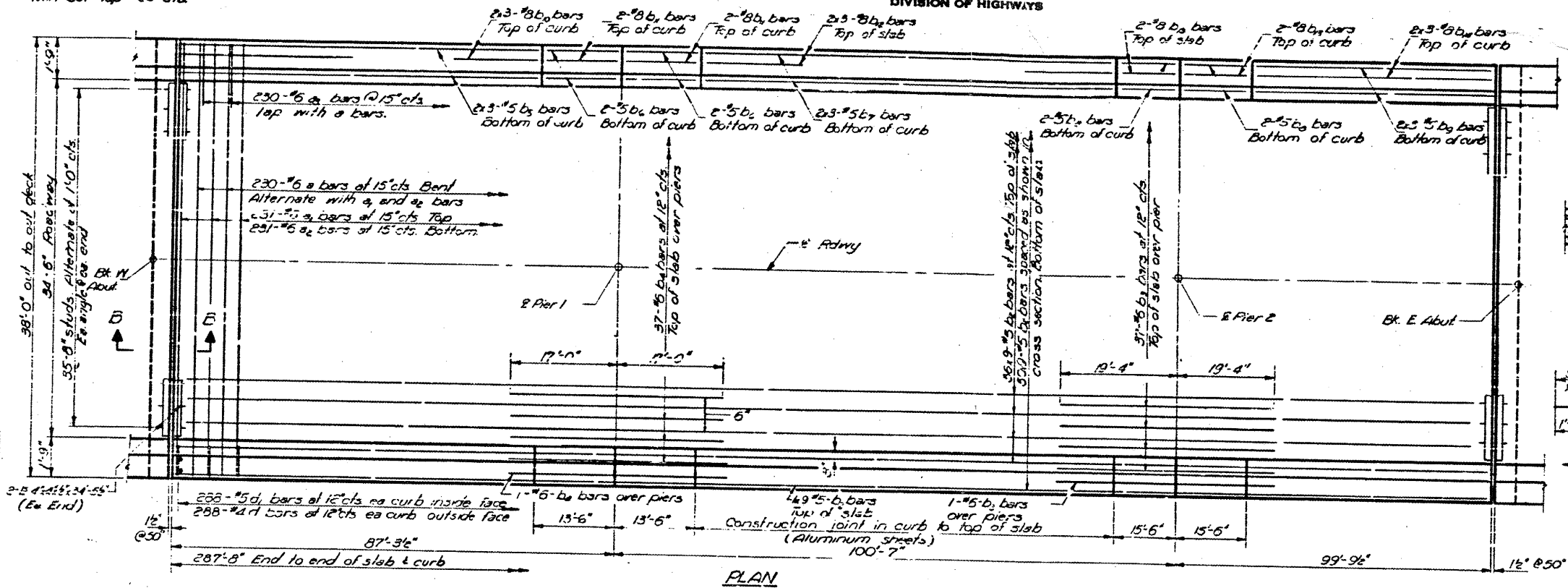
EXAMINED J.W. CLARK
PASSED J.W. CLARK
APPROVED J.W. CLARK

Rev. 9/15/68 JBN

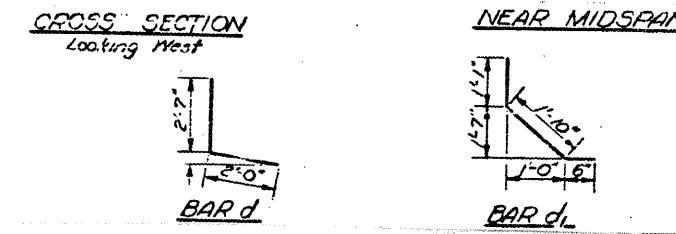
FOR INFORMATION ONLY

EXISTING PLANS, SN 084-0081
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

Notes:
Bars indicated thus 20x3-5etc
indicates 20 lines of bars with
3 lengths per line.
Min bar lap = 2d dia.



DESIGNED: A. R. ...
CHECKED: ...
DRAWN: J. M. ...
CHECKED: ...



BILL OF MATERIAL

Bar No	Size	Length	Shape
a	230 #6	38'-5"	—
a	231 #6	37'-4"	—
a	231 #6	37'-0"	—
a	460 #6	4'-0"	—
b ₂	792 #5	33'-3"	—
b ₂	30 #6	33'-5"	—
b ₂	33 #6	32'-4"	—
b ₂	12 #5	25'-6"	—
b ₂	8 #5	13'-3"	—
b ₂	12 #5	22'-8"	—
b ₂	8 #5	13'-9"	—
b ₂	12 #5	28'-10"	—
b ₂	12 #5	26'-0"	—
b ₂	8 #5	13'-5"	—
b ₂	12 #5	25'-4"	—
b ₂	8 #5	15'-3"	—
b ₂	12 #5	29'-4"	—
d	576 #4	4'-7"	L
d	576 #5	3'-5"	L
Reinforcement Bars	Lbs	21,870	
Structural Steel	Lbs	31,100	
Class X Concrete	Cu Yd	310.8	

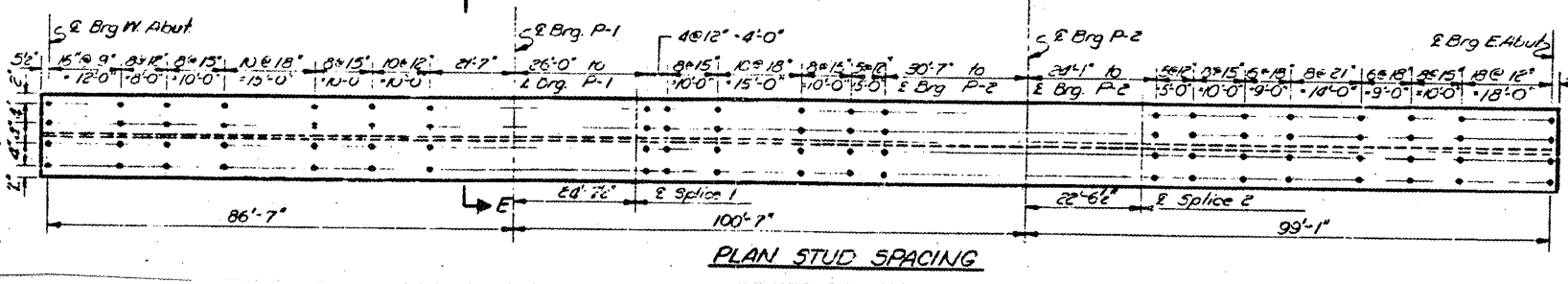
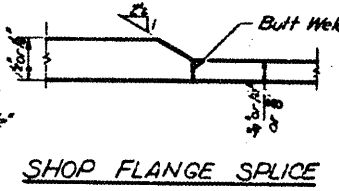
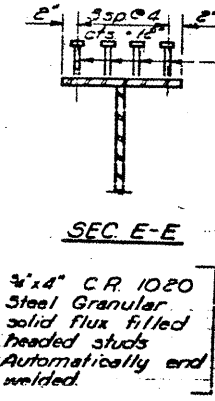
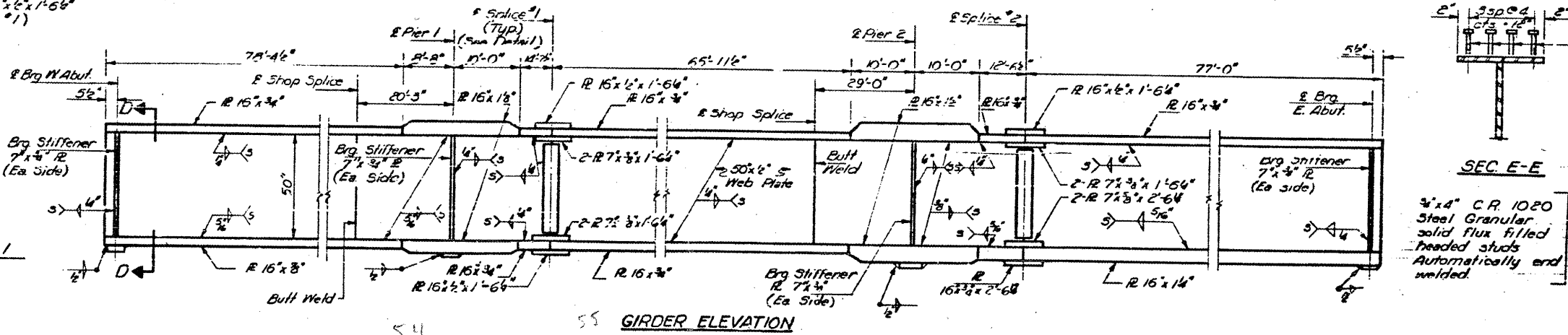
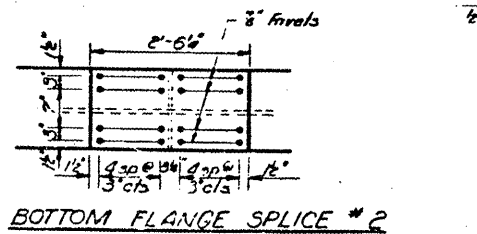
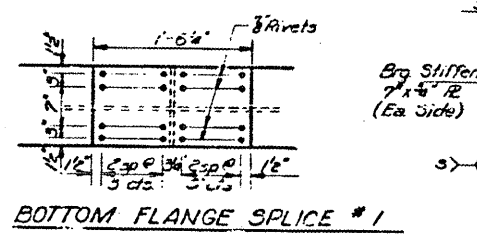
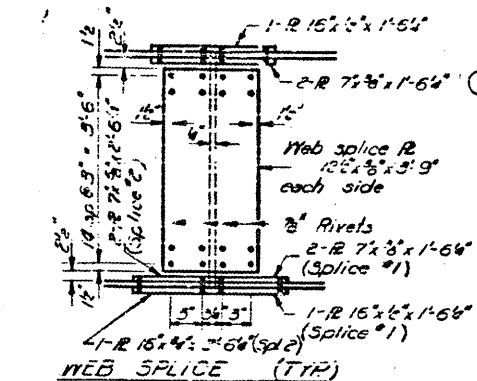
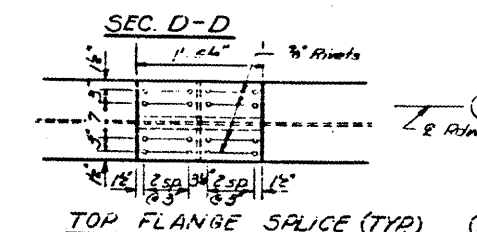
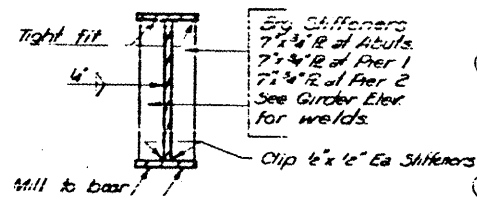
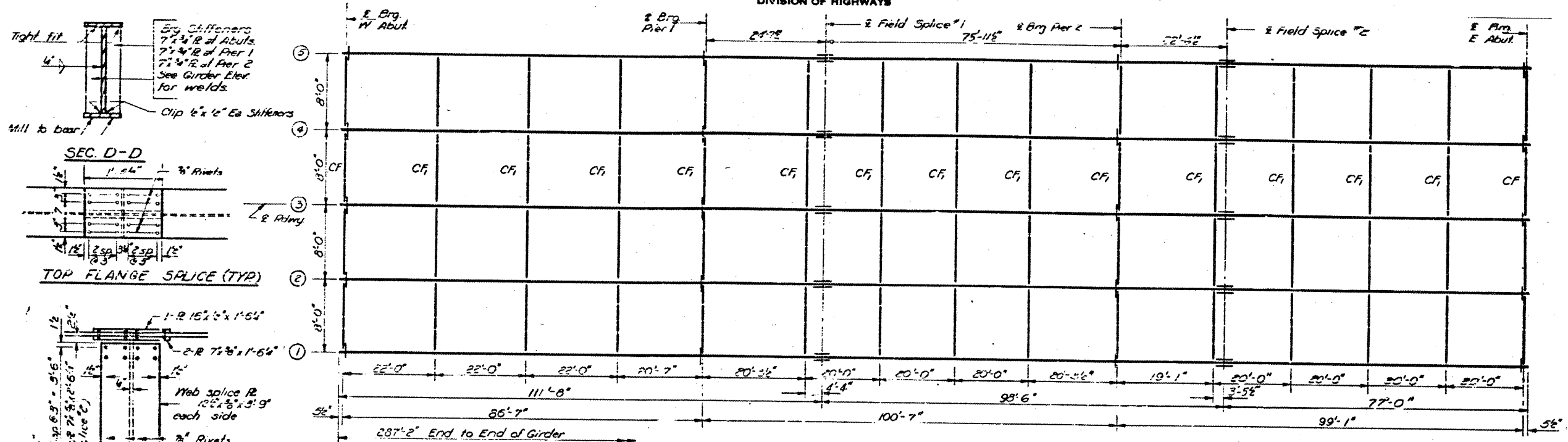
Height of bearing assemblies with lead plates and anchor bolts are included as structural steel. Est. 111-7930

The lengths and quantities of longitudinal reinforcement and Class X Concrete in parapets are not included in above quantities. See sheet 10

SUPERSTRUCTURE DETAILS
EAL RT. 55 SEC. 84-4-318
SANGAMON COUNTY
STA. 224+10.10

EXISTING PLANS, SN 084-0081
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

FOR INFORMATION ONLY

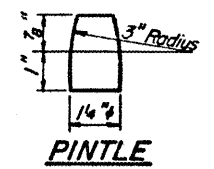
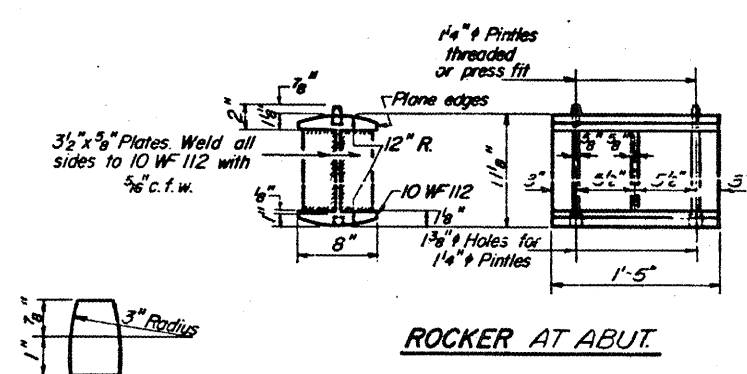
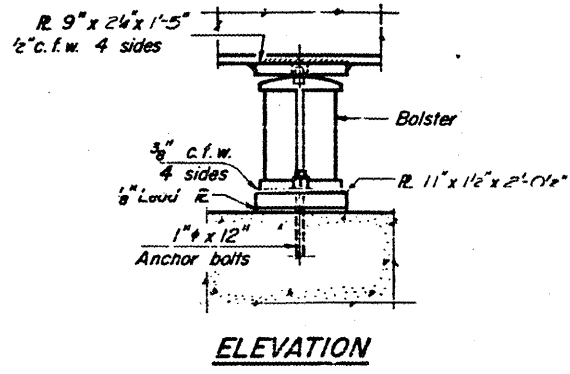
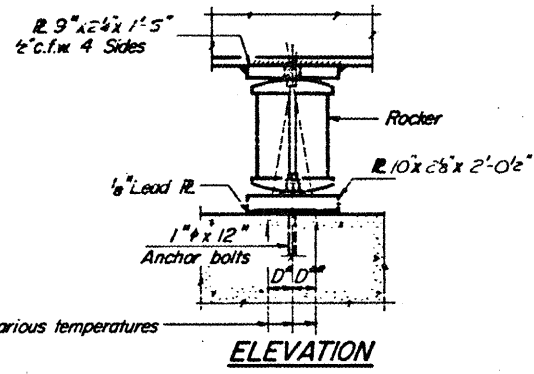
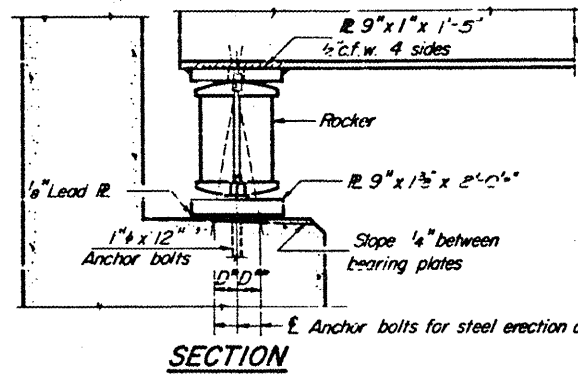


DESIGNED *P.A. Hummel*
CHECKED *J.W. Clark*
DRAWN *J.M. Heninger*
CHECKED *J.W. Clark*

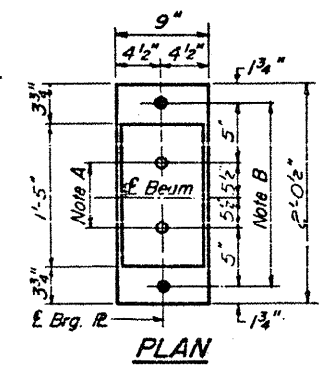
STRUCTURAL STEEL DETAILS
FAI RI 55 SEC 84-4-3 HB
SANGAMON COUNTY
STA. 224+10.10

FOR INFORMATION ONLY

EXISTING PLANS, SN 084-0081
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

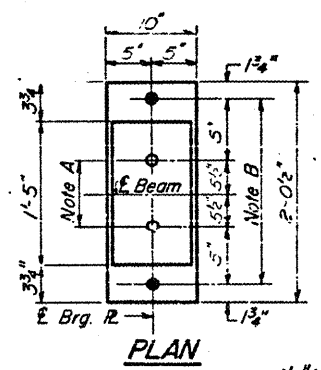


ROCKER AT ABUT.



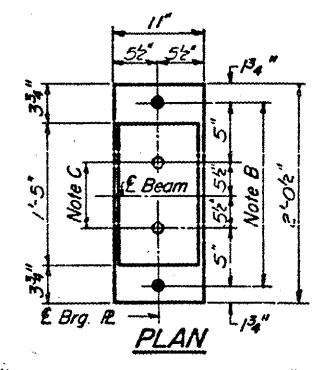
AT ABUTMENT

NOTE A
1 1/2" Holes - 1" deep in top flange for pintles. Thread or press fit pintles into bottom flange.



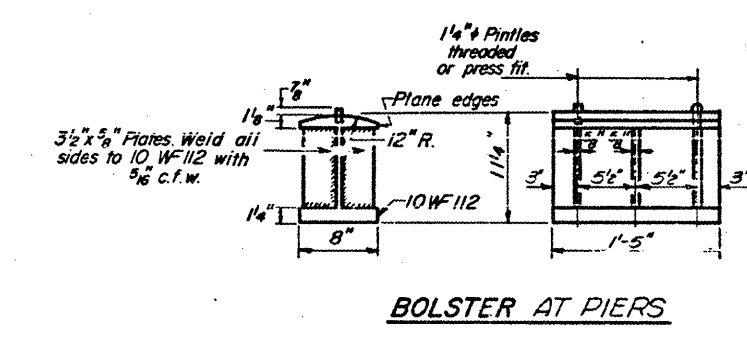
AT PIER 1

NOTE B
1 1/2" Holes for 1 1/4" anchor bolts. 2 1/2" x 2 1/2" x 5/16" R. Washers under nut.



AT PIER 2

NOTE C
1 1/2" Holes 1" deep in top flange only for 1 1/4" pintles.



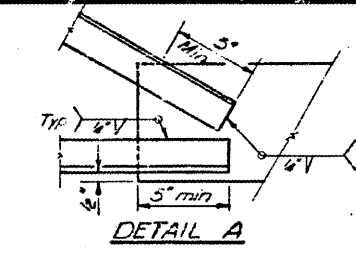
BOLSTER AT PIERS

NOTES ON SETTING OF ANCHOR BOLTS AT EXP. BRGS.

- a) D* (Side of brg. away from fixed brg.)
D* = 1/8" per each 100' of expansion for every 15° fall below the normal temp. of 50°F.
- D** (Side of brg. toward fixed brg.)
D** = 1/8" per each 100' of expansion for every 15° rise above the normal temp. of 50°F.

b) After beams have been erected and dimensions D* or D** determined, holes shall be drilled and anchor bolts shall be grouted in place. All fixed anchor bolts may be built into the masonry.

BEARING ASSEMBLY DETAILS



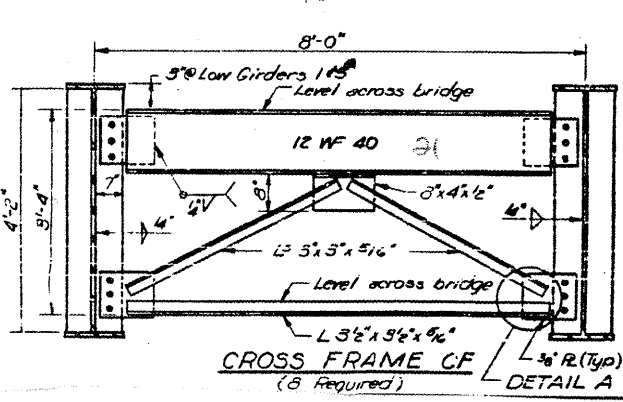
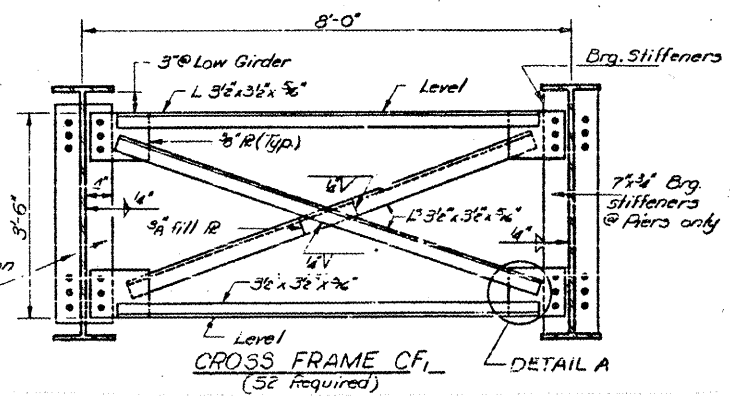
ELEVATION TOP OF WEB
(for fabrication only)

Loc. of Girder	1#3	2#4	3
E Brg. W. Abut.	618.57	618.72	618.84
E Shop 5#1	618.90	619.05	619.17
E Brg. P-1	618.94	619.09	619.21
E Field 5#1	619.39	619.14	619.26
E Shop 5#2	619.07	619.22	619.34
E Brg. P-2	619.04	619.19	619.31
E Field 5#2	619.02	619.17	619.29
E Brg. E. Abut.	618.89	619.04	619.16

Note:
See sheet # 8 for framing plan. All butt welded joints shall be prequalified according to AWS D2.0-66 Specifications. For details of welding designations see AWS D2.0-66 Specifications.
Height of structural steel billed on sheet #7. The contact surfaces of all field splices shall be free of all oil & paint.
Work this sheet with sheet #8

DESIGNED: *A. J. Hoffmann*
CHECKED: *J. W. Clark*
DRAWN: *J. M. H.*
CHECKED: *John W. Clark*
EXAMINED: *Carl E. ...*
APPROVED: *...*

4" x 3/8" connection plate intermediate frames



I-2-B 9-1-65

CROSS FRAMES & BEARING DETAILS
FAI. RT. 55 SEC. 84-4-3HB
SANGAMON COUNTY
STA. 224 + 10.10

FOR INFORMATION ONLY

EXISTING PLANS, SN 084-0081
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

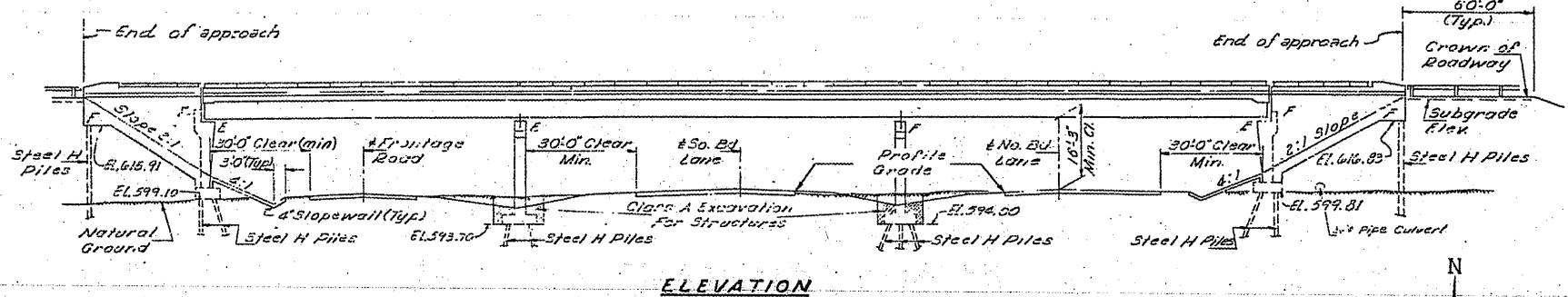
084-015

B.M.: #16 Elev 600.29, "Cut in center of Culvert Headwall, RT 143" Station 303+89

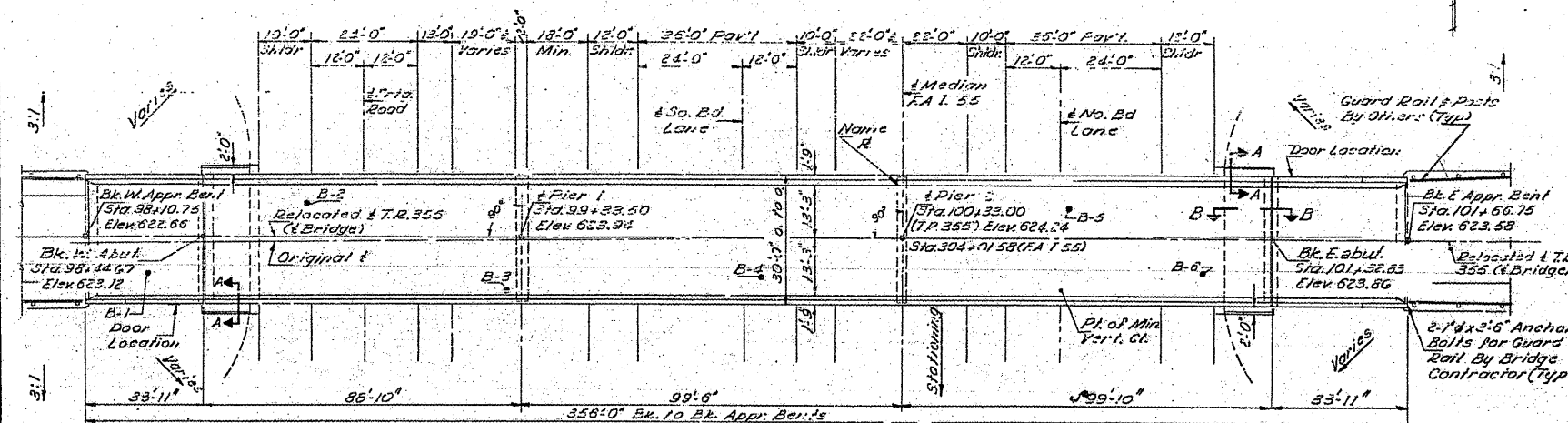
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

NO.	SECTION	DATE	BY	CHKD	SHEET NO.
1	84-1	3/26/63	SWANSON	57	11
15 SHEETS					

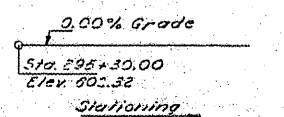
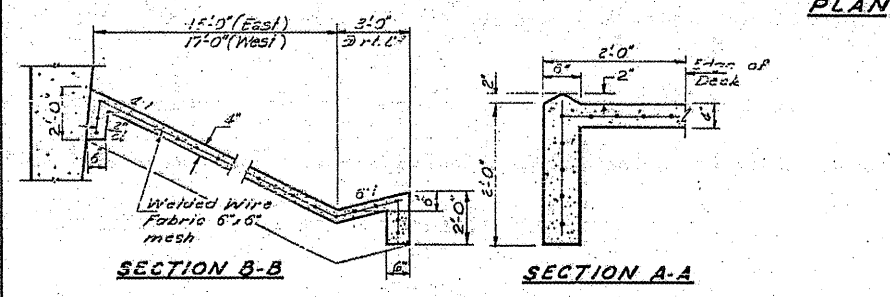
015



ELEVATION



PLAN



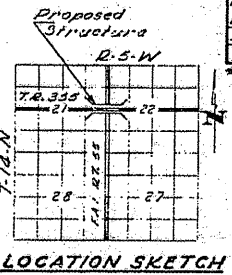
PROFILE PROP PAV'T
AT MED EDGE FAI 55

DESIGN STRESSES
PREC. PREST UNITS

fc = 5000 psi
ft = 4000 psi
fs = 440,000 psi strands
fs = 173,600 psi strands

FIELD UNITS

fc = 1200 psi Slab
fc = 1400 psi Sub. Curb & Parapet
fs = 75 psi Flgs
fs = 20,000 psi Reinft
fs = 20,000 psi Struct. (A-36)
n = 10
Allowable Deflection:
Composite = 1/800
Loading HS 15.44



LOCATION SKETCH

GENERAL NOTES

All reinforcement bars shall be lapped 24 diameters unless otherwise shown.
Rivets 1/4", open holes 1/8" unless otherwise noted.
Except as otherwise provided, all structural steel shall receive one shop coat of red lead paint, and two field coats of paint. See Spl. Prov. for field paint.
Field welding of construction accessories will not be permitted in the bottom of flange of beams or girders nor on the top flange for a distance equal to one-fourth the span length each way from the pier supports. Field welding in other areas will be permitted only when approved by the Engineer.
Anchor bolts shall be set before riveting cross frames over supports.
Slope wall shall be reinforced with welded wire fabric 6" x 6" mesh, weighing 55# per 100 sq. ft.
The contractor shall drive 2 steel test piles in appropriate location of the West Abutment & Pier 2 as directed by the Engineer before ordering the remainder of the piles. An alternate strand spacing using Extra High Strength Prestressing Strand (570 K.S.I.) is permitted. The embankment configuration shown shall be the minimum embankment that must be constructed prior to construction of the abutments.
The concrete rail section above the mandatory construction joint at the top of the slab shall be constructed of Class X Concrete, except the aggregates shall conform to the requirements of Handrail Concrete.
Drive all steel piles to refusal.
Excavation for slope wall is incidental to Class A Excavation for structures.

TOTAL BILL OF MATERIAL

Item	Unit	Super	Sub.	Total
Class A Excav. For Structure	Cu Yds.		27	27
Class X Concrete	Cu Yds.	339.8	254.9	604.7
Structural Steel	Lbs.	241,177		241,177
Aluminum Scaffolding	Lin. Ft.	570		570
Reinforcement Bars	Lbs.	76,500	62,540	139,040
Test Piles Steel (Q&P36)	Lin. Ft.		1728	1728
Test Piles Steel (Q&P36)	ea.		2	2
Name Plates	ea.		1	1
Slope Wall F.F.	Sq. Yds.		190	190
Bridge Seal Sealant	L.S.		1	1
Stud Shear Connectors	Each	2,320		2,320
Furnishing & Erecting Precast Prestressed Concrete I Beams (36)	Lin. Ft.	261		261
Protective Coat	Sq. Yds.	1310		1310

*All abutments only

084 015

GENERAL PLAN & ELEVATION
PROJ. 7-3 (67)88
FAI RT 55 SEC. 84-4-3HB-1
SANGAMON COUNTY
STATION 304+01.58 (FAI 55)

DESIGNED P.A. Humm
CHECKED V. J. ...
DRAWN V. J. ...
APPROVED ...

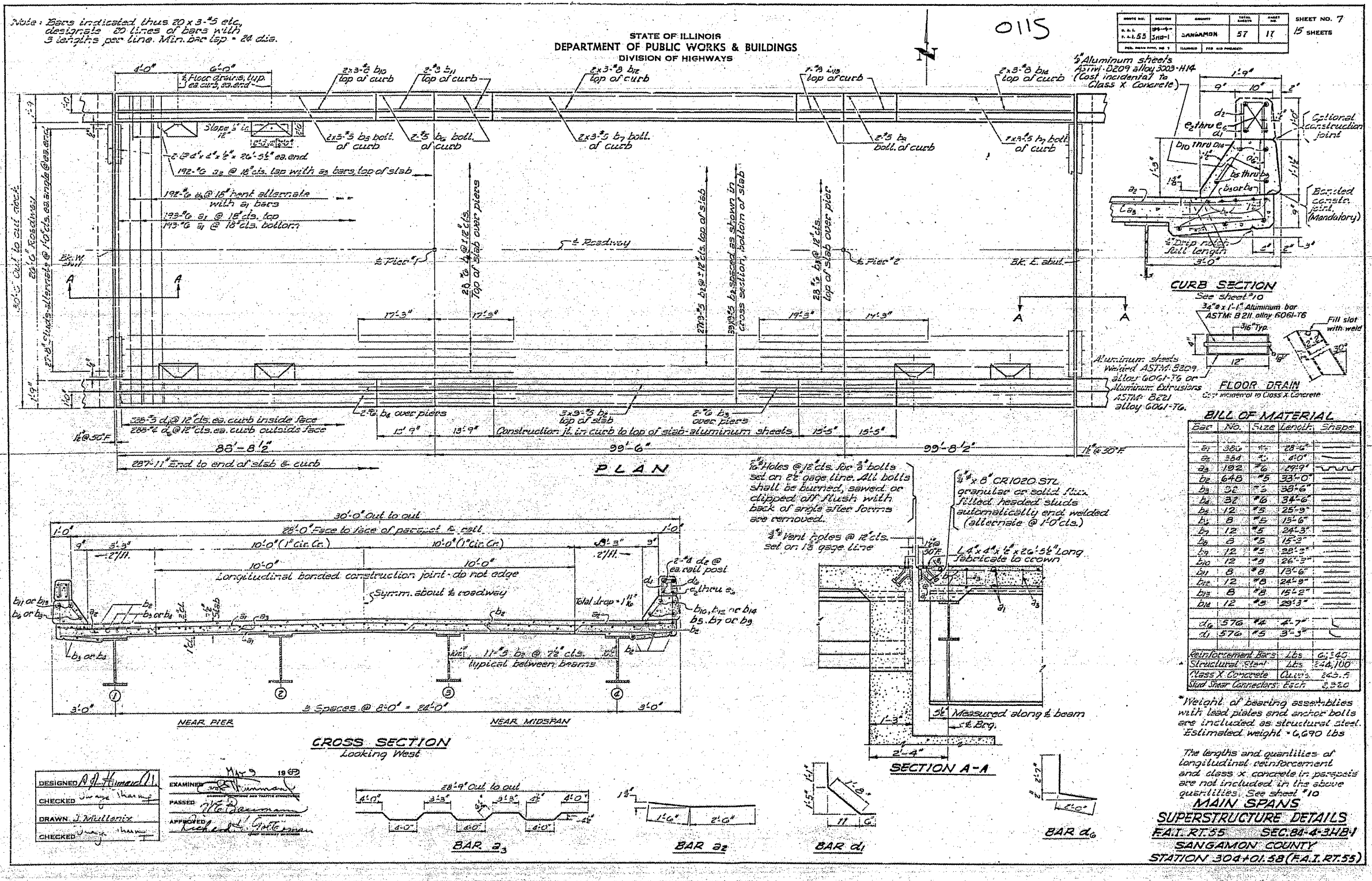
STATION 304+01.58
BUILT BY
STATE OF ILLINOIS
FAI RT 55 SEC. 84-4-3HB-1
FAI 55 I 3 3 67185
STATION 1-12

NAME PLATE
(See Std. 213-1)

PROPOSED PROFILE
TR 355

FOR INFORMATION ONLY

EXISTING PLANS, SN 084-0115
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY



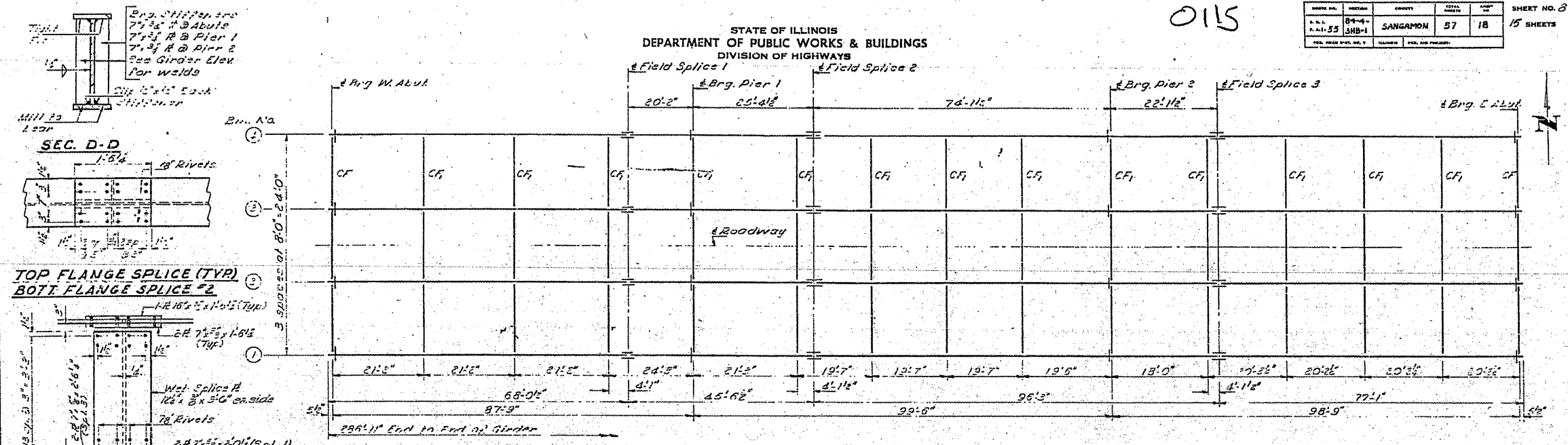
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FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

FOR INFORMATION ONLY

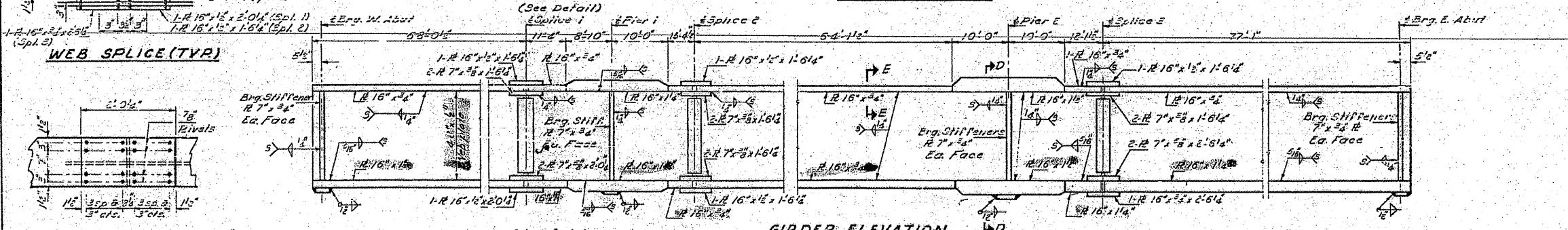
015

STATE NO.	COUNTY	SECTION	SHEET NO.	SHEET TOTAL
55	SANGAMON	57	18	15 SHEETS

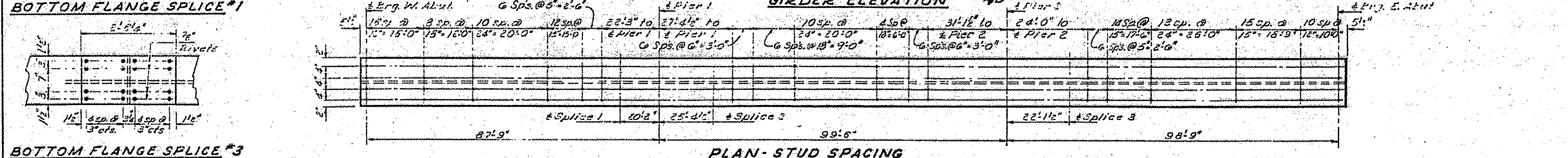
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS



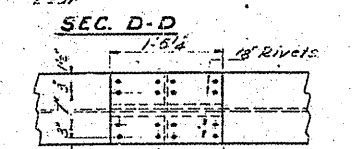
FRAMING PLAN



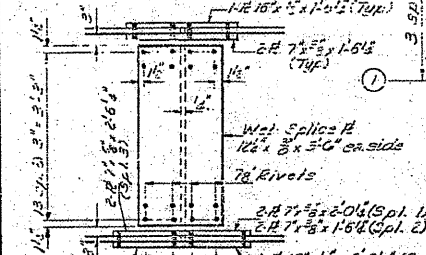
GIRDER ELEVATION



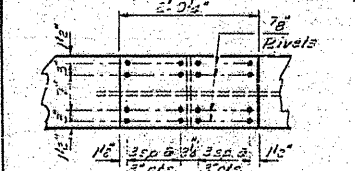
PLAN-STUD SPACING



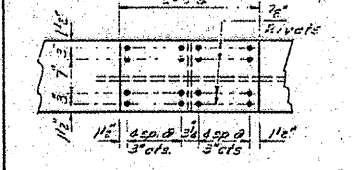
TOP FLANGE SPLICE (TYR)
BOTT. FLANGE SPLICE #2



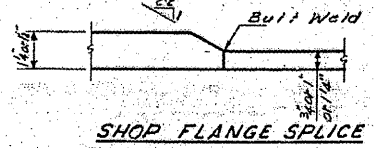
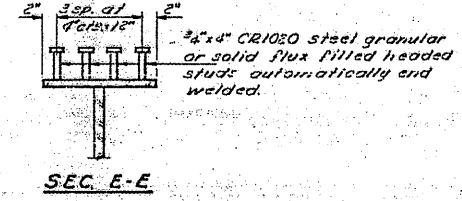
WEB SPLICE (TYR)



BOTT. FLANGE SPLICE #1



BOTT. FLANGE SPLICE #3



STRUCTURAL STEEL DETAILS
FAI RT 55 SEC. R4-43HB-1
SANGAMON COUNTY
STA. 304+01.58 (FAI 55)

DESIGNED	W. H. Hummer	EXAMINED	W. H. Hummer
CHECKED	W. H. Hummer	PASSED	W. H. Hummer
DRAWN	J. K. Kessler	APPROVED	Richard H. Goller
CHECKED	W. H. Hummer		

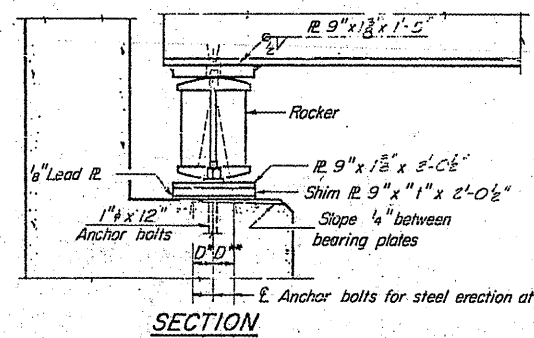
FOR INFORMATION ONLY

EXISTING PLANS, SN 084-0115
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY

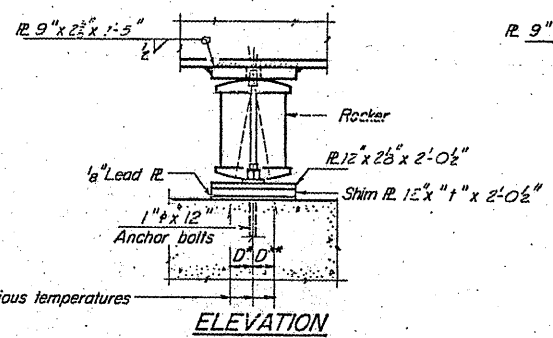
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

015

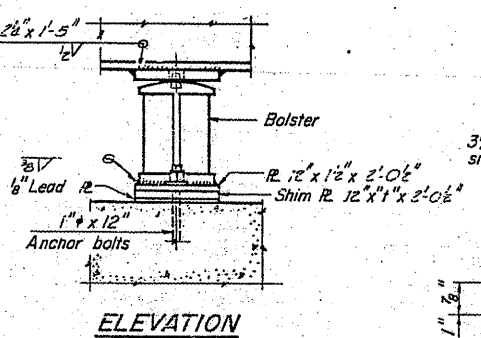
PROJECT NO.	SECTION	CONTRACT	SHEET NO.	TOTAL SHEETS
...	B4-4	SANGAMON	57	19
...	3HB-1			
SHEET NO. 7 15 SHEETS				



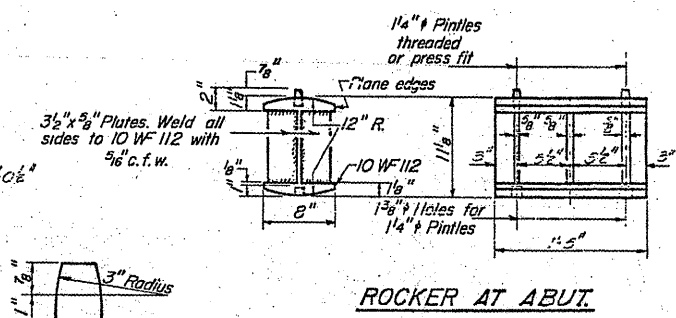
SECTION



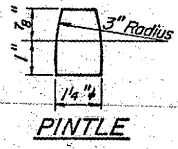
ELEVATION



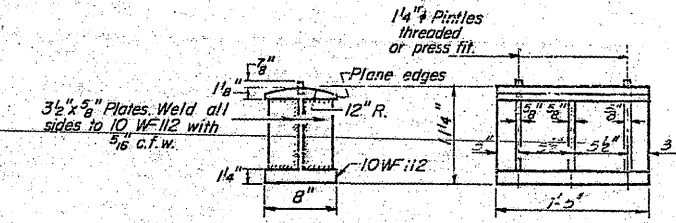
ELEVATION



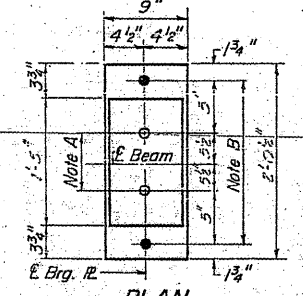
ROCKER AT ABUT.



PINTLE

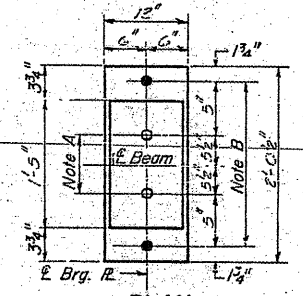


BOLSTER AT PIERS.



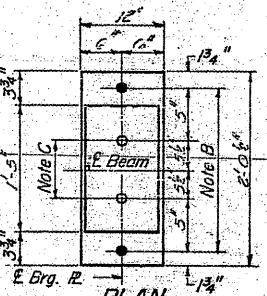
PLAN AT ABUTMENT

NOTE A
1 3/8" Holes - 1" deep in top IR for pintles. Thread or press fit pintles into bottom IR.



PLAN AT PIER 1

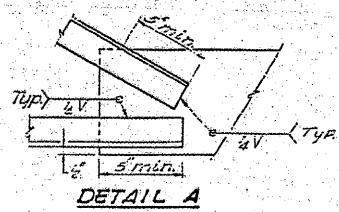
NOTE B
1 1/2" Holes for 1" anchor bolts. 2 1/2" x 2 1/2" x 5/16" IR. Washers under nut.



PLAN AT PIER 2

NOTE C
1 3/8" Holes - 1" deep in top IR only for 1/4" pintles.

BEARING ASSEMBLY DETAILS



DETAIL A

ELEVATION TOP OF WEB
For fabrication only

Loc. Girder	1 E 4	2 E 3
Abut. W. Abut.	622.35	622.45
Field Sp. 1	623.01	623.11
Bro. Sp. 1	623.12	623.22
Field Sp. 2	623.27	623.37
Sec. 2 P-2	623.39	623.50
Field Sp. 3	623.44	623.54
Sec. 5 Sp. 1	623.08	623.18

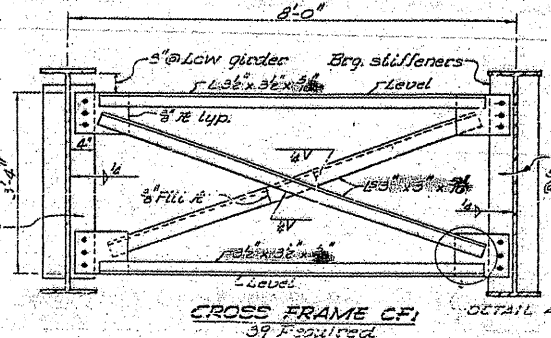
Weights of structural steel listed on sheet #7
The contact surfaces of field splices shall be free of all oil and paint.

Work this sheet with sheet #8
See sheet #2 for Stress Table & Section Properties.

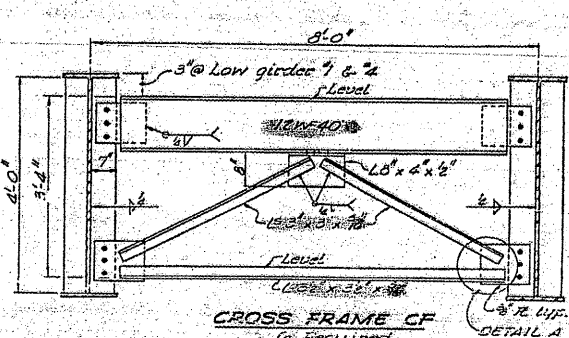
NOTES ON SETTING OF ANCHOR BOLTS AT EXP. BRGS.

- a) D* (Side of brg. away from fixed brg.)
D** 1/8" per each 100" of expansion for every 15° fall below the normal temp. of 50°F.
- D** (Side of brg. toward fixed brg.)
D** 1/8" per each 100" of expansion for every 15° rise above the normal temp. of 50°F.

- b) After beams have been erected and dimensions D* or D** determined, holes shall be drilled and anchor bolts shall be grouted in place. All fixed anchor bolts may be built into the masonry.



CROSS FRAME CF
39 I-Required



CROSS FRAME CF
6 Required

CROSS FRAMES AND BEARING DETAILS
F.A.I. RT. 55 SEC. 84-A-3HB-1
SANGAMON COUNTY
STATION 304+01.58 (F.A.I. RT. 55)

DESIGNED	A. D. Hammond	EXAMINED	Mar 3 1963
CHECKED	L. M. ...	PASSED	...
DRAWN	P. C. Barnett	APPROVED	Richard L. ...
CHECKED	...		

I-2-B 9-1-65

FOR INFORMATION ONLY

EXISTING PLANS, SN 084-0115
FAI 55 (I-55)
D-6 REHAB BDGE PAINTING 2009
SANGAMON COUNTY