

04-24-2015 LETTING ITEM 133

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF LA GRANGE

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
**PLANS FOR PROPOSED  
 FEDERAL AID HIGHWAY**  
 FAU ROUTE 1698 (KENSINGTON AVENUE)  
 BURLINGTON AVENUE TO COSSITT AVENUE  
 RESURFACING  
 SECTION: 14-00091-00-RS  
 PROJECT: M-4003(412)  
 VILLAGE OF LA GRANGE  
 COOK COUNTY  
 JOB NO.: C-91-096-15

**TRAFFIC DATA**

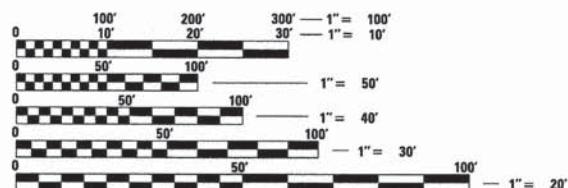
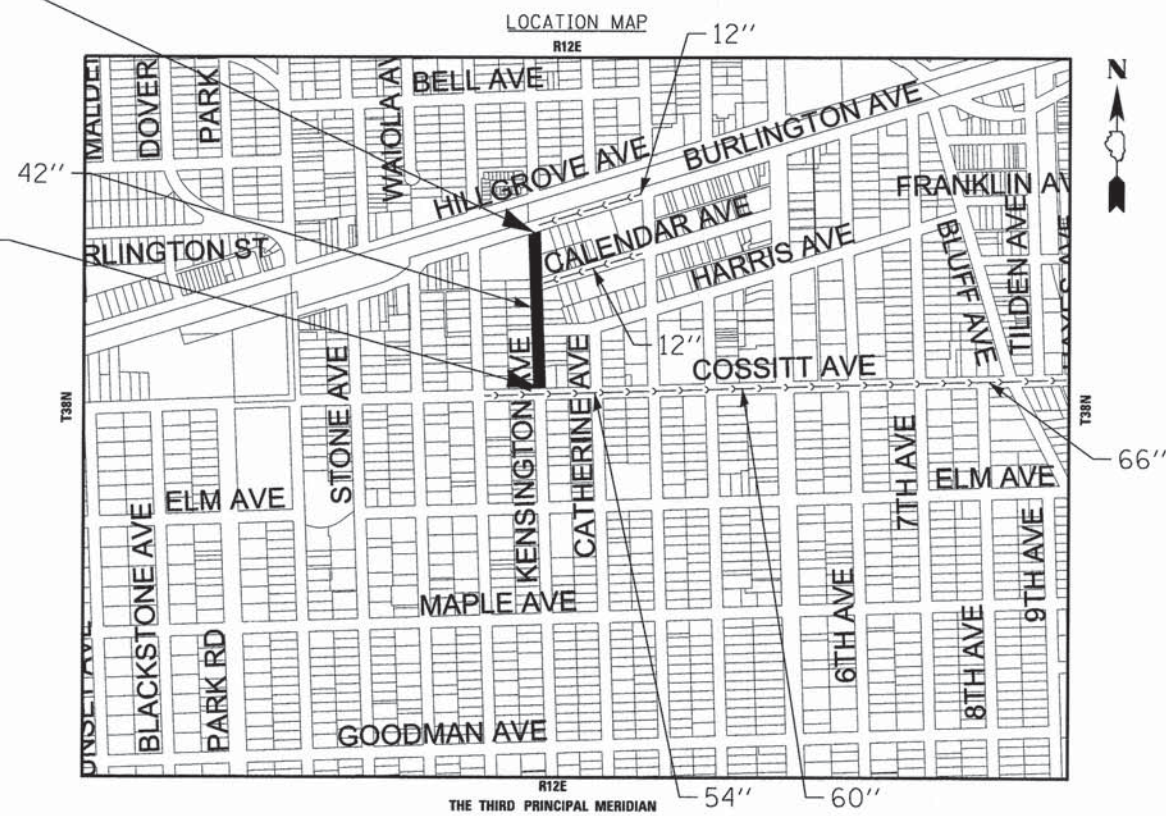
KENSINGTON AVENUE  
 POSTED SPEED LIMIT = 25 MPH  
 2015 ADT = 3,100 VPD

**DESIGN DESIGNATION**

MAJOR COLLECTOR

FAU ROUTE 1698 (KENSINGTON AVENUE)  
 END IMPROVEMENTS  
 STA 85 + 57

FAU ROUTE 1698 (KENSINGTON AVENUE)  
 BEGIN IMPROVEMENTS  
 STA 76 + 19



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST  
 DIG. No. X3150427



CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:  
 COUNTY = COOK  
 CITY-TWNSHP. = LA GRANGE - LYONS  
 SEC. & 1/4 SEC. NO. = 4 NW, T38N R12E  
 48 HOURS (2 working days) BEFORE YOU DIG

CONTRACT NO. 61B30

THE THIRD PRINCIPAL MERIDIAN  
 LYONS TOWNSHIP  
 GROSS LENGTH OF IMPROVEMENT = 938 FT. = 0.178 MILES  
 NET LENGTH OF IMPROVEMENT = 938 FT. = 0.178 MILES

**BAXTER & WOODMAN**  
 Consulting Engineers



STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

APPROVED *Ry Shillif* 1/27/2015  
 VILLAGE OF LA GRANGE, DIRECTOR OF PUBLIC WORKS

PASSED *February 18, 2015*  
*C. Holt*  
 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID  
 BASED ON LIMITED  
 REVIEW *February 18, 2015*  
*Shawn Fortman*  
 DEPUTY DIRECTOR OF  
 HIGHWAYS & REGION 1 ENGINEER

PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS

B&W PROJECT NO.: 130584 DATE: 12-05-14

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	1
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS	FED AID PROJECT M-4003(412)	
CONTRACT 61B30				

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 FEDERAL AID PROGRAM ENGINEER FAWAD AQUEEL, PE PTDE (847-705-4021), SCHAUMBURG, IL



**GENERAL NOTES**

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
- UTILITY LOCATIONS HAVE NOT BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY THE VILLAGE DIRECTOR OF PUBLIC WORKS AND THE ENGINEER AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE VILLAGE DIRECTOR OF PUBLIC WORKS AND THE ENGINEER.
- MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, SIDEWALKS AND EXCAVATION FOR NEW SIDEWALKS ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
- THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
- ACCESS TO PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING TEMPORARY AGGREGATE.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO SIDEWALK REPLACEMENT, CURB AND GUTTER OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS AT LEAST 24 HOURS PRIOR TO PLANNED CLOSURE. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED. THE COST OF THIS WORK IS INCLUDED IN THE PAY ITEM CAUSING THE CLOSURE.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, THEIR AGENTS OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ALL SIGNS AND MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH VILLAGE STANDARDS AND IN ACCORDANCE WITH ARTICLE 107.20. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
- EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE COST OF THE APPROPRIATE REMOVAL PAY ITEM.
- IN AREAS WHERE THE EXISTING DRIVEWAY (HOT-MIX ASPHALT OR PCC) OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS DRIVEWAY PAVEMENT REMOVAL OR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES PRIVATE DRIVEWAYS AND 8-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES COMMERCIAL DRIVEWAYS TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET.
- ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
- A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT.
- THE CONTRACTOR SHALL CONTACT THE LOCAL AGENCY MATERIAL INSPECTOR (SOIL AND MATERIAL CONSULTANTS - THOMAS JOHNSON - 847-870-0544) AT LEAST 48 HOURS PRIOR TO ANY CONCRETE OR HOT-MIX ASPHALT MATERIAL DELIVERIES.
- THE DAYS PAVING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
- THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTORS OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DETERMINED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
- CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
- THE CONTRACTOR WILL ONLY BE ALLOWED TO REMOVE AND REPLACE CURB AND GUTTER ON ONE SIDE OF THE ROAD AT A TIME TO MINIMIZE CONGESTION. REPLACEMENT MUST BE COMPLETE ON ONE SIDE OF THE ROAD BEFORE THE CONTRACTOR IS ALLOWED TO BEGIN REMOVING CURB AND GUTTER ON THE OTHER SIDE OF THE ROAD.
- THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCHES ABOVE THE GUTTER FLAG.
- A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELOCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE PAY ITEM FOR MOBILIZATION IN ACCORDANCE WITH ARTICLE 107.08.
- THE DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN ON THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS SI CONCRETE TO AN ELEVATION 2-1/2" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS PLACED HIGHER THAN 2-1/2" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-1/2" DEPTH. THIS WORK IS INCLUDED IN THE COST OF THE APPLICABLE CURB AND GUTTER PAY ITEMS.

- ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE EXISTING STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE IN ADDITION TO THE REQUIREMENTS FOR ANY STRUCTURE TO BE ADJUSTED AND SHALL BE PAID FOR ONCE AT THE CONTRACT UNIT PRICE FOR THE APPLICABLE PAY ITEM.
- ALL FRAME AND LID CASTINGS LOCATED WITHIN THE PAVEMENT WHICH REQUIRE RESETTING TO FINISH GRADE SHALL BE BACKFILLED WITH CLASS PP-1 CONCRETE. HMA MATERIALS WILL NOT BE ALLOWED AS BACKFILL AROUND AN ADJUSTED CASTING. THIS WORK SHALL APPLY TO ALL CASTINGS ADJUSTED OR RECONSTRUCTED AS PART OF THIS CONTRACT, WHETHER PAID FOR SEPARATELY OR INCLUDED IN OTHER CONTRACT WORK. SEE SPECIAL PROVISION.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND SHALL NOT EXCEED 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. A MAXIMUM GRADE DIFFERENCE OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H), AS DETERMINED BY THE ENGINEER.
- THE DAYS MILLING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. ANY LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY MILLING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE MILLING OF THE ADJACENT LANE IN THE SAME DAY. A TEMPORARY RAMP SHALL BE INSTALLED AT THE SINGLE TRANSVERSE JOINT AT THE END OF EACH DAY. THE COST OF THIS WORK IS INCLUDED IN THE APPLICABLE HOT-MIX ASPHALT SURFACE REMOVAL PAY ITEM.
- ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS."
- DURING CONSTRUCTION, THE CONTRACTOR WILL BE PERMITTED TO LIMIT ON-STREET PARKING IN ORDER TO COMPLETE CONSTRUCTION OPERATIONS. THE CONTRACTOR WILL BE REQUIRED TO COORDINATE WITH THE MUNICIPALITY A MINIMUM OF 48 HOURS IN ADVANCE. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PLACE ADVANCE SIGNS TO ALERT RESIDENTS AND COMMUTERS OF THE CONSTRUCTION WORK. THE PLACEMENT OF THESE SIGNS SHALL TAKE PLACE 48 HOURS IN ADVANCE IN ORDER TO ALLOW SUFFICIENT TIME FOR RESIDENTS AND GENERAL PUBLIC TO REVISE THEIR PARKING PATTERNS.
- INLET FILTERS SHALL BE CLEANED OF ALL SEDIMENT AND DEBRIS OR REPLACED AFTER EVERY 1/2" OR GREATER RAINFALL OR AS REQUIRED BY THE ENGINEER. COST INCLUDED IN PAY ITEM INLET FILTERS.
- THE LOCATIONS OF CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER DURING CONSTRUCTION.
- DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE 24" X 60" NOMINAL PANEL WIDTH. THE PANEL SHALL BE A POLYMER COMPOSITE AND COMPLY WITH ADA REQUIREMENTS. THE DOMES LOCATED ON THE PANEL SHALL PARALLEL THE PAVEMENT CROSS WALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. THE PANEL COLOR SHALL BE SELECTED BY THE ENGINEER AS COORDINATED WITH THE VILLAGE. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.

**HIGHWAY STANDARDS**

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424016-02	MID-BLOCK CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-04	FRAME AND LIDS TYPE 1
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

**INDEX OF SHEETS**

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS AND GENERAL NOTES
3 - 4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS
6	ROADWAY PLAN - KENSINGTON AVENUE
7	MISCELLANEOUS DETAILS
8	DISTRICT ONE DETAIL - BD-02 DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)
9	DISTRICT ONE DETAIL - BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
10	DISTRICT ONE DETAIL - BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
11	DISTRICT ONE DETAIL - BD-24 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
12	DISTRICT ONE DETAIL - BD-32 BUTT JOINT AND HMA TAPER DETAILS
13	DISTRICT ONE DETAIL - TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
14	DISTRICT ONE DETAIL - TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
15	DISTRICT ONE DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN

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<b>BAXTER &amp; WOODMAN</b> Consulting Engineers	DESIGNED - MWP	REVISED -
	DRAWN - KAR	REVISED -
	CHECKED - TMS	REVISED -
	DATE - 12-05-14	FILE - 130584SHT-GenNotes.dgn

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, HIGHWAY STANDARDS  
AND GENERAL NOTES**

SCALE: NONE STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	2
				CONTRACT NO. 61B30
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-4003(412)				



**SUMMARY OF QUANTITIES**

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE	
				RESURFACING	STU
20101200	TREE ROOT PRUNING	EACH	6		6
20200100	EARTH EXCAVATION	CU YD	6		6
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	62		62
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	493		493
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	11		11
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	11		11
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	11		11
25200110	SODDING, SALT TOLERANT	SQ YD	493		493
25200200	SUPPLEMENTAL WATERING	UNIT	31		31
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	12		12
28000510	INLET FILTERS	EACH	10		10
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	722		722
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	48		48
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	15		15
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	2,458		2,458

\* SPECIALTY ITEM  
# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE	
				RESURFACING	STU
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10		10
# 40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	179		179
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	84		84
# 40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	326		326
42001300	PROTECTIVE COAT	SQ YD	897		897
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	75		75
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	38		38
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	5,101		5,101
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	578		578
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	50		50
42400800	DETECTABLE WARNINGS	SQ FT	180		180
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	3,640		3,640
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	124		124
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	18		18
44000600	SIDEWALK REMOVAL	SQ FT	5,795		5,795

\* SPECIALTY ITEM  
# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

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DESIGNED - MWP	REVISED -
DRAWN - KAR	REVISED -
CHECKED - TMS	REVISED -
DATE - 12-05-14	FILE - 130584SHT-S00.dgn

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

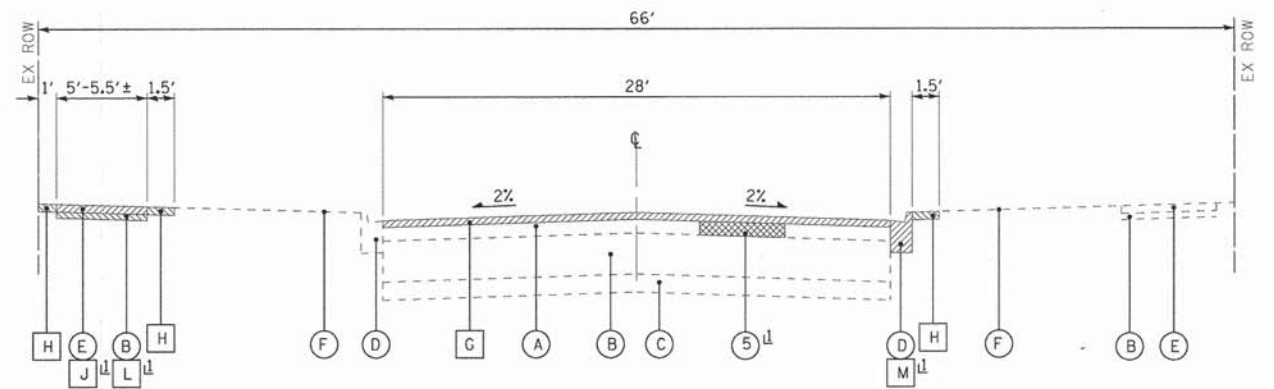
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	3
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-4003(4)27			CONTRACT NO. 61B30	



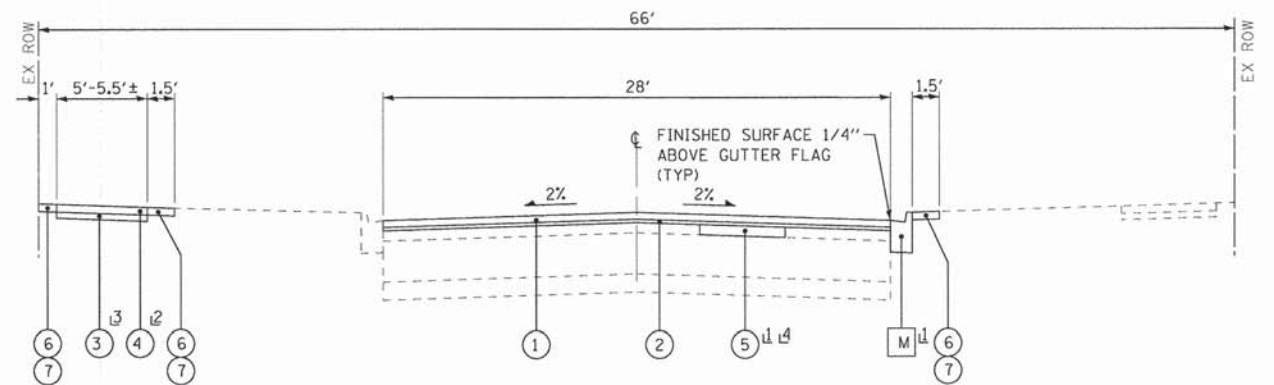






**EXISTING TYPICAL SECTION**  
STA 76+19 TO STA 85+57, KENSINGTON AVENUE

1 AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER



**PROPOSED TYPICAL SECTION**  
STA 76+19 TO STA 85+57, KENSINGTON AVENUE

1 AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
 2 THICKEN TO 6-INCH IN RESIDENTIAL DRIVEWAYS AND 8-INCH IN COMMERCIAL DRIVEWAYS  
 3 THICKEN TO 6-INCH IN COMMERCIAL DRIVEWAYS  
 4 INSTALL CLASS D PATCHES, 6-INCH AT STRUCTURE REPLACEMENT, MANHOLE CONSTRUCTION AND UTILITY STRUCTURE REMOVAL

**EXISTING LEGEND**

- A EXISTING HOT-MIX ASPHALT PAVEMENT
- B EXISTING AGGREGATE BASE COURSE
- C EXISTING SUB-GRADE
- D EXISTING COMBINATION CONCRETE CURB AND GUTTER
- E EXISTING SIDEWALK
- F EXISTING GROUND SURFACE
- G HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- H REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)
- J SIDEWALK REMOVAL
- L AGGREGATE BASE COURSE REMOVAL (INCLUDED IN J PAY ITEM) (NOT MEASURED FOR PAYMENT AND NOT PAID FOR SEPARATELY)
- M COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- ITEM TO BE REMOVED

**PROPOSED LEGEND**

- 1 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- 2 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- 3 AGGREGATE BASE COURSE, TYPE B 4"
- 4 PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- 5 CLASS D PATCHES, 3-INCH AND 6-INCH
- 6 TOPSOIL FURNISH AND PLACE, 4"
- 7 SODDING

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

CONTRACTOR WILL MILL FIRST

MIXTURE TYPE	AIR VOIDS @ Ndes
<b>RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm); 1-1/2"	4% @ 50 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	3.5% @ 50 Gyr.
<b>DRIVEWAYS</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm); 3"	4% @ 50 Gyr.
<b>PATCHING</b>	
CLASS D PATCHES (HMA BINDER, IL-19 mm, N50); 3" (1-LIFT), 6" (2-LIFTS)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS.

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DRAWN - KAR	REVISED -
CHECKED - TMS	REVISED -
DATE - 12-05-14	FILE - 130584SHT-TypSec.dgn

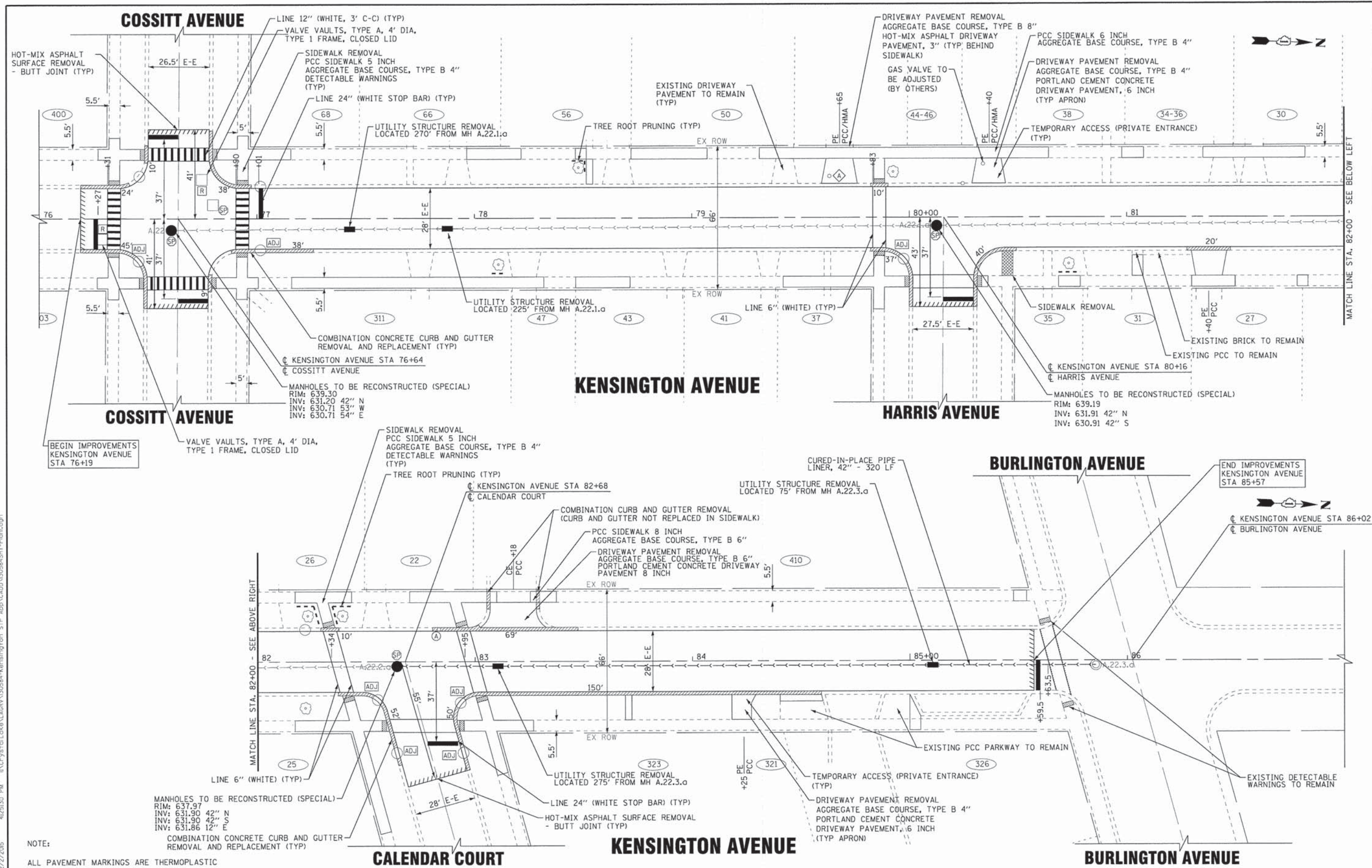
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**TYPICAL SECTIONS AND  
HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

SCALE: NONE STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	5
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-4003(412)			CONTRACT NO. 61B30	





NOTE:

ALL PAVEMENT MARKINGS ARE THERMOPLASTIC UNLESS OTHERWISE NOTED.

**BAXTER & WOODMAN**  
Consulting Engineers

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DRAWN - KAR	REVISED -
CHECKED - TMS	REVISED -
DATE - 12-05-14	FILE - 130584SHT-Plan1.dgn

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN - KENSINGTON AVENUE

SCALE: 1" = 20'

STA. 76+00 TO STA. 87+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	6
FED. ROAD DIST. NO. ILLINOIS/FED. AID PROJECT M-40034127			CONTRACT NO. 61B30	

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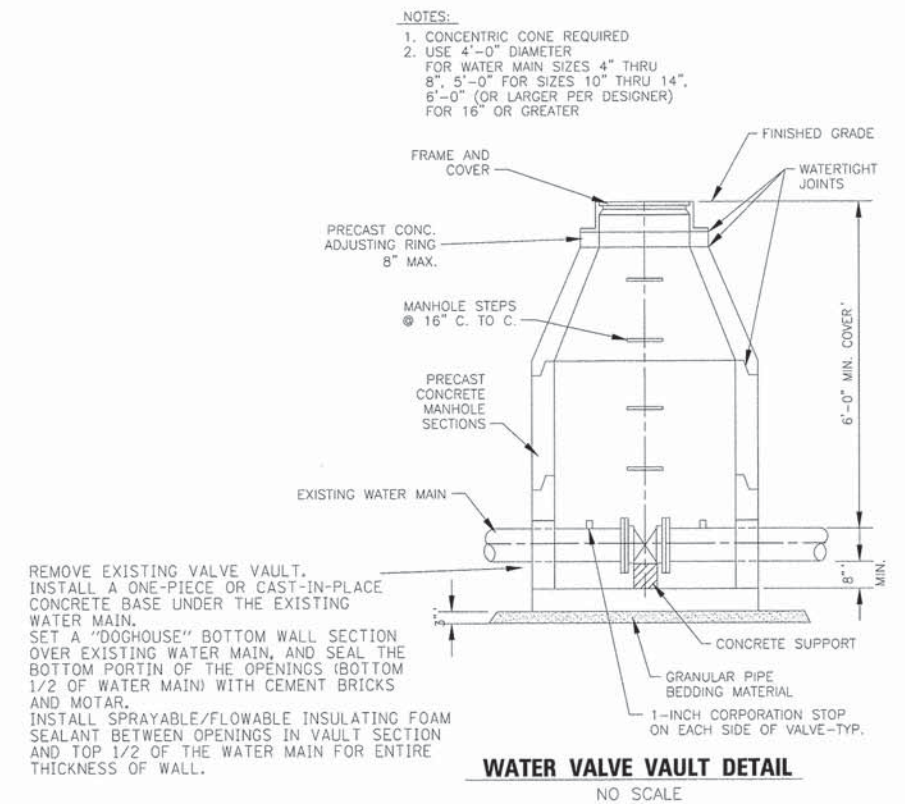
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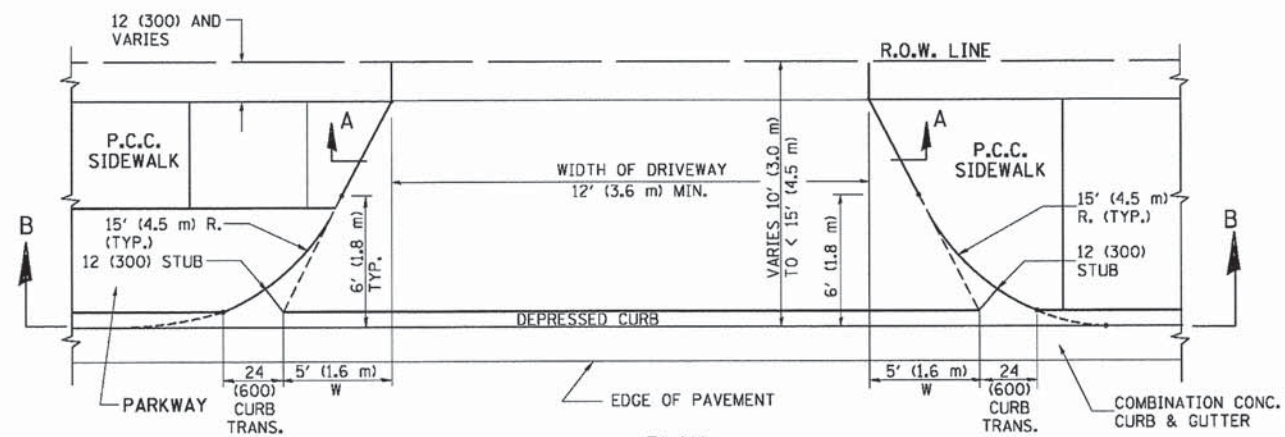
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MISCELLANEOUS DETAILS**

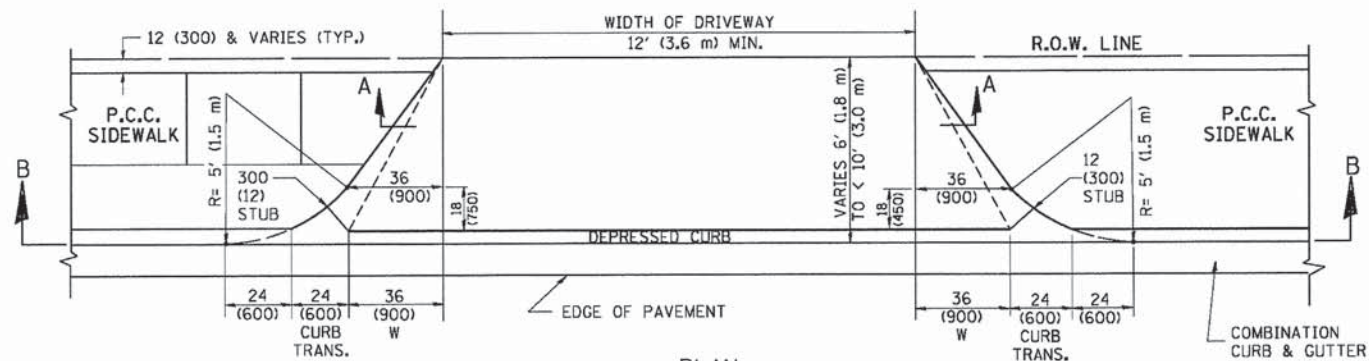
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CONTRACT NO. 61B30				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-4003(412)				

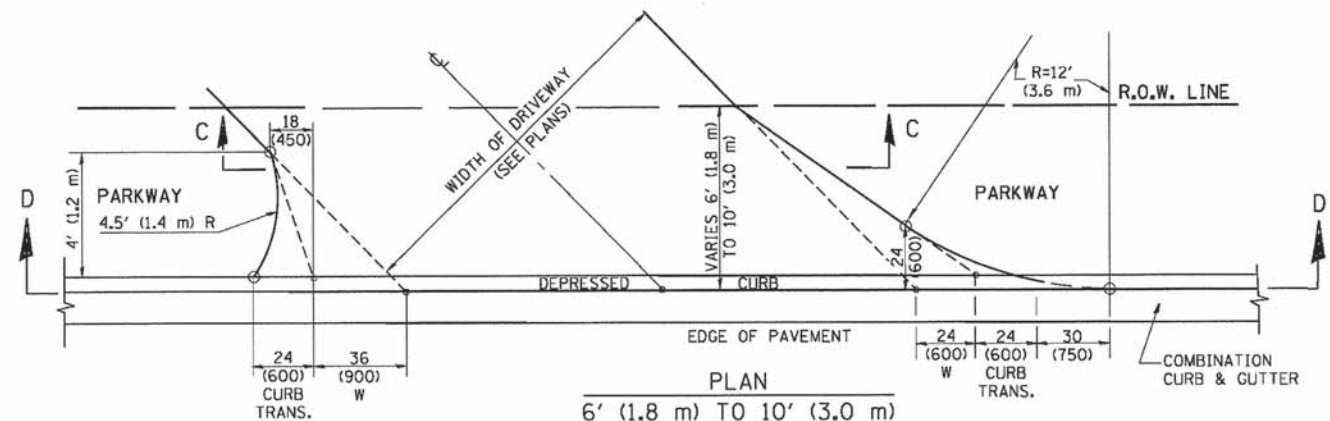




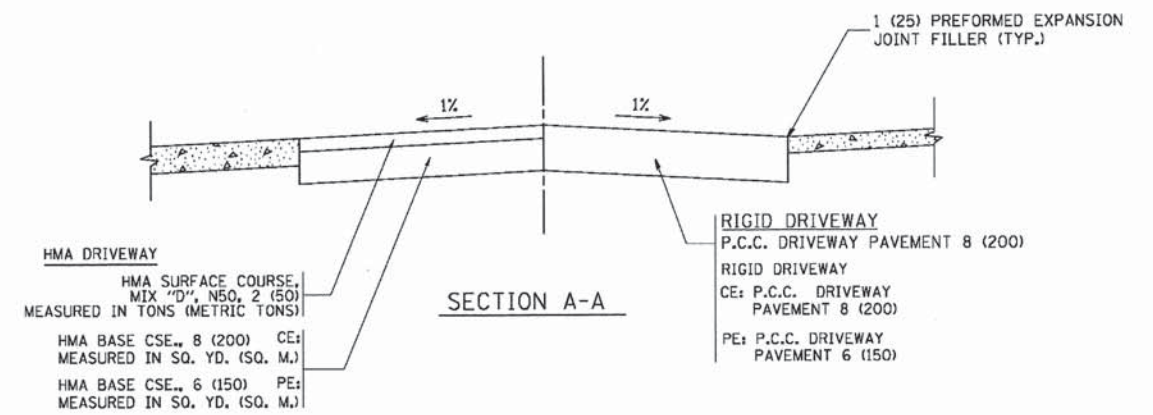
PLAN  
10' (3.0 m) TO < 15' (4.5 m)



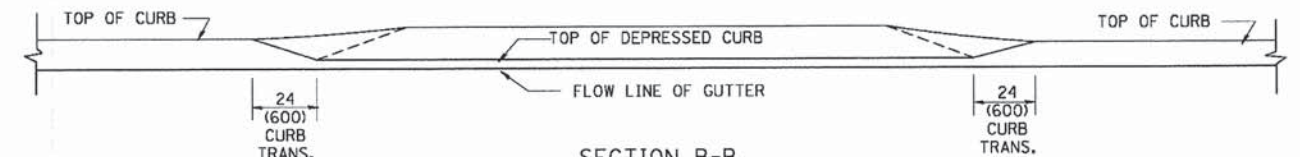
PLAN  
6' (1.8 m) TO < 10' (3.0 m)



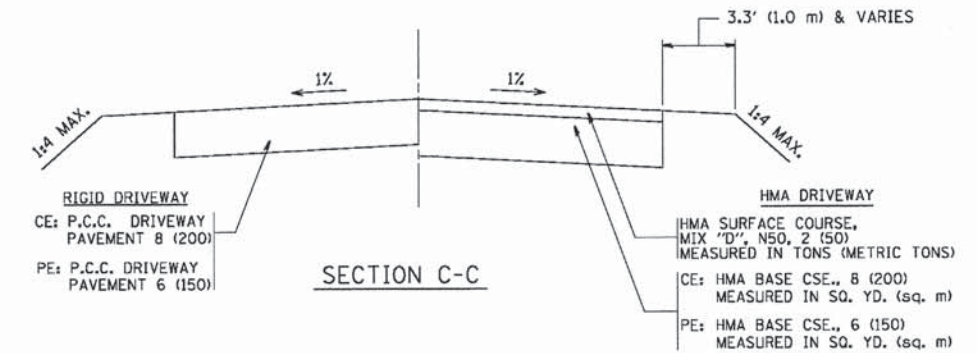
PLAN  
6' (1.8 m) TO 10' (3.0 m)



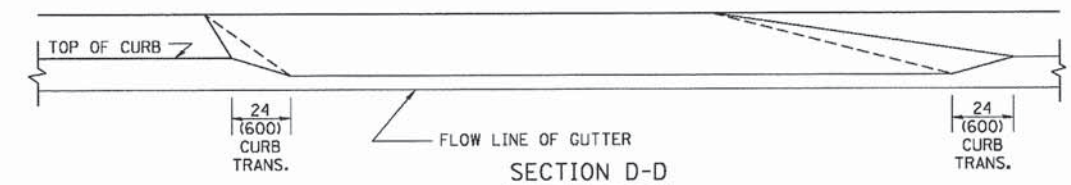
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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PLOT SCALE = 1/8" = 1'-0"		CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 10/28/2011		DATE - 11-06-95	REVISED - R. BORO 09-06-11

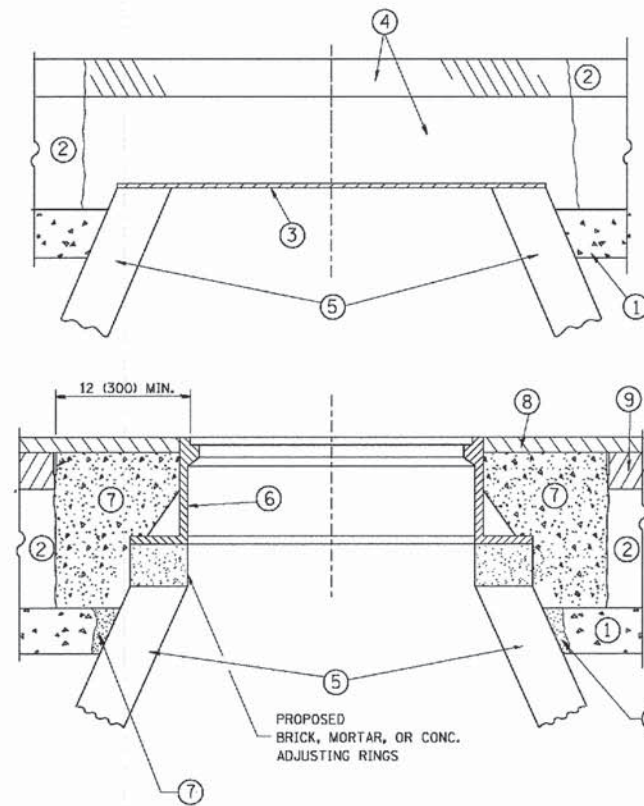
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	8
BD400-02 (BD-02)			CONTRACT NO. 61B30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(412)				



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 USER NAME = baord  
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 REVISED - R. WIEDEMAN 05-14-04  
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 REVISED - R. BORO 01-01-07  
 CHECKED -  
 REVISED - R. BORO 03-09-11  
 DATE - 10-25-94  
 REVISED - R. BORO 12-06-11  
 PLOT SCALE = 1/8" = 1'-0"  
 PLOT DATE = 12/6/2011



**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

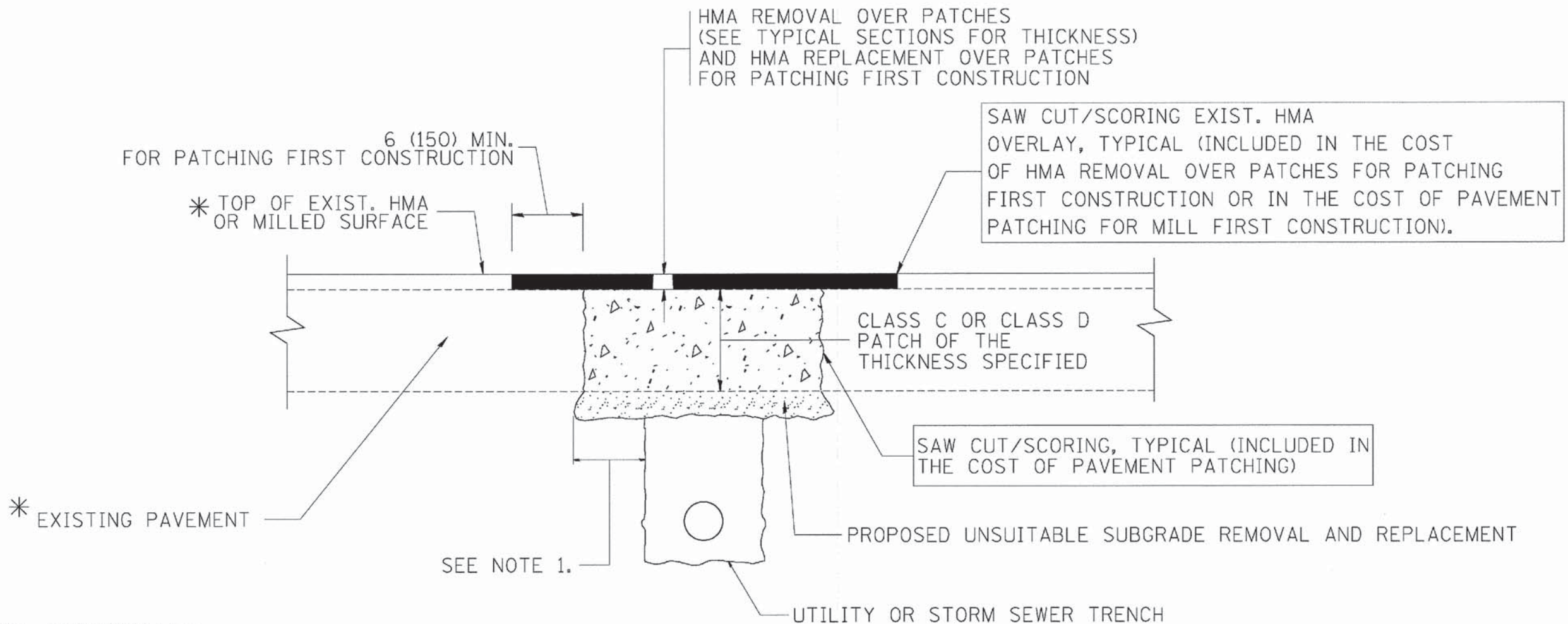
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = c:\pwork\pwork\baord\1\080315\ba08.dgn	USER NAME = baord	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>				F.A.U. RTE. 1698	SECTION 14-00091-00-RS	COUNTY COOK	TOTAL SHEETS 15	SHEET NO. 9
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	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 03-09-11		CONTRACT NO. 61B30								
			REVISED - R. BORO 12-06-11		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(412)								





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

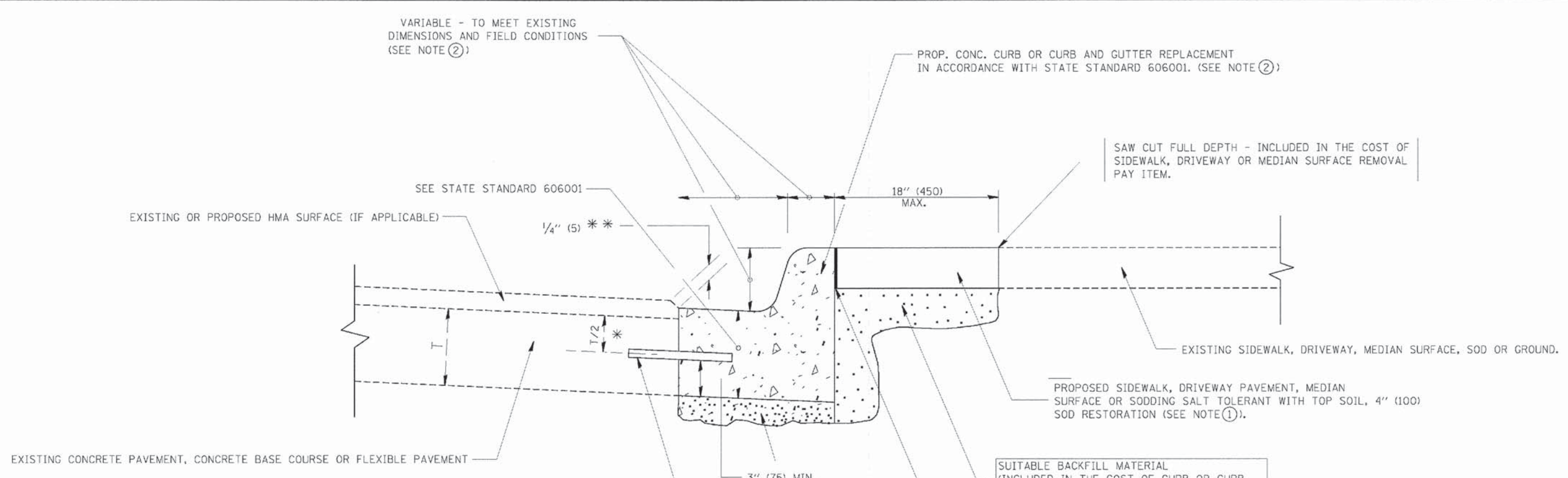
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08			BD400-04 (BD-22)		CONTRACT NO. 61B30					





- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

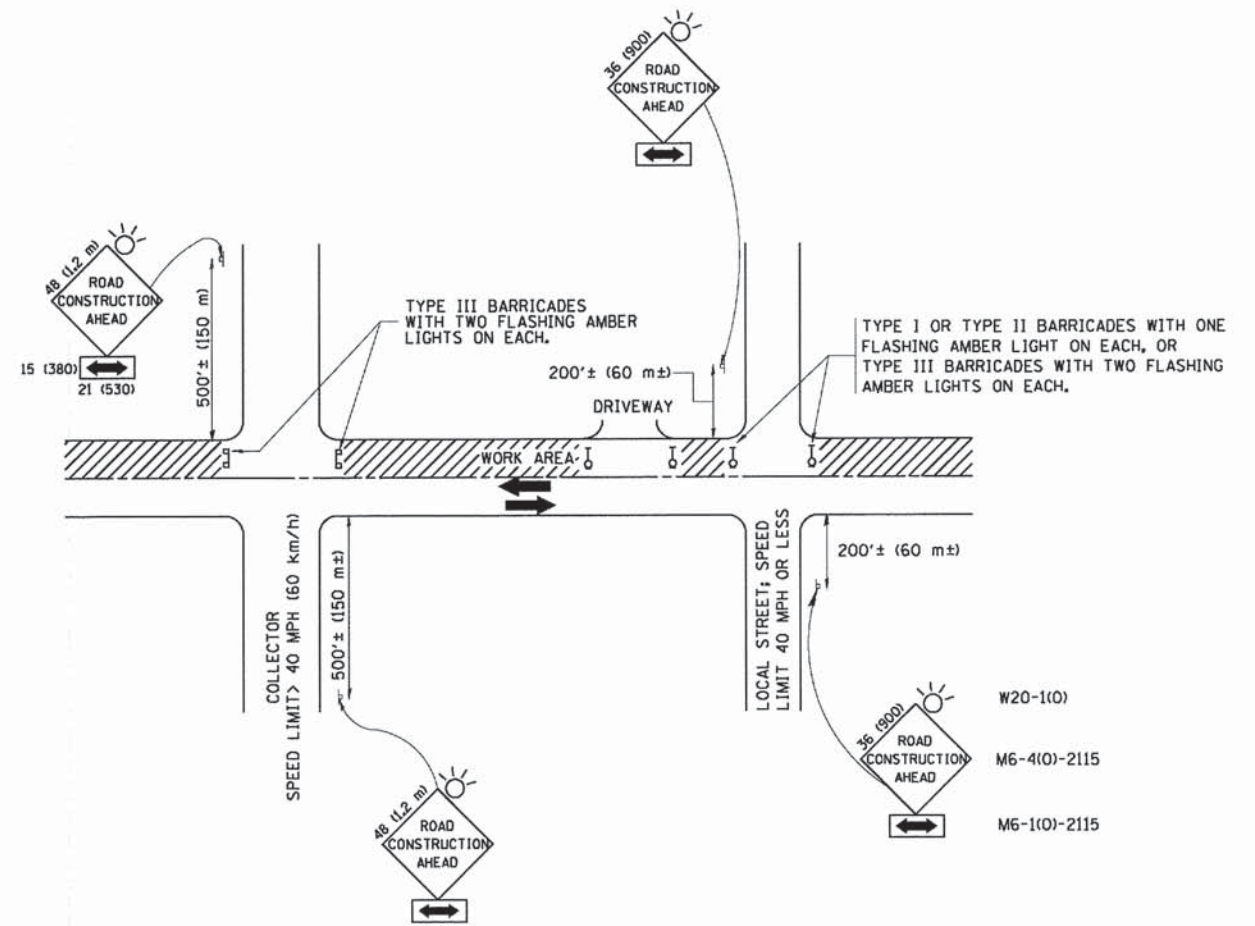
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DESIGNED	A. HOUSEH	REVISED	R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
DRAWN	-	REVISED	A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	1698	14-00091-00-RS	COOK	15	11
CHECKED	-	REVISED	M. GOMEZ 01-22-01		BD600-06 (BD-24)										
DATE	03-11-94	REVISED	R. BORO 12-15-09		CONTRACT NO. 61830										
PLCT DATE	12/15/2009	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT M-4003(412)													









TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  - B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

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 REVISED - J. OBERLE 10-18-95  
 REVISED - A. HOUSEH 03-06-96  
 REVISED - A. HOUSEH 10-15-96  
 REVISED - T. RAMMACHER 01-06-00

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PLOT SCALE = 58.000 1/ IN.		CHECKED -	REVISED - A. HOUSEH 10-15-96
PLOT DATE = 1/4/2008		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

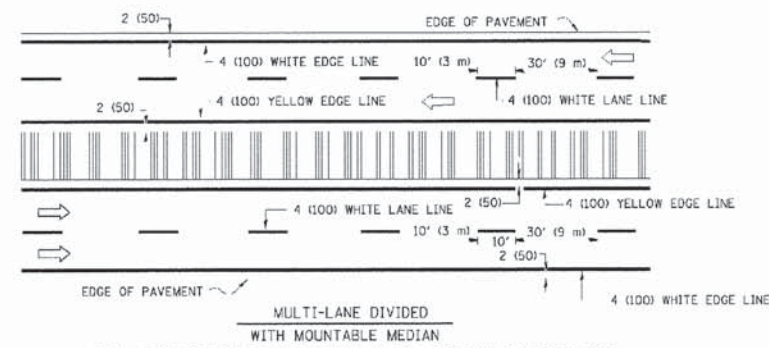
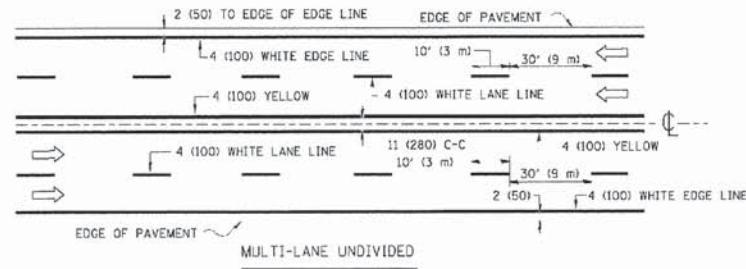
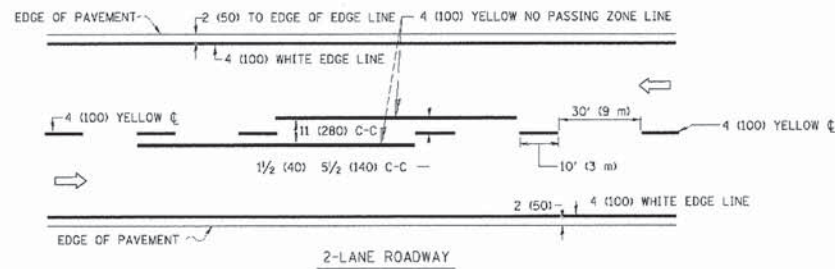
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

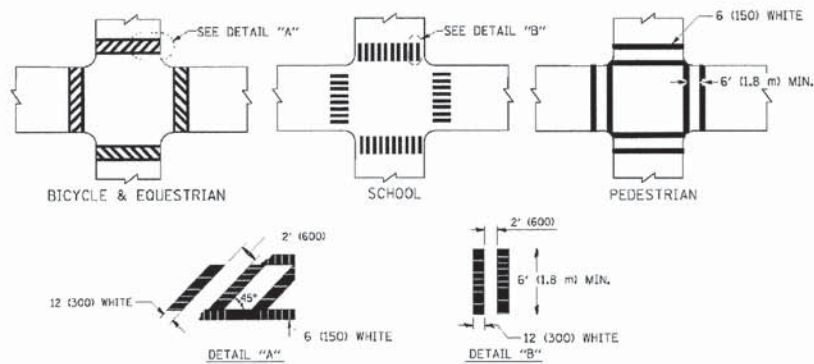
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	13
TC-10			CONTRACT NO. 61B30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(412)				



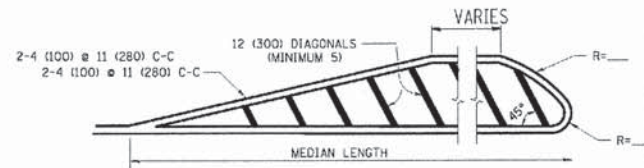
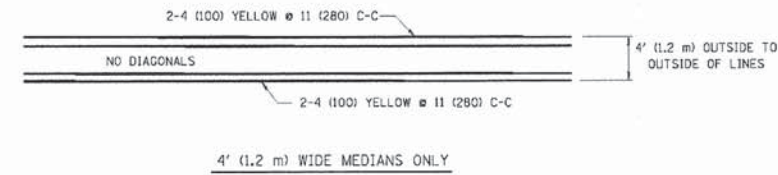


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**

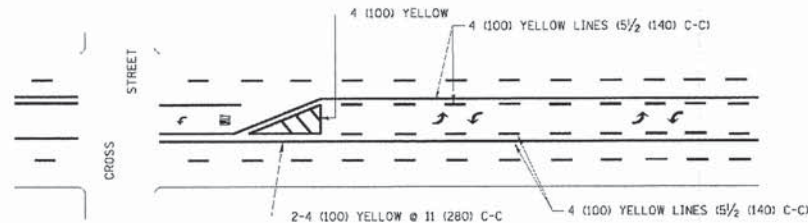


**TYPICAL CROSSWALK MARKING**



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
 150' (45 m) C-C (OVER 45MPH (70 km/h))

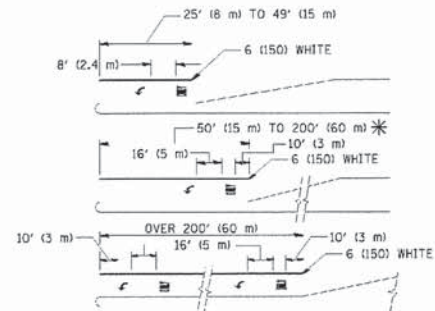
**MEDIANS OVER 4' (1.2 m) WIDE**



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

**MEDIAN WITH TWO-WAY LEFT TURN LANE**

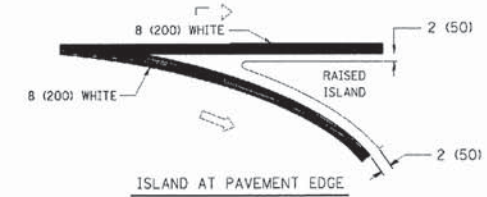
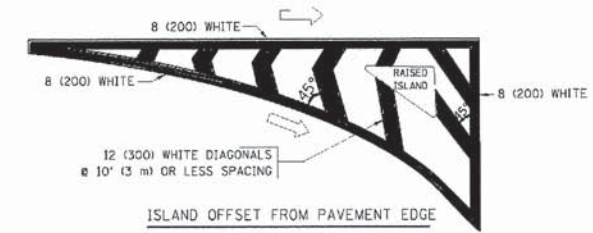
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 6 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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 4/26/08 PW

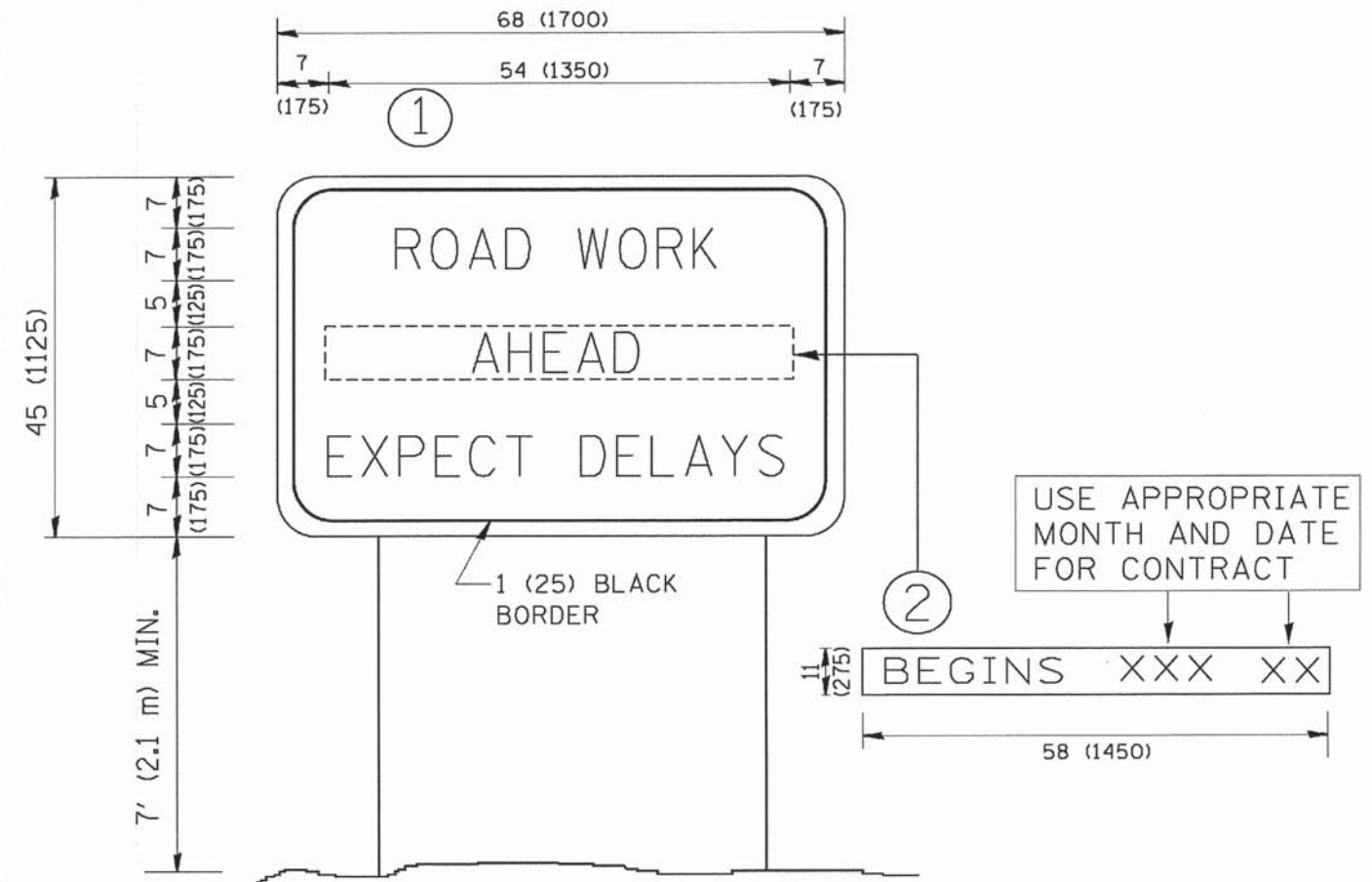
DESIGNED	EVERS	REVISED	T. RAMMACHER 10-27-94
DRAWN		REVISED	-C. JUCIUS 09-09-09
CHECKED		REVISED	
DATE	03-19-90	REVISED	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	14
<b>TC-13</b>		CONTRACT NO. 61B30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(412)				





**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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 I:\CD\yato\Love\LAGRVA\20084-Amen\hngton\_SIP\_Asp\CAD\DD\20084.DISTRICT ONE DETAILS - TC.dgn

FILE NAME = M:\disto\22x34\to22.dgn	USER NAME = geglienab	DESIGNED - DRAWN -	REVISED - REVISED -
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. MIRS 09-15-97	REVISED - R. MIRS 12-11-97
PLOT DATE = 1/4/2008	DATE -	REVISED - T. RAMMACHER 02-02-99	REVISED - C. JUCLUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1698	14-00091-00-RS	COOK	15	15
<b>TC-22</b>		<b>CONTRACT NO. 61B30</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(412)				