

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3890	14-00105-00-RS	KANE	19	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO.	61B35	

INDEX OF SHEETS

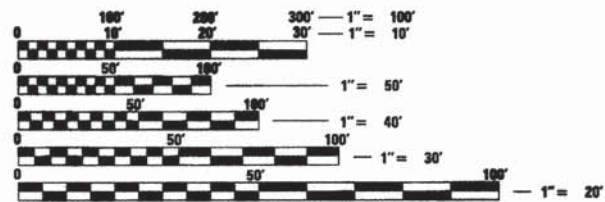
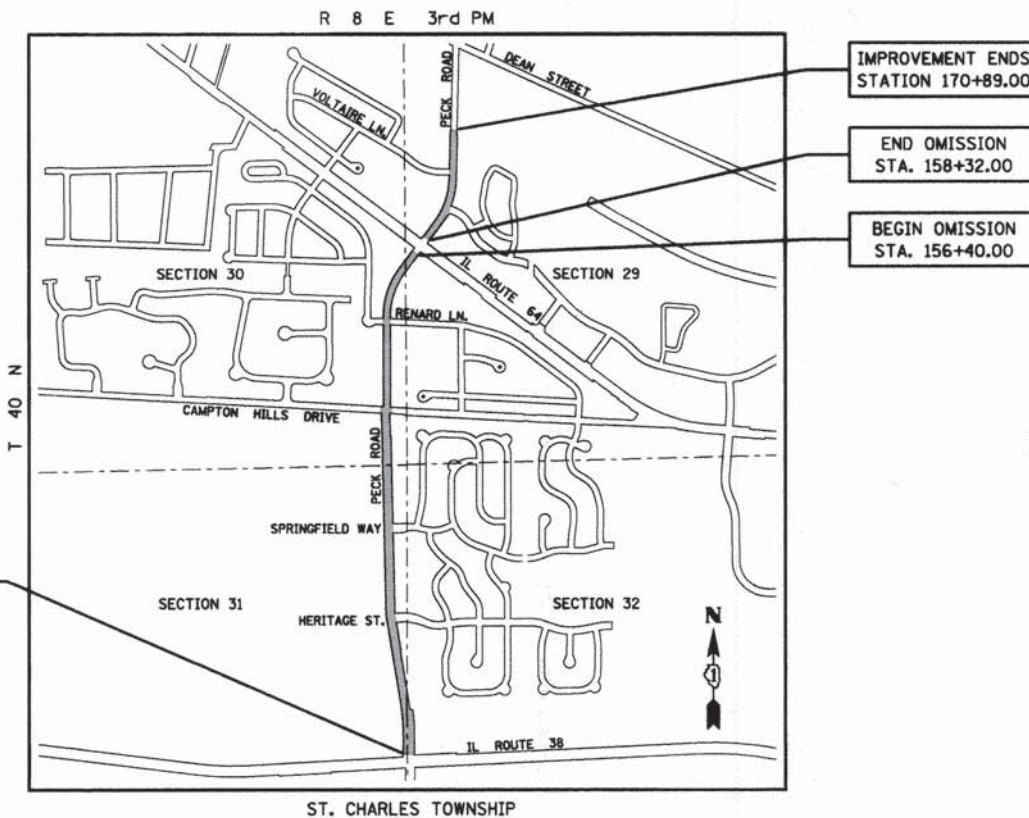
SHEET NO.	DESCRIPTION
01	COVER SHEET
02	GENERAL NOTES
03	SUMMARY OF QUANTITIES
04 - 07	TYPICAL SECTIONS
08 - 10	PAVEMENT MARKING PLAN
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FAU 3890 (PECK ROAD)  
FAP 347 (IL ROUTE 38) TO (DEAN STREET)  
RESURFACING  
SECTION 14-00105-00-RS  
PROJECT M-4003(404)  
CITY OF ST. CHARLES  
KANE COUNTY  
C-91-077-15



TRAFFIC DATA

PECK ROAD  
ADT (2013) = 14,000  
SPEED LIMIT = 45 MPH RT. 38 TO RENARD LN.  
SPEED LIMIT = 35 MPH RENARD LN. TO DEAN ST.  
MAJOR COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

Approved: 2/4/15  
Karen R. Young P.E.  
City of St. Charles, Assistant Director of Public Works - Engineering

Passed: FEBRUARY 10, 2015  
CHRISTOPHER HOLT  
District #1 Engineer of Local Roads & Streets

Releasing for Bid Based on Limited Review: February 19, 2015  
John Fortson  
Deputy Director of Highways, Region One Engineer

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OF THE STATE OF ILLINOIS

FEDERAL AID PROGRAM ENGINEER : FAWAD AQUEEL, P.E. 847-705-4021 SCHAUMBURG, IL



Joint Utility Locating Information for Excavators

IDOT BUREAU OF TRAFFIC  
MEADE ELECTRIC  
773-287-7682

LOCATION MAP - SCALE : 1" = 1200'

GROSS LENGTH OF IMPROVEMENTS - PECK ROAD = 7028 LINEAL FEET (1.331 MILES)  
NET LENGTH OF IMPROVEMENTS - PECK ROAD = 6836 LINEAL FEET (1.295 MILES)



# GENERAL NOTES

## SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2015; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS"; THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.
- BEFORE STARTING ALL EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 1-800-892-0123 FOR FIELD LOCATION OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR A PROFESSIONAL LAND SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- ALL PAY ITEMS FOR REMOVAL AND REPLACEMENT MUST BE FIELD MEASURED AND MARKED BY THE ENGINEER PRIOR TO CONSTRUCTION. NO PAYMENT WILL BE MADE FOR ANY ITEMS OF WORK, WHICH HAVE BEEN REMOVED AND/OR REPLACED WITHOUT HAVING BEEN FIELD MEASURED AND MARKED BY THE ENGINEER. NO ADDITIONAL PAYMENT WILL BE MADE FOR REMOVAL AND/OR REPLACEMENT BEYOND FIELD MARKINGS UNLESS SPECIFICALLY AUTHORIZED BY THE ENGINEER.
- BY THE END OF EACH WORKING DAY THE CONTRACTOR AND HIS SUBCONTRACTORS SHALL REMOVE RUBBISH, WASTE MATERIAL AND ACCUMULATIONS FROM THE PREMISES AND SHALL KEEP THE PREMISES CLEAN. THE CONTRACTOR SHALL KEEP THE PREMISES CLEAN DURING CONSTRUCTION TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF MOBILIZATION.
- ALL EXPOSED CONCRETE INSTALLED UNDER THIS CONTRACT (CURB & GUTTER, SIDEWALK, DRIVEWAY AREAS) SHALL RECEIVE A PROTECTIVE SURFACE TREATMENT CONSISTING OF TWO (2) COATS OF BOILED LINSEED OIL AND PETROLEUM SPIRITS MIXTURE, FORMULATED AND APPLIED ACCORDING TO ARTICLE 420.18 OF THE STANDARD SPECIFICATIONS. IF THE ENGINEER FOR BLOTTER MATERIAL REQUIRES AN APPLICATION OF SAND, IT WILL BE INCLUDED IN THE COST OF THE CONCRETE. THE ENGINEER SHALL BE NOTIFIED 24 HOURS IN ADVANCE PRIOR TO APPLICATION. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN COST OF THE CONCRETE ITEM PROVIDED.
- THE CONTRACTOR MUST SCHEDULE THE REMOVAL AND REPLACEMENT OF THE CURB AND GUTTER SUCH THAT ONLY ONE SIDE OF A GIVEN STREET WILL BE UNDER CONSTRUCTION AT ANY ONE TIME. ALL HOMEOWNERS SHALL BE GIVEN A MINIMUM 24 HOURS NOTICE PRIOR TO EXCAVATION OF THEIR DRIVEWAY. IN NO CASE SHALL AN OPEN EXCAVATION CAUSED BY REMOVAL OF EXISTING CURBING, WHETHER FORMED OR NOT FORMED, REMAIN OPEN FOR MORE THAN 3 WORKING DAYS.
- DISTURBED PAVEMENT, DRIVEWAY AND PARKWAY AREAS SHALL BE RESTORED IMMEDIATELY FOLLOWING REPLACEMENT OPERATIONS. IN ALL CASES WITHIN 3 WORKING DAYS FROM THE DATE CURB AND GUTTER OR SIDEWALK IS CAST. THE ENGINEER SHALL STOP THE CONTRACTOR FROM FURTHER REMOVAL OPERATIONS AT ANY TIME HE DETERMINES THE RESTORATION IS NOT BEING DONE IN A TIMELY MANNER.
- DURING THE REMOVAL OF MATERIAL AND PREPARATION FOR PLACEMENT OF P.C.C. MATERIAL, A TOLERANCE OF NO MORE THAN TWO FEET OF THE EXISTING LIMITS SHALL BE EXCEEDED FOR RESTORATION. ALL RESTORATION OUTSIDE OF THESE LIMITS SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED CURB AND GUTTER, OR SIDEWALK REMOVAL PAY ITEM.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF ST. CHARLES, CITY ENGINEER, 630-377-4405, 48 HOURS PRIOR TO THE COMMENCEMENT OF WORK.
- WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER, AND/OR OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS, WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES, WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED. THE CONTRACTOR IS PROHIBITED FROM BREAKING UP CONCRETE BY DROPPING IT ON THE PAVEMENT OR IN ANY OTHER MANNER, WHICH IN THE OPINION OF THE ENGINEER MAY DAMAGE EXISTING OR PROPOSED PAVEMENTS OR OTHER ROADWAY APPURTENANCES.
- DETECTABLE WARNINGS - THE CONTRACTOR SHALL UTILIZE THE VITRIFIED POLYMER COMPOSITE NON-REMOVABLE DETECTABLE TACTILE WARNING SYSTEM IN THE RED COLOR IN CONFORMANCE WITH ADAAG.

## STORM SEWERS, WATER MAINS, AND UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES 3 DAYS PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF MANHOLES TO BE ADJUSTED, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY ARE NOT SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER OR REPLACED. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE, IN ACCORDANCE WITH ARTICLES 107.20 AND 107.31.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE CITY FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND BE READILY VISIBLE AT ALL TIMES.
- THE USE OF CITY OWNED AND OPERATED FIRE HYDRANTS IS PROHIBITED. THE CONTRACTOR SHALL CONTACT THE ST. CHARLES PUBLIC WORKS WATER DIVISION 630-377-4405, TO COORDINATE USEAGE OF CITY HYDRANTS. ALL FINES ASSOCIATED WITH DAMAGE CAUSED BY THE USE OF A CITY FIRE HYDRANT WITHOUT PROPER AUTHORIZATION SHALL BE SOLELY BORNE BY THE CONTRACTOR.
- UNLESS OTHERWISE NOTED IN THE CONTRACT DOCUMENTS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. PRIOR TO COMMENCING WORK THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES THAT ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS. UNLESS RECONSTRUCTION OR ADJUSTMENT OF AN EXISTING MANHOLE, CATCH BASIN, INLET OR ADJUSTMENT TO THE FRAME AND GRATE IS CALLED FOR IN THE CONTRACT DOCUMENTS OR ORDERED BY THE ENGINEER, THE PROPOSED WORK SHALL MEET THE EXISTING ELEVATION OF THESE STRUCTURES. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS WHEN WORKING NEAR OR ABOVE EXISTING SEWERS TO PROTECT THESE SEWERS FROM ANY DAMAGE RESULTING FROM HIS OPERATIONS. ALL WORK AND MATERIAL NECESSARY TO REPAIR ANY EXISTING SEWERS DAMAGED DUE TO NON-COMPLIANCE WITH THIS PROVISION SHALL BE PROVIDED, AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH SECTION 550 OF THE STANDARD SPECIFICATIONS, AT THE CONTRACTOR'S EXPENSE WITH NO EXTRA COMPENSATION BEING ALLOWED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DIRECT THE WORK AND PROTECT THE FACILITIES FROM DAMAGE DURING CONSTRUCTION, IN ACCORDANCE WITH ARTICLES 107.20 AND 107.31.

## SIGNING AND STRIPING

- SEE IDOT STANDARD 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.

## TRAFFIC CONTROL

- THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE SPECIAL PROVISIONS & PLANS.
- THE CONTRACTOR SHALL MAINTAIN EXISTING SIDE STREET ACCESS AND EXISTING DRIVEWAY ACCESS TO ADJUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION OF THIS PROJECT.
- ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 107.14 OF THE STANDARD SPECIFICATIONS.
- THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

## HIGHWAY STANDARDS

STD. 000001-06  
 STD. 424001-08  
 STD. 604001-04  
 STD. 604006-05  
 STD. 606001-06  
 STD. 701006-05  
 STD. 701301-04  
 STD. 701311-03  
 STD. 701501-06  
 STD. 701701-09  
 STD. 701801-05  
 STD. 701901-04  
 STD. 780001-05

## DISTRICT 1 DETAILS

TC-10  
 TC-13  
 TC-16  
 TC-22  
 BD-01  
 BD-22  
 BD-24  
 BD-32

## COMMITMENTS

THERE ARE NO COMMITMENTS FOR THIS PROJECT.

FILE NAME	USER NAME =	DESIGNED - BH	REVISED - 02/02/2015	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES PECK ROAD</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	DRAWN - BH	REVISED -			3890	14-00105-00-RS	KANE	19	2
	PLOT DATE =	CHECKED -	REVISED -			CONTRACT NO. 61B35		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT
		DATE - 12/05/2014	REVISED -	SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	



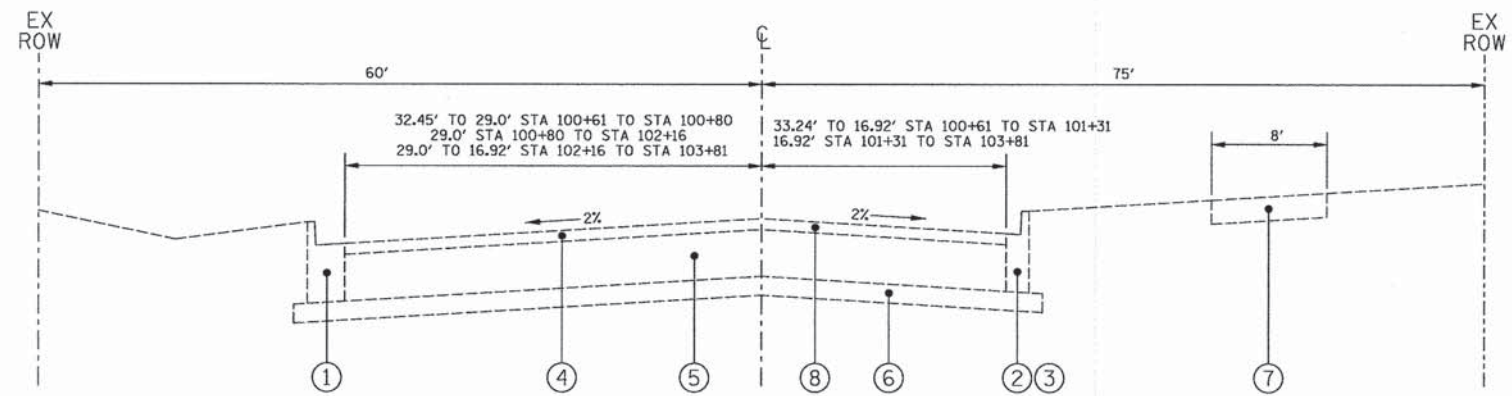
# SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
				STU FUNDS			
				75% FED 25% CITY ROADWAY 0005 URBAN	75% FED 25% CITY ENVIRONMENT 0020 URBAN	75% FED 25% CITY SAFETY 0021 URBAN	75% FED 25% CITY LANDSCAPE 0031 URBAN
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	680				680
25000110	SEEDING, CLASS 1A	ACRE	0.25				0.25
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	14				14
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	14				14
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	14				14
25100630	EROSION CONTROL BLANKET	SQ YD	680		680		
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	22520	22520			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	3	3			
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	1187	1187			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	350	350			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3164	3164			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1400			1400	
42400800	DETECTABLE WARNINGS	SQ FT	325			325	
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	1075	1075			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	27175	27175			
44000600	SIDEWALK REMOVAL	SQ FT	700	700			
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	265	265			
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	108	108			
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	28250	28250			
60100072	SHOULDER REMOVAL AND REPLACEMENT 5"	FOOT	1225	1225			
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	5	5			
60260100	INLETS TO BE ADJUSTED	EACH	4	4			
67100100	MOBILIZATION	L SUM	1	1			
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1			
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	8040			8040	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	900			900	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	500			500	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	19175			19175	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2825			2825	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1825			1825	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	275			275	
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	130			130	
X0327036	BIKE PATH REMOVAL	SQ YD	100	100			
X6025600	MANHOLES TO BE ADJUSTED (SPECIAL)	EACH	5	5			
XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	110	110			
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	590	590			
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4			
Z0058668	GRADING AND SHAPING FORESLOPES	SQ YD	210	210			

\* - SPECIALTY ITEMS

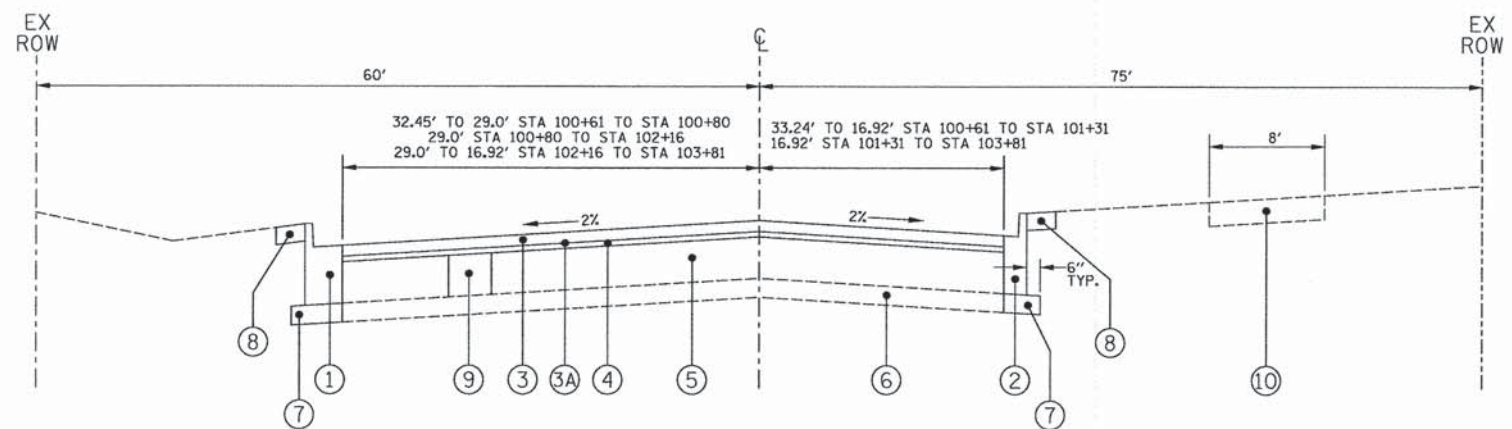
FILE NAME	USER NAME =	DESIGNED - BH	REVISED - 02/02/2015	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES PECK ROAD</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
	PLOT SCALE =	DRAWN - BH	REVISED - 02/10/2015		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	3890	14-00105-00-RS	KANE	19	3
	PLOT DATE =	CHECKED -	REVISED -												
		DATE - 12/05/2014	REVISED -								FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		CONTRACT NO. 61B35





**EXISTING TYPICAL SECTION**  
STA 100+61 TO STA 103+81 PECK ROAD

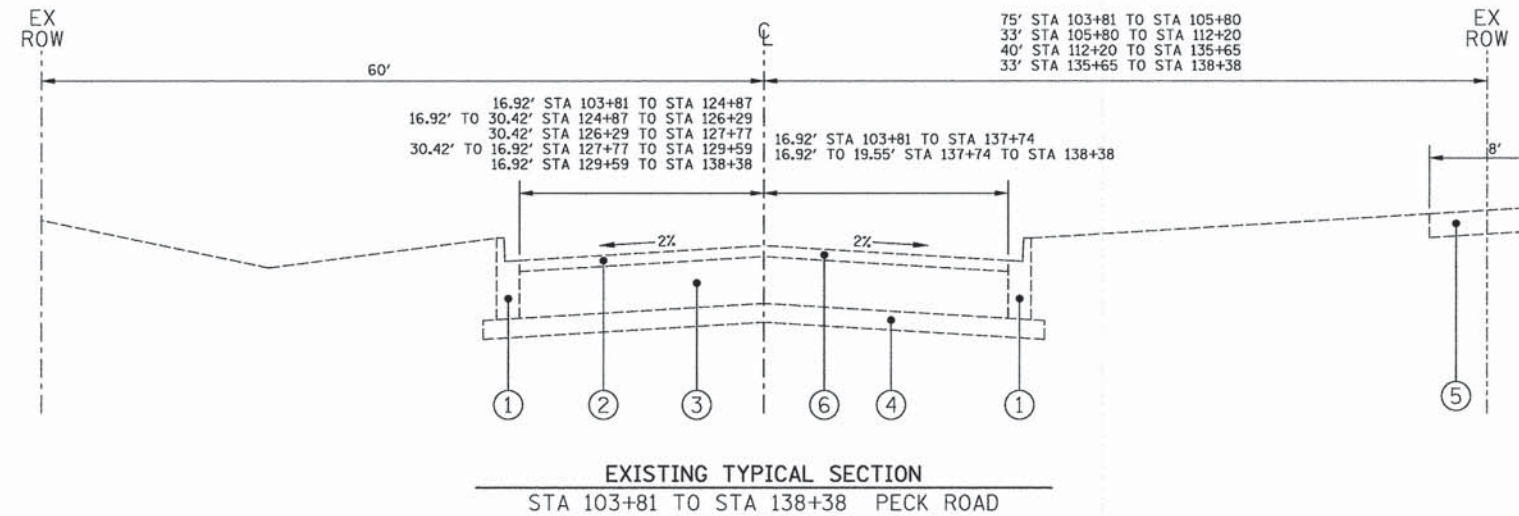
- |  |  |
|--|--|
| ① EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24                             | ⑤ EXISTING HOT-MIX ASPHALT BINDER COURSE, 8-1/2" |
| ② EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24<br>STA 100+61 TO STA 101+31 | ⑥ EXISTING SUB-BASE GRANULAR MATERIAL, 4"        |
| ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12<br>STA 101+31 TO STA 103+81 | ⑦ EXISTING HOT-MIX ASPHALT RECREATIONAL PATH     |
| ④ EXISTING HOT-MIX ASPHALT SURFACE COURSE, 2"  | ⑧ BITUMINOUS SURFACE REMOVAL, 2-1/2"             |



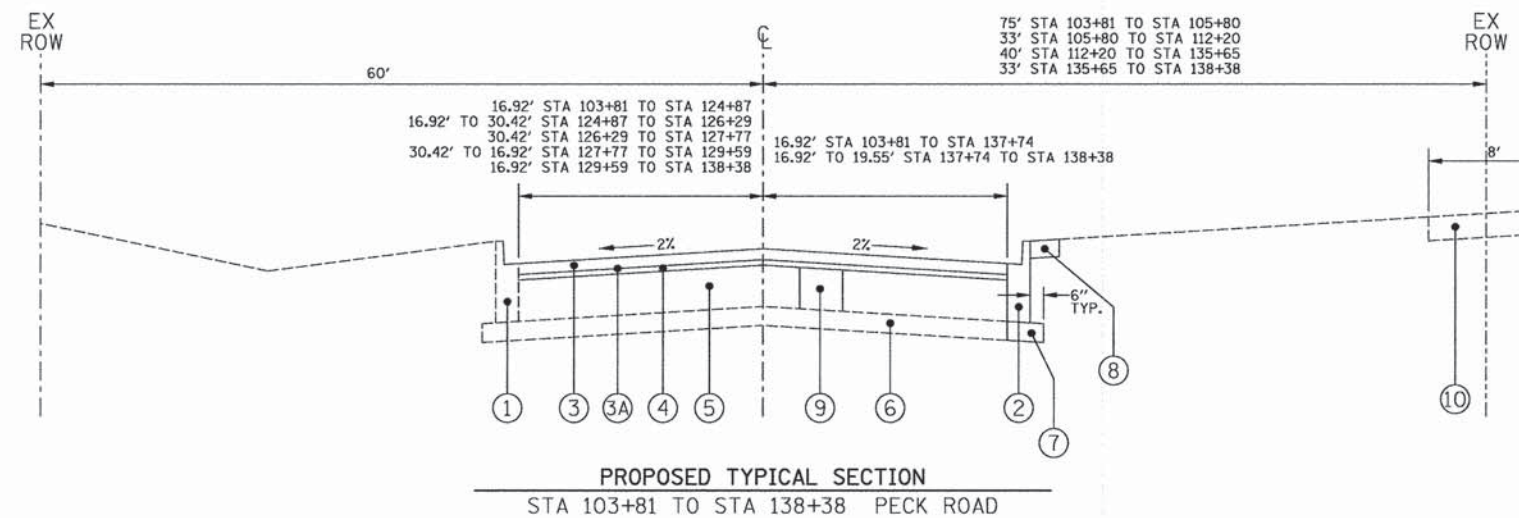
**PROPOSED TYPICAL SECTION**  
STA 100+61 TO STA 103+81 PECK ROAD

- |   |  |
|---|--|
| ① PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24<br>(LOCATIONS OF CURB & GUTTER OTHER THAN SHOWN ON PLANS<br>WILL BE DETERMINED IN THE FIELD BY THE ENGINEER) | ⑥ EXISTING SUB-BASE GRANULAR MATERIAL, 4"  |
| ② PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12<br>(LOCATIONS OF CURB & GUTTER OTHER THAN SHOWN ON PLANS<br>WILL BE DETERMINED IN THE FIELD BY THE ENGINEER) | ⑦ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"  |
| ③ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"  | ⑧ PARKWAY TO BE RESTORED WITH 6" TOPSOIL & SEED (2' TYP.)  |
| ③A AREA REFLECTIVE CRACK CONTROL TREATMENT  | ⑨ PROPOSED CLASS D PATCH, TYPE II, 8"<br>(LOCATIONS OF PATCHES WILL BE DETERMINED IN THE FIELD<br>BY THE ENGINEER) |
| ④ PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"   | ⑩ EXISTING HOT-MIX ASPHALT RECREATIONAL PATH   |
| ⑤ EXISTING HOT-MIX ASPHALT BINDER COURSE, 7-3/4"  |  |

FILE NAME	USER NAME *	DESIGNED - BH	REVISED - 02-02-2015	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS PECK ROAD</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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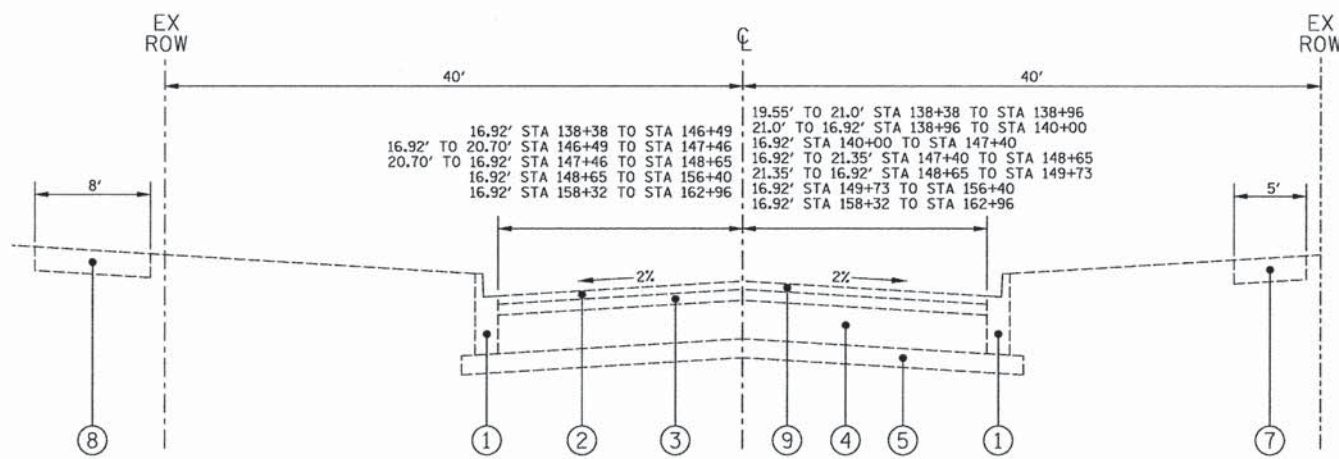
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- ④ EXISTING SUB-BASE GRANULAR MATERIAL, 4"
- ⑤ EXISTING HOT-MIX ASPHALT RECREATIONAL PATH
- ⑥ BITUMINOUS SURFACE REMOVAL, 2-1/2"



- ① EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ② PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 (LOCATIONS OF CURB & GUTTER OTHER THAN SHOWN ON PLANS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER)
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- ⑨ PROPOSED CLASS D PATCH, TYPE II, 8" (LOCATIONS OF PATCHES WILL BE DETERMINED IN THE FIELD BY THE ENGINEER)
- ⑩ EXISTING HOT-MIX ASPHALT RECREATIONAL PATH

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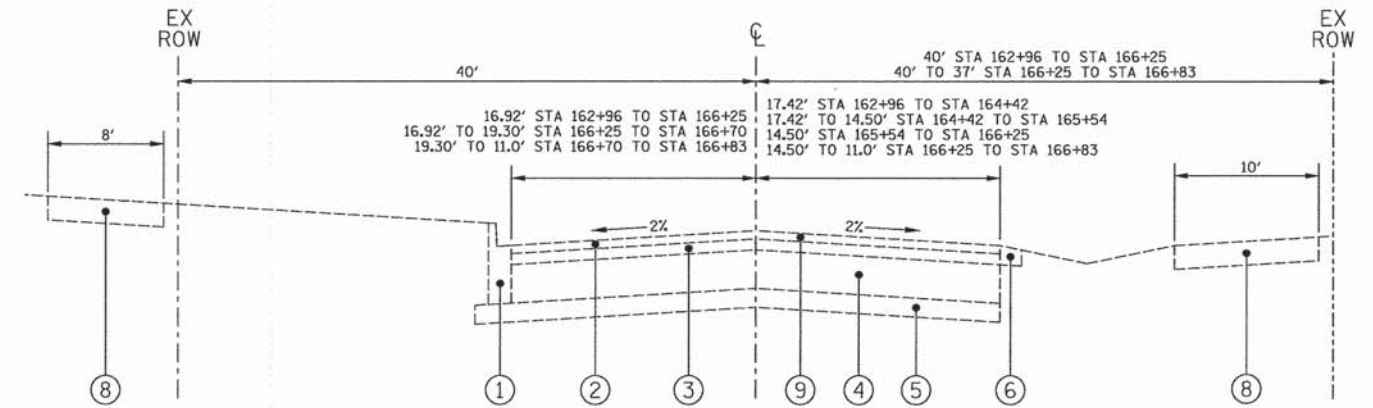




EXISTING TYPICAL SECTION

STA 138+38 TO STA 156+40 PECK ROAD  
STA 158+32 TO STA 162+96 PECK ROAD

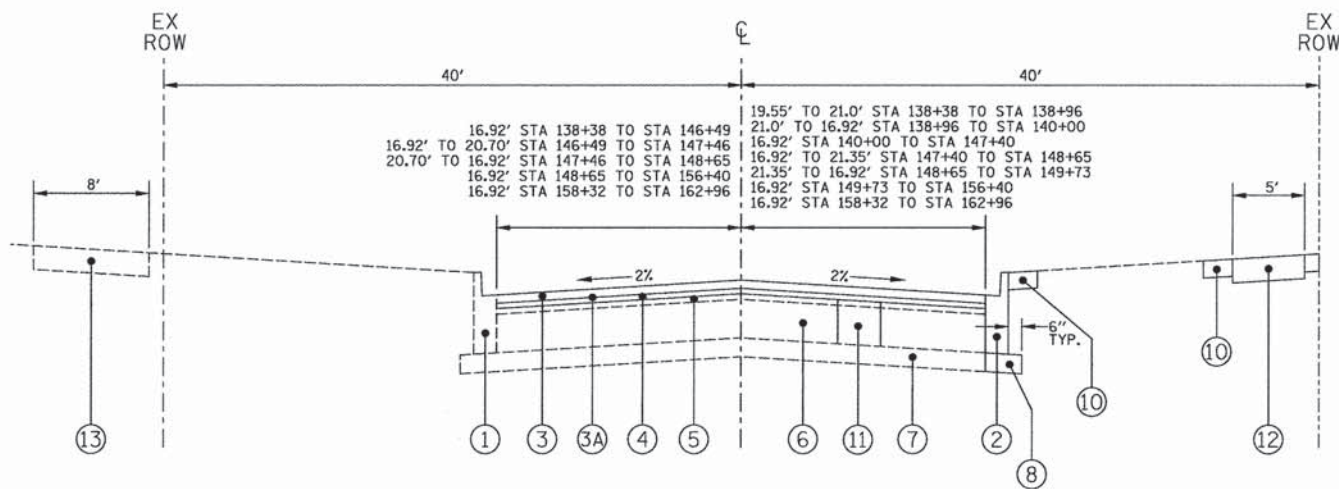
- ① EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ② EXISTING HOT-MIX ASPHALT SURFACE COURSE, 1-1/2"
- ③ EXISTING HOT-MIX ASPHALT BINDER COURSE, 2"
- ④ EXISTING HOT-MIX ASPHALT BASE COURSE, 7"
- ⑤ EXISTING SUB-BASE GRANULAR MATERIAL, 4"



EXISTING TYPICAL SECTION

STA 162+96 TO STA 166+83 PECK ROAD

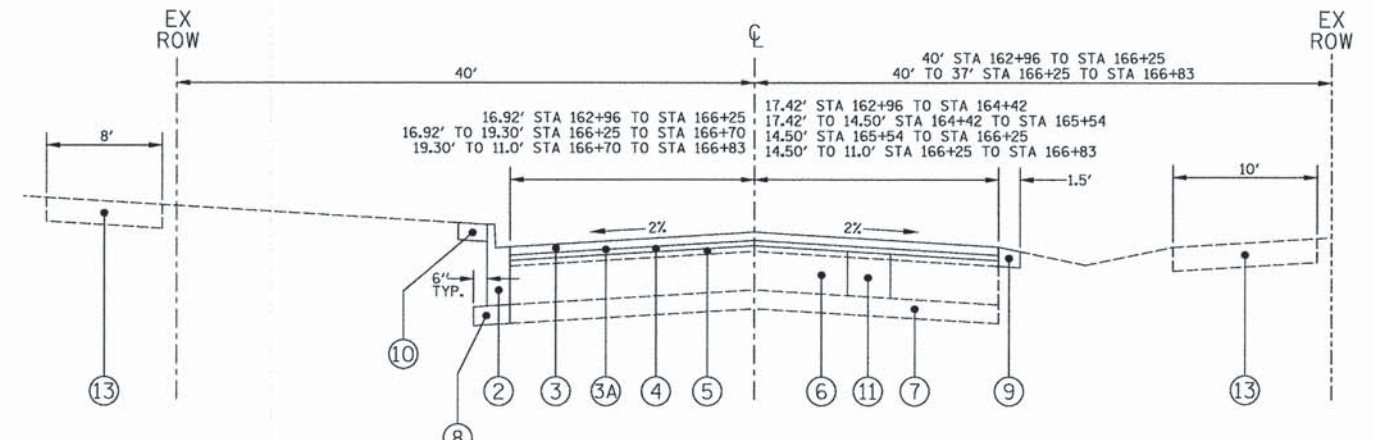
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ EXISTING PCC SIDEWALK
- ⑧ EXISTING HOT-MIX ASPHALT RECREATIONAL PATH
- ⑨ BITUMINOUS SURFACE REMOVAL, 2-1/2"



PROPOSED TYPICAL SECTION

STA 138+38 TO STA 156+40 PECK ROAD  
STA 158+32 TO STA 162+96 PECK ROAD

- ① EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ② PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 (LOCATIONS OF CURB & GUTTER OTHER THAN SHOWN ON PLANS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER)
- ③ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- ③A AREA REFLECTIVE CRACK CONTROL TREATMENT
- ④ PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑤ EXISTING HOT-MIX ASPHALT BINDER COURSE, 3/4"
- ⑥ EXISTING HOT-MIX ASPHALT BASE COURSE, 7"
- ⑦ EXISTING SUB-BASE GRANULAR MATERIAL, 4"
- ⑧ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- ⑨ SHOULDER REMOVAL AND REPLACEMENT



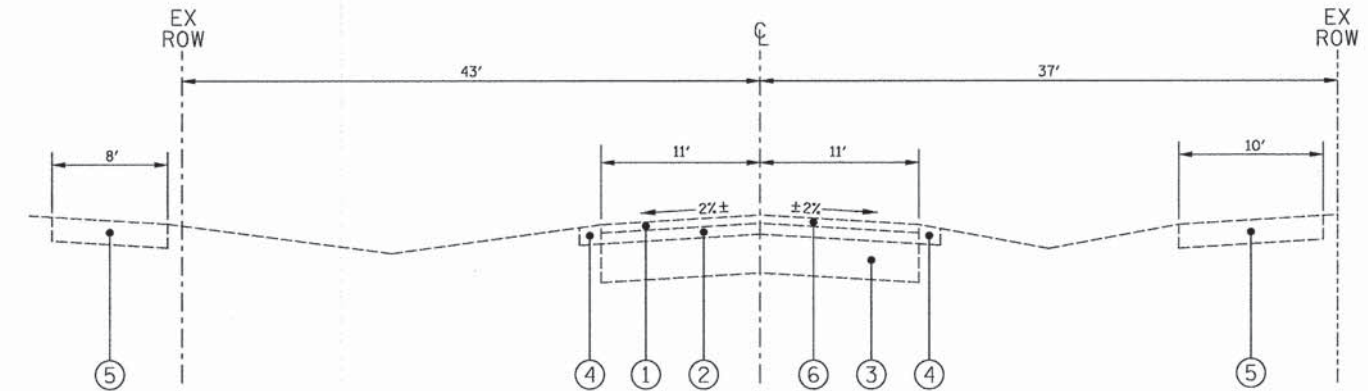
PROPOSED TYPICAL SECTION

STA 162+96 TO STA 166+83 PECK ROAD

- ⑩ PARKWAY TO BE RESTORED WITH 6" TOPSOIL & SEED (2' TYP.)
- ⑪ PROPOSED CLASS D PATCH, TYPE II, 8" (LOCATIONS OF PATCHES WILL BE DETERMINED IN THE FIELD BY THE ENGINEER)
- ⑫ PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 5" OR 6" THICK AT DRIVEWAYS (LOCATIONS OF SIDEWALKS OTHER THAN SHOWN ON PLANS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER)
- ⑬ EXISTING HOT-MIX ASPHALT RECREATIONAL PATH

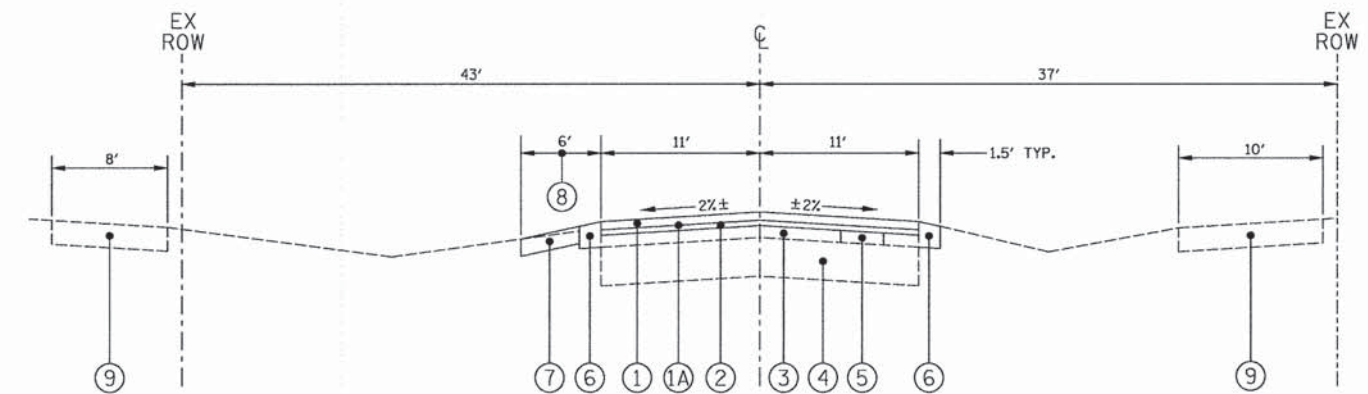
FILE NAME	USER NAME =	DESIGNED - BH	REVISED - 02/02/2015	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS PECK ROAD</b>				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - BH	REVISED -						3890	14-00105-00-RS	KANE	19	6
	PLOT SCALE =	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 61B35				
	PLOT DATE =	DATE - 12/05/2014	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								





**EXISTING TYPICAL SECTION**  
STA 166+83 TO STA 170+89 PECK ROAD

- ① EXISTING HOT-MIX ASPHALT SURFACE COURSE, 2"
- ② EXISTING HOT-MIX ASPHALT BINDER COURSE, 4" - 6"
- ③ EXISTING GRANULAR MATERIAL, 6" - 10"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING HOT-MIX ASPHALT RECREATIONAL PATH
- ⑥ BITUMINOUS SUREFACE REMOVAL, 2-1/4"



**PROPOSED TYPICAL SECTION**  
STA 166+83 TO STA 170+89 PECK ROAD

- ① PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- ② PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ③ EXISTING HOT MIX ASPHALT BINDER COURSE, 4" - 6"
- ④ EXISTING GRANULAR MATERIAL, 6" - 10"
- ⑤ PROPOSED CLASS D PATCH, TYPE II, 6" (LOCATIONS OF PATCHES WILL BE DETERMINED IN THE FIELD BY THE ENGINEER)
- ⑥ SHOULDER REMOVAL AND REPLACEMENT
- ⑦ PARKWAY TO BE RESTORED WITH 6" TOPSOIL & SEED
- ⑧ GRADING AND SHAPING FORESLOPES
- ⑨ EXISTING HOT MIX ASPHALT RECREATIONAL PATH

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @Ndes
<b>PAVEMENT RESURFACING</b>	
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% @ 70 Gyr.
LEVELING BINDER (MACHINE METHOD), N70 (IL 9.5 mm)	4% @ 70 Gyr.
<b>DRIVEWAYS</b>	
HMA SURFACE COURSE, MIX "D", N50; 2" (IL-9.5 mm)	4% @ 50 Gyr.
HMA BASE COURSE; 6" (HMA BINDER IL-19 mm)	4% @ 50 Gyr.
<b>PATCHING</b>	
CLASS D PATCHES, TYPE II, 6 INCH	4% @ 70 Gyr.
CLASS D PATCHES, TYPE II, 8 INCH	4% @ 70 Gyr.

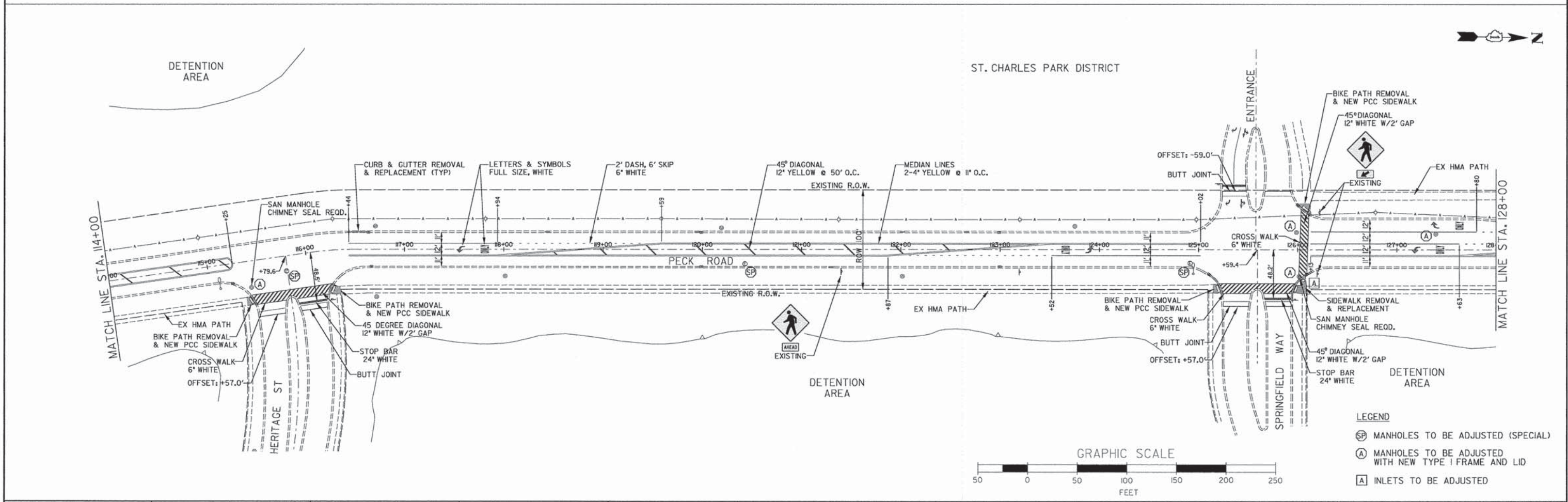
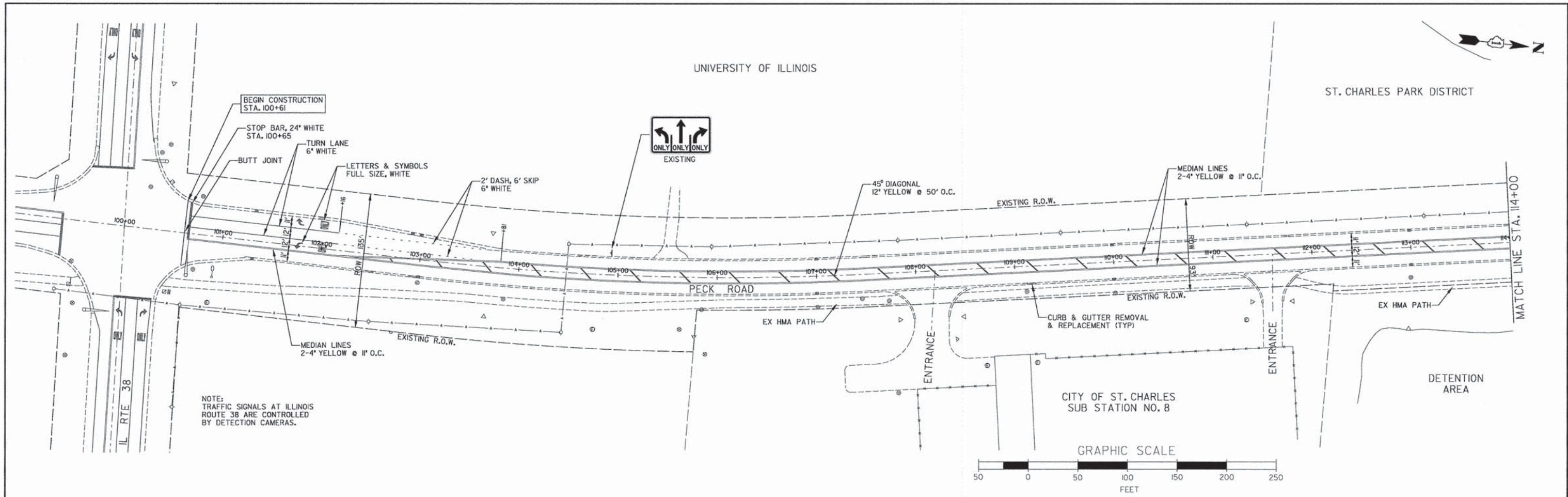
**NOTES:**

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

CONTRACTOR SHALL MILL BEFORE PATCHING.





FILE NAME	USER NAME =	DESIGNED - BH	REVISED - 02/02/2015	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>RESURFACING AND PAVEMENT MARKING PLAN PECK ROAD</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
	PLOT SCALE =	DRAWN - BH	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	3890	14-00105-00-RS	KANE	19	8
	PLOT DATE =	CHECKED -	REVISED -											
		DATE - 12/05/2014	REVISED -											
								FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 61B35			





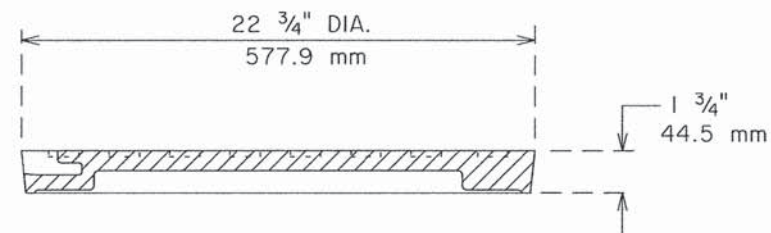
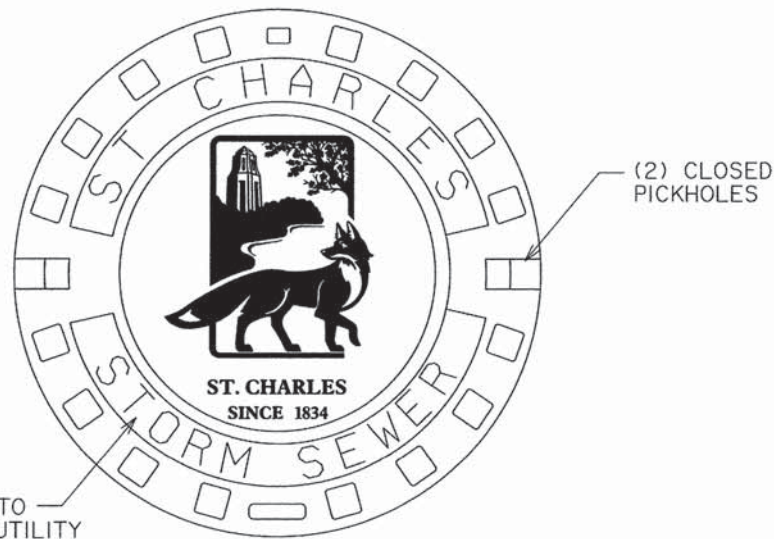






# SPECIAL LETTERED R-1713 HDSI WATER VAULT, SANITARY AND STORM SEWER MANHOLE COVER

1 1/2" (38.1 mm) LETTERS  
(RECESSED FLUSH)

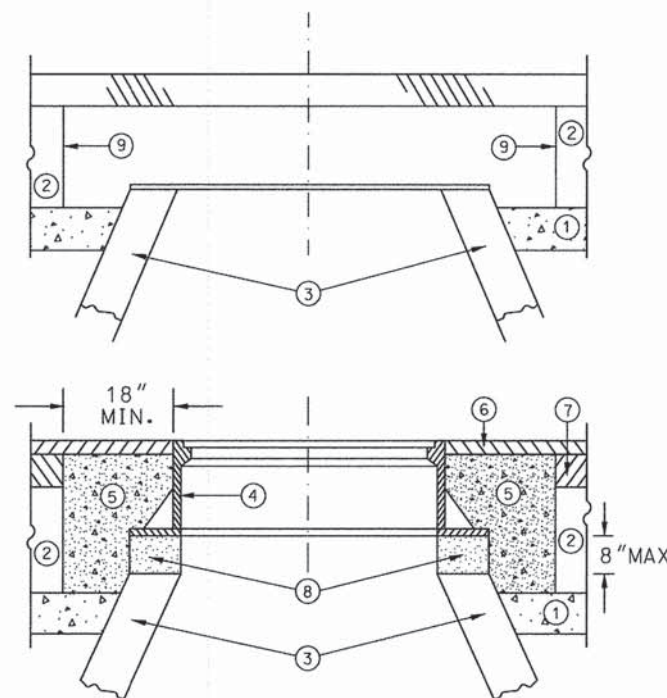


HEAVY DUTY  
MATERIAL ASTM A48 CL 35  
MACHINED BEARING SURFACE  
COVER WT: 125 LBS ( 56.7 kg)

NOT TO SCALE  
**STANDARD  
COVER  
DETAIL**

DATE: 11-19-09

D-7



### LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT TO BE REMOVED
- ③ EXISTING STRUCTURE
- ④ FRAME AND LID (SEE NOTES)
- ⑤ CLASS SI CONCRETE
- ⑥ PROPOSED HMA SURFACE COURSE
- ⑦ PROPOSED HMA BINDER COURSE
- ⑧ PROPOSED MORTAR, STEEL SHIMS & ADJUSTING RINGS 8" MAXIMUM
- ⑨ FULL DEPTH SAW CUT

### NOTES:

EXISTING FRAMES AND LIDS ARE THE PROPERTY OF THE CITY OF ST. CHARLES & SHALL BE REMOVED AND RETURNED TO THE CITY OF ST. CHARLES BY THE CONTRACTOR AND SHALL BE REPLACED AS NOTED ON PLAN.

INSTALLATION OF NEW FRAMES & LIDS WILL NOT BE PAID FOR SEPARATELY BUT ARE INCLUDED AS PART OF THE COST PER NEW STRUCTURE.

### CONSTRUCTION PROCEDURES

#### STAGE 1 (PRIOR TO PAVEMENT MILLING AND PLACEMENT OF HMA BINDER)

- A) FULL DEPTH SAW CUT 5' X 5' SQUARE OR DIAMOND AROUND CENTER OF FRAME TO BE ADJUSTED
- B) REMOVE FRAME, EXISTING PAVEMENT AND AGGREGATE MATERIAL TO A DEPTH OF 2" BELOW TOP OF MANHOLE STRUCTURE & 2" BELOW BOTTOM OF LOWEST ADJUSTMENT RING.
- C) PLATE AND TEMPORARILY PATCH WITH HMA UP TO EXISTING SURFACE GRADE.

#### STAGE 2 (AFTER PAVEMENT MILLING AND PLACEMENT OF HMA BINDER)

- A) REMOVE TEMPORARY PATCH AND PLATE.
- B) INSTALL NEW FRAME AND LID TO MATCH PROPOSED HMA PAVEMENT SURFACE GRADE USING MORTAR, STEEL SHIMS EMBEDDED IN MORTAR, AND PRECAST CONCRETE ADJUSTING RINGS.
- C) COMPACT EXISTING AGGREGATE STONE BASE.
- D) FILL 5'X5' SURROUNDING SPACE WITH IDOT APPROVED CLASS SI CONCRETE TO THE SAME ELEVATION OF THE TOP OF THE BINDER COURSE.
- E) PAVE SURFACE COURSE OVER 5'X 5' CONCRETE COLLAR.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS

DATE: 12-8-14

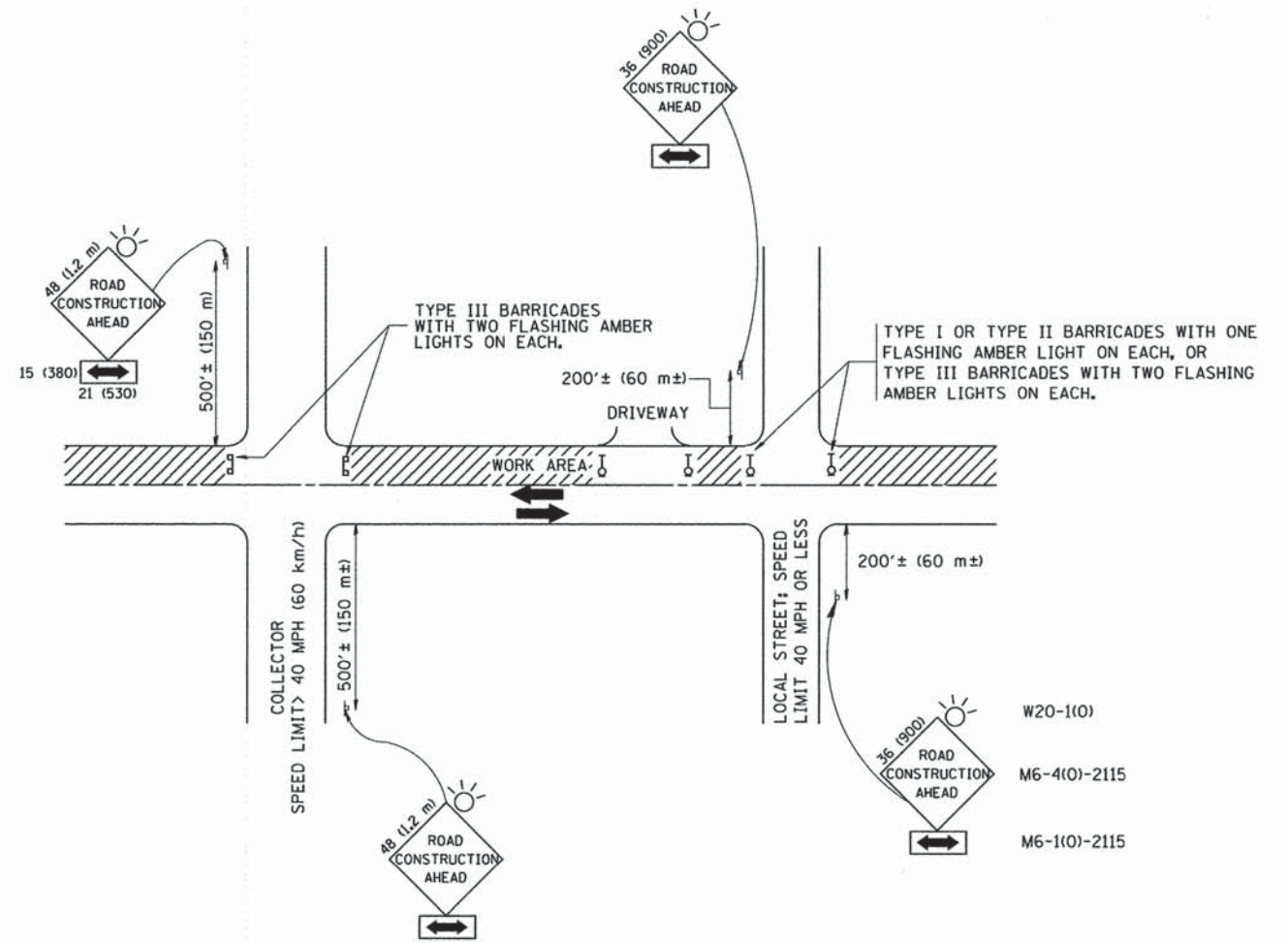
NOT TO SCALE

**FRAME  
AND LID  
ADJUSTMENT  
WITH  
CONCRETE  
COLLAR**

E-6

FILE NAME	USER NAME =	DESIGNED - BH	REVISED - 02/02/2015	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS PECK ROAD</b>	F.A.U. RTE. 3890	SECTION 14-00105-00-RS	COUNTY KANE	TOTAL SHEETS 19	SHEET NO. 11
	PLOT SCALE =	CHECKED -	REVISED -		SCALE:					
	PLOT DATE =	DATE - 12/05/2014	REVISED -		SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 61B35	





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

  - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = geglennob	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
W:\diststd\22x34\td18.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / 1"	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

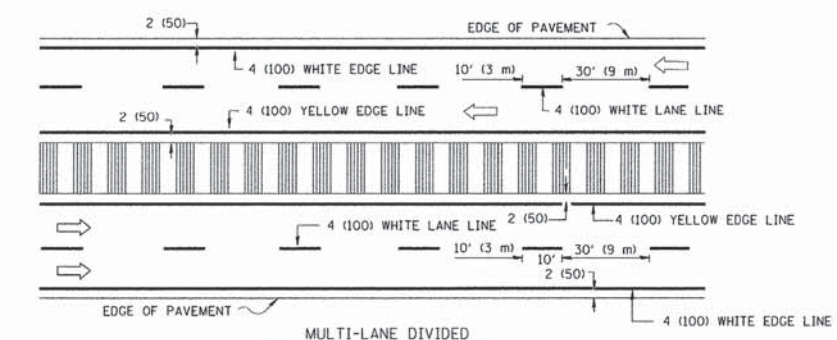
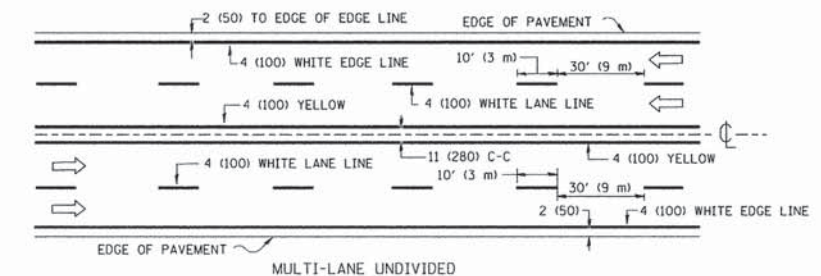
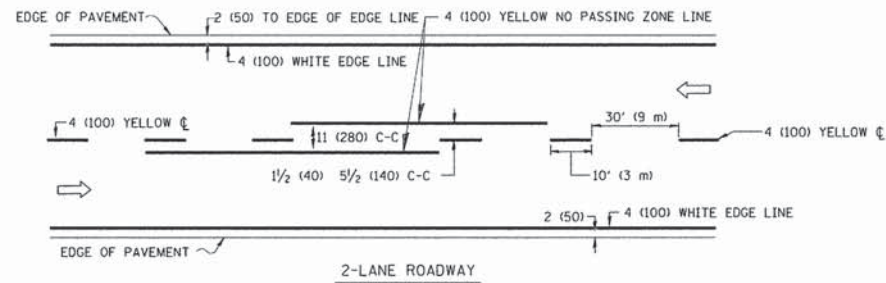
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

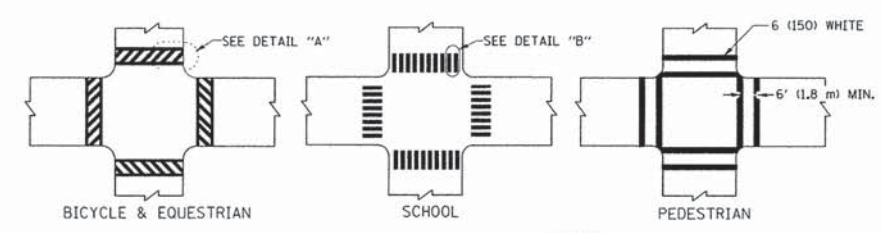
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3890	14-00105-00-RS	KANE	19	12
TC-10			CONTRACT NO. 61B35	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



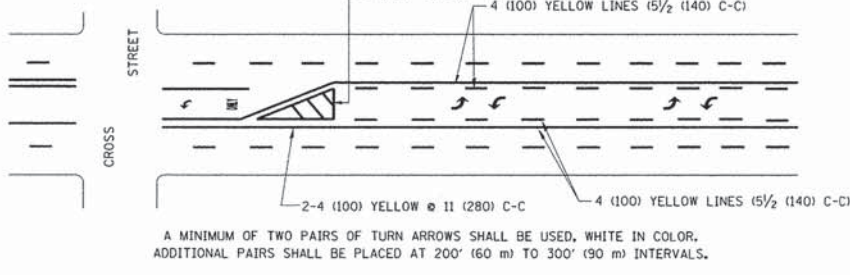
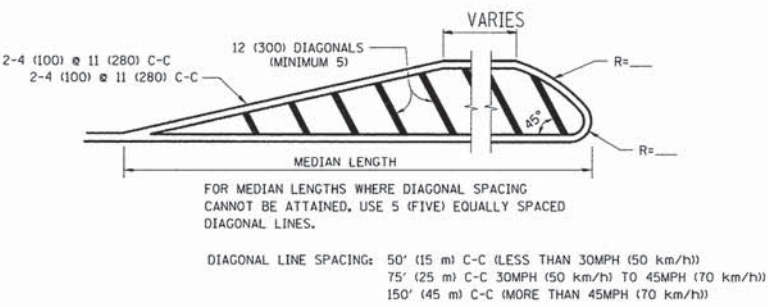
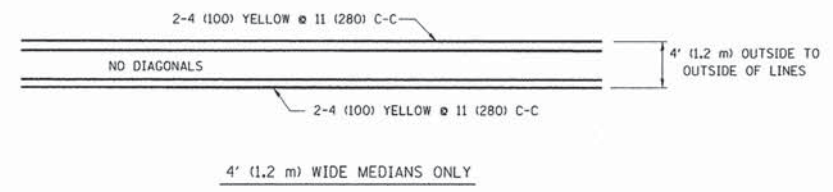


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

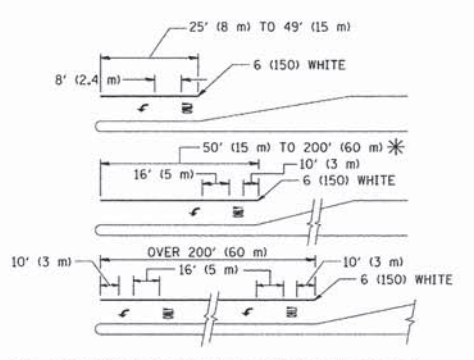


TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

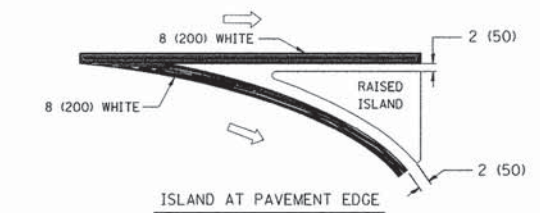
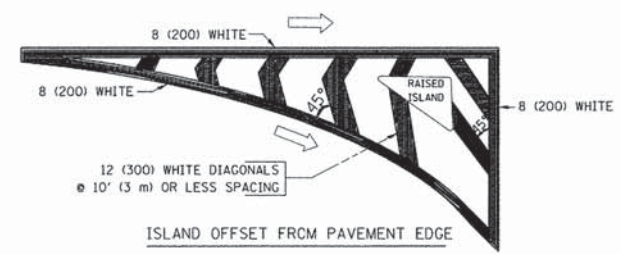
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

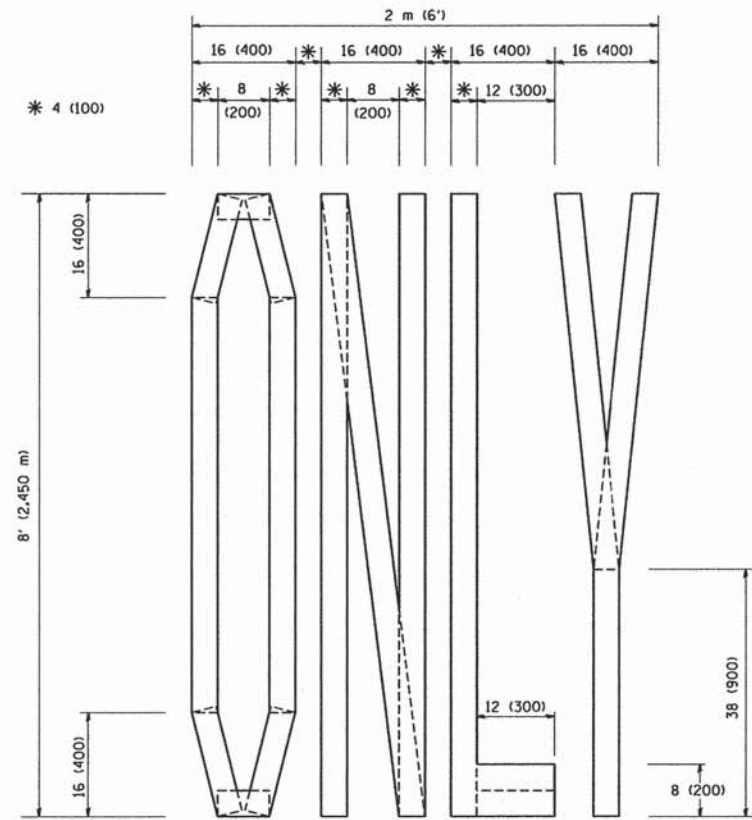
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

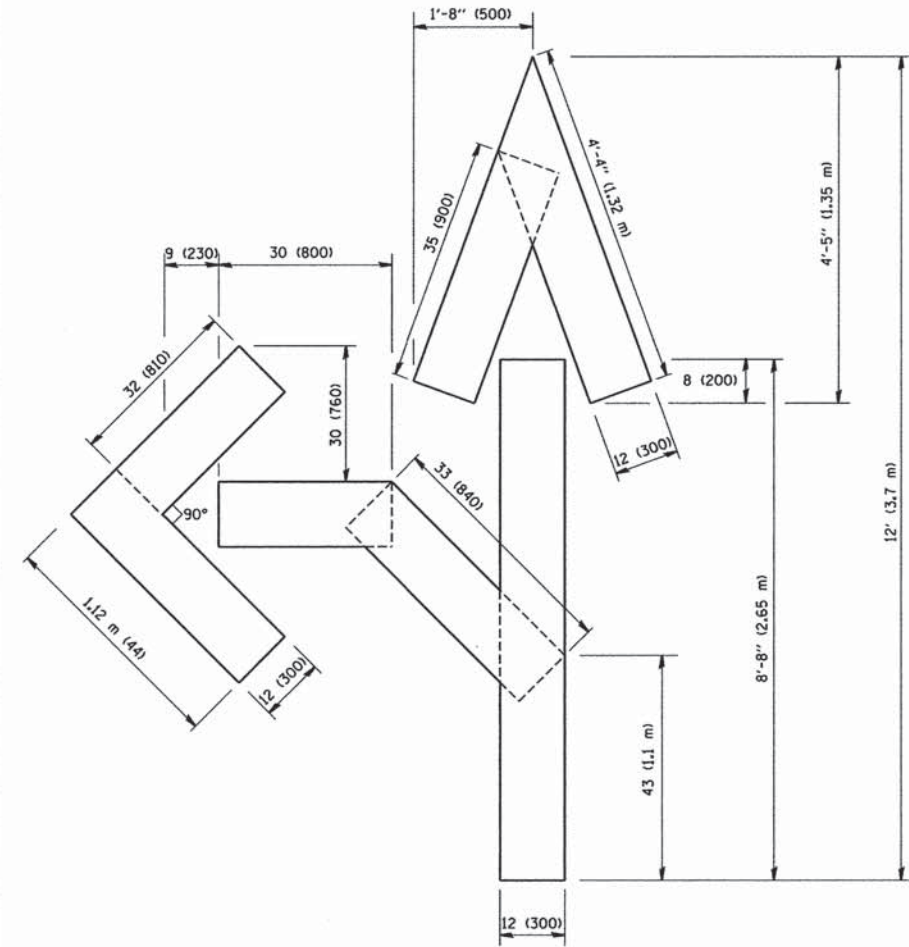
DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		3890	14-00105-00-RS	KANE	19	13
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	

CONTRACT NO. 61B35	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT	

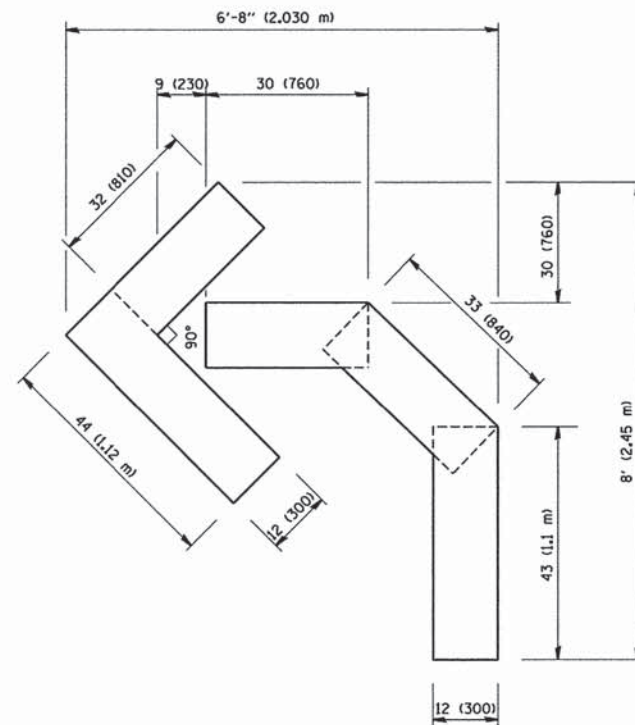




QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED -T. RAMMACHER 11-04-97
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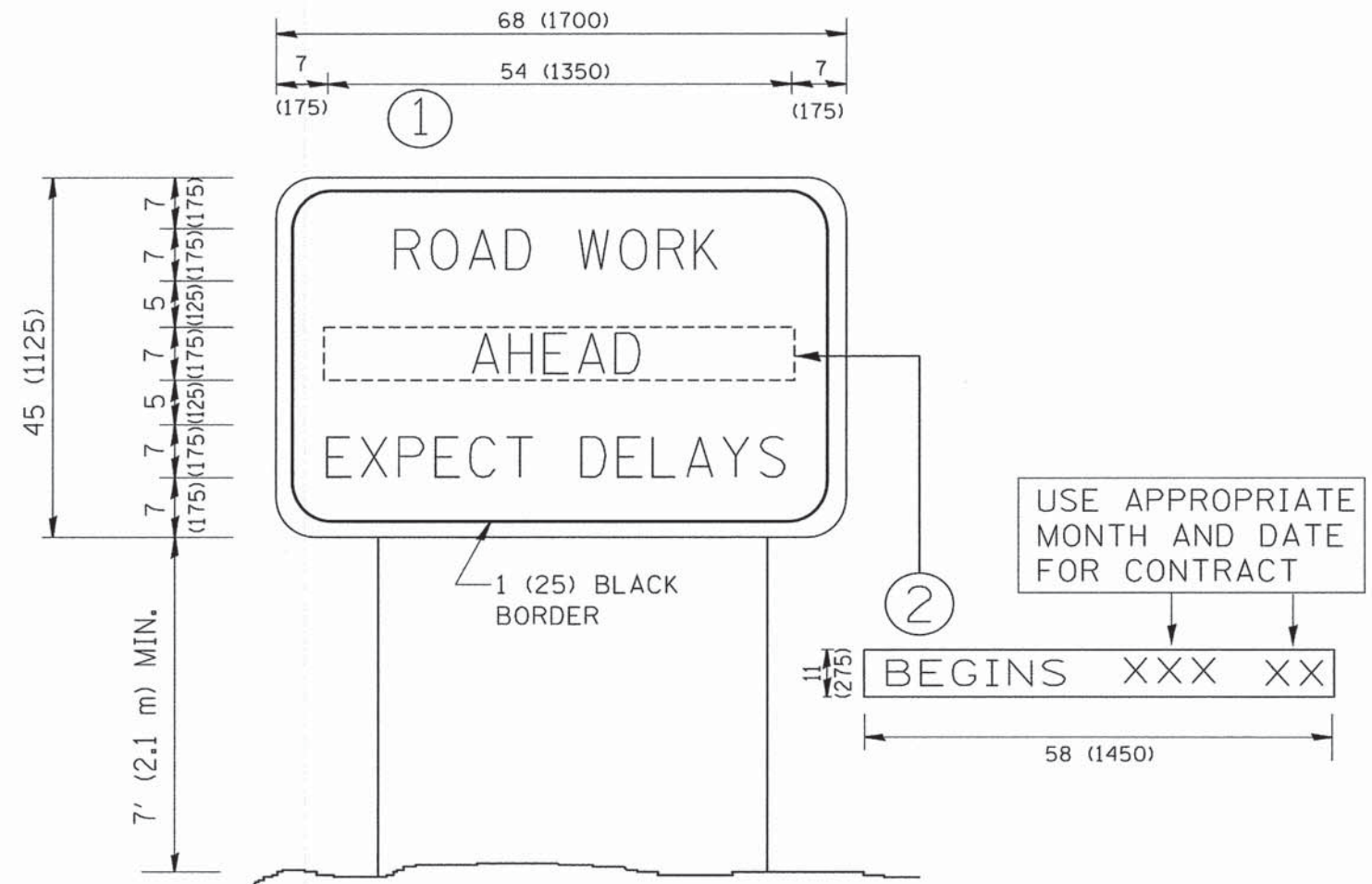
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3890	14-00105-00-RS	KANE	19	14
TC-16		CONTRACT NO. 61B35		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

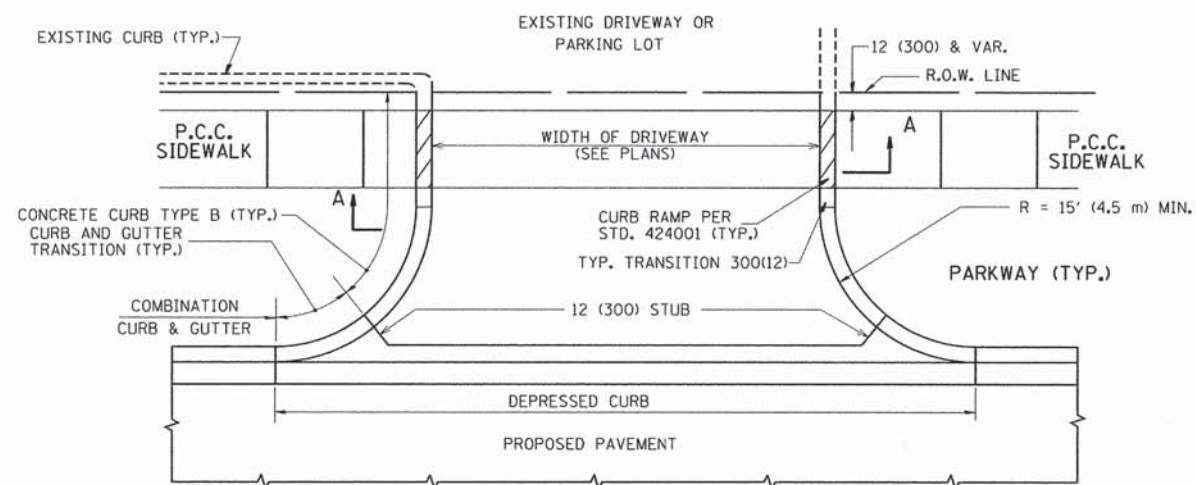
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

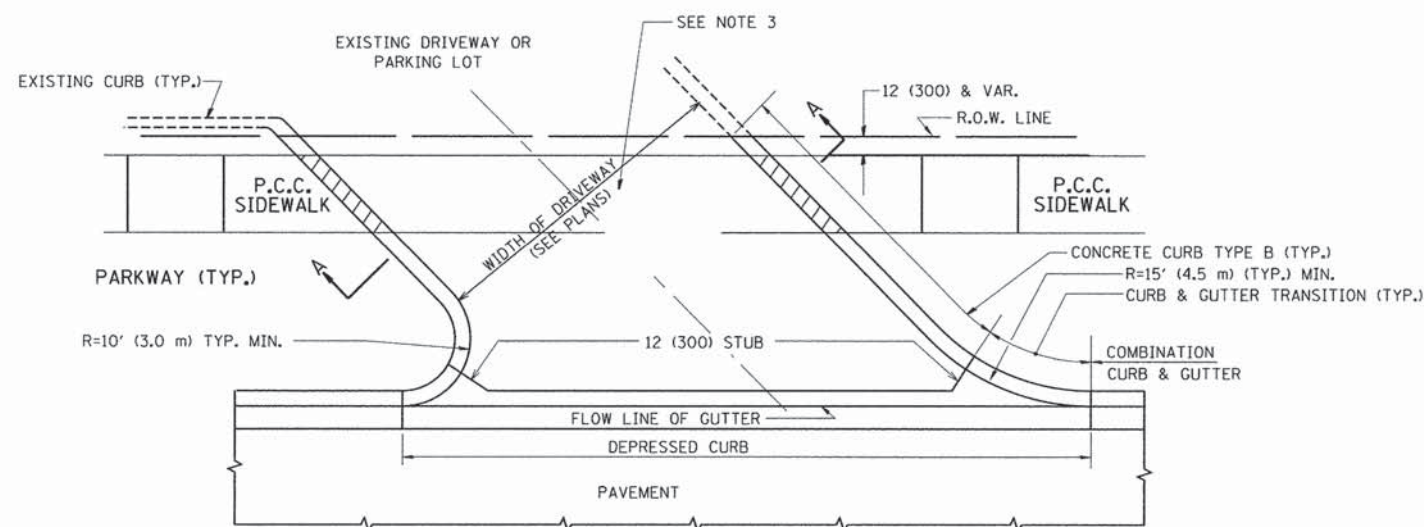
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3890	14-00105-00-RS	KANE	19	15
<b>TC-22</b>			<b>CONTRACT NO. 61B35</b>	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				

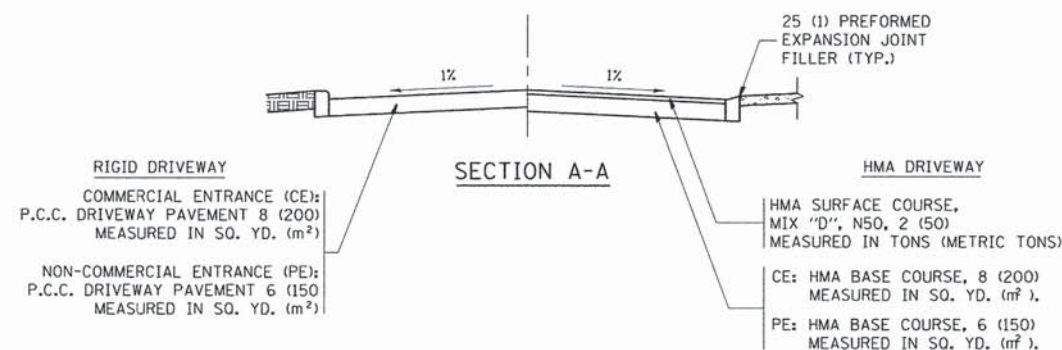




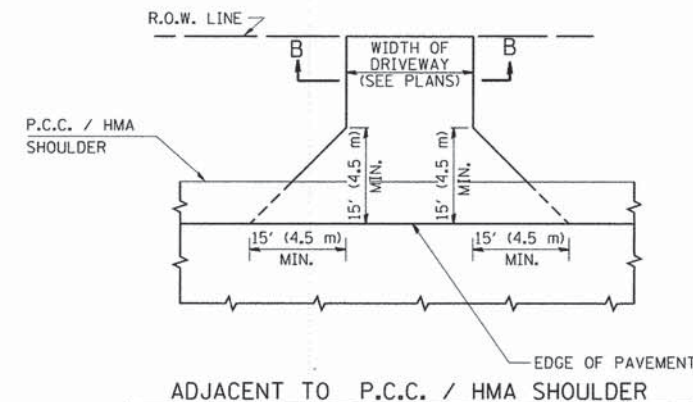
WITH CONCRETE CURB, TYPE B



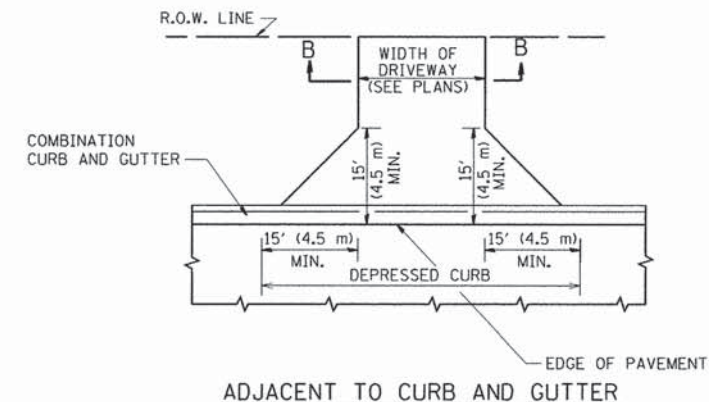
WITH CONCRETE CURB, TYPE B



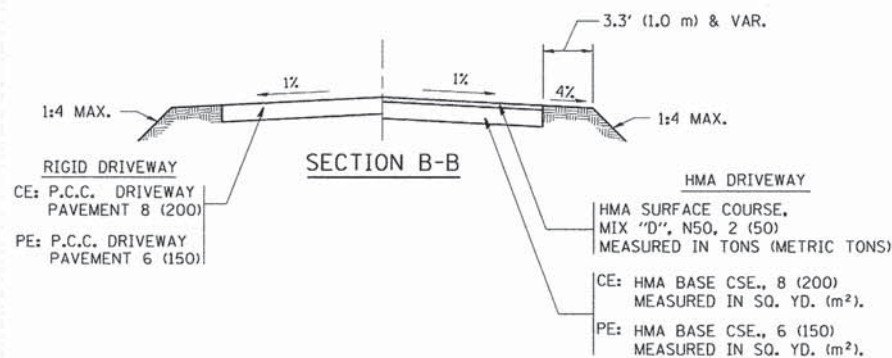
SECTION A-A



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m<sup>2</sup>)

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

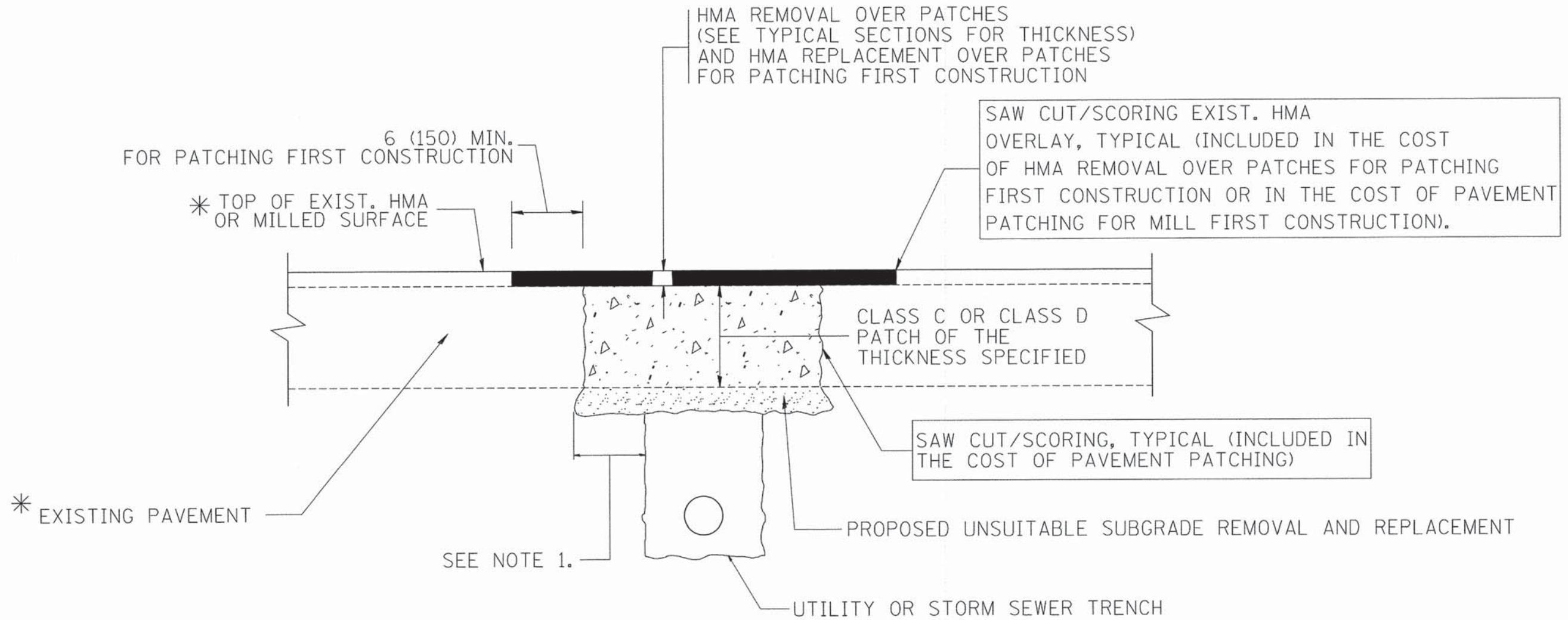
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		CHECKED -	REVISED - R. BORO 06-11-08
		DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3890	14-00105-00-RS	KANE	19	16
BD0156-07 (BD-01)			CONTRACT NO. 61B35	
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT				





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

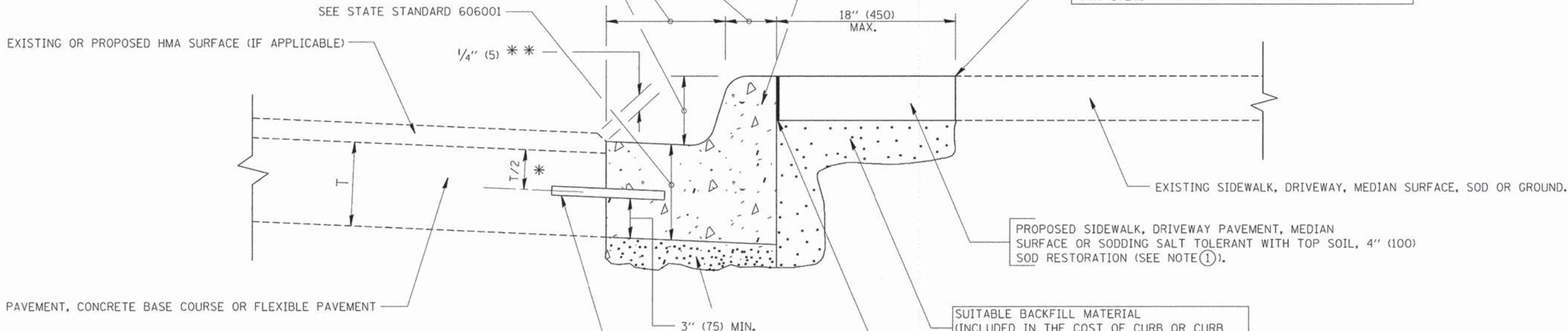
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	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.				



VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**

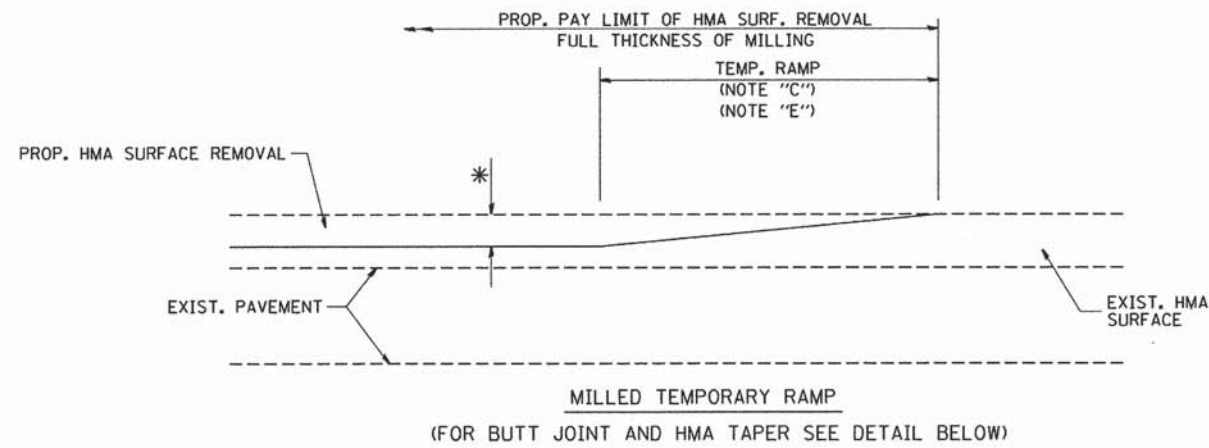
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

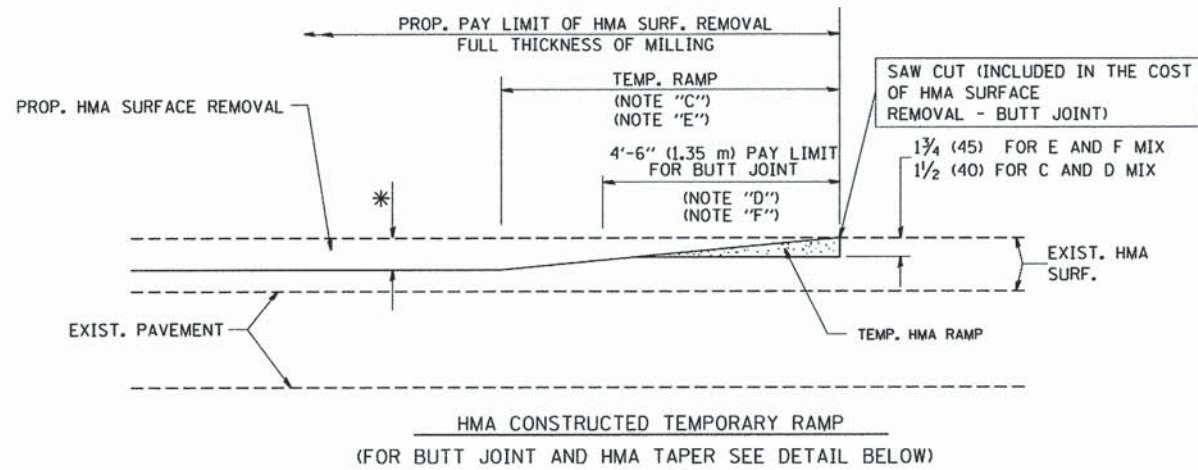
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PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01	<b>BD600-06 (BD-24)</b>			CONTRACT NO. 61B35				
PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT							
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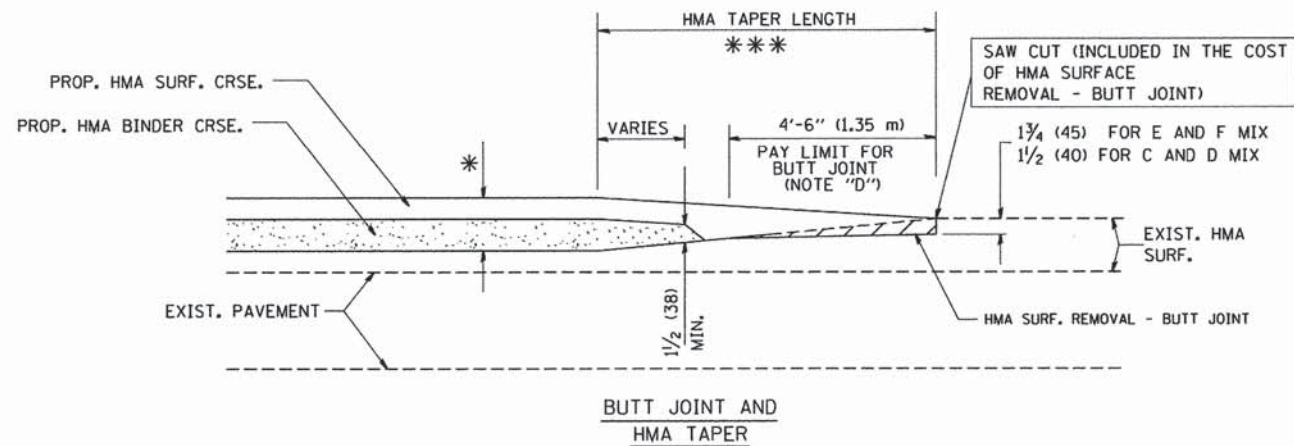




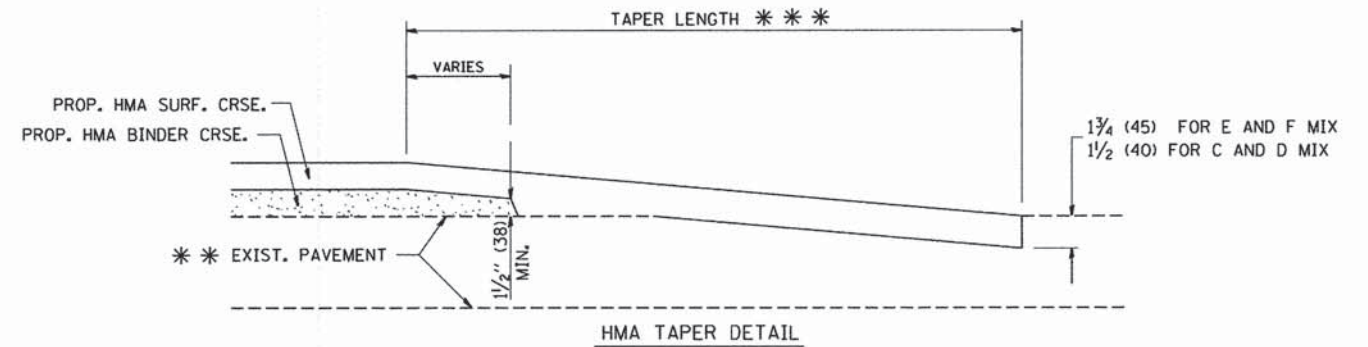
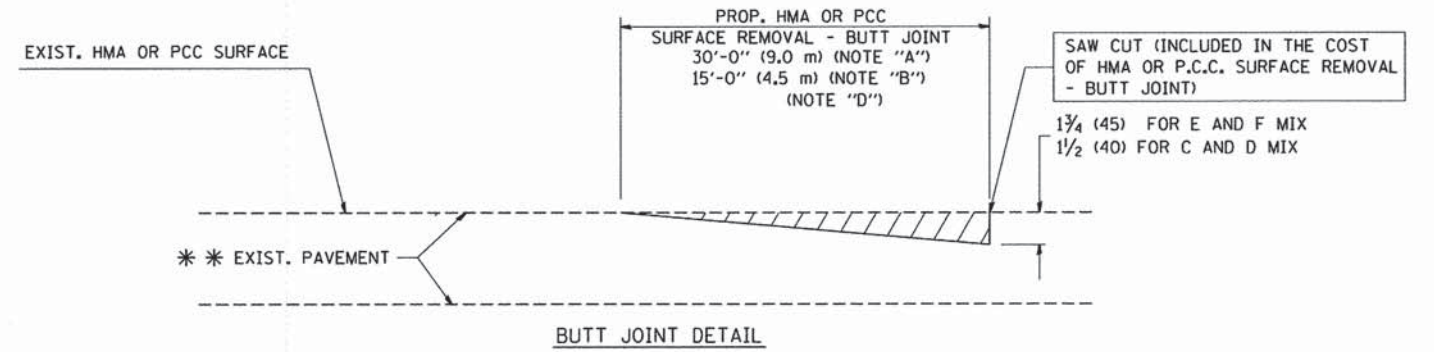
**OPTION 1**



**OPTION 2**  
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distotd\22x34\bd32.dgn	USER NAME = geglennob	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND**  
**HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 3890	SECTION 14-00105-00-RS	COUNTY KANE	TOTAL SHEETS 19	SHEET NO. 19
BD400-05 BD32			CONTRACT NO. 61B35	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				