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STATE STANDARDS

- 701006-05 OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
- 701101-04 OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
- 701401-09 LANE CLOSURE, FREEWAY/EXPRESSWAY
- 701402-10 LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER
- 701406-09 LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
- 701411-09 LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
- 701451-03 RAMP CLOSURE FREEWAY/EXPRESSWAY
- 701901-04 TRAFFIC CONTROL DEVICES
- 704001-07 TEMPORARY CONCRETE BARRIER
- 720011-01 METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
- 728001-01 TELESCOPING STEEL SIGN SUPPORT
- 729001-01 APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)

FILE NAME = D:\BR\Draw\Whiteside\64K48 Partial Clearing & Painting SN 098-0092, 0093 & 0094\DRAWING\48-shr-cover.dgn	USER NAME = dseadd	DESIGNED - _____	REVISED - 8-18-2014 DD	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 40,0000 / / in.	CHECKED - _____	REVISED - _____	REVISED - _____			88	095-38 & 195-38-1P	WHITESIDE	21	2
PLOT DATE = Thu Dec 10 09:27:35 2014	DATE - _____	REVISED - _____	REVISED - _____			SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____		CONTRACT NO. 64K48 ILLINOIS FED. AID PROJECT		

GENERAL NOTES

All Borrow/Waste/Use sites must be approved by the Department prior to removing any material from the project or initiating any earthmoving activities, including temporary stockpiling outside the limits of construction.

The Contractor shall be responsible for protecting utility property during construction operations as outlined in Article 107.31 of the Standard Specifications. A minimum of 48 hours advance notice is required for non-emergency work. The JULIE number is 800-892-0123. The following listed utilities located within the project limits or immediately adjacent to the project construction limits are members of JULIE:

ELECTRIC:

COMMONWEALTH EDISON COMPANY
ATTN: NORA FERNANDEZ (815) 490-2869
123 ENERGY AVENUE
ROCK ISLAND, IL 61109

TELEPHONE:

AT&T
ATTN: DAVID CREEN (309) 757-4707
2800 7TH STREET
MOLINE, IL 61265

COMMUNICATIONS:

G4S TECHNOLOGY, LLC
ATTN: CECIL KUHSE (630) 343-2155
565 WILLOWBROOK CENTER PARKWAY
WILLOWBROOK, IL 60527

LIGHTCORE

ATTN: JUSTIN FRENCH (636) 887-4755
1151 CENTURYTEL DRIVE, BLDG A
WENTZVILLE, MO 63385

WATER:

IL AMERICAN WATER COMPANY
ATTN: (815) 625-3196
304 SECOND AVENUE
STERLING, IL 61081

CATV:

COMCAST CABLE
ATTN: MICHAEL OWENS (815) 395-8977
4450 KISHWAUKEE STREET
ROCKFORD, IL 61109

SEWER:

CITY OF STERLING
ATTN: SCOTT SHUMARD (815) 632-6624
212 3RD AVENUE
STERLING, IL 61081

IDOT is not a member of JULIE. If you are near any overhead lighting, intersection lighting or traffic signals, contact the IDOT Traffic Office at 815/284-5469 at least 48 hours prior to work.

No Stream Permits have been procured for this project. None are required from the Regulatory Agencies as long as there is no debris falling or being placed into the stream. Any temporary fill in the stream or the compromising of the dike (if applicable) will not be allowed. If the contractor chooses to use alternate/modified construction method(s) from the aforementioned, they will be responsible for obtaining the proper permits. No relief or compensation will be given for any delays, working days charged, or calendar days expired as a result of the loss of time or delay during the re-submittal process to procure the necessary permit(s) due to the contractor choosing alternate/modified construction method(s). Absolutely no construction activities will take place without the proper permits being secured

A MINIMUM OF 2 AIR MONITORS WILL BE REQUIRED TO MONITOR ABRASIVE BLASTING OPERATIONS, SEE SPECIAL PROVISION FOR "CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES".

THE CONTRACTOR SHALL SEED ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS. SEEDING CLASS 2A SHALL BE USED. THIS WORK WILL BE INCLUDED IN THE CONTRACT UNIT PRICE PER LUMP SUM FOR CLEANING AND PAINTING STEEL BRIDGE NO. 1.

FERTILIZER SHALL BE APPLIED TO ALL DISTURBED AREAS AND INCORPORATED INTO THE SEEDBED PRIOR TO SEEDING OR PLACEMENT OF SOD AT THE RATE SPECIFIED IN SECTIONS 250 AND 252 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST OF CLEANING AND PAINTING STEEL BRIDGE NO. 1.

MULCH METHOD II SHALL BE APPLIED OVER ALL SEEDER AREAS. THIS SHALL BE INCLUDED IN THE COST OF THE CLEANING AND PAINTING STEEL BRIDGE NO. 1.

TEMPORARY IMPACT ATTENUATORS WILL BE MEASURED AS EACH FOR EACH ATTENUATOR SUPPLIED ON THE JOB AS SPECIFIED IN THE PLANS, AND SHALL INCLUDE THE COST OF RENTING/OWNING THE ATTENUATOR FOR THE TIME REQUIRED ON THE JOB PLUS HAULING TO AND FROM THE PROJECT SITE, AS WELL AS ONE PLACEMENT AND REMOVAL FROM THE ROADWAY. THIS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR IMPACT ATTENUATORS, TEMPORARY OF THE TYPE SPECIFIED.

THIS WORK SHALL BE DONE IN ACCORDANCE WITH SECTION 704 OF THE STANDARD SPECIFICATIONS. TEMPORARY CONCRETE BARRIER WILL BE MEASURED IN FEET ALONG THE CENTERLINE OF THE BARRIER AND SHALL INCLUDE THE COST OF RENTING/OWNING THE BARRIER FOR THE TIME REQUIRED ON THE JOB PLUS HAULING TO AND FROM THE PROJECT SITE, AS WELL AS ONE PLACEMENT AND REMOVAL FROM THE ROADWAY IN ACCORDANCE WITH SECTION 704 OF THE STANDARD AND SPECIFICATION. THIS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR TEMPORARY CONCRETE BARRIER.

THE SSPC QP1 & QP2 CONTRACT CERTIFICATIONS WILL BE REQUIRED FOR THIS CONTRACT.

HAZARDS BEHIND TEMPORARY CONCRETE BARRIER WALL:
NO MATERIALS, EQUIPMENT, VEHICLES OR OTHER HAZARDS SHALL BE WITHIN 3'-6" OF THE TEMPORARY CONCRETE BARRIER WALL, MEASURED FROM THE CLOSEST POINT OF THE BARRIER WALL PERPENDICULAR TO THE HAZARD. EQUIPMENT THAT IS ACTIVELY WORKING WITHIN 3'-6" OF THE BARRIER WALL MAY BE ALLOWED FOR A SHORT DURATION, IF APPROVED BY THE ENGINEER, REQUIRED FOR THE WORK ACTIVITY, AND THERE IS NOT ANOTHER METHOD TO COMOPLETE THE WORK.

FILE NAME : D:\BR\Draws\Whiteside\64K48 Partial Cleaning & Painting SH 090-0092, 0093 & 0094\CD\02\DRAWING\64K48-sh-cov-rec.dgn	USER NAME : dmsdd	DESIGNED - _____	REVISED - _____	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES					F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE * 40.0000 * / in.	CHECKED - _____	REVISED - _____	REVISED - _____		88	(195-38 & 195-38-1)P	WHITESIDE	21	3					
PLOT DATE * Thu Dec 18 09:28:15 2014	DATE - _____	REVISED - _____	REVISED - _____		SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____					ILLINOIS FED. AID PROJECT				
													CONTRACT NO. 64K48	

SUMMARY OF QUANTITIES

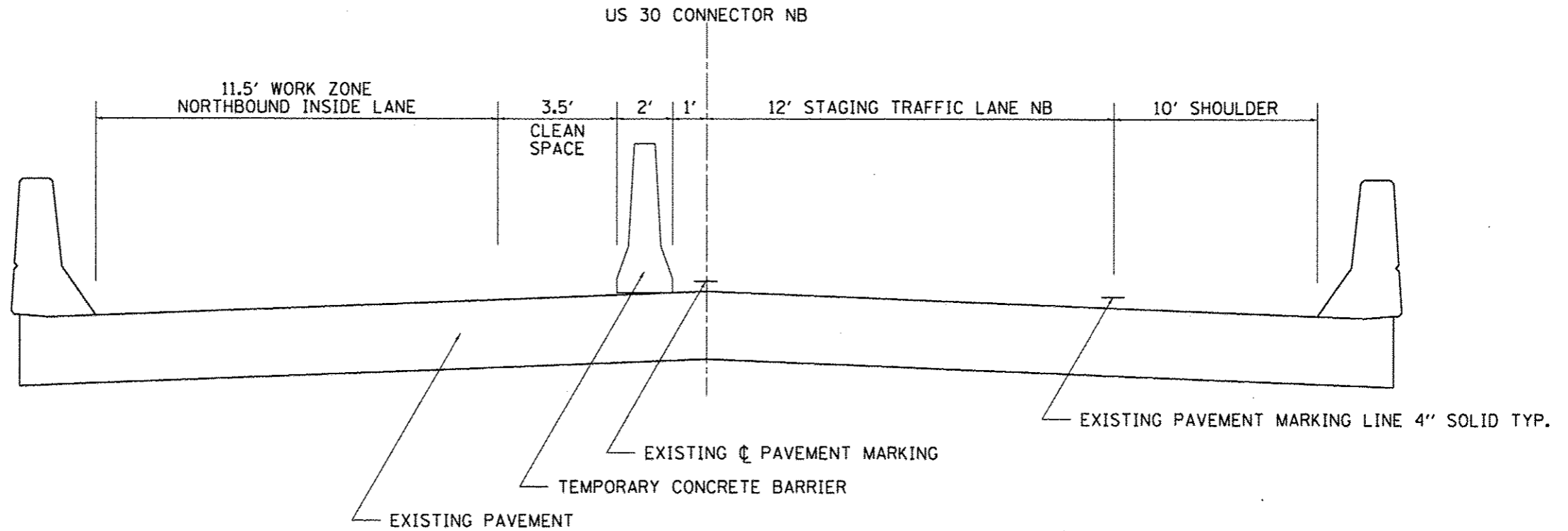
0014
100%
STATE

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
67100100	MOBILIZATION	L SUM	1
70100207	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	EACH	1
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2
70300220	TEMPORARY PAVEMENT MARKING LINE 4"	FOOT	2,522
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	841
70400100	TEMPORARY CONCRETE BARRIER	FOOT	595.49
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	1
Z0007101	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 1	L SUM	1
Z0007102	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 2	L SUM	1
Z0007103	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 3	L SUM	1
Z0010501	CLEANING AND PAINTING STEEL BRIDGE NO. 1	L SUM	1
Z0010502	CLEANING AND PAINTING STEEL BRIDGE NO. 2	L SUM	1
Z0010503	CLEANING AND PAINTING STEEL BRIDGE NO. 3	L SUM	1

17

STAGING TYPICAL

STAGING NORTHBOUND (SN 098-0093 OVER ELKHORN CREEK)



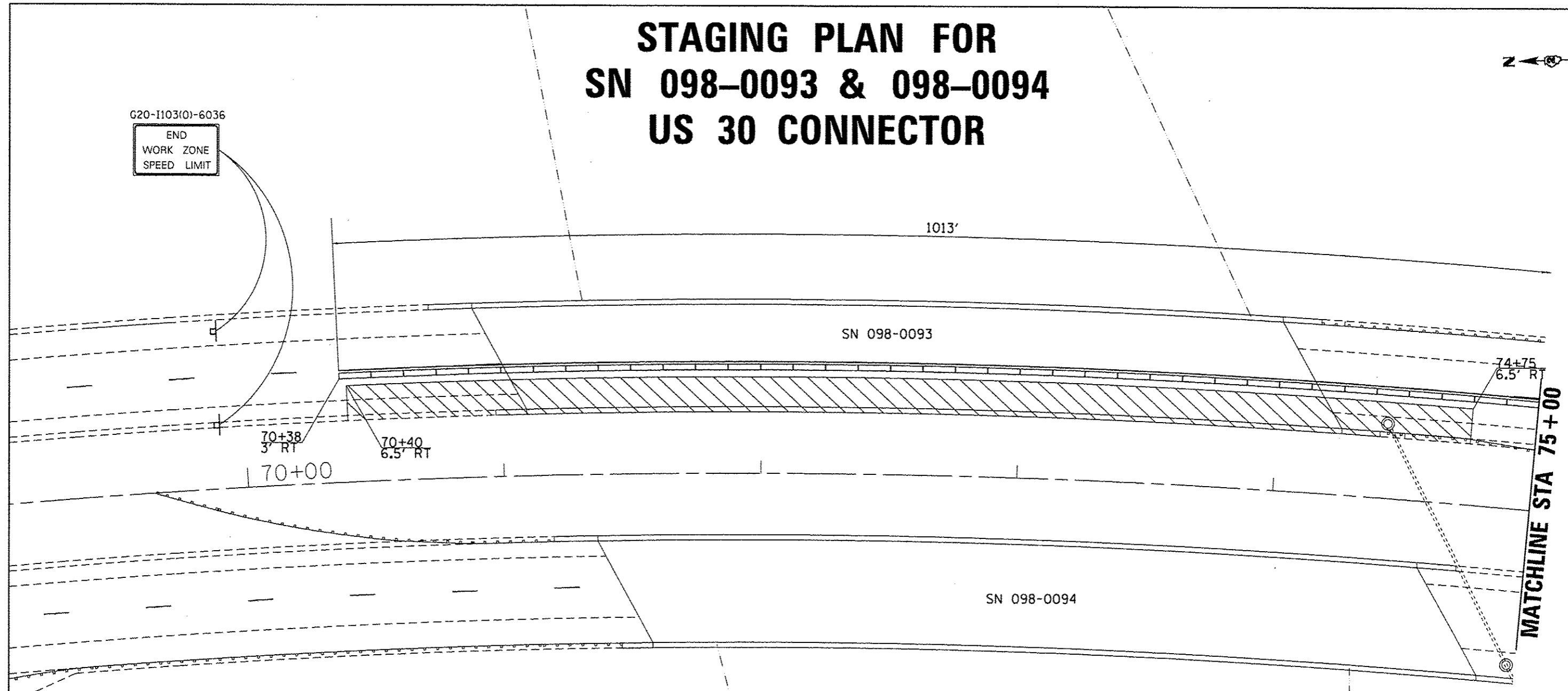
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Q:\BR\Draw\Whiteside\64K48 Partial Clearing & Pointing SN 098-0092, 0093 & 0094\CA	DRAWN: K48-shi-cover.dgn	CHECKED -	REVISED -			88	(195-3B & 195-3B-11P)	WHITESIDE	21	5	
Default	PLOT SCALE : 48.0000' / in.	DATE -	REVISED -			CONTRACT NO. 64K48		ILLINOIS FED. AID PROJECT			
	PLDT DATE : Thu Dec 18 09:20:32 2014					SCALE: _____	SHEET _____ OF _____ SHEETS	STA. _____ TO STA. _____			

STAGING PLAN FOR SN 098-0093 & 098-0094 US 30 CONNECTOR



G20-1103(0)-6036

END
WORK ZONE
SPEED LIMIT



SYMBOLS

- Arrow board
- Work area
- Sign
- Direction indicator barricade with steady burn monodirectional light
- Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Temporary concrete barrier
- Monodirectional barrier wall/guardrail marker
- Impact attenuator

NOTES

ALL WORK ON STRUCTURES 098-0093 AND 098-0094 SHALL BE COMPLETED FROM THIS SET UP.

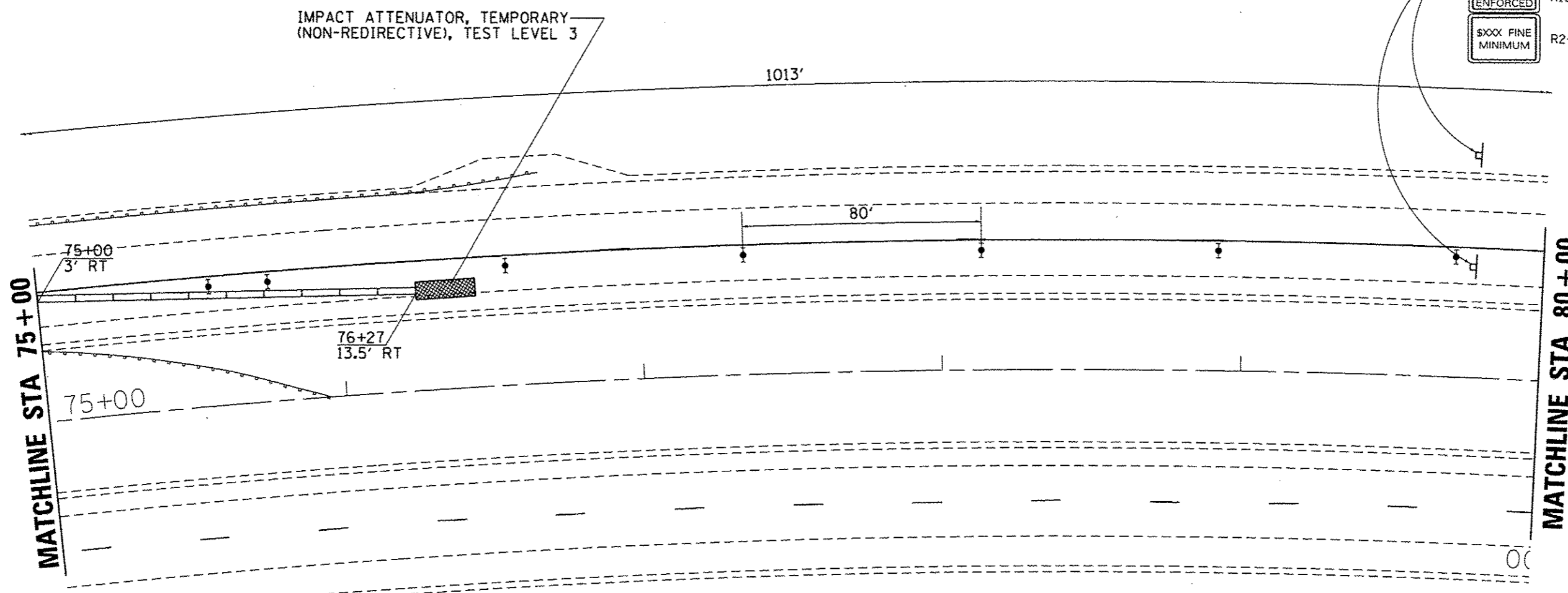
REFER TO STANDARDS 701402, 701411 AND 704001 FOR ADDITIONAL TRAFFIC CONTROL AND INFORMATION NOT SHOWN.

BARRIER WALL OFFSETS ARE TO THE CONSTRUCTION SIDE OF THE BARRIER WALL.

OFFSETS SHALL BE MEASURED FROM THE EXISTING NORTHBOUND CENTERLINE STRIPE

FILE NAME :	USER NAME : dswadd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING PLAN FOR SN 098-0093 & 098-0094 US 30 CONNECTOR	F.A.I. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT SCALE = 48.0000' / in.	DATE -	REVISED -		SCALE: _____					CONTRACT NO. 64K48
	PLOT DATE = Thu Dec 10 09:20:39 2014				SHEET _____ OF _____ SHEETS					ILLINOIS FED. AID PROJECT

STAGING PLAN FOR SN 098-0093 & 098-0094 US 30 CONNECTOR



WORK ZONE	W2-1115(O)-3618
SPEED LIMIT	R2-1-3648
55	
PHOTO ENFORCED	R10-1108p-3618
XXXX FINE MINIMUM	R2-1106p-3618



SYMBOLS

- Arrow board
- Work area
- Sign
- Direction indicator barricade with steady burn monodirectional light
- Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Temporary concrete barrier
- Monodirectional barrier wall/guardrail marker
- Impact attenuator

NOTES

ALL WORK ON STRUCTURES 098-0093 AND 098-0094 SHALL BE COMPLETED FROM THIS SET UP.

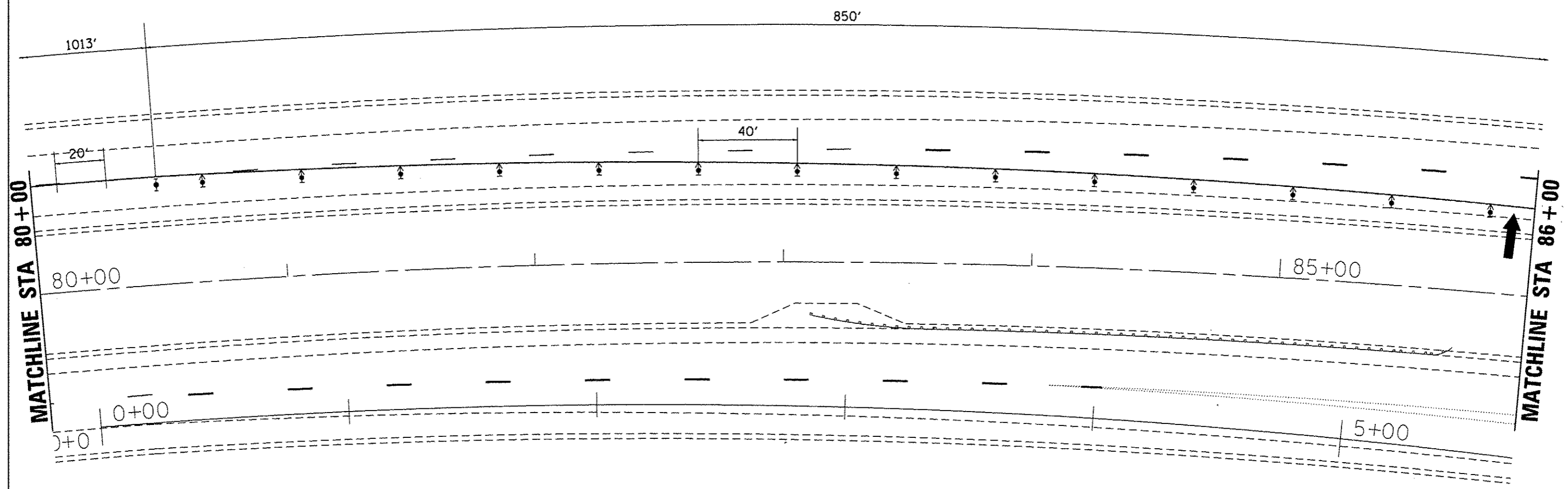
REFER TO STANDARDS 701402, 701411 AND 704001 FOR ADDITIONAL TRAFFIC CONTROL AND INFORMATION NOT SHOWN.

BARRIER WALL OFFSETS ARE TO THE CONSTRUCTION SIDE OF THE BARRIER WALL.

OFFSETS SHALL BE MEASURED FROM THE EXISTING NORTHBOUND CENTERLINE STRIPE

FILE NAME :	USER NAME : dossed	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING PLAN FOR SN 098-0093 & 098-0094 US 30 CONNECTOR	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default						88	(195-3B & 195-3B-1P)	WHITESIDE	21	7
									CONTRACT NO. 64K4B	
									ILLINOIS FED. AID PROJECT	

STAGING PLAN FOR SN 098-0093 & 098-0094 US 30 CONNECTOR



SYMBOLS

- Arrow board
- Work area
- Sign
- Direction indicator barricade with steady burn monodirectional light
- Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Temporary concrete barrier
- Monodirectional barrier wall/guardrail marker
- Impact attenuator

NOTES

ALL WORK ON STRUCTURES 098-0093 AND 098-0094 SHALL BE COMPLETED FROM THIS SET UP.

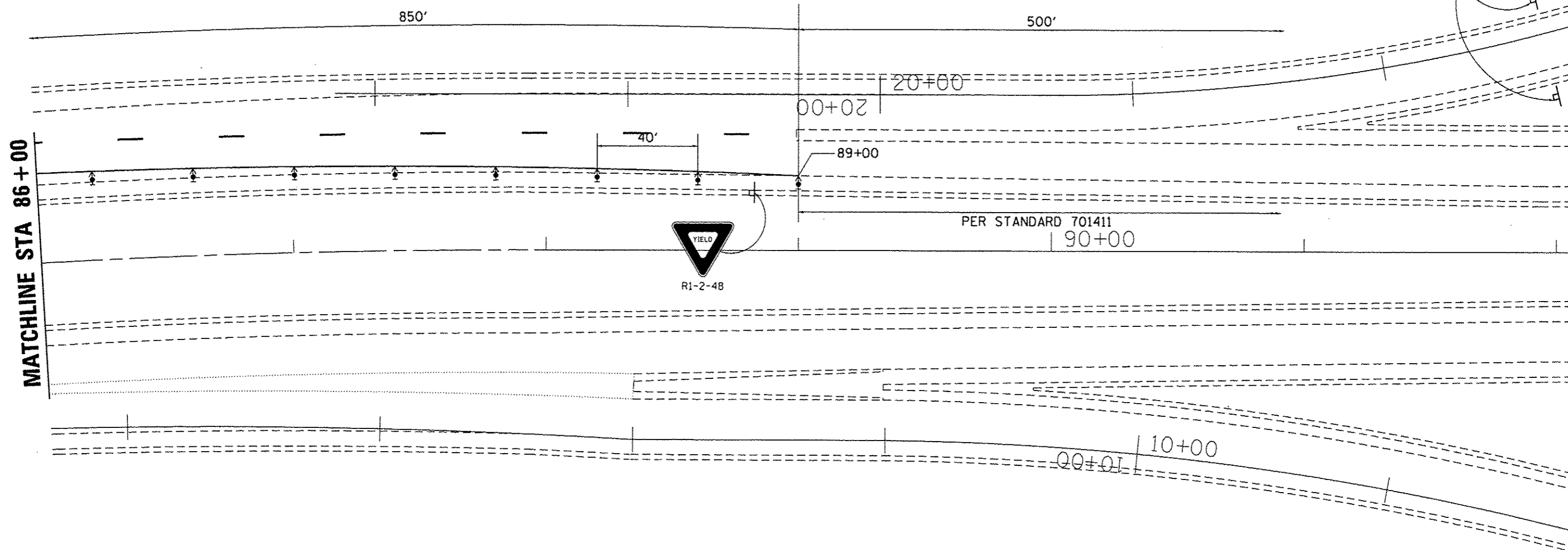
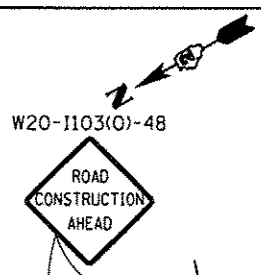
REFER TO STANDARDS 701402, 701411 AND 704001 FOR ADDITIONAL TRAFFIC CONTROL AND INFORMATION NOT SHOWN.

BARRIER WALL OFFSETS ARE TO THE CONSTRUCTION SIDE OF THE BARRIER WALL.

OFFSETS SHALL BE MEASURED FROM THE EXISTING NORTHBOUND CENTERLINE STRIPE

FILE NAME =	USER NAME = dowedd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING PLAN FOR SN 098-0093 & 098-0094 US 30 CONNECTOR	F.A.I. RTE. 88	SECTION (195-3B & 195-3B-1UP)	COUNTY WHITESIDE	TOTAL SHEETS 21	SHEET NO. 8
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	PLOT DATE = Thu Dec 18 09:20:51 2014	DATE -	REVISED -							

STAGING PLAN FOR SN 098-0093 & 098-0094 US 30 CONNECTOR



MATCHLINE STA 86+00

SYMBOLS

- Arrow board
- Work area
- Sign
- Direction indicator barricade with steady burn monodirectional light
- Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Temporary concrete barrier
- Monodirectional barrier wall/guardrail marker
- Impact attenuator

NOTES

ALL WORK ON STRUCTURES 098-0093 AND 098-0094 SHALL BE COMPLETED FROM THIS SET UP.

REFER TO STANDARDS 701402, 701411 AND 704001 FOR ADDITIONAL TRAFFIC CONTROL AND INFORMATION NOT SHOWN.

BARRIER WALL OFFSETS ARE TO THE CONSTRUCTION SIDE OF THE BARRIER WALL.

OFFSETS SHALL BE MEASURED FROM THE EXISTING NORTHBOUND CENTERLINE STRIPE

FILE NAME =	USER NAME = dosadd	DESIGNED -	REVISED -
Default			
		CHECKED -	REVISED -
		DATE -	REVISED -

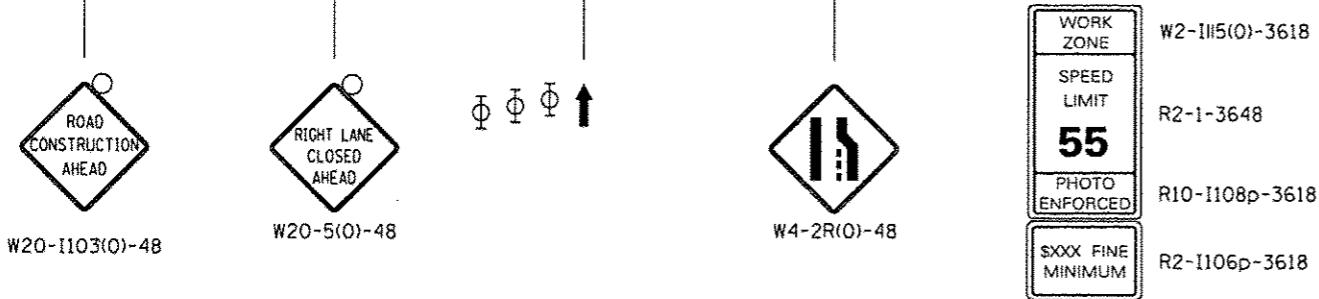
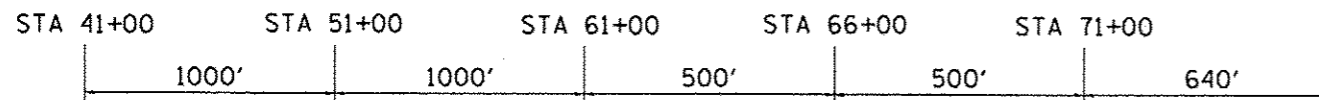
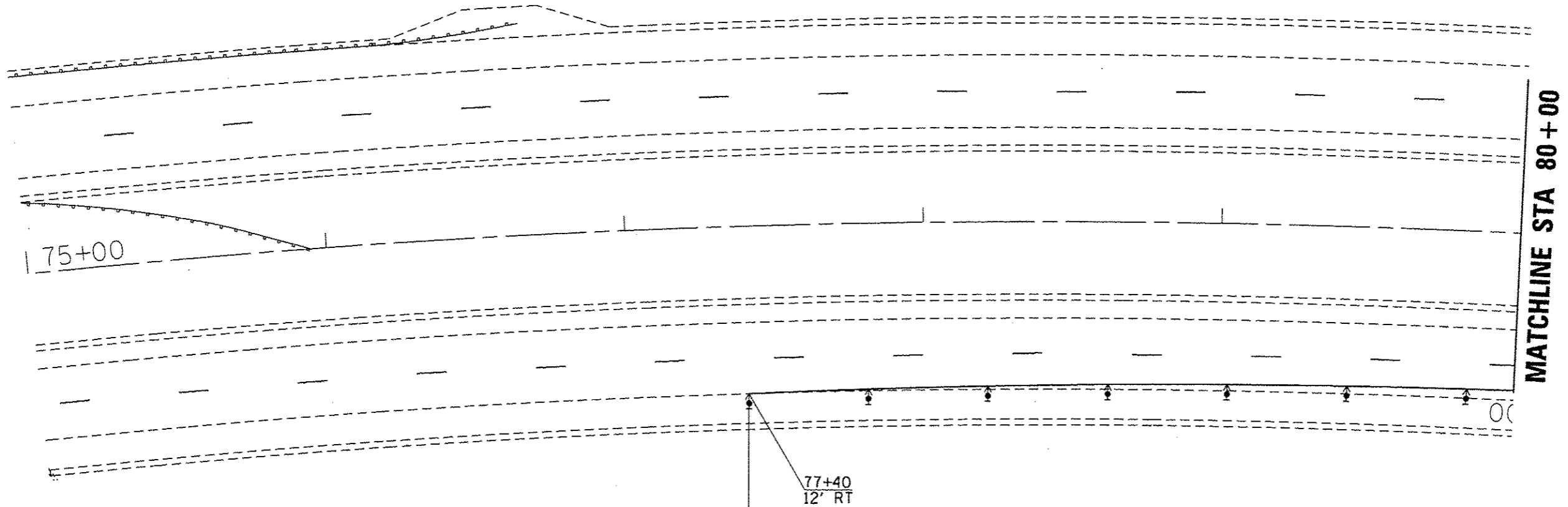
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGING PLAN FOR SN 098-0093 & 098-0094
US 30 CONNECTOR**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	(195-3B & 195-3B-11P)	WHITESIDE	21	9
CONTRACT NO. 64K48			ILLINOIS FED. AID PROJECT	

RAMP CLOSURE PLAN FOR SN 098-0092



SYMBOLS

- Arrow board
- Drum with steady burning light
- Sign
- Direction Indicator barricade with steady burn monodirectional light
- Type III barricade

NOTES

OFFSETS SHALL BE MEASURED FROM THE EXISTING SOUTHBOUND CENTERLINE STRIPE

THE SIGNING AND TAPER SHOWN SHALL BE PAID FOR ACCORDING TO THE TRAFFIC CONTROL AND PROTECTION STANDARD 701401.

FILE NAME :	USER NAME : dowsdd	DESIGNED -	REVISED -
D:\BR\Draws\Whiteside\64K48 Partial Closing & Pointing SN 098-0092, 0093 & 0094\CD\DRAWN\64K48-shr-cover.dgn		REVISIONS	
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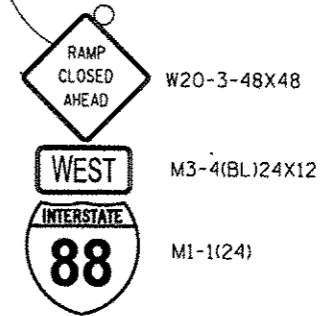
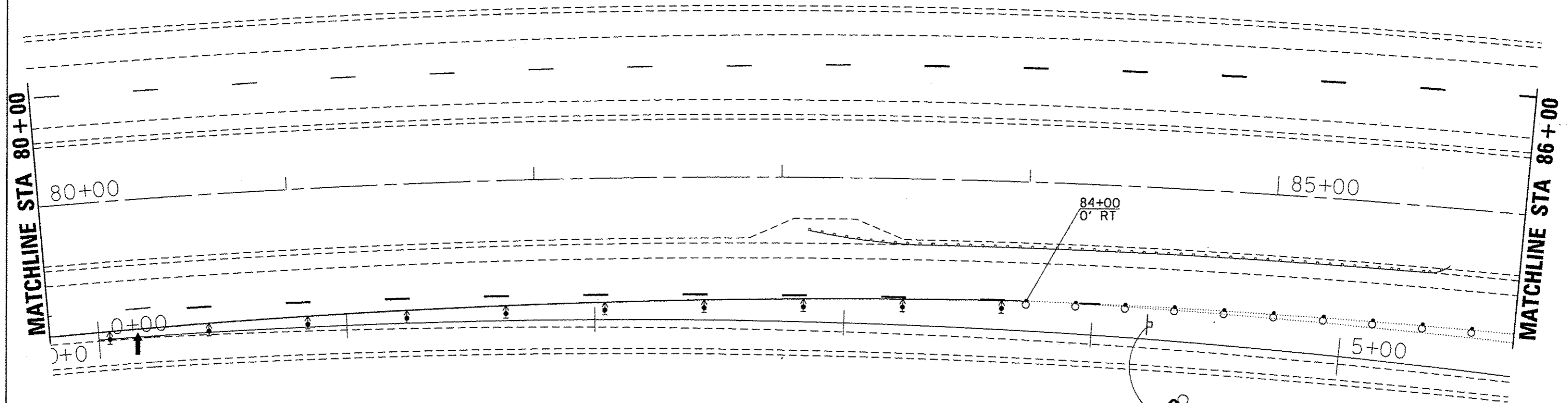
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RAMP CLOSURE PLAN
FOR SN 098-0092**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	(195-3B & 195-3B-11P)	WHITESIDE	21	10
CONTRACT NO. 64K48			ILLINOIS FED. AID PROJECT	

RAMP CLOSURE PLAN FOR SN 098-0092



INSTALL CLOSED SIGN ON
OVERHEAD SIGN

NOTES

OFFSETS SHALL BE MEASURED FROM THE EXISTING SOUTHBOUND CENTERLINE STRIPE

THE SIGNING AND TAPER SHOWN SHALL BE PAID FOR ACCORDING TO THE TRAFFIC CONTROL AND PROTECTION STANDARD 701401.

SYMBOLS

- ↑ Arrow board
- Drum with steady burning light
- ⊥ Sign
- ⬆ Direction Indicator barricade with steady burn monodirectional light
- ⌌ Type III barricade

FILE NAME =	USER NAME = dswdd	DESIGNED -	REVISED -
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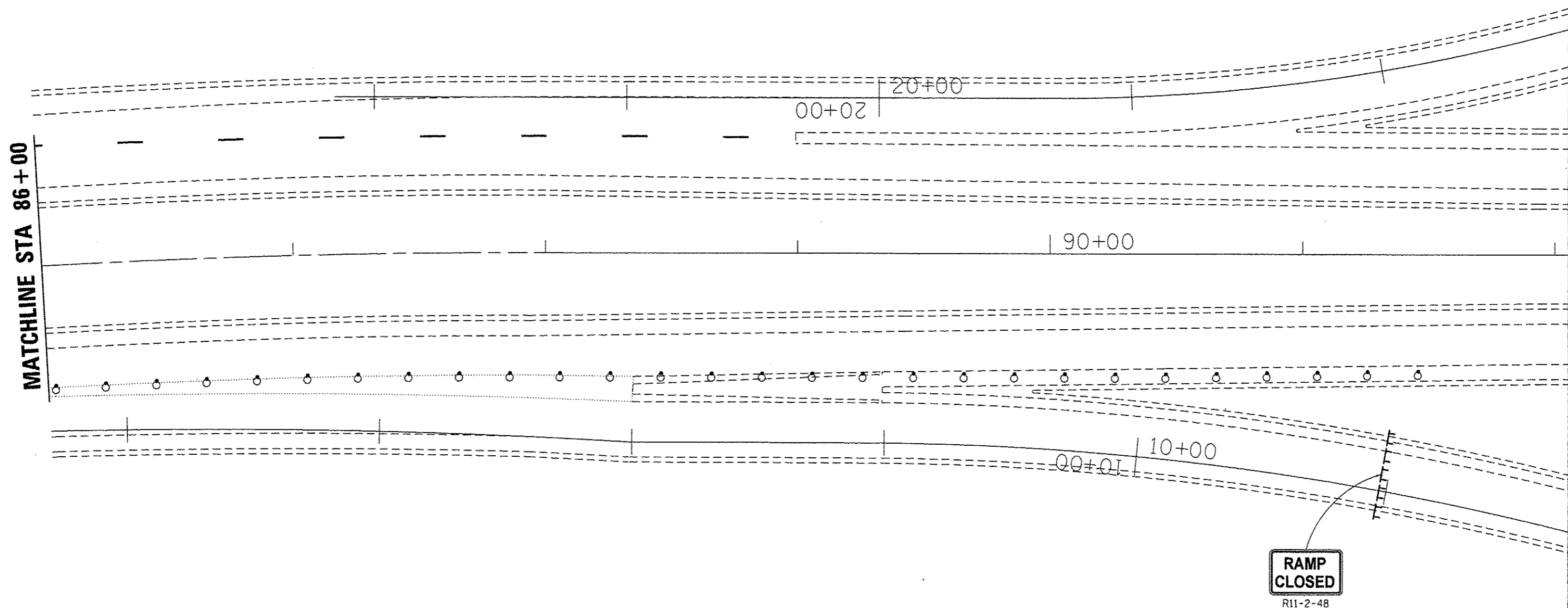
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RAMP CLOSURE PLAN
FOR SN 098-0092**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	(195-3B & 195-3B-1)P	WHITESIDE	21	11
CONTRACT NO. 64K48				
ILLINOIS FED. AID PROJECT				

RAMP CLOSURE PLAN FOR SN 098-0092



**RAMP
CLOSED**

R11-2-48

NOTES

OFFSETS SHALL BE MEASURED FROM THE EXISTING SOUTHBOUND CENTERLINE STRIPE

THE SIGNING AND TAPER SHOWN SHALL BE PAID FOR ACCORDING TO THE TRAFFIC CONTROL AND PROTECTION STANDARD 701401.

SYMBOLS

- ↑ Arrow board
- Drum with steady burning light
- ⊥ Sign
- ⬆ Direction Indicator barricade with steady burn monodirectional light
- ⌈ Type III barricade

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RAMP CLOSURE PLAN
FOR SN 098-0092**

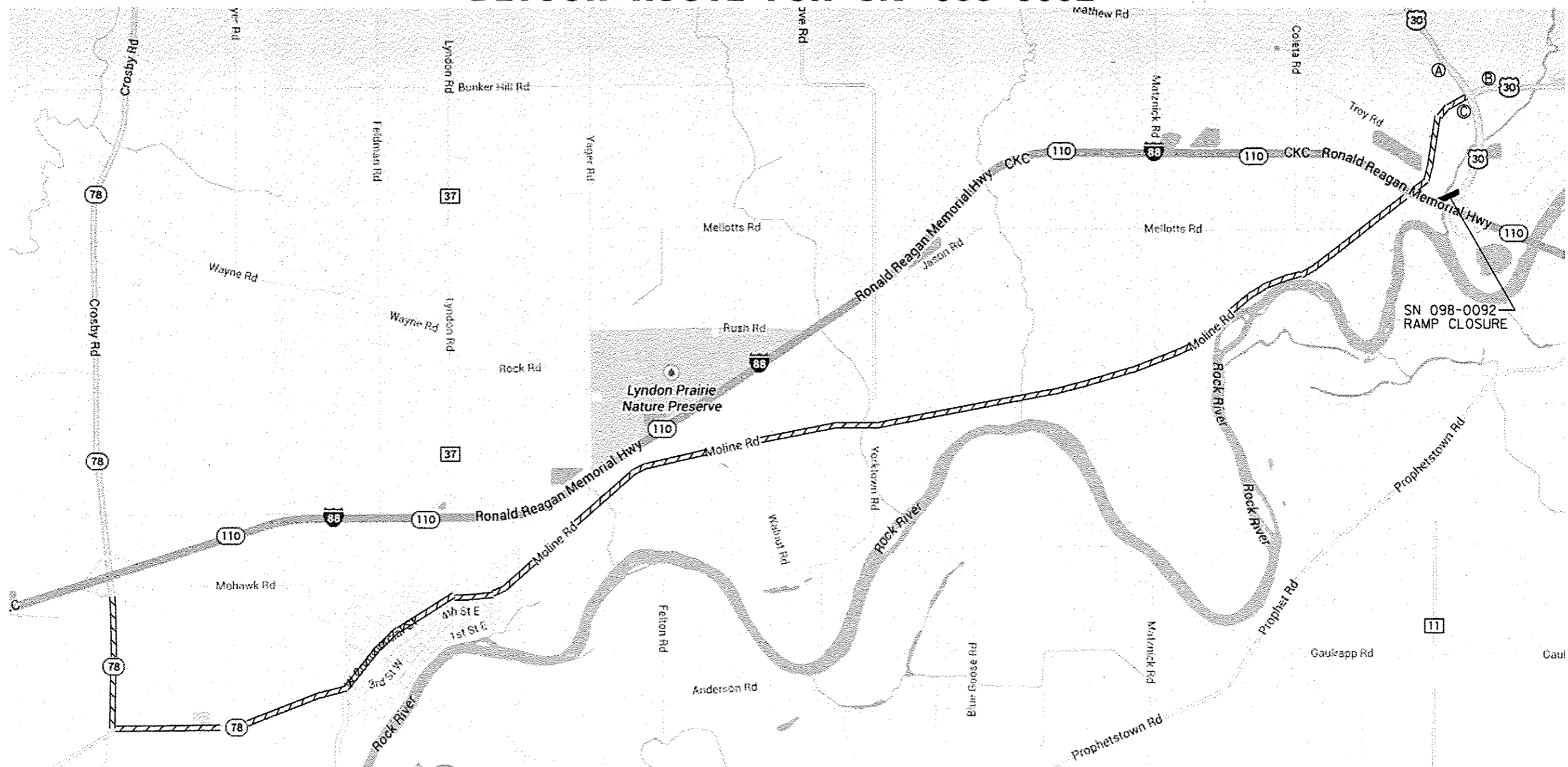
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	(195-3B & 195-3B-1P)	WHITESIDE	21	12
CONTRACT NO. 64K48				

FILE NAME :	USER NAME : dswdd	DESIGNED -	REVISED -
D:\BRY\Draws\Whiteside\64K48 Partial Clearing & Painting SN 098-0092, 0093 & 0094\CAD\DRAWING\64K48-111-over.dgn		CHECKED -	REVISED -
PLOT SCALE = 48.0000 "/in.		DATE -	REVISED -
PLOT DATE = Thu Dec 18 09:29:27 2014			

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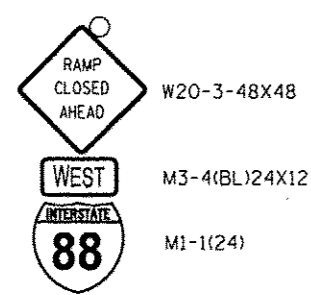
ILLINOIS FED. AID PROJECT

DETOUR ROUTE FOR SN 098-0092

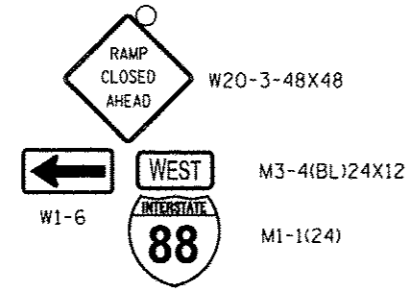


SN 098-0092
RAMP CLOSURE

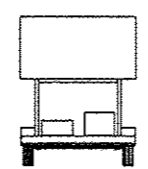
Ⓐ STA 46+00



Ⓑ



Ⓒ PORTABLE CHANGEABLE MESSAGE SIGN



RAMP TO I-88 WB CLOSED / FOLLOW MARKED ROUTE / USE MOLINE ROAD

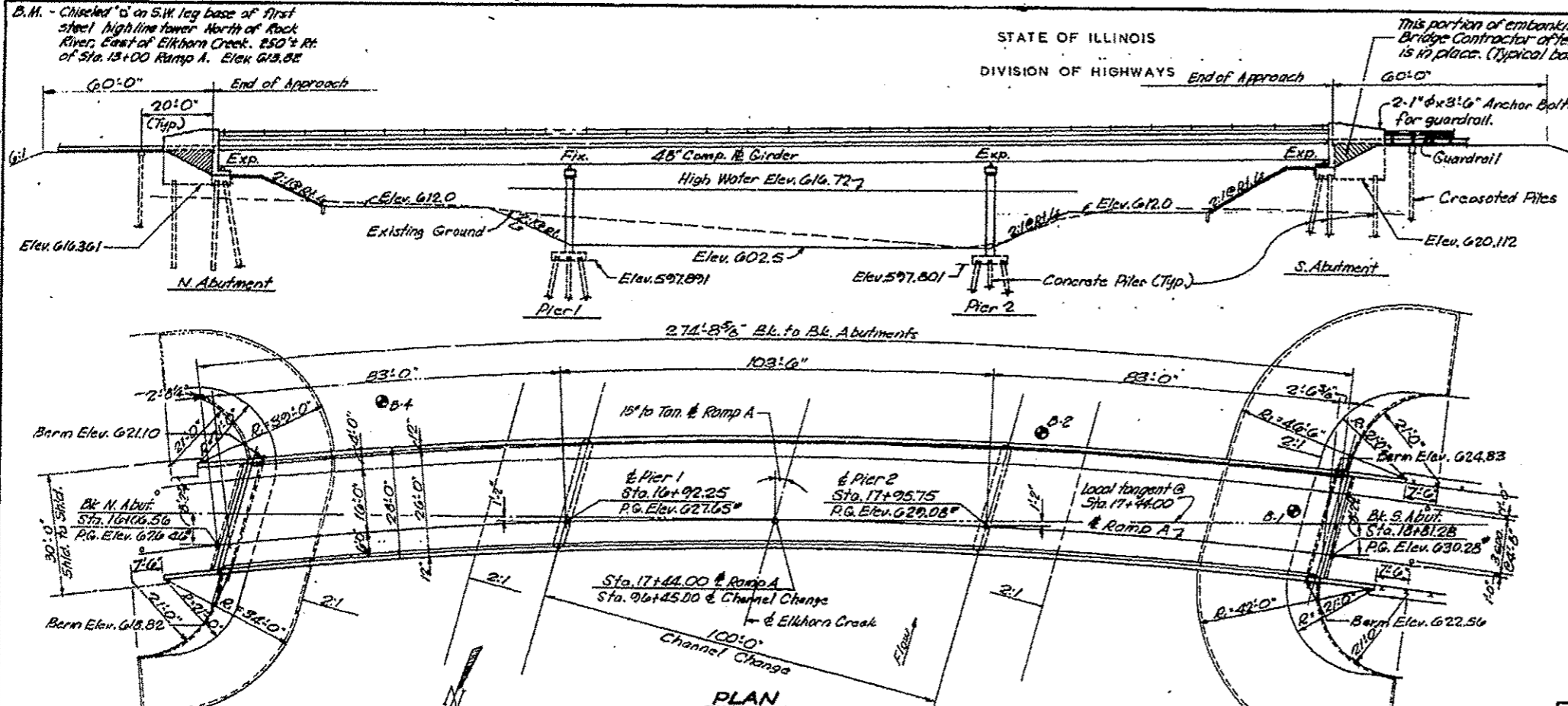
SYMBOLS

- DETOUR ROUTE
- RAMP CLOSURE

FILE NAME :	USER NAME : dowsdd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETOUR ROUTE FOR SN 098-0092	F.A.I. RTE. 88	SECTION (195-3B & 195-3B-1)P	COUNTY WHITESIDE	TOTAL SHEETS 21	SHEET NO. 13
Default	Plot Scale = 40,0000' / in.	CHECKED -	REVISED -			SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____		CONTRACT NO. 64K48 ILLINOIS FED. AID PROJECT		

FOR INFORMATION ONLY

08-0592



ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
F.A. 403	195-3	WHITESIDE	589	163
FED. ROAD DIST. NO. 7 ILLINOIS			F.A. PROJ.	

GENERAL NOTES

All reinforcement bars shall be lapped 24 diameters unless otherwise shown.
 Fasteners shall be high strength bolts, 5/8"; open holes 5/8" φ, unless otherwise noted.
 Calculated weight of Structural Steel = 229,300 lbs.
 The basic lead silico chromate paint system shall be used for shop and field painting of Structural Steel.
 Field welding of construction accessories will not be permitted to the bottom flange of girders nor to the top flange for a distance equal to one fourth the span length each way from the pier supports. Field welding in other areas will be permitted only when approved by the Engineer.
 Anchor bolts shall be set before bolting cross frames over supports. Slope wall shall be reinforced with welded wire fabric, 6"x6" mesh, weighing 50 lbs. per sq. ft.
 The Contractor shall drive one concrete test pile at each abutment and pier in a permanent location as directed by the Engineer before ordering the remainder of piles.
 Concrete piles at abutments shall be driven in holes prepared through the embankment in accordance with Article 513.09(c) of the Standard Specifications.
 The concrete rail section above the mandatory construction joint at the top of the slab shall be constructed of Class X Concrete, except the aggregates shall conform to requirements of Handrail Concrete.
 Protective Coat shall not be applied to surfaces to which Coal Tar Interlayer Protective Coat is applied.
 Layout of slope walls may be varied in the field to suit ground conditions as directed by the Engineer.
 The embankment configuration shown shall be the minimum embankment that must be constructed prior to construction of abutments.

BILL OF MATERIAL

Item	Unit	Super.	Sub.	Total
Structure Excavation	Cu. Yds.	---	140	140
Class X Concrete	Cu. Yds.	240.3	277.6	477.9
Structural Steel	Lump Sum	1	---	1
Reinforcement Bars	Lbs.	58854	16885	77287
Concrete Piles	Lin. Ft.	---	1360	1360
Test Piles, Concrete	Eq.	---	4	4
Creosoted Timber Piles (Railroad)	Lin. Ft.	---	196	196
Slope Wall 6"	Sq. Yds.	---	773	773
Bit Conc. Surf Course, Class I	Tons	42	---	42
Coal Tar Inter. Prot. Coat	Sq. Yds.	73.5	---	73.5
Aluminum Rolling	Lin. Ft.	543	---	543
Preformed J. Sealer	Lin. Ft.	30	---	30
Neoprene Exp. Joint (1/2")	Lin. Ft.	23	---	23
Steel Splice Connectors	Eq.	1944	---	1944
Protective Coat	Sq. Yds.	230	---	230
Name Plate	Eq.	1	---	1

DESIGN STRESSES

$f_c = 1200$ psi Deck Slab
 $f_c = 1400$ psi Curb, Parapet, & Substructure
 $f_c = 75$ psi Footings
 $n = 10$
 $f_s = 20,000$ psi Reinf.
 $f_s = 20,000$ psi Struct.
 Allowable δ Deflection $L/1200$
 Design Specifications - 1969 MASHO
 Add 25% per sq. ft. for future wearing surface

LOADING - HS 20-44

WATERWAY INFORMATION

Drainage Area: 252 Sq. Miles
 $Q_{50} = 10,062$ c.f.s.
 Total Req'd. Opening (50 year Flood): 2100 Sq. Ft.
 Created Head: 0.32'
 Proposed Opening: 2120 Sq. Ft.

CURVE DATA

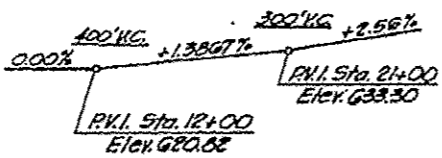
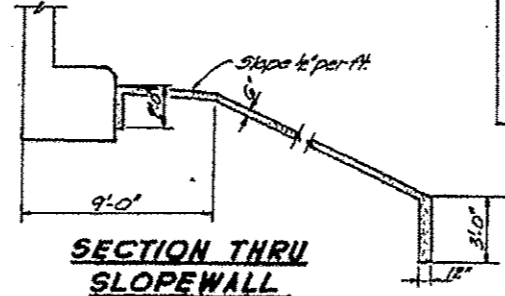
P.I. 18+72.32
 $\Delta = 80^\circ 40' 48''$
 $D = 5'00'00''$
 $T = 973.19'$
 $E = 357.48'$
 $R = 1145.92'$
 $L = 1613.60'$
 $S = 0.0811$

APPROACH PILE DATA

Type: Creosoted Piles
 No. Req'd.: 6
 2" N. Appr.
 Length Req'd.: 27'-5" Appr.
 See Special Provisions

STATION 17+44.00
 BUILT 1972 BY
 STATE OF ILLINOIS
 F.A. RTE. 403 - SEC. 195-3B-1
 LOADING HS 20

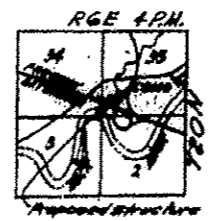
NAME PLATE
 Sec Std. 2113



PROFILE - RAMP A

* Elevations are to top of Class I

DESIGNED H.M.W.	EXAMINED	13
CHECKED C.D.C.	PASSED	
DRAWN C.D.C. & L.R.	APPROVED	
CHECKED S.M.C.		

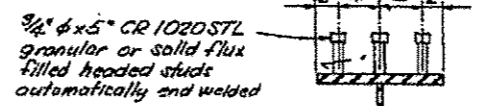
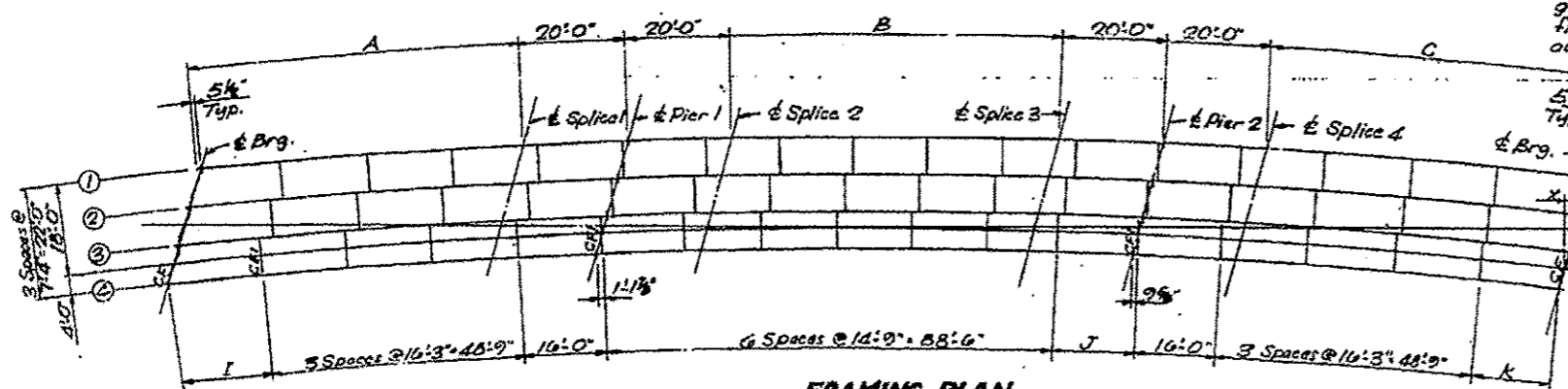


GENERAL PLAN & ELEVATION

RAMP A OVER ELKHORN CREEK
 F.A. RTE. 403 - SEC. 195-3B-1
 WHITESIDE COUNTY
 STATION 17+44.00

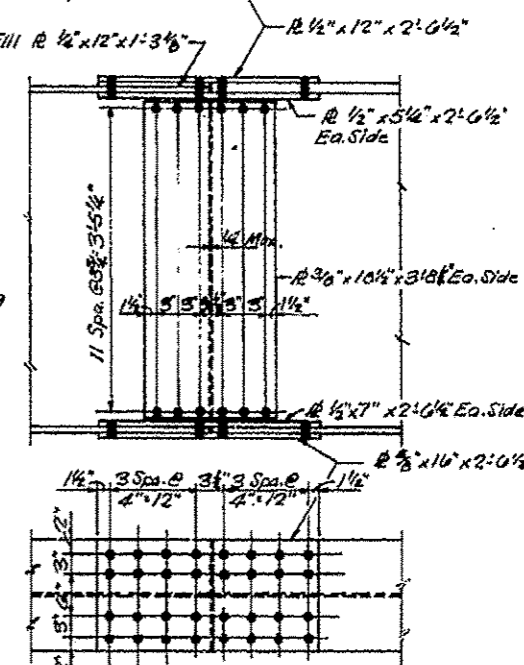
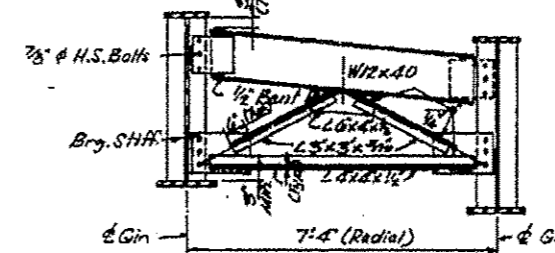
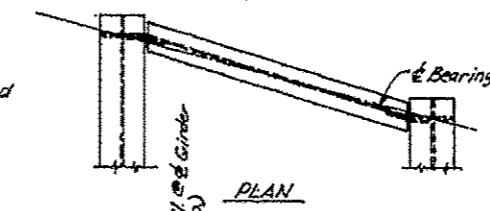
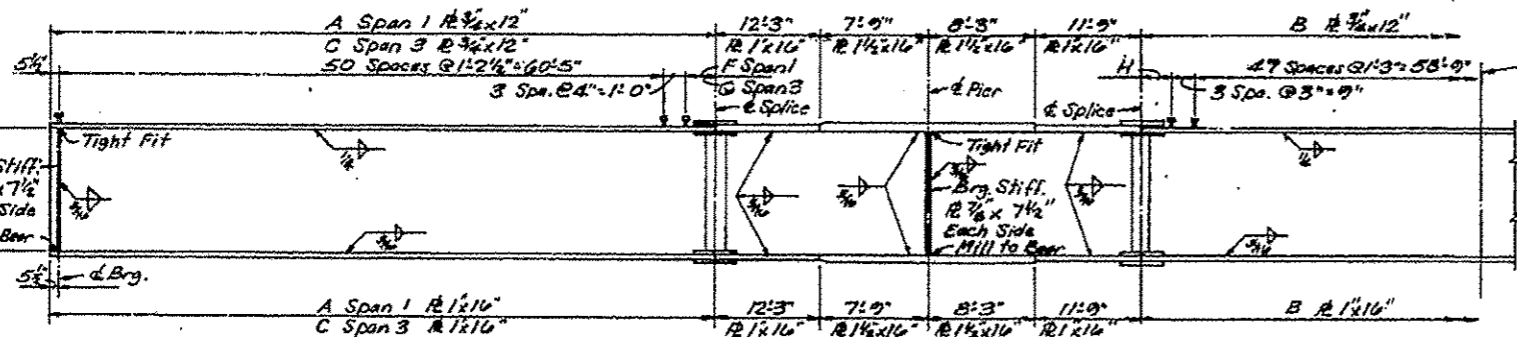
FOR INFORMATION ONLY

STATE OF ILLINOIS
DIVISION OF HIGHWAYS



ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 403	195-3	WHITESIDE	589	128

FED. ROAD DIST. NO. 7 ILLINOIS F.A. PROJ.
1 1/2" x 4 1/2" Sp. @ 3' 1 1/2" Sp. @ 3' 1 1/2" Sp. @ 3' 1 1/2"



	Abut.	Pier
R _{DL} (k)	45.1	127.3
R _{LL} (k)	46.5	67.5
R _{IMP} (k)	11.2	15.5
TOTAL (k)	102.8	210.3

	Span 1	Pier	Span 2
L _s (in)	18358	34020	18358
S _x (in ²)	809	1324	809
DL (k)	0.964	0.964	0.964
MLL (k)	4.18	9.17	3.99
L _s DL (k)	5.61	9.28	4.74
S _x (in ²)	35025	—	35025
S _y (in ²)	1115	—	1115
SDL (k)	0.880	0.880	0.880
M _{DL} (k)	285	406	287
L _s SDL (k)	252	3.65	255
L _s DL (k)	49420	—	49420
S _x (in ²)	1229	—	1229
M _{DL} (k)	688	588	728
M _{IMP} (k)	162	129	159
M _{TOTAL} (k)	847	601	887
S _x (in ²)	833	525	873
S _y (in ²)	1644	1784	1628
VR	527	—	527

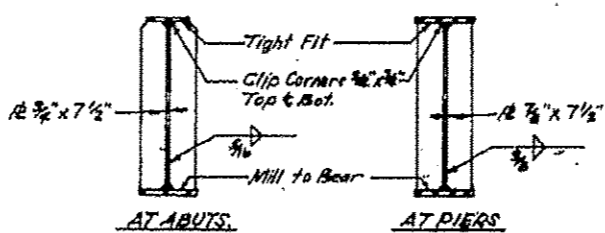
Dim.	Gir. 1	Gir. 2	Gir. 3	Gir. 4
A	63'-3 1/2"	63'-4 1/2"	63'-5 1/2"	63'-6"
B	63'-4 1/2"	63'-5 1/2"	63'-5 1/2"	63'-6 1/2"
C	63'-5"	63'-5 1/2"	63'-5 1/2"	63'-5 1/2"
F	1'-5"	1'-5 1/2"	1'-6"	1'-7 1/4"
G	1'-6 1/2"	1'-6 1/2"	1'-6 1/2"	1'-7 1/4"
H	1'-6 1/2"	1'-7 1/4"	1'-7 1/4"	1'-7 1/4"
I	19'-2 1/2"	19'-3 1/2"	19'-4 1/2"	19'-5 1/2"
J	18'-9 1/2"	18'-0 1/2"	18'-7 1/4"	18'-0 1/2"
K	17'-4 1/2"	17'-5 1/2"	17'-5 1/2"	17'-5 1/2"

	Gir. 1	Gir. 2	Gir. 3	Gir. 4
Abut. N.A.	627.02	626.300	625.739	625.132
Splice 1	627.860	627.240	626.620	626.005
Pier 1	628.143	627.525	626.906	626.287
Splice 2	628.412	627.796	627.179	626.562
Splice 3	629.277	628.667	628.050	627.440
Pier 2	629.554	628.940	628.337	627.728
Splice 4	629.829	629.217	628.610	627.990
Abut. S.A.	630.647	630.040	629.445	628.863

*For Fabrication Only

DESIGNED	J.D.	EXAMINED	L
CHECKED	S.M.K.	PASSED	
DRAWN	C.D.C.	APPROVED	
CHECKED	S.M.K.		

Girder 1	Girder 2	Girder 3	Girder 4
1163'-11"	1156'-7"	1149'-3"	1141'-7"

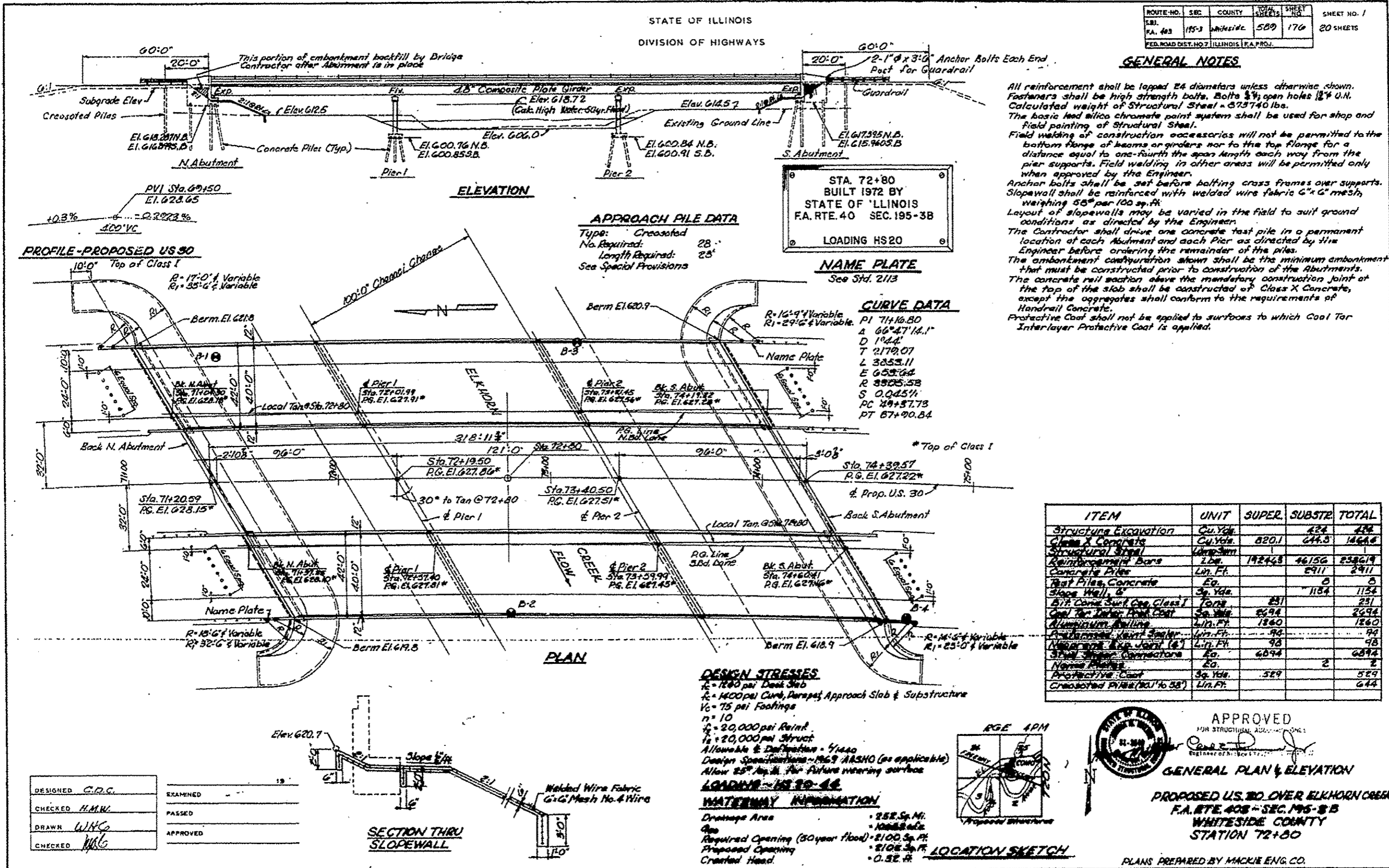


Station	17+00	17+10	17+20	17+30	17+40	17+50	17+60	17+70	17+80	17+90	18+00
X	0	10	20	30	40	50	60	70	80	90	100
Y	0	10	20	30	40	50	60	70	80	90	100

STRUCTURAL STEEL
F.A. RTE. 403-SEC. 195-3B-1
WHITESIDE COUNTY
STATION 17+44.00

FOR INFORMATION ONLY

098-0093 & 098-0094



GENERAL NOTES

All reinforcement shall be lapped 24 diameters unless otherwise shown. Fasteners shall be high strength bolts. Bolts $\frac{3}{4}$ open holes $\frac{1}{8}$ U.N. Calculated weight of Structural Steel = 575740 lbs. The basic lead silica chromate paint system shall be used for shop and field painting of Structural Steel. Field welding of construction accessories will not be permitted to the bottom flange of beams or girders nor to the top flange for a distance equal to one-fourth the span length each way from the pier supports. Field welding in other areas will be permitted only when approved by the Engineer. Anchor bolts shall be set before bolting cross frames over supports. Slope wall shall be reinforced with welded wire fabric 6" x 6" mesh weighing 55# per 100 sq. ft. Layout of slope walls may be varied in the field to suit ground conditions as directed by the Engineer. The Contractor shall drive one concrete test pile in a permanent location at each Abutment and each Pier as directed by the Engineer before ordering the remainder of the piles. The embankment configuration shown shall be the minimum embankment that must be constructed prior to construction of the Abutments. The concrete rail section above the mandatory construction joint of the top of the slab shall be constructed of Class X Concrete, except the aggregates shall conform to the requirements of Handrail Concrete. Protective Coat shall not be applied to surfaces to which Cool Tar Interlayer Protective Coat is applied.

ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1
88	195-3	Whiteside	589	176	20 SHEETS

STA. 72+80
BUILT 1972 BY
STATE OF ILLINOIS
F.A. RTE. 40 SEC. 195-38
LOADING HS20

NAME PLATE

See Std. 2113

CURVE DATA

PI 71+16.80
A 66°47'14.1"
D 194.4'
T 2179.07
L 3353.11
E 653.64
R 3505.58
S 0.0451'
PC 72+57.73
PT 87+90.84

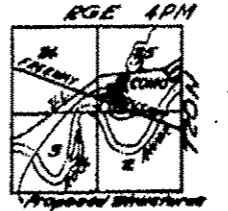
ITEM	UNIT	SUPER.	SUBSTR.	TOTAL
Structural Excavation	Cu. Yds.		424	424
Class X Concrete	Cu. Yds.	820.1	644.3	1464.4
Structural Steel	Welded			1
Reinforcing Bars	Lbs.	192438	46156	238594
Concrete Piles	Lin. Ft.		2911	2911
Test Piles Concrete	Ea.		0	0
Slope Wall	Sq. Yds.		1154	1154
Bit Coat Surf. Class I	Tons		231	231
Cool Tar Interlayer Coat	Sq. Yds.		2694	2694
Aluminum Scaffolding	Lin. Ft.		1860	1860
Propane Gas Vent Spigot	Lin. Ft.		99	99
Non-solvent Epoxy Joint (E)	Lin. Ft.		48	48
Steel Bridge Connectors	Ea.		6894	6894
Name Plates	Ea.		2	2
Protective Coat	Sq. Yds.		529	529
Crossed Piles (30' to 58')	Lin. Ft.			644

DESIGN STRESSES

$f_c = 1200$ psi Deck Slab
 $f_c = 1400$ psi Curb, Approach Slab & Substructure
 $f_c = 75$ psi Footings
 $n = 10$
 $f_s = 20,000$ psi Reinft.
 $f_s = 20,000$ psi Struct.
Allowable & Distribution = 5/1600
Design Specifications = 1963 ARSNO (as applicable)
Allow 25' max. ft. for future wearing surface
LOADING - HS 20-44

WATERWAY INFORMATION

Drainage Area = 252.56 Ac.
Q = 1088.8 cfs
Required Opening (50 year flood) = 2100.36 cfs
Proposed Opening = 2106.36 cfs
Crested Head = 0.52 ft.



APPROVED
FOR STRUCTURAL ACCOUNT ONLY
Mackie Eng. Co.
Engineer of Bridge Structures

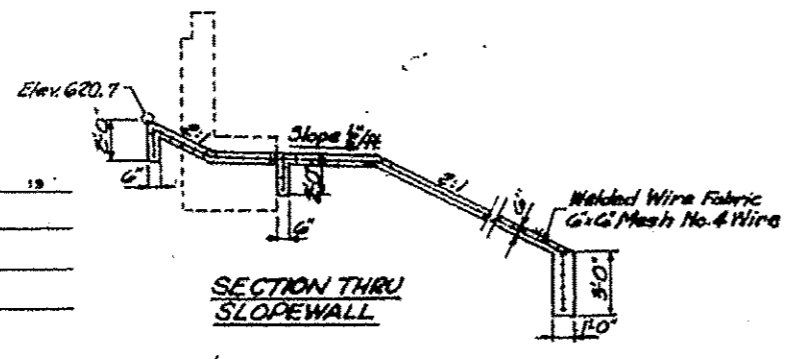
GENERAL PLAN & ELEVATION

PROPOSED U.S. 30 OVER ELKHORN CREEK
F.A. RTE. 408 - SEC. 195-38
WHITESIDE COUNTY
STATION 72+80

PLANS PREPARED BY MACKIE ENG. CO.

DESIGNED	C.D.C.	EXAMINED	
CHECKED	H.M.W.	PASSED	
DRAWN	W.H.G.	APPROVED	
CHECKED	M.B.G.		

SECTION THRU SLOPE WALL

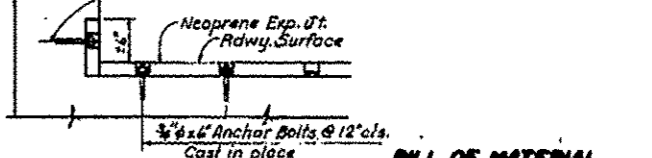
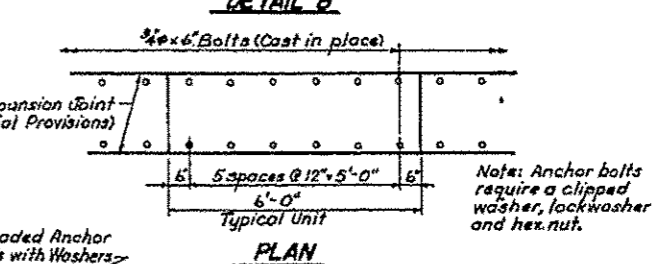
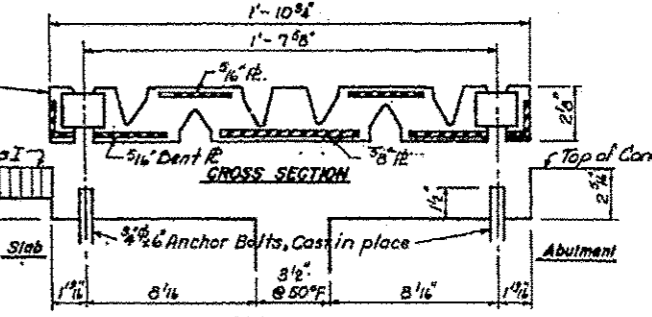
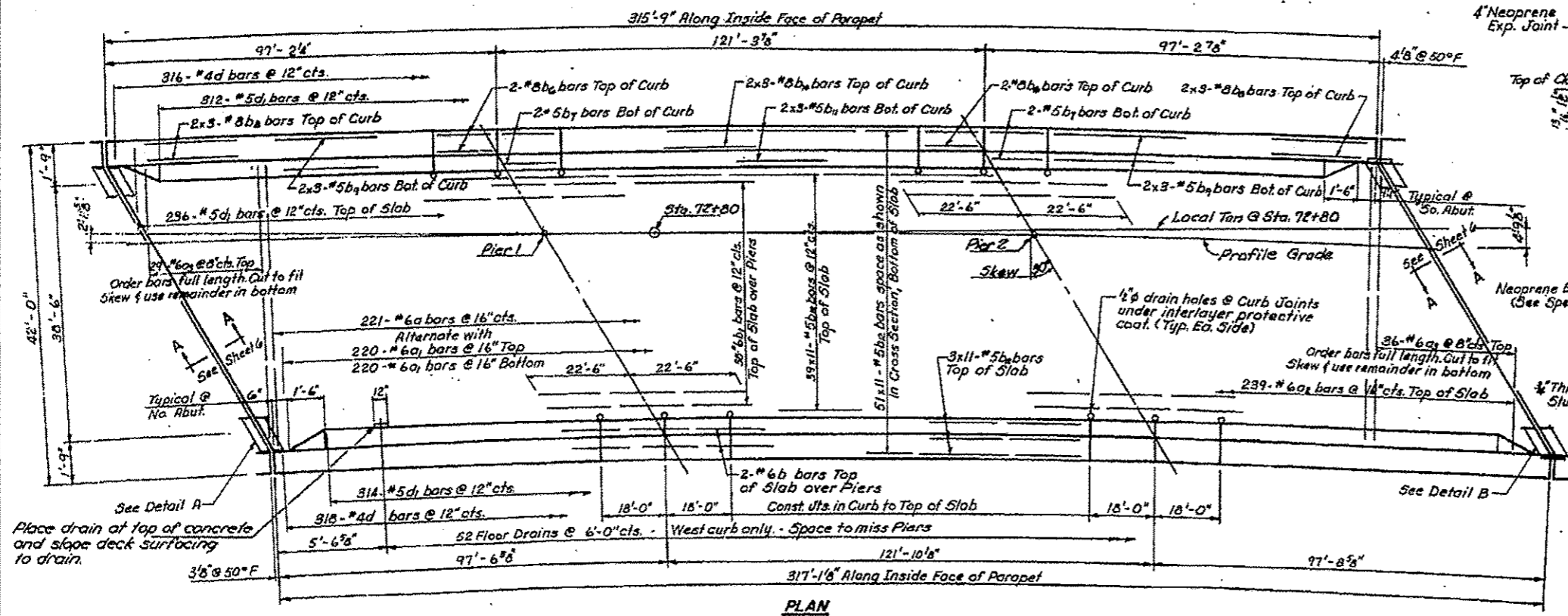


FOR INFORMATION ONLY

NOTE:
 Bars indicated thus 20 x 3-#5 etc.
 indicates 20 lines of bars with 3
 lengths per line.
 Min. bar laps = 24 dia.

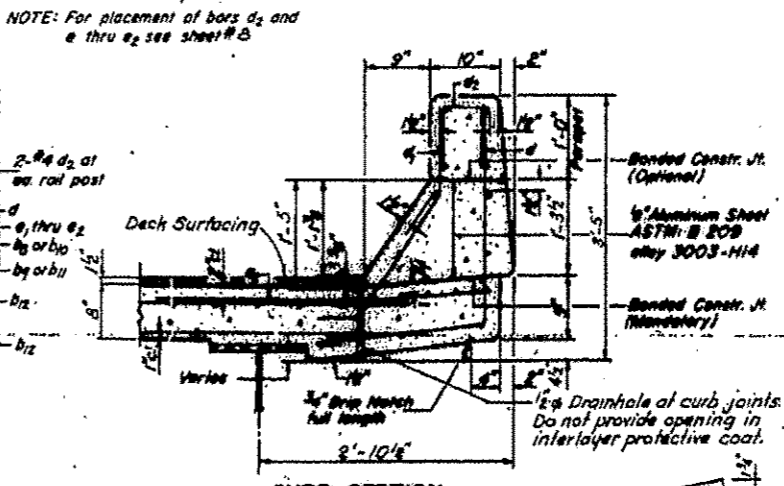
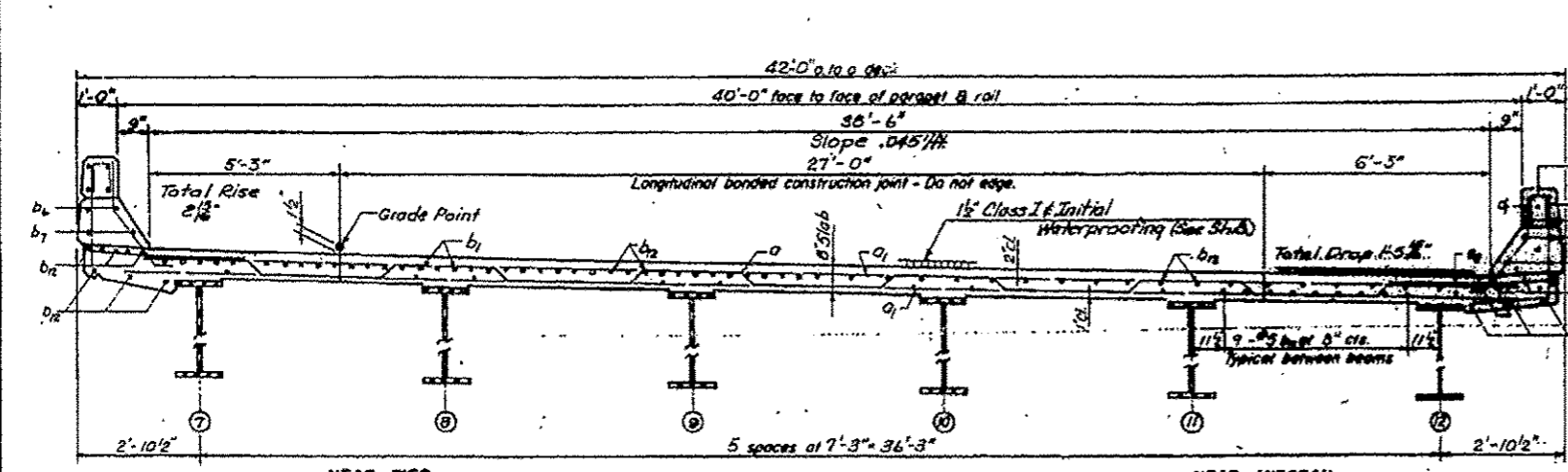
STATE OF ILLINOIS
 DIVISION OF HIGHWAYS

DATE	BY	CHKD	APP'D	DRWING NO.
195-3	W.H.C.	588	182	20

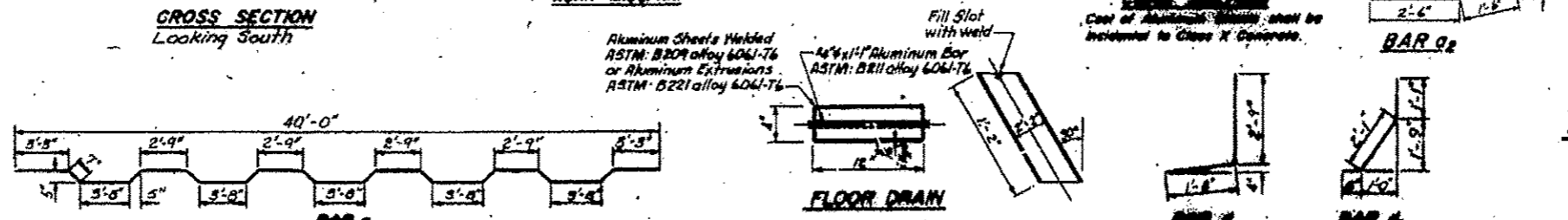


ALL OF MATERIAL

Bar	Qty	Size	Length	Notes
a	221	#6	41'-8"	
b	440	#6	40'-0"	
c	475	#6	4'-2"	
d	63	#6	42'-6"	
b ₁₂	1056	#6	29'-11"	
b ₁	84	#6	48'-0"	
b ₂	16	#6	17'-8"	
b ₃	16	#6	17'-8"	
b ₄	24	#6	27'-8"	
b ₅	24	#6	27'-8"	
b ₆	12	#6	29'-10"	
b ₇	12	#6	29'-10"	
e	436	#6	3'-3"	
f	626	#6	3'-9"	
Reinforcement Bars	264		9483'	
Class I Concrete	61.76		550.5	



DESIGNED	W.H.C.
CHECKED	W.H.C.
DRAWN	E.W.W.
CHECKED	W.H.C.



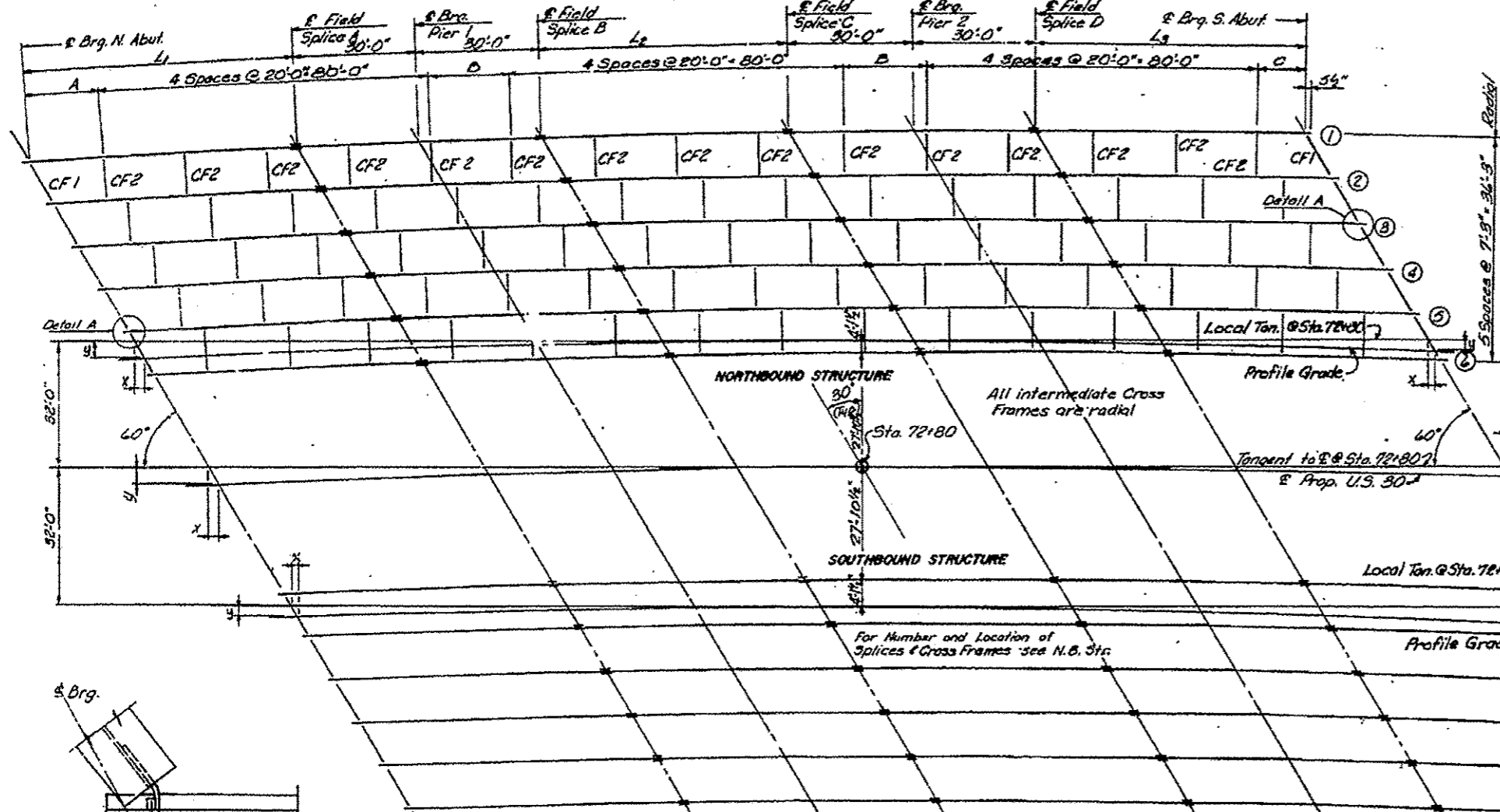
Parapet Reinforcement and Class X Concrete are filled on sheet #20

SUPERSTRUCTURE-SB BRIDGE
 F.A. RTE. 403-SEC. 195-3B
 WHITESIDE COUNTY
 STATION 72+80

FOR INFORMATION ONLY

STATE OF ILLINOIS
DIVISION OF HIGHWAYS

ROUTE NO. SEC. COUNTY TOTAL SHEETS SHEET NO.
S.B.I. 403 NS-3 Whiteside 589 104 20 SHEETS
FED. ROAD DIST. NO. 7 ILLINOIS F.A. PROJ.



OFFSET DIMENSIONS (FT)

Girder	Br. N.A.	Splice A	Br. P1	Splice B	Splice C	Br. P2	Splice D	Br. S.A.
U	X	R	X	U	X	U	X	U
1	5.499	2.775	2.395	1.883	1.977	0.807	0.117	0.885
2	5.281	2.697	2.192	1.677	1.804	0.721	0.088	0.880
3	5.064	2.619	2.105	1.578	1.715	0.678	0.088	0.877
4	4.847	2.541	2.018	1.479	1.626	0.635	0.088	0.874
5	4.630	2.463	1.931	1.380	1.537	0.592	0.088	0.871
6	4.413	2.385	1.844	1.281	1.448	0.549	0.088	0.868
7	4.196	2.307	1.757	1.182	1.359	0.506	0.088	0.865
8	3.979	2.229	1.670	1.083	1.270	0.463	0.088	0.862
9	3.762	2.151	1.583	0.984	1.181	0.420	0.088	0.859
10	3.545	2.073	1.496	0.885	1.092	0.377	0.088	0.856
11	3.328	1.995	1.409	0.786	1.003	0.334	0.088	0.853
12	3.111	1.917	1.322	0.687	0.914	0.291	0.088	0.850
P.G. N.B.	4.501	2.633						
P.G. S.B.	2.922	1.640						

The additional stress due to curvature is approx. and in the order of 2.5% Not included in table. I_y and S_y are the moment of inertia and section modulus of the steel section. I_c and S_c are the moment of inertia and section modulus of the composite section used in computing f_a . V_e is the maximum LL + Impact shear range in span.

INTERIOR GIRDER MOMENT TABLE

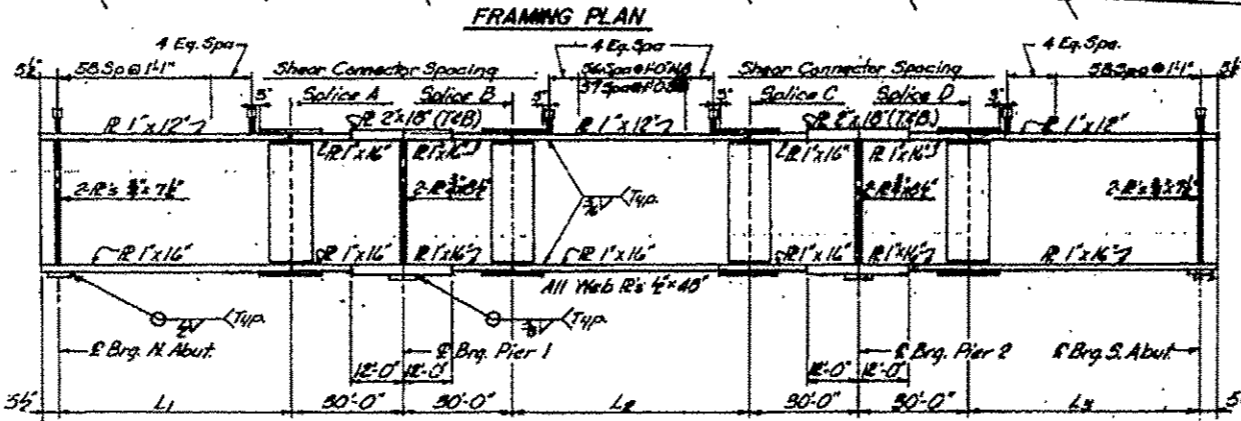
	0.5 Sp. 1	Pier	0.5 Sp. 2
I_y (in ⁴)	21252.6	47652.0	21252.6
S_y (in ³)	918.5	1908.9	918.5
DL (k)	0.963	0.963	0.963
MSDL (k)	5.50	1316	4.71
f_a DL (ksi)	4.92	0.87	6.16
I_c (in ⁴)	36656.7	24650.0	36656.7
S_c (in ³)	1123.0	1908.9	1123.0
SDL (k)	0.417	0.417	0.417
MSDL (k)	2.69	4.80	2.84
f_a SDL (ksi)	2.87	3.02	3.08
V_e (k)	899.87	2968.8	899.87
S_e (in ³)	1222.6	1908.9	1222.6
MSDL (k)	796	763	830
MSDL (k)	179	122	168
MSDL (k)	975	925	998
f_a LL (ksi)	9.55	5.81	9.77
f_a TOTAL (ksi)	19.33	17.10	18.95
V_e (k)	57.69		81.11

INTERIOR GIRDER REACTION TABLE

	Abut.	Pier 1	Pier 2
RDL (k)	48.1	175.5	175.5
RLL (k)	46.8	78.2	78.2
IMP (k)	10.6	16.1	16.1
RTOTAL (k)	105.5	269.8	269.8

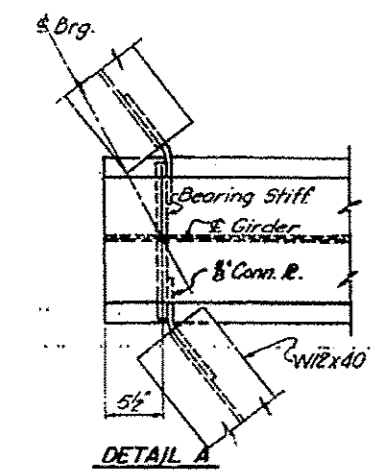
GIRDER DIMENSIONS

Girder	Radius	L1	L2	L3	A	B	C
1	5849.655	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
2	5842.205	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
3	5834.755	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
4	5827.305	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
5	5819.855	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
6	5812.405	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
7	5804.955	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
8	5797.505	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
9	5790.055	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
10	5782.605	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
11	5775.155	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"
12	5767.705	60'-8"	60'-8"	60'-8"	17'-6"	20'-8"	18'-2"



GIRDER ELEVATION
(To be fabricated to its required radius)

Note: All bolts for field splices shall be 1/2" high strength bolts. Contact surfaces of all splices shall be free of oil or paint.



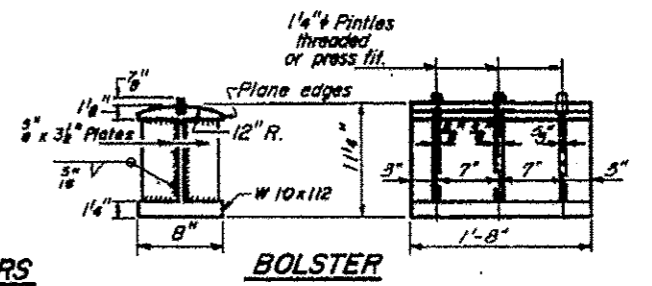
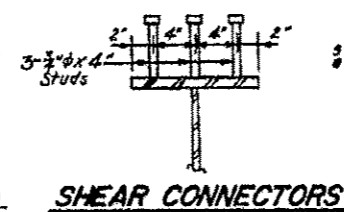
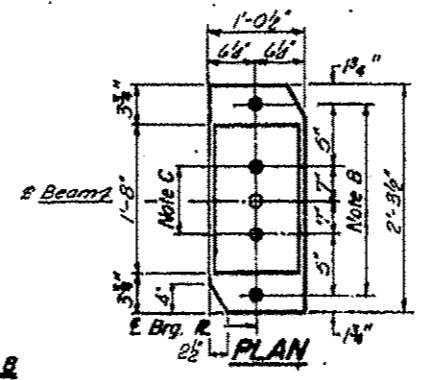
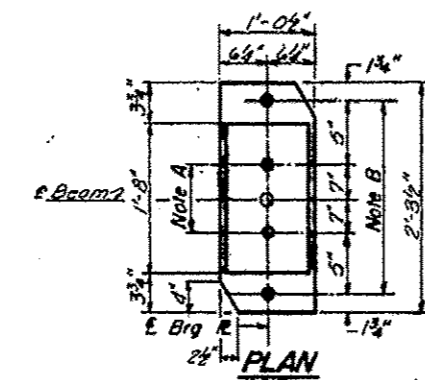
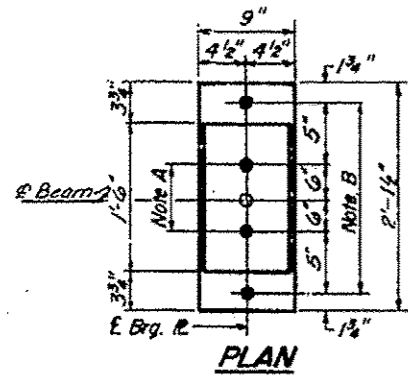
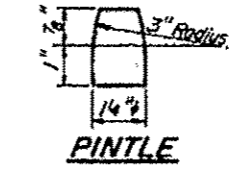
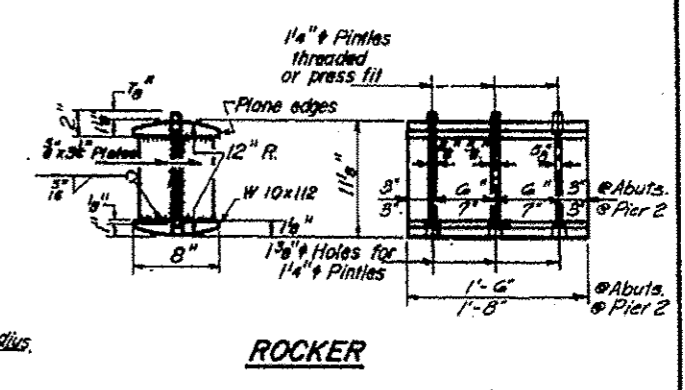
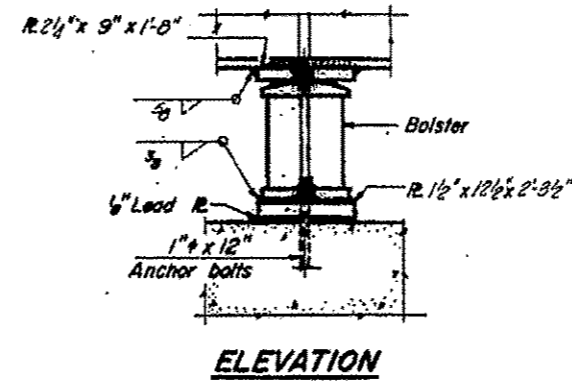
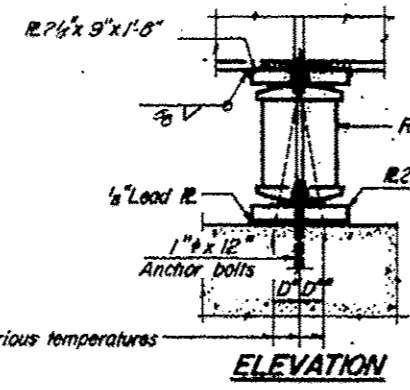
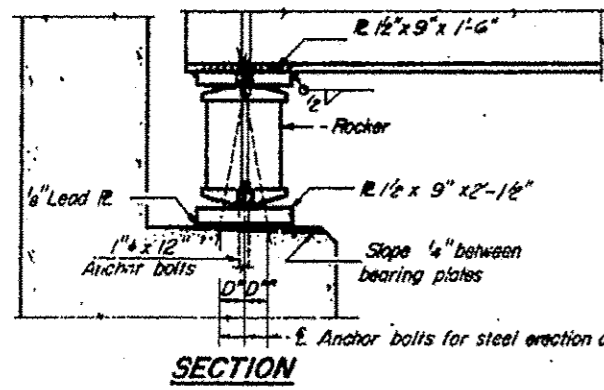
DESIGNED	JFK.	EXAMINED	
CHECKED	M/LC	PASSED	
DRAWN	R/LC	APPROVED	
CHECKED	M/LC		

FRAMING PLAN
F.A. RTE. 403-SEC. 195-3B
WHITESIDE COUNTY
STATION 72+80

FOR INFORMATION ONLY

DATE	BY	CHKD.	TITLE	NO.	SHEET NO.
195-3	White	589	185	20	20 SHEETS

STATE OF ILLINOIS
DIVISION OF HIGHWAYS



NOTE A
1 3/8" Holes - 1" deep in top R. for pintles Thread or press fit pintles into bottom R.

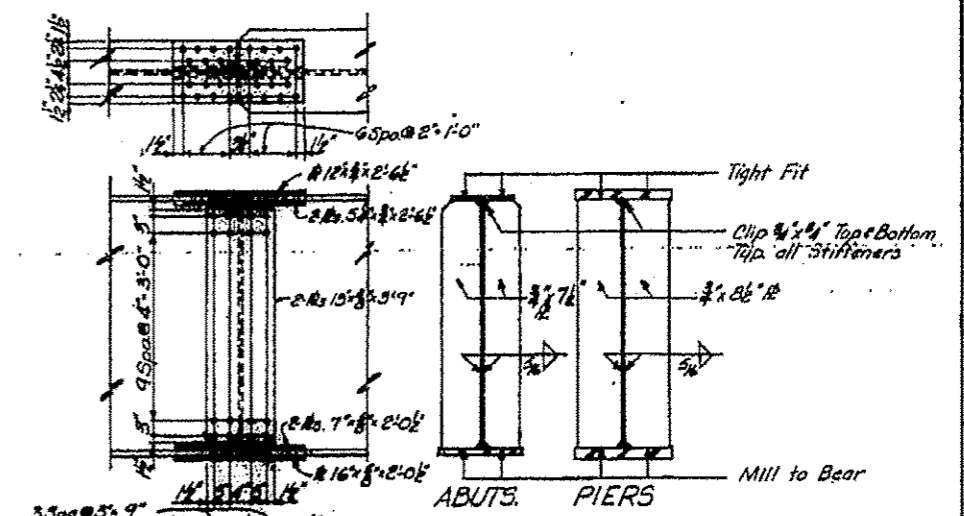
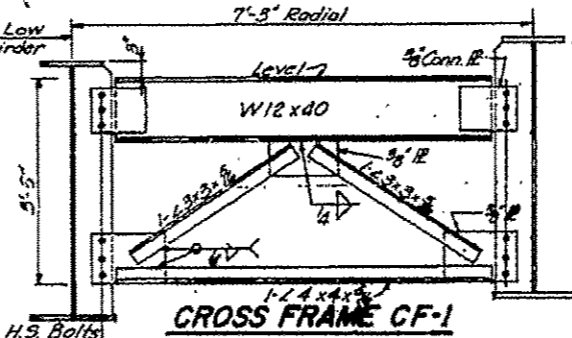
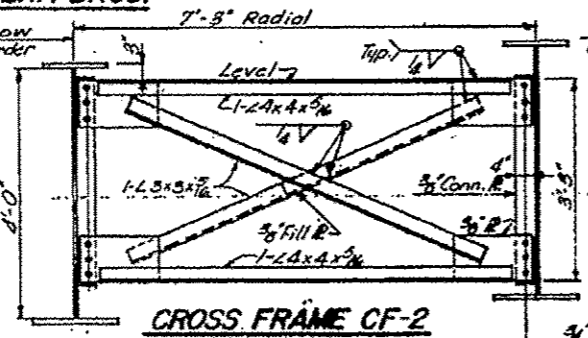
NOTE B
1 1/2" Holes for 1" anchor bolts. 2" x 2 1/2" x 2 1/2" R. Washers under nut.

NOTE C
1 3/8" Holes 1" deep in top R. only for 1 1/4" pintles.

NOTES ON SETTING OF ANCHOR BOLTS AT EXP. BRGS.

- a) D* (Side of brg away from fixed brg)
D* = 1/8" per each 100' of expansion for every 15° fall below the normal temp. of 50°F
- D** (Side of brg toward fixed brg)
D** = 1/8" per each 100' of expansion for every 15° rise above the normal temp. of 50°F
- b) After beams have been erected and dimensions D* or D** determined, holes shall be drilled and anchor bolts shall be grouted in place. All fixed anchor bolts may be built into the masonry.

BEARINGS ASSEMBLY DETAILS



ELEVATIONS-TOP OF WEB (For Fabrication Only)

Girder	E. Brg. N. Abut.	E. Splice A	E. Brg. Pier 1	E. Splice B	E. Splice C	E. Brg. Pier 2	E. Splice D	E. Brg. S. Abut.
1	628.71	628.66	628.98	628.87	628.12	628.08	627.94	627.85
2	628.38	628.16	628.04	627.95	627.79	627.69	627.61	627.51
3	627.08	627.79	627.70	627.61	627.64	627.85	627.77	627.17
4	627.71	627.85	627.56	627.28	627.10	627.02	626.93	626.83
5	627.58	627.11	627.08	626.94	626.76	626.68	626.59	626.49
6	627.04	626.77	626.69	626.40	626.42	626.34	626.25	626.15
7	627.34	627.04	626.97	626.80	626.70	626.61	626.52	626.42
8	627.00	626.72	626.68	626.54	626.36	626.27	626.18	626.08
9	626.47	626.38	626.29	626.20	626.02	625.93	625.84	625.74
10	626.53	626.04	625.95	625.86	625.68	625.59	625.50	625.40
11	625.99	625.70	625.62	625.53	625.34	625.25	625.16	625.06

DESIGNED	J.F.K.	EXAMINED	
CHECKED	MLO	DRAWN	
DRAWN	R.L.C.	APPROVED	
CHECKED	MLO		