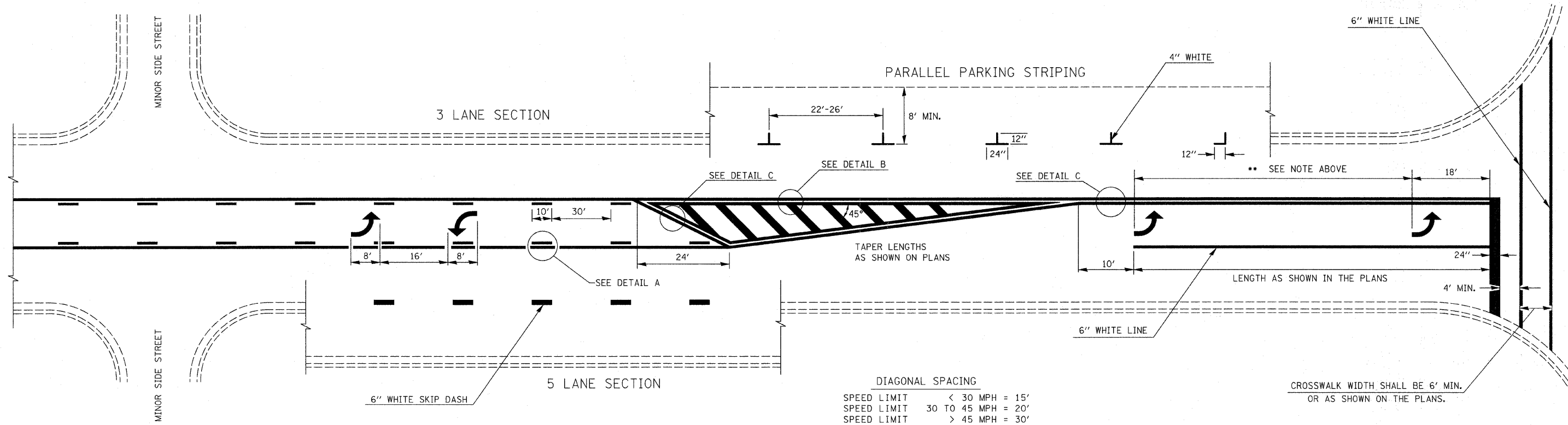


1/29/2008  
 1 2 3 4 5 6 7 8 9  
 19 20 21 22 23 24 25 26 27  
 28 29 30 31 32 33 34 35 36  
 37 38 39 40 41 42 43 44 45  
 46 47 48 49 50 51 52 53 54  
 55 56 57 58 59 60 61 62 63

c:\projects\74144\dshdtetails.74144.dgn  
 \*DATE\*

FAP RITE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
322	19RS-4	SHELBY	33	33
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT NO.	

\*\* PLACE THE FIRST ARROW EVEN WITH THE BEGINNING OF THE TURN BAY.  
 PLACE ANOTHER ARROW 18' BACK FROM THE STOP BAR. IF REQUIRED,  
 ADD ADDITIONAL ARROWS TO MAINTAIN A MAXIMUM OF 80' SPACING.



DIAGONAL SPACING

SPEED LIMIT	< 30 MPH = 15'
SPEED LIMIT	30 TO 45 MPH = 20'
SPEED LIMIT	> 45 MPH = 30'

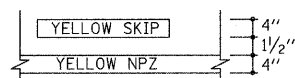
CROSSWALK WIDTH SHALL BE 6' MIN.  
 OR AS SHOWN ON THE PLANS.

**TYPICAL BI-DIRECTIONAL TURN LANE MARKINGS**  
 (SHOWING TRANSITION TO EXCLUSIVE LEFT TURN LANE)

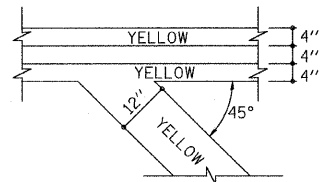
NOTES:

- NO LESS THAN 2 ARROWS SHALL BE USED.
- TAPER RATES, LANE WIDTHS, AND STORAGE LENGTHS AS SHOWN ON THE PLANS.
- AVOID PLACING BI-DIRECTIONAL ARROWS IN INTERSECTIONS.
- BI-DIRECTIONAL ARROWS SHALL BE SPACED A MAXIMUM OF 250'.

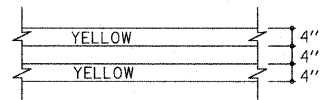
DETAIL A



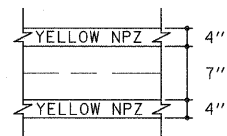
DETAIL B



DETAIL C



DETAIL D



NOT TO SCALE

REVISIONS	
NAME	DATE
MMO	12-99
DRM	08-04

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TYPICAL APPLICATION OF  
 PAVEMENT MARKINGS**  
**SHEET 2 OF 2**

SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY