

TYPICAL SECTION THRU ABUTMENT
(Horiz. dim. @ Rt. L's)

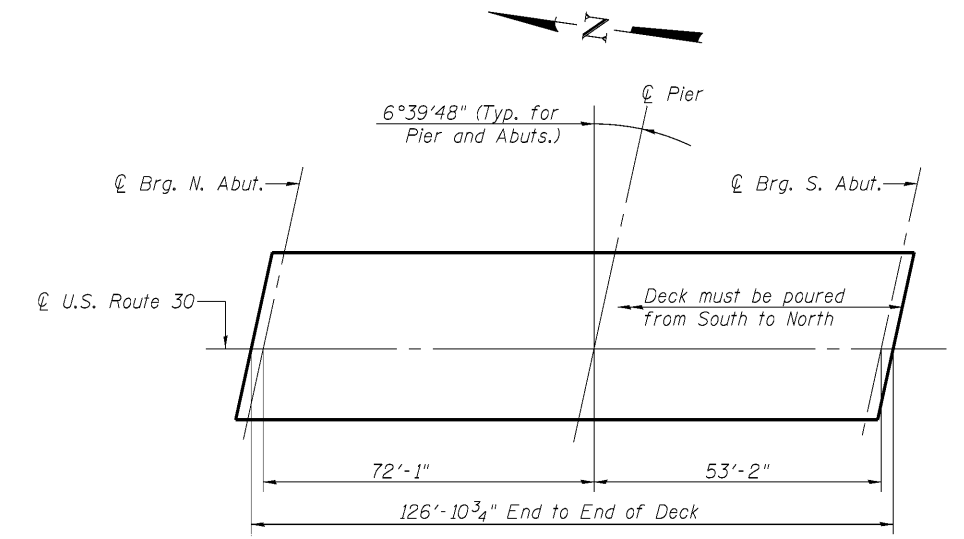
* In order to alleviate downdrag on the piles, the Contractor shall:
Construct the MSE walls first, wait **6 months, and then drive piles through sleeves that were placed before MSE wall construction. Place sleeves after removal of unsuitable material and before backfilling with Aggregate Subgrade Improvement where this is necessary. Cost included with Furnishing Metal Shell Piles, 14" φ x 0.25".

**Or until the settlement platforms for the MSE walls indicate that 1.6" of the estimated 2.0" of settlement has occurred at the abutment.

***MSE Wall Supplier to provide internal stability design for load transfer system to accommodate horizontal forces from abutment

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DECK POURING SEQUENCE

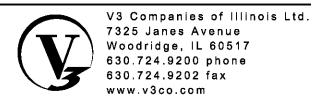
GENERAL NOTES

Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts. Bolts 7/8 in. φ, holes 15/16 in. φ, unless otherwise noted.
Calculated weight of Structural Steel: AASHTO M270 Grade 50 = 257,950 lbs.
AASHTO M270 Grade 36 = 25,030 lbs.

No field welding is permitted except as specified in the contract documents. Reinforcement bars designated (E) shall be epoxy coated. Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 1/8 inch (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings. Concrete Sealer shall be applied to the backwalls, bridge seats and front face of the pile caps of the abutments. The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be reddish brown, Munsell No. 2.5YR 3/4. Slipforming of the parapets is not allowed.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Structure Excavation	Cu. Yd.		154	154
Concrete Structures	Cu. Yd.		349.2	349.2
Concrete Superstructure	Cu. Yd.	553.4		553.4
Bridge Deck Grooving	Sq. Yd.	1,146		1,146
Protective Coat	Sq. Yd.	1,794		1,794
Furnishing and Erecting Structural Steel	L. Sum	1		1
Stud Shear Connectors	Each	6,435		6,435
Reinforcement Bars, Epoxy Coated	Pound	125,950	43,790	169,740
Bar Splicers	Each	158		158
Bridge Fence Railing (Sidewalk)	Foot	98		98
Bicycle Railing	Foot	58		58
Parapet Railing	Foot	156		156
Furnishing Metal Shell Piles, 14" x 0.25"	Foot		3,058	3,058
Driving Piles	Foot		3,058	3,058
Test Pile, Metal Shells	Each		3	3
Name Plates	Each	1		1
Preformed Joint Strip Seal	Foot	162		162
Elastomeric Bearing Assembly, Type I	Each	26		26
Anchor Bolt, 1"	Each	78		78
Concrete Sealer	Sq. Ft.		1,332	1,332
Temporary Sheet Piling	Sq. Ft.		675	675



USER NAME =	DESIGNED - WJV	REVISED -
CHECKED - CJB	REVISED -	
PLOT SCALE =	DRAWN - WJV	REVISED -
PLOT DATE =	CHECKED - CJB	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES, INDEX OF SHEETS AND TOTAL BILL OF MATERIAL
STRUCTURE NO. 016-1350
SHEET NO. 2 OF 35 SHEETS

F.A.P. RTE. 353	SECTION 11-Y-A	COUNTY COOK	TOTAL SHEETS 354	SHEET NO. 187
			CONTRACT NO. 60R19	
ILLINOIS FED. AID PROJECT				