

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	ESTIMATED QUANTITY	UNIT	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	1	LS	
AR150520	MOBILIZATION	1	LS	
AR150540	HAUL ROUTE	1	LS	
AR152410	UNCLASSIFIED EXCAVATION	2,600	CY	
AR152540	SOIL STABILIZATION FABRIC	5,300	SY	
AR156520	INLET PROTECTION	8	EACH	
AR208515	POROUS GRANULAR EMBANKMENT	230	CY	
AR208606	6" AGGREGATE BASE COURSE	5,050	SY	
AR209606	CRUSHED AGG. BASE COURSE - 6"	5,300	SY	
AR209650	AGGREGATE BASE PREPARATION	1,460	SY	
AR401610	BITUMINOUS SURFACE COURSE	570	TON	
AR401900	REMOVE BITUMINOUS PAVEMENT	6,500	SY	
AR403610	BITUMINOUS BASE COURSE	950	TON	
AR602510	BITUMINOUS PRIME COAT	1,950	GAL	
AR603510	BITUMINOUS TACK COAT	970	GAL	
AR620520	PAVEMENT MARKING - WATERBORNE	1,300	SF	
AR701008	8" PVC STORM SEWER	88	LF	
AR701010	10" PVC STORM SEWER	370	LF	
AR701900	REMOVE PIPE	450	LF	
AR705526	6" PERFORATED UNDERDRAIN	470	LF	
AR751412	INLET TYPE B	6	EACH	
AR751900	REMOVE INLET	4	EACH	

INDEX TO SHEETS

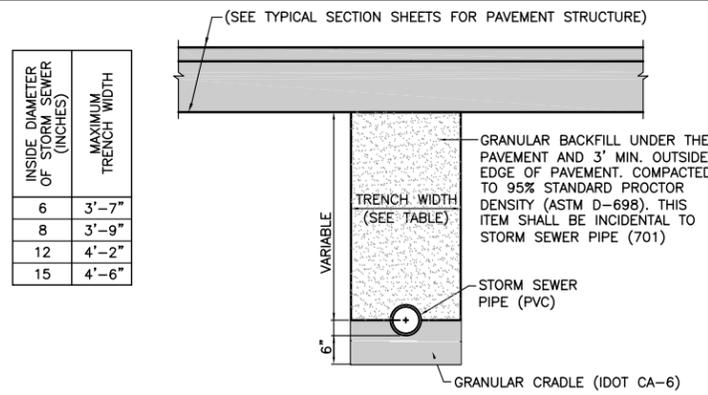
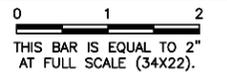
- COVER SHEET
- INDEX TO SHEETS / SUMMARY OF QUANTITIES / DETAILS
- SITE PLAN / PROJECT CONTROL PLAN
- SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS
- SEQUENCE OF CONSTRUCTION PER AC 150/5370-2F (LATEST EDITION)
- STORM WATER POLLUTION PREVENTION PLAN NOTES
- TYPICAL SECTIONS
- EXISTING CONDITIONS/PROPOSED REMOVALS
- GRADING AND DRAINAGE PLAN
- PAVEMENT MARKING PLAN

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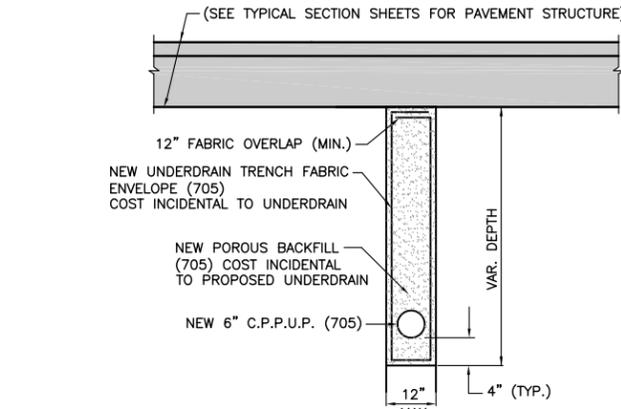
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 LAYOUT: Layout1
 UPDATE BY: Adham Odeh
 SURVEY BOOK #
 DATE: Thursday, March 6, 2014 8:48:27 AM
 XREF DWG: tbcInt.dwg
 tb.dwg

REVISIONS

NUMBER	BY	DATE



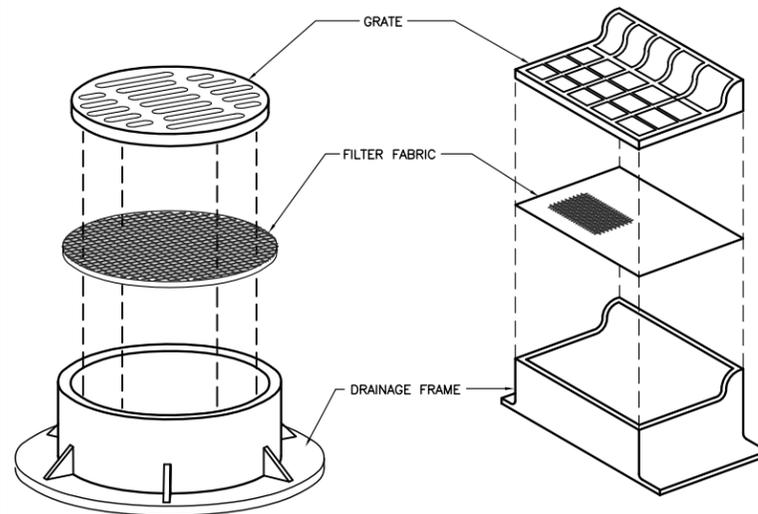
TRENCH DETAILS - STORM SEWER
NOT TO SCALE



UNDERDRAIN DETAIL - PAVED AREAS
N.T.S.

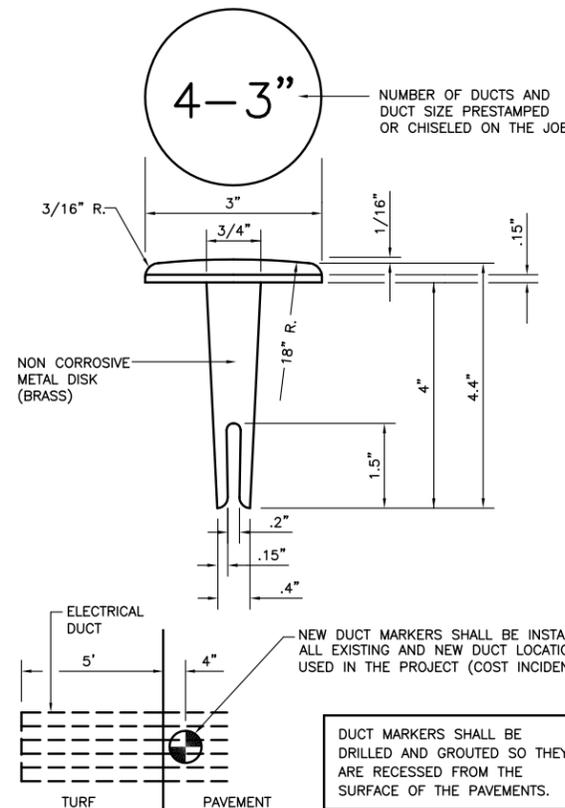
UNDER DRAIN NOTES:

- ALL UNDERDRAIN CONNECTIONS, TEES, BENDS, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- UNDERDRAIN CONFLICTS WITH THE EXISTING CONDITIONS SHALL BE RESOLVED AND SHALL BE INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- THE 6" UNDERDRAIN SHALL BE INSTALLED AFTER THE SUBGRADE IS COMPACTED.

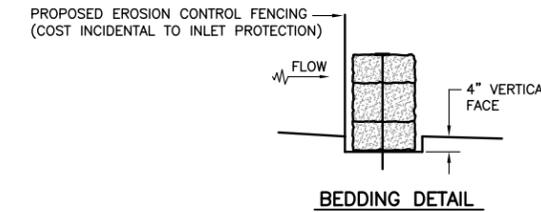


DRAINAGE STRUCTURE FILTER WRAP
N.T.S.

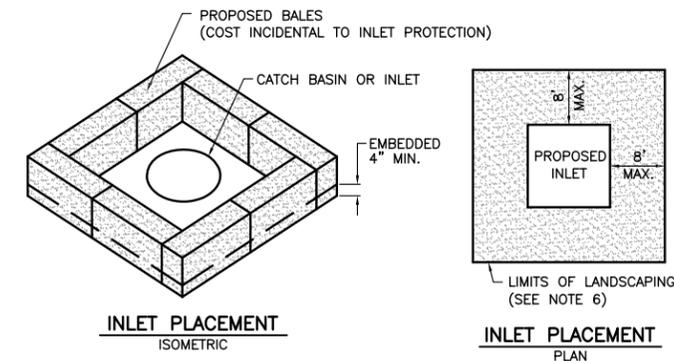
- FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
- FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2012.
- FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
- CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
- FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
- COST OF FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



DUCT MARKER DETAIL
NOT TO SCALE



BEDDING DETAIL



STORM INLET SEDIMENT TRAP DETAIL - TURF AREAS

NOTES:

- BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
- EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
- BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
- INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
- BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR INLET PROTECTION/DITCH CHECK.
- AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD, EXCELSIOR BLANKET WITH SEED OR KNITTED STRAW MAT WITH SEED OVER THE DISTURBED AREAS. COST INCIDENTAL TO INLET PROTECTION.

LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS
REHABILITATE NORTH QUADRANT T-HANGER
PAVEMENTS & ADDITIONAL TAXIWAYS
INDEX TO SHEETS / SUMMARY
OF QUANTITIES / DETAILS

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DESIGN BY:	AAO
DRAWN BY:	AAO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	02/28/14
JOB No:	13297-02-00
I.L. PROJECT: IGQ-4297 S.B.G. PROJECT: 3-17-SPGP-XX	
SHEET	2 OF 10 SHEETS