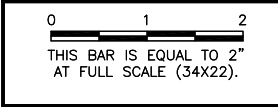


LEGEND

- EXISTING BITUMINOUS PAVEMENT TO BE REHABILITATED / REMOVED
- EXISTING INLET/MANHOLE
- EXISTING FLARED END SECTION
- EXISTING SLOPE BOX
- EXISTING STORM SEWER AND SIZE
- EXISTING UNDERDRAIN COLLECTION STRUCTURE
- EXISTING FIRE HYDRANT
- EXISTING UNDERGROUND COMED ELECTRIC LINE
- EXISTING OVERHEAD COMED ELECTRIC LINE
- EXISTING UTILITY POLE
- EXISTING HANGAR MOUNTED LIGHT
- EXISTING TELEPHONE
- EXISTING 6' FENCE
- EXISTING LANSING DRAINAGE DITCH
- EXISTING UNDERDRAIN
- EXISTING WATERMAIN
- EXISTING GUARDRAIL
- EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- SOIL BORING
- PAVEMENT CORE
- REMOVAL ITEM
- STRUCTURE TO BE REMOVED

REVISIONS

NUMBER	BY	DATE



NOTES

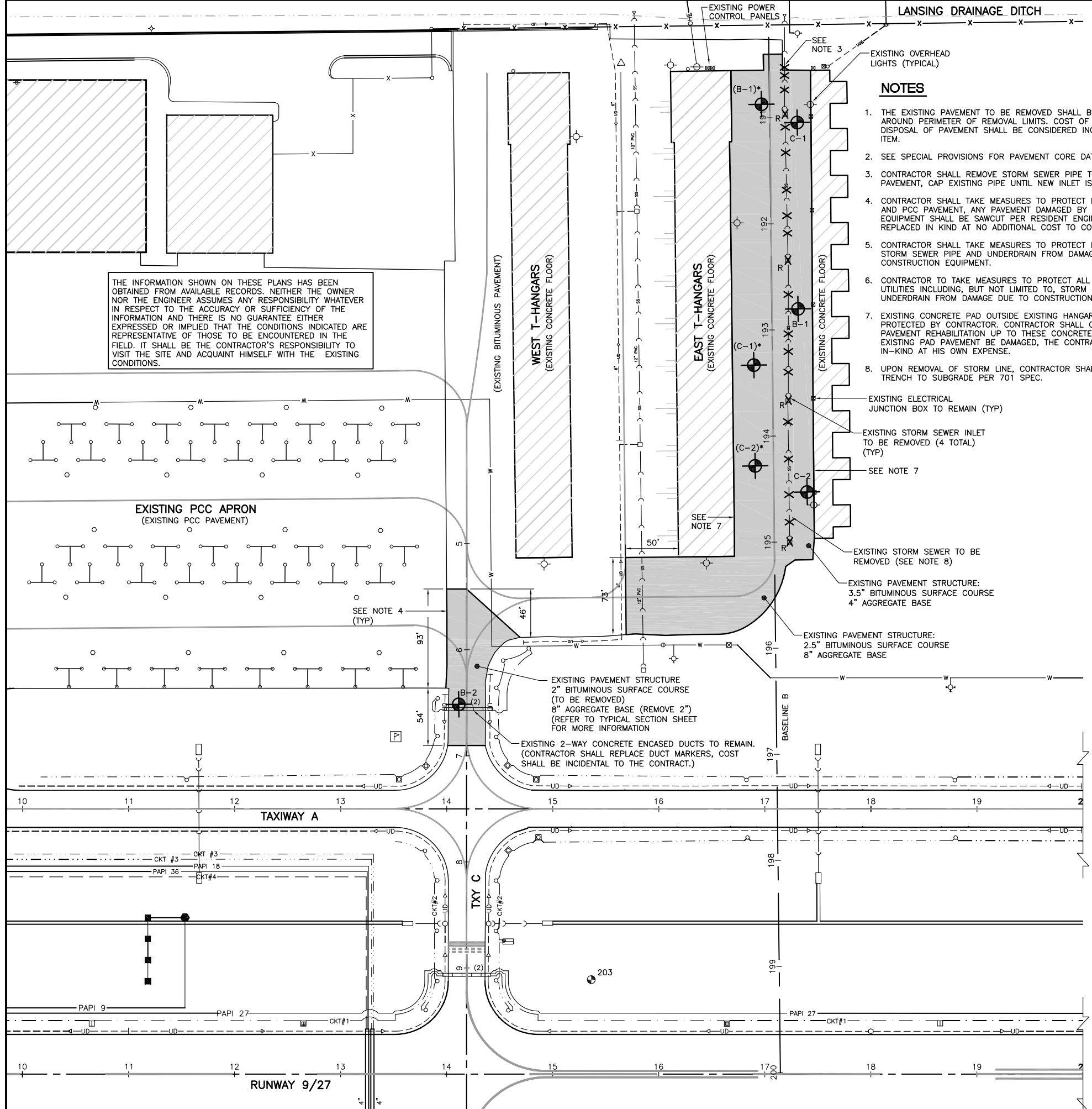
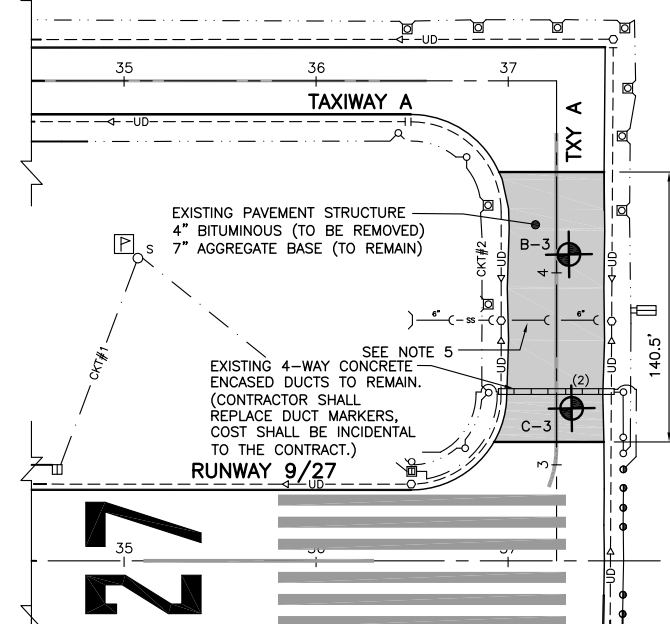
1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWS FULL DEPTH AROUND PERIMETER OF REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. SEE SPECIAL PROVISIONS FOR PAVEMENT CORE DATA AND BORING LOGS.
3. CONTRACTOR SHALL REMOVE STORM SEWER PIPE TO EDGE OF PAVEMENT, CAP EXISTING PIPE UNTIL NEW INLET IS INSTALLED.
4. CONTRACTOR SHALL TAKE MEASURES TO PROTECT EXISTING BITUMINOUS AND PCC PAVEMENT, ANY PAVEMENT DAMAGED BY CONTRACTORS EQUIPMENT SHALL BE SAWCUT PER RESIDENT ENGINEER LAYOUT AND REPLACED IN KIND AT NO ADDITIONAL COST TO CONTRACT.
5. CONTRACTOR SHALL TAKE MEASURES TO PROTECT EXISTING AND NEW STORM SEWER PIPE AND UNDERDRAIN FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.
6. CONTRACTOR TO TAKE MEASURES TO PROTECT ALL UNDERGROUND UTILITIES INCLUDING, BUT NOT LIMITED TO, STORM SEWER PIPE AND UNDERDRAIN FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.
7. EXISTING CONCRETE PAD OUTSIDE EXISTING HANGAR FACE SHALL BE PROTECTED BY CONTRACTOR. CONTRACTOR SHALL COMPLETE PROPOSED PAVEMENT REHABILITATION UP TO THESE CONCRETE PADS. SHOULD THE EXISTING PAD PAVEMENT BE DAMAGED, THE CONTRACTOR SHALL REPLACE IN-KIND AT HIS OWN EXPENSE.
8. UPON REMOVAL OF STORM LINE, CONTRACTOR SHALL FILL AND COMPACT TRENCH TO SUBGRADE PER 701 SPEC.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

GEOTECHNICAL LAYOUT TABLE

BORING NO.	ELEVATION	BITUMINOUS PAVEMENT THICKNESS (IN)	AGGREGATE BASE THICKNESS (IN)
B-1	613.30	3	4
B-2	614.50	4	10
B-3	615.88	4.5	5
C-1	613.33	4.75	4
C-2	613.35	3.5	4
C-3	615.87	4.25	4
(B-1)*	613.11	2.5	7.5
(C-1)*	613.39	2.2	7
(C-2)*	613.29	2.1	5

* SOIL BORINGS / CORES WERE TAKEN FROM PRIOR GEOTECHNICAL INVESTIGATION COMPLETE 12/13/2007



**LANSING MUNICIPAL AIRPORT
 LANSING, ILLINOIS
 REHABILITATE NORTH QUADRANT T-HANGAR
 PAVEMENTS & ADDITIONAL TAXIWAYS
 EXISTING CONDITIONS/
 PROPOSED REMOVALS**

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DESIGN BY:	AAO
DRAWN BY:	AAO
CHECKED BY:	AAO
APPROVED BY:	DKP
DATE:	02/28/14
JOB No:	13297-02-00
IL PROJECT:	IGQ-4297
S.B.G. PROJECT:	3-17-SPGP-XX
SHEET	8 OF 10 SHEETS