





ST. LOUIS  
DOWNTOWN AIRPORT

BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: G-002-FLP.DWG  
DESIGN BY: BSS 3/11/2025  
DRAWN BY: CWS 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

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**GENERAL NOTES:**

**QUANTITIES PAYMENT**  
WILL BE MADE UNDER THE ITEM NUMBERS, DESCRIPTIONS AND UNITS NOTED IN THE ABOVE TABLE IN ACCORDANCE WITH THE BASIS OF PAYMENT FOR EACH RESPECTIVE WORK ITEM COMPLETED AND ACCEPTED BY THE ENGINEER.

**CERTIFIED PAYROLLS**  
THE RESIDENT ENGINEER/TECHNICIAN CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

**MATERIAL CERTIFICATIONS**  
MATERIALS TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL. ALL MATERIALS TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. USE OF MATERIALS WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.

**FOR BID**





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6100 Archview Drive  
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RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
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IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
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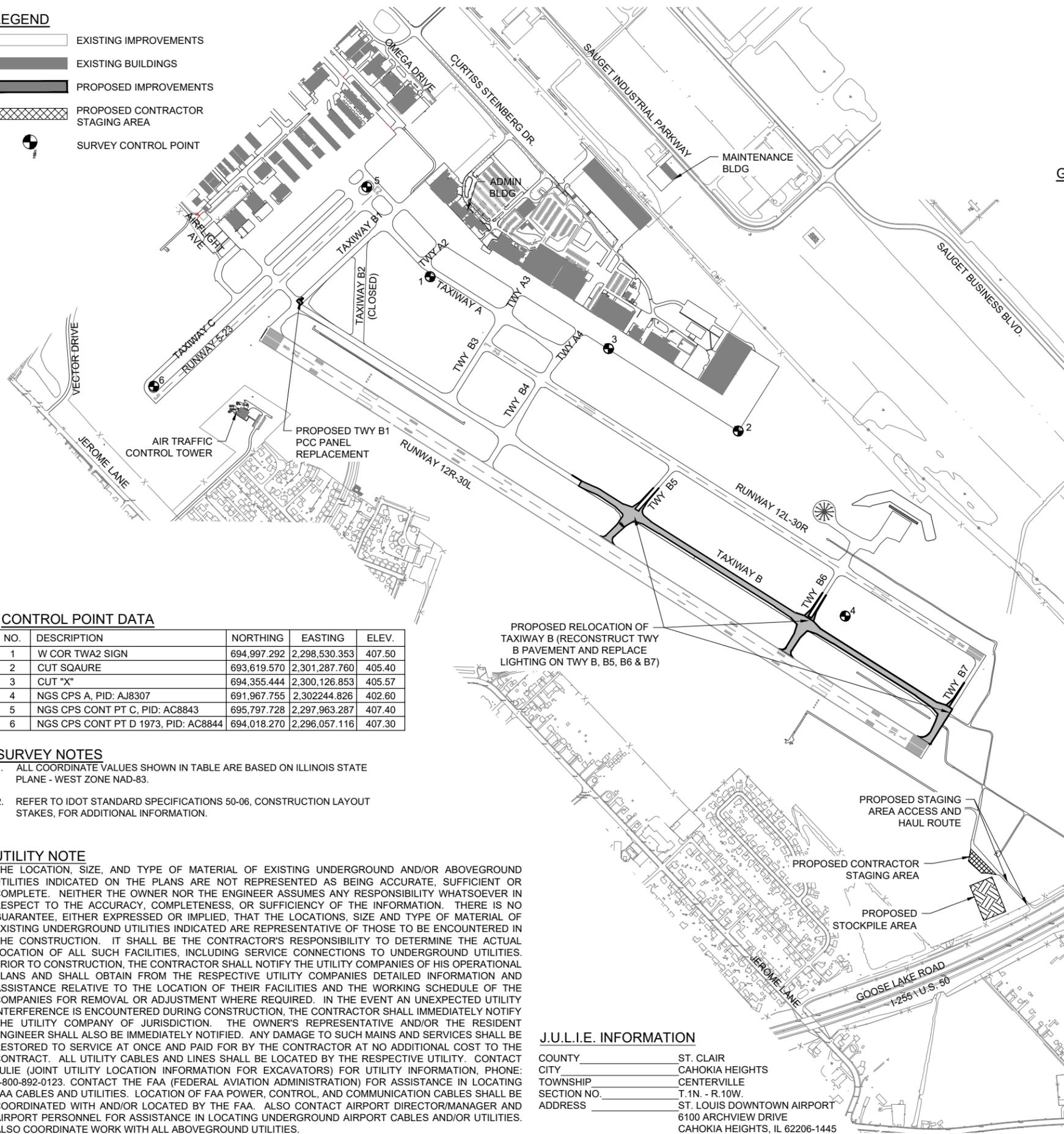
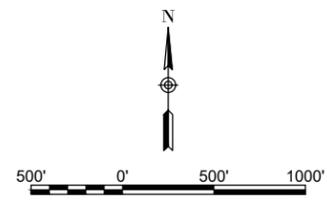
SHEET TITLE

SCOPE OF WORK

**FOR BID**

**LEGEND**

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED IMPROVEMENTS
- PROPOSED CONTRACTOR STAGING AREA
- SURVEY CONTROL POINT



**GENERAL NOTES**

1. THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS AND THEIR APPROXIMATE LOCATIONS AND LIMITS, FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. THIS SHEET SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE FOLLOWING PLAN SHEETS FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
2. WORK FOR THIS PROJECT SHALL CONSIST OF, BUT IS NOT LIMITED TO, THIS PROJECT CONSISTS OF RELOCATION/RECONSTRUCTION OF A PORTION OF TAXIWAY B AND ASSOCIATED TAXIWAY CONNECTORS. THE PROJECT INCLUDES PAVEMENT MILLING AND REMOVAL, EARTHWORK GRADING, AGGREGATE PLACEMENT, CONCRETE PAVING, BITUMINOUS PAVING, AIRFIELD LIGHTING AND SIGNAGE SYSTEMS INSTALLATION, PAVEMENT MARKING, EROSION CONTROL ITEMS AND INCIDENTALS.
3. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER/ TECHNICIAN IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS.
4. THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN.
5. THE CONTRACTOR IS NOT PERMITTED TO USE THE AIRPORT ENTRANCE DRIVE AND AUTO PARKING LOT FOR MATERIAL AND EQUIPMENT HAULING OR STORAGE. THE CONSTRUCTION ENTRANCE AS SHOWN ON THE SCOPE OF WORK AND/OR SAFETY PHASING PLAN ARE ONLY TO BE USED FOR THE PROJECT. ACCESS TO THE PROJECT FOR ALL HAULING OF MATERIALS AND EQUIPMENT SHALL BE RESTRICTED TO THE DESIGNATED CONSTRUCTION ENTRANCE.
6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT.
7. NO EQUIPMENT SHALL BE PERMITTED TO CROSS OR USE ANY EXISTING PAVEMENT OUTSIDE THE CONSTRUCTION LIMITS, GENERAL PROJECT AREA OR HAUL ROUTE.
8. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
9. UNLESS OTHERWISE NOTED, ALL DISTURBED AREAS OUTSIDE OF THE PROPOSED CONSTRUCTION LIMITS SHALL BE GRADED, SEEDED AND/OR HYDROMULCH SEEDED AT NO ADDITIONAL COST TO THE CONTRACT.
10. ALL WASTE MATERIAL SHALL BE HAULED FROM THE AIRPORT AND PROPERLY DISPOSED OF UNLESS OTHERWISE SPECIFIED HEREIN.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
12. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
13. THE CONTRACTOR SHALL COORDINATE WITH THE RESIDENT ENGINEER/ TECHNICIAN SO THEY MAY DEVELOP ONE SET OF REDLINED AS-BUILT DRAWINGS AT THE COMPLETION OF THE PROJECT.
14. CONTRACTOR SHALL NOTE THAT ALL AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING ANY WORK. ALL AREAS WHICH HAVE BEEN FARMED AND OR DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION, AND HAVE BEEN DISTURBED BY CONSTRUCTION ACTIVITY, SHALL BE CHISEL PLOWED (36" MAX.) OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER.)
15. UNLESS OTHERWISE NOTED, CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS TO BE REPAIRED, GRADED, AND MULCHED SEEDED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF THE HAUL ROUTE.
16. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER PERSONS FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
17. THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE FOR ALL AREAS AFFECTED BY HIS/HER WORK.

**CONTROL POINT DATA**

NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	W COR TWA2 SIGN	694,997.292	2,298,530.353	407.50
2	CUT SQAURE	693,619.570	2,301,287.760	405.40
3	CUT "X"	694,355.444	2,300,126.853	405.57
4	NGS CPS A, PID: AJ8307	691,967.755	2,302,244.826	402.60
5	NGS CPS CONT PT C, PID: AC8843	695,797.728	2,297,963.287	407.40
6	NGS CPS CONT PT D 1973, PID: AC8844	694,018.270	2,296,057.116	407.30

**SURVEY NOTES**

1. ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE - WEST ZONE NAD-83.
2. REFER TO IDOT STANDARD SPECIFICATIONS 50-06, CONSTRUCTION LAYOUT STAKES, FOR ADDITIONAL INFORMATION.

**UTILITY NOTE**

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

**J.U.L.I.E. INFORMATION**

COUNTY \_\_\_\_\_ ST. CLAIR  
 CITY \_\_\_\_\_ CAHOKIA HEIGHTS  
 TOWNSHIP \_\_\_\_\_ CENTERVILLE  
 SECTION NO. \_\_\_\_\_ T.1N. - R.10W.  
 ADDRESS \_\_\_\_\_ ST. LOUIS DOWNTOWN AIRPORT  
 6100 ARCHVIEW DRIVE  
 CAHOKIA HEIGHTS, IL 62206-1445

MAR 14, 2025 9:27 AM GROVED02594 I:\23\JOBS\23A0001D\CAD\AIRPORT\SHEET\2025 CPS-5078 COMBINED SHEETS\C-003-SOW

NO.	DATE	DESCRIPTION

NO.	DATE	DESCRIPTION	DES	DWN	REV

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SHEET TITLE

CONSTRUCTION SAFETY AND PHASING PLAN OVERALL

**GENERAL NOTES**

- AIRPORT SECURITY SHALL BE MAINTAINED THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL BE RESTRICTED TO THE DESIGNATED WORK AREAS. THE CONTRACTOR SHALL ENSURE THAT ACCESS POINTS USED BY CONSTRUCTION VEHICLES AND PERSONNEL ARE CLOSED WHEN NOT IN USE AND LOCKED AT THE END OF THE WORKING DAY TO PREVENT UNAUTHORIZED ACCESS TO THE AIRPORT MOVEMENT AREA.
- CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
- AT THE COMPLETION OF ALL CONSTRUCTION, THE HAUL ROUTE AND CONSTRUCTION EQUIPMENT PARKING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS PER THE SPECIFICATIONS.
- THE COSTS FOR PROVISION, PLACEMENT, MAINTENANCE AND REMOVAL OF BARRICADES, CLOSURE CROSSES, TRAFFIC CONTROL SIGNAGE, AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER TRAFFIC MAINTENANCE.
- THE COSTS FOR CONSTRUCTION, MAINTENANCE OF ACCESS GATE, HAUL ROUTE AND EQUIPMENT STAGING AREA, AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER MOBILIZATION.

**SAFETY PLAN COMPLIANCE DOCUMENT**

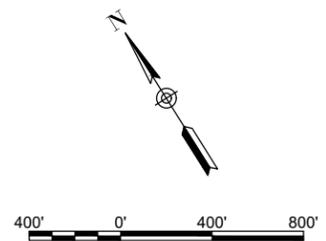
PRIOR TO THE ISSUANCE OF A CONSTRUCTION NOTICE-TO-PROCEED (NTP), THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING AND SUBMITTING A SAFETY PLAN COMPLIANCE DOCUMENT IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G, PARAGRAPH 2.4.2, OR EQUIVALENT SECTION IN SUBSEQUENT/CURRENT ISSUE. THE AIRPORT DIRECTOR SHALL APPROVE THIS DOCUMENT AND SUBMIT TO THE AIRPORT FOR APPROVAL PRIOR TO THE NTP ISSUANCE.

**SEQUENCE OF CONSTRUCTION**

- THIS PROJECT WILL BE BROKEN INTO SEPARATE WORK AREAS IN AN EFFORT TO MINIMIZE IMPACTS TO AIRPORT OPERATIONS. THE CONTRACTOR SHALL EXPEDITE WORK TO ENSURE THE AIRCRAFT MOVEMENT AREAS ARE OPEN WHEN POSSIBLE.
  - TAXIWAY CONNECTORS B5, B6 AND B7 SHALL BE CLOSED TO AIRCRAFT FOR A PORTION OF THE PROJECT. A PORTION OF TAXIWAY B SHALL BE CLOSED FOR THE DURATION OF THE PROJECT, AND THE INTERSECTION OF TAXIWAY B AND B1 SHALL BE CLOSED FOR A PORTION OF THE PROJECT. DURING INSTALLATION OF THE AIRFIELD ELECTRICAL HOMERUN PORTIONS OF TAXIWAY B AND ASSOCIATED CONNECTOR TAXIWAYS WILL BE CLOSED.
  - THE CONTRACTOR IS REQUIRED TO COMPLETE ALL WORK AREAS, HOWEVER:
    - WORK AREAS 1 AND 2 MAY NOT BE PERFORMED SIMULTANEOUSLY.
    - WORK AREAS 1A AND 3 MAY NOT BE PERFORMED SIMULTANEOUSLY.
    - WORK AREAS 2A AND 3 MAY NOT BE PERFORMED SIMULTANEOUSLY.
    - WORK AREAS 1B AND 1C MAY NOT BE PERFORMED SIMULTANEOUSLY.
    - WORK AREAS 2B AND 2C MAY NOT BE PERFORMED SIMULTANEOUSLY.
    - WORK AREA 4A SHALL BE PERFORMED WHILE WORK AREA 1A OR 2A IS BEING PERFORMED TO MINIMIZE RUNWAY DOWNTIME.
    - WORK AREAS 4B THROUGH 4F SHALL BE PERFORMED CONSECUTIVELY TO MINIMIZE TAXIWAY B DOWNTIME.
- NO DEVIATION FROM THESE PROVISIONS WILL BE ALLOWED UNLESS THE CONTRACTOR CAN PROVIDE A SIGNIFICANT BENEFIT TO THE OWNER.

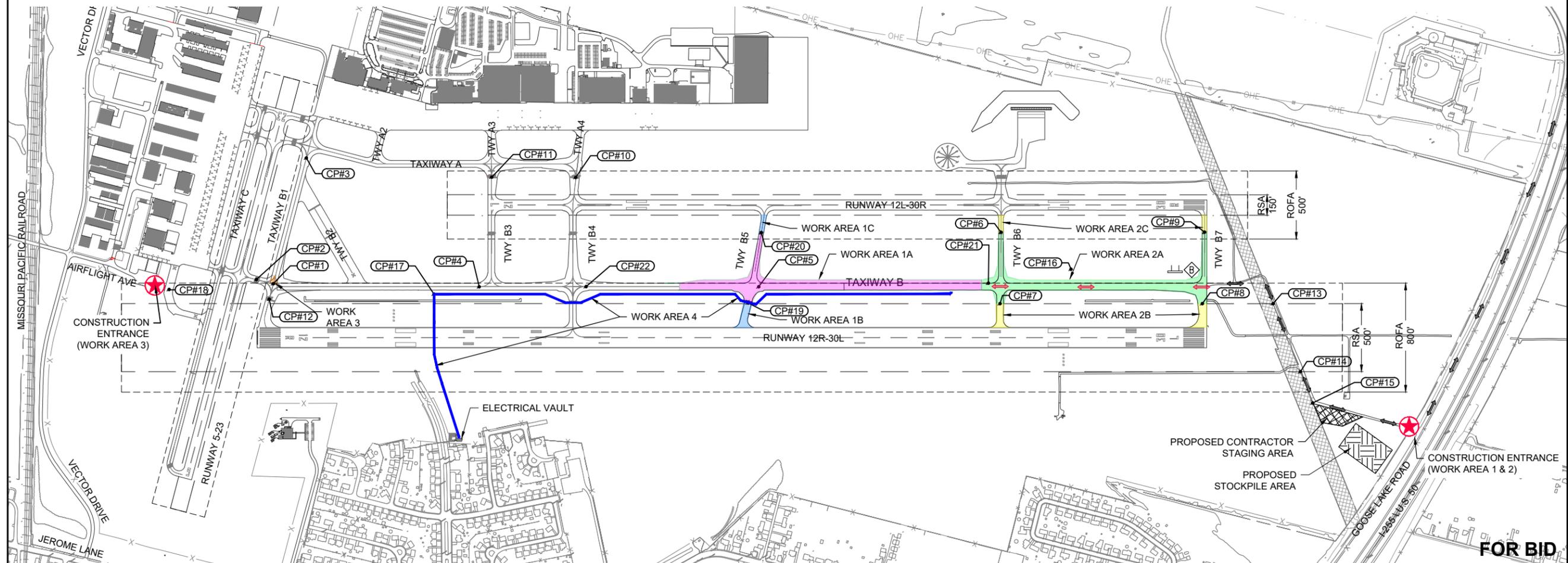
CRITICAL POINTS						
POINT #	LATITUDE	LONGITUDE	SE	AGL	AMSL	DESCRIPTION
1	038° 34' 29.09"	-090° 09' 50.30"	411'	25'	436'	EQUIPMENT
2	038° 34' 30.01"	-090° 09' 51.43"	405'	25'	430'	EQUIPMENT
3	038° 34' 35.53"	-090° 09' 41.50"	410'	2'	412'	BARRICADES
4	038° 34' 20.90"	-090° 09' 34.27"	411'	2'	413'	BARRICADES
5	038° 34' 10.04"	-090° 09' 12.32"	410'	2'	412'	BARRICADES
6	038° 34' 03.97"	-090° 08' 50.60"	404'	2'	406'	BARRICADES
7	038° 33' 59.63"	-090° 08' 54.20"	407'	25'	432'	EQUIPMENT
8	038° 33' 51.77"	-090° 08' 38.32"	406'	25'	431'	EQUIPMENT
9	038° 33' 56.08"	-090° 08' 34.61"	403'	2'	405'	BARRICADES
10	038° 34' 23.87"	-090° 09' 21.31"	401'	2'	403'	BARRICADES
11	038° 34' 27.15"	-090° 09' 27.88"	402'	2'	404'	BARRICADES
12	038° 34' 28.29"	-090° 09' 51.38"	411'	2'	413'	BARRICADES
13	038° 33' 49.02"	-090° 08' 32.76"	406'	25'	431'	HAUL ROUTE
14	038° 33' 43.77"	-090° 08' 33.97"	399'	25'	424'	HAUL ROUTE
15	038° 33' 41.37"	-090° 08' 34.55"	397'	25'	422'	STAGING AREA
16	038° 33' 58.98"	-090° 08' 46.93"	402'	25'	427'	WIND CONE
17	038° 34' 22.20"	-090° 09' 38.19"	410'	25'	435'	EQUIPMENT
18	038° 34' 32.78"	-090° 09' 58.77"	410'	25'	435'	STAGING AREA
19	038° 34' 09.46"	-090° 09' 13.95"	411'	2'	413'	BARRICADES
20	038° 34' 13.27"	-090° 09' 09.44"	406'	2'	408'	BARRICADES
21	038° 34' 01.34"	-090° 08' 54.11"	406'	2'	408'	BARRICADES
22	038° 34' 16.76"	-090° 09' 25.87"	410'	2'	412'	BARRICADES

DOWNTOWN GROUND FREQUENCY = 121.80



**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- PROPOSED HAUL ROUTE
- PROPOSED EQUIPMENT PARKING AREA
- EXISTING FENCE
- PROPOSED BARRICADES
- PROPOSED CLOSURE CROSS
- CONSTRUCTION SIGN
- CRITICAL POINT
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- AIRFIELD SECURITY GATE



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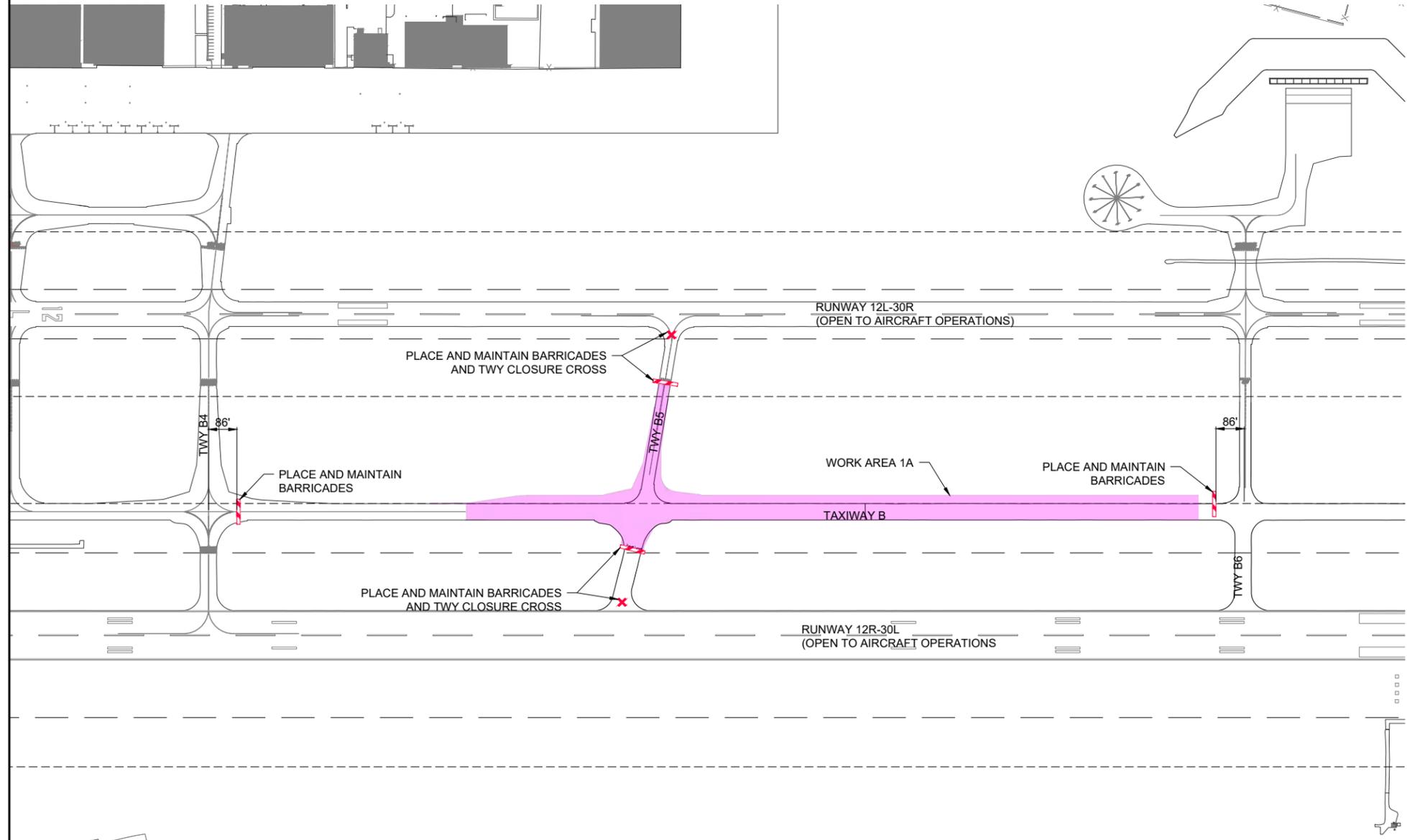
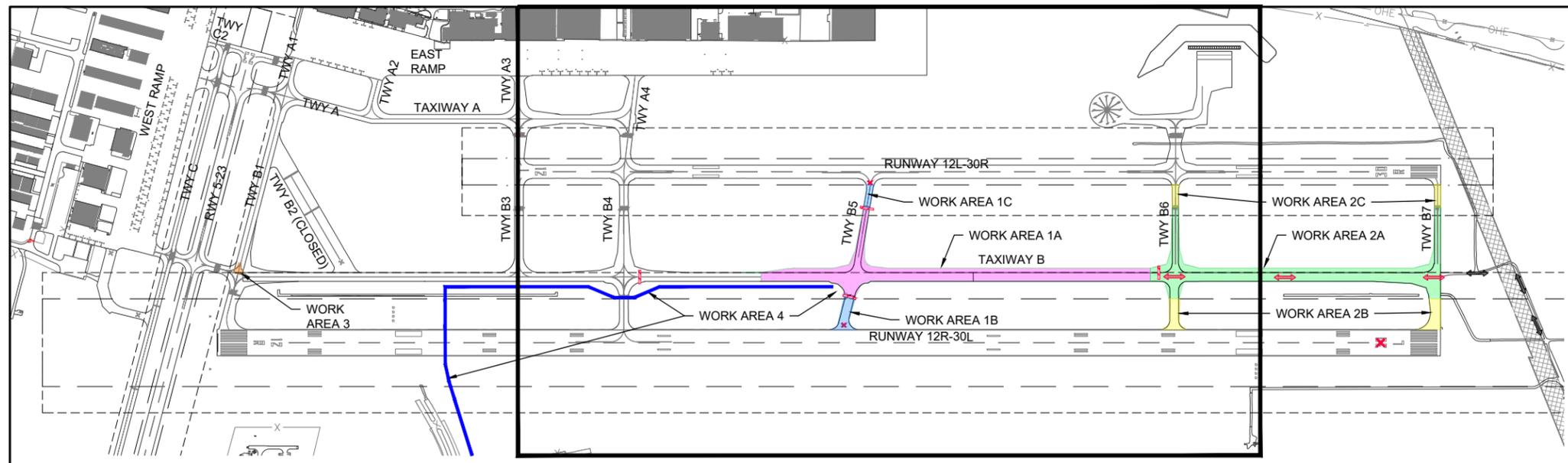
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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
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DRAWN BY: CWS 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

CONSTRUCTION SAFETY AND PHASING PLAN - WORK AREA 1A



**AREA 1A WORK TO BE ACCOMPLISHED**

1. THE WORK ITEMS TO BE COMPLETED IN THIS PHASE ARE TO INCLUDE EARTH EXCAVATION, DRAINAGE IMPROVEMENTS, PAVEMENT REMOVAL, INSTALL NEW BASE AND PCC PAVEMENT, AIRFIELD LIGHTING AND SIGNAGE SYSTEM INSTALLATION, PAVEMENT MARKING, SEEDING/MULCHING AND EROSION CONTROL.
2. WORK AREAS 1A AND 2A MAY NOT BE PERFORMED SIMULTANEOUSLY.
3. WORK AREAS 1A AND 3 MAY NOT BE PERFORMED SIMULTANEOUSLY.
4. WORK AREA 4A SHALL BE PERFORMED WHILE WORK AREA 1B IS BEING PERFORMED TO MINIMIZE RUNWAY DOWNTIME.
5. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
6. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROUTE, EQUIPMENT PARKING AREA, AND GATE ARE TO BE LEFT IN PLACE IN THEIR PRE-CONSTRUCTION CONDITION.
7. ANY UTILITY DISCONNECTED OR DAMAGED SHALL BE RECONNECTED OR REPAIRED WITHIN NORMAL CONSTRUCTION HOURS SUCH THAT NO SINGLE UTILITY LINE IS OUT OF SERVICE OVERNIGHT, UNLESS APPROVAL IS OBTAINED FROM THE OWNER PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND RESIDENT TECHNICIAN/ENGINEER IN ADVANCE IN ORDER TO SHUTDOWN THE UTILITIES FOR REROUTING IF REQUIRED.
8. PAVEMENTS SHALL BE CLEAN AND LIGHTING CIRCUITS SHALL BE OPERATIONAL, TO THE OWNER'S SATISFACTION BEFORE REOPENING TO AIRCRAFT TRAFFIC.

**OPERATIONAL SAFETY NOTES:**

1. FLAGPERSONS AND OR ESCORT WITH RADIOS SHALL BE REQUIRED TO CONTROL VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. NO CONSTRUCTION PERSONNEL/EQUIPMENT ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR TAXIWAY SAFETY AREA (TSA) WHEN PAVEMENTS ARE OPEN TO AIRCRAFT TRAFFIC. PAVEMENTS ARE TO BE KEPT FREE OF DEBRIS AT ALL TIMES. ANY DAMAGE TO PAVEMENTS BY THE CONTRACTOR'S FORCES SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL DEMONSTRATE THE ABILITY TO ACCESS THE CONSTRUCTION SITE WHILE MAINTAINING AIRFIELD SECURITY DURING ALL PHASES OF CONSTRUCTION.
3. ALL NOTAMS TO BE ISSUED SHALL BE ISSUED BY AIRPORT REPRESENTATIVES.
4. AIRPORT AND CONTRACTOR SHALL COORDINATE THE LOCATION OF UNDERGROUND UTILITIES WITHIN WORK AREAS PRIOR TO THE START OF CONSTRUCTION.

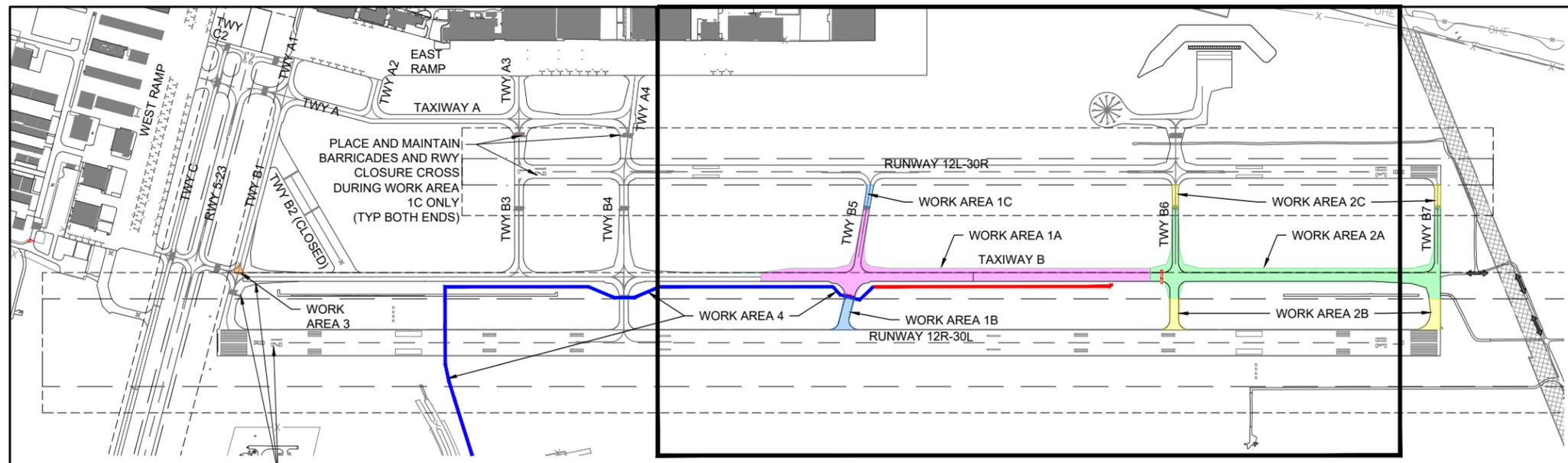
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NO.	DATE	DESCRIPTION		
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ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: G-004-SFY.DWG  
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REVIEWED BY: BSS 3/12/2025

SHEET TITLE

**CONSTRUCTION SAFETY AND PHASING PLAN - WORK AREAS 1B AND 1C**



**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- PROPOSED HAUL ROUTE
- PROPOSED EQUIPMENT PARKING AREA
- EXISTING FENCE
- PROPOSED BARRICADES
- PROPOSED CLOSURE CROSS
- CONSTRUCTION SIGN
- CRITICAL POINT
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- AIRFIELD SECURITY GATE

PLACE AND MAINTAIN BARRICADES AND RWY CLOSURE CROSS DURING WORK AREA 1C ONLY (TYP BOTH ENDS)

PARALLEL RUNWAYS SHALL NOT BE SHUT DOWN AT THE SAME TIME. FOR EXAMPLE; WORK WITHIN RWY 12L-30R SAFETY AREA MUST BE COMPLETED AND RUNWAY OPENED FOR OPERATION BEFORE CLOSING AND WORKING WITHIN THE SAFETY AREA OF RWY 12R-30L.

DURING CONSTRUCTION OF WORK AREA 1B, THE TEMPORARY CLOSURE OF RUNWAY 12R-30L MAY NEED TO BE TEMPORARILY SUSPENDED IN ORDER TO ACCOMMODATE THE ARRIVAL OR DEPARTURE OF A LARGER AIRCRAFT CHARTER OPERATION. TYPICALLY THESE OPERATIONS ARE KNOWN WITH A 24-HOUR PRIOR NOTICE AND OCCUR 1-2 TIMES PER WEEK DURING NORMAL WORKING HOURS. ONCE AWARE OF THE AIRCRAFT'S POTENTIAL OPERATION TIME WINDOW, THE AIRPORT WILL NOTIFY THE CONTRACTOR WITH THE EXPECTATION THAT THE CONTRACTOR'S OPERATIONS WILL BE TEMPORARILY MOVED OUTSIDE OF THE RUNWAY SAFETY AREA TO ACCOMMODATE THE AIRCRAFT OPERATION, WITH WORK ALLOWED TO RESUME IMMEDIATELY FOLLOWING.

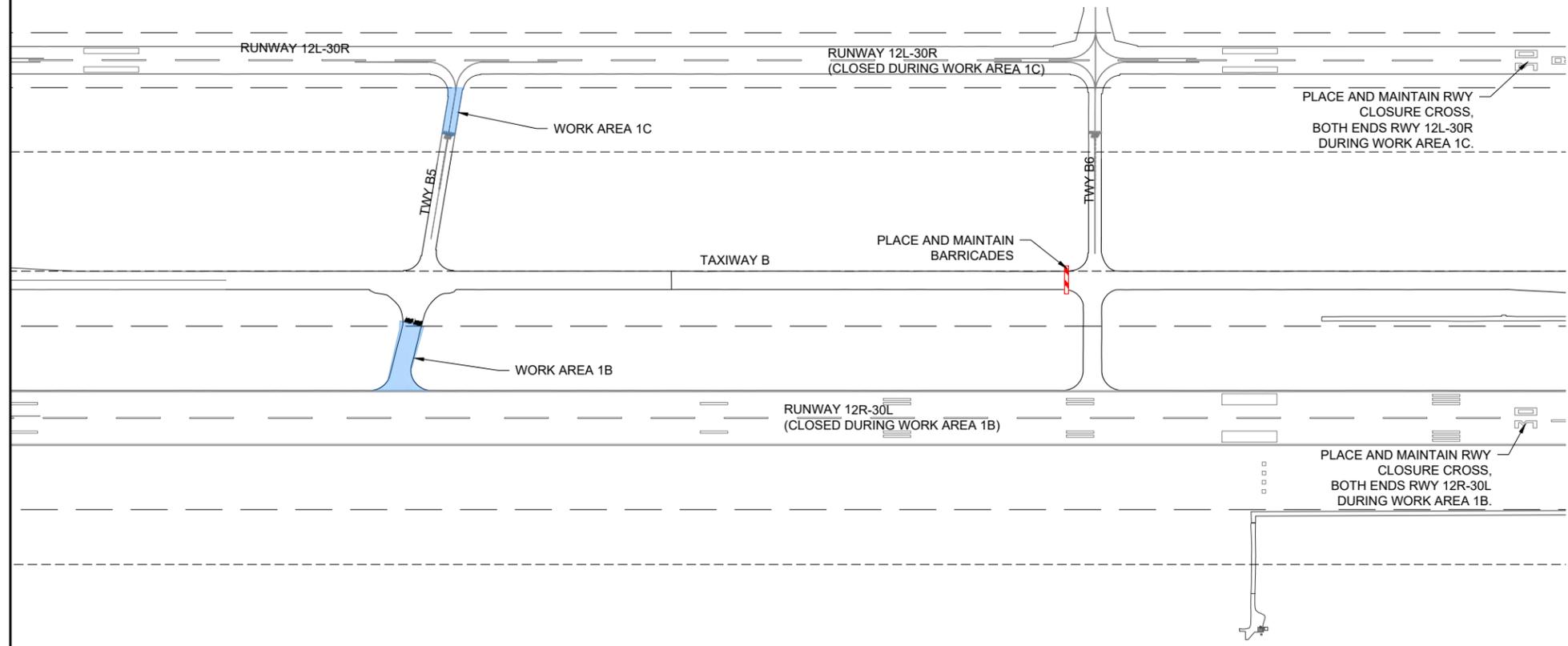
**AREAS 1B & 1C WORK TO BE ACCOMPLISHED**

1. THE WORK ITEMS TO BE COMPLETED IN THIS PHASE ARE TO INCLUDE EARTH EXCAVATION, DRAINAGE IMPROVEMENTS, PAVEMENT REMOVAL, INSTALL NEW BASE AND PCC PAVEMENT, AIRFIELD LIGHTING AND SIGNAGE SYSTEM INSTALLATION, PAVEMENT MARKING, SEEDING/MULCHING AND EROSION CONTROL.
2. WORK AREAS 1B AND 1C MAY NOT BE PERFORMED SIMULTANEOUSLY.
3. WORK AREA 4A SHALL BE PERFORMED WHILE WORK AREA 1B OR 2B IS BEING PERFORMED TO MINIMIZE RUNWAY DOWNTIME.
4. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
5. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROUTE, EQUIPMENT PARKING AREA, AND GATE ARE TO BE LEFT IN PLACE IN THEIR PRE-CONSTRUCTION CONDITION.
6. ANY UTILITY DISCONNECTED OR DAMAGED SHALL BE RECONNECTED OR REPAIRED WITHIN NORMAL CONSTRUCTION HOURS SUCH THAT NO SINGLE UTILITY LINE IS OUT OF SERVICE OVERNIGHT, UNLESS APPROVAL IS OBTAINED FROM THE OWNER PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND RESIDENT TECHNICIAN/ENGINEER IN ADVANCE IN ORDER TO SHUTDOWN THE UTILITIES FOR REROUTING IF REQUIRED.
7. PAVEMENTS SHALL BE CLEAN AND LIGHTING CIRCUITS SHALL BE OPERATIONAL, TO THE OWNER'S SATISFACTION BEFORE REOPENING TO AIRCRAFT TRAFFIC.

**OPERATIONAL SAFETY NOTES:**

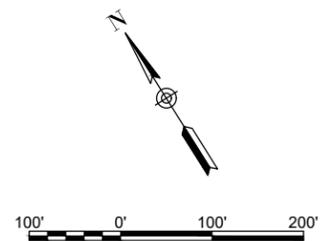
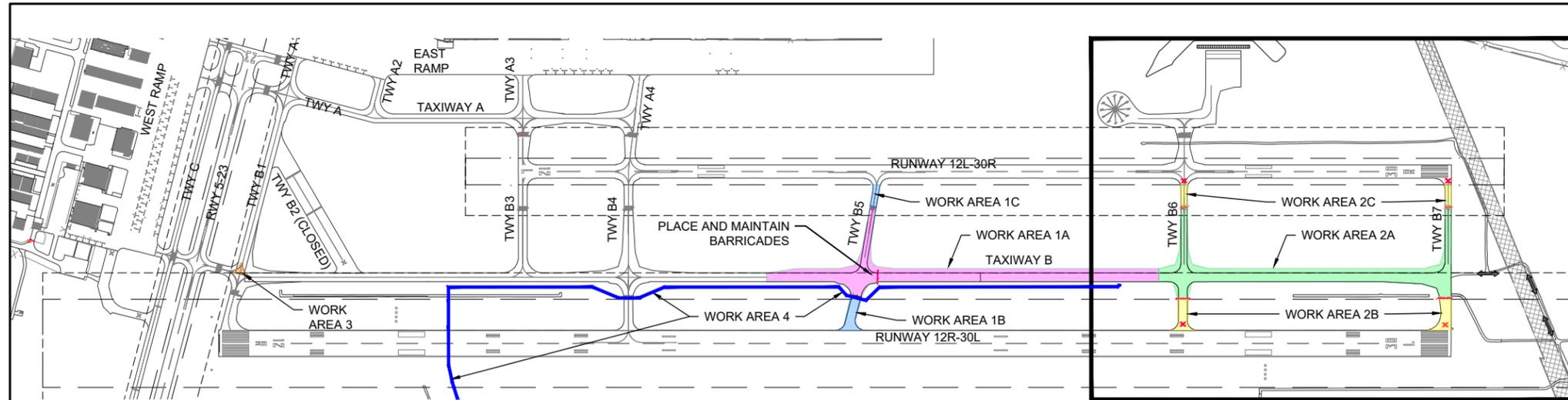
1. FLAGPERSONS AND OR ESCORT WITH RADIOS SHALL BE REQUIRED TO CONTROL VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. NO CONSTRUCTION PERSONNEL/EQUIPMENT ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR TAXIWAY SAFETY AREA (TSA) WHEN PAVEMENTS ARE OPEN TO AIRCRAFT TRAFFIC. PAVEMENTS ARE TO BE KEPT FREE OF DEBRIS AT ALL TIMES. ANY DAMAGE TO PAVEMENTS BY THE CONTRACTOR'S FORCES SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL DEMONSTRATE THE ABILITY TO ACCESS THE CONSTRUCTION SITE WHILE MAINTAINING AIRFIELD SECURITY DURING ALL PHASES OF CONSTRUCTION.
3. ALL NOTAMS TO BE ISSUED SHALL BE ISSUED BY AIRPORT REPRESENTATIVES.
4. AIRPORT AND CONTRACTOR SHALL COORDINATE THE LOCATION OF UNDERGROUND UTILITIES WITHIN WORK AREAS PRIOR TO THE START OF CONSTRUCTION.

**FOR BID**



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SHEET TITLE



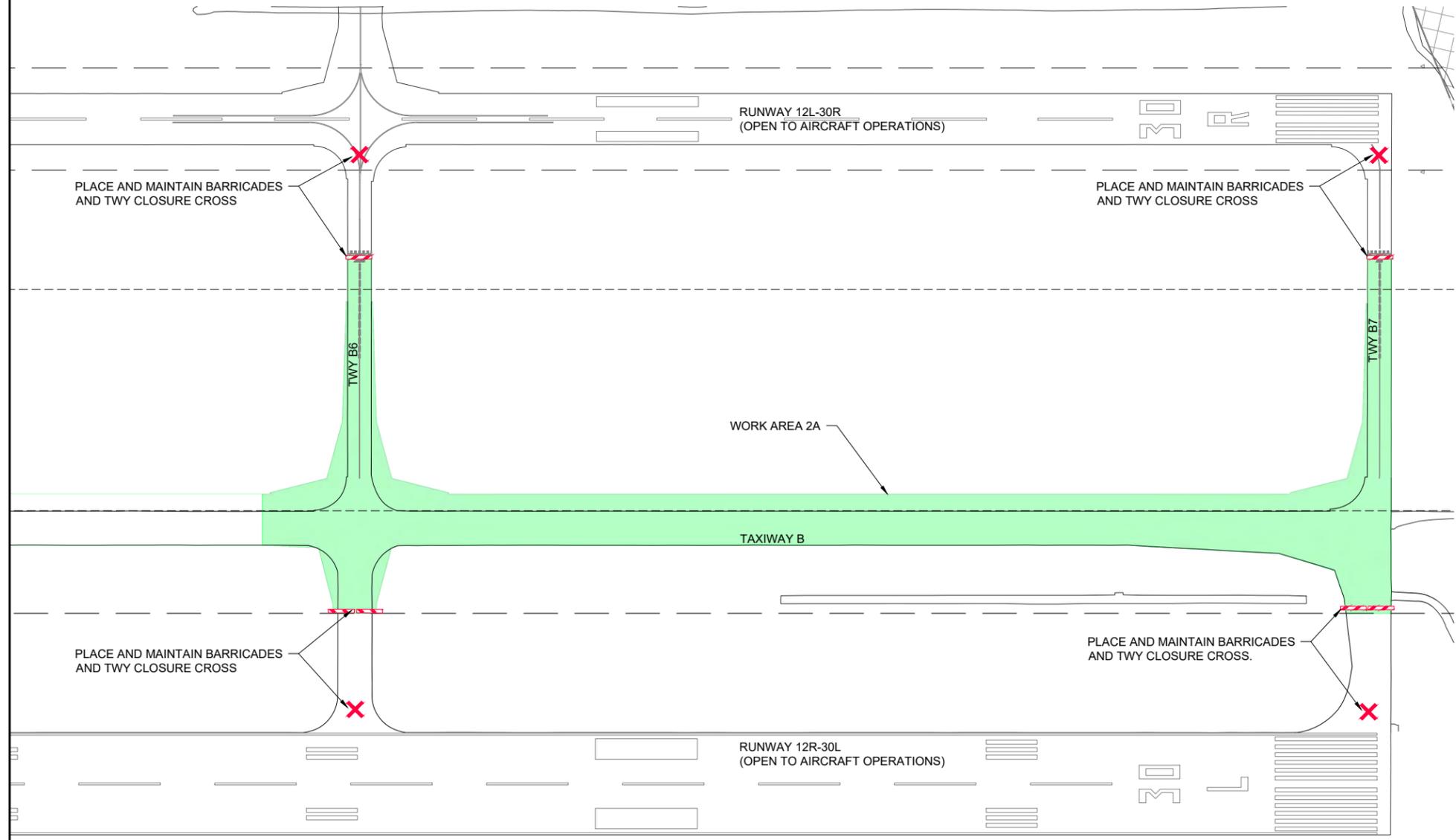
- LEGEND**
- EXISTING IMPROVEMENTS
  - PROPOSED IMPROVEMENTS
  - PROPOSED HAUL ROUTE
  - PROPOSED EQUIPMENT PARKING AREA
  - EXISTING FENCE
  - PROPOSED BARRICADES
  - PROPOSED CLOSURE CROSS
  - CONSTRUCTION SIGN
  - CRITICAL POINT
  - RUNWAY SAFETY AREA
  - RUNWAY OBJECT FREE AREA
  - AIRFIELD SECURITY GATE

**AREA 2A WORK TO BE ACCOMPLISHED**

- THE WORK ITEMS TO BE COMPLETED IN THIS PHASE ARE TO INCLUDE EARTH EXCAVATION, DRAINAGE IMPROVEMENTS, PAVEMENT REMOVAL, INSTALL NEW BASE AND PCC PAVEMENT, AIRFIELD LIGHTING AND SIGNAGE SYSTEM INSTALLATION, PAVEMENT MARKING, SEEDING/MULCHING AND EROSION CONTROL.
- WORK AREAS 1A AND 2A MAY NOT BE PERFORMED SIMULTANEOUSLY.
- WORK AREAS 2A AND 3 MAY NOT BE PERFORMED SIMULTANEOUSLY.
- AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
- AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROUTE, EQUIPMENT PARKING AREA, AND GATE ARE TO BE LEFT IN PLACE IN THEIR PRE-CONSTRUCTION CONDITION.
- ANY UTILITY DISCONNECTED OR DAMAGED SHALL BE RECONNECTED OR REPAIRED WITHIN NORMAL CONSTRUCTION HOURS SUCH THAT NO SINGLE UTILITY LINE IS OUT OF SERVICE OVERNIGHT, UNLESS APPROVAL IS OBTAINED FROM THE OWNER PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND RESIDENT TECHNICIAN/ENGINEER IN ADVANCE IN ORDER TO SHUTDOWN THE UTILITIES FOR REROUTING IF REQUIRED.
- PAVEMENTS SHALL BE CLEAN AND LIGHTING CIRCUITS SHALL BE OPERATIONAL, TO THE OWNER'S SATISFACTION BEFORE REOPENING TO AIRCRAFT TRAFFIC.

**OPERATIONAL SAFETY NOTES:**

- FLAGPERSONS AND OR ESCORT WITH RADIOS SHALL BE REQUIRED TO CONTROL VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. NO CONSTRUCTION PERSONNEL/EQUIPMENT ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR TAXIWAY SAFETY AREA (TSA) WHEN PAVEMENTS ARE OPEN TO AIRCRAFT TRAFFIC. PAVEMENTS ARE TO BE KEPT FREE OF DEBRIS AT ALL TIMES. ANY DAMAGE TO PAVEMENTS BY THE CONTRACTOR'S FORCES SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR SHALL DEMONSTRATE THE ABILITY TO ACCESS THE CONSTRUCTION SITE WHILE MAINTAINING AIRFIELD SECURITY DURING ALL PHASES OF CONSTRUCTION.
- ALL NOTAMS TO BE ISSUED SHALL BE ISSUED BY AIRPORT REPRESENTATIVES.
- AIRPORT AND CONTRACTOR SHALL COORDINATE THE LOCATION OF UNDERGROUND UTILITIES WITHIN WORK AREAS PRIOR TO THE START OF CONSTRUCTION.



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**FOR BID**



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

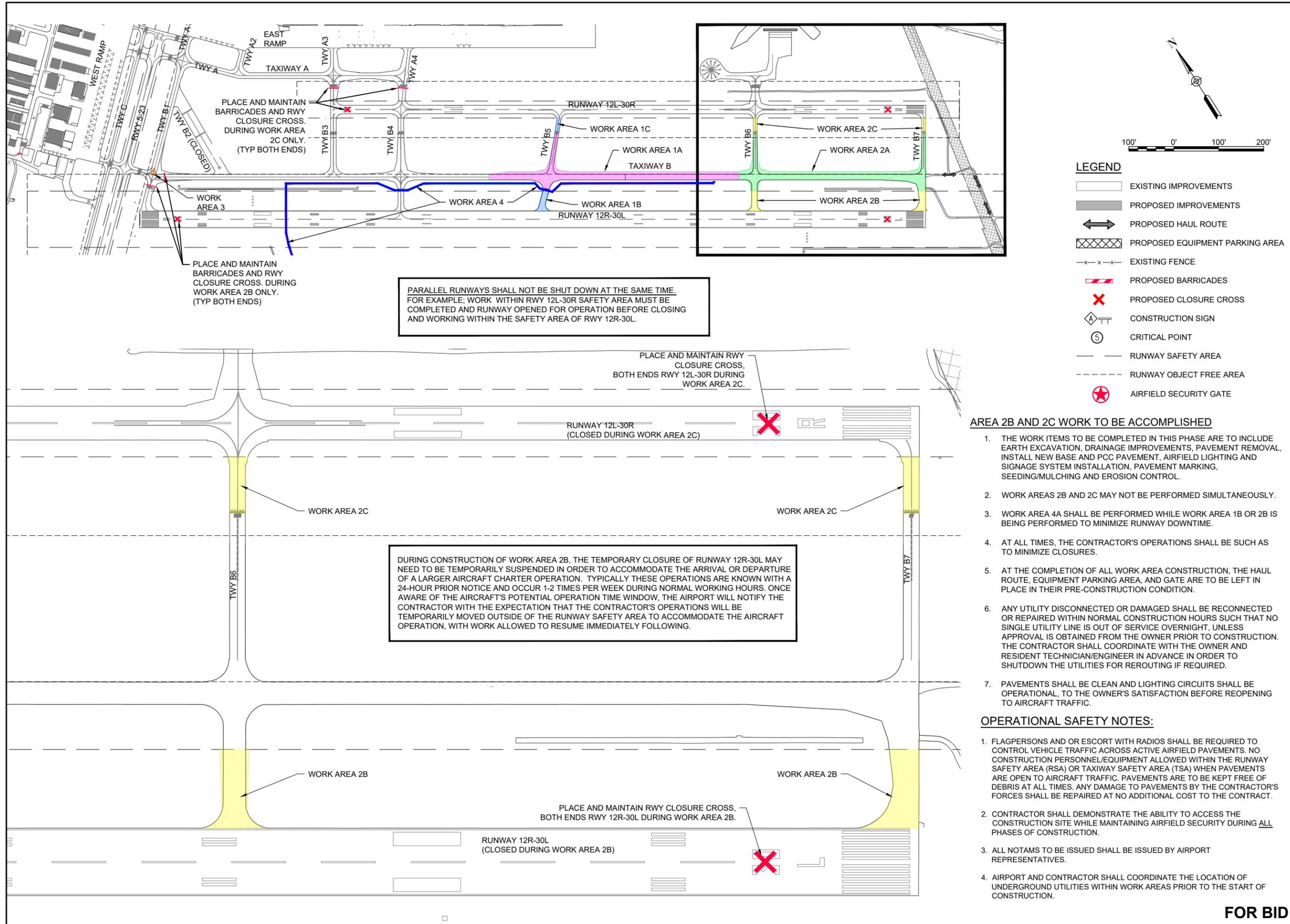
RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

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SHEET TITLE

CONSTRUCTION SAFETY AND PHASING PLAN - WORK AREAS 2B AND 2C



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SHEET TITLE

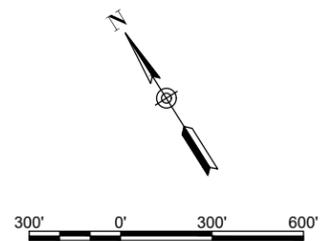
CONSTRUCTION SAFETY AND PHASING PLAN - WORK AREA 4

**AREA 4 WORK TO BE ACCOMPLISHED**

1. THE WORK ITEMS TO BE COMPLETED IN THIS PHASE ARE TO INCLUDE INSTALLATION OF THE AIRFIELD LIGHTING HOMERUN FROM THE TAXIWAY B WORK AREA BACK TO THE AIRFIELD ELECTRICAL VAULT BUILDING.
2. WORK AREA 4A SHALL BE PERFORMED WHILE WORK AREA 1B OR 2B IS BEING PERFORMED TO MINIMIZE RUNWAY DOWNTIME.
3. WORK AREAS 4B THROUGH 4F SHALL BE PERFORMED CONSECUTIVELY TO MINIMIZE TAXIWAY B DOWNTIME.
4. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
5. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROUTE, EQUIPMENT PARKING AREA, AND GATE ARE TO BE LEFT IN PLACE IN THEIR PRE-CONSTRUCTION CONDITION.
6. ANY UTILITY DISCONNECTED OR DAMAGED SHALL BE RECONNECTED OR REPAIRED WITHIN NORMAL CONSTRUCTION HOURS SUCH THAT NO SINGLE UTILITY LINE IS OUT OF SERVICE OVERNIGHT, UNLESS APPROVAL IS OBTAINED FROM THE OWNER PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND RESIDENT TECHNICIAN/ENGINEER IN ADVANCE IN ORDER TO SHUTDOWN THE UTILITIES FOR REROUTING IF REQUIRED.
7. PAVEMENTS SHALL BE CLEAN AND LIGHTING CIRCUITS SHALL BE OPERATIONAL, TO THE OWNER'S SATISFACTION BEFORE REOPENING TO AIRCRAFT TRAFFIC.

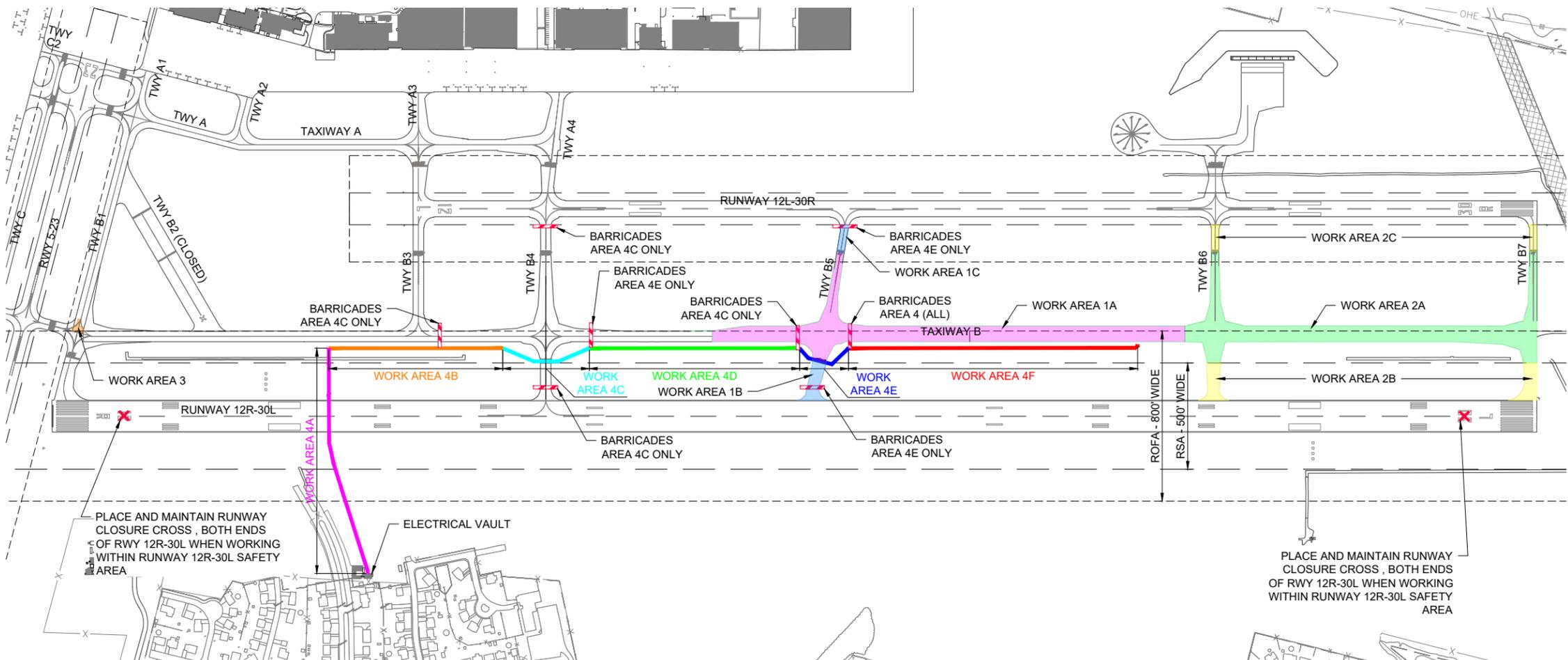
**OPERATIONAL SAFETY NOTES:**

1. FLAGPERSONS AND OR ESCORT WITH RADIOS SHALL BE REQUIRED TO CONTROL VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. NO CONSTRUCTION PERSONNEL/EQUIPMENT ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR TAXIWAY SAFETY AREA (TSA) WHEN PAVEMENTS ARE OPEN TO AIRCRAFT TRAFFIC. PAVEMENTS ARE TO BE KEPT FREE OF DEBRIS AT ALL TIMES. ANY DAMAGE TO PAVEMENTS BY THE CONTRACTOR'S FORCES SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL DEMONSTRATE THE ABILITY TO ACCESS THE CONSTRUCTION SITE WHILE MAINTAINING AIRFIELD SECURITY DURING ALL PHASES OF CONSTRUCTION.
3. ALL NOTAMS TO BE ISSUED SHALL BE ISSUED BY AIRPORT REPRESENTATIVES.
4. AIRPORT AND CONTRACTOR SHALL COORDINATE THE LOCATION OF UNDERGROUND UTILITIES WITHIN WORK AREAS PRIOR TO THE START OF CONSTRUCTION.



**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- PROPOSED HAUL ROUTE
- PROPOSED EQUIPMENT PARKING AREA
- EXISTING FENCE
- PROPOSED BARRICADES
- PROPOSED CLOSURE CROSS
- CONSTRUCTION SIGN
- CRITICAL POINT
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- AIRFIELD SECURITY GATE



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**FOR BID**



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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DRAWN BY: CWS 3/11/2025  
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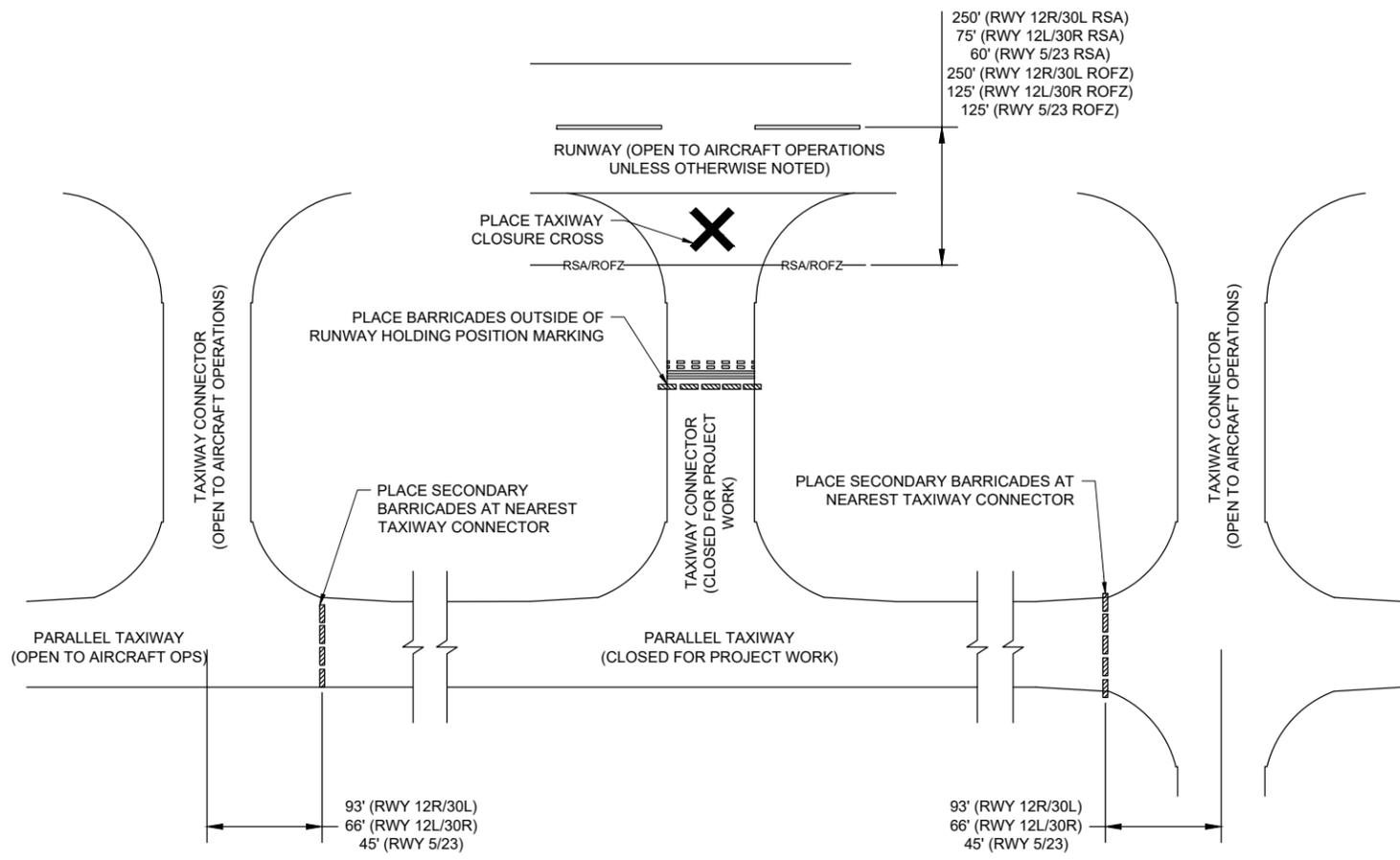
SHEET TITLE

CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 1

**SAFETY NOTES**

- FOLLOWING ARE THE CONSTRUCTION SAFETY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET AND THIS SHEET.
- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 93' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY AIRPORT OPERATIONS AREA WILL BE PERMITTED UNLESS PROPERLY MARKED. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT. HOWEVER OTHER EQUIPMENT TALLER THAN 25' MAY BE PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEP, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE (150' FOR RUNWAY 5-23) OR WITHIN 93' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 7 DAYS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED BY THE AIRPORT MANAGER TO THE APPROPRIATE FLIGHT SERVICE STATION.
- UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.

**FOR BID**



**PLACEMENT OF BARRICADES AROUND WORK AREAS**  
NOT TO SCALE





**ST. LOUIS DOWNTOWN AIRPORT**  
BI-STATE DEVELOPMENT  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

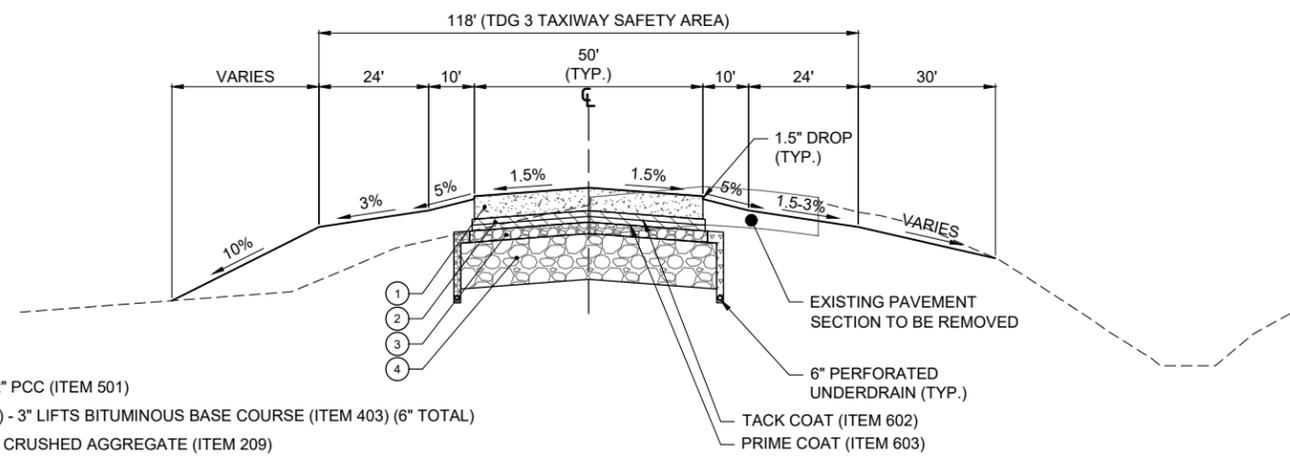
IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO. DATE DESCRIPTION  
DES DWN REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-301-TYP.DWG  
DESIGN BY: JRH 3/11/2025  
DRAWN BY: JRH 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

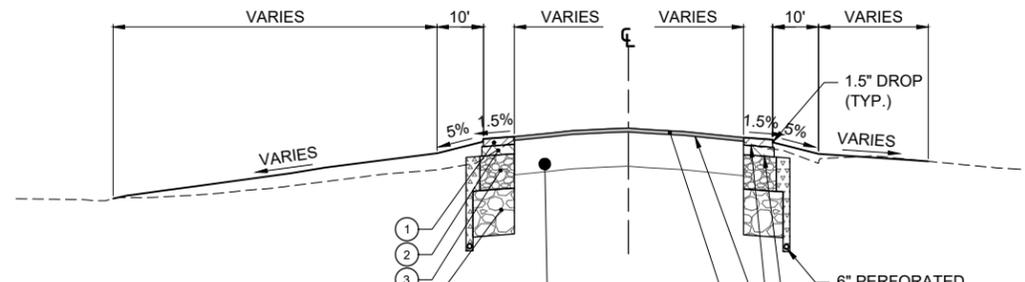
TYPICAL SECTIONS - SHEET 1



- ① 12" PCC (ITEM 501)
- ② (2) - 3" LIFTS BITUMINOUS BASE COURSE (ITEM 403) (6" TOTAL)
- ③ 6" CRUSHED AGGREGATE (ITEM 209)
- ④ 24" OVERSIZE AGGREGATE (ITEM 208)

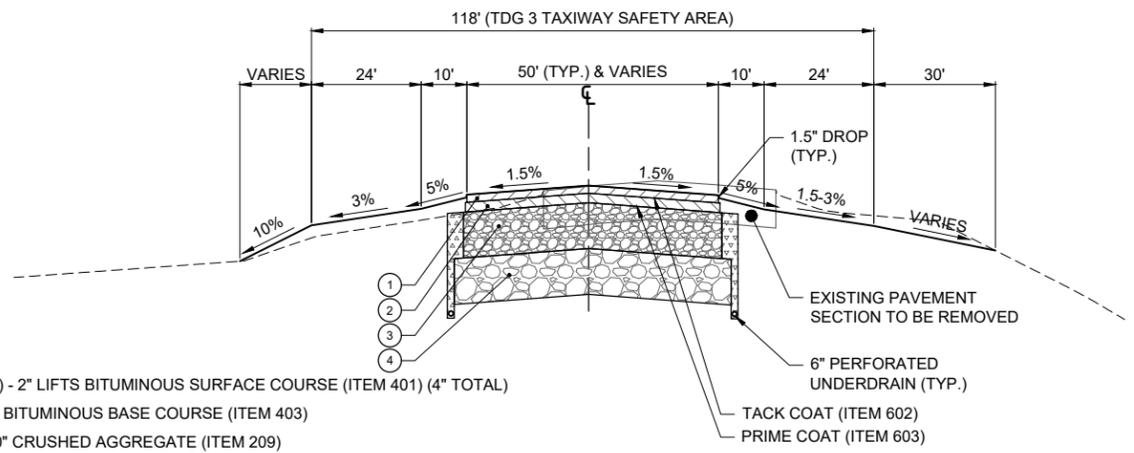
**TYPICAL SECTION A-A - PROPOSED TAXIWAY B**  
NOT TO SCALE

**NOTE:**  
BASED ON BORINGS CONDUCTED ON 7/27/22 AND HISTORICAL PAVEMENT DATA, THE EXISTING PAVEMENT TO BE REMOVED CONSISTS OF:  
 • APPROX. 8" OF CONCRETE ON 6" OF CRUSHED AGGREGATE FROM STA. 131+05 TO STA. 143+22.  
 • APPROX. 15" OF ASPHALT PLACED DIRECTLY ON THE SUBGRADE FROM STA. 143+22 TO STA. 155+00.  
 • APPROX. 11" OF ASPHALT ON 9" OF CRUSHED AGGREGATE FROM STA. 155+00 TO STA. 170+00



- ① (2) - 2" LIFTS BITUMINOUS SURFACE COURSE (ITEM 401) (4" TOTAL)
- ② 5" BITUMINOUS BASE COURSE (ITEM 403)
- ③ 18" CRUSHED AGGREGATE (ITEM 209)
- ④ 24" OVERSIZE AGGREGATE (ITEM 208)

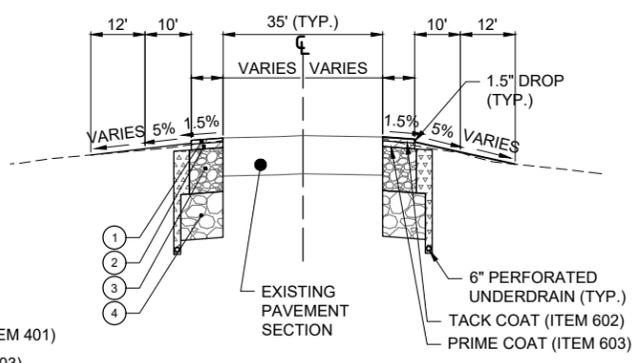
**TYPICAL SECTION C-C - PROPOSED TAXIWAY B5 SOUTH - FILLETS**  
NOT TO SCALE



- ① (2) - 2" LIFTS BITUMINOUS SURFACE COURSE (ITEM 401) (4" TOTAL)
- ② 5" BITUMINOUS BASE COURSE (ITEM 403)
- ③ 20" CRUSHED AGGREGATE (ITEM 209)
- ④ 24" OVERSIZE AGGREGATE (ITEM 208)

**TYPICAL SECTION B-B - PROPOSED TAXIWAY B TRANSITION**  
NOT TO SCALE

**NOTE:**  
BASED ON BORINGS CONDUCTED ON 7/27/22 AND HISTORICAL PAVEMENT DATA, THE EXISTING PAVEMENT TO BE REMOVED CONSISTS OF:  
 • APPROX. 8" OF CONCRETE ON 6" OF CRUSHED AGGREGATE FROM STA. 131+05 TO STA. 143+22.  
 • APPROX. 15" OF ASPHALT PLACED DIRECTLY ON THE SUBGRADE FROM STA. 143+22 TO STA. 155+00.  
 • APPROX. 11" OF ASPHALT ON 9" OF CRUSHED AGGREGATE FROM STA. 155+00 TO STA. 170+00



- ① 2" BITUMINOUS SURFACE COURSE (ITEM 401)
- ② 3" BITUMINOUS BASE COURSE (ITEM 403)
- ③ 23" CRUSHED AGGREGATE (ITEM 209)
- ④ 24" OVERSIZE AGGREGATE (ITEM 208)

**TYPICAL SECTION D-D - PROPOSED TAXIWAY B5 NORTH - FILLETS**  
NOT TO SCALE

HMA MIXTURE REQUIREMENTS					
APPLICATION	DESIGN AIR VOIDS	AC / PG	AGG. QUALITY	MAX. RAP	DENSITY ACCEPTANCE
AR401613 BIT. SURF. COURSE-METHOD I, SUPERPAVE	3.0% @ Ndesign = 50	SBS PG 70-22	A	0%	MAT: NUCLEAR GAUGE PER 401-6.1 JOINT: CORES PER 401-6.1
AR403614 BIT. BASE COURSE-METHOD II, SUPERPAVE	3.0% @ Ndesign = 50	SBS PG 70-22	B OR BETTER	30% PER 403-3.4	MAT: NUCLEAR GAUGE PER 403-6.1 JOINT: CORES PER 403-6.1

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MAR 14, 2025 9:32 AM GROVED2594 I:\23\JOBS\23A0001\DCAD\AIRPORT\REPORT\SHEET\2025 CPS-5078 COMBINED SHEETS\C-301-TYP



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

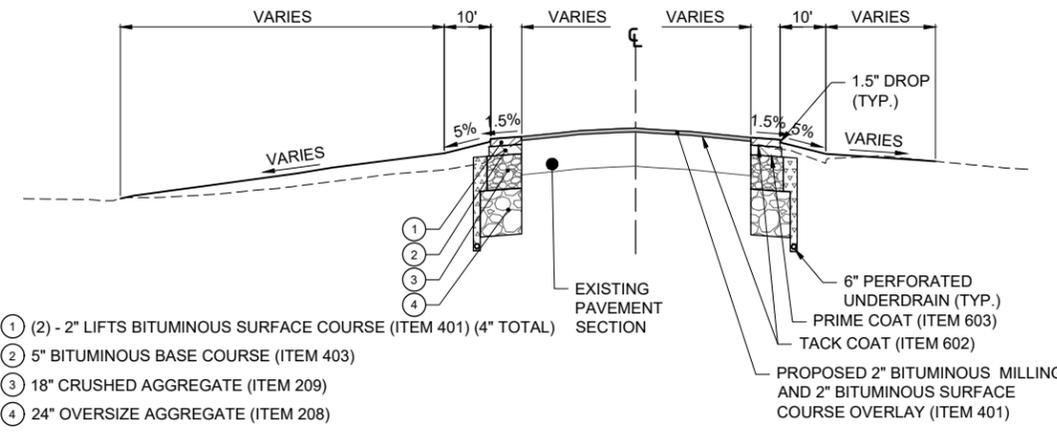
IDA NO.: CPS-5078  
CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
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DESIGN BY: JRH 3/11/2025  
DRAWN BY: JRH 3/11/2025  
REVIEWED BY: BSS 3/12/2025

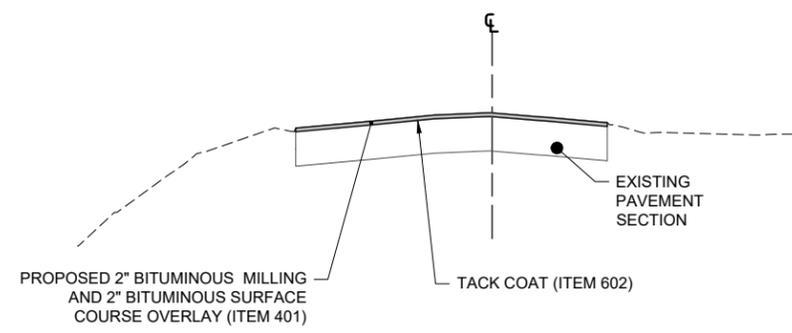
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TYPICAL SECTIONS - SHEET 2

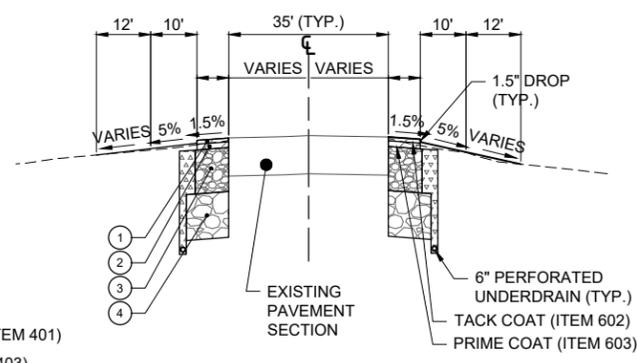


**TYPICAL SECTION E-E - PROPOSED TAXIWAY B6 SOUTH - FILLETS**  
NOT TO SCALE

- ① (2) - 2" LIFTS BITUMINOUS SURFACE COURSE (ITEM 401) (4" TOTAL)
- ② 5" BITUMINOUS BASE COURSE (ITEM 403)
- ③ 18" CRUSHED AGGREGATE (ITEM 209)
- ④ 24" OVERSIZE AGGREGATE (ITEM 208)

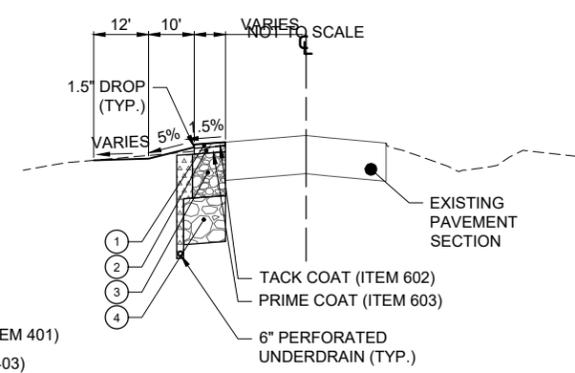


**TYPICAL SECTION G-G - PROPOSED TAXIWAY B7 SOUTH - MILL & OVERLAY**



**TYPICAL SECTION F-F - PROPOSED TAXIWAY B6 NORTH - FILLETS**  
NOT TO SCALE

- ① 2" BITUMINOUS SURFACE COURSE (ITEM 401)
- ② 3" BITUMINOUS BASE COURSE (ITEM 403)
- ③ 23" CRUSHED AGGREGATE (ITEM 209)
- ④ 24" OVERSIZE AGGREGATE (ITEM 208)



**TYPICAL SECTION H-H - PROPOSED TAXIWAY B7 NORTH - FILLET**  
NOT TO SCALE

- ① 2" BITUMINOUS SURFACE COURSE (ITEM 401)
- ② 3" BITUMINOUS BASE COURSE (ITEM 403)
- ③ 23" CRUSHED AGGREGATE (ITEM 209)
- ④ 24" OVERSIZE AGGREGATE (ITEM 208)

HMA MIXTURE REQUIREMENTS					
APPLICATION	DESIGN AIR VOIDS	AC / PG	AGG. QUALITY	MAX. RAP	DENSITY ACCEPTANCE
AR401613 BIT. SURF. COURSE-METHOD I, SUPERPAVE	3.0% @ Ndesign = 50	SBS PG 70-22	A	0%	MAT: NUCLEAR GAUGE PER 401-6.1 JOINT: CORES PER 401-6.1
AR403614 BIT. BASE COURSE-METHOD II, SUPERPAVE	3.0% @ Ndesign = 50	SBS PG 70-22	B OR BETTER	30% PER 403-3.4	MAT: NUCLEAR GAUGE PER 403-6.1 JOINT: CORES PER 403-6.1

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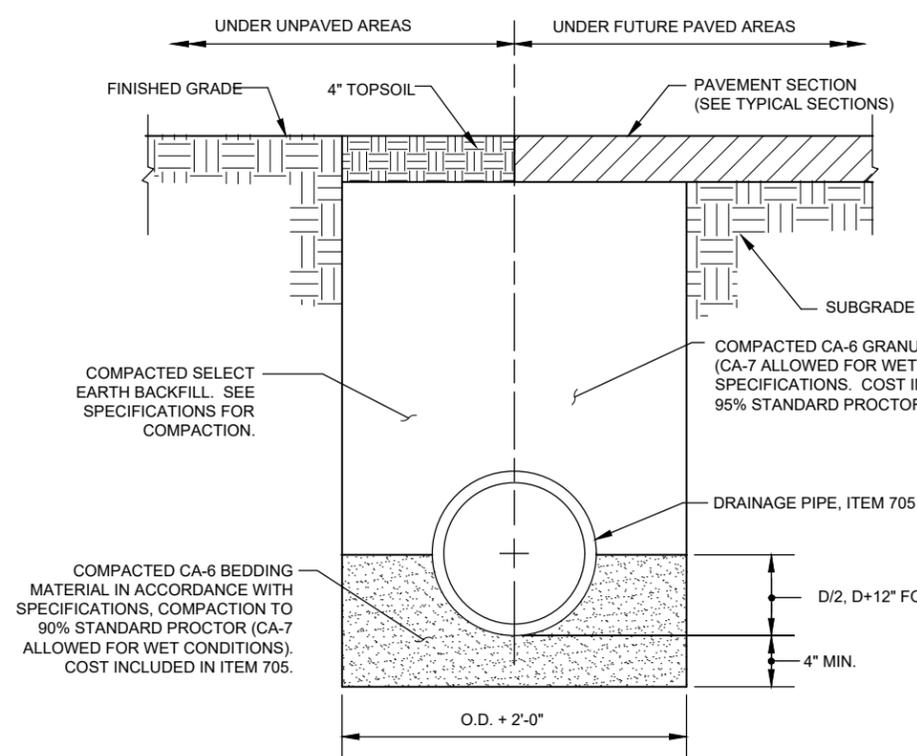

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DESIGN BY: JRH 3/11/2025  
DRAWN BY: JRH 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

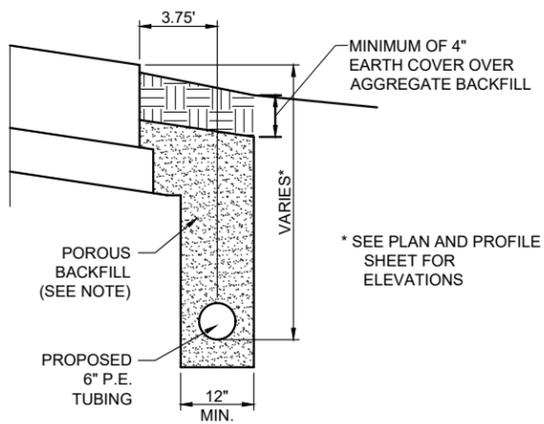
DRAINAGE DETAILS - SHEET 1

**FOR BID**



- NOTES:**
1. UNSUITABLE MATERIAL ENCOUNTERED DURING PLACEMENT OF BEDDING SHALL BE REMOVED AND REPLACED.
  2. WITHIN 3 FEET OF PAVED AREA, GRANULAR BACKFILL IS TO BE USED INSTEAD OF EARTH BACKFILL.
  3. AT CONTRACTOR'S OPTION IDOT CONTROLLED LOW STRENGTH MATERIAL WITH A HIGH EARLY STRENGTH, "FLASH FILL", MAY BE USED INSTEAD OF GRANULAR TRENCH BACKFILL UNDER PAVEMENTS AT NO ADDITIONAL COST TO THE CONTRACT.

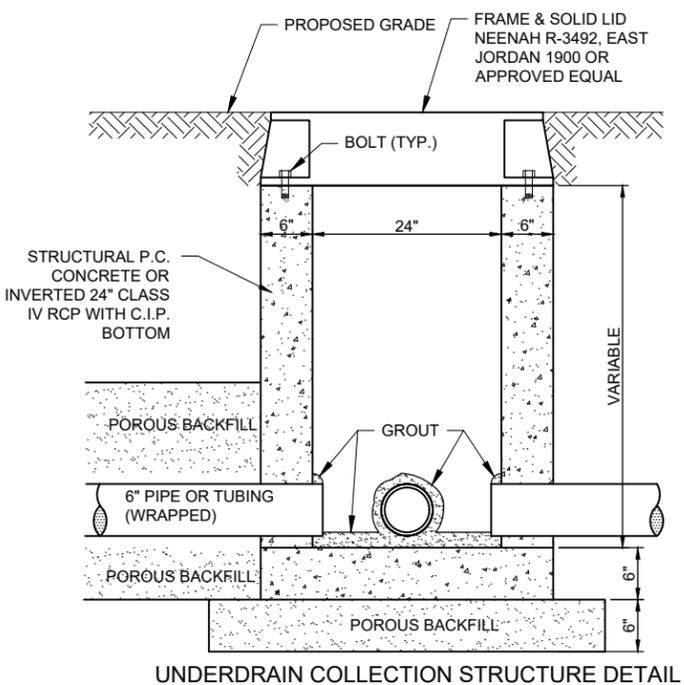
**PIPE TRENCH DETAIL**  
NOT TO SCALE



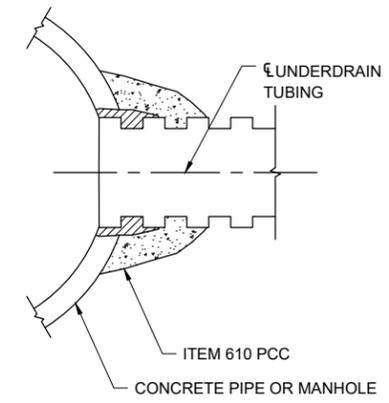
**UNDERDRAIN DETAIL**  
NOT TO SCALE

NOTE:  
POROUS BACKFILL SHALL CONFORM TO THE REQUIREMENTS FOR IDOT CA-14 OR IDOT CA-16 AND WILL BE CONSIDERED INCIDENTAL TO AR705506 6" PERF. UNDERDRAIN AND NO ADDITIONAL COMPENSATION ALLOWED. CONTRACTOR SHALL PLACE AND CONSOLIDATE THE POROUS BACKFILL TO THE SATISFACTION OF THE RESIDENT ENGINEER/TECHNICIAN.

- UNDERDRAIN NOTES**
1. THE CONTRACTOR SHALL INSTALL THE PROPOSED 6" P.E. TUBING UNDERDRAINS TO THE DEPTH AND GRADES SHOWN ON THE PLANS. THE UNDERDRAINS SHALL BE INSTALLED AFTER THE LIME SUBGRADE PROCESSING HAS BEEN COMPLETED.
  2. THE 6" P.E. TUBING SHALL BE CAPPED AT THE ENDS WHICH DO NOT CONNECT INTO EXISTING STRUCTURES.
  3. CONNECTING UNDERDRAINS TO EXISTING STRUCTURES SHALL BE INCLUDED IN THE COST OF THE UNDERDRAINS THEMSELVES, AND MAY INCLUDE CORING INTO THE EXISTING STRUCTURE WALL AND GROUTING THE UNDERDRAIN IN PLACE.
  4. THE TRENCH SHALL BE BACKFILLED AND COMPACTED WITH POROUS BACKFILL NO. 1 MATERIAL. THE TRENCH LOCATED IN THE PROPOSED PAVEMENT AREAS WILL BE BACKFILLED AS SHOWN IN THE DETAIL ON THIS SHEET. THE TRENCH LOCATED IN TURF AREAS SHALL BE BACKFILLED UP TO WITHIN 12" OF THE EXISTING GROUND ELEVATION. THE REMAINING 12" OF TRENCH WILL BE BACKFILLED AND COMPACTED WITH EARTH MATERIAL.
  5. POROUS BACKFILL SHALL CONFORM TO THE REQUIREMENTS FOR IDOT CA-14 OR IDOT CA-16 AND WILL BE CONSIDERED INCIDENTAL TO AR705506 6" PERF. UNDERDRAIN AND NO ADDITIONAL COMPENSATION ALLOWED. CONTRACTOR SHALL PLACE AND CONSOLIDATE THE POROUS BACKFILL TO THE SATISFACTION OF THE RESIDENT ENGINEER/TECHNICIAN.



**UNDERDRAIN COLLECTION STRUCTURE DETAIL**  
NOT TO SCALE



**STORM SEWER CONCRETE COLLAR AND GROUT CONNECTION**  
NOT TO SCALE

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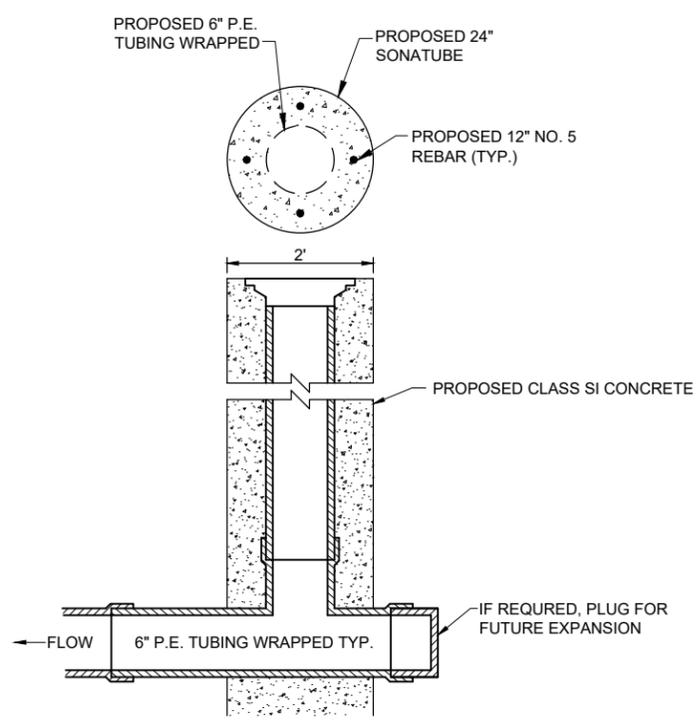

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-301-TYP.DWG  
DESIGN BY: JRH 3/11/2025  
DRAWN BY: JRH 3/11/2025  
REVIEWED BY: BSS 3/12/2025

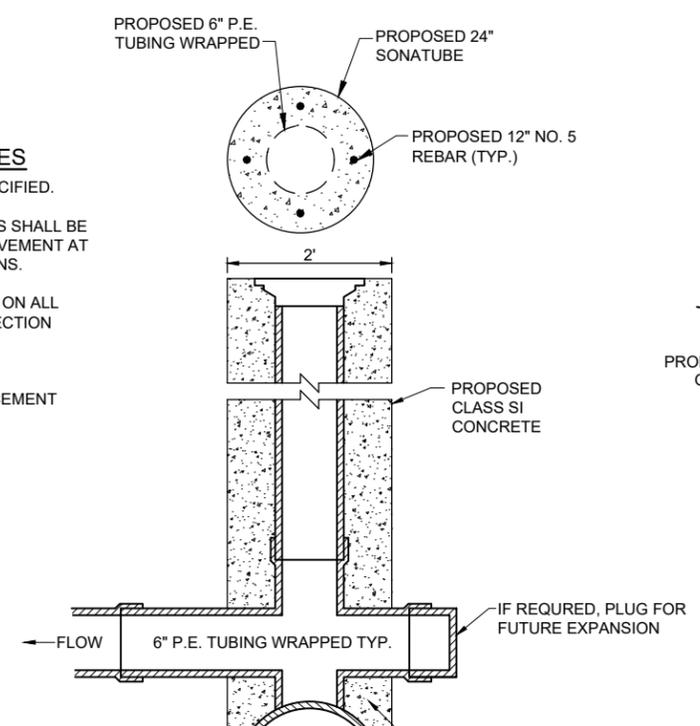
SHEET TITLE

DRAINAGE DETAILS - SHEET 2

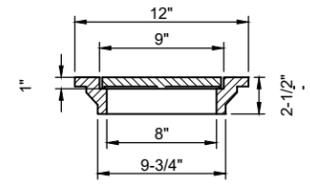
**FOR BID**



**INSPECTION HOLE-TYPE A**  
NOT TO SCALE



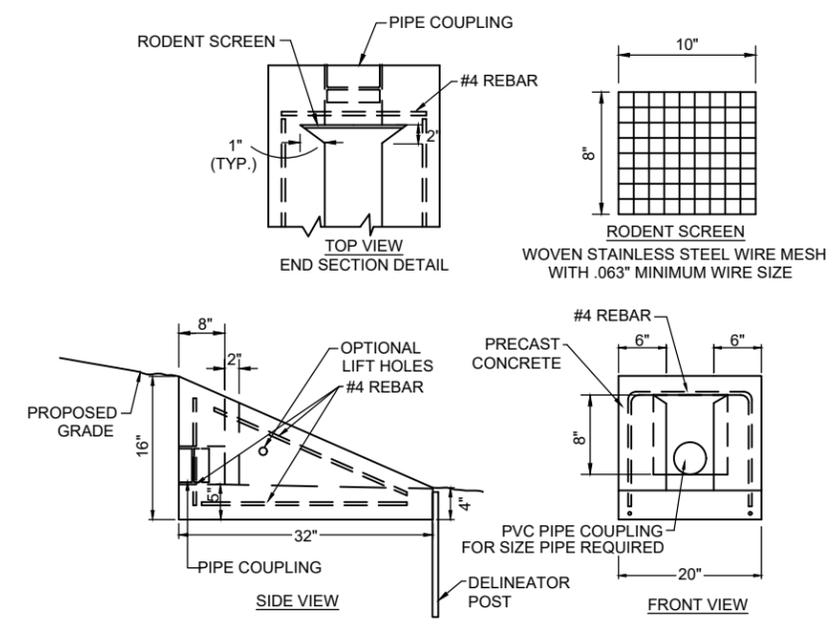
**INSPECTION HOLE-TYPE B**  
NOT TO SCALE



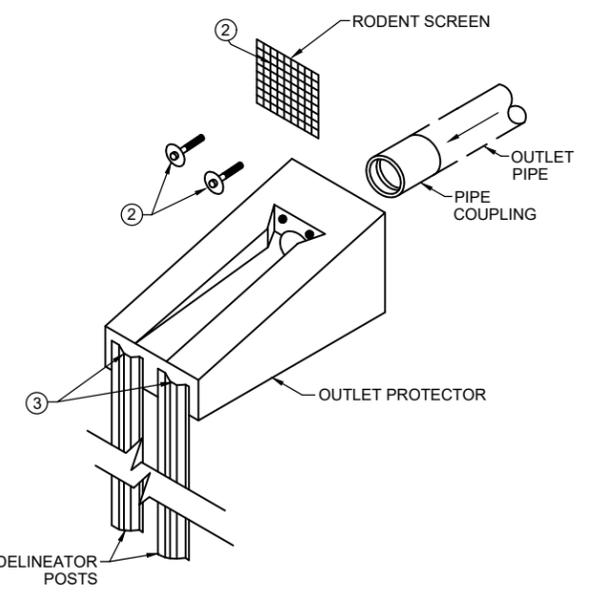
**CAST IRON FRAME AND GRATE**  
NEENAH R-6450-AG,  
EAST JORDAN 6206 DRAINAGE GRATE,  
OR APPROVED EQUAL  
NOTE: CONTRACTOR TO VERIFY  
FRAME/GRATE WILL WORK WITH PROPOSED  
PIPE PRIOR TO ORDERING.

**INSPECTION HOLE NOTES**

1. DIAMETER OF PIPE AS SPECIFIED.
2. TOP OF INSPECTION HOLES SHALL BE 0.10' BELOW ADJACENT PAVEMENT AT LOCATION SHOWN ON PLANS.
3. 1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF INSPECTION HOLES.
4. THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE.



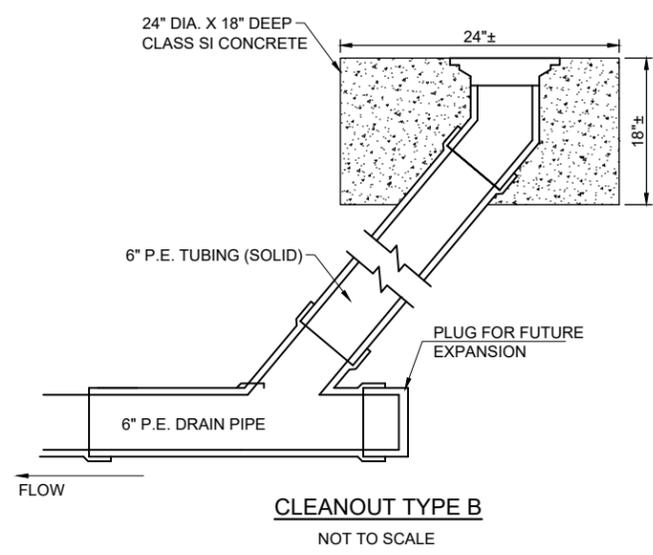
**UNDERDRAIN END SECTION**  
NOT TO SCALE



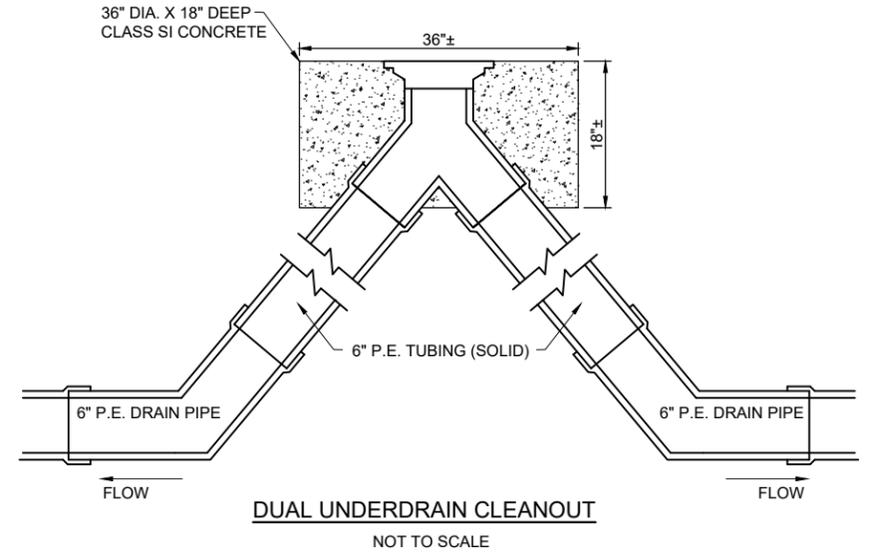
**UNDERDRAIN END SECTION ISOMETRIC DETAIL**  
NOT TO SCALE

**UNDERDRAIN END SECTION NOTES:**

1. THE CONTRACTOR SHALL INSTALL THE PROPOSED UNDERDRAIN END SECTION AT THE LOCATION AND GRADE SHOWN ON THE PLANS. ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS.
2. THE RODENT SCREEN SHALL BE 1/3" SQUARE (3 OPENINGS PER INCH). IT SHALL BE FASTENED TO THE OUTLET PROTECTOR WITH TWO 1/4" BY 1" HEX HEAD LAG SCREWS WITH FLAT WASHERS AND ANCHORS. THE APPROXIMATE LOCATION OF THE ANCHOR HOLES SHALL BE AS SHOWN ON THE DETAILS.
3. THE DELINEATOR POSTS USED TO ANCHOR THE OUTLET PROTECTOR SHALL BE EMBEDDED A MINIMUM OF 2'. THE EXPOSED END OF THESE DELINEATOR POSTS SHALL NOT PROTRUDE ABOVE THE TOP EDGE OF THE DOWNSTREAM END OF THE OUTLET PROTECTOR.
4. THE PROPOSED UNDERDRAIN END SECTION WILL BE PAID FOR UNDER ITEM AR705620 UNDERDRAIN END SECTION.



**CLEANOUT TYPE B**  
NOT TO SCALE



**DUAL UNDERDRAIN CLEANOUT**  
NOT TO SCALE

**CLEANOUT NOTES**

1. DIAMETER OF PIPE AS SPECIFIED.
2. TOP OF CLEANOUTS SHALL BE FLUSH WITH FINISHED GROUND LINE AT LOCATION SHOWN ON PLANS.
3. 1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF CLEANOUTS.
4. THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE (NON-REINFORCED) IN ACCORDANCE WITH SPECIFICATION 610.
5. PAYMENT FOR ONE (1) DUAL UNDERDRAIN CLEANOUT SHALL BE MADE UNDER ITEM AR705640 AS A QUANTITY OF 1 EACH FOR THE COMPLETE DUAL CLEANOUT.

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**ST. LOUIS  
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**BI-STATE DEVELOPMENT  
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RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


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CAD FILE: C-111-ALG.DWG

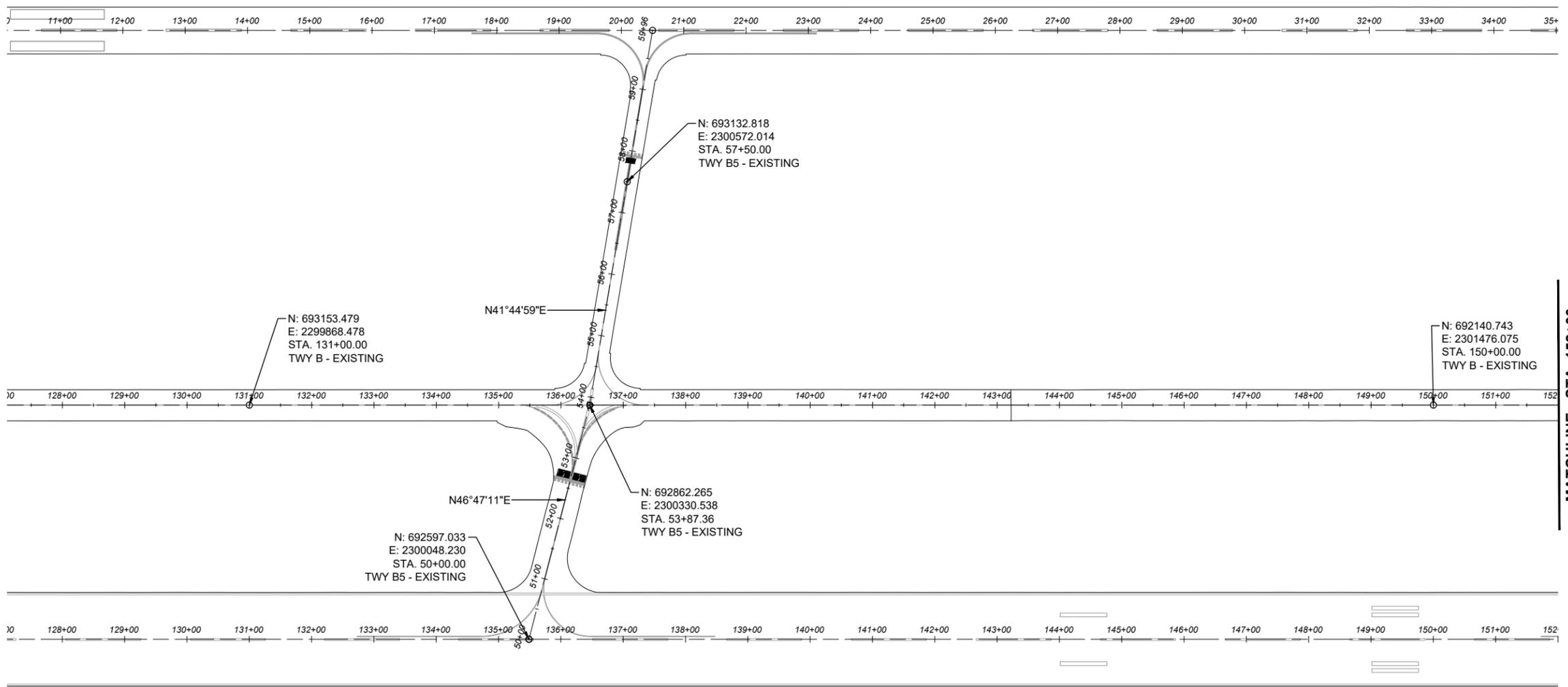
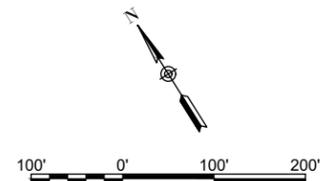
DESIGN BY: JRH 3/5/2025

DRAWN BY: AJC 3/11/2025

REVIEWED BY: BSS 3/12/2025

SHEET TITLE

EXISTING  
ALIGNMENT LAYOUT  
PLAN - SHEET 1



MAR 14, 2025 10:21 AM GROVE02594 I:\23\JOBS\23A0001D\CAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BS-22X34\_BORDER

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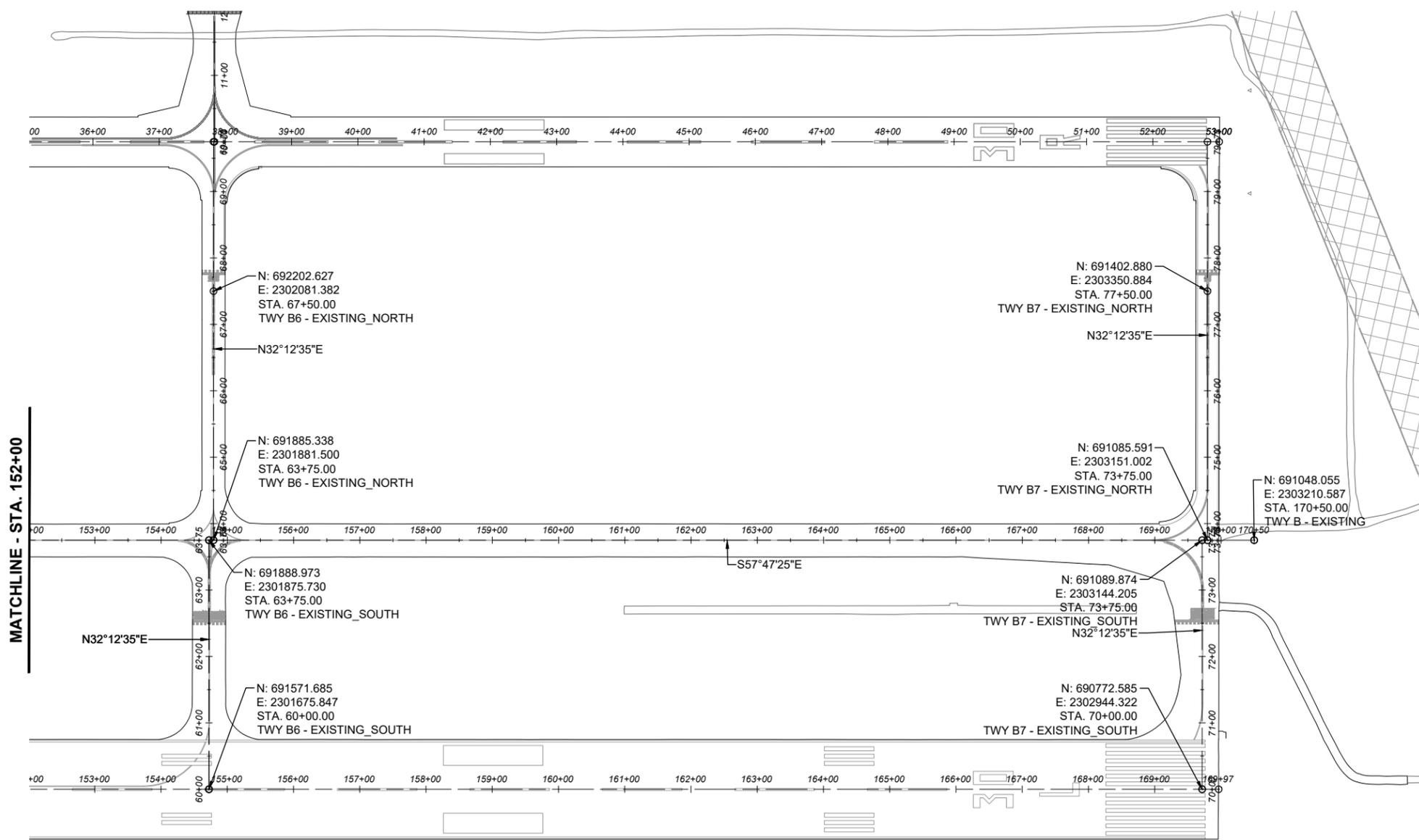
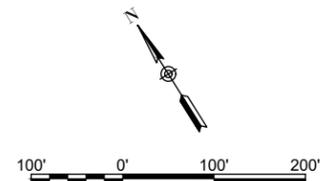
DESIGN BY: JRH 3/5/2025

DRAWN BY: AJC 3/11/2025

REVIEWED BY: BSS 3/12/2025

SHEET TITLE

EXISTING  
ALIGNMENT LAYOUT  
PLAN - SHEET 2



MAR 14, 2025 10:22 AM GROVE02594 I:\23\JOBS\23A0001D\CAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BS-22X34\_BORDER

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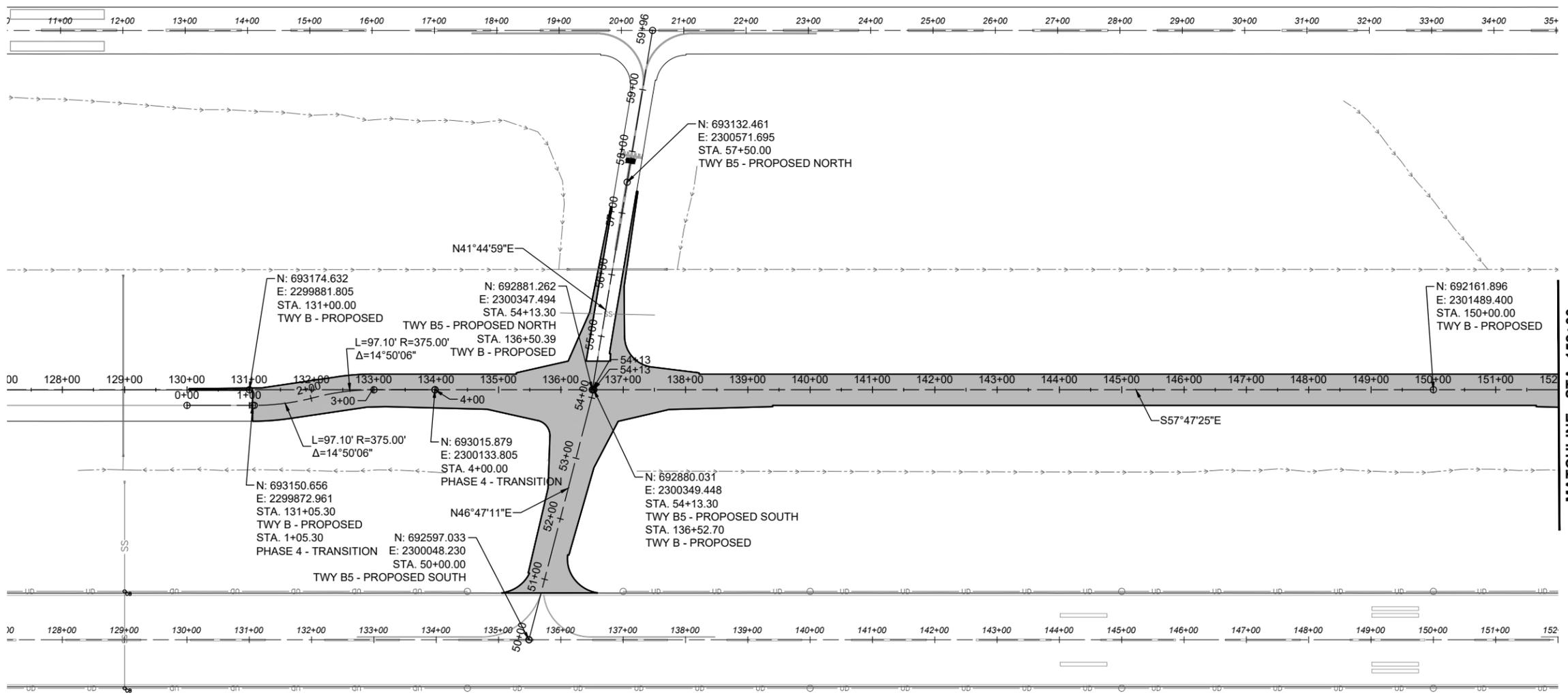
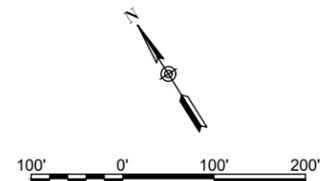

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CAD FILE: C-112-ALG.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED  
ALIGNMENT LAYOUT  
PLAN - SHEET 1

FOR BID



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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
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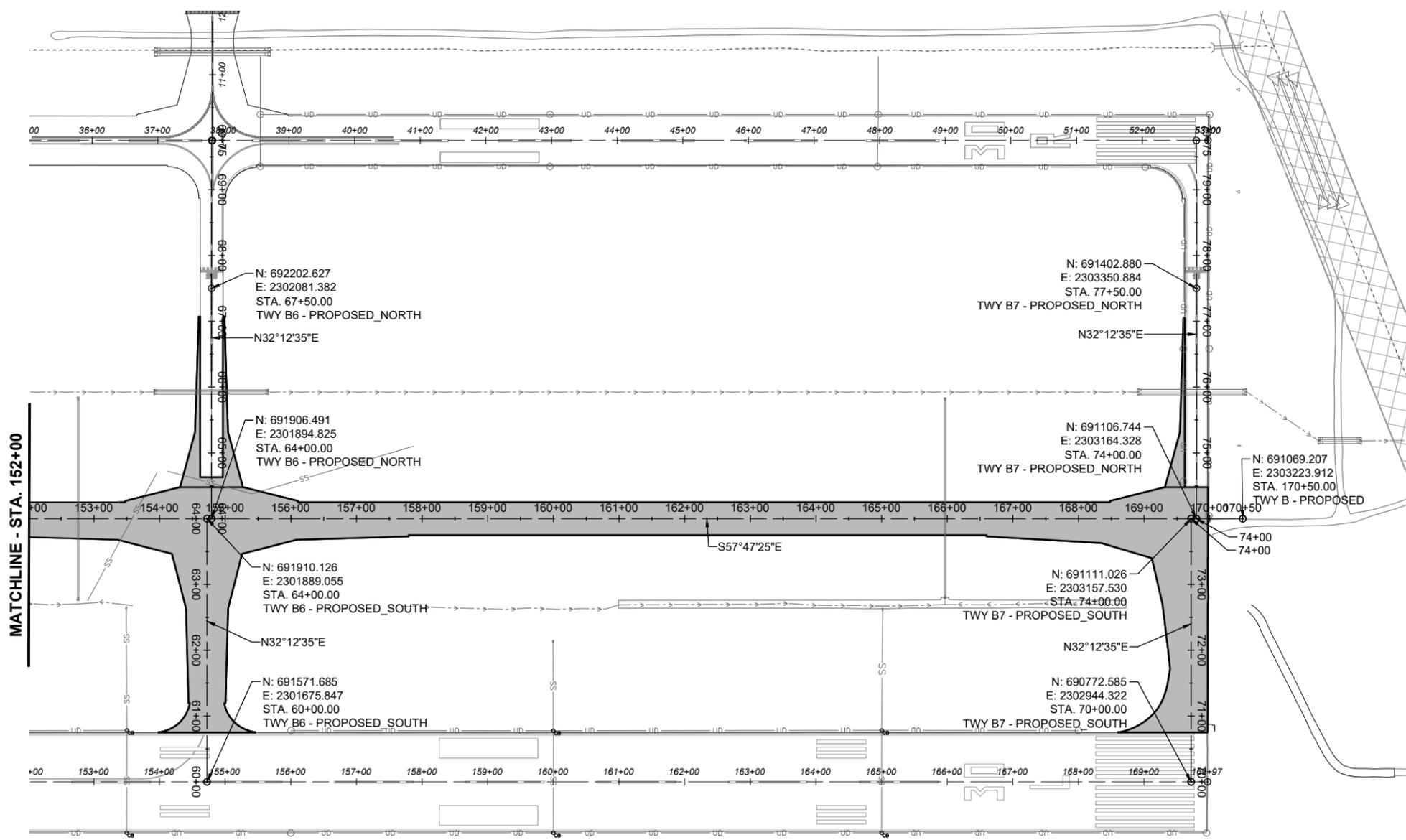
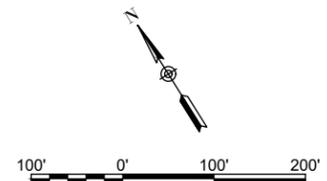
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REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED ALIGNMENT LAYOUT PLAN - SHEET 2



MAR 14, 2025 10:23 AM GROVE02594 I:\23\JOBS\23A0001D\CAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BS-22X34\_BORDER

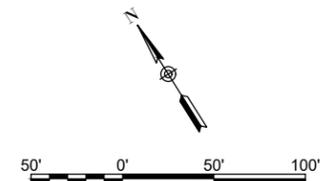
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NO.	DATE	DESCRIPTION		
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DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

DEMOLITION PLAN  
STA. 100+00 TO STA.  
109+00

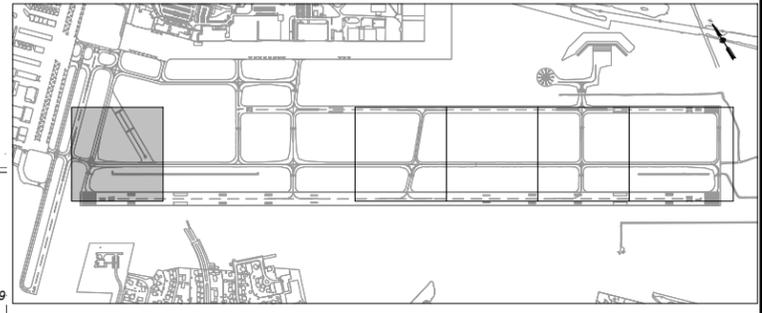


- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED PAVEMENT
  - PROPOSED BITUMINOUS MILL & FILL
  - PROPOSED PAVEMENT REMOVAL

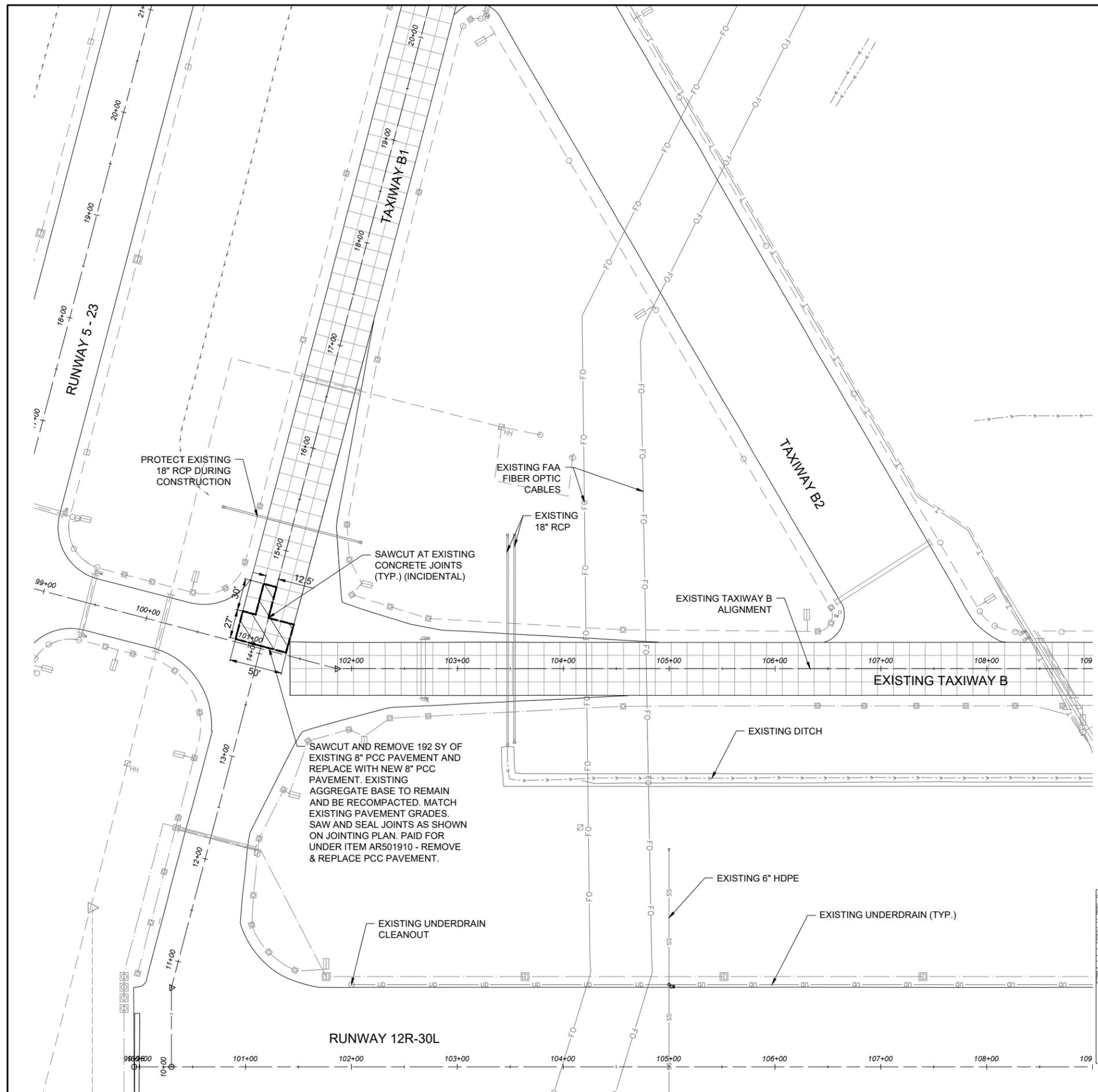
**DEMOLITION NOTES:**

- APPROXIMATE LOCATIONS OF KNOWN UTILITIES ARE SHOWN. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATIONS AND SHALL PROTECT ALL UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO SATISFACTION OF THE UTILITY OWNER.
- EXISTING CONCRETE PAVEMENT TO BE REMOVED ON THIS SHEET CONSISTS OF 8" PCC ON 6" CRUSHED AGGREGATE. THE 8" PCC SHALL BE REMOVED; THE 6" CRUSHED AGGREGATE BASE SHALL REMAIN.

**DEMOLITION PLAN - KEY MAP**



**FOR BID**



MAR 14, 2025 9:35 AM GROVED2594  
I:\23\JOBS\23A0001\DCAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BSS-22X34\_BORDER





DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

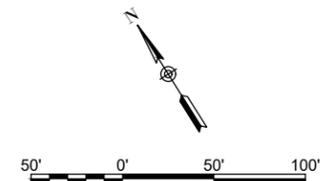
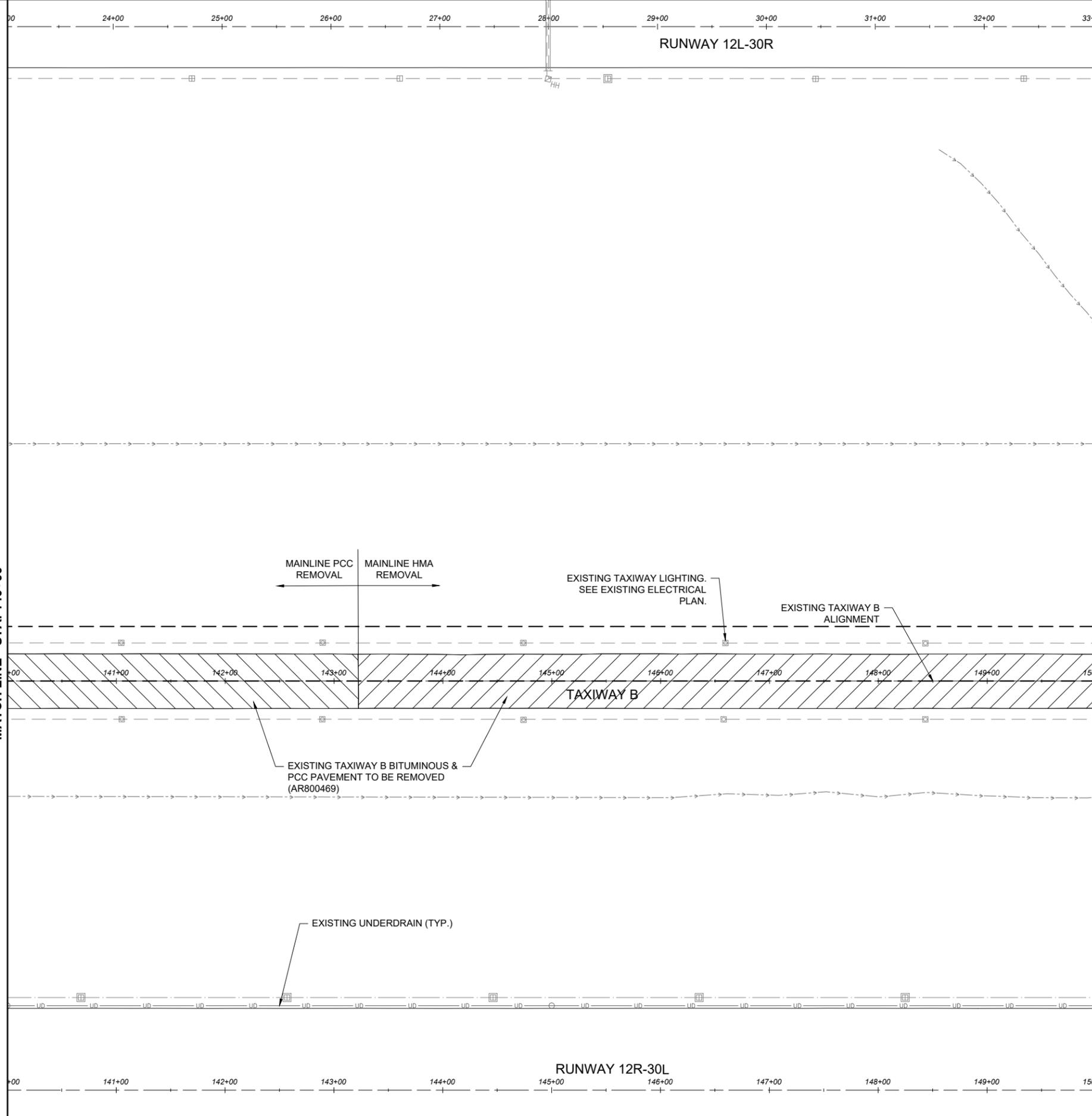
IDA NO.: CPS-5078  
CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-121-DEM.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

DEMOLITION PLAN  
STA. 140+00 TO STA. 150+00



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED PAVEMENT
  - PROPOSED BITUMINOUS MILL & FILL
  - PROPOSED PAVEMENT REMOVAL

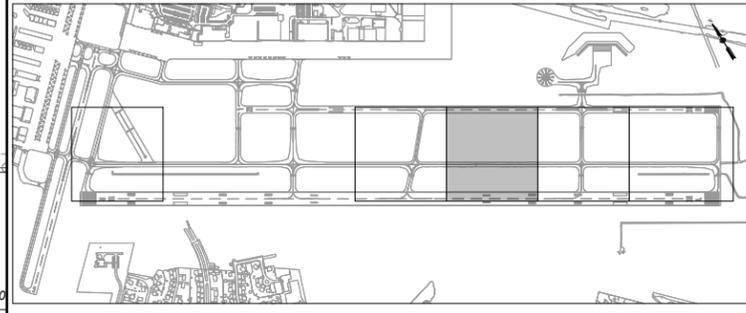
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2. BASED ON BORINGS CONDUCTED ON 7/27/22 AND HISTORICAL PAVEMENT DATA, THE EXISTING PAVEMENT TO BE REMOVED CONSISTS OF:
  - 2.1. APPROX. 8" OF CONCRETE ON 6" OF CRUSHED AGGREGATE FROM STA. 131+05 TO STA. 143+22.
  - 2.2. APPROX. 15" OF ASPHALT PLACED DIRECTLY ON THE SUBGRADE FROM STA. 143+22 TO STA. 155+00.
  - 2.3. APPROX. 11" OF ASPHALT ON 9" OF CRUSHED AGGREGATE FROM STA. 155+00 TO STA. 170+00

MATCH LINE - STA. 150+00

MATCH LINE - STA. 140+00

**DEMOLITION PLAN - KEY MAP**



**FOR BID**

MAR 14, 2025 9:35 AM GROVED2594  
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**BI-STATE DEVELOPMENT**  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

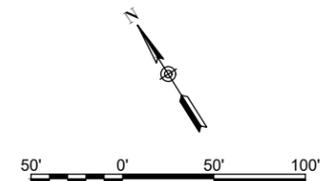
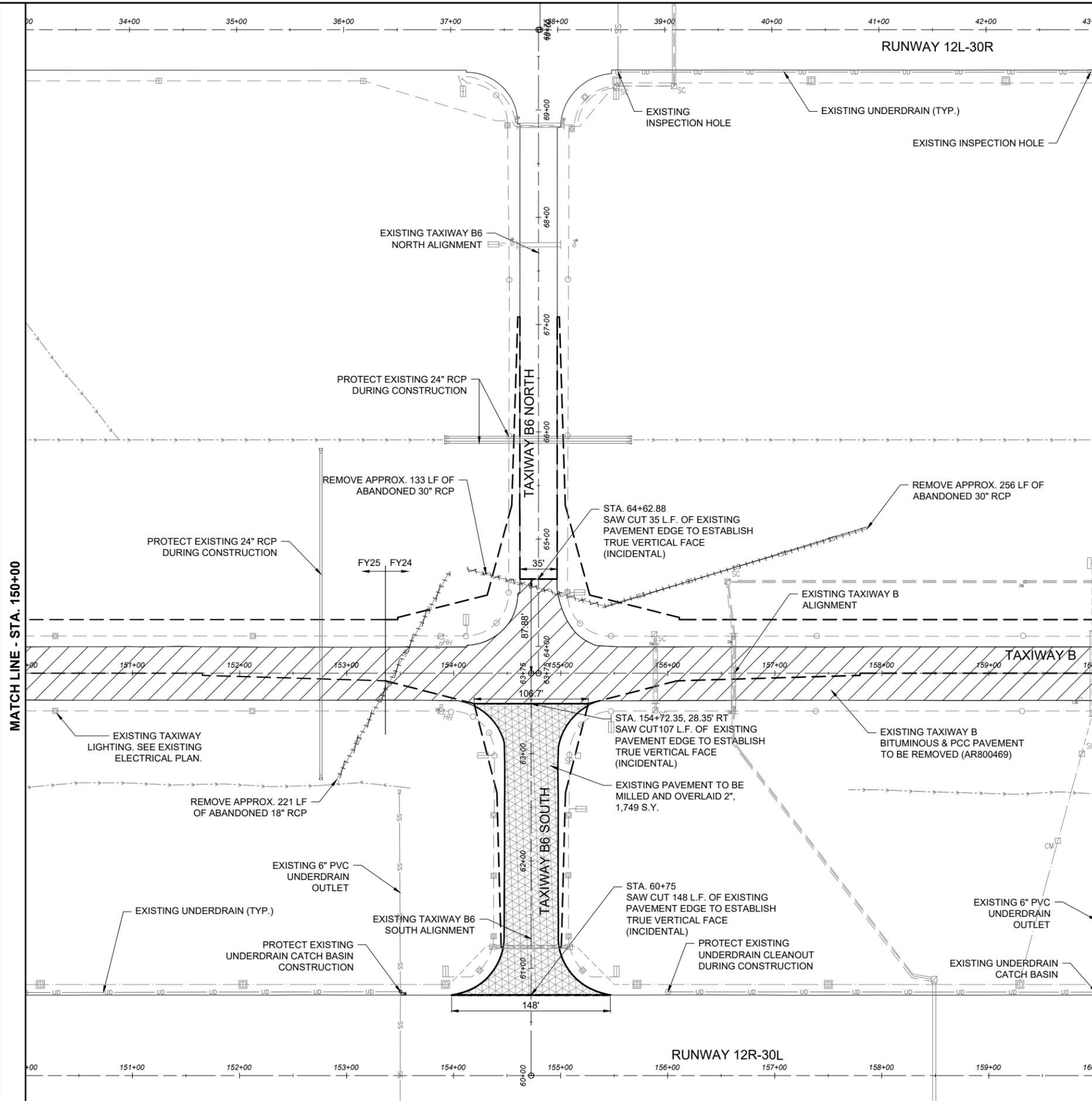
IDA NO.: CPS-5078  
CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-121-DEM.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

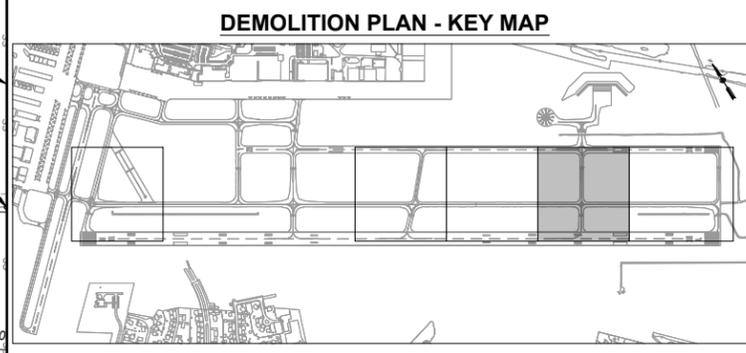
DEMOLITION PLAN  
STA. 150+00 TO STA.  
160+00



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED PAVEMENT
  - PROPOSED BITUMINOUS MILL & FILL
  - PROPOSED PAVEMENT REMOVAL

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**FOR BID**

MAR 14, 2025 9:36 AM GROVED2594  
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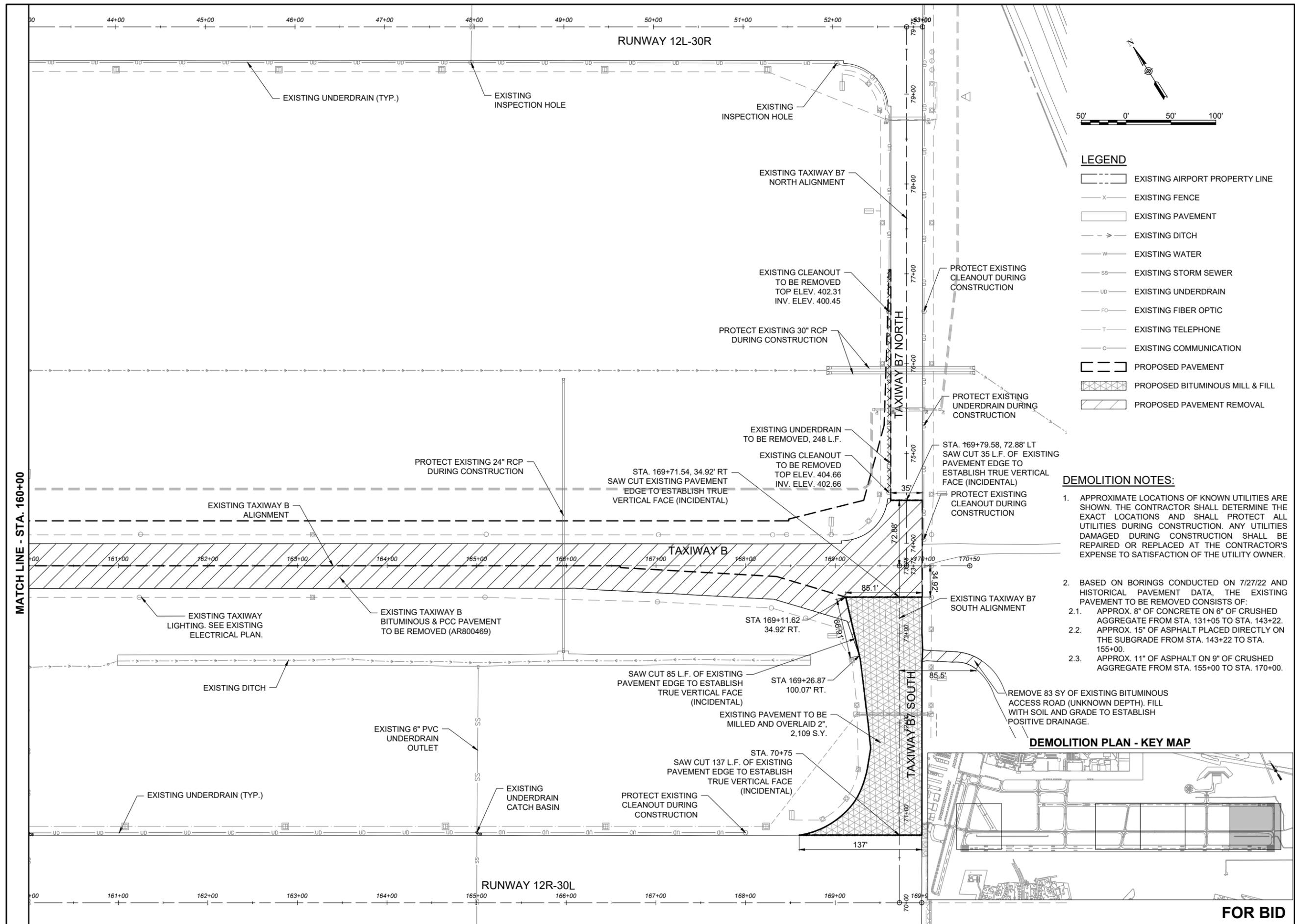
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025

PROJECT NO: 23A0001D  
CAD FILE: C-121-DEM.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

DEMOLITION PLAN  
STA. 160+00 TO STA. 170+50



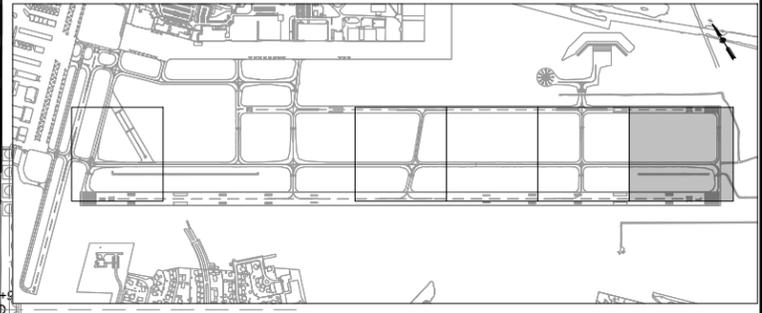
**LEGEND**

- EXISTING AIRPORT PROPERTY LINE
- EXISTING FENCE
- EXISTING PAVEMENT
- EXISTING DITCH
- EXISTING WATER
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING FIBER OPTIC
- EXISTING TELEPHONE
- EXISTING COMMUNICATION
- PROPOSED PAVEMENT
- PROPOSED BITUMINOUS MILL & FILL
- PROPOSED PAVEMENT REMOVAL

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  - APPROX. 15" OF ASPHALT PLACED DIRECTLY ON THE SUBGRADE FROM STA. 143+22 TO STA. 155+00.
  - APPROX. 11" OF ASPHALT ON 9" OF CRUSHED AGGREGATE FROM STA. 155+00 TO STA. 170+00.
- REMOVE 83 SY OF EXISTING BITUMINOUS ACCESS ROAD (UNKNOWN DEPTH). FILL WITH SOIL AND GRADE TO ESTABLISH POSITIVE DRAINAGE.

**DEMOLITION PLAN - KEY MAP**



**FOR BID**

MAR 14, 2025 9:36 AM GROVED2594  
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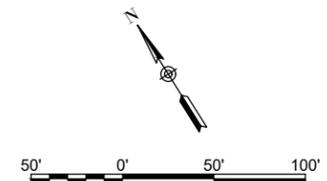
MATCH LINE - STA. 160+00

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-122-CON.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

CONSTRUCTION  
PLAN STA. 100+00 TO  
STA. 109+00

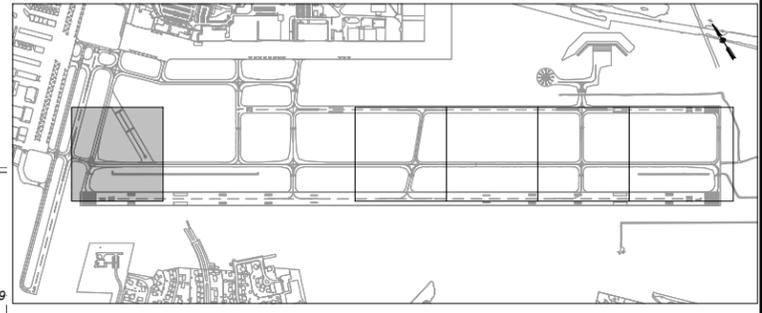


- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED BITUMINOUS MILL & FILL

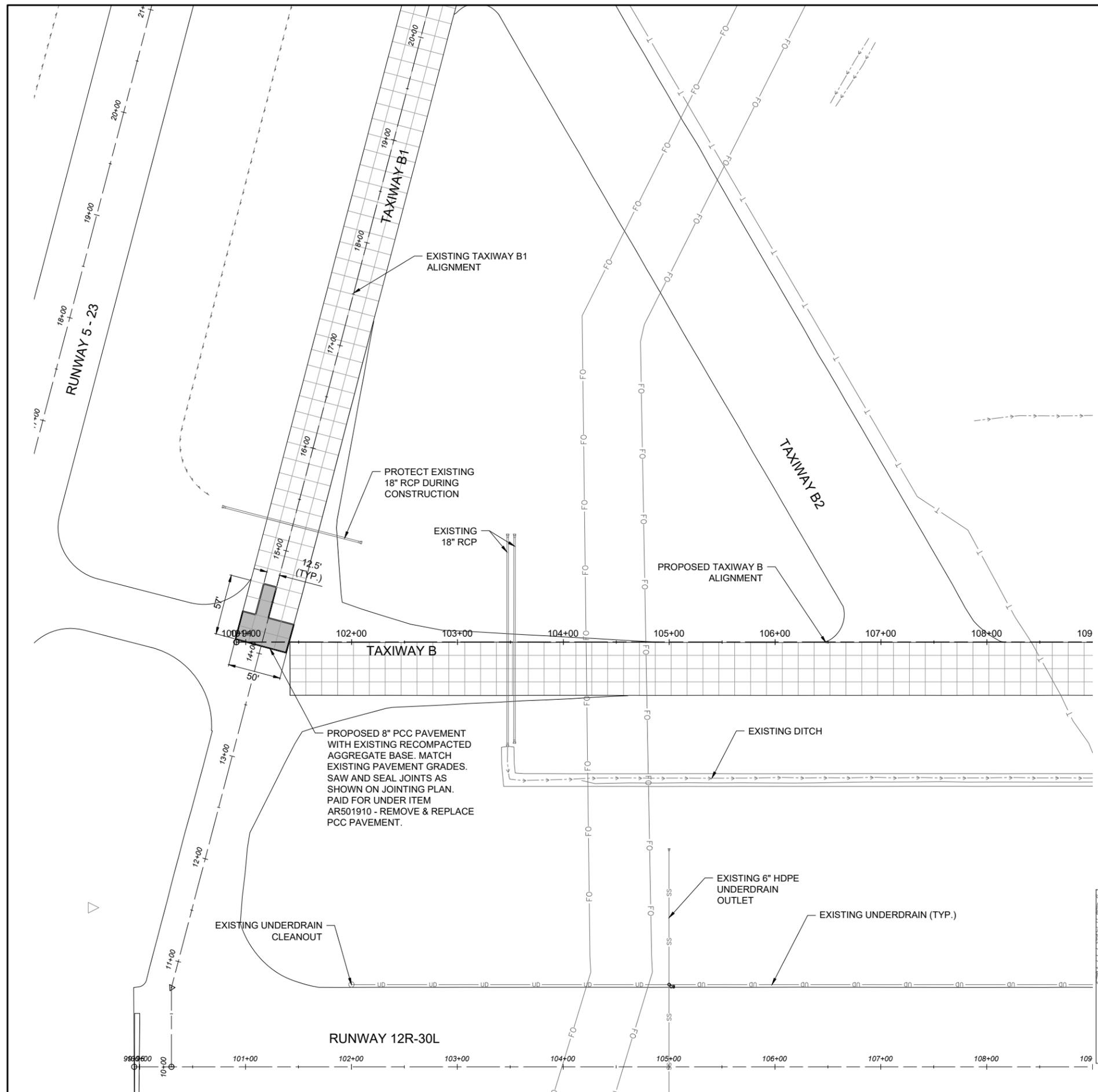
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**CONSTRUCTION PLAN - KEY MAP**



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**ST. LOUIS DOWNTOWN AIRPORT**  
BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

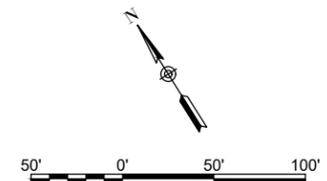
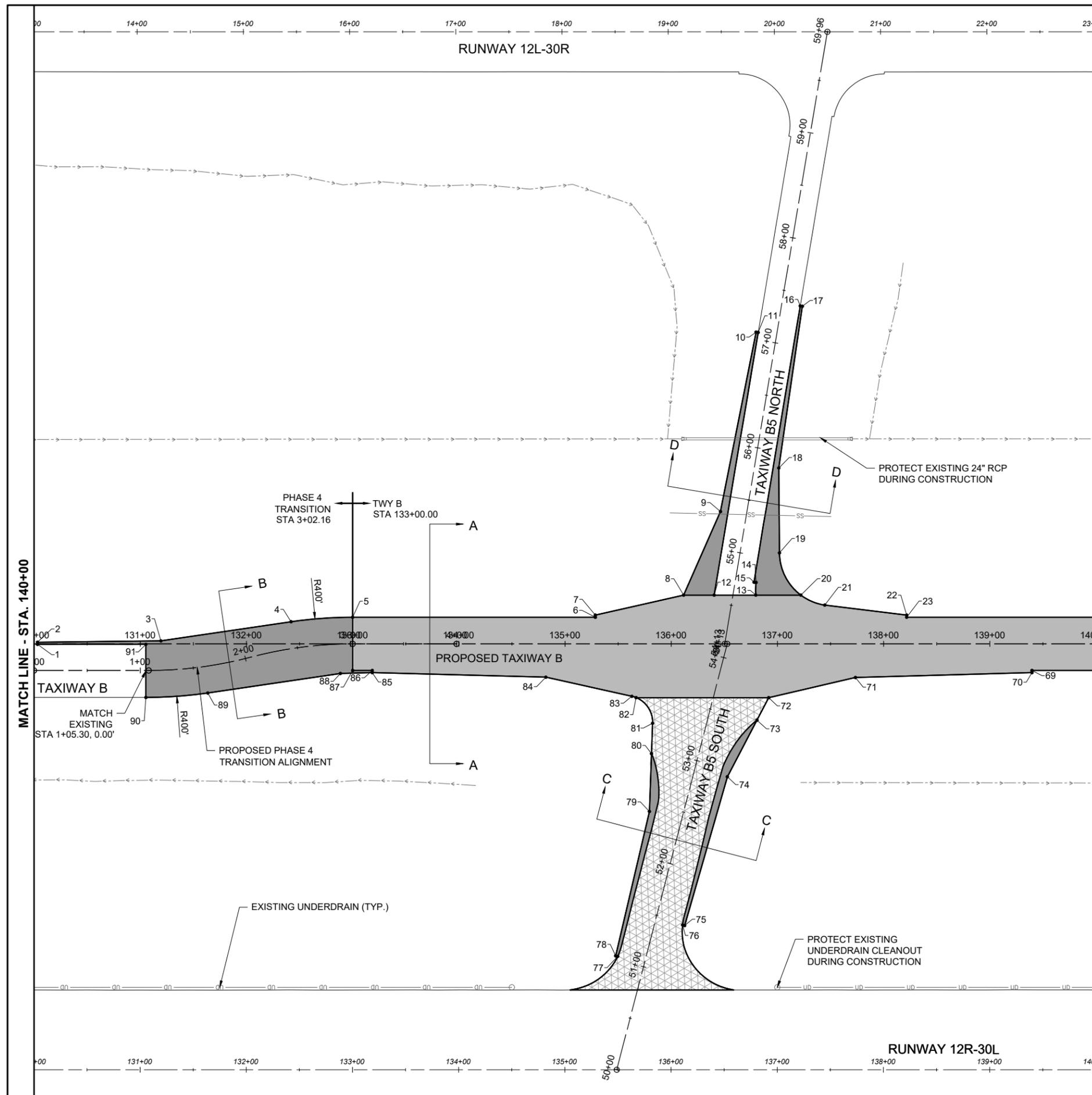
IDA NO.: CPS-5078  
CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-122-CON.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

CONSTRUCTION PLAN STA. 130+00 TO STA. 140+00



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED BITUMINOUS MILL & FILL

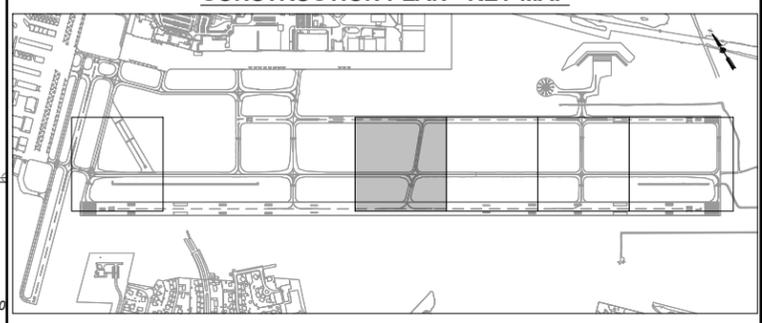
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MATCH LINE - STA. 150+00

MATCH LINE - STA. 140+00

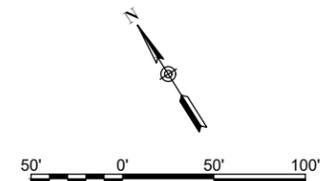
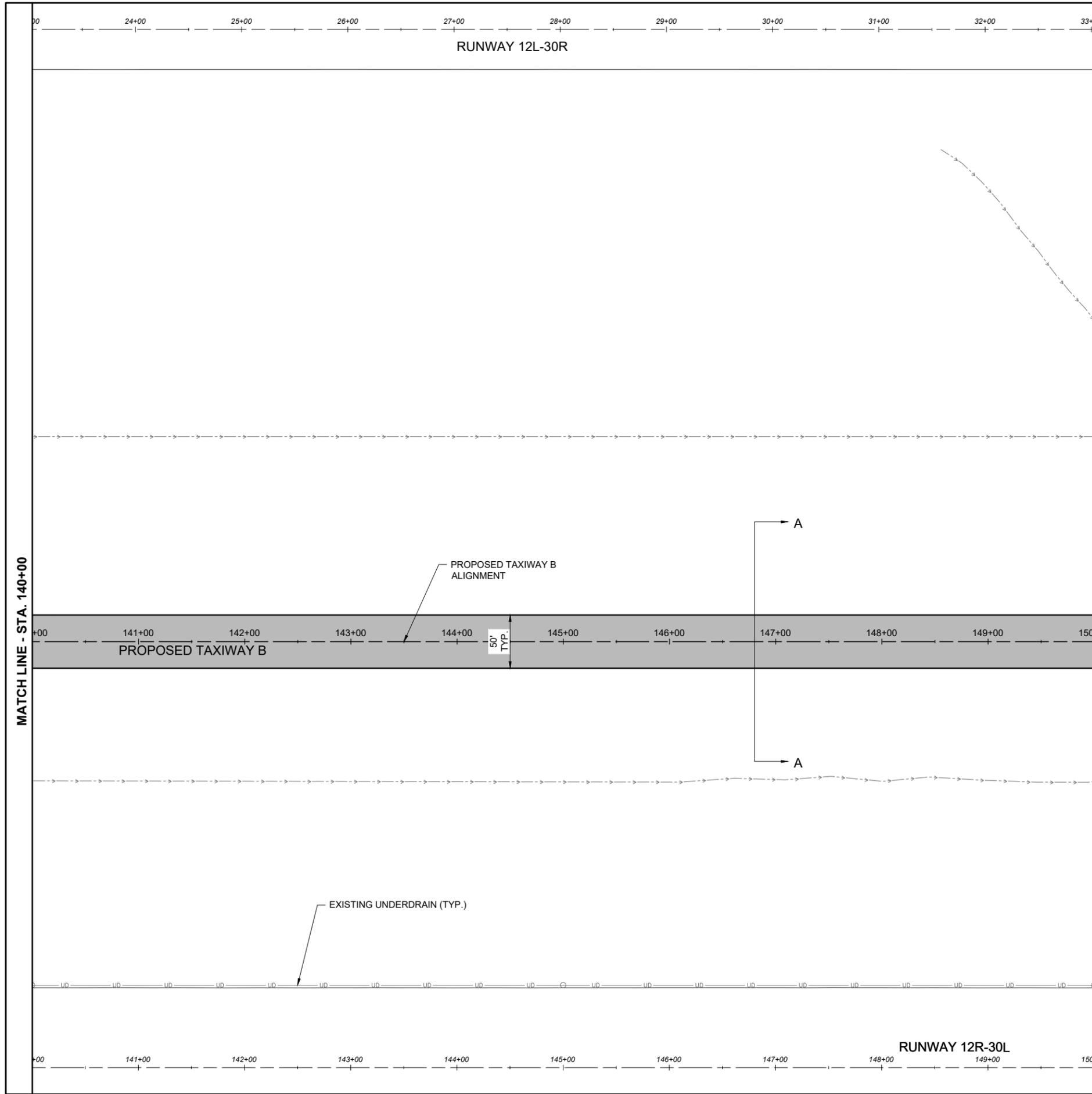
**CONSTRUCTION PLAN - KEY MAP**



**FOR BID**

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MAR 14, 2025 9:37 AM GROVED2594  
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- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
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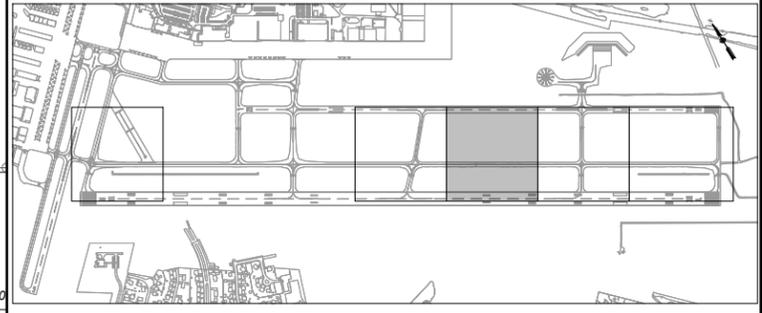
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MATCH LINE - STA. 150+00

MATCH LINE - STA. 140+00

**CONSTRUCTION PLAN - KEY MAP**



**FOR BID**

**HANSON**  
 Engineering | Planning | Allied Services  
 Offices Nationwide  
 www.hanson-inc.com  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2886  
 Telephone: 217.788.2450  
 Fax: 217.788.2503

**ST. LOUIS DOWNTOWN AIRPORT**  
 BI-STATE DEVELOPMENT  
 ST. LOUIS DOWNTOWN AIRPORT  
 6100 Archview Drive  
 Cahokia Heights, Illinois 62206



DATE: 3/14/2025  
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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
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IDA NO.: CPS-5078  
 CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
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ISSUE: MARCH 14, 2025  
 PROJECT NO: 23A0001D  
 CAD FILE: C-122-CON.DWG  
 DESIGN BY: JRH 3/5/2025  
 DRAWN BY: AJC 3/11/2025  
 REVIEWED BY: BSS 3/12/2025

SHEET TITLE

CONSTRUCTION PLAN STA. 140+00 TO STA. 150+00

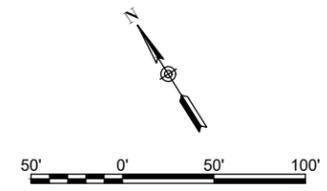
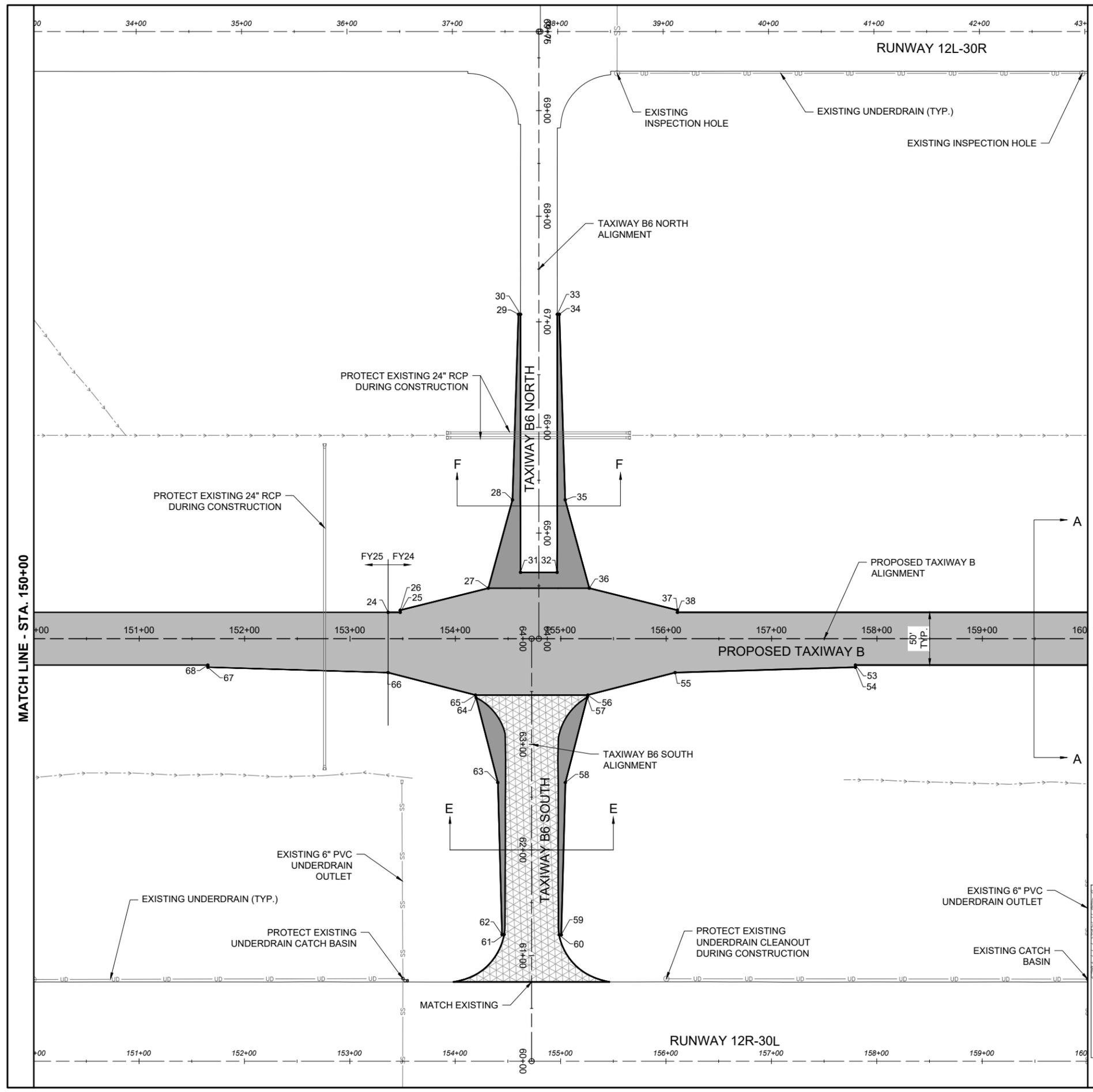
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ISSUE: MARCH 14, 2025  
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CAD FILE: C-122-CON.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

CONSTRUCTION PLAN STA. 150+00 TO STA. 160+00

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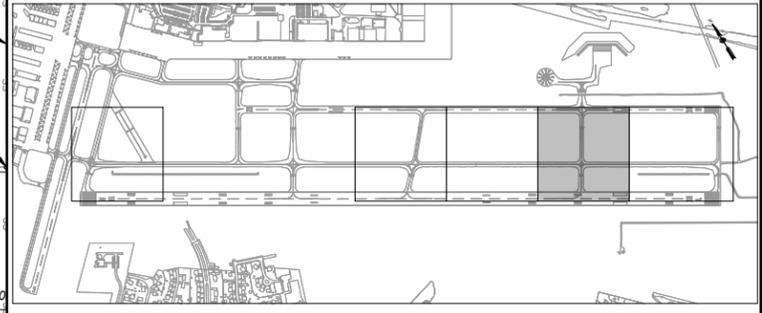


- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED BITUMINOUS MILL & FILL

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**CONSTRUCTION PLAN - KEY MAP**



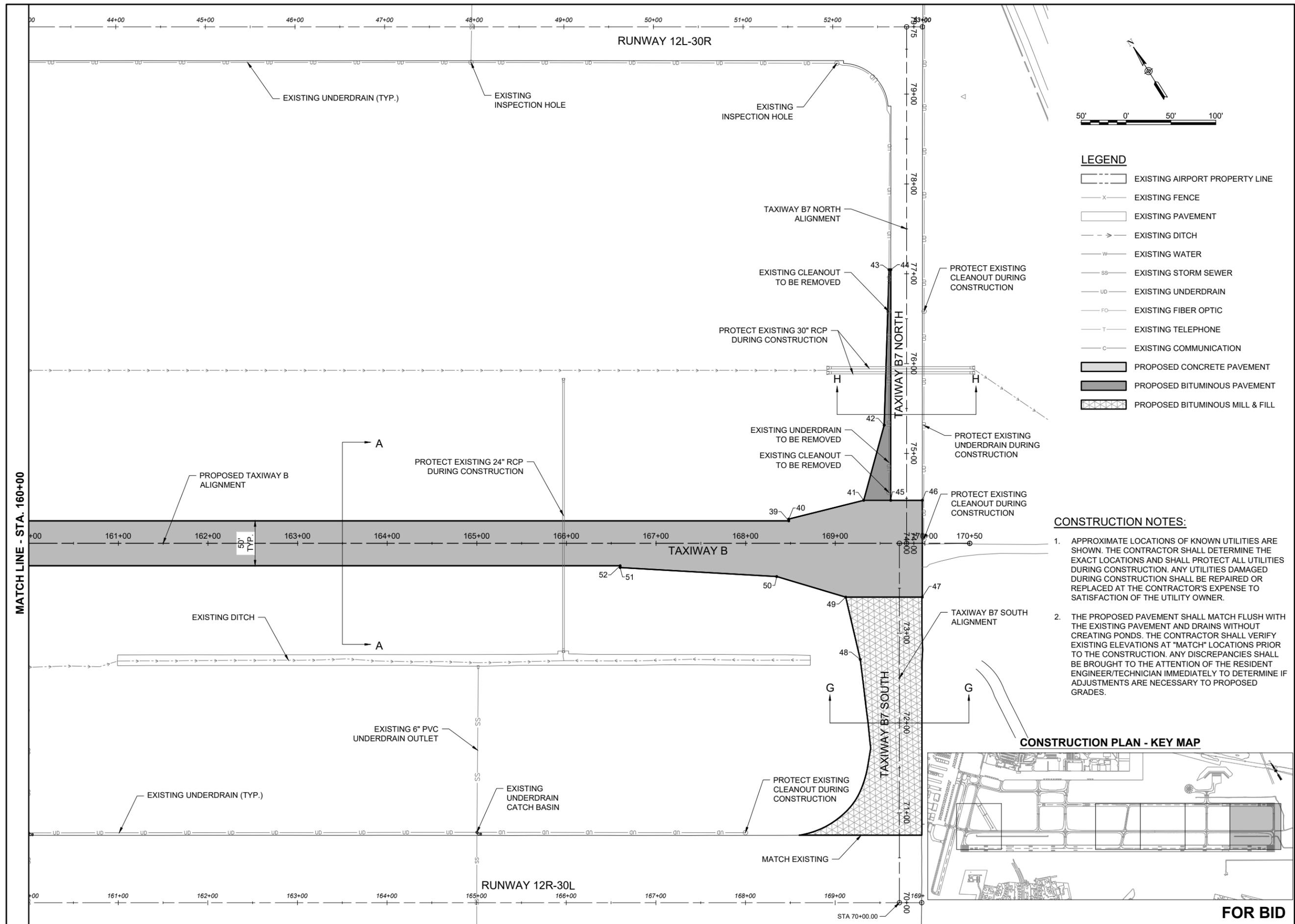
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DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

CONSTRUCTION PLAN STA. 160+00 TO STA. 170+50



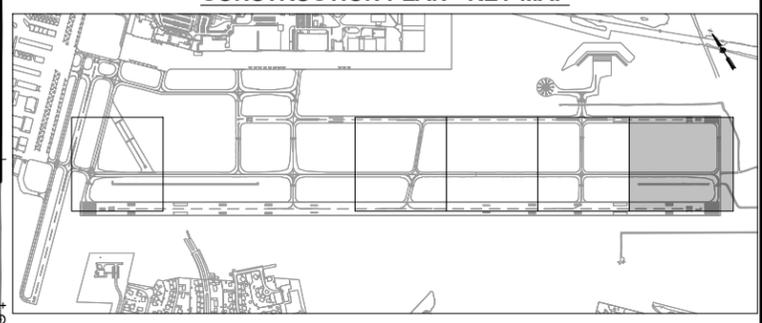
**LEGEND**

- EXISTING AIRPORT PROPERTY LINE
- EXISTING FENCE
- EXISTING PAVEMENT
- EXISTING DITCH
- EXISTING WATER
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING FIBER OPTIC
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**CONSTRUCTION PLAN - KEY MAP**



**FOR BID**

MAR 14, 2025 9:38 AM GROVED2594  
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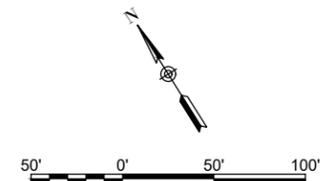



NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-701-PNP.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 03/11/2025  
REVIEWED BY: BSS 3/12/2025

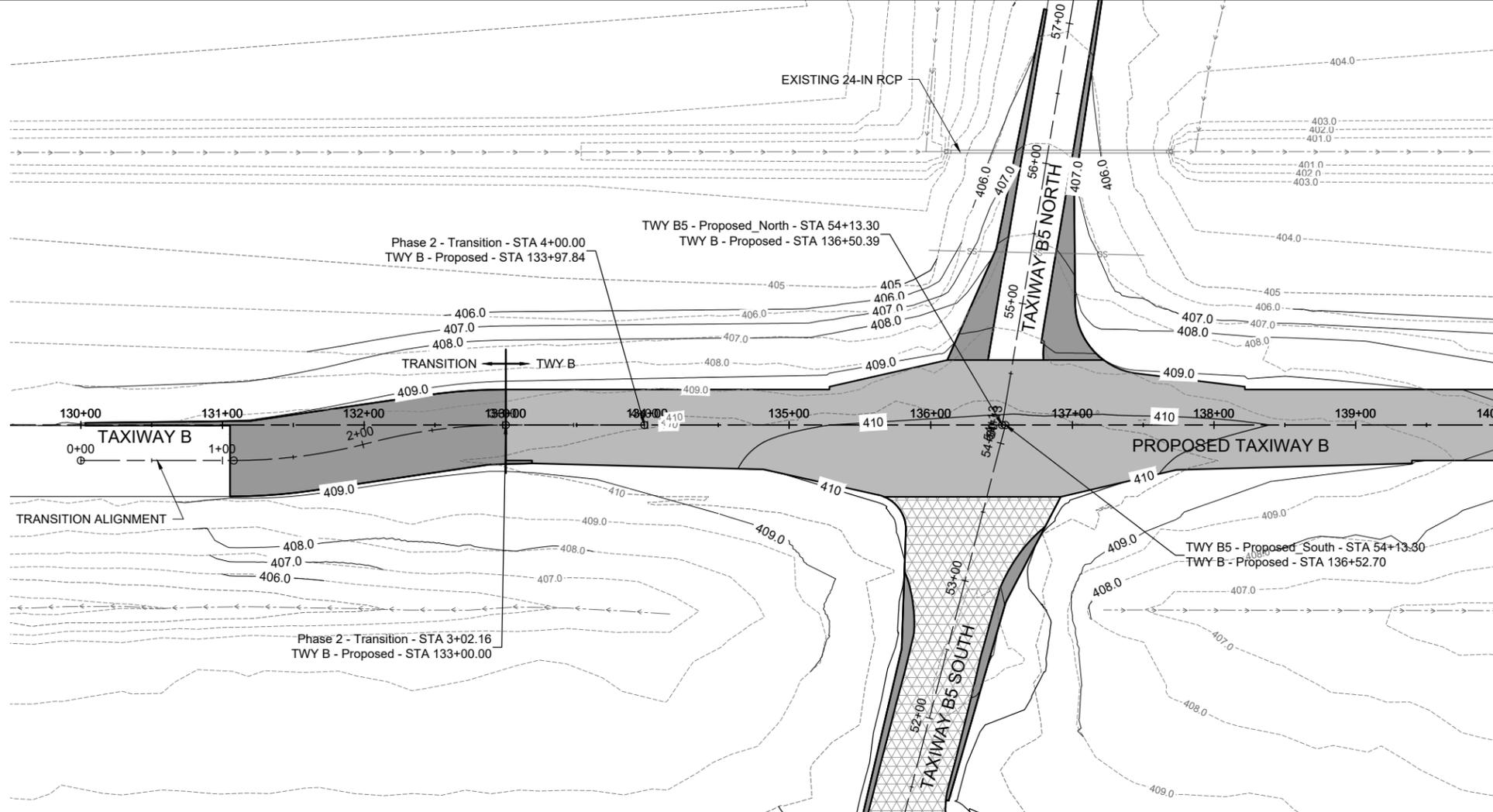
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PROPOSED PLAN AND PROFILE - TRANSITION

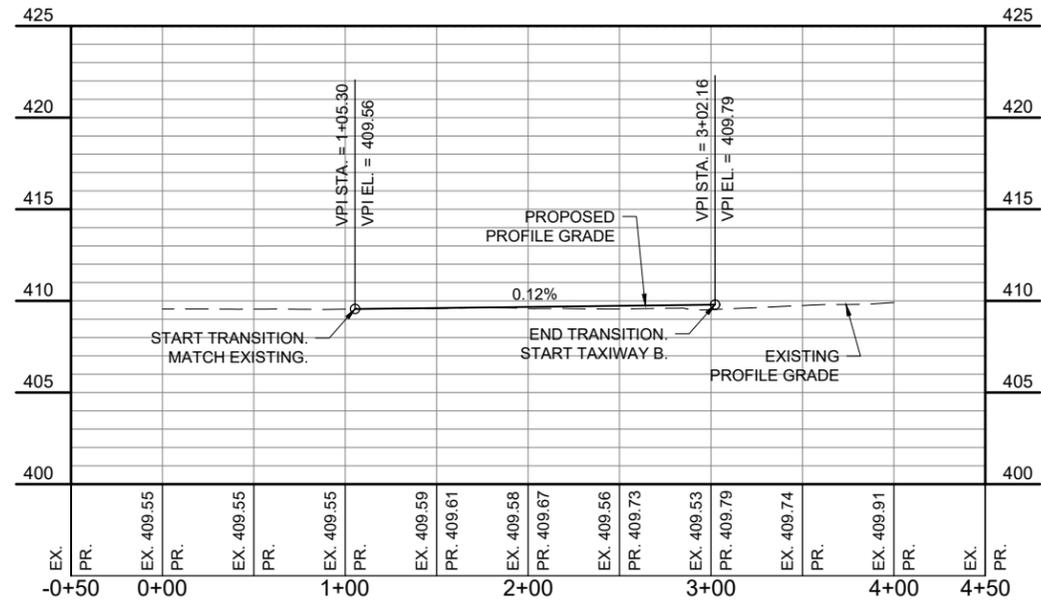


- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED MILL & OVERLAY

MATCH LINE - STA. 140+00



PLAN VIEW - TRANSITION



PROFILE VIEW - TRANSITION

MAR 14, 2025 9:36 AM GROVED2594 I:\23\JOBS\23A0001\DCAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BSS-22X34\_BORDER

**FOR BID**



**BI-STATE DEVELOPMENT**  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025

PROJECT NO: 23A0001D

CAD FILE: C-701-PNP.DWG

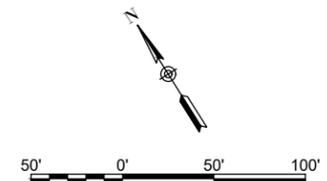
DESIGN BY: JRH 3/5/2025

DRAWN BY: AJC 03/11/2025

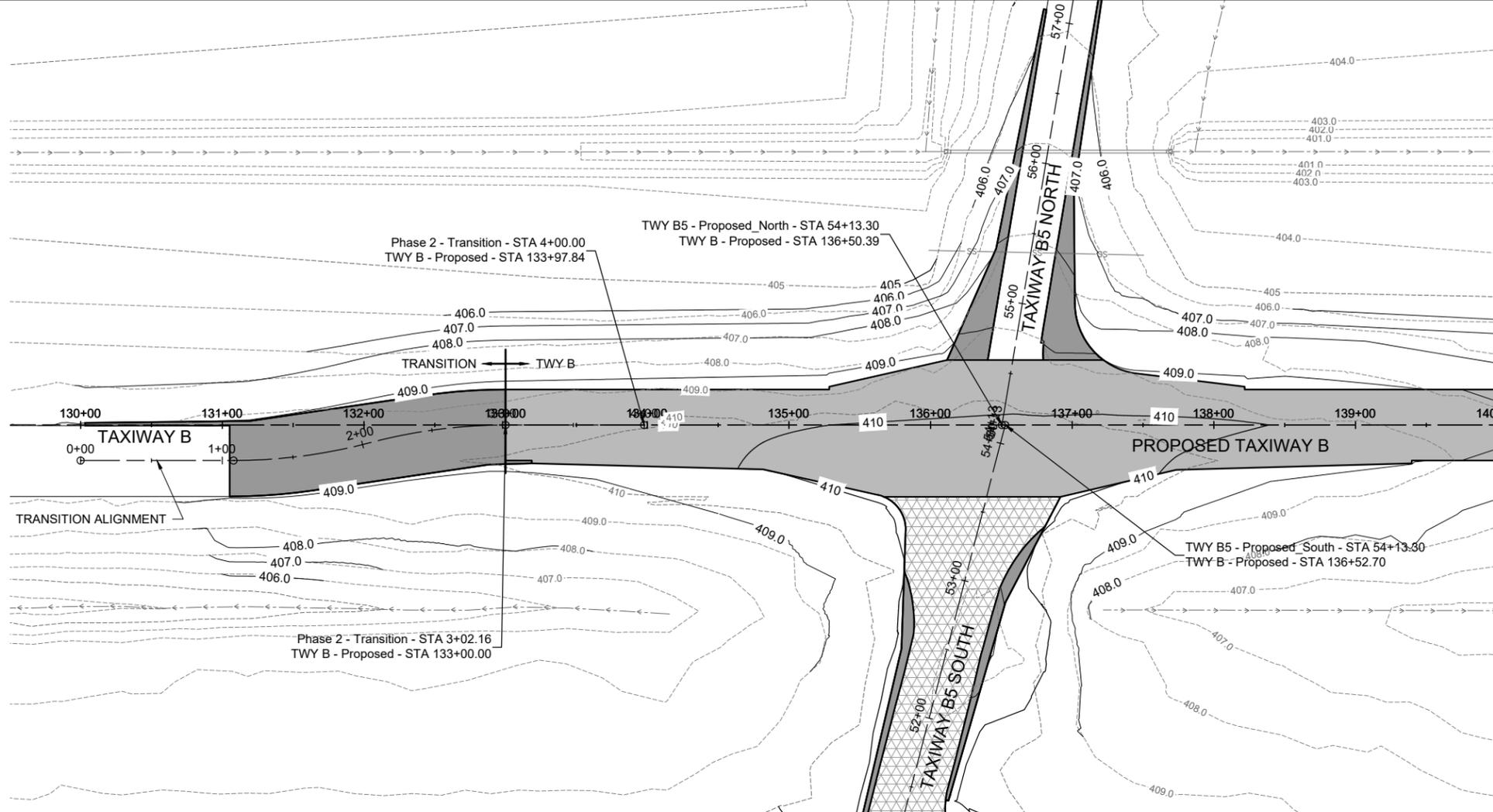
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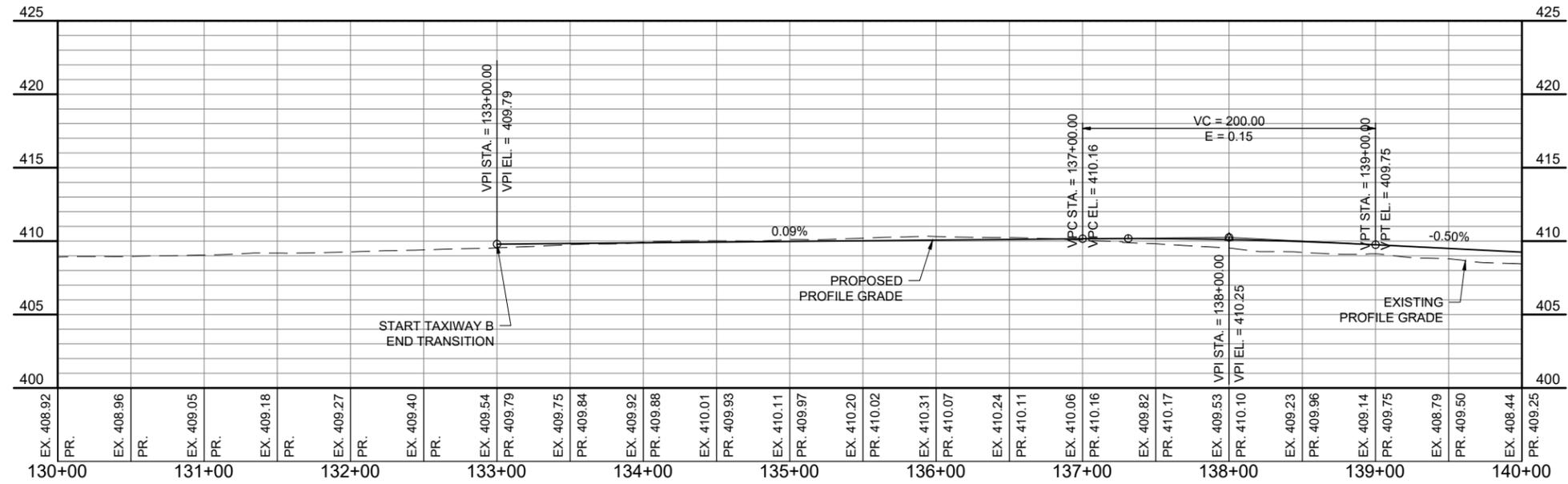
PROPOSED PLAN  
AND PROFILE - STA.  
130+00 TO STA.  
140+00



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED MILL & OVERLAY



PLAN VIEW - STA. 140+00 TO STA. 150+00



PROFILE VIEW - STA. 130+00 TO STA. 140+00

**FOR BID**

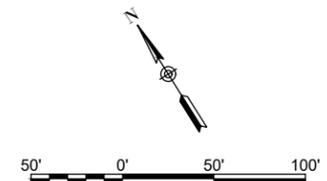
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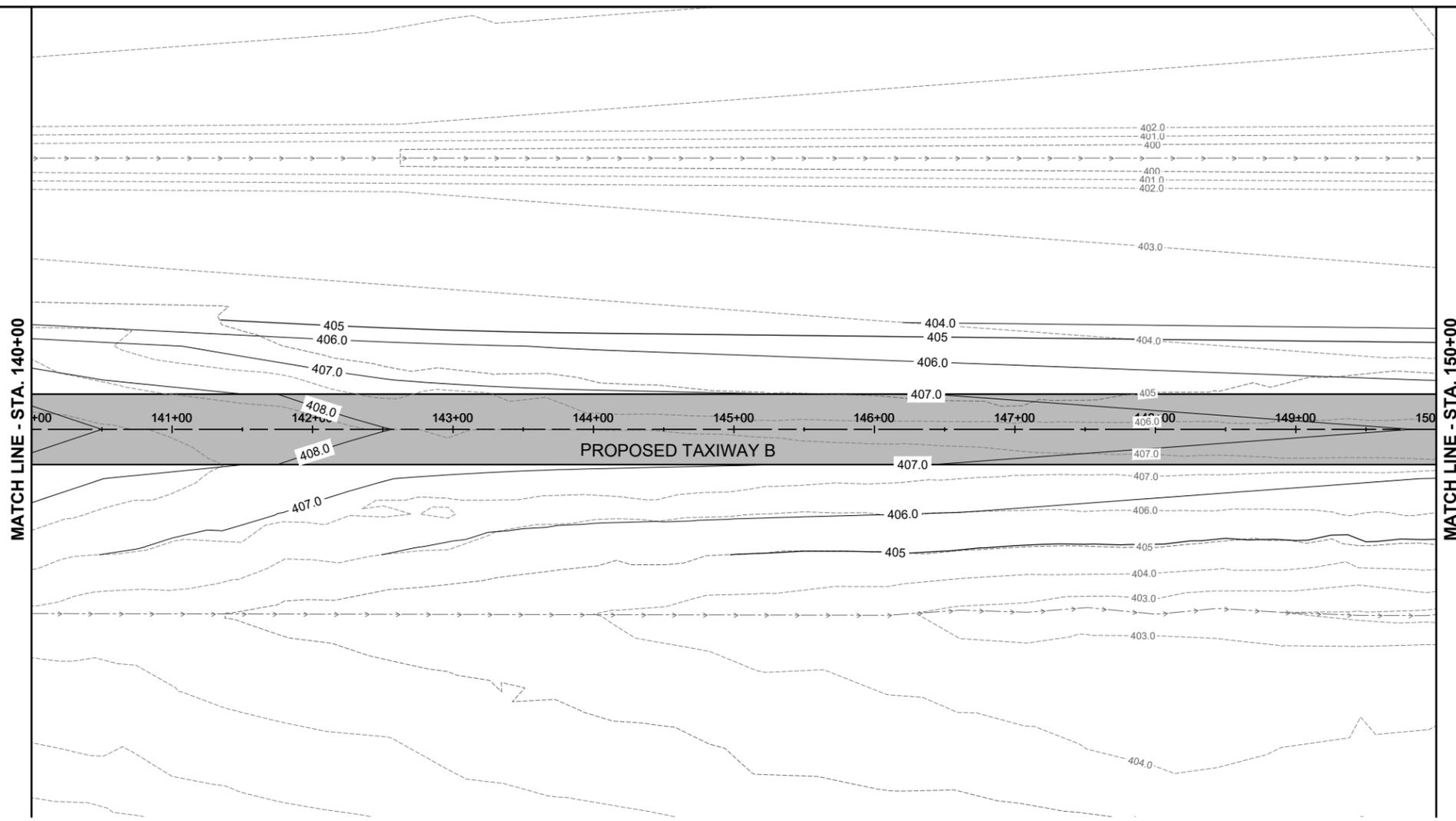
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PROJECT NO: 23A0001D  
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REVIEWED BY: BSS 3/12/2025

SHEET TITLE

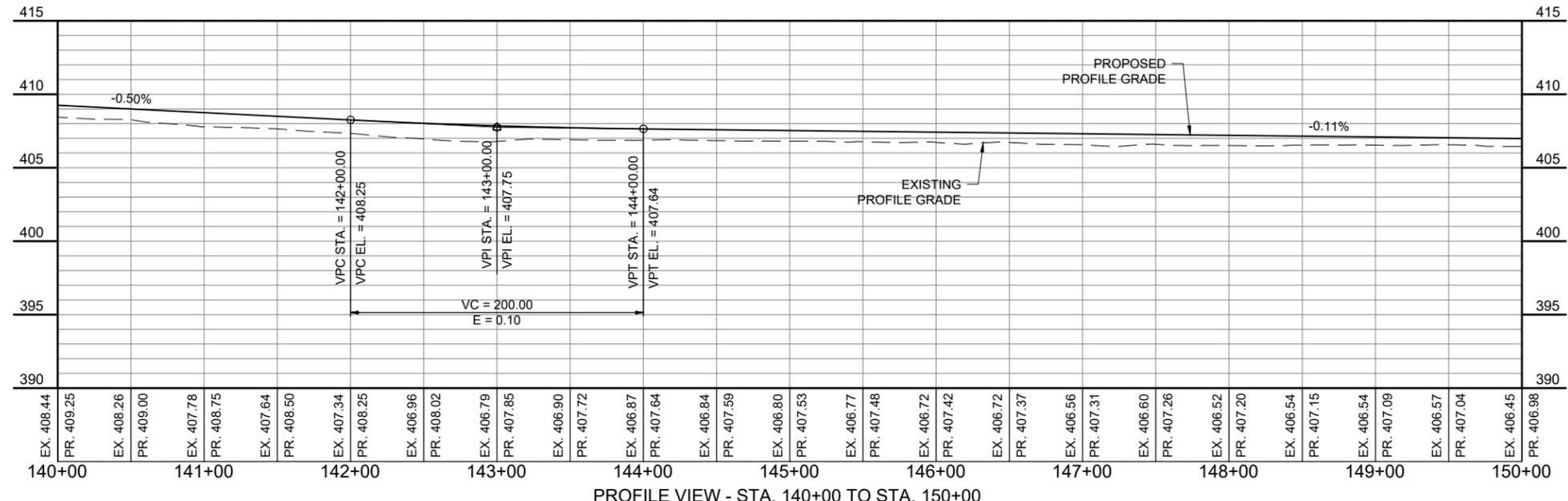
PROPOSED PLAN  
AND PROFILE - STA.  
140+00 TO STA.  
150+00



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED MILL & OVERLAY



PLAN VIEW - STA. 140+00 TO STA. 150+00



PROFILE VIEW - STA. 140+00 TO STA. 150+00

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**FOR BID**



**BI-STATE DEVELOPMENT**  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE: 3/14/2025 LICENSE: 062-057281  
SIGNED: 3/14/2025 EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

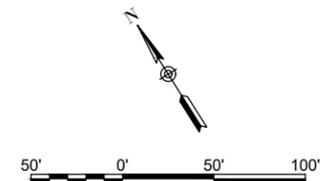
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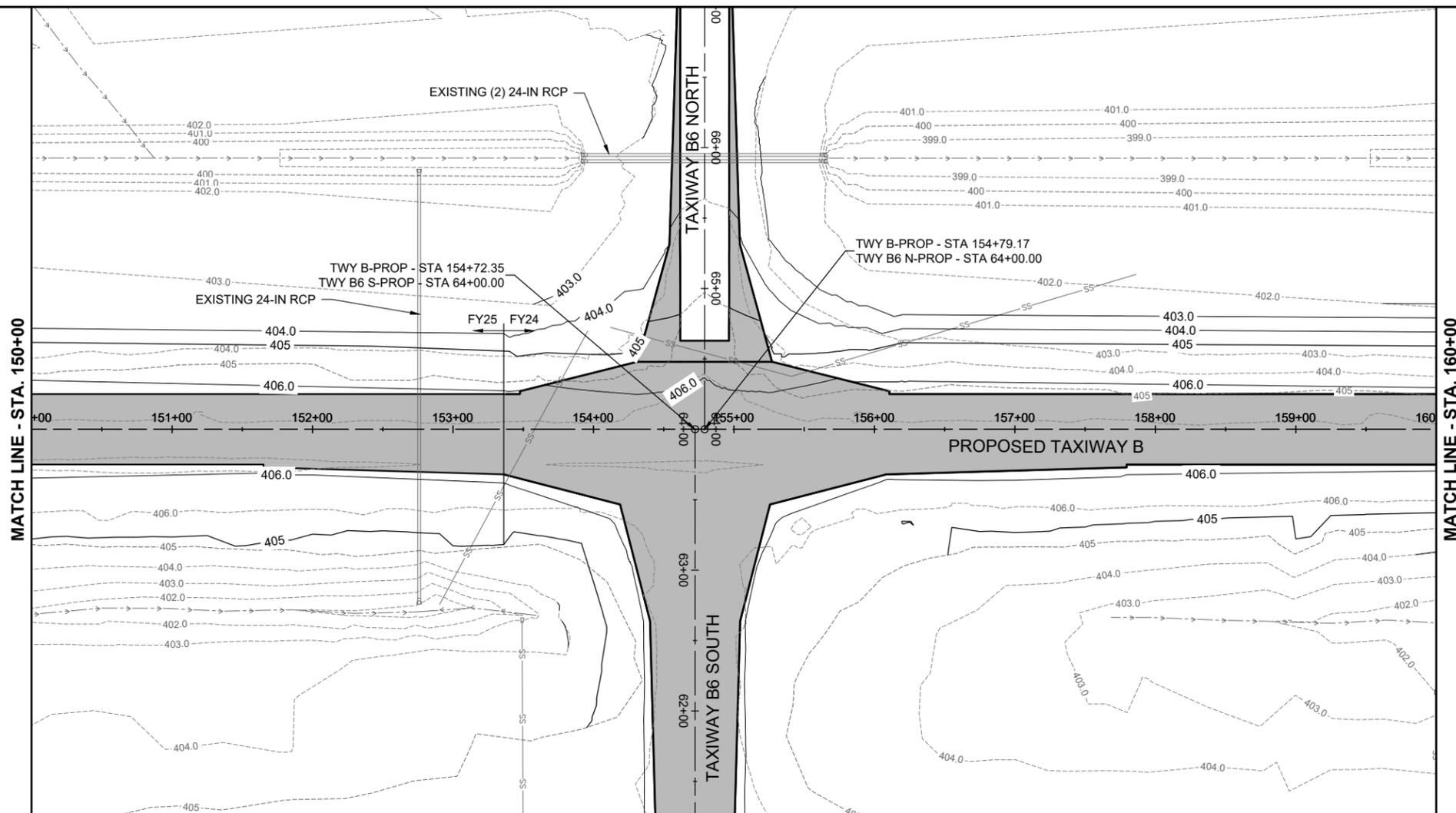
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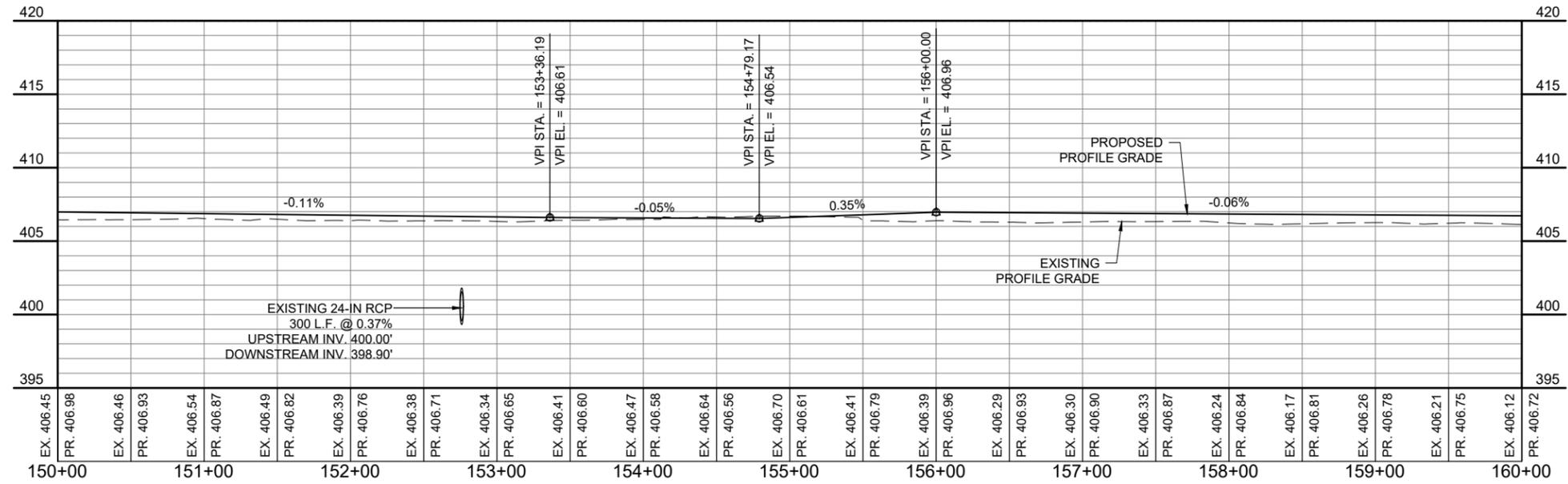
PROPOSED PLAN  
AND PROFILE - STA.  
150+00 TO STA.  
160+00



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED MILL & OVERLAY



PLAN VIEW - STA. 150+00 TO STA. 160+00



PROFILE VIEW - STA. 150+00 TO STA. 160+00

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**FOR BID**


NO.	DATE	DESCRIPTION		
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ISSUE: MARCH 14, 2025

PROJECT NO: 23A0001D  
CAD FILE: C-701-PNP.DWG

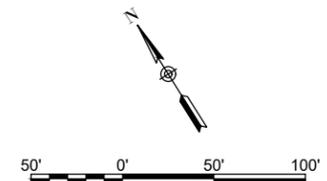
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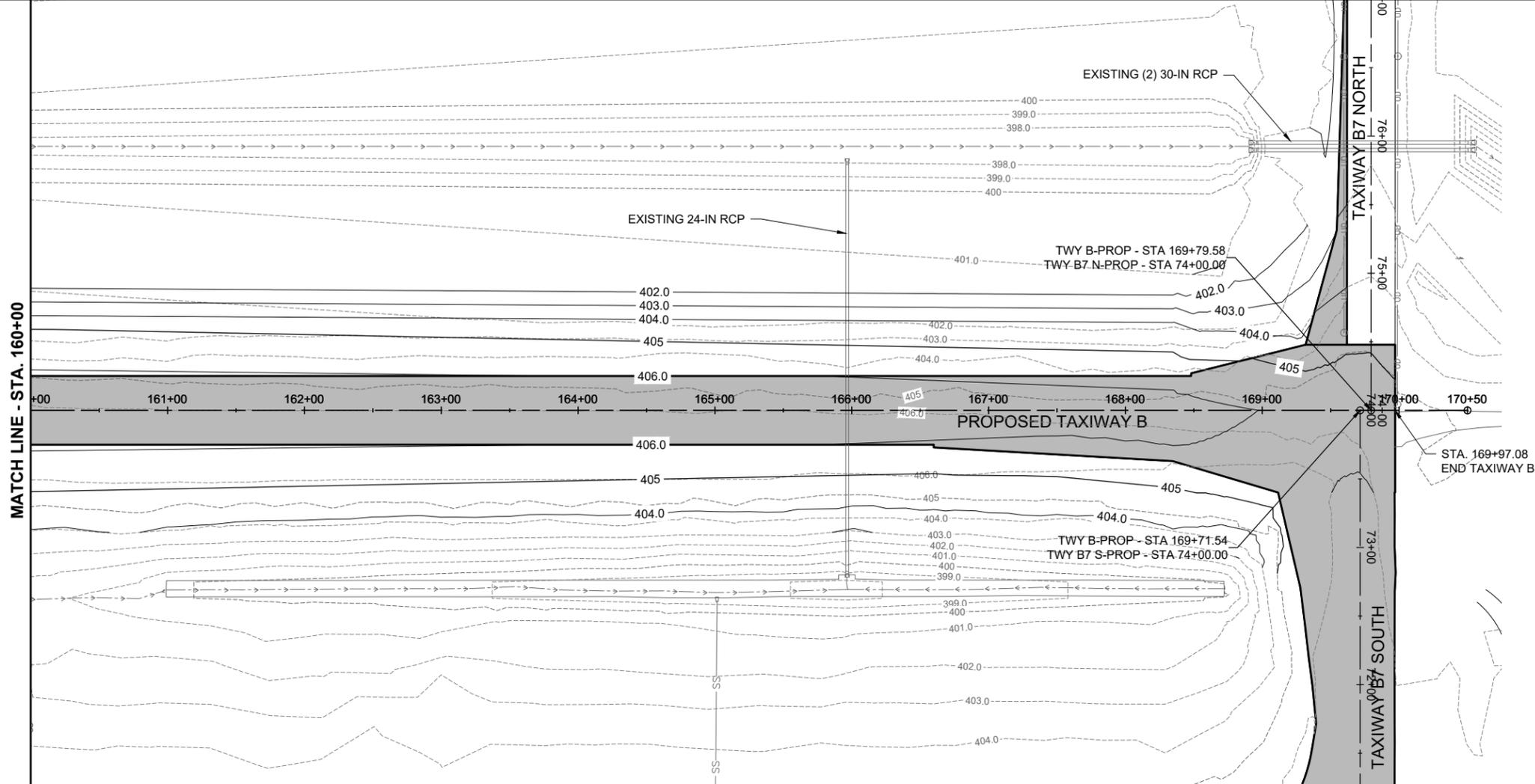
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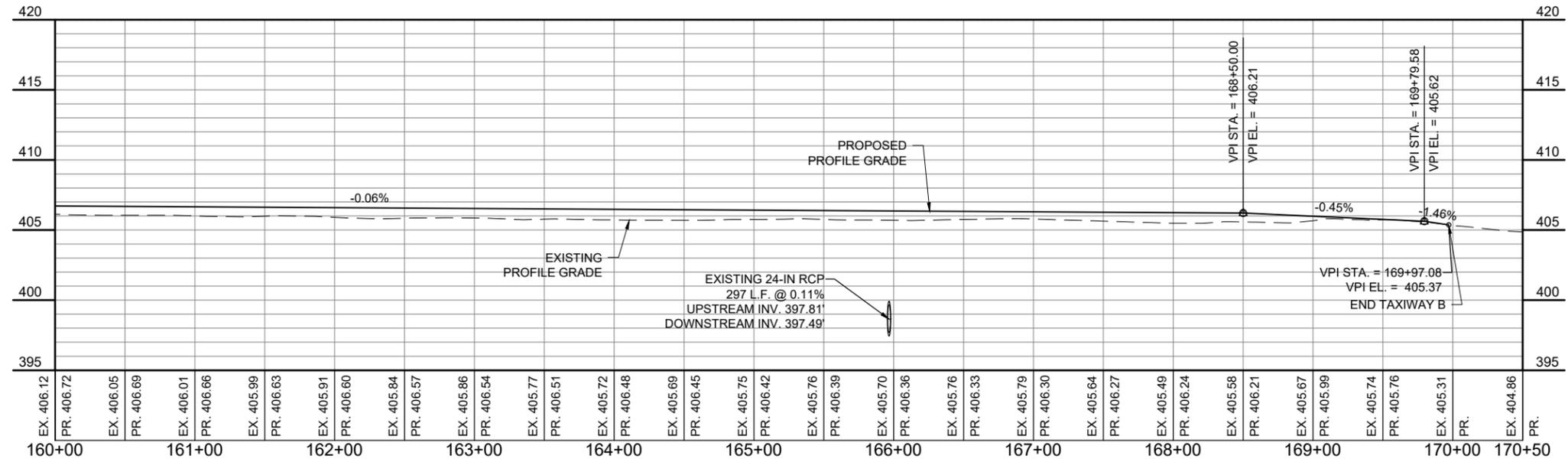
PROPOSED PLAN AND PROFILE - STA. 160+00 TO STA. 170+50



- LEGEND**
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  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED MILL & OVERLAY



PLAN VIEW - STA. 160+00 TO STA. 170+50



PROFILE VIEW - STA. 160+00 TO STA. 170+50

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**FOR BID**



**BI-STATE DEVELOPMENT**  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

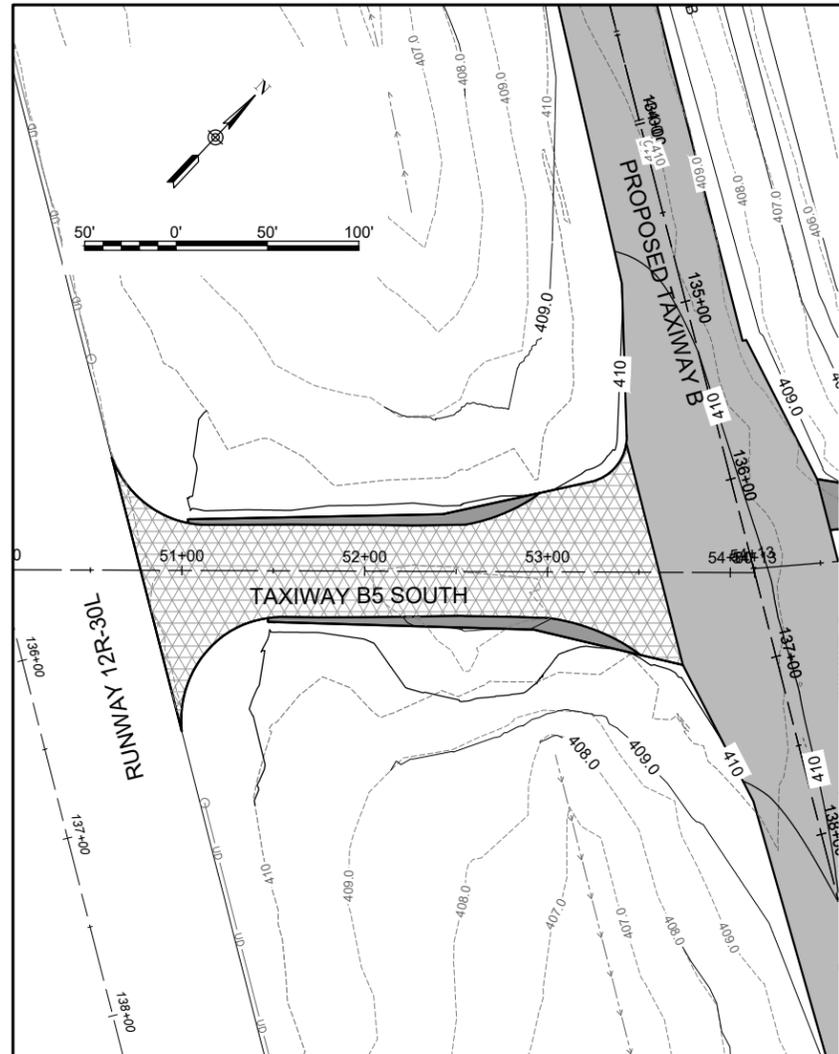
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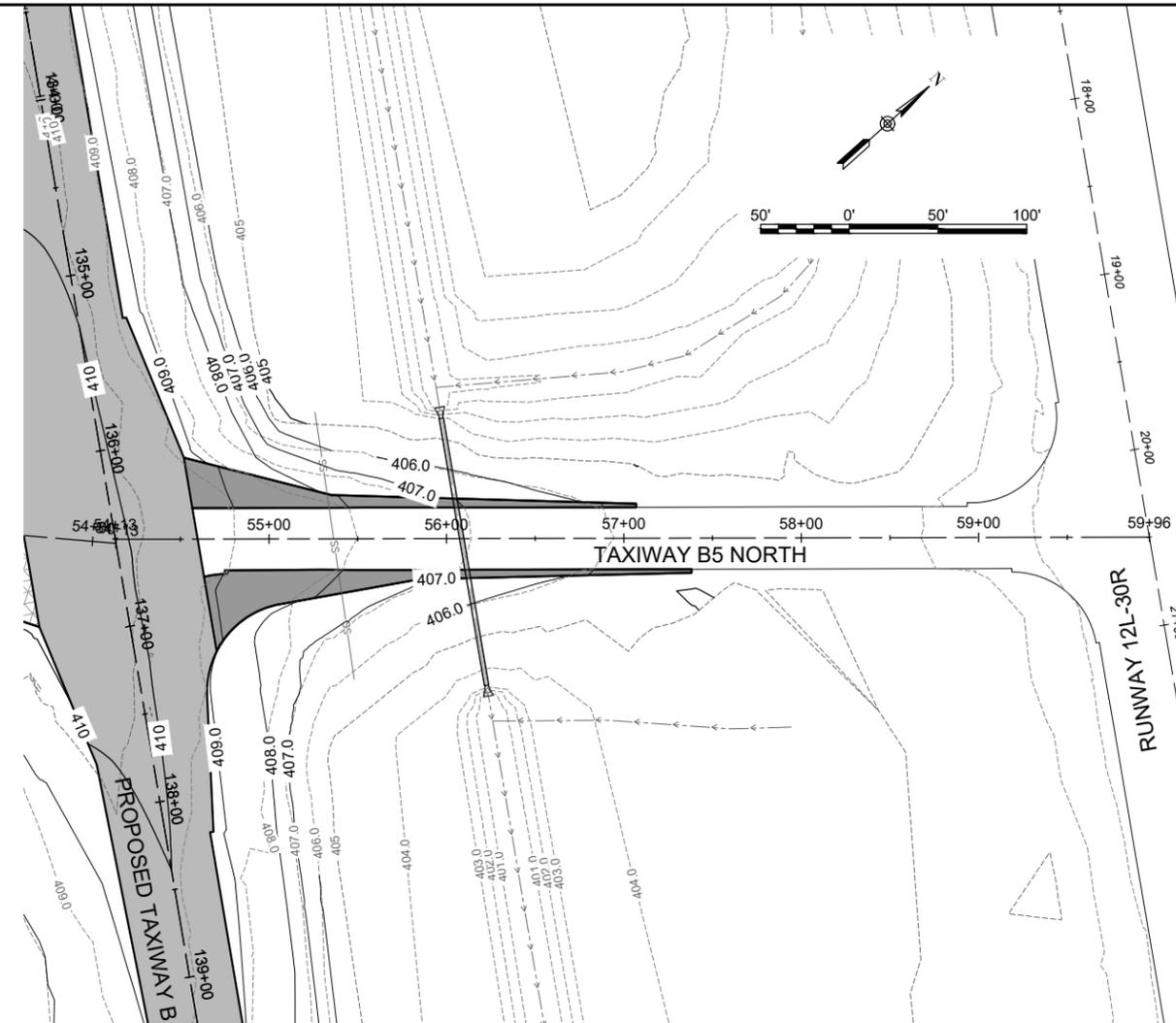
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PROJECT NO: 23A0001D  
CAD FILE: C-701-PNP.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 03/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED PLAN  
AND PROFILE -  
TAXIWAY B5

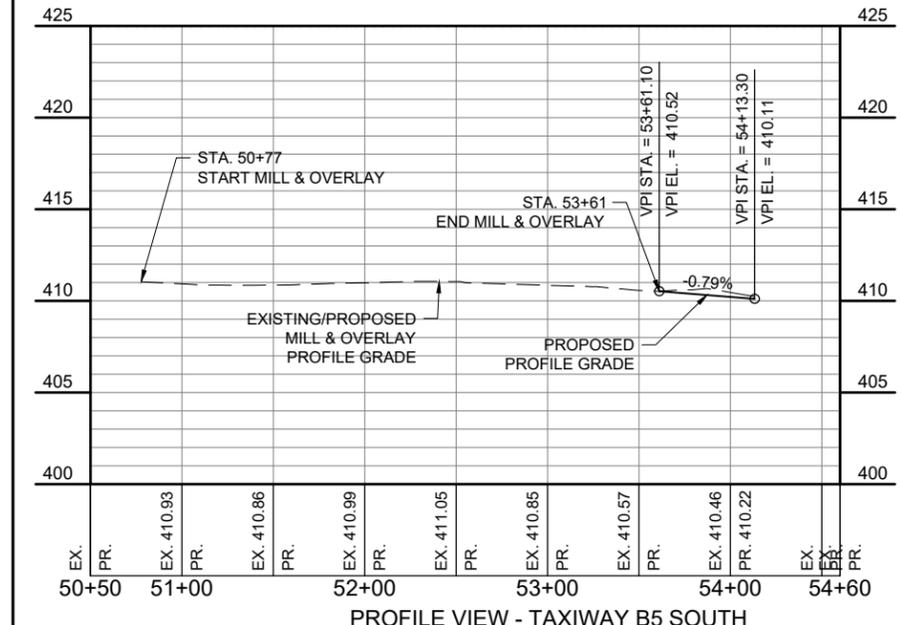


PLAN VIEW - TAXIWAY B5 SOUTH

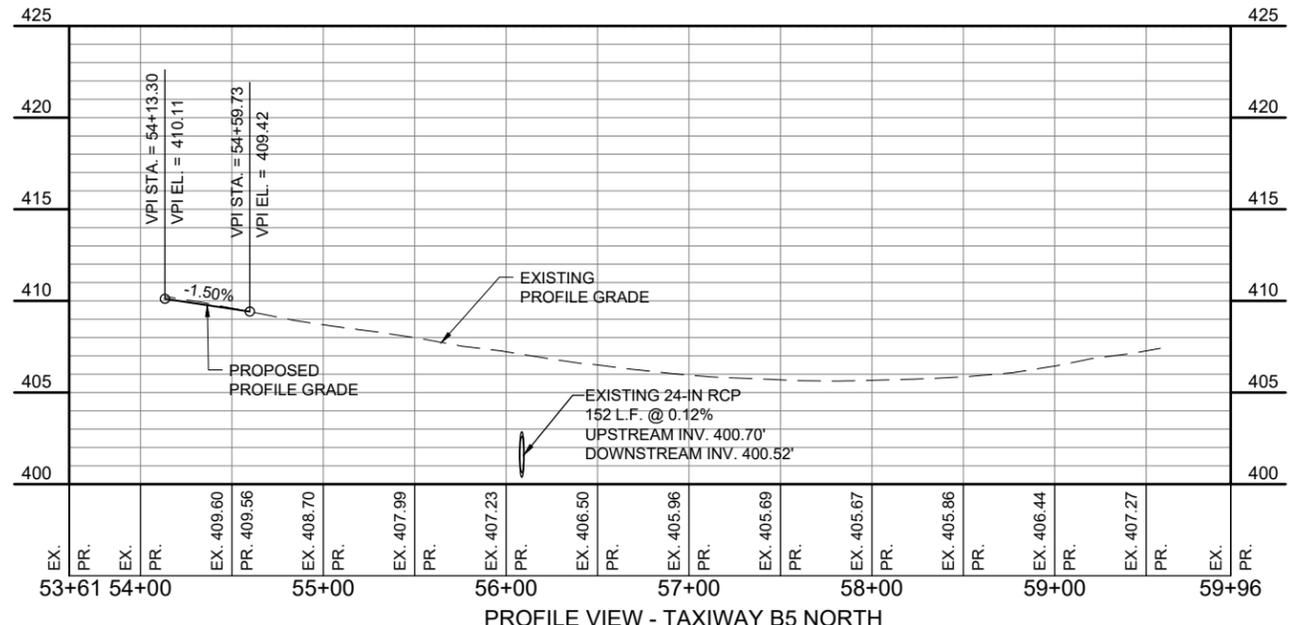


PLAN VIEW - TAXIWAY B5 NORTH

- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED MILL & OVERLAY



PROFILE VIEW - TAXIWAY B5 SOUTH



PROFILE VIEW - TAXIWAY B5 NORTH

**FOR BID**

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**BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

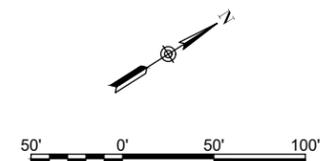
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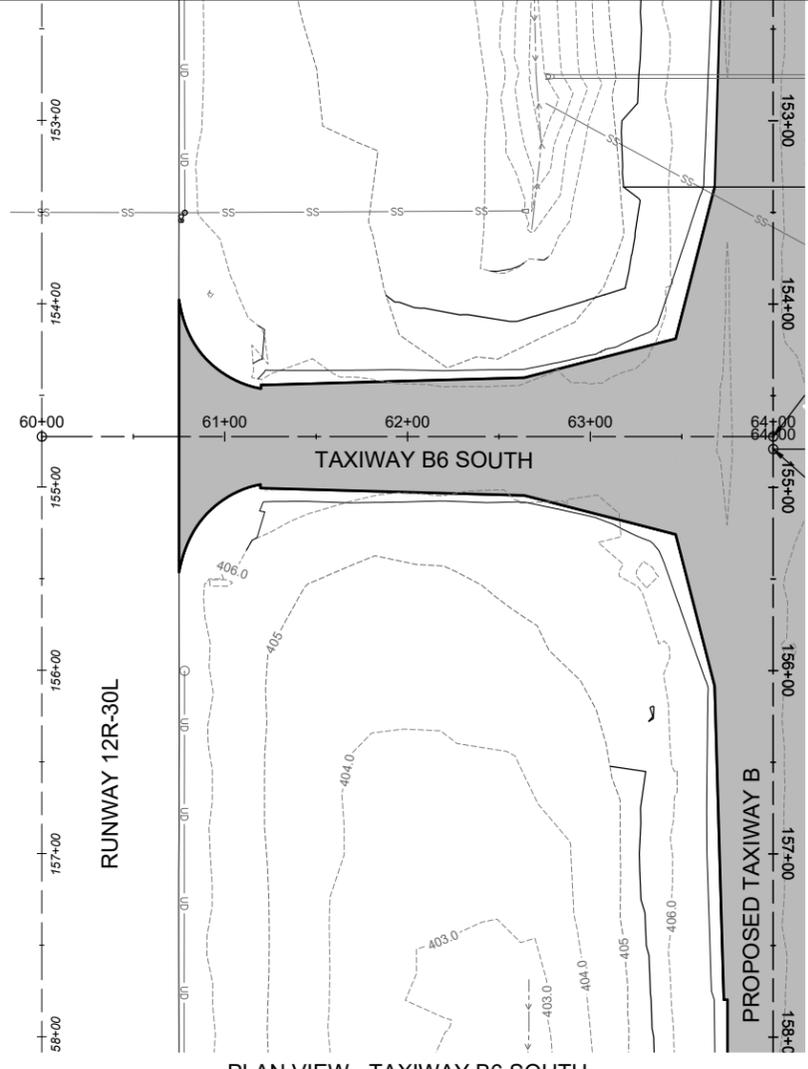
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-701-PNP.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 03/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

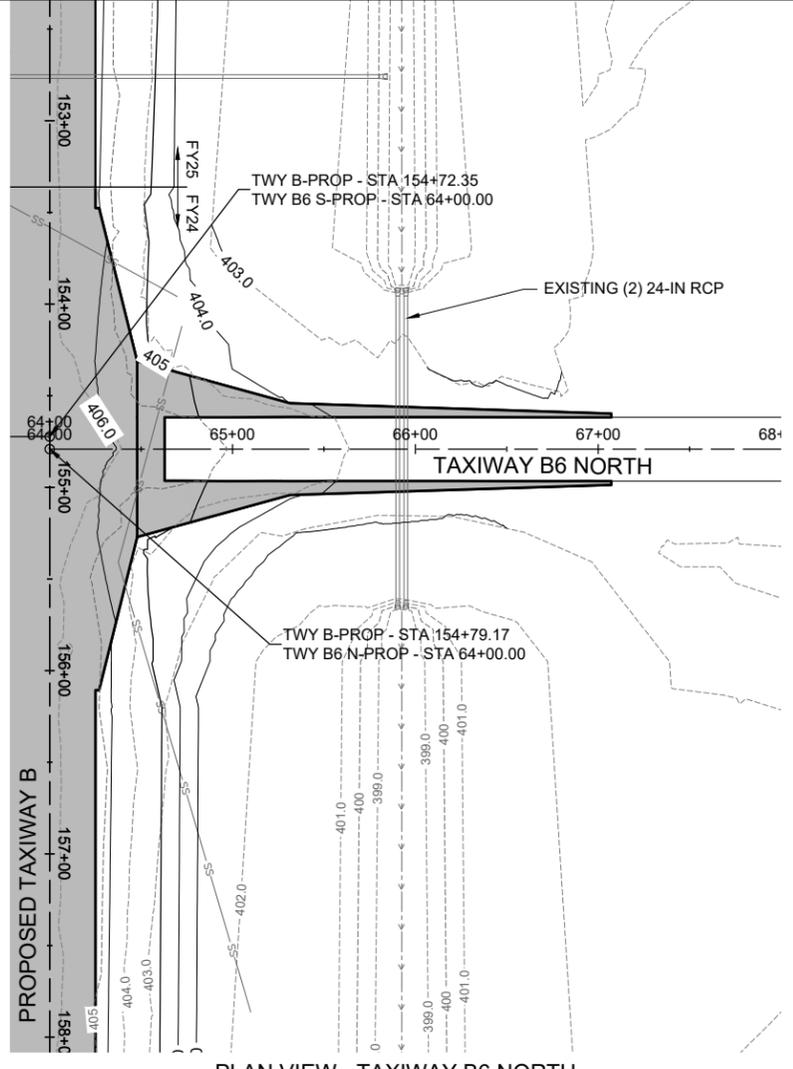
PROPOSED PLAN  
AND PROFILE -  
TAXIWAY B6



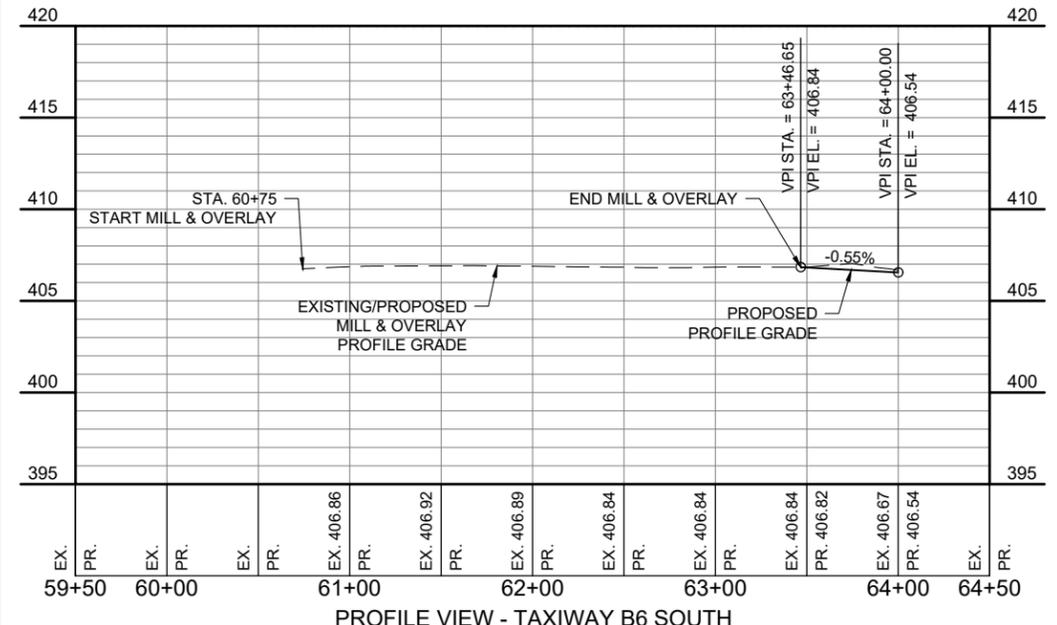
- LEGEND**
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  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED MILL & OVERLAY



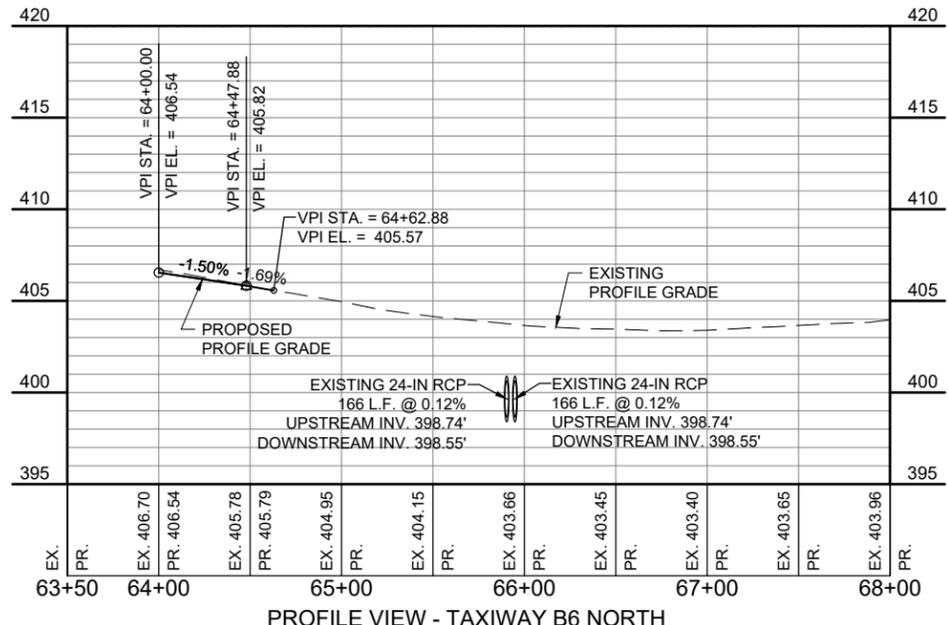
PLAN VIEW - TAXIWAY B6 SOUTH



PLAN VIEW - TAXIWAY B6 NORTH



PROFILE VIEW - TAXIWAY B6 SOUTH



PROFILE VIEW - TAXIWAY B6 NORTH

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**FOR BID**



**BI-STATE DEVELOPMENT**  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

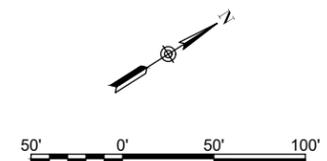
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CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
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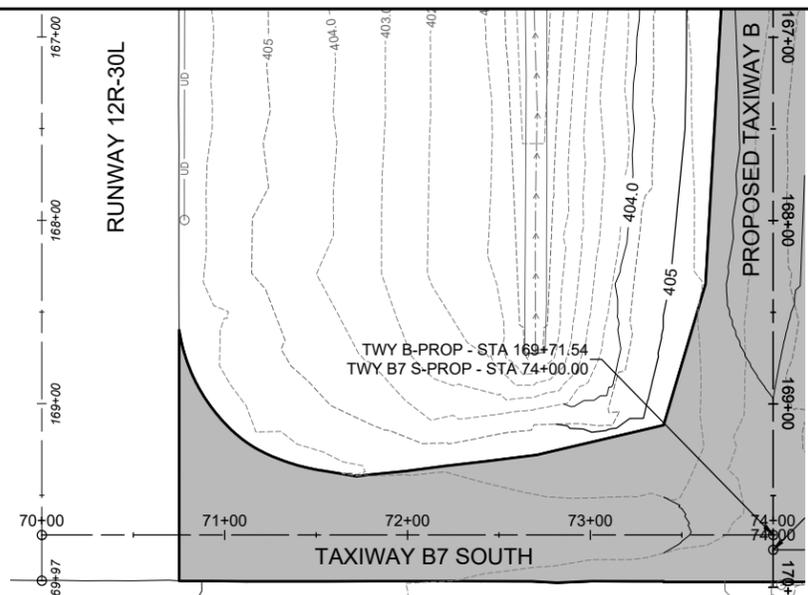
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PROJECT NO: 23A0001D  
CAD FILE: C-701-PNP.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 03/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

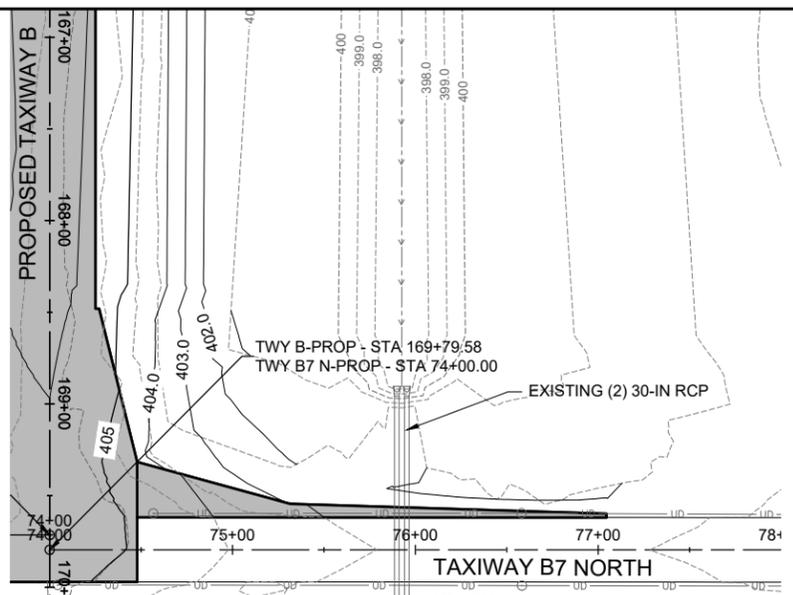
PROPOSED PLAN  
AND PROFILE -  
TAXIWAY B7



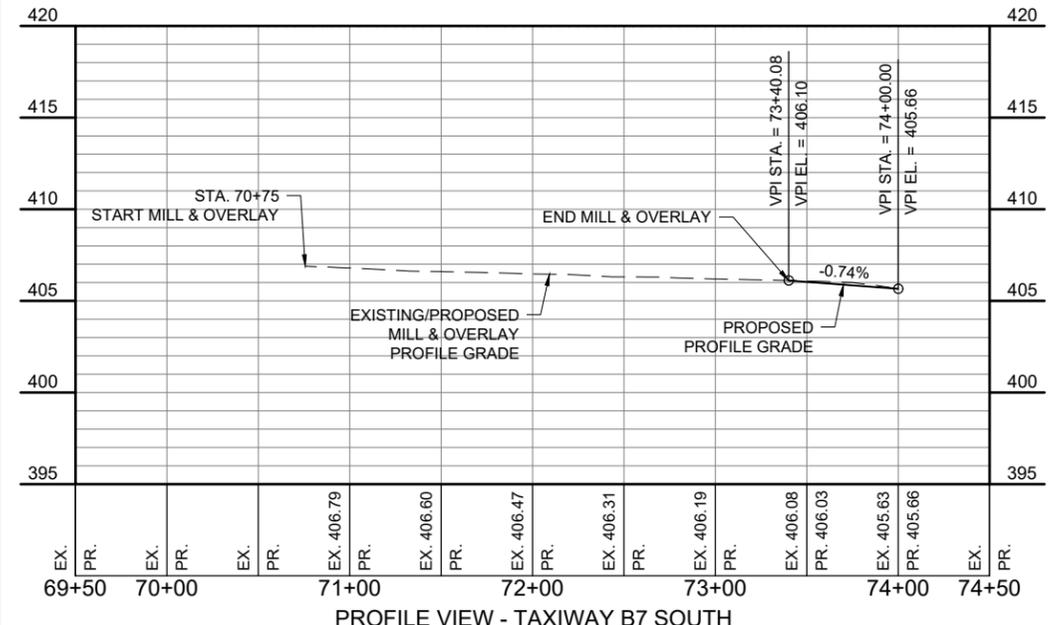
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- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED CONCRETE PAVEMENT
  - PROPOSED BITUMINOUS PAVEMENT
  - PROPOSED MILL & OVERLAY



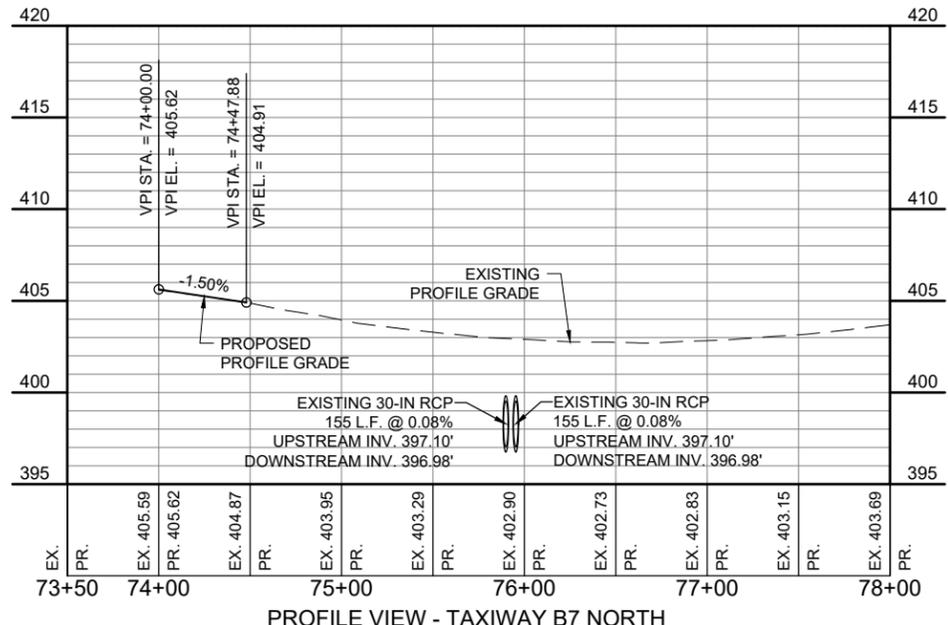
PLAN VIEW - TAXIWAY B7 SOUTH



PLAN VIEW - TAXIWAY B7 NORTH



PROFILE VIEW - TAXIWAY B7 SOUTH



PROFILE VIEW - TAXIWAY B7 NORTH

**FOR BID**

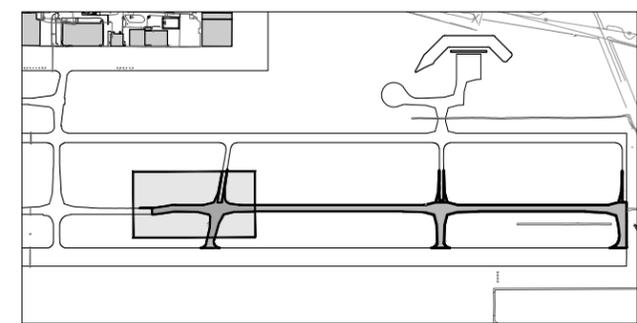
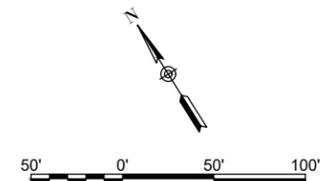
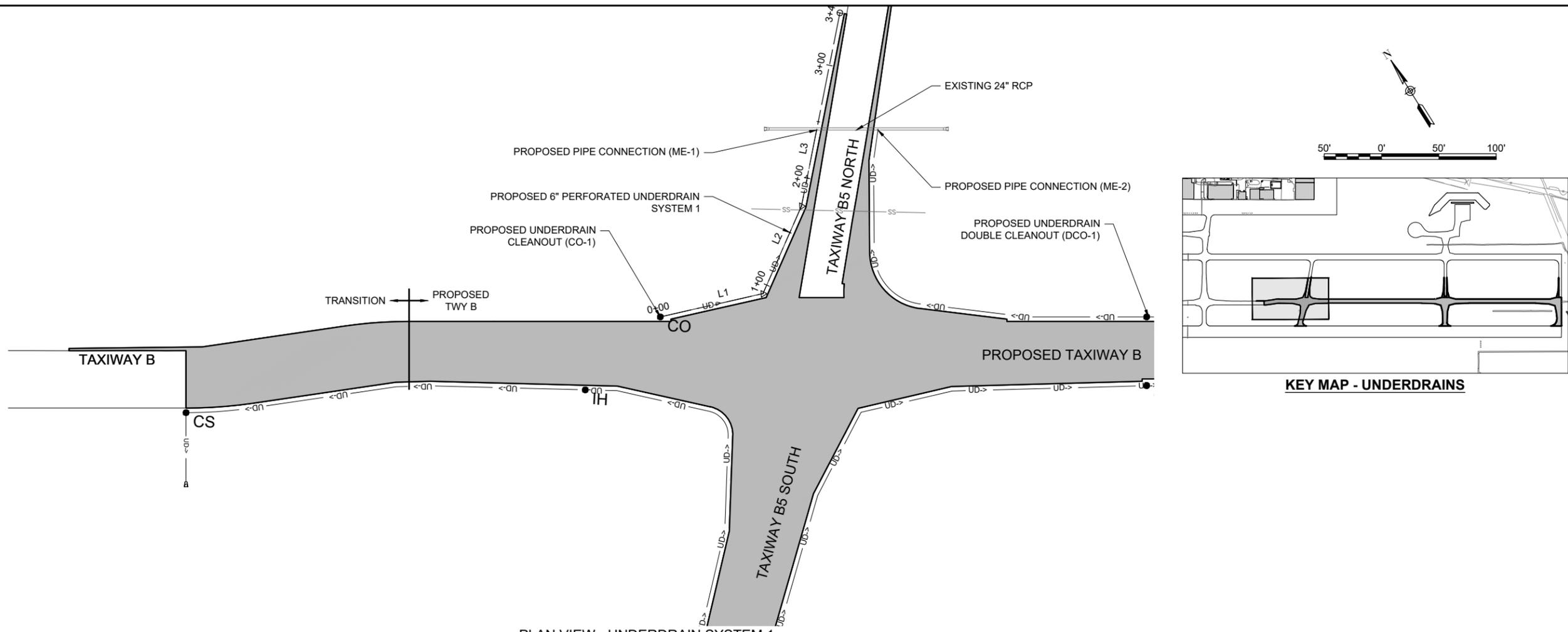
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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

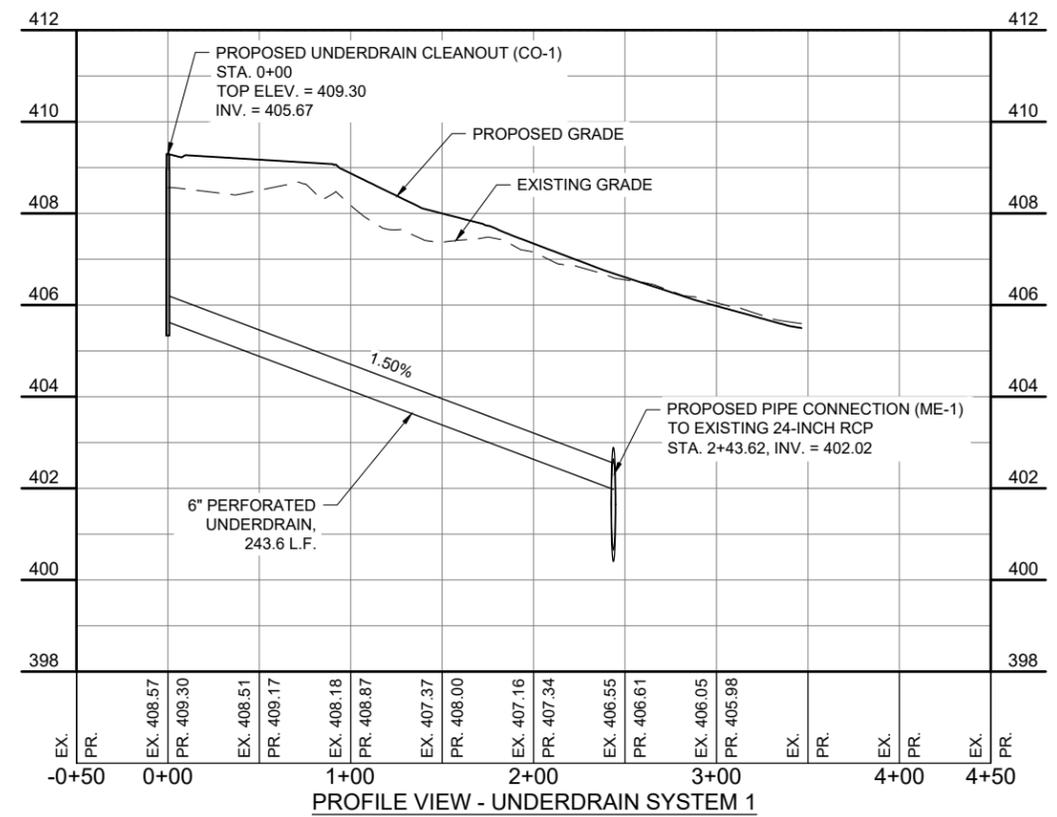
SHEET TITLE

PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 1



KEY MAP - UNDERDRAINS

PLAN VIEW - UNDERDRAIN SYSTEM 1



PROFILE VIEW - UNDERDRAIN SYSTEM 1

FOR BID



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

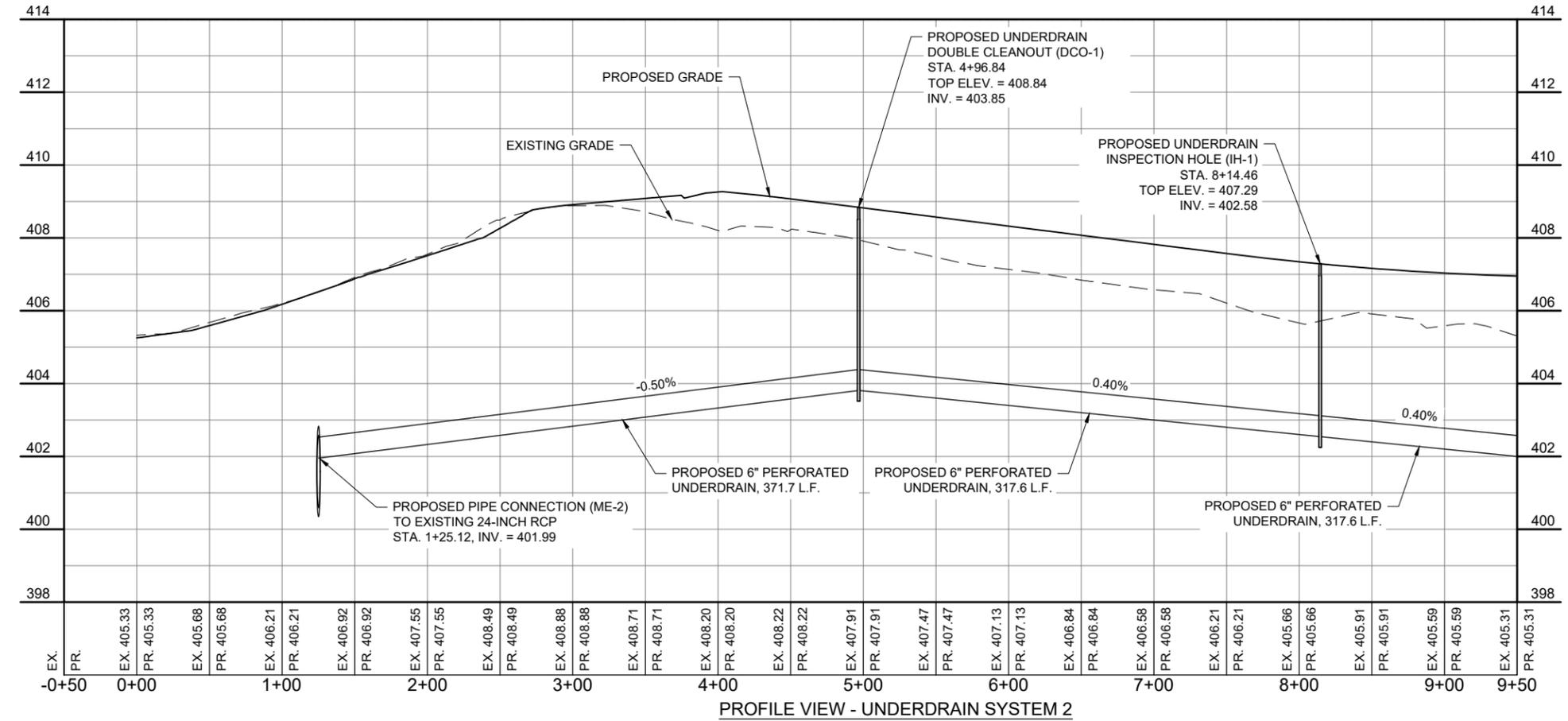
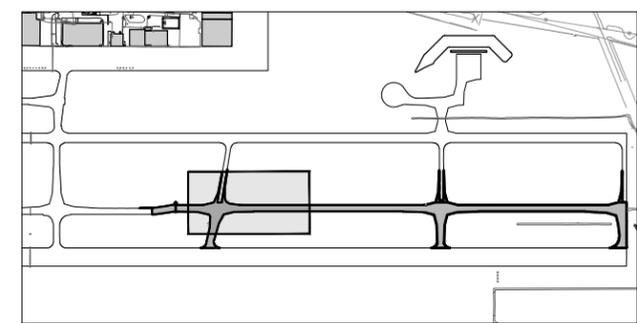
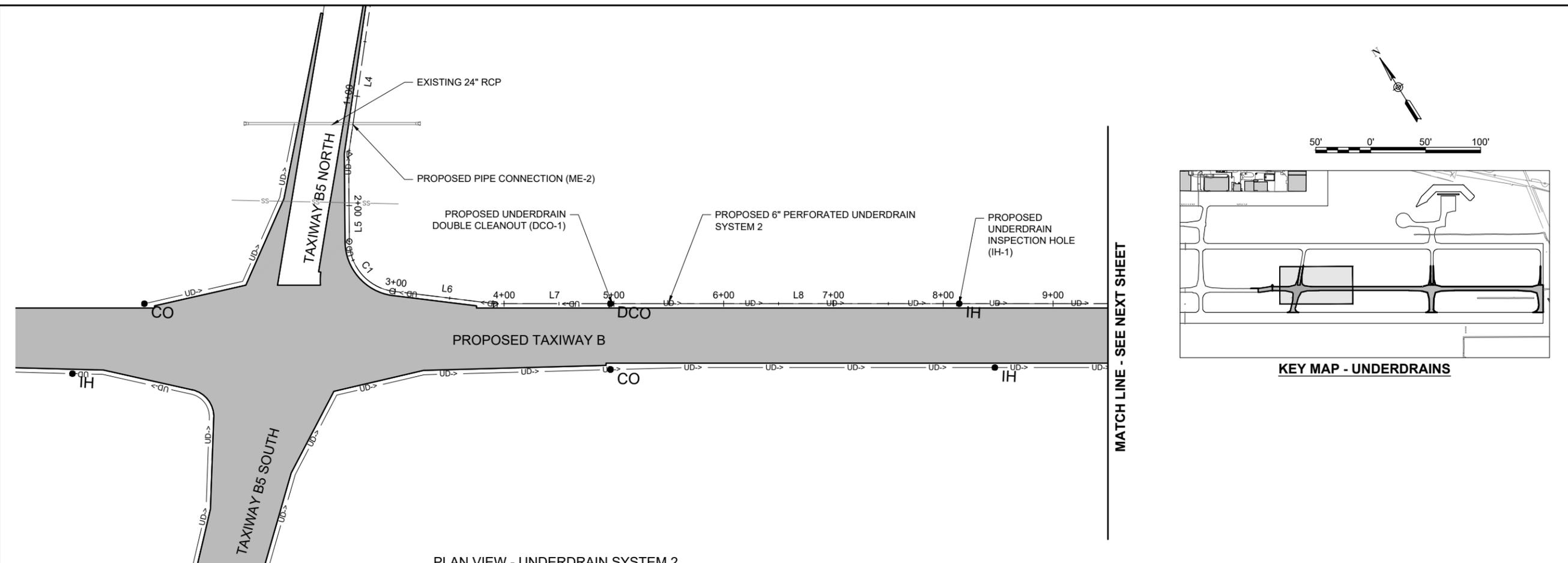
RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION  
IDA NO.: CPS-5078  
CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 2 (SHEET 1)



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**FOR BID**



**ST. LOUIS DOWNTOWN AIRPORT**  
BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

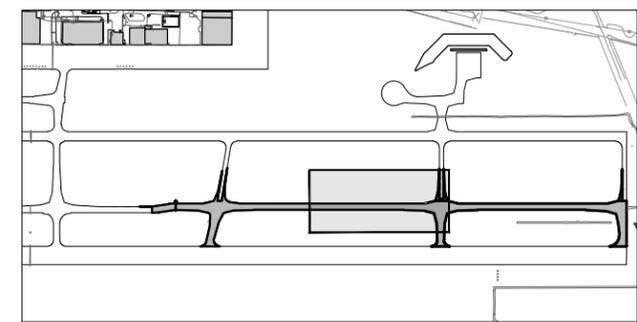
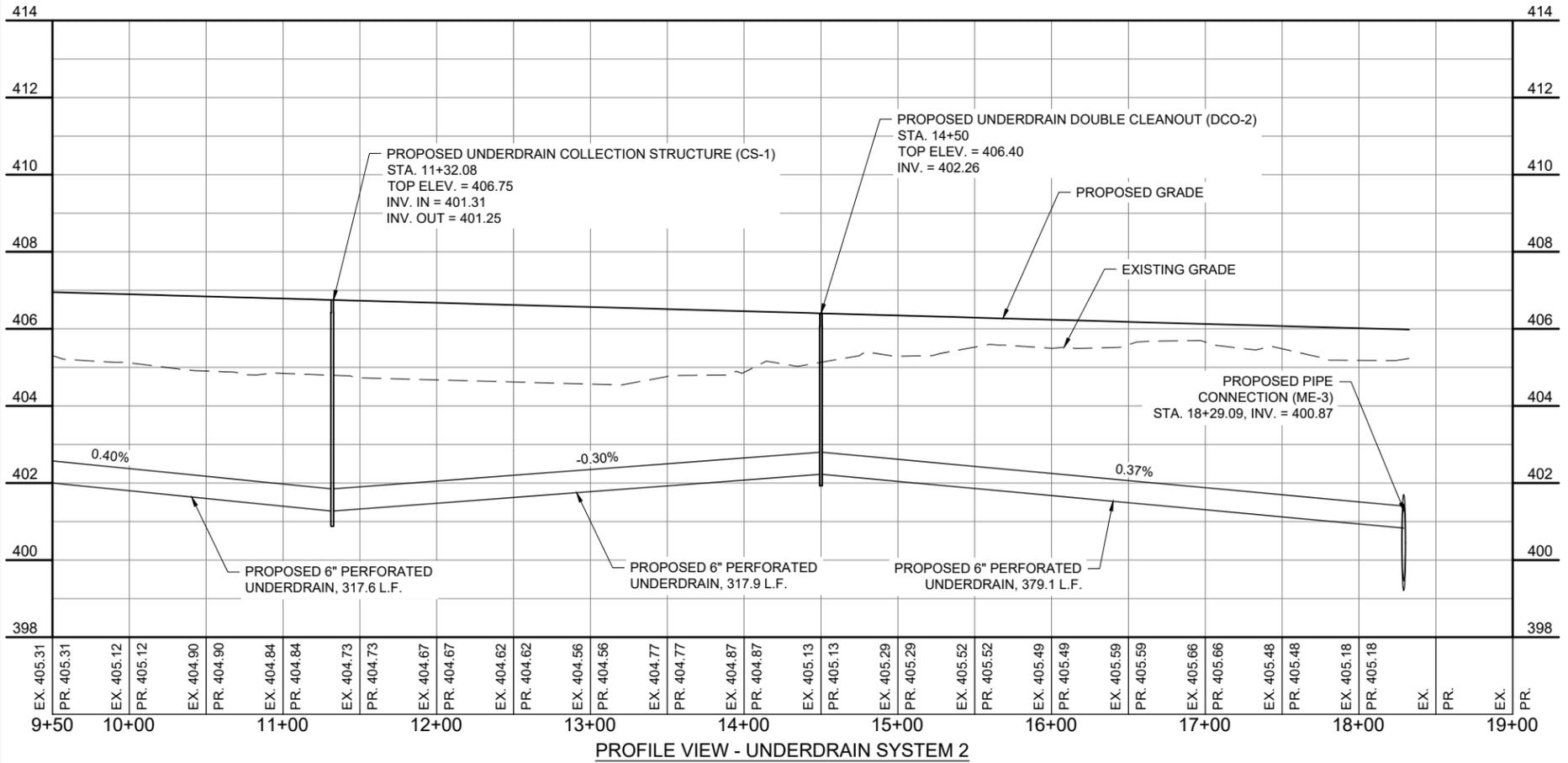
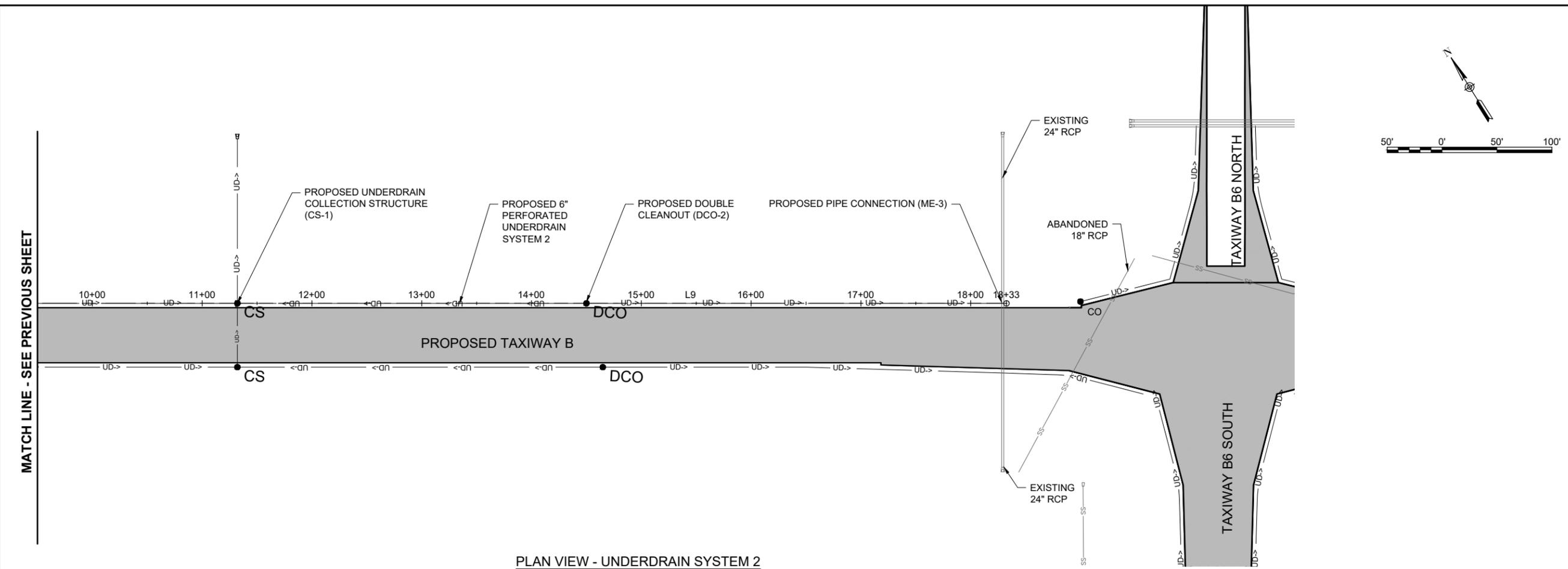
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DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 2 (SHEET 2)

**FOR BID**



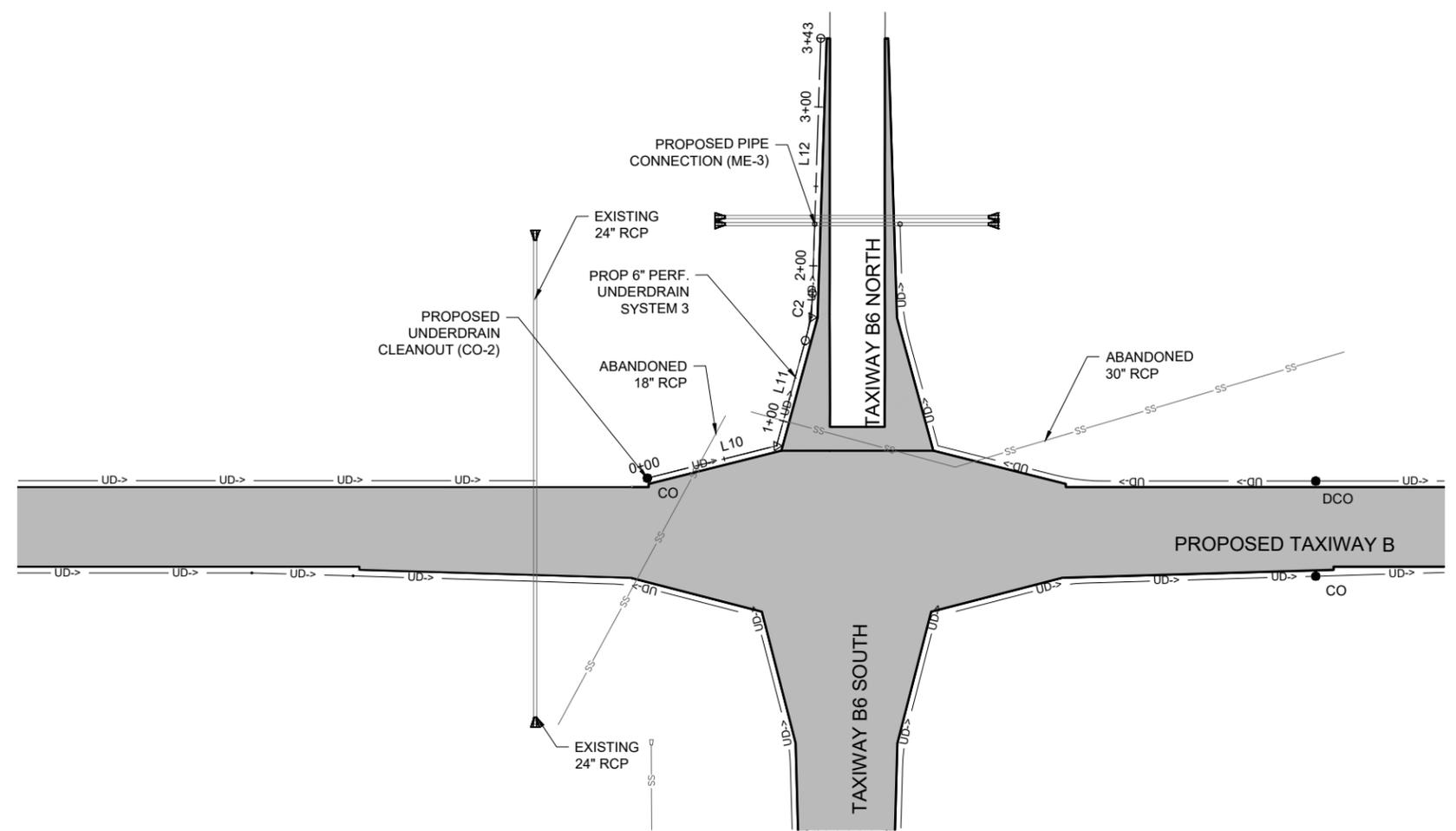
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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

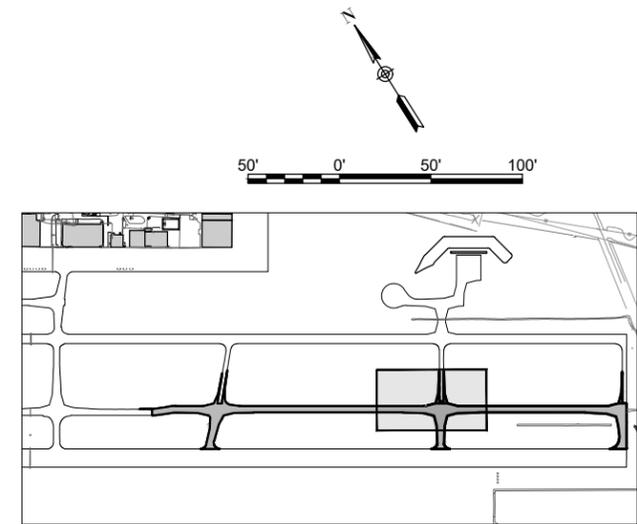
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

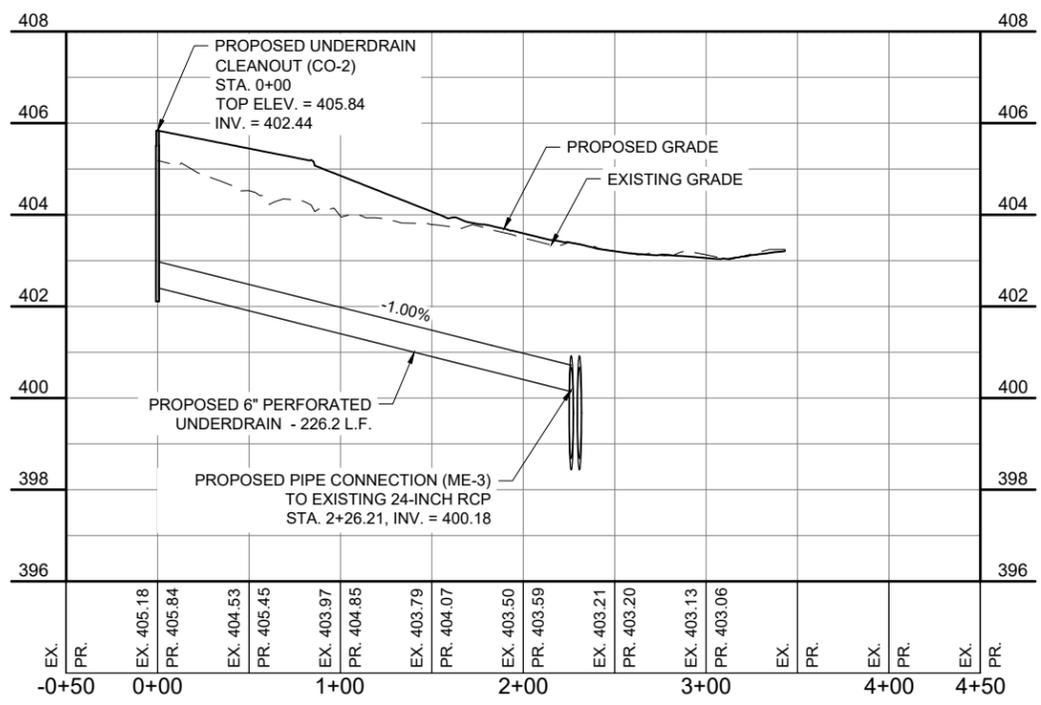
PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 3



PLAN VIEW - UNDERDRAIN SYSTEM 3



KEY MAP - UNDERDRAINS



PROFILE VIEW - UNDERDRAIN SYSTEM 3

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BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

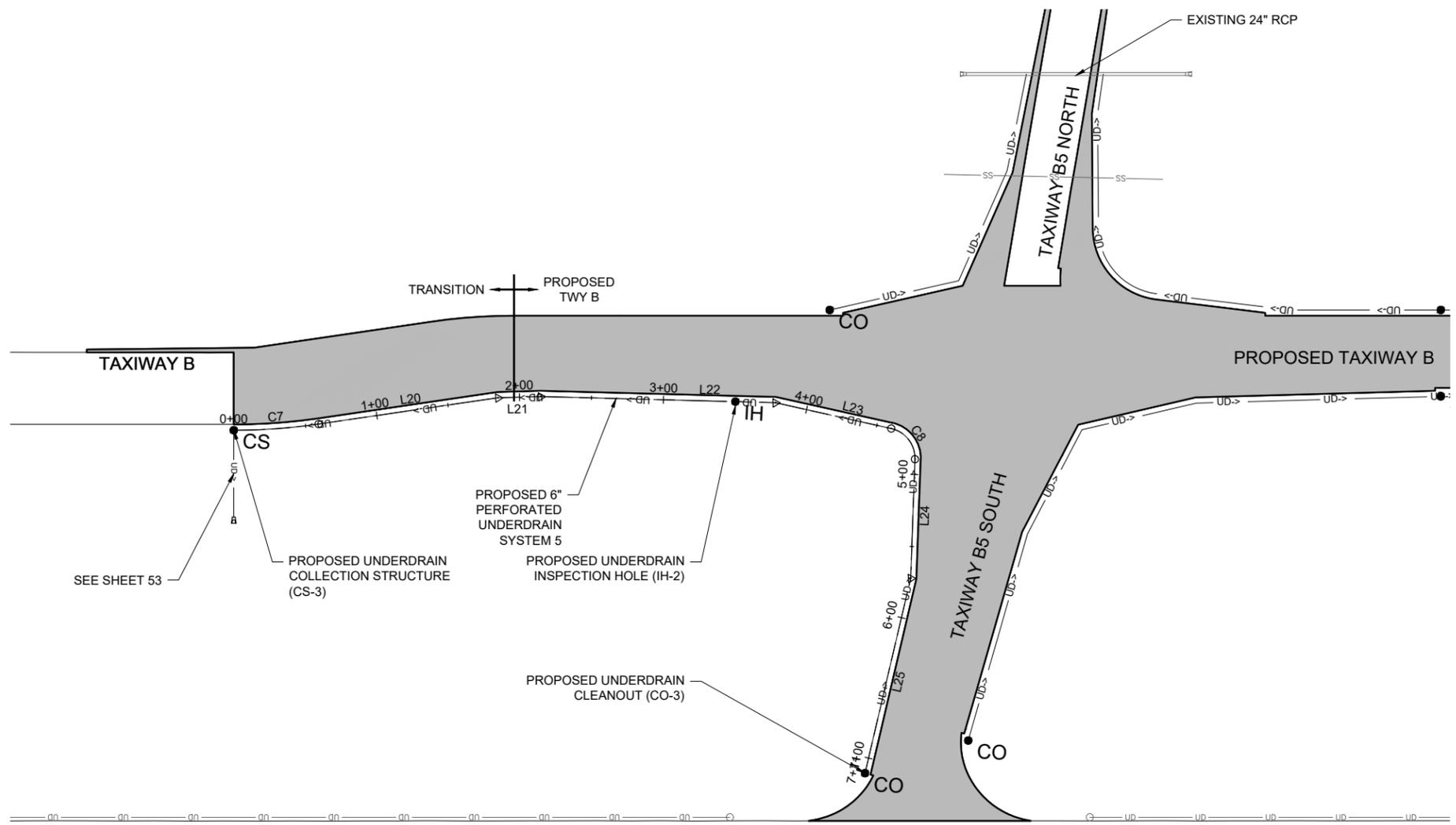

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

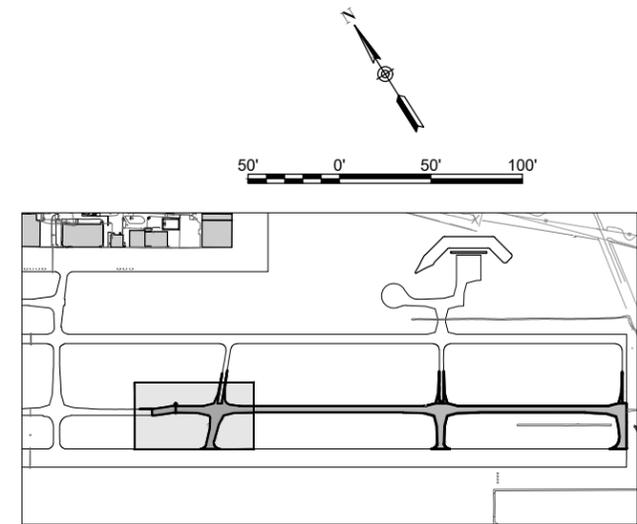
SHEET TITLE

PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 5

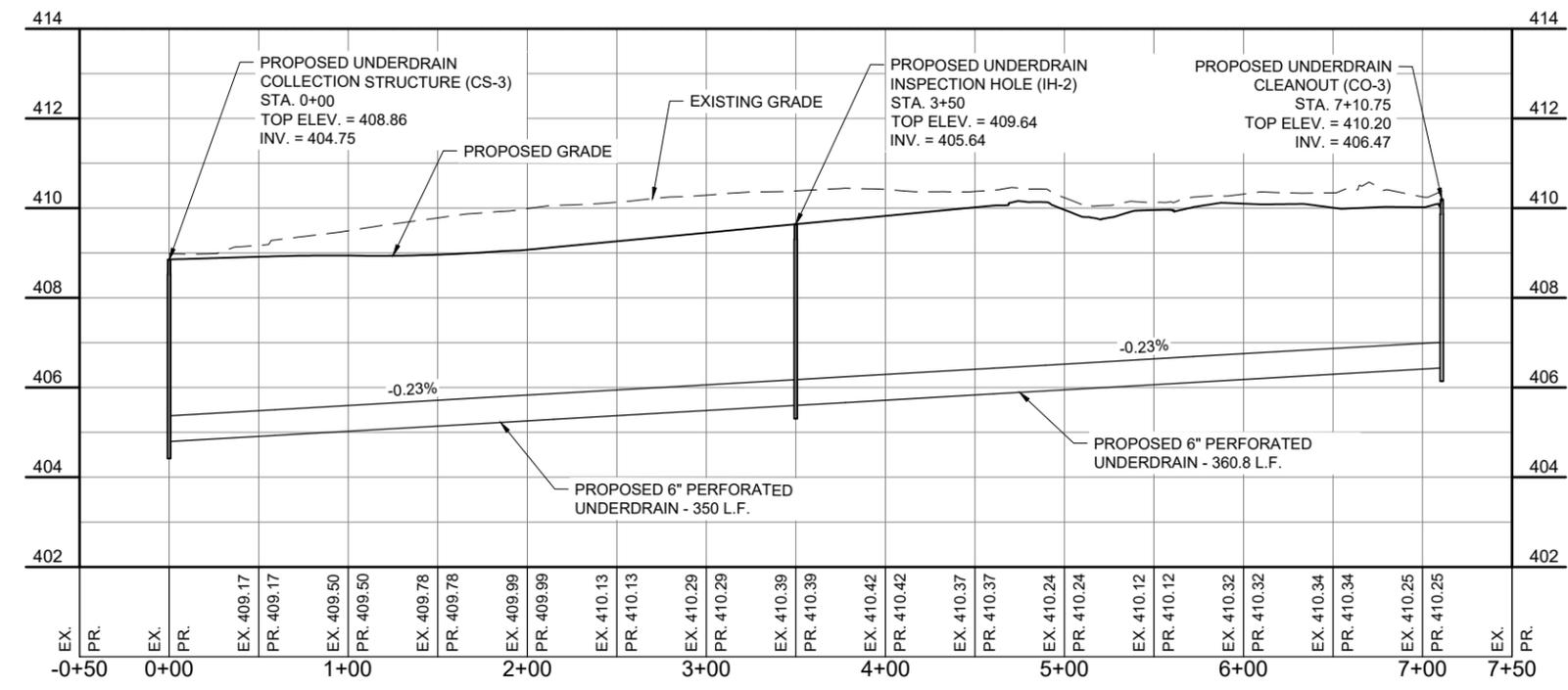
**FOR BID**



PLAN VIEW - UNDERDRAIN SYSTEM 5



KEY MAP - UNDERDRAINS



PROFILE VIEW - UNDERDRAIN SYSTEM 5

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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

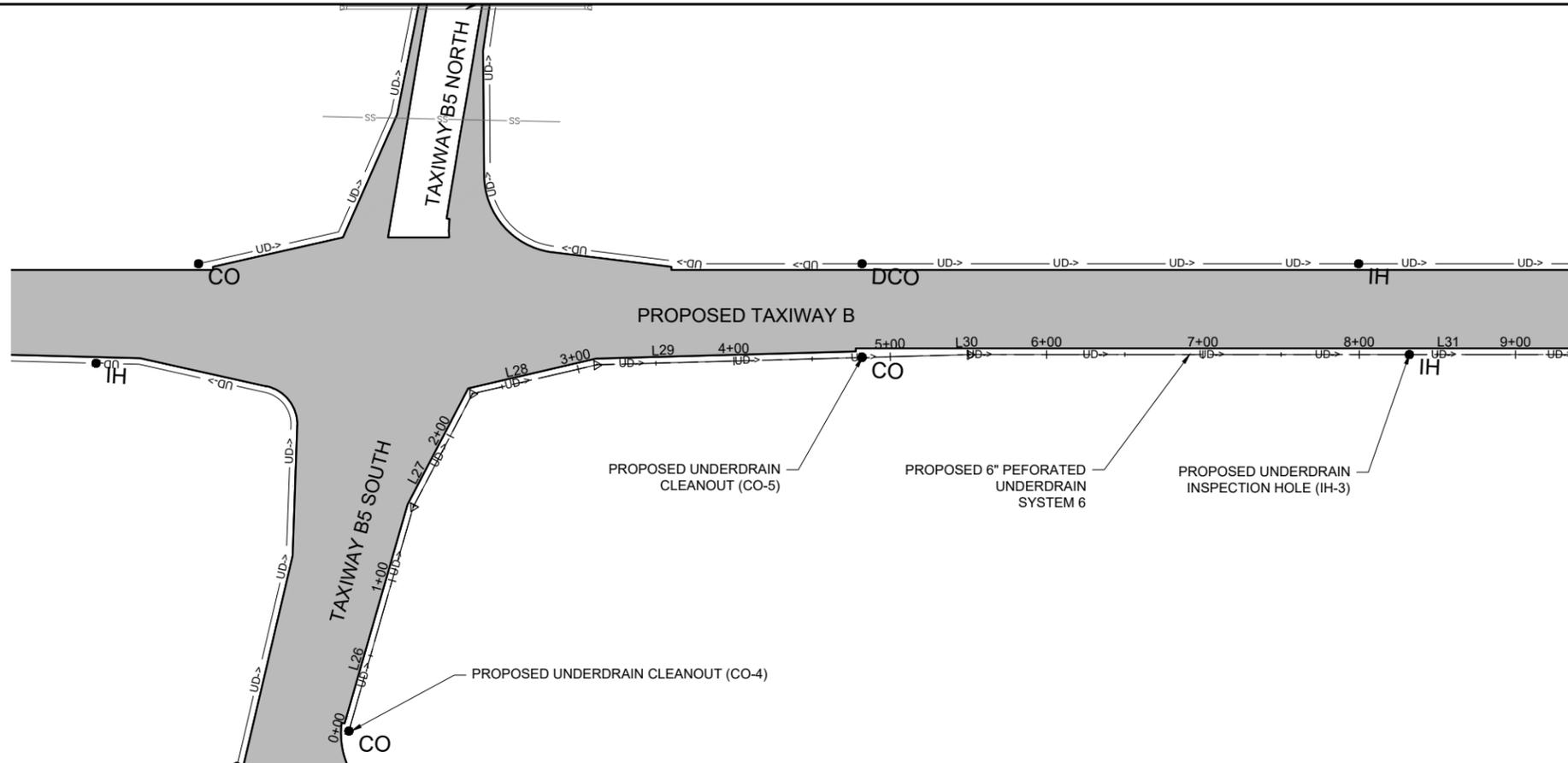
IDA NO.: CPS-5078 CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

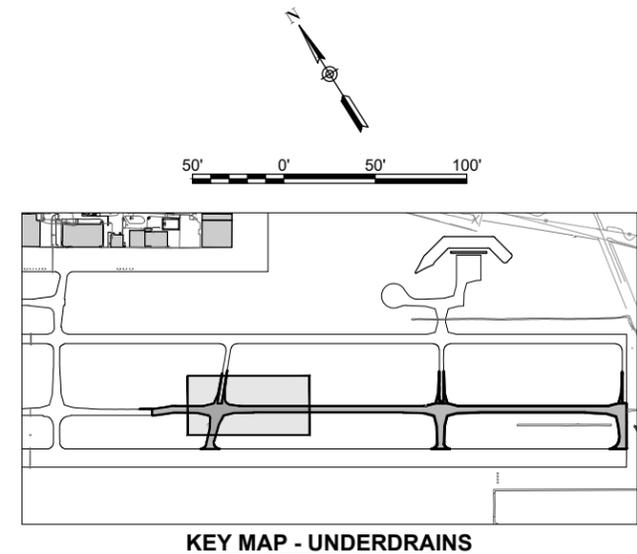
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 6 (SHEET 1)

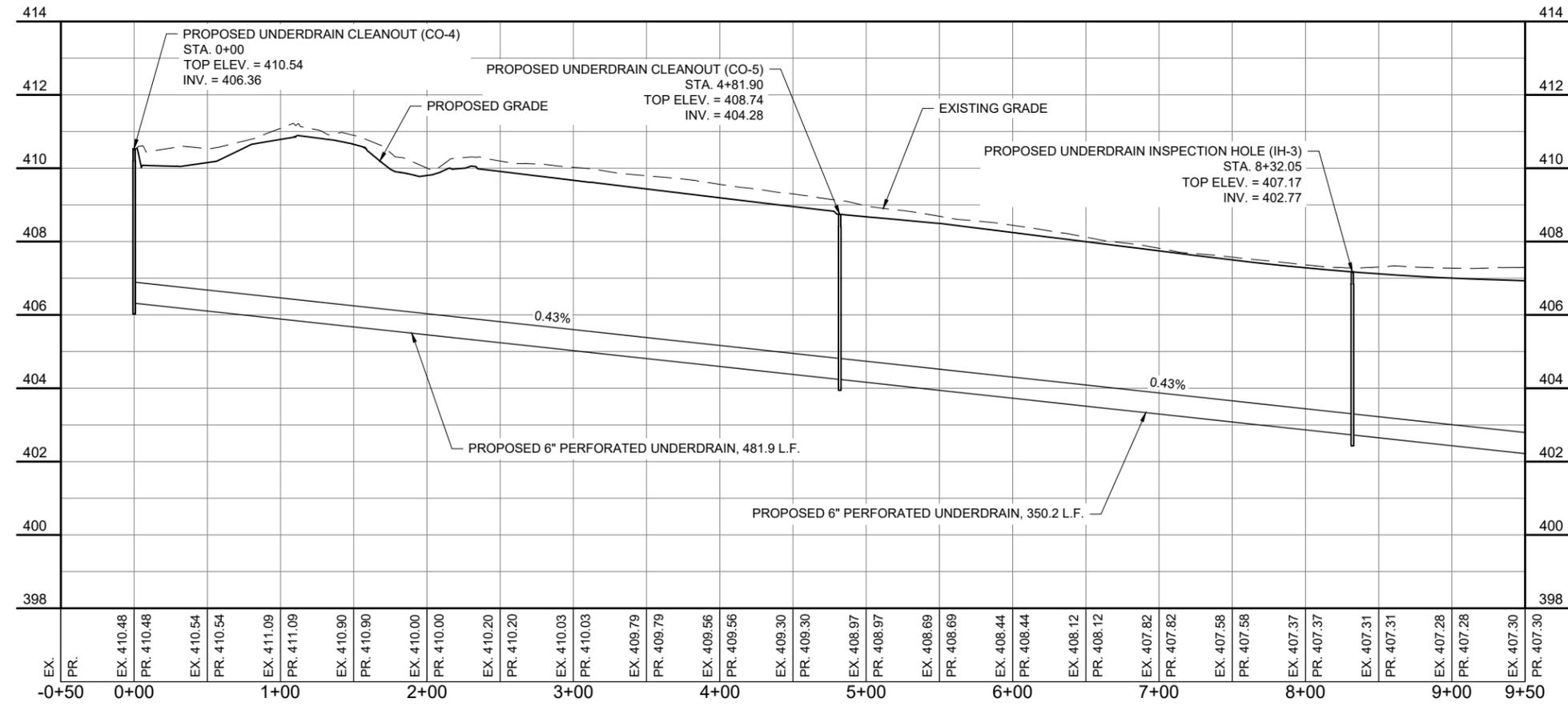


MATCH LINE - SEE NEXT SHEET



KEY MAP - UNDERDRAINS

PLAN VIEW - UNDERDRAIN SYSTEM 6



PROFILE VIEW - UNDERDRAIN SYSTEM 6

EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.		
-0+50		0+00		1+00		2+00		3+00		4+00		5+00		6+00		7+00		8+00		9+00		9+50	

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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

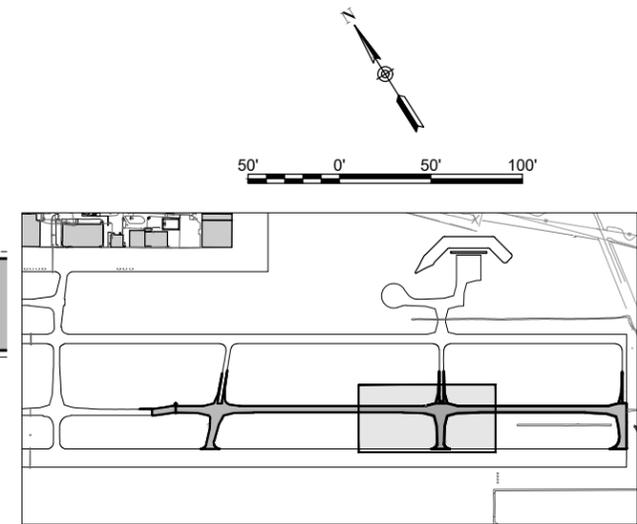
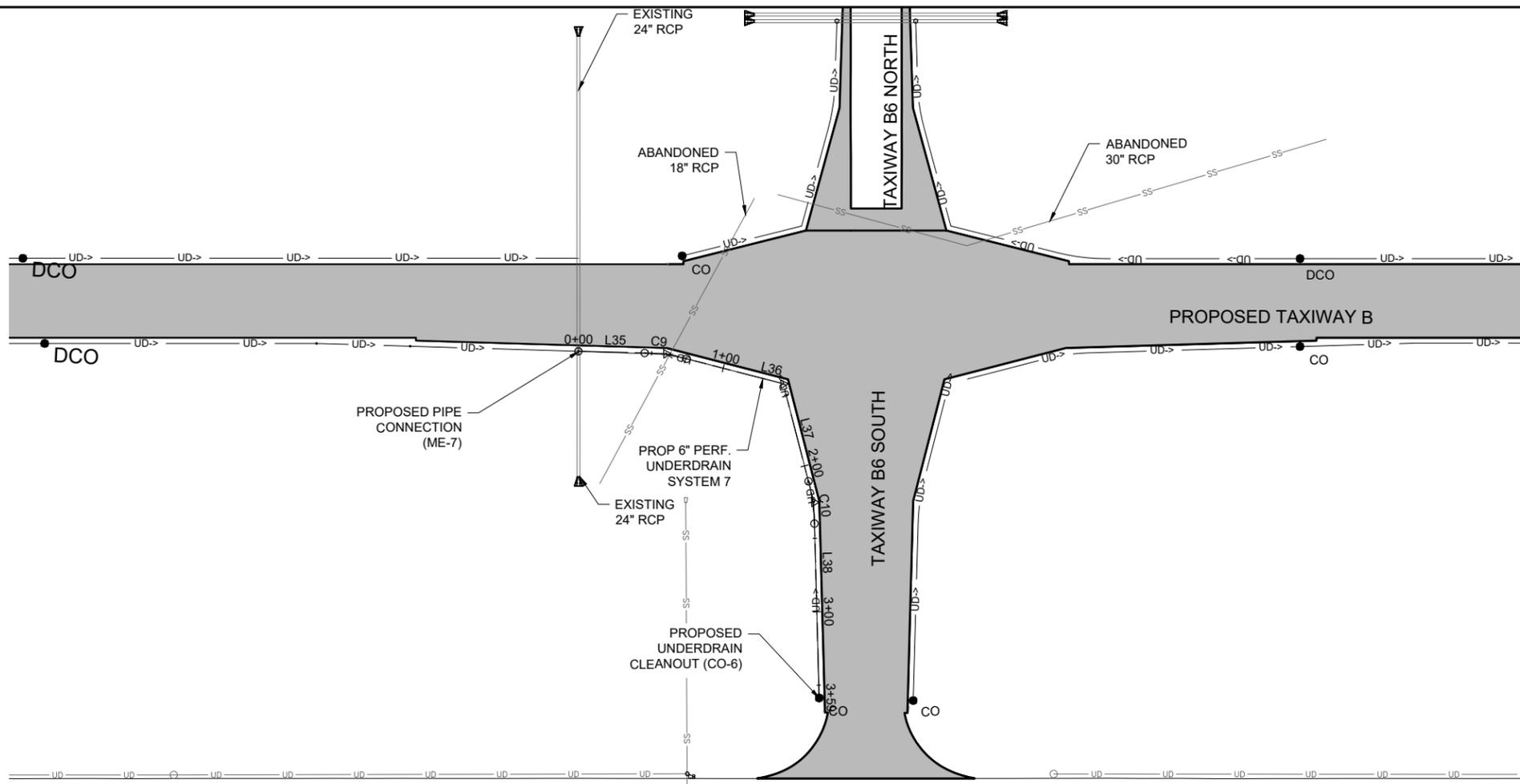

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

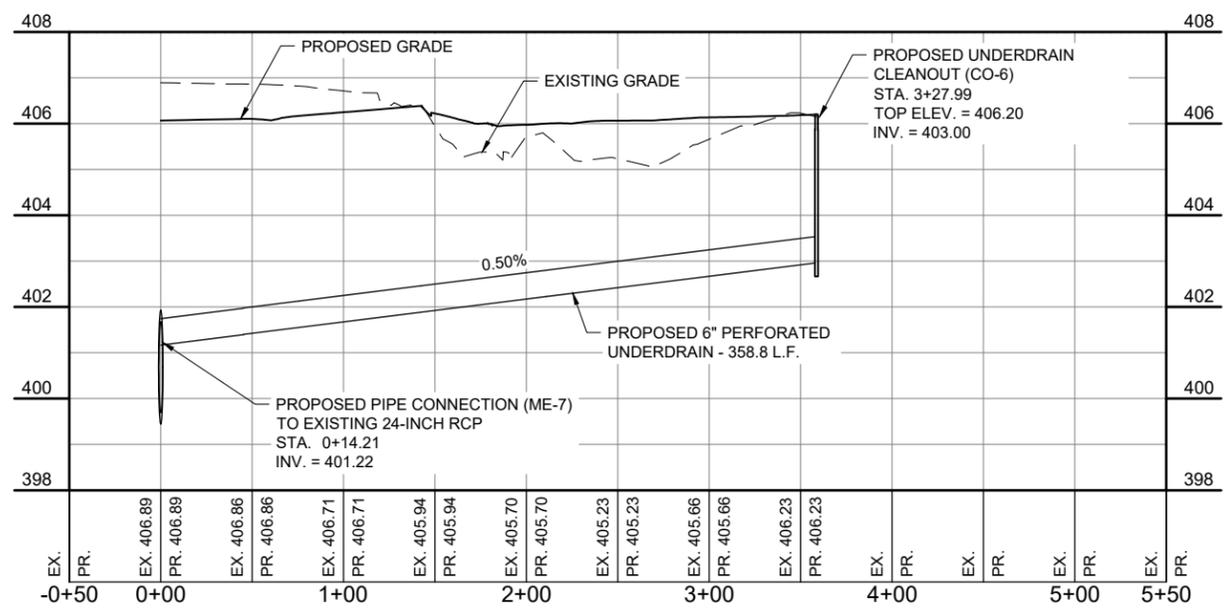
SHEET TITLE

PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 7

**FOR BID**



PLAN VIEW - UNDERDRAIN SYSTEM 7



PROFILE VIEW - UNDERDRAIN SYSTEM 7

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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

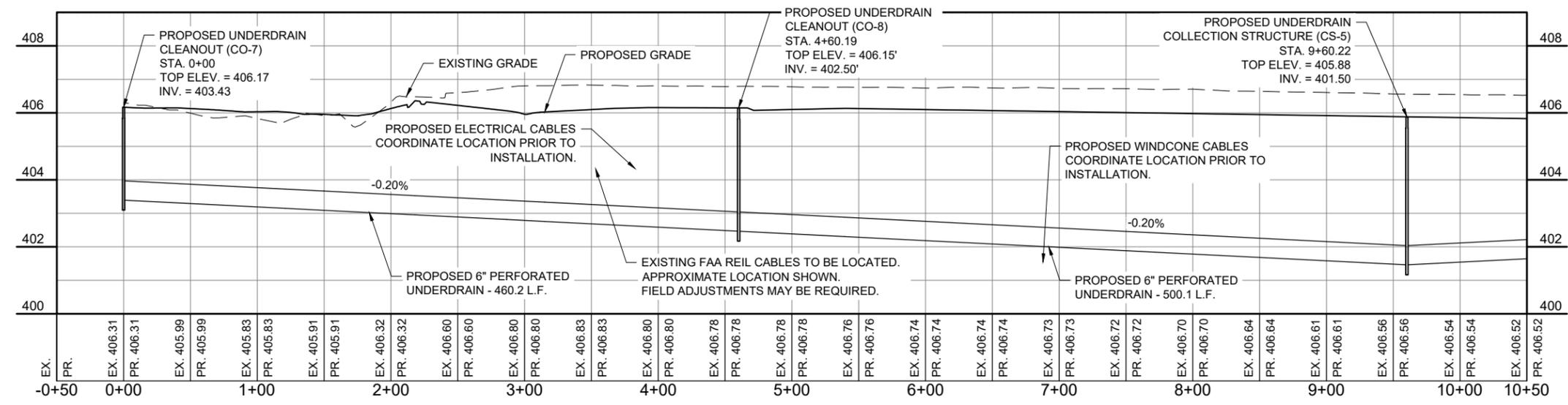
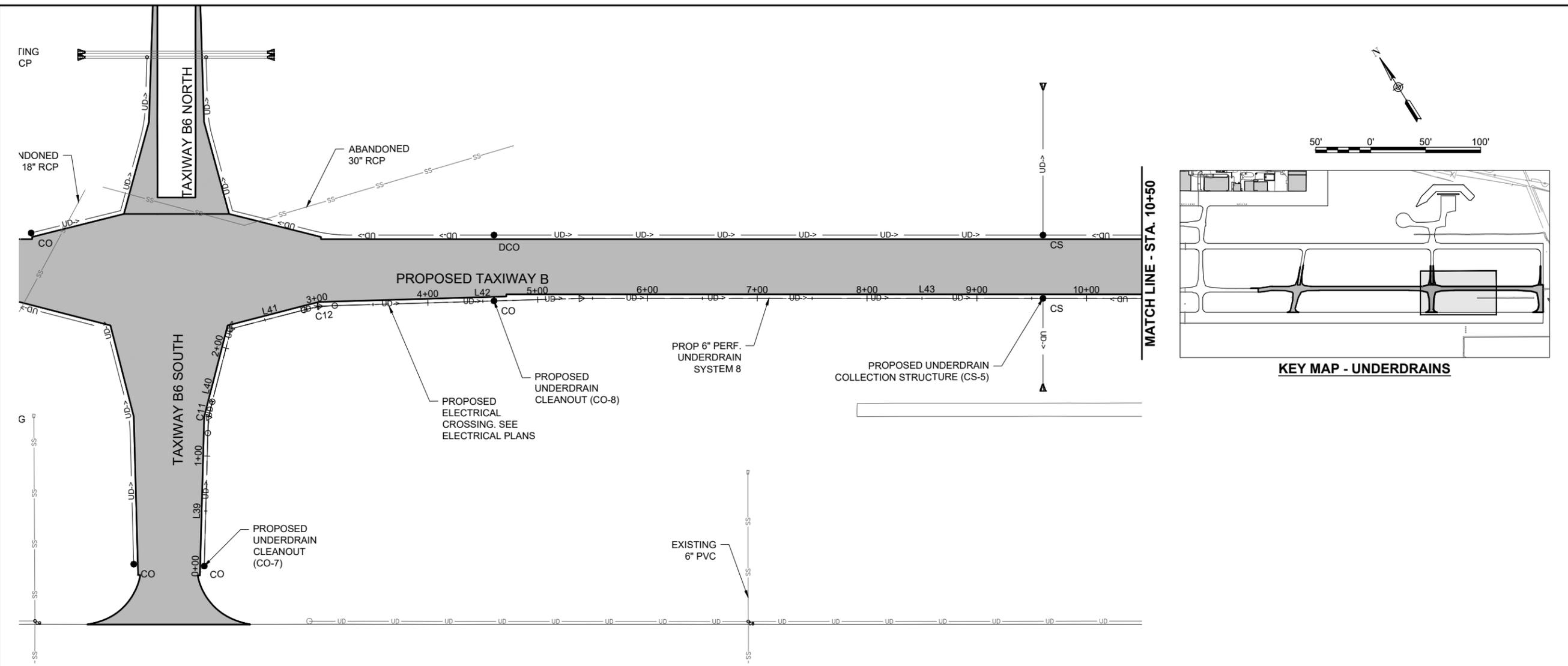
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 8 (SHEET 1)

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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025

PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG

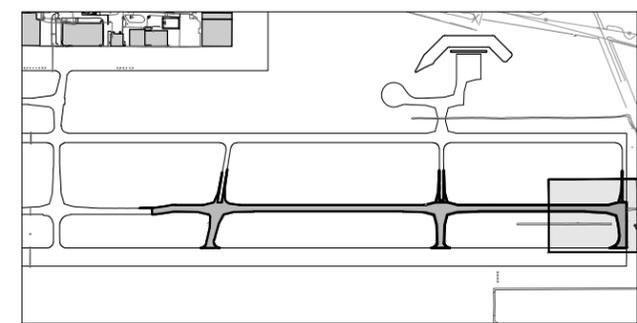
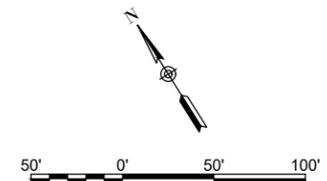
DESIGN BY: AJC 3/7/2025

DRAWN BY: AJC 3/9/2025

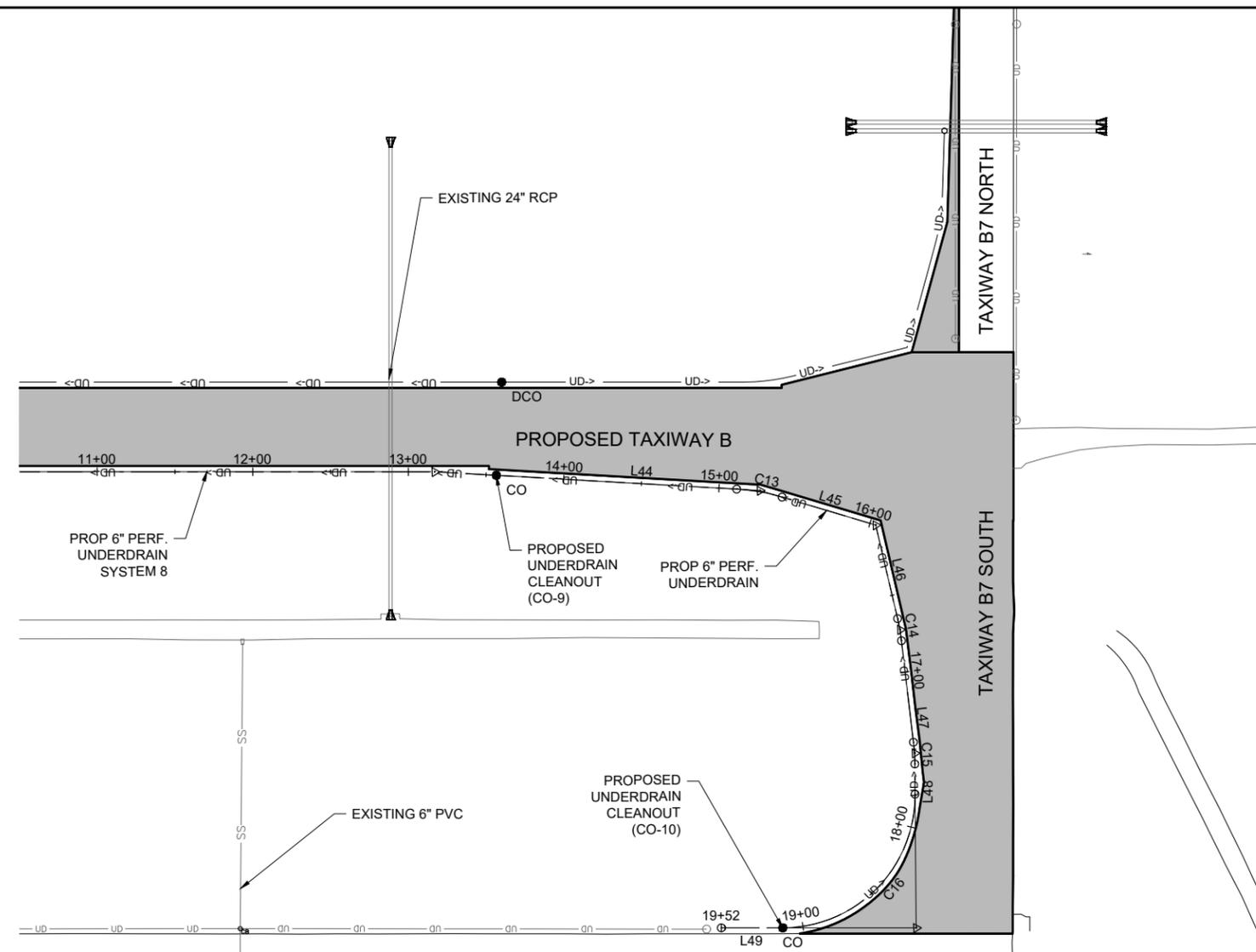
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

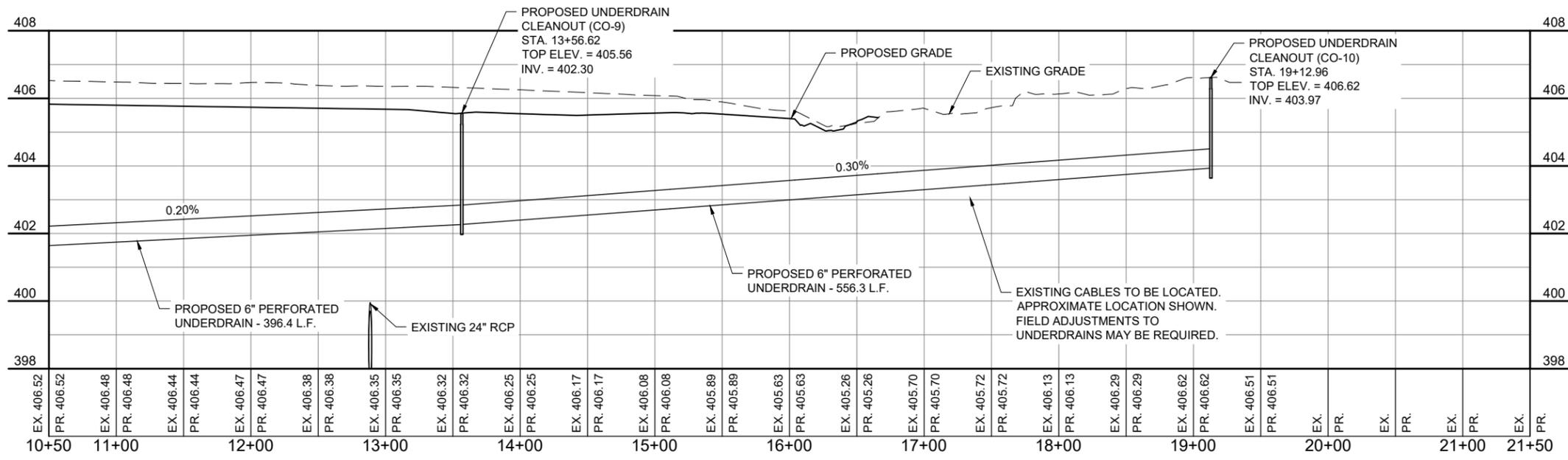
PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 8 (SHEET 2)



KEY MAP - UNDERDRAINS



PLAN VIEW - UNDERDRAIN SYSTEM 8



PROFILE VIEW - UNDERDRAIN SYSTEM 8

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**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

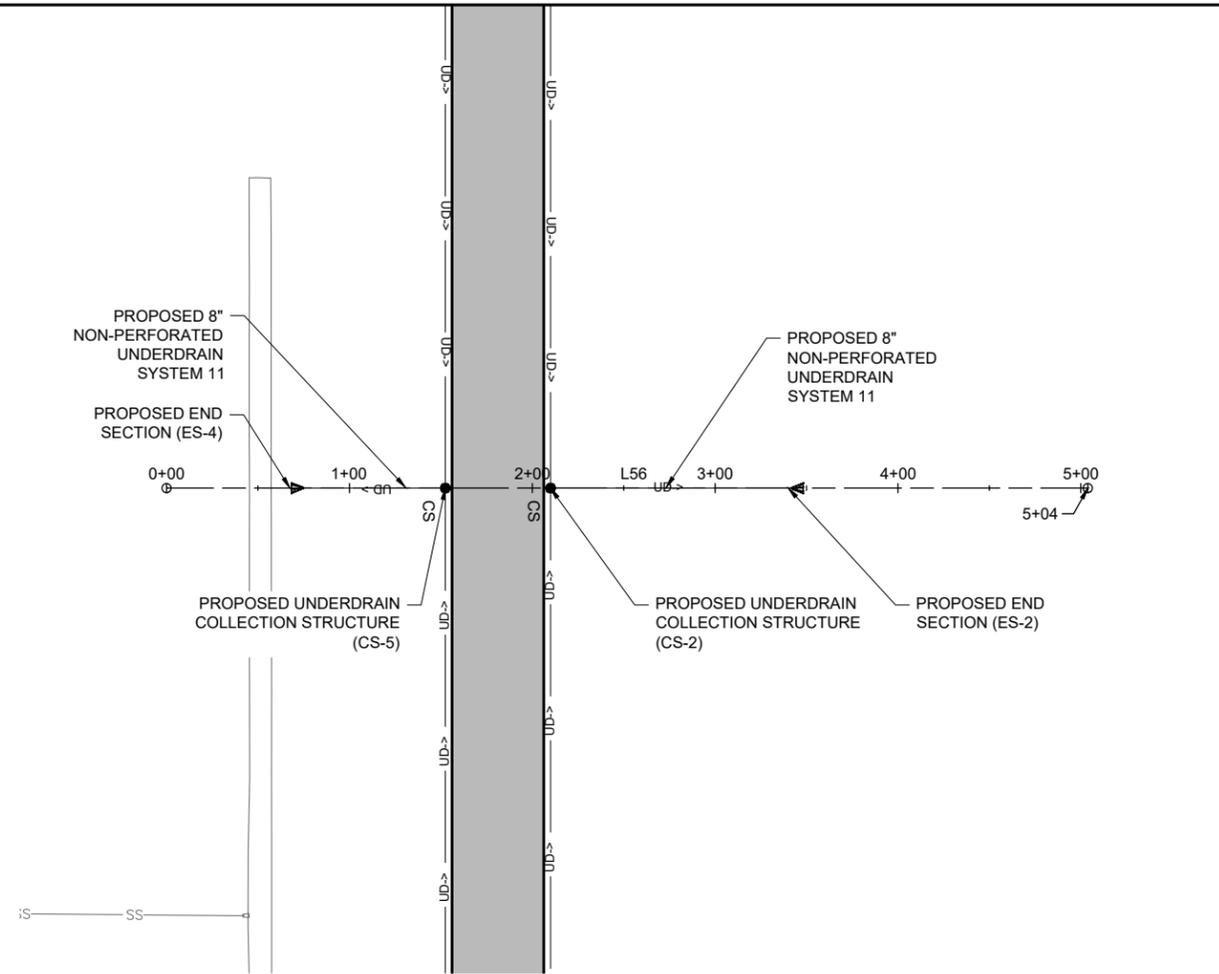
IDA NO.: CPS-5078  
CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
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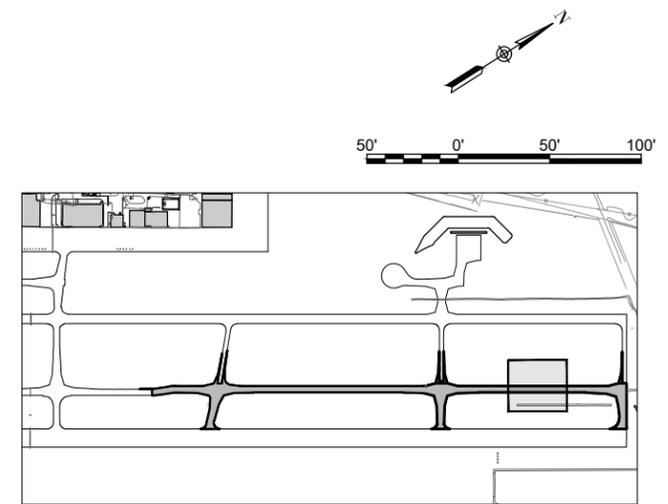
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
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DRAWN BY: AJC 3/9/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

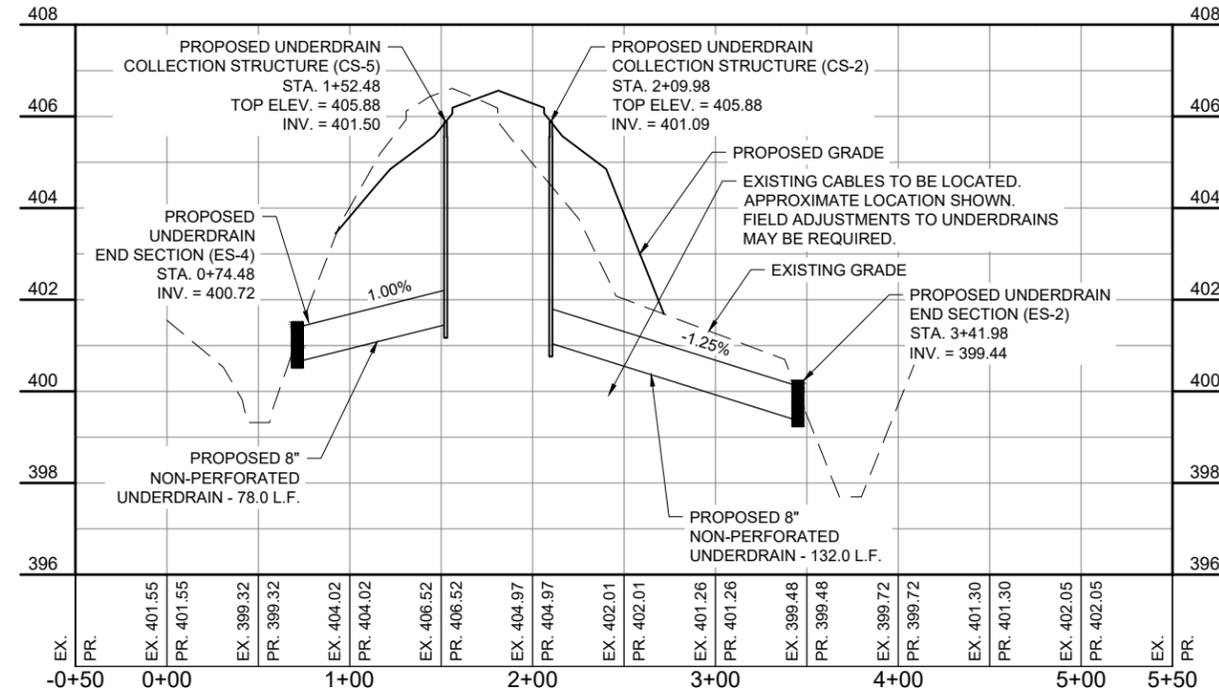
PROPOSED PLAN AND PROFILE - UNDERDRAIN SYSTEM 11



PLAN VIEW - UNDERDRAIN SYSTEM 11



KEY MAP - UNDERDRAINS



PROFILE VIEW - UNDERDRAIN SYSTEM 11

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BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-702-PNP.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/10/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED  
UNDERDRAIN  
ALIGNMENT DATA  
TABLES - SHEET 1

ALIGNMENT DATA UNDERDRAIN SYSTEM 1

Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L1	0+00.00	0+91.85	91.85	109° 28' 30"	692975.72,2300251.96	692945.09,2300338.56
L2	0+91.85	1+75.20	83.35	56° 02' 52"	692945.09,2300338.56	692991.64,2300407.70
L3	1+75.20	3+46.51	171.31	43° 28' 23"	692991.64,2300407.70	693115.96,2300525.56

ALIGNMENT DATA UNDERDRAIN SYSTEM 2

Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L4	0+00.00	1+53.11	153.11	220° 30' 29"	693107.93,2300581.26	692991.51,2300481.81
L5	1+53.11	2+32.49	79.37	211° 45' 11"	692991.51,2300481.81	692924.02,2300440.04
C1	2+32.49	2+97.57	65.08	IN=S31° 45' 11"W OUT=S49° 18' 38"E DEL=81°03'49"	692924.02,2300440.04	692864.93,2300449.16
L6	2+97.57	3+91.47	93.90	129° 10' 04"	692864.93,2300449.16	692805.63,2300521.96
L7	3+91.47	4+96.84	105.37	122° 12' 35"	692805.63,2300521.96	692749.46,2300611.11
L8	4+96.84	11+32.08	635.25	122° 12' 35"	692749.46,2300611.11	692410.87,2301148.60
L9	11+32.08	18+32.57	700.49	122° 12' 35"	692410.87,2301148.60	692037.49,2301741.29

ALIGNMENT DATA UNDERDRAIN SYSTEM 3

Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L10	0+00.00	0+83.99	83.99	108° 11' 22"	692002.99,2301799.12	691976.77,2301878.92
L11	0+83.99	1+52.69	68.70	47° 32' 21"	691976.77,2301878.92	692023.15,2301929.60
C2	1+52.69	1+82.78	30.08	IN=N47° 32' 21"E OUT=N34° 04' 23"E DEL=13°27'58"	692023.15,2301929.60	692045.87,2301949.22
L12	1+82.78	3+43.19	160.42	34° 04' 23"	692045.87,2301949.22	692178.75,2302039.09

ALIGNMENT DATA UNDERDRAIN SYSTEM 4

Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L13	0+00.00	1+60.13	160.13	210° 24' 50"	692154.05,2302078.28	692015.96,2301997.21
C3	1+60.13	1+90.28	30.15	IN=S30° 24' 50"W OUT=S16° 55' 11"W DEL=13°29'39"	692015.96,2301997.21	691988.41,2301985.14
L14	1+90.28	2+59.18	68.91	196° 55' 11"	691988.41,2301985.14	691922.49,2301965.08
L15	2+59.18	3+35.23	76.05	136° 13' 47"	691922.49,2301965.08	691867.57,2302017.69
C4	3+35.23	3+66.55	31.32	IN=S43° 46' 13"E OUT=S57° 47' 25"E DEL=14°01'12"	691867.57,2302017.69	691847.82,2302041.90
L16	3+66.55	15+55.54	1188.99	122° 12' 35"	691847.82,2302041.90	691214.07,2303047.91
C5	15+55.54	15+86.86	31.32	IN=S57° 47' 25"E OUT=S71° 48' 38"E DEL=14°01'12"	691214.07,2303047.91	691200.76,2303076.17
L17	15+86.86	16+62.91	76.05	108° 11' 22"	691200.76,2303076.17	691177.02,2303148.42
L18	16+62.91	17+31.81	68.90	47° 29' 58"	691177.02,2303148.42	691223.57,2303199.22
C6	17+31.81	17+61.97	30.16	IN=N47° 29' 58"E OUT=N34° 00' 01"E DEL=13°29'58"	691223.57,2303199.22	691246.37,2303218.86
L19	17+61.97	19+19.52	157.55	34° 00' 01"	691246.37,2303218.86	691376.98,2303306.96

ALIGNMENT DATA UNDERDRAIN SYSTEM 5

Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
C7	0+00.00	0+59.11	59.11	IN=S57° 44' 34"E OUT=S66° 07' 32"E DEL=8°22'57"	693125.83,2299857.32	693098.05,2299909.43
L20	0+59.11	1+85.06	125.95	113° 52' 28"	693098.05,2299909.43	693047.07,2300024.60
L21	1+85.06	2+14.75	29.69	120° 38' 03"	693047.07,2300024.60	693031.94,2300050.15
L22	2+14.75	3+77.73	162.98	123° 42' 10"	693031.94,2300050.15	692941.50,2300185.74
L23	3+77.73	4+60.27	82.54	134° 44' 37"	692941.50,2300185.74	692883.40,2300244.36
C8	4+60.27	4+89.41	29.14	IN=S45° 15' 23"E OUT=S34° 15' 08"W DEL=79°30'30"	692883.40,2300244.36	692856.66,2300246.94
L24	4+89.41	5+71.95	82.54	214° 15' 08"	692856.66,2300246.94	692788.44,2300200.48
L25	5+71.95	7+10.75	138.81	225° 24' 53"	692788.44,2300200.48	692691.01,2300101.63

ALIGNMENT DATA UNDERDRAIN SYSTEM 6

Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L26	0+00.00	1+48.91	148.91	48° 15' 29"	692671.98,2300174.33	692771.12,2300285.44
L27	1+48.91	2+30.49	81.58	59° 51' 35"	692771.12,2300285.44	692812.08,2300355.99
L28	2+30.49	3+12.07	81.58	109° 08' 10"	692812.08,2300355.99	692785.34,2300433.07
L29	3+12.07	4+81.90	169.83	120° 38' 20"	692785.34,2300433.07	692698.79,2300579.19
L30	4+81.90	5+50.91	69.01	120° 38' 20"	692698.79,2300579.19	692663.62,2300638.57
L31	5+50.91	11+17.18	566.26	122° 12' 35"	692663.62,2300638.57	692361.79,2301117.68
L32	11+17.18	16+35.47	518.29	122° 12' 35"	692361.79,2301117.68	692085.53,2301556.21
L33	16+35.47	16+99.42	63.96	123° 53' 51"	692085.53,2301556.21	692049.87,2301609.29
L34	16+99.42	18+16.13	116.71	123° 53' 51"	692049.87,2301609.29	691984.77,2301706.17

ALIGNMENT DATA UNDERDRAIN SYSTEM 7

Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L35	0+00.00	0+45.23	45.23	123° 40' 11"	691985.82,2301704.62	691960.74,2301742.26
C9	0+45.23	0+74.21	28.98	IN=S56° 19' 49"E OUT=S43° 21' 33"E DEL=12°58'16"	691960.74,2301742.26	691942.09,2301764.36
L36	0+74.21	1+42.25	68.04	136° 38' 27"	691942.09,2301764.36	691892.62,2301811.08
L37	1+42.25	2+10.86	68.61	197° 38' 38"	691892.62,2301811.08	691827.24,2301790.28
C10	2+10.86	2+39.96	29.10	IN=S17° 38' 38"W OUT=S30° 40' 10"W DEL=13°01'32"	691827.24,2301790.28	691800.75,2301778.40
L38	2+39.96	3+58.68	118.72	210° 40' 10"	691800.75,2301778.40	691698.63,2301717.84

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ALIGNMENT DATA UNDERDRAIN SYSTEM 8						
Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L39	0+00.00	1+20.89	120.89	33° 46' 24"	691662.92,2301771.24	691763.41,2301838.45
C11	1+20.89	1+49.64	28.75	IN=N33° 46' 24"E OUT=N46° 38' 27"E DEL=12°52'03"	691763.41,2301838.45	691785.32,2301856.97
L40	1+49.64	2+18.07	68.44	46° 38' 27"	691785.32,2301856.97	691832.30,2301906.72
L41	2+18.07	2+86.65	68.58	107° 46' 43"	691832.30,2301906.72	691811.36,2301972.03
C12	2+86.65	3+15.12	28.47	IN=S72° 13' 17"E OUT=S59° 28' 42"E DEL=12°44'36"	691811.36,2301972.03	691799.74,2301997.95
L42	3+15.12	5+39.36	224.24	120° 31' 18"	691799.74,2301997.95	691685.86,2302191.12
L43	5+39.36	13+16.88	777.53	122° 12' 35"	691685.86,2302191.12	691271.42,2302848.98
L44	13+16.88	15+11.33	194.44	125° 29' 27"	691271.42,2302848.98	691158.53,2303007.30
C13	15+11.33	15+41.02	29.69	IN=S54° 30' 33"E OUT=S41° 13' 06"E DEL=13°17'27"	691158.53,2303007.30	691138.66,2303029.27
L45	15+41.02	16+03.80	62.78	138° 46' 54"	691138.66,2303029.27	691091.43,2303070.64
L46	16+03.80	16+65.43	61.62	199° 00' 56"	691091.43,2303070.64	691033.17,2303050.56
C14	16+65.43	16+79.82	14.40	IN=S19° 00' 56"W OUT=S25° 27' 36"W DEL=6°26'40"	691033.17,2303050.56	691019.85,2303045.11
L47	16+79.82	17+45.37	65.55	205° 27' 36"	691019.85,2303045.11	690960.67,2303016.94
C15	17+45.37	17+59.38	14.01	IN=S25° 27' 36"W OUT=S31° 43' 54"W DEL=6°16'18"	690960.67,2303016.94	690948.37,2303010.23
L48	17+59.38	17+78.73	19.35	211° 43' 54"	690948.37,2303010.23	690931.92,2303000.06
C16	17+78.73	19+12.96	134.23	IN=S31° 43' 54"W OUT=N57° 47' 25"W DEL=90°28'40"	690931.92,2303000.06	690904.70,2302882.46
L49	19+12.96	19+52.48	39.52	302° 12' 35"	690904.70,2302882.46	690925.77,2302849.02

ALIGNMENT DATA UNDERDRAIN SYSTEM 9						
Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L50	0+00.00	1+00.00	100.00	32° 12' 35"	693041.22,2299804.02	693125.83,2299857.32

ALIGNMENT DATA UNDERDRAIN SYSTEM 10						
Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L51	0+00.00	0+41.00	41.00	32° 12' 35"	692261.11,2301054.25	692295.80,2301076.11
L52	0+41.00	1+19.00	78.00	32° 12' 35"	692295.80,2301076.11	692361.79,2301117.68
L53	1+19.00	1+77.00	58.00	32° 12' 35"	692361.79,2301117.68	692410.87,2301148.60
L54	1+77.00	3+09.00	132.00	32° 12' 35"	692410.87,2301148.60	692522.55,2301218.96
L55	3+09.00	3+50.00	41.00	32° 12' 35"	692522.55,2301218.96	692557.24,2301240.81

ALIGNMENT DATA UNDERDRAIN SYSTEM 11						
Number	START STATION	END STATION	Length	AZIMUTH	START (N,E)	END (N,E)
L56	0+00.00	5+03.80	503.80	32° 12' 35"	691332.51,2302465.94	691758.78,2302734.47



Offices Nationwide  
 www.hanson-inc.com

Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2886  
 Telephone: 217.788.2450  
 Fax: 217.788.2503



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
 TAXIWAY B, PHASE 3:  
 CONSTRUCT FROM  
 TAXIWAY B5 TO B7  
 AND B1 INTERSECTION

IDA NO.: CPS-5078  
 CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
 PROJECT NO: 23A0001D  
 CAD FILE: C-702-PNP.DWG  
 DESIGN BY: AJC 3/7/2025  
 DRAWN BY: AJC 3/10/2025  
 REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED  
 UNDERDRAIN  
 ALIGNMENT DATA  
 TABLES - SHEET 2

FOR BID



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

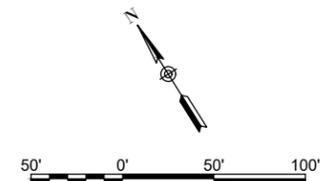
IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-161-INT.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/10/2025  
REVIEWED BY: BSS 3/12/2025

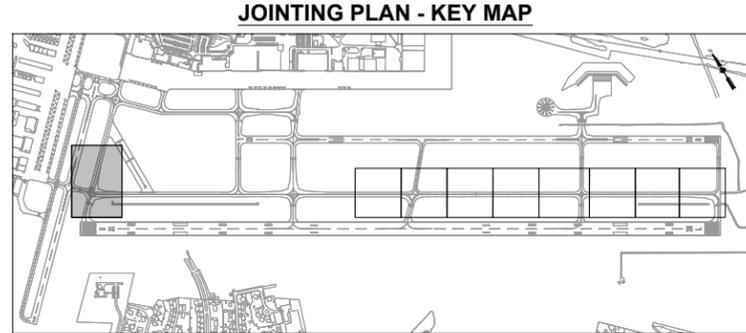
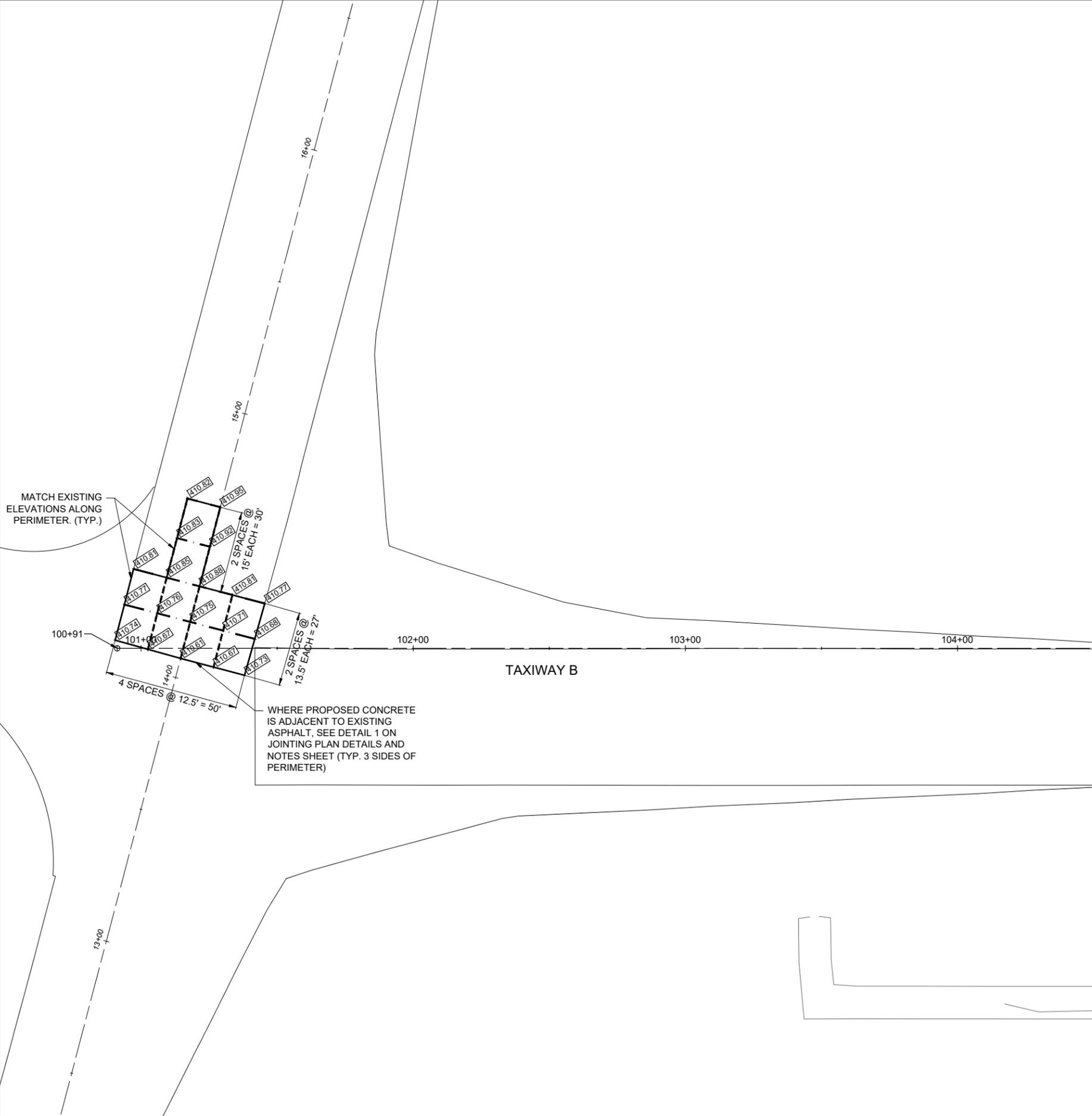
SHEET TITLE

PROPOSED JOINTING PLAN STA. 100+00 TO STA. 104+50



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - TYPE A1-MODIFIED, ISOLATION JOINT
  - TYPE B, CONTRACTION JOINT
  - TYPE C, CONTRACTION JOINT
  - TYPE D, CONTRACTION JOINT
  - TYPE E, CONSTRUCTION JOINT
  - PROPOSED REINFORCED PANEL
  - PROPOSED PAVEMENT

- NOTES:**
- CONTRACTOR MAY PROPOSE ALTERNATIVE PAVING/JOINTING PLAN FOR REVIEW AND APPROVAL FOLLOWING AWARD.
  - CONTRACTOR SHALL VERIFY EXISTING ELEVATIONS AT "MATCH" LOCATIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER/TECHNICIAN IMMEDIATELY TO DETERMINE IF ADJUSTMENTS ARE NECESSARY TO PROPOSED GRADES.



**FOR BID**

MAR 14, 2025 9:50 AM GROVED2594  
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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

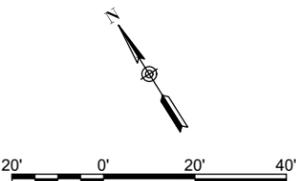
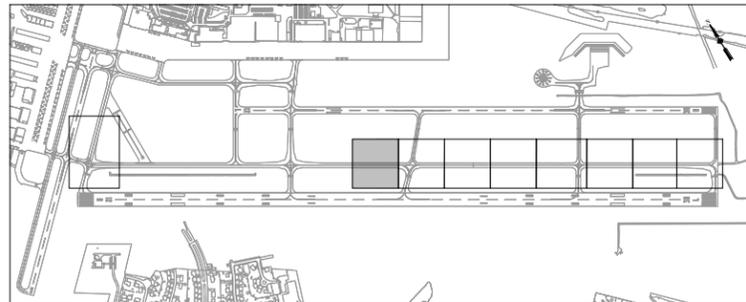
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-161-INT.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/10/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED JOINTING PLAN STA. 130+00 TO STA. 135+00

FOR BID

**JOINTING PLAN - KEY MAP**

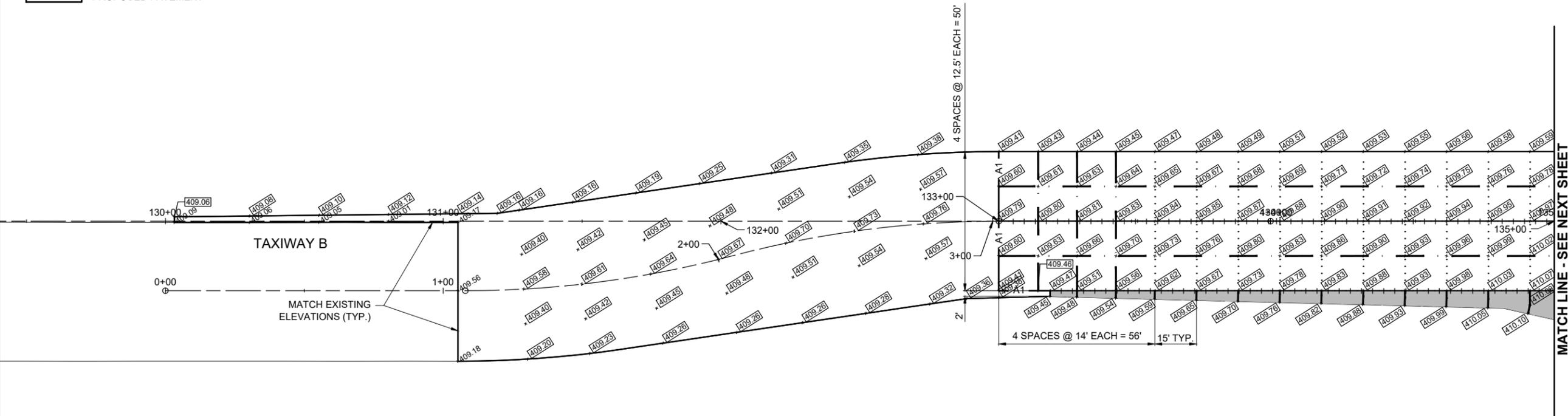


**LEGEND**

- EXISTING AIRPORT PROPERTY LINE
- EXISTING FENCE
- EXISTING PAVEMENT
- TYPE A1-MODIFIED, ISOLATION JOINT
- TYPE B, CONTRACTION JOINT
- TYPE C, CONTRACTION JOINT
- TYPE D, CONTRACTION JOINT
- TYPE E, CONSTRUCTION JOINT
- PROPOSED REINFORCED PANEL
- PROPOSED PAVEMENT

**NOTES:**

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MAR 14, 2025 9:51 AM GROVED2594 I:\23\JOBS\23A0001\DCAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BS-22X34\_BORDER







**ST. LOUIS DOWNTOWN AIRPORT**  
BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

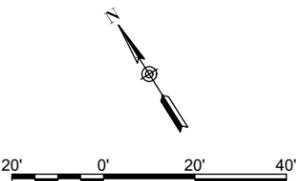
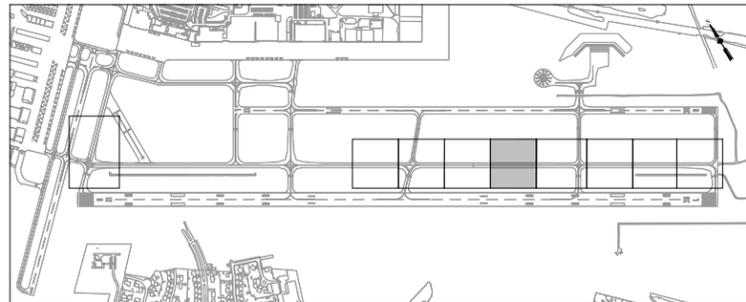
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-161-INT.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/10/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED JOINTING  
PLAN STA. 145+00 TO  
STA. 150+00

**FOR BID**

**JOINTING PLAN - KEY MAP**

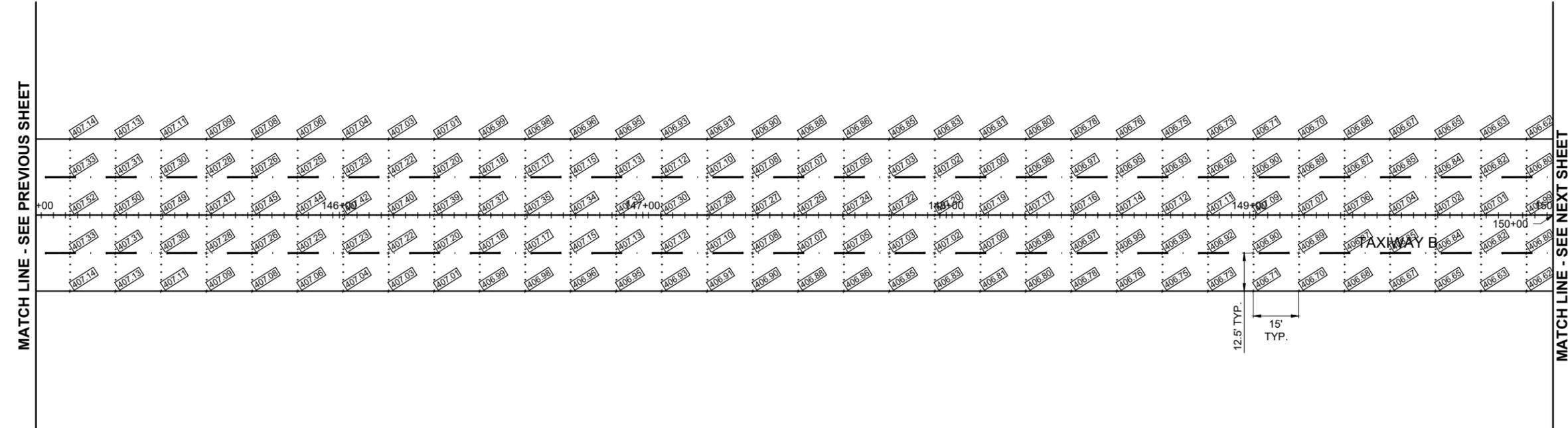


**LEGEND**

- EXISTING AIRPORT PROPERTY LINE
- EXISTING FENCE
- EXISTING PAVEMENT
- TYPE A1-MODIFIED, ISOLATION JOINT
- TYPE B, CONTRACTION JOINT
- TYPE C, CONTRACTION JOINT
- TYPE D, CONTRACTION JOINT
- TYPE E, CONSTRUCTION JOINT
- PROPOSED REINFORCED PANEL
- PROPOSED PAVEMENT

**NOTES:**

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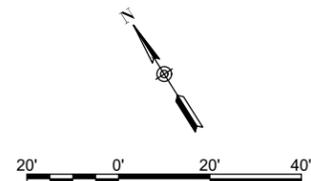


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DES	DWN	REV

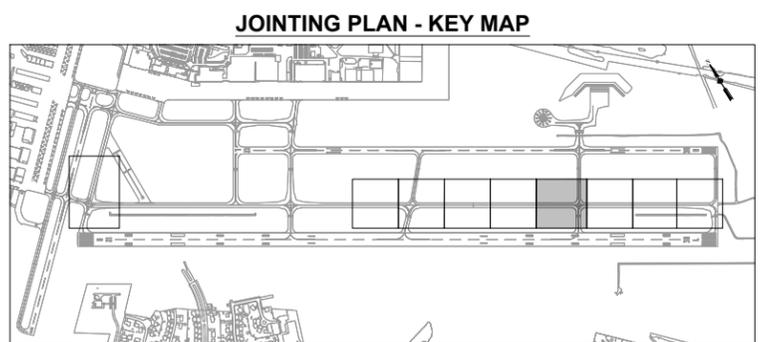
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PROJECT NO: 23A0001D  
CAD FILE: C-161-JNT.DWG  
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DRAWN BY: AJC 3/10/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

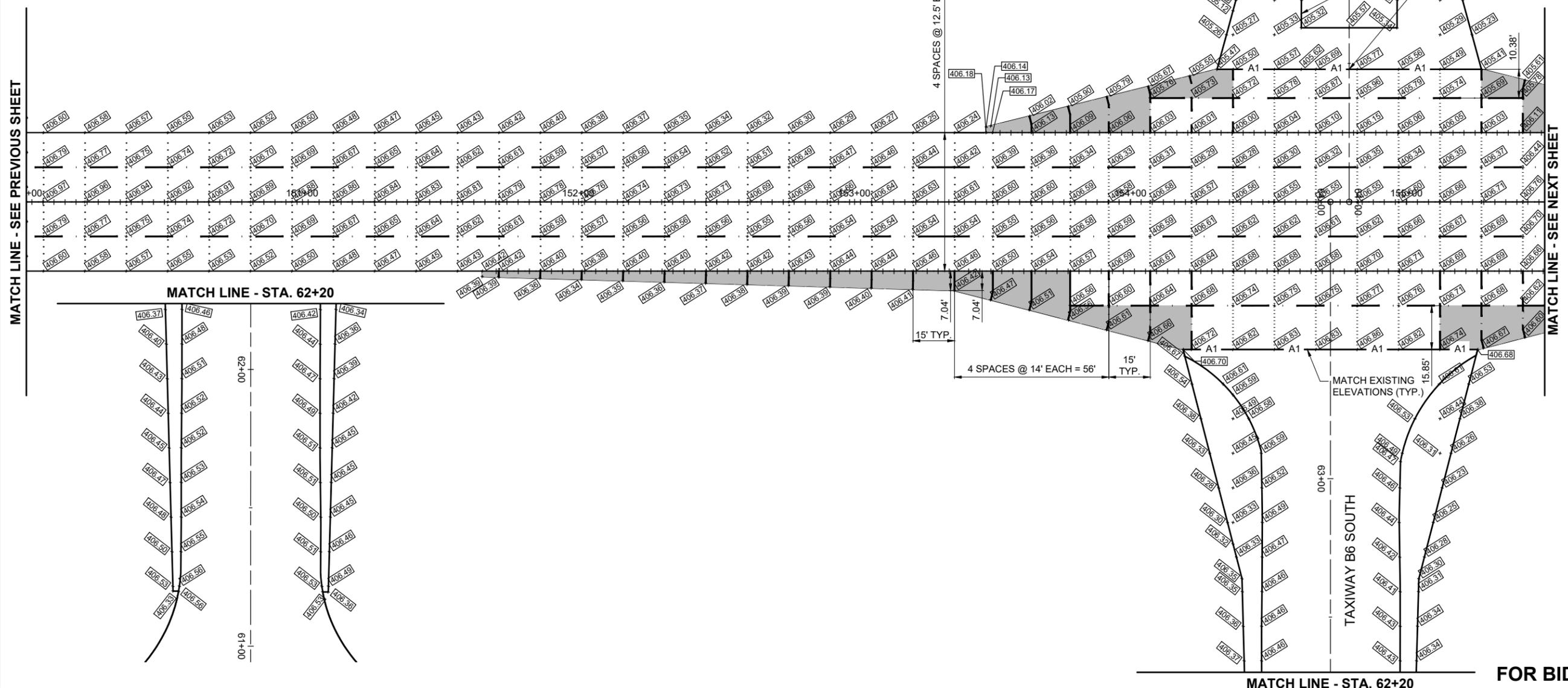
PROPOSED JOINTING  
PLAN STA. 150+00 TO  
STA. 155+50



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - TYPE A1-MODIFIED, ISOLATION JOINT
  - TYPE B, CONTRACTION JOINT
  - TYPE C, CONTRACTION JOINT
  - TYPE D, CONTRACTION JOINT
  - TYPE E, CONSTRUCTION JOINT
  - PROPOSED REINFORCED PANEL
  - PROPOSED PAVEMENT



- NOTES:**
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**FOR BID**



**ST. LOUIS DOWNTOWN AIRPORT**  
BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE: 3/14/2025 LICENSE: 11/30/2025  
SIGNED: EXPIRES:

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

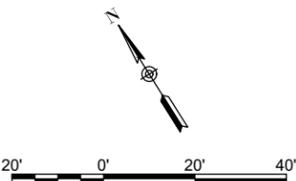
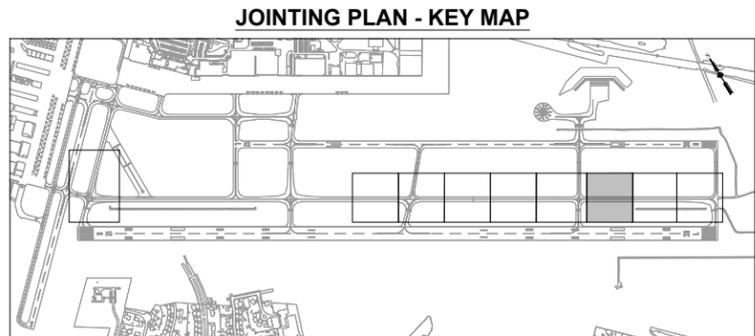

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-161-JNT.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/10/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED JOINTING  
PLAN STA. 155+50 TO  
STA. 160+50

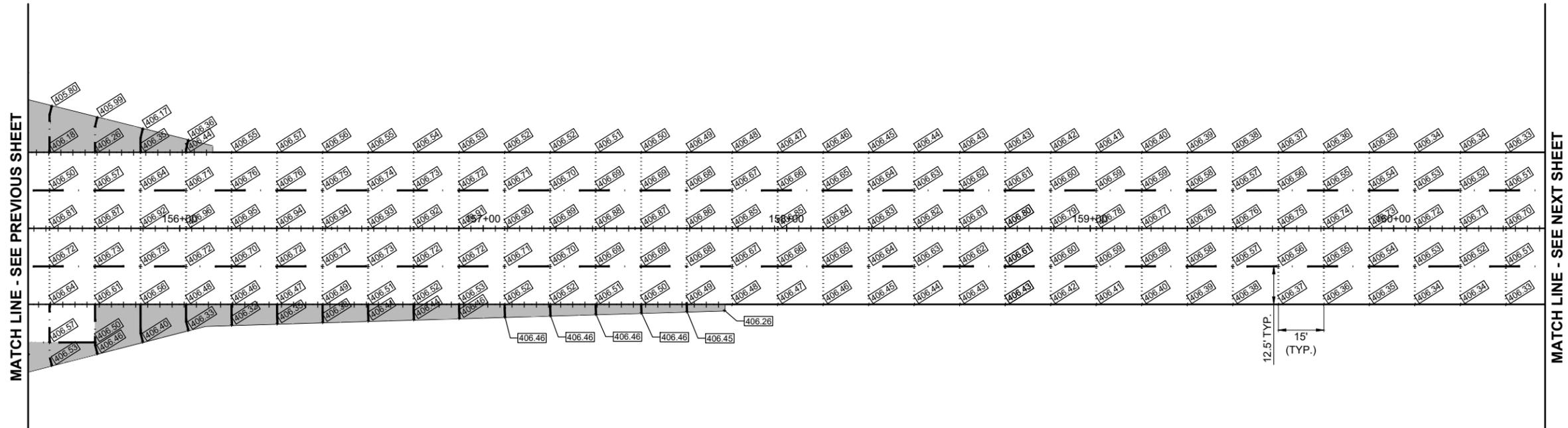
FOR BID



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - TYPE A1-MODIFIED, ISOLATION JOINT
  - TYPE B, CONTRACTION JOINT
  - TYPE C, CONTRACTION JOINT
  - TYPE D, CONTRACTION JOINT
  - TYPE E, CONSTRUCTION JOINT
  - PROPOSED REINFORCED PANEL
  - PROPOSED PAVEMENT

**NOTES:**

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MAR 14, 2025 9:53 AM GROVED2594 I:\23\JOBS\23A0001\DCAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BS-22X34\_BORDER



**BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

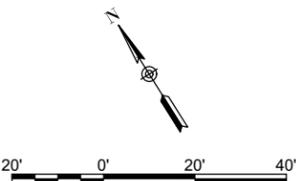
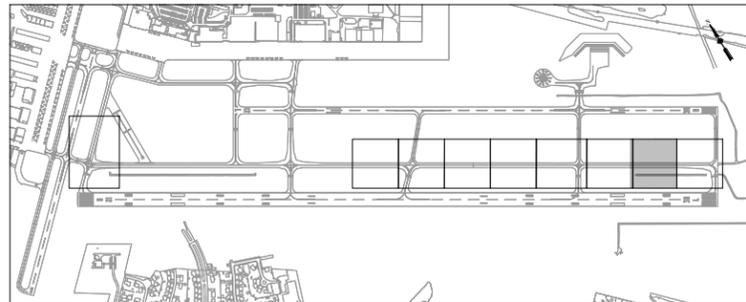
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-161-INT.DWG  
DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/10/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED JOINTING  
PLAN STA. 160+50 TO  
STA. 165+30

**FOR BID**

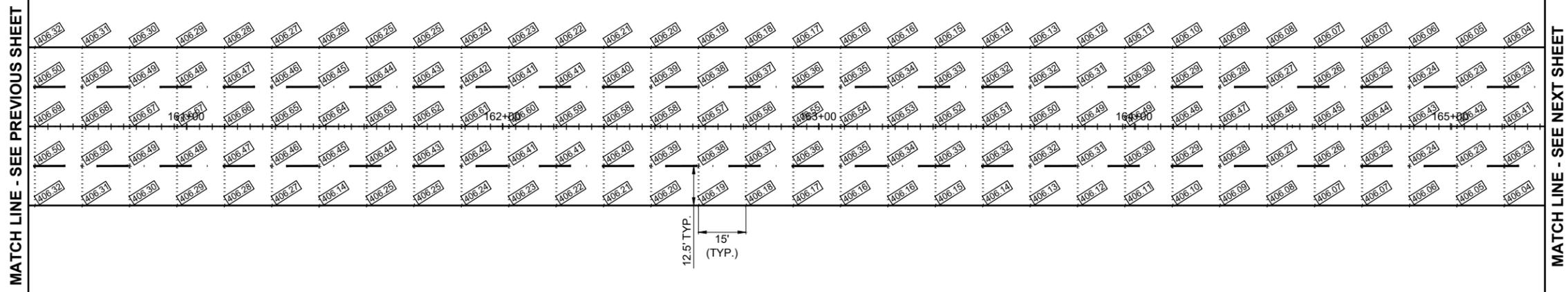
**JOINTING PLAN - KEY MAP**



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - TYPE A1-MODIFIED, ISOLATION JOINT
  - TYPE B, CONTRACTION JOINT
  - TYPE C, CONTRACTION JOINT
  - TYPE D, CONTRACTION JOINT
  - TYPE E, CONSTRUCTION JOINT
  - PROPOSED REINFORCED PANEL
  - PROPOSED PAVEMENT

**NOTES:**

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**ST. LOUIS DOWNTOWN AIRPORT**  
BI-STATE DEVELOPMENT  
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6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

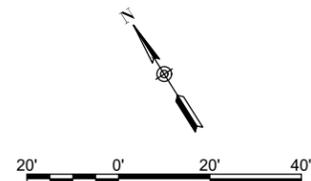
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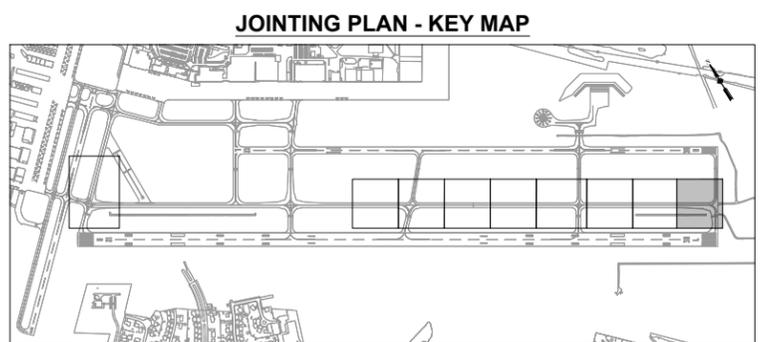
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PROJECT NO: 23A0001D  
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DESIGN BY: AJC 3/7/2025  
DRAWN BY: AJC 3/10/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

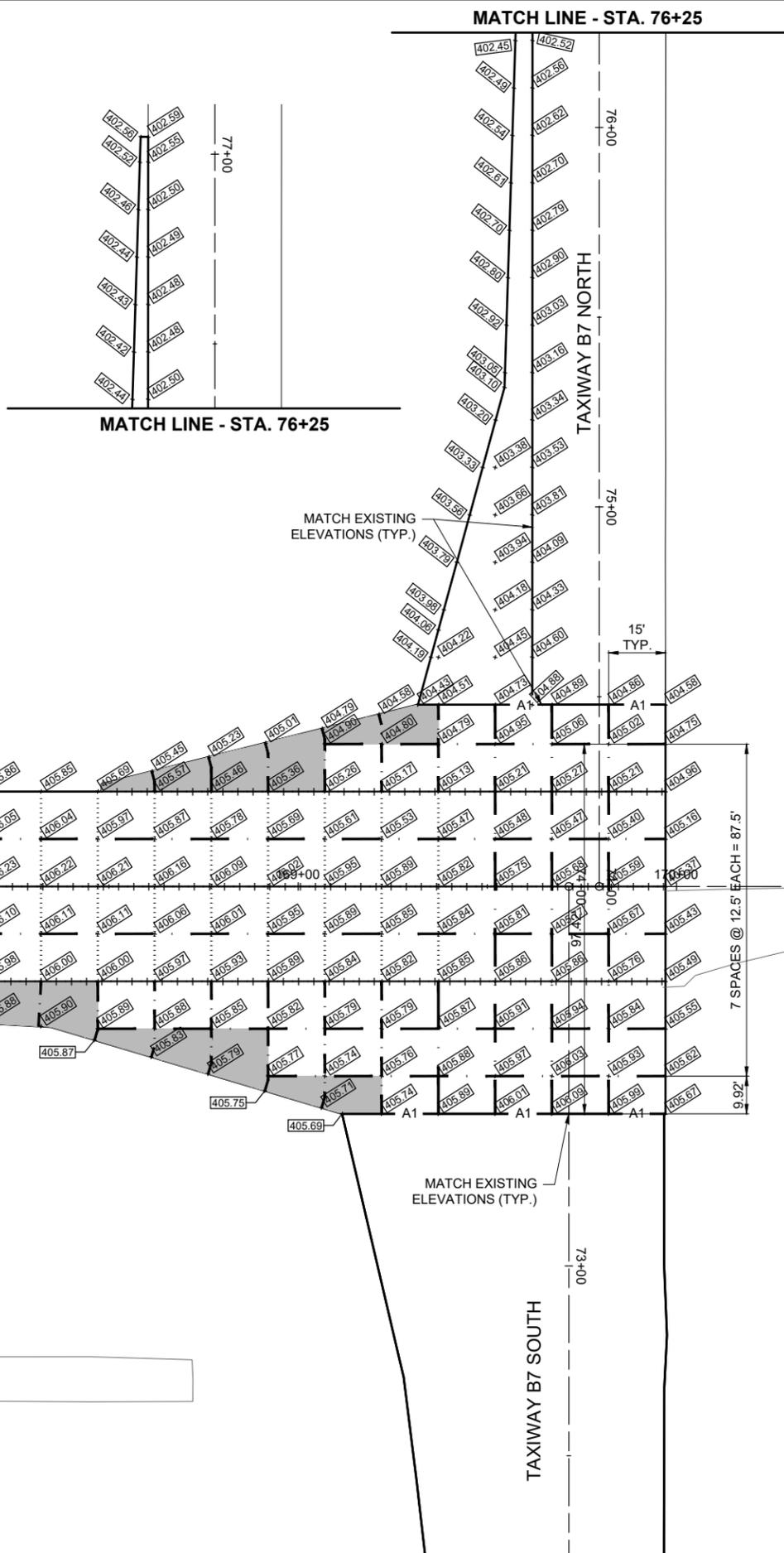
PROPOSED JOINTING  
PLAN STA. 165+30 TO  
STA. 170+50



- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - x- EXISTING FENCE
  - EXISTING PAVEMENT
  - A1- TYPE A1-MODIFIED, ISOLATION JOINT
  - - - TYPE B, CONTRACTION JOINT
  - . - . TYPE C, CONTRACTION JOINT
  - . . . . TYPE D, CONTRACTION JOINT
  - + + + TYPE E, CONSTRUCTION JOINT
  - PROPOSED REINFORCED PANEL
  - PROPOSED PAVEMENT



- NOTES:**
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MATCH LINE - SEE PREVIOUS SHEET

**FOR BID**



**ST. LOUIS DOWNTOWN AIRPORT**  
BI-STATE DEVELOPMENT  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

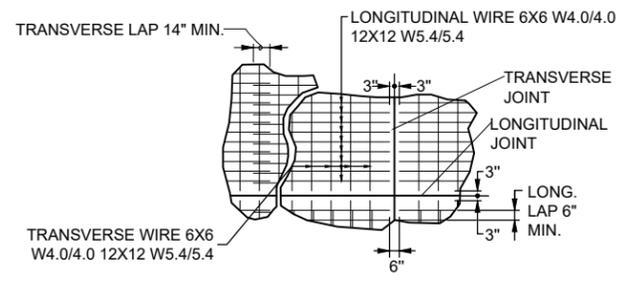
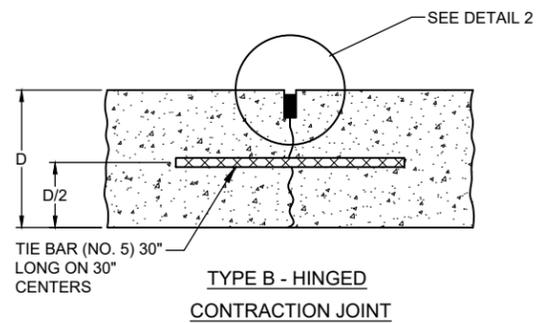
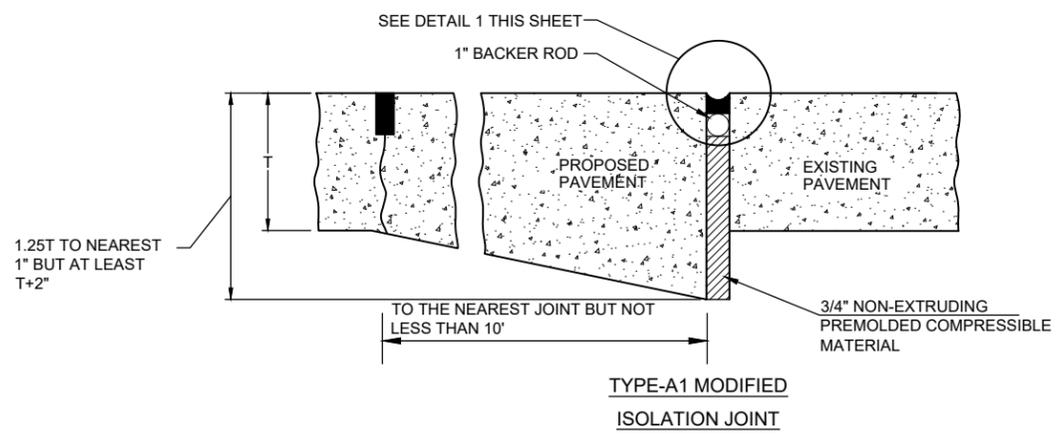

NO.	DATE	DESCRIPTION		
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ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-561-JNT.DWG  
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DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

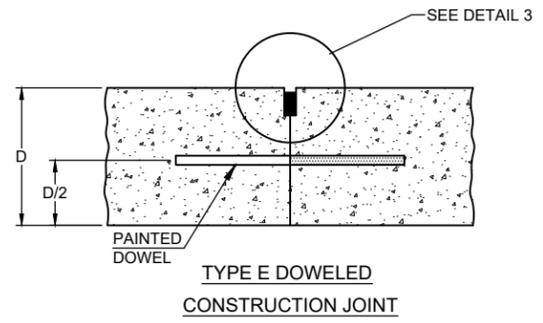
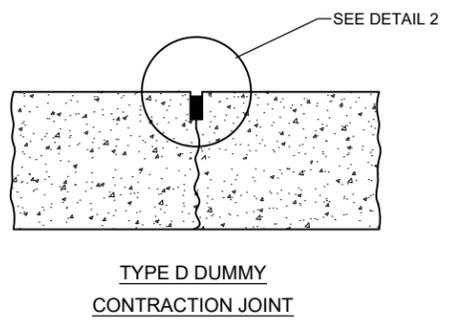
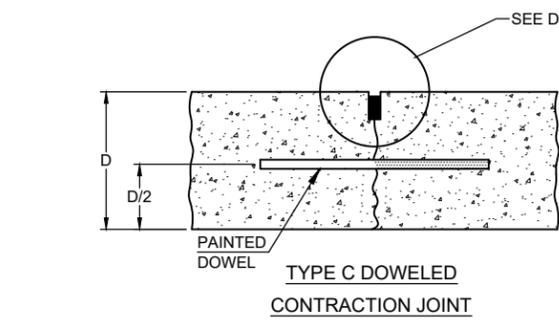
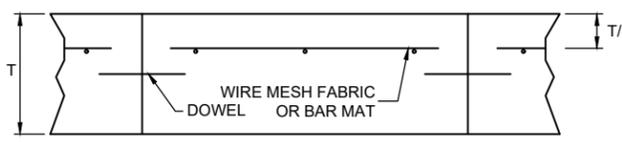
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PROPOSED JOINTING PLAN DETAILS AND NOTES

**FOR BID**

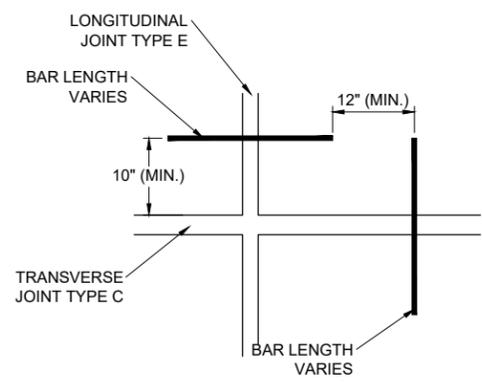


NOTE:  
DIFFERENT WIRE MESH SIZES/CONFIGURATIONS ARE PERMITTED TO PROVIDE A MINIMUM OF 0.05% OF THE PANEL CROSS-SECTIONAL AREA IN BOTH DIRECTIONS.

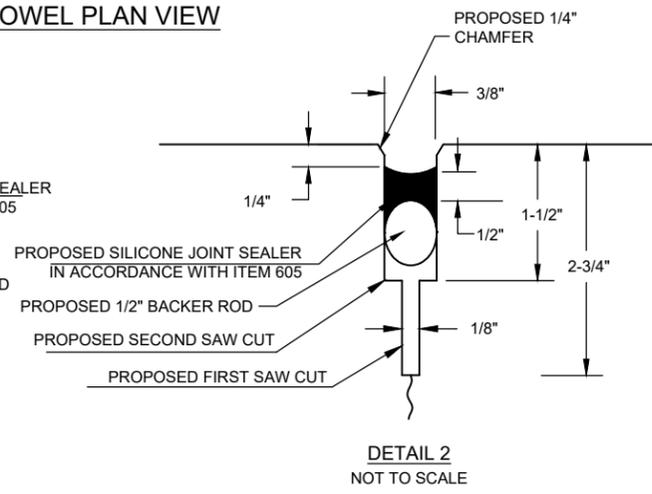
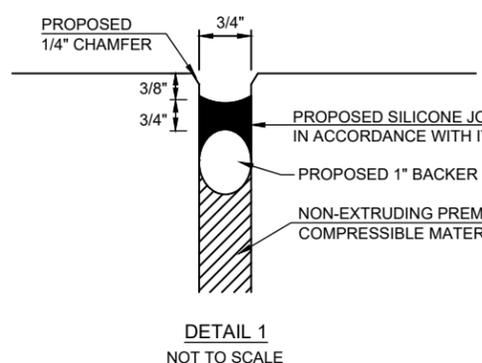


**JOINTING NOTES:**

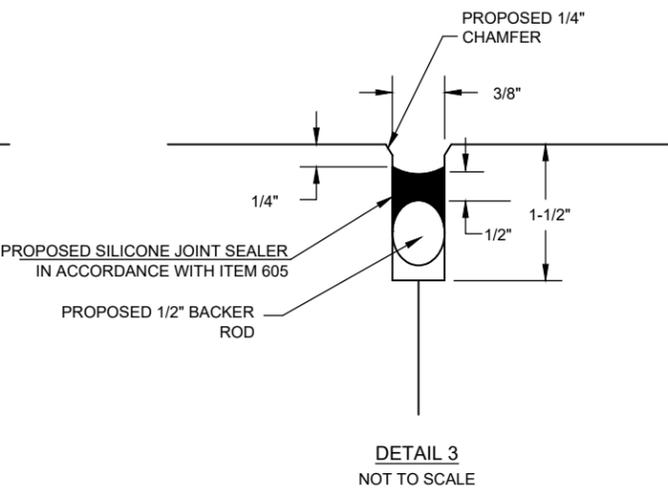
- ALL JOINT EDGES SHALL BE SAWCUT TO PRODUCE THE 1/4" CHAMFER.
- ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO INSTALLATION.
- DOWEL BARS FOR 12 IN. & 8 IN. THICK PAVEMENT SHALL BE 1 IN. DIAMETER, 18 IN. LONG AT 12 IN. SPACING.
- ALL TIE BARS SHALL BE HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.
- ALL TIE BARS SHALL BE PLACED AT A POINT NOT LESS THAN 6" OR MORE THAN 15" FROM A TRANSVERSE OR EXPANSION JOINT.
- DOWELS IN TRANSVERSE CONTRACTION AND LONGITUDINAL CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.
- ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE ±1/8" FOR CONSTRUCTION JOINTS AND ±1/4" FOR CONTRACTION JOINTS.
- THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED DOWELS IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE DIVISION OF AERONAUTICS PRIOR TO USE.
- THE COST OF ALL DOWEL BARS, TIE-BARS, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE PCC PAVEMENT.
- WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL PROTECT THE TRANSVERSE JOINTS.
- JOINT SEALANT SHALL BE AS SPECIFIED IN THE STANDARD SPECIFICATIONS, ITEM 605.
- CURING COMPOUND SHALL BE AS SPECIFIED IN THE STANDARD SPECIFICATIONS, ITEM 501-2.9, AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE RESIDENT ENGINEER/TECHNICIAN.
- ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.



**DOWEL PLAN VIEW**



**JOINT SEALING DETAILS**



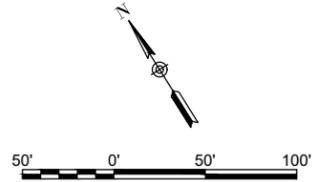
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		DES	DWN	REV

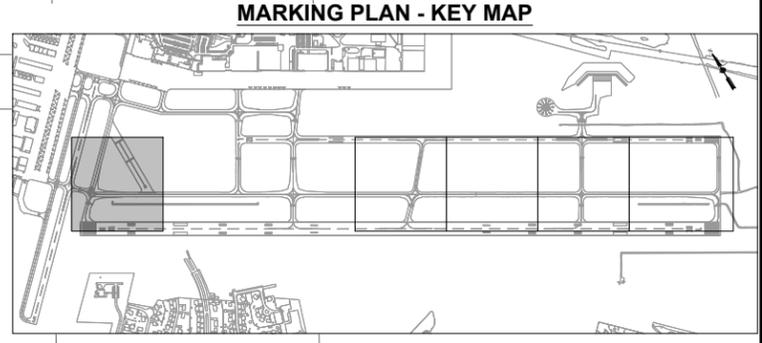
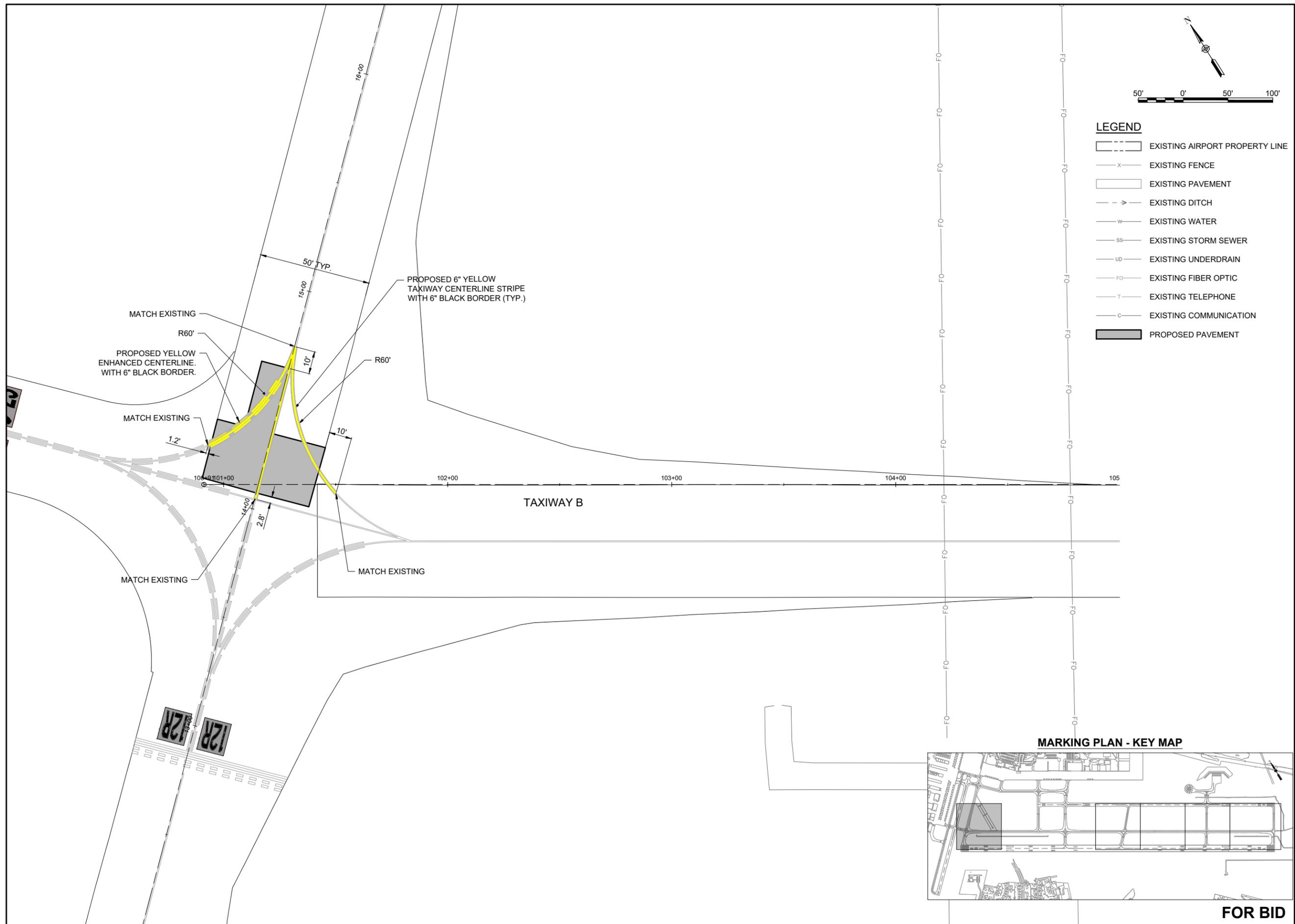
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-151-MRK.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED MARKING PLAN STA. 100+00 TO STA. 105+00

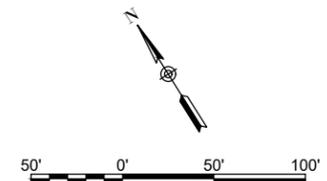
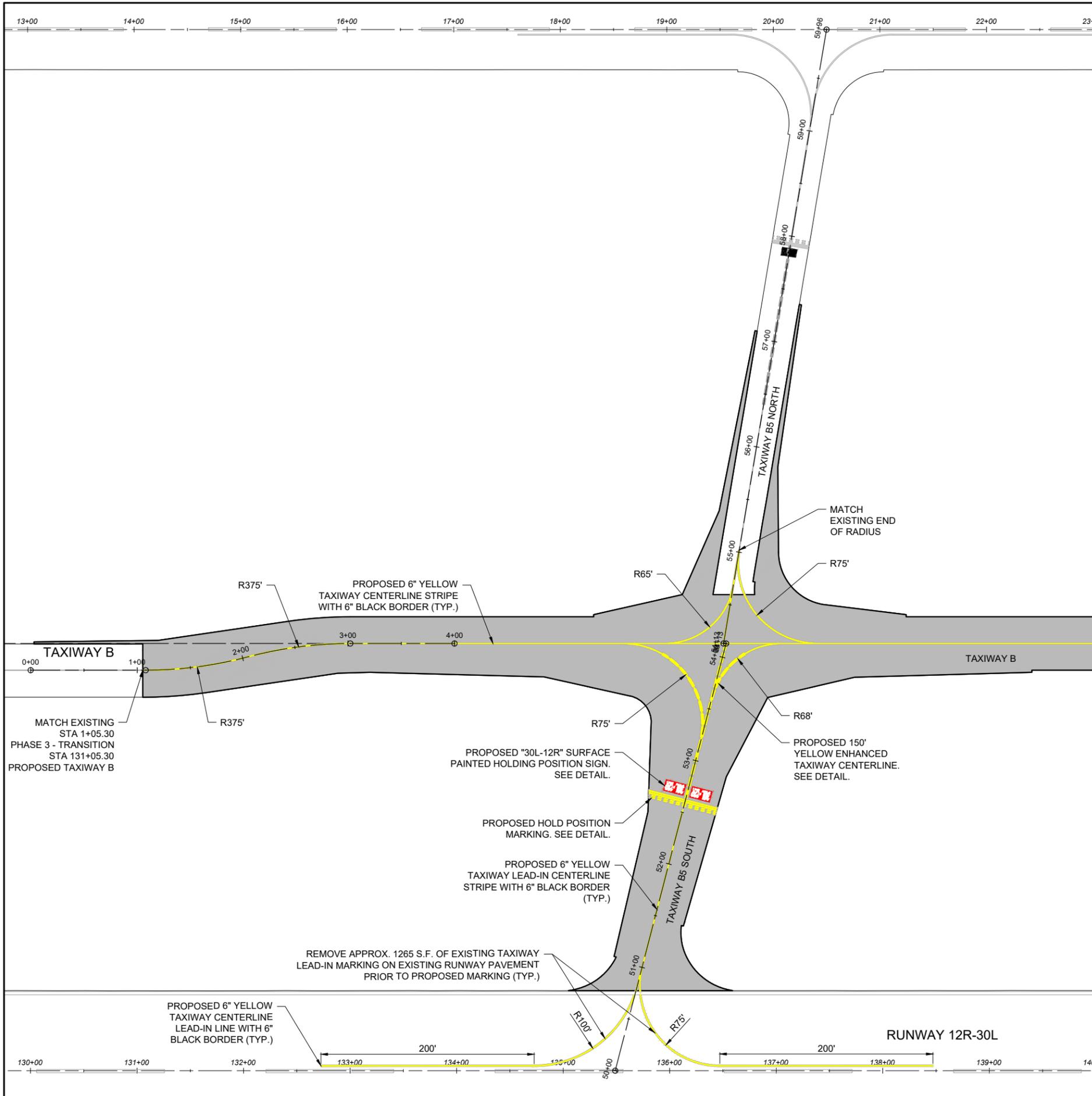


- LEGEND**
-  EXISTING AIRPORT PROPERTY LINE
  -  EXISTING FENCE
  -  EXISTING PAVEMENT
  -  EXISTING DITCH
  -  EXISTING WATER
  -  EXISTING STORM SEWER
  -  EXISTING UNDERDRAIN
  -  EXISTING FIBER OPTIC
  -  EXISTING TELEPHONE
  -  EXISTING COMMUNICATION
  -  PROPOSED PAVEMENT



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- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
  - EXISTING FIBER OPTIC
  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED PAVEMENT

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Cahokia Heights, Illinois 62206

STATE OF ILLINOIS  
BARRY S. STOLZ  
062-057281  
LICENSED PROFESSIONAL ENGINEER  
*BSS*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

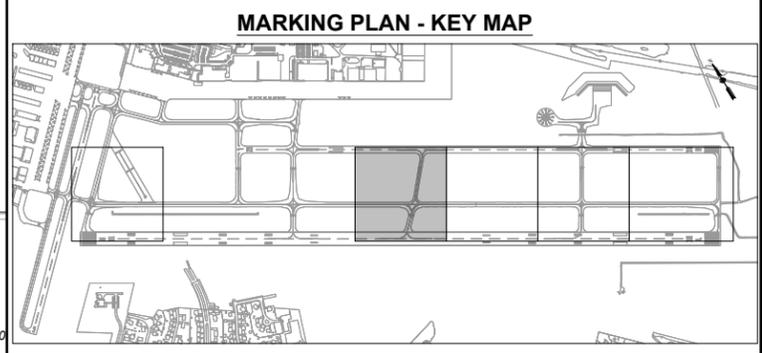
ISSUE: MARCH 14, 2025

PROJECT NO: 23A0001D  
CAD FILE: C-151-MRK.DWG  
DESIGN BY: JRH 3/5/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED MARKING PLAN STA. 130+00 TO STA. 140+00

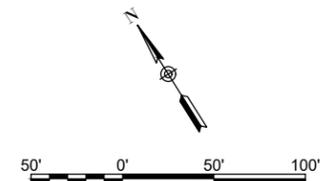
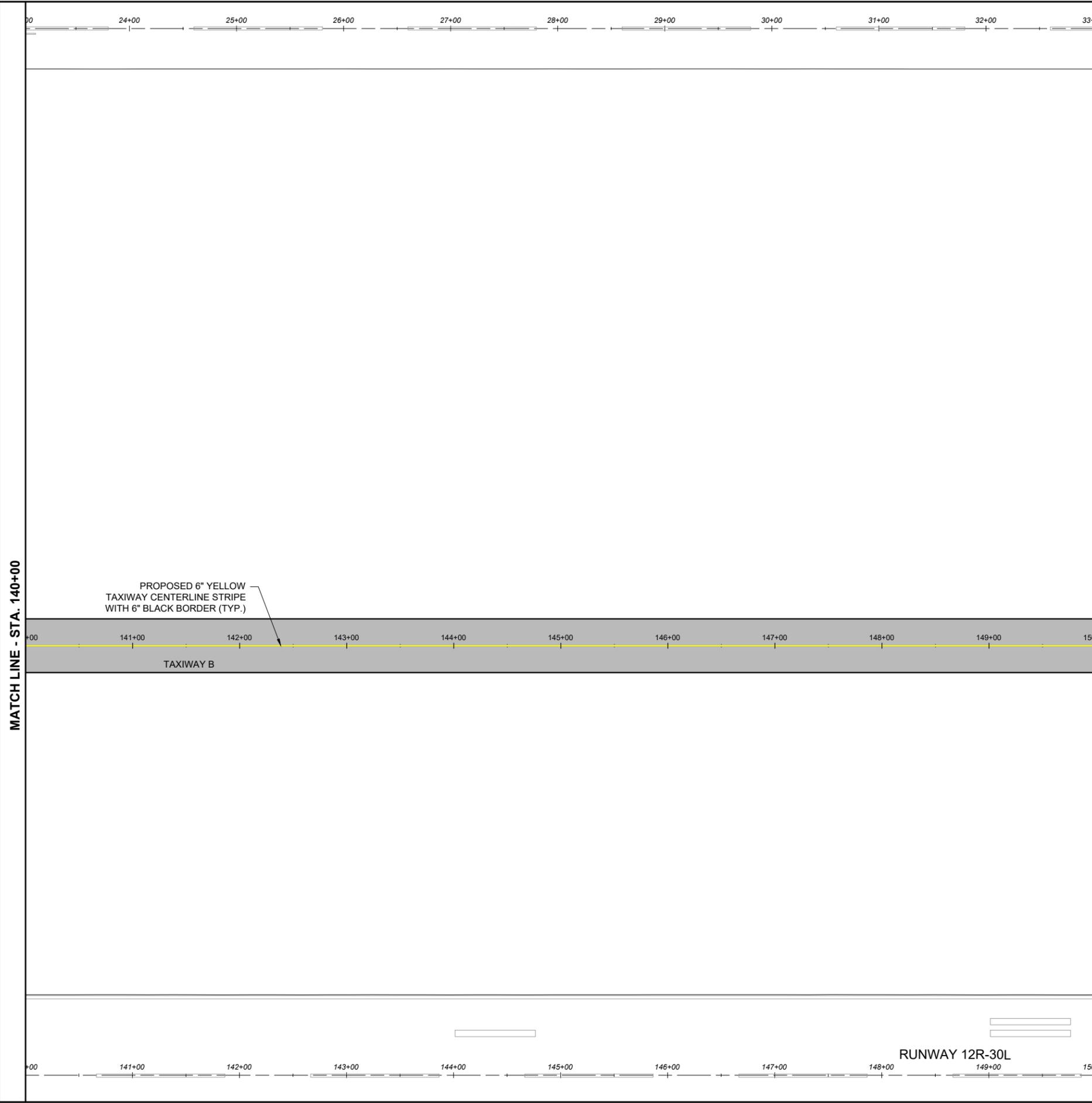
MATCH LINE - STA. 140+00



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- LEGEND**
- EXISTING AIRPORT PROPERTY LINE
  - EXISTING FENCE
  - EXISTING PAVEMENT
  - EXISTING DITCH
  - EXISTING WATER
  - EXISTING STORM SEWER
  - EXISTING UNDERDRAIN
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  - EXISTING TELEPHONE
  - EXISTING COMMUNICATION
  - PROPOSED PAVEMENT

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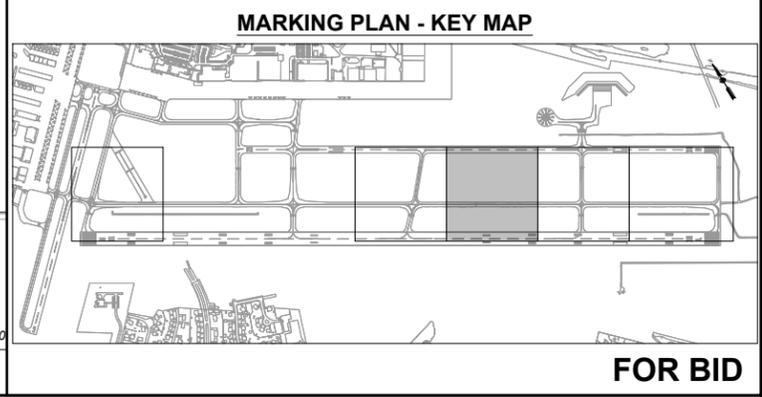
RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
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 IDA NO.: CPS-5078  
 CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
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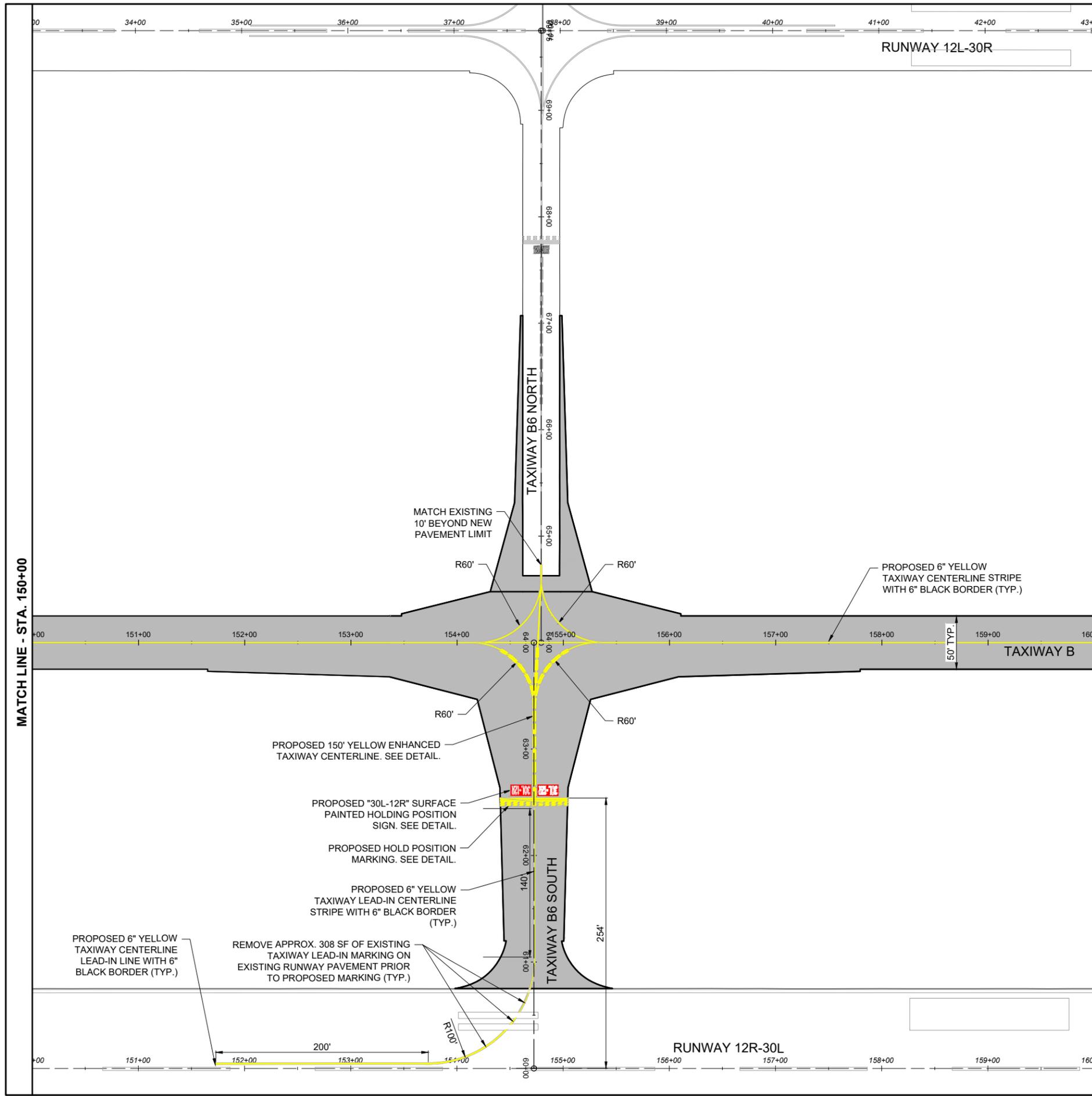
ISSUE: MARCH 14, 2025  
 PROJECT NO: 23A0001D  
 CAD FILE: C-151-MRK.DWG  
 DESIGN BY: JRH 3/5/2025  
 DRAWN BY: AJC 3/11/2025  
 REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PROPOSED MARKING PLAN STA. 140+00 TO STA. 150+00



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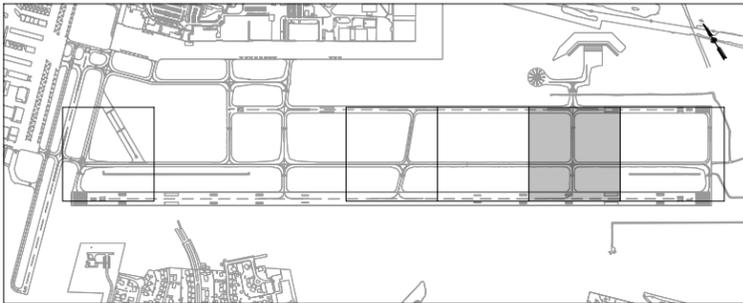




**LEGEND**

- EXISTING AIRPORT PROPERTY LINE
- EXISTING FENCE
- EXISTING PAVEMENT
- EXISTING DITCH
- EXISTING WATER
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING FIBER OPTIC
- EXISTING TELEPHONE
- EXISTING COMMUNICATION
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**MARKING PLAN - KEY MAP**



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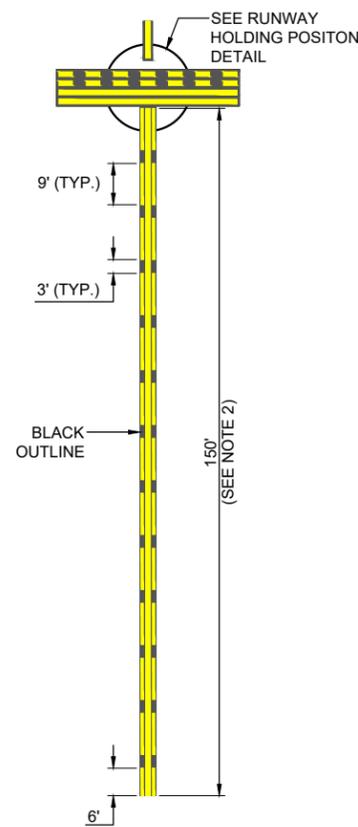
SHEET TITLE

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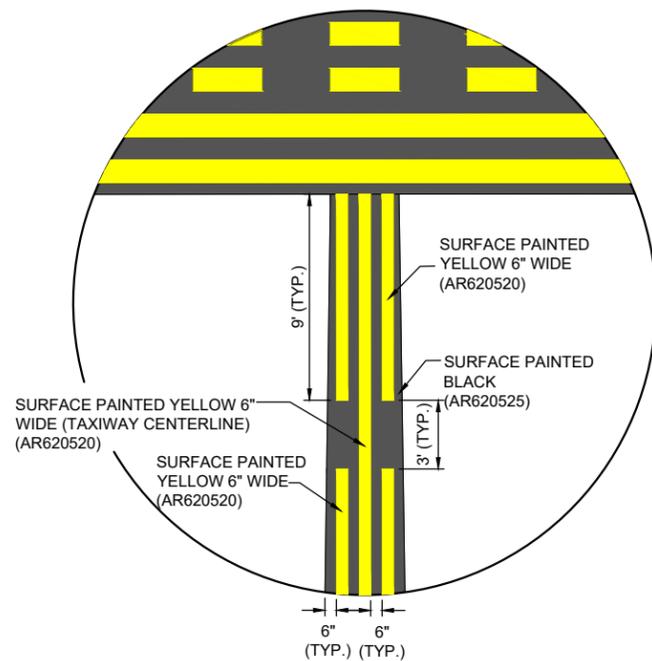




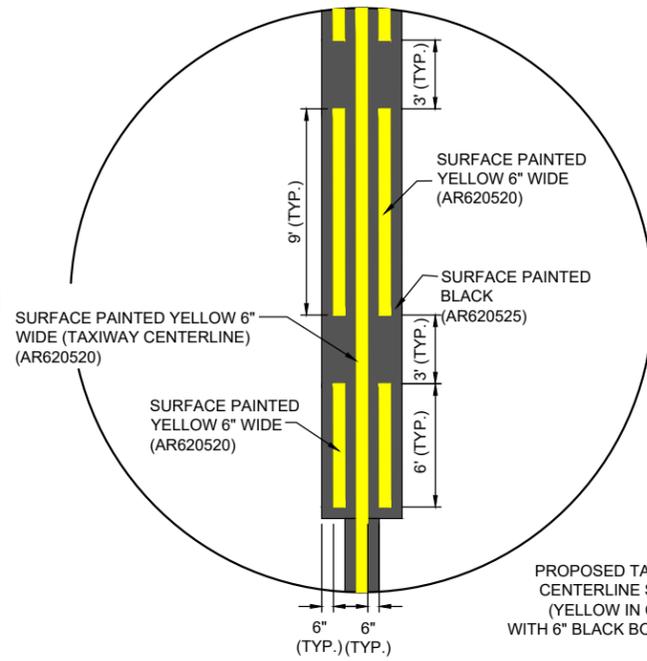




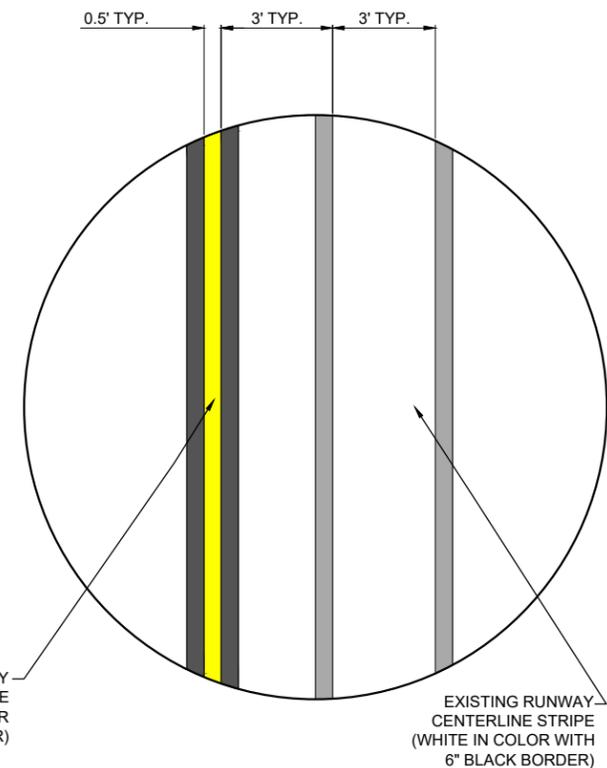
**ENHANCED TAXIWAY CENTERLINE MARKING DETAIL**  
NOT TO SCALE



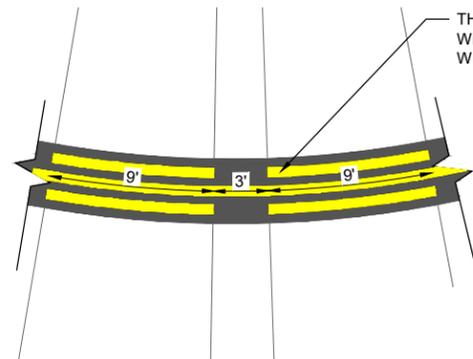
**ENHANCED TAXIWAY MARKING DETAIL (BEGIN)**  
NOT TO SCALE



**ENHANCED TAXIWAY MARKING DETAIL (END)**  
NOT TO SCALE

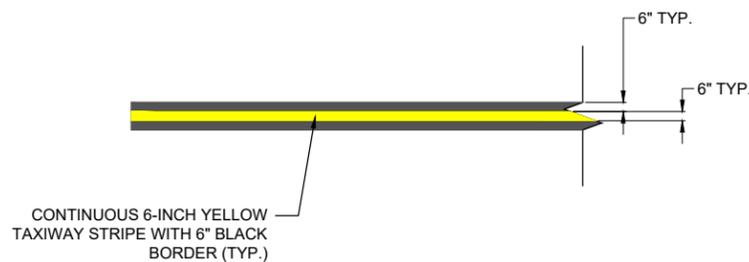


**TAXIWAY CENTERLINE LEAD-IN DETAIL**  
NOT TO SCALE



**ENHANCED TAXIWAY CENTERLINE DETAIL ON CURVE**  
NOT TO SCALE

THE 3-FT GAPS AND 9-FT DASHES ARE MEASURED FROM THE CENTERLINE. WHEN THE ENHANCED TAXIWAY IS CURVED THE DASHES ON THE OUTSIDE WILL BE MORE THAN 9-FT LESS THAN 9 FT ON THE INSIDE OF THE CURVE.



**TAXIWAY CENTERLINE DETAIL**  
NOT TO SCALE

CONTINUOUS 12-INCH YELLOW TAXIWAY LEAD-IN STRIPE WITH 6" BLACK BORDER FROM HOLDING POSITION MARKING TO RWY 12R-30L.

**ENHANCED TAXIWAY CENTERLINE MARKING NOTES:**

- TAXIWAY CENTERLINE MARKINGS SHALL BE YELLOW IN COLOR AND OUTLINED IN BLACK.
- TAXIWAY CENTERLINE MARKINGS SHALL BE ENHANCED FOR 150-FT PRIOR TO A RUNWAY HOLDING POSITION MARKING, UNLESS NOTED OTHERWISE. FOR A CURVED TAXIWAY CENTERLINE, THIS DISTANCE SHALL BE MEASURED ALONG THE CENTERLINE BEING ENHANCED TO A DISTANCE OF 150-FT.
- WHERE TWO TAXIWAY CENTERLINES CONVERGE AT OR BEFORE THE RUNWAY HOLDING POSITION MARKING, PARTIAL INNER DASHED LINES LESS THAN 5 FEET AT THE POINT OF CONVERGENCE MAY BE OMITTED.
- DASHES ON EITHER SIDE OF THE TAXIWAY CENTERLINE MUST BE ALIGNED, STARTING AND STOPPING WITH THE DASHES ON THE OPPOSITE SIDE OF THE CENTERLINE. TO ACCOMPLISH THIS FOR CURVED TAXIWAY CENTERLINES, THE MEASUREMENTS FOR THE DASHES AND GAPS SHALL BE MADE AT THE CENTERLINE AND EXTENDED PERPENDICULAR FROM THE CENTERLINE TO OBTAIN THE LOCATIONS OF THE DASHES.
- ENHANCED TAXIWAY CENTERLINE MARKINGS SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF FAA AC 150/5340-1, "STANDARDS FOR AIRPORT MARKING".
- IF THE CONTRACTOR ELECTS TO "BLOCK PAINT" THE BLACK PAINT AND THEN PAINT EITHER YELLOW, RED, OR WHITE PAINT OVER THE BLACK PAINT; ONLY THE VISIBLE BLACK PAINT WILL BE ELIGIBLE FOR PAYMENT.


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE

PAVEMENT MARKING DETAILS - SHEET 3

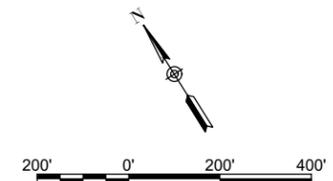
**FOR BID**

**STORM WATER POLLUTION PREVENTION NOTES**

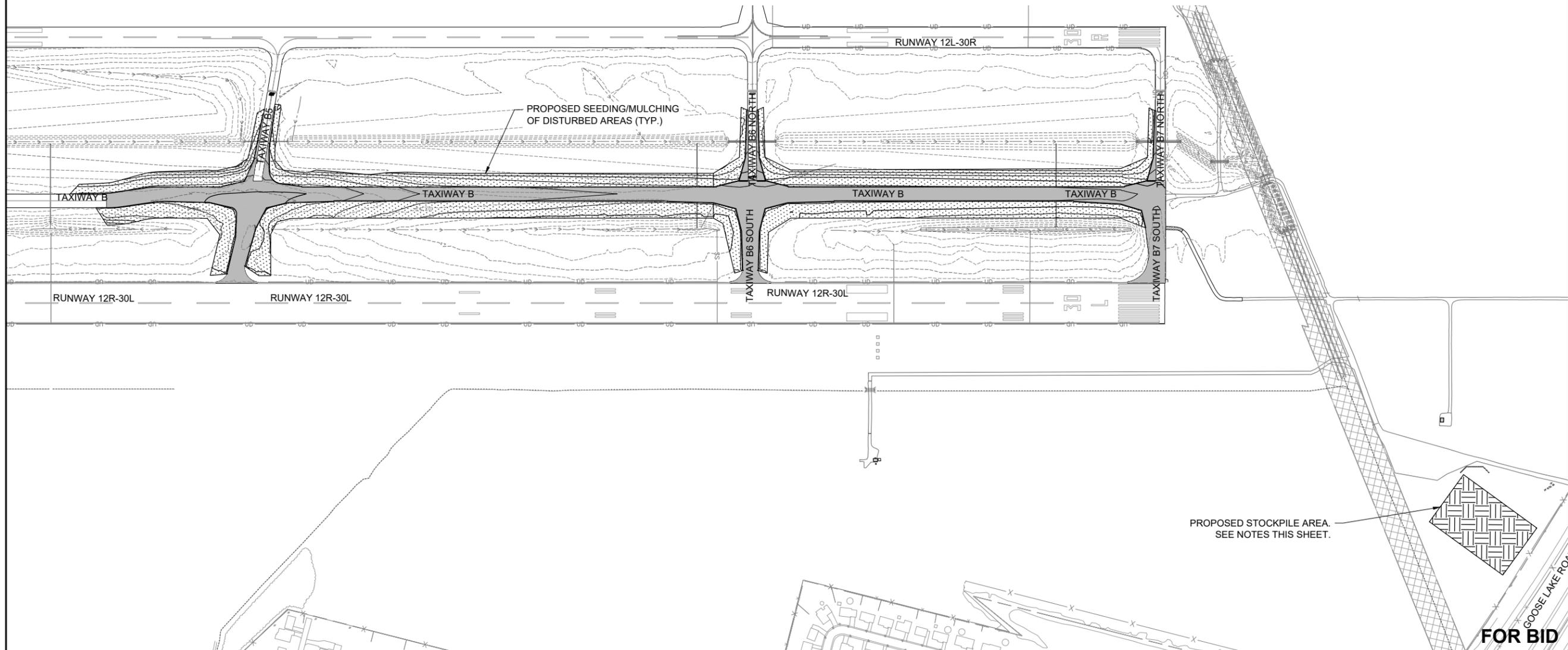
1. THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DAMAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.
2. THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHERE EVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.
3. POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.
4. THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.
5. AS THIS PROJECT WILL BE COVERED BY A NPDES PERMIT, PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR MUST COMPLETE AND SUBMIT TO THE RESIDENT ENGINEER/TECHNICIAN THE "CONTRACTOR CERTIFICATION STATEMENT" AS PROVIDED ON FORM BDE 2342, STORM WATER POLLUTION PROTECTION PLAN (SWPPP).

**STOCKPILE NOTES**

1. THE STOCKPILE LIMITS SHOWN ON THE SCOPE OF WORK AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS ARE APPROXIMATE AND SHALL BE COORDINATED IN THE FIELD. THE STOCKPILE HEIGHT SHALL BE LIMITED TO A MAXIMUM OF 15' ABOVE EXISTING GROUND. THE STOCKPILE SHALL BE GRADED AND MAINTAINED IN ORDER TO PROVIDE POSITIVE DRAINAGE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING POSITIVE DRAINAGE FOR THE DURATION OF CONSTRUCTION. THE CONTRACTOR SHALL INSTALL AND MAINTAIN SILT FENCE SURROUNDING THE PERIMETER OF THE STOCKPILE AREA, SUBTRACTING SECTIONS OF SILT FENCE FOR CONSTRUCTION ACCESS. COST FOR SILT FENCE SHALL BE PAID FOR UNDER PAY ITEM AR156500.
2. AT THE COMPLETION OF CONSTRUCTION, THE PROPOSED STOCKPILE AREA SHALL BE GRADED WITH MOWABLE SLOPES NOT GREATER THAN 4:1 TO THE SATISFACTION OF THE OWNER, AND SHALL BE SEEDED AND HYDROMULCHED. COST FOR FINAL GRADING SHALL BE INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL PAYMENT WILL BE ALLOWED. COST FOR SEEDING AND HYDROMULCHING SHALL BE PAID FOR UNDER THE RESPECTIVE PAY ITEMS. ALTHOUGH THE STOCKPILE DIMENSIONS/LIMITS HAVE NOT BEEN DEFINED SPECIFICALLY, AN ESTIMATED QUANTITY OF 2.00 ACRES OF SEEDING AND MULCHING EACH HAVE BEEN INCLUDED WITHIN THE BID QUANTITIES TO ACCOMMODATE STOCKPILE RESTORATION.



- EXISTING PAVEMENT
- EXISTING UNDERDRAIN
- EXISTING DITCH
- EXISTING MANHOLE/INLET
- EXISTING STORM SEWER
- PROPOSED SEEDING/MULCHING
- PROPOSED PAVEMENT



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LICENSE EXPIRES: 11/30/2025

**RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION**

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
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DESIGN BY: JRH 3/11/2025  
DRAWN BY: AJC 3/11/2025  
REVIEWED BY: BSS 3/12/2025

SHEET TITLE  
**STORM WATER POLLUTION PREVENTION PLAN**

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Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

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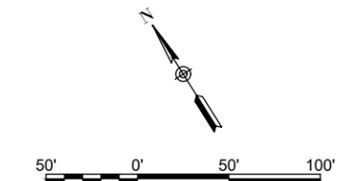
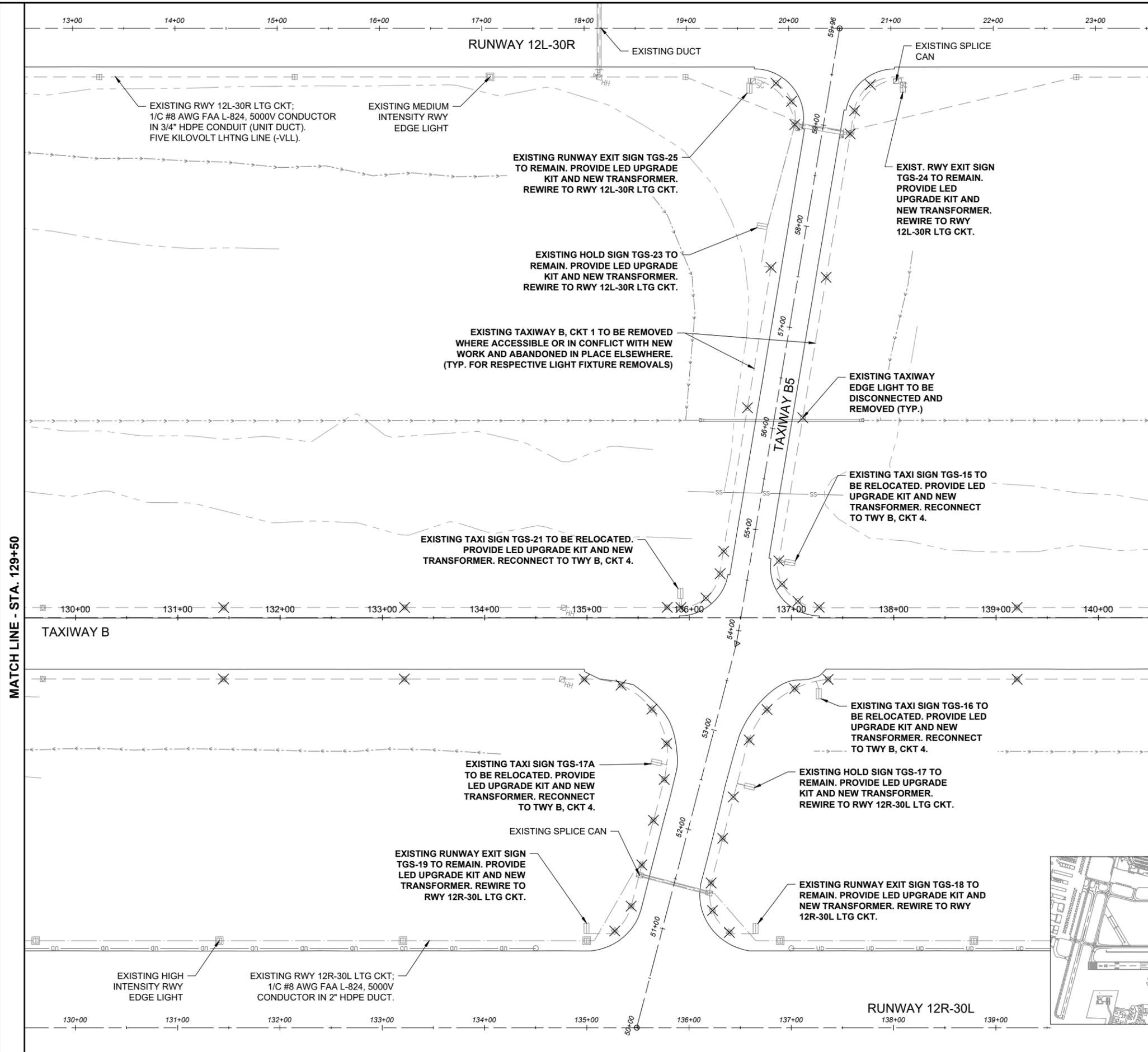
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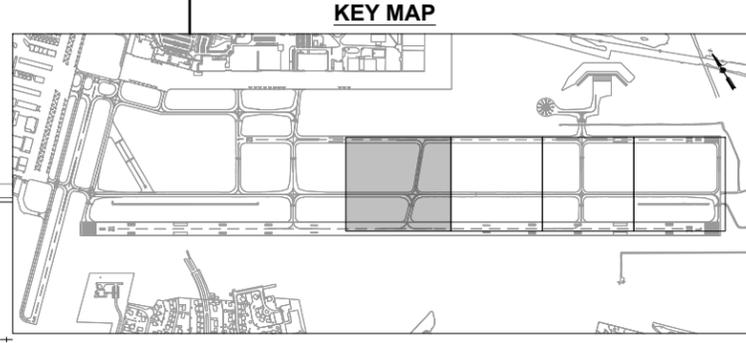
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: C-141-ELE.DWG  
DESIGN BY: KNL 3/5/2025  
DRAWN BY: CWS 3/7/2025  
REVIEWED BY: KNL 3/11/2025

SHEET TITLE

EXISTING ELECTRICAL PLAN  
STA. 129+50 TO STA. 140+50



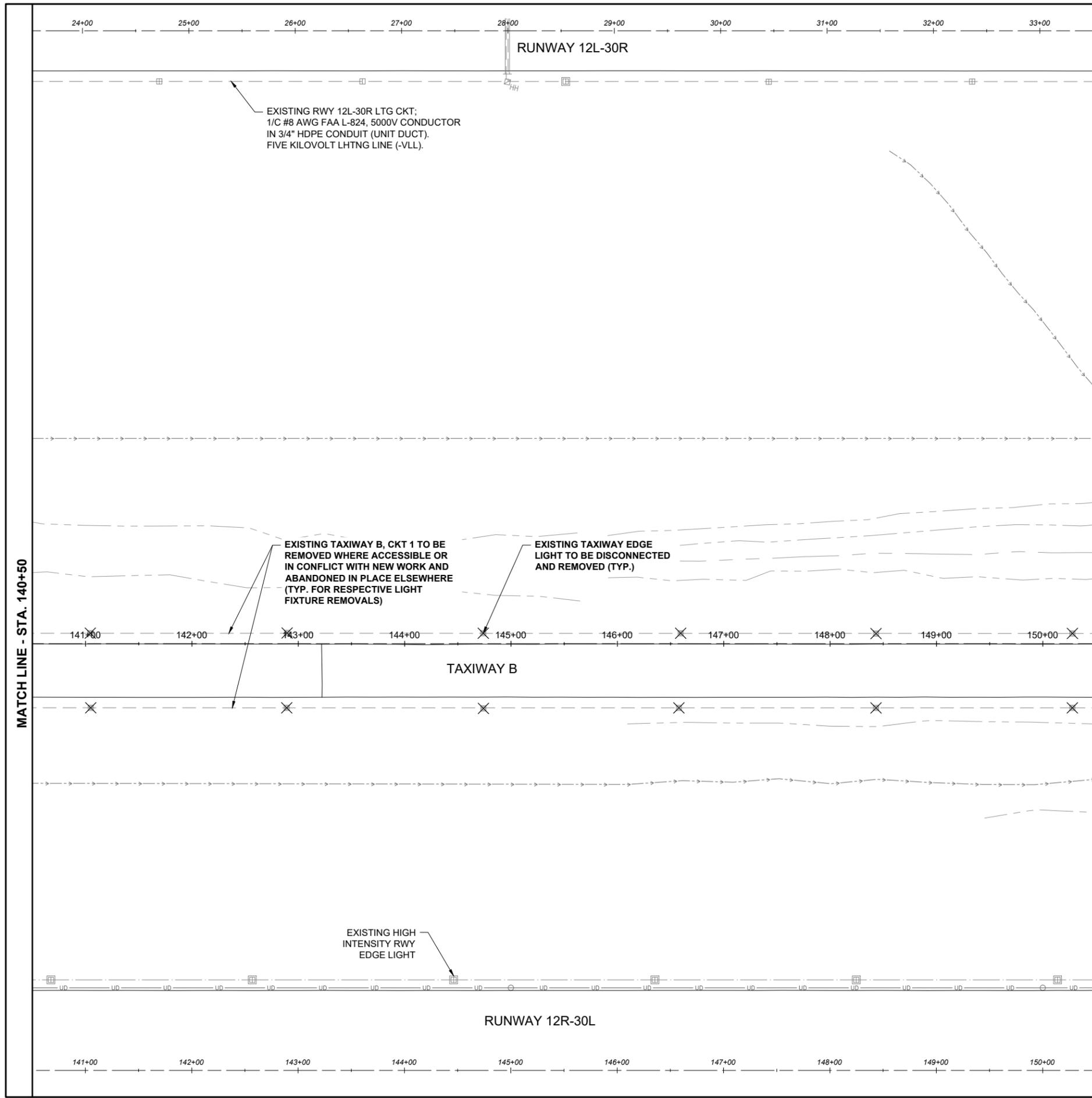
- LEGEND:**
- EXISTING PAVEMENT
  - EXISTING ELECTRICAL DUCT
  - EXISTING DRAINAGE CHANNEL
  - EXISTING TAXIWAY LTG ELECTRICAL CABLE
  - EXISTING RUNWAY LTG ELECTRICAL CABLE
  - EXISTING UG ELECTRIC
  - EXISTING ELECTRICAL CABLES
  - EXISTING STORM SEWER/UNDERDRAIN
  - EXISTING UNDERDRAIN
  - EXISTING SANITARY SEWER
  - EXISTING COMMUNICATION LINE
  - EXISTING FIBER OPTIC
  - EXISTING WATER
  - EXISTING TELEPHONE
  - EXISTING FENCE
  - EXISTING TAXIWAY LIGHT
  - EXISTING TAXIWAY LIGHTS TO BE REMOVED
  - EXISTING BASE MOUNTED RUNWAY LIGHT
  - EXISTING AND/OR RELOCATED RUNWAY/TAXI GUIDANCE SIGN
  - EXISTING JUNCTION CAN, HANDHOLE, OR MANHOLE
  - EXISTING DRAINAGE PIPE



**FOR BID**

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**LEGEND:**

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- EXISTING ELECTRICAL DUCT
- EXISTING DRAINAGE CHANNEL
- EXISTING TAXIWAY LTG ELECTRICAL CABLE
- EXISTING RUNWAY LTG ELECTRICAL CABLE
- EXISTING UG ELECTRIC
- EXISTING ELECTRICAL CABLES
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**KEY MAP**

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 DESIGN BY: KNL 3/5/2025  
 DRAWN BY: CWS 3/7/2025  
 REVIEWED BY: KNL 3/11/2025

SHEET TITLE

EXISTING  
 ELECTRICAL PLAN  
 STA. 140+50 TO STA.  
 150+50





**ST. LOUIS  
DOWNTOWN AIRPORT**

BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025

PROJECT NO: 23A0001D

CAD FILE: C-141-ELE.DWG

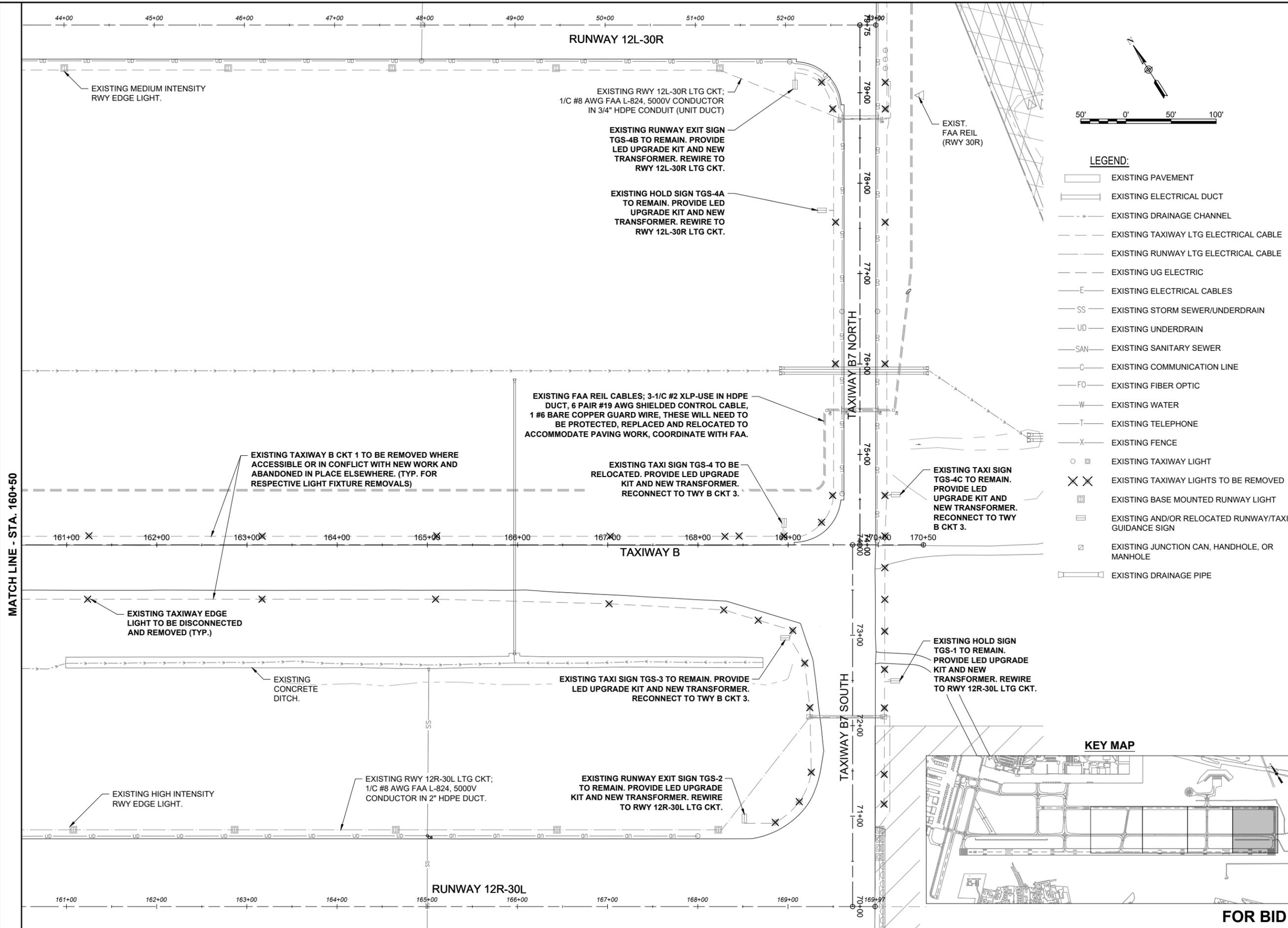
DESIGN BY: KNL 3/5/2025

DRAWN BY: CWS 3/7/2025

REVIEWED BY: KNL 3/11/2025

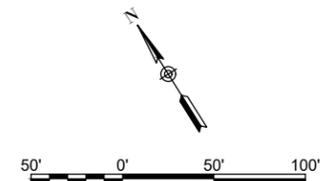
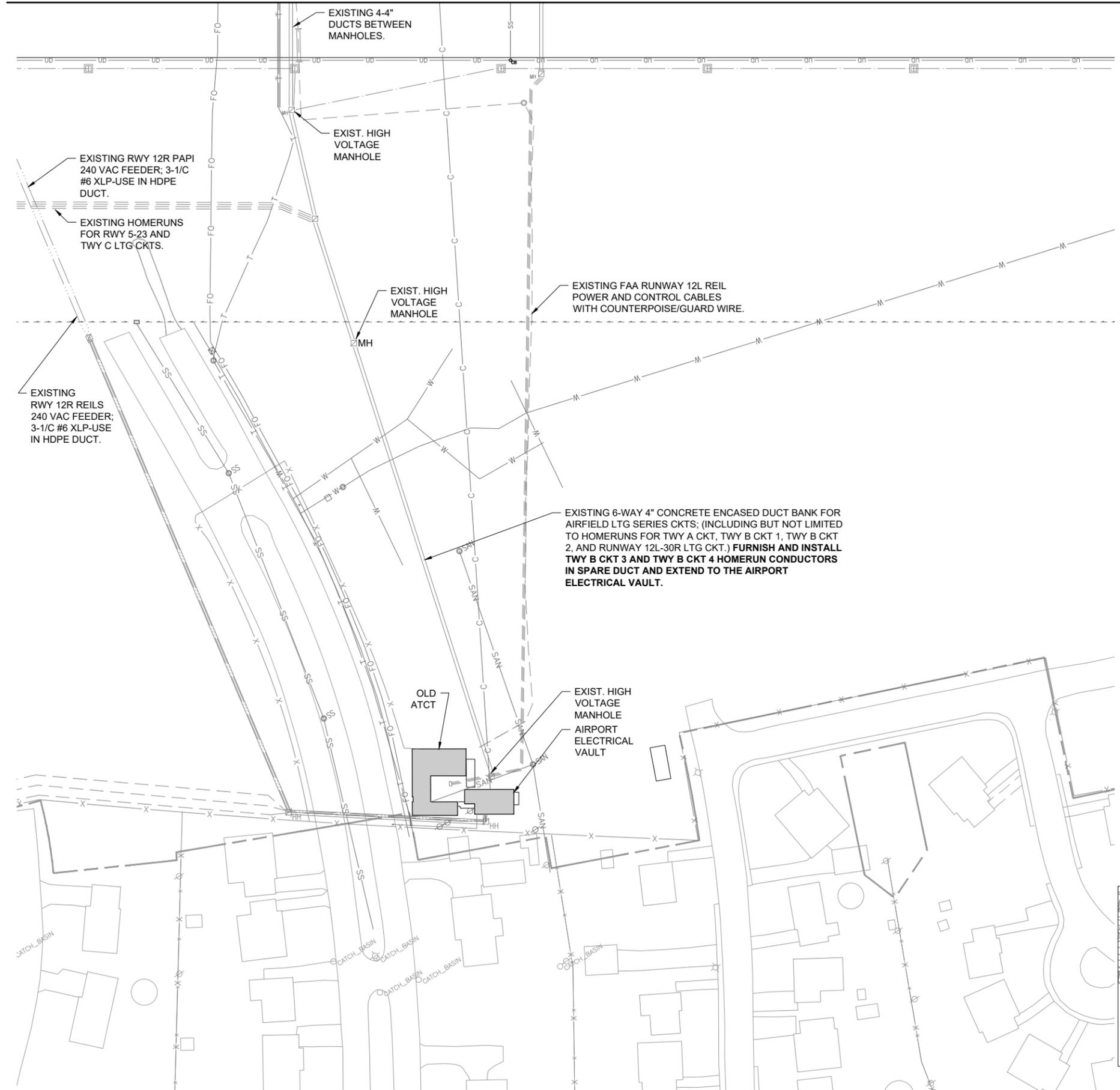
SHEET TITLE

EXISTING  
ELECTRICAL PLAN  
STA. 160+50 TO STA.  
170+50



MAR 14, 2025 9:56 AM GROVED2594  
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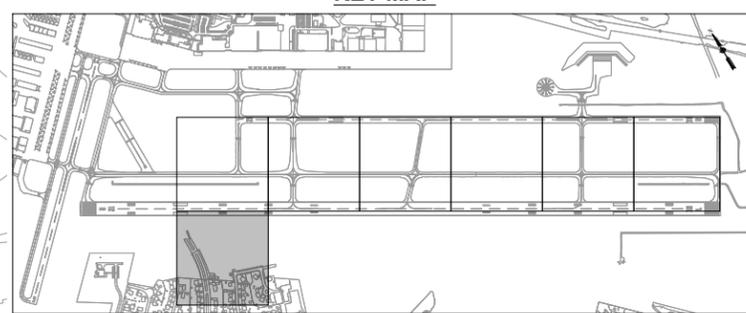
MATCH LINE - 426.32' RT



**LEGEND:**

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING ELECTRICAL DUCT
- PROPOSED ELECTRICAL DUCT
- EXISTING DRAINAGE CHANNEL
- EXISTING TAXIWAY LTG ELECTRICAL CABLE
- EXISTING RUNWAY LTG ELECTRICAL CABLE
- EXISTING UG ELECTRIC
- EXISTING ELECTRICAL CABLES
- EXISTING STORM SEWER/UNDERDRAIN
- EXISTING UNDERDRAIN
- EXISTING SANITARY SEWER
- EXISTING COMMUNICATION LINE
- EXISTING FIBER OPTIC
- EXISTING WATER
- EXISTING TELEPHONE
- EXISTING FENCE
- PROPOSED 1/C #8 AWG, FAA L-824, 5000 VOLT TYPE C UNDERGROUND CABLE IN 2" SCHED 40 (MIN.) PVC OR HDPE DUCT
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- EXISTING BASE MOUNTED RUNWAY LIGHT
- PROPOSED BASE MOUNTED TAXIWAY LIGHT
- EXISTING AND/OR RELOCATED RUNWAY/TAXI GUIDANCE SIGN
- EXISTING JUNCTION CAN, HANDHOLE, OR MANHOLE
- PROPOSED SPLICE CAN

**KEY MAP**



**FOR BID**



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www.hanson-inc.com  
  
Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62703-2886  
Telephone: 217.788.2450  
Fax: 217.788.2503



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IDA NO.: CPS-5078  
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NO.	DATE	DESCRIPTION		
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ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
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DESIGN BY: KNL 3/5/2025  
DRAWN BY: CWS 3/7/2025  
REVIEWED BY: KNL 3/11/2025

SHEET TITLE

PROPOSED  
ELECTRICALVAULT  
HOMERUN PLAN

MAR 14, 2025 9:59 AM GROVED2594 I:\23\JOBS\23A0001\DCAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BS-22X34\_BORDER



**ST. LOUIS DOWNTOWN AIRPORT**

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6100 Archview Drive  
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*Kevin N. Lightfoot*

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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
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ISSUE: MARCH 14, 2025

PROJECT NO: 23A0001D

CAD FILE: C-142-ELE.DWG

DESIGN BY: KNL 3/5/2025

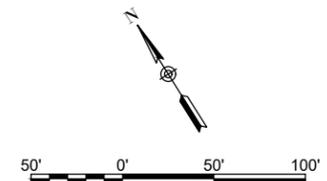
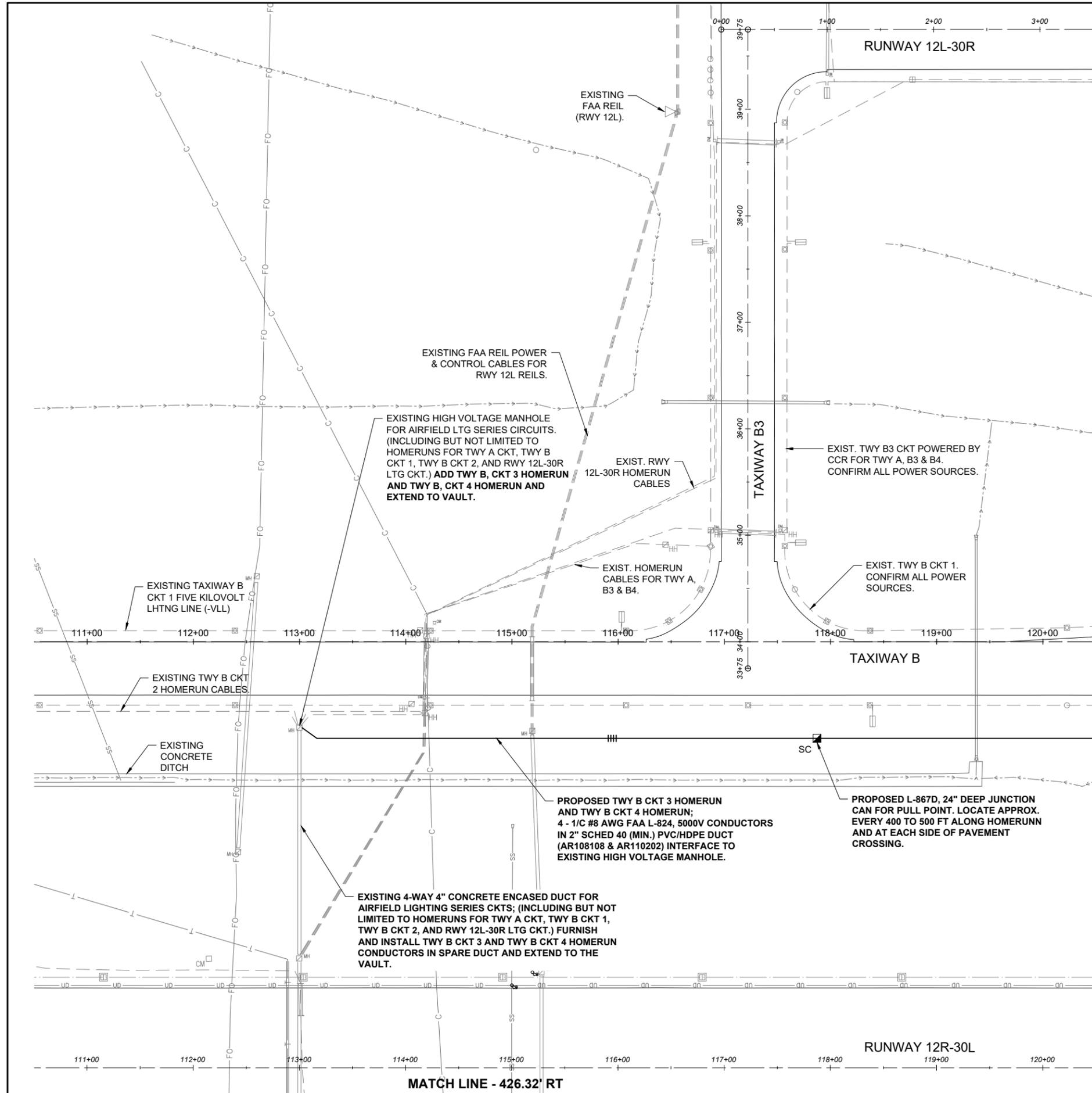
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REVIEWED BY: KNL 3/11/2025

SHEET TITLE

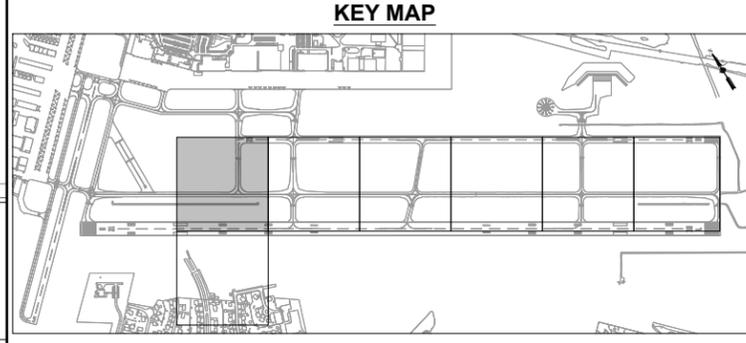
PROPOSED ELECTRICAL PLAN  
STA. 110+50 TO STA. 120+50

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- LEGEND:**
- [Symbol] EXISTING PAVEMENT
  - [Symbol] PROPOSED PAVEMENT
  - [Symbol] EXISTING ELECTRICAL DUCT
  - [Symbol] PROPOSED ELECTRICAL DUCT
  - [Symbol] EXISTING DRAINAGE CHANNEL
  - [Symbol] EXISTING TAXIWAY LTG ELECTRICAL CABLE
  - [Symbol] EXISTING RUNWAY LTG ELECTRICAL CABLE
  - [Symbol] EXISTING UG ELECTRIC
  - [Symbol] EXISTING ELECTRICAL CABLES
  - [Symbol] SS EXISTING STORM SEWER/UNDERDRAIN
  - [Symbol] UD EXISTING UNDERDRAIN
  - [Symbol] SAN EXISTING SANITARY SEWER
  - [Symbol] C EXISTING COMMUNICATION LINE
  - [Symbol] FO EXISTING FIBER OPTIC
  - [Symbol] W EXISTING WATER
  - [Symbol] T EXISTING TELEPHONE
  - [Symbol] X EXISTING FENCE
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  - [Symbol] EXISTING BASE MOUNTED RUNWAY LIGHT
  - [Symbol] PROPOSED BASE MOUNTED TAXIWAY LIGHT
  - [Symbol] EXISTING AND/OR RELOCATED RUNWAY/TAXI GUIDANCE SIGN
  - [Symbol] EXISTING JUNCTION CAN, HANDHOLE, OR MANHOLE
  - [Symbol] SC PROPOSED SPLICE CAN

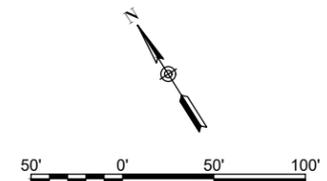
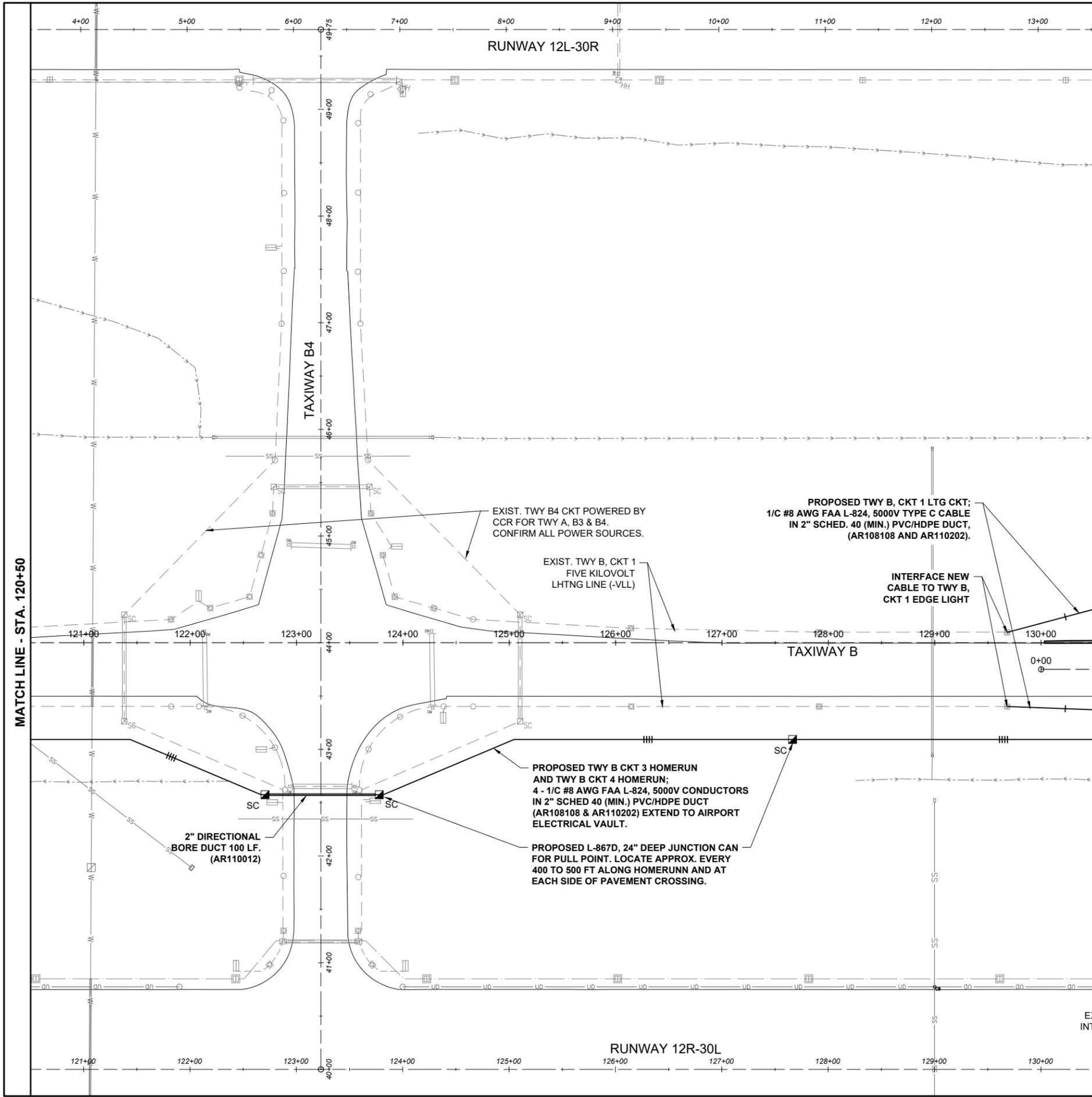
MATCH LINE - STA. 120+50



**FOR BID**

MATCH LINE - 426.32' RT

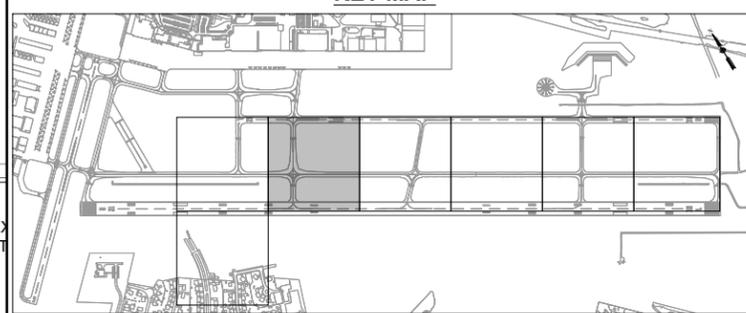
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**LEGEND:**

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- EXISTING DRAINAGE CHANNEL
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**KEY MAP**



**FOR BID**

**HANSON**  
 Engineering | Planning | Allied Services  
 Offices Nationwide  
 www.hanson-inc.com  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2886  
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COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
 CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

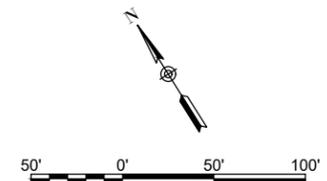
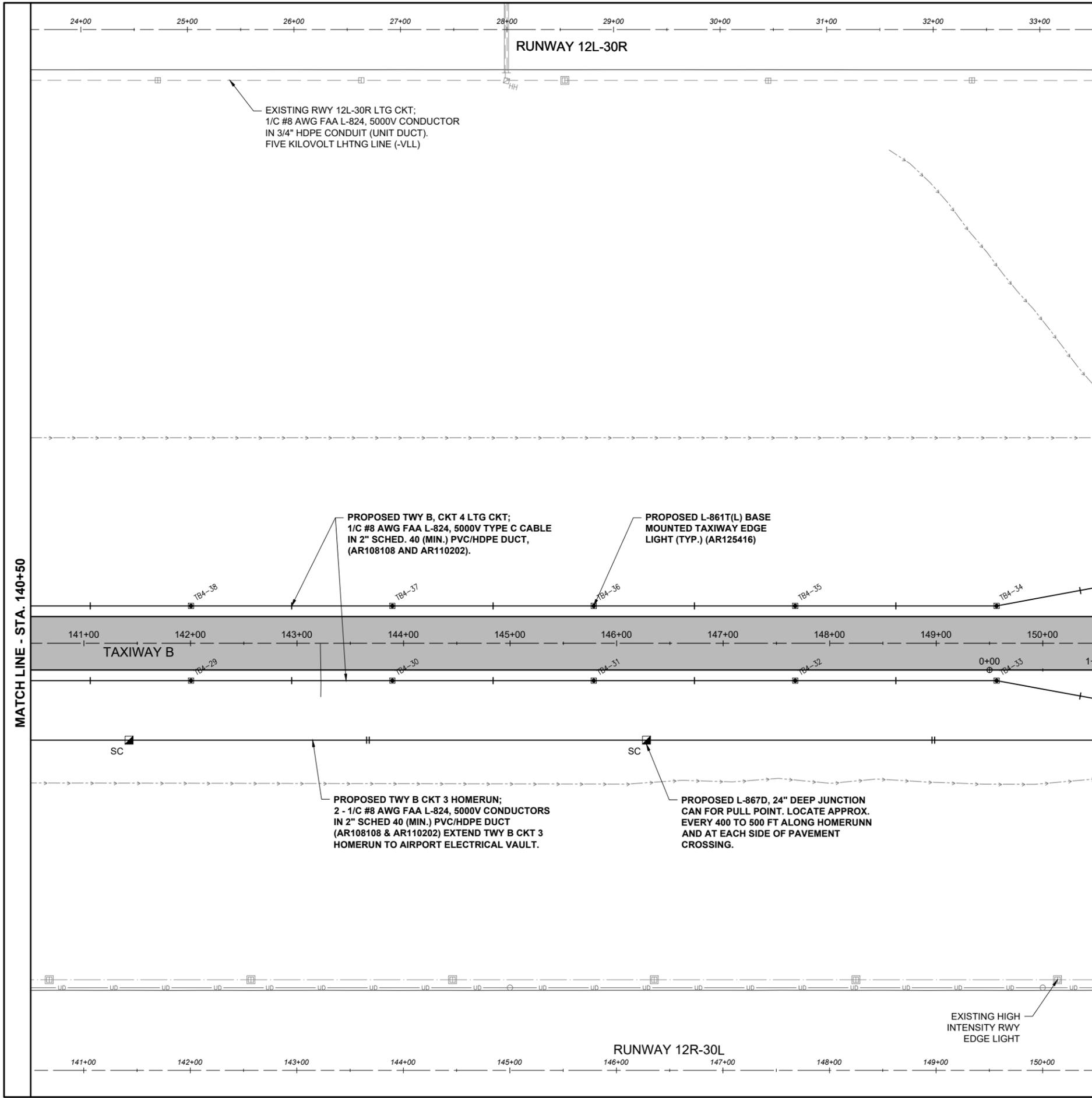
ISSUE: MARCH 14, 2025  
 PROJECT NO: 23A0001D  
 CAD FILE: C-142-ELE.DWG  
 DESIGN BY: KNL 3/5/2025  
 DRAWN BY: CWS 3/7/2025  
 REVIEWED BY: KNL 3/11/2025

SHEET TITLE

PROPOSED ELECTRICAL PLAN  
 STA. 120+50 TO STA. 130+50



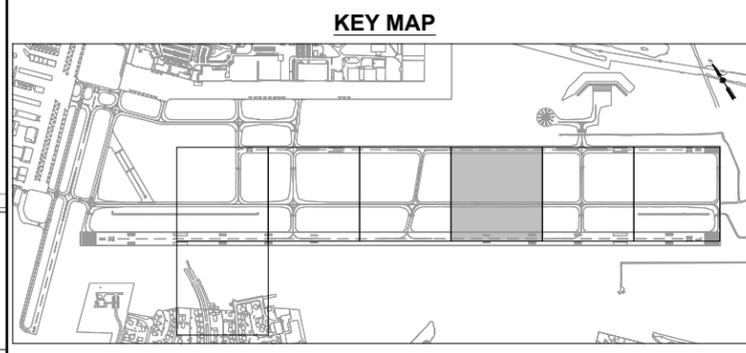
MAR 14, 2025 10:00 AM GROVE02594  
 I:\23\JOBS\23A000\1\DCAD\AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BS-22X34\_BORDER



- LEGEND:**
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  - PROPOSED PAVEMENT
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MATCH LINE - STA. 150+50

MATCH LINE - STA. 140+50



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 DESIGN BY: KNL 3/5/2025  
 DRAWN BY: CWS 3/7/2025  
 REVIEWED BY: KNL 3/11/2025

SHEET TITLE

PROPOSED ELECTRICAL PLAN  
 STA. 140+50 TO STA. 150+50





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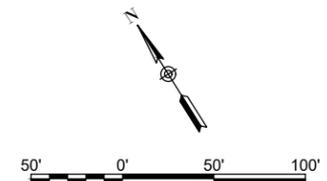
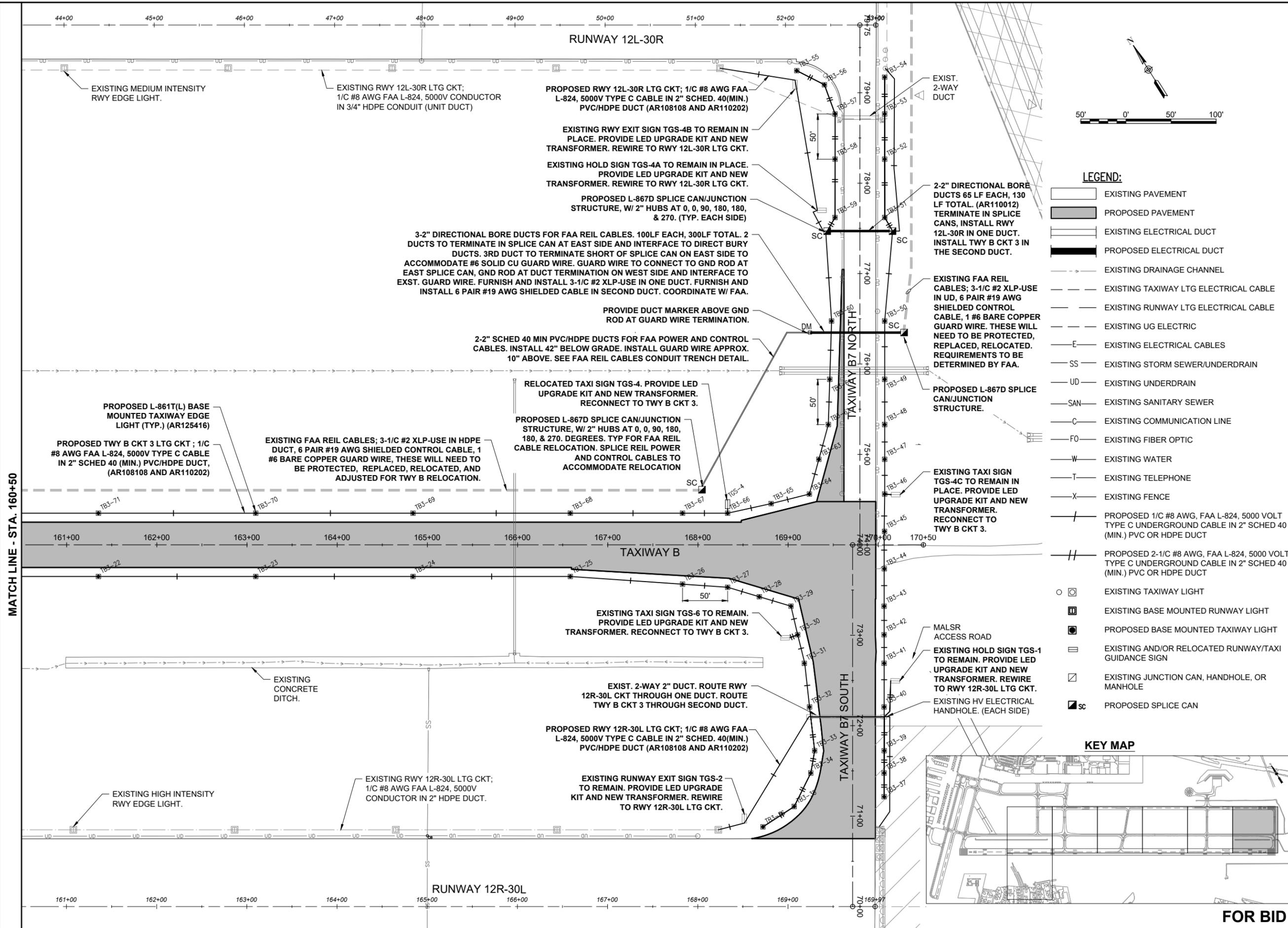
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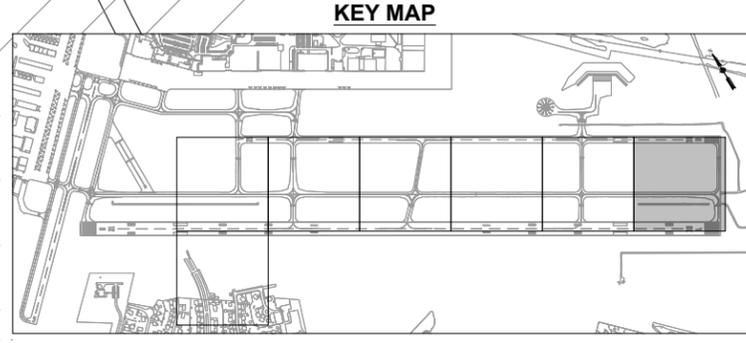
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SHEET TITLE

PROPOSED  
ELECTRICAL PLAN  
STA. 160+50 TO STA.  
170+50



- LEGEND:**
- EXISTING PAVEMENT
  - PROPOSED PAVEMENT
  - EXISTING ELECTRICAL DUCT
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  - EXISTING JUNCTION CAN, HANDHOLE, OR MANHOLE
  - PROPOSED SPLICE CAN



**FOR BID**

MAR 14, 2025 10:00 AM GROVE02594 1:23:JOBS23A0001DCAD:AIRPORT\LIBRARY\2025 CPS-5078 COMBINED LIBRARY\G-BS-22X34\_BORDER

**AIRFIELD LIGHTING REMOVAL, RELOCATION, AND INSTALLATION NOTES**

- KEEP ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS COORDINATED WITH THE AIRPORT DIRECTOR/MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT). WHERE THE FACILITY IS NOT EQUIPPED WITH LOCK/TAGOUT EQUIPMENT THE RESPECTIVE PERSONNEL WILL BE RESPONSIBLE FOR PROVIDING THE APPROPRIATE LOCKOUT/TAGOUT EQUIPMENT. FAILURE TO SHUT DOWN AND LOCKOUT THE CIRCUIT(S) PRESENTS A DANGEROUS HAZARD FOR PERSONNEL WORKING ON THIS SYSTEM.
- EACH RESPECTIVE PERSON PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH, AND QUALIFIED TO WORK ON, 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT. ONLY QUALIFIED ELECTRICAL CONTRACTORS SHALL PERFORM ELECTRICAL WORK ON THIS PROJECT. NEC DEFINES A QUALIFIED PERSON AS FOLLOWS; "ONE WHO HAS SKILLS AND KNOWLEDGE RELATED TO THE CONSTRUCTION AND OPERATION OF THE ELECTRICAL EQUIPMENT AND INSTALLATIONS AND HAS RECEIVED SAFETY TRAINING TO RECOGNIZE AND AVOID THE HAZARDS INVOLVED".
- VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, RELOCATING, INSTALLING, CONNECTING OR WORKING ON THE RESPECTIVE AIRFIELD LIGHTING, DISTANCE REMAINING SIGN, RUNWAY SIGN, TAXI SIGN, NAVAID, VAULT EQUIPMENT OR OTHER DEVICE.
- INSTALL AIRFIELD LIGHTING, SIGNS, SPLICE CANS, ELECTRICAL DUCTS, HANDHOLES, MANHOLES, AND CABLE AT THE LOCATIONS SHOWN AND IN COMPLIANCE WITH THE SPECIFICATIONS, SPECIAL PROVISIONS, RESPECTIVE DETAILS, AND MANUFACTURER'S RECOMMENDATIONS.
- NEW AIRFIELD LIGHTING SYSTEM INSTALLATIONS, ADJUSTMENTS, RELOCATIONS, REINSTALLATIONS, AND/OR UPGRADES SHALL USE BASE (L-867 OR L-868) MOUNTED FIXTURES AND 1/C #8, FAA L-824 5000V TYPE C CABLE IN A CLOSED CONDUIT SYSTEM.
- LIGHTING CABLE FOR AIRFIELD LIGHTING SERIES CIRCUITS SHALL BE 1/C, #8 AWG, FAA L-824, 5000 VOLT, TYPE C UNDERGROUND CABLE IN 2" (MIN.) CONDUIT AND/OR DUCT. CABLE SHALL BE FAA APPROVED.
- IN AREAS WHERE THERE IS A CONGESTION OF CABLES OR WHERE THE PROPOSED CABLE CROSSES AN EXISTING CABLE, THE CONTRACTOR IS REQUIRED TO HAND DIG THE TRENCH NECESSARY FOR THE PROPOSED CABLE. AT OTHER LOCATIONS, THE PROPOSED CABLE MAY BE TRENCHED OR PLOWED INTO PLACE. HAND DIGGING, TRENCHING AND/OR PLOWING WILL BE CONSIDERED INCIDENTAL TO THE PROPOSED CABLES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- GROUND ROD MUST BE INSTALLED AT EACH LIGHT FIXTURE, RUNWAY/TAXI SIGN AND SPLICE CAN. THE PURPOSE OF THE LIGHT BASE GROUND IS TO PROVIDE A DEGREE OF PROTECTION FOR MAINTENANCE PERSONNEL FROM POSSIBLE CONTACT WITH AN ENERGIZED LIGHT BASE OR MOUNTING STAKE THAT MAY RESULT FROM A SHORTED POWER CABLE OR ISOLATION TRANSFORMER. PER NATIONAL ELECTRICAL CODE ARTICLE 250.53 "GROUNDING ELECTRODE SYSTEM INSTALLATION" RESISTANCE FROM THE GROUND ROD/ELECTRODE TO EARTH GROUND MUST BE 25 OHMS OR LESS VIA MEASUREMENT WITH A GROUND TESTER. GROUND RODS FOR LIGHT BASE GROUNDS SHALL BE 3/4-INCH BY 10-FEET MINIMUM LENGTH UL LISTED COPPER-CLAD STEEL SECTIONAL RODS. GROUND RODS SHALL BE PRODUCED FROM 100% DOMESTIC STEEL. EACH GROUND ROD SHALL BE TESTED AND THE RESULTS RECORDED FOR EACH AIRFIELD LIGHT FIXTURE AND RUNWAY/TAXI SIGN INSTALLATION. COPIES OF GROUND SYSTEM TEST RESULTS SHALL BE FURNISHED TO THE PROJECT ENGINEER OF RECORD; KEVIN LIGHTFOOT, AND THE RESIDENT ENGINEER/TECHNICIAN.
- HOMERUN CABLES FOR A RESPECTIVE CIRCUIT THAT ARE INSTALLED IN CONDUIT OR DUCT SHALL BE RUN TOGETHER IN THE SAME RACEWAY OR DUCT.
- THE CONTRACTOR SHALL TEST THE RESPECTIVE AIRFIELD LIGHTING CIRCUITS IN AREAS OF WORK WHERE RESPECTIVE CIRCUITS MIGHT BE AFFECTED. THE RESPECTIVE RUNWAY AND TAXIWAY LIGHTING CCR'S (FOR THE AREAS OF WORK ON THIS PROJECT) SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, AND/OR ADDITIONS AND AFTER THE NEW CABLES AND LIGHTING SYSTEM MODIFICATIONS AND ADDITIONS HAVE BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATOR IN THE AUTOMATIC AND MANUAL MODES OF OPERATIONS. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT ENGINEER/TECHNICIAN. TEST RESULTS SHALL BE COORDINATED WITH AND PROVIDED TO THE PROJECT ENGINEER OF RECORD; KEVIN LIGHTFOOT, AND RESIDENT ENGINEER/TECHNICIAN.
- FAA AC 150/5370-10G "STANDARDS FOR SPECIFYING CONSTRUCTION OF AIRPORTS", ITEM L-108 "UNDERGROUND POWER CABLE FOR AIRPORTS", REQUIRES THAT EVERY AIRFIELD LIGHTING CABLE SPLICER SHALL BE QUALIFIED IN MAKING CABLE SPLICES AND TERMINATIONS ON CABLES RATED ABOVE 5,000 VOLTS AC. CABLE SPLICING/TERMINATING PERSONNEL SHALL HAVE A MINIMUM OF THREE (3) YEARS CONTINUOUS EXPERIENCE IN TERMINATING/SPLICING MEDIUM VOLTAGE CABLE.
- OTHER CONSTRUCTION PROJECTS MIGHT BE IN PROGRESS AT THE AIRPORT AT THE SAME TIME AS THIS PROJECT. THE CONTRACTOR WILL BE REQUIRED TO COOPERATE WITH ALL OTHER CONTRACTORS AND THE AIRPORT MANAGER IN THE COORDINATION OF THE WORK.
- OBTAIN APPROVAL FROM THE AIRPORT MANAGER PRIOR TO SHUTTING DOWN A RUNWAY OR TAXIWAY. WHEN A RESPECTIVE RUNWAY IS CLOSED THE RESPECTIVE RUNWAY LIGHTING AND NAVAIDS FOR THAT RUNWAY SHALL BE SHUT OFF. WHEN A RESPECTIVE TAXIWAY IS CLOSED THE RESPECTIVE TAXIWAY LIGHTING FOR THAT TAXIWAY SHALL BE SHUT OFF.
- THE CONTRACTOR IS REQUIRED TO FILL IN ALL HOLES AND DEPRESSIONS RESULTING FROM THE NEW WORK, WITH EARTH MATERIAL. THE AREAS SHALL BE COMPACTED TO PREVENT FUTURE SETTLEMENT AND FERTILIZED, SEEDED, AND MULCHED IN ACCORDANCE WITH ITEMS 901 AND 908 RESPECTIVELY.
- IN THE EVENT A CONFLICT IS DETERMINED WITH RESPECT TO MANUFACTURER INSTALLATION INSTRUCTIONS, NEC, AND/OR THE CONTRACT DOCUMENTS, CONTACT THE PROJECT ENGINEER FOR FURTHER DIRECTION.
- SEE SAFETY PLAN AND NOTES FOR SAFETY AND CONSTRUCTION COORDINATION REQUIREMENTS.
- EXISTING AIRFIELD LIGHTS AND/OR SIGNS DESIGNATED FOR REMOVAL SHALL BE CAREFULLY REMOVED IN THEIR ENTIRETY. THE CONTRACTOR SHALL DISCONNECT AND REMOVE THE EXISTING LIGHTS AND SIGNS, AS NOT TO DAMAGE THEM, INCLUDING MOUNTING STAKES, BASES, FOUNDATIONS AND TRANSFORMERS. THE EXISTING AIRFIELD LIGHTS, TRANSFORMERS, LIGHT BASES, COVERS AND MOUNTING STAKES SHALL BE TURNED OVER TO THE AIRPORT. SIGNS SHALL BE TURNED OVER TO THE AIRPORT FOR THEIR RIGHT OF FIRST REFUSAL. LIGHT BASES AND SIGN FOUNDATIONS SHALL BE REMOVED AND DISPOSED OF OFF SITE. ANY MATERIAL NOT SALVAGED BY THE AIRPORT SHALL BE DISPOSED OF OFF THE AIRPORT SITE, IN A LEGAL MANNER, AT THE CONTRACTOR'S OWN EXPENSE. EXISTING DUCTS AND CABLES ASSOCIATED WITH AIRFIELD LIGHTING REMOVALS, RELOCATIONS, REPLACEMENTS AND/OR CABLE OR DUCT REPLACEMENTS SHALL BE REMOVED AND DISPOSED OF OFF SITE AT NO ADDITIONAL COST TO THE CONTRACT WHERE ACCESSIBLE AND ABANDONED IN PLACE ELSEWHERE. PROVIDE TEMPORARY CABLES AND DUCTS TO ACCOMMODATE AIRFIELD LIGHTING CIRCUITS THAT ARE TO REMAIN ACTIVE DURING CONSTRUCTION. CONTRACTOR MAY REMOVE ABANDONED CABLES AT NO ADDITIONAL COST TO THE CONTRACT AND SHALL HAVE THE SALVAGE RIGHTS TO ABANDONED CABLES. REMOVAL OF EXISTING AIRFIELD LIGHTING WILL BE PAID FOR UNDER ITEM AR800476 REMOVE AIRFIELD LIGHTING PER LUMP SUM.
- OWNER SHALL BE KEPT INFORMED OF WORK AND SCHEDULES.
- ROUTE NEW CABLES AND DUCTS TO AVOID INTERFERENCES WITH OTHER UTILITIES, LINES, CABLES AND STRUCTURES.
- ALL ELECTRICAL EQUIPMENT (INCLUDING AIRFIELD LIGHTING AND NAVAIDS) AND MATERIALS SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRIC CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS, AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, INTERNEK TESTING SERVICES VERIFICATION/ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
- CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2G (OR MOST CURRENT ISSUE) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E - STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.
- RUNWAY AND TAXIWAY LIGHTING CIRCUITS SHALL BE ACTIVE AT THE END OF EACH CONSTRUCTION DAY FOR AN OPEN RUNWAY OR AN OPEN TAXIWAY. THE CONTRACTOR SHALL PROVIDE TEMPORARY CABLE & CONNECTIONS WHERE NECESSARY TO MAINTAIN A RUNWAY OR TAXIWAY LIGHTING SYSTEM. TEMPORARY CABLE FOR AIRFIELD LIGHTING SERIES CIRCUITS SHALL BE 1/C #8 FAA L-824 5KV UG CABLE IN DUCT OR UNIT DUCT.
- WHEN A RUNWAY IS CLOSED THE LIGHTING AND NAVAIDS FOR THAT RUNWAY SHALL BE SHUT OFF. KEEP RESPECTIVE NAVAIDS ACTIVE DURING TIMES WHEN RESPECTIVE RUNWAY IS OPEN. NAVAIDS RECEIVING MAINTENANCE SHALL BE SHUT OFF UNTIL OPERATING PROPERLY. COORDINATE WITH AIRPORT MANAGER TO ISSUE NOTAMS WHEN AIRFIELD LIGHTING AND/OR NAVAIDS ARE OUT OF SERVICE.
- ALL ABOVEGROUND JUMPERS SHALL BE IN A DUCT WITH ALL CONNECTIONS SEALED. THE CONTRACTOR SHALL SECURE, IDENTIFY AND PLACE ALL TEMPORARY EXPOSED WIRING IN CONDUIT, DUCT, OR UNIT DUCT TO PREVENT ELECTROCUTION AND FIRE IGNITION SOURCES AS PER THE REQUIREMENTS OF FAA 150/5370-2G, OPERATION SAFETY ON AIRPORTS DURING CONSTRUCTION, SECTION 2.18.3 "LIGHTING AND VISUAL NAVAIDS". ALL LABOR, MATERIALS, AND TIME NECESSARY TO COMPLY WITH THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- CONTRACTOR SHALL INTERFACE EXISTING AIRFIELD LIGHTING AND/OR SIGNS TO THE NEW, REMOVED, REINSTALLED, ADJUSTED, REPLACED, AND/OR RELOCATED AIRFIELD LIGHTING AND ASSOCIATED CIRCUITS.
- ALL AIRFIELD LIGHT FIXTURES SHALL BE TAGGED BY THE CONTRACTOR IN ACCORDANCE WITH THE RESPECTIVE LIGHT FIXTURE NUMBERS. CONFIRM LIGHT FIXTURE NUMBERING WITH THE AIRPORT MANAGER/MAINTENANCE SUPERVISOR.
- HIGH VOLTAGE CIRCUITS (AIRFIELD LIGHTING 5000 VOLT SERIES CIRCUITS AND OTHER CIRCUITS RATED ABOVE 600 VOLTS) AND LOW VOLTAGE CIRCUITS (RATED 600 VOLTS AND BELOW) SHALL NOT BE INSTALLED IN THE SAME WIREWAY, CONDUIT, DUCT, RACEWAY, JUNCTION STRUCTURE, OR HANDHOLE.
- THE CONTRACTOR IS REQUIRED TO RESTORE ALL DISTURBED PAVEMENT ASSOCIATED WITH REMOVAL WORK AND/OR NEW AIRFIELD LIGHTING INSTALLATIONS.
- NO CONNECTION TO AN ACTIVE LIGHTING CIRCUIT WILL BE BROKEN UNTIL THE CIRCUIT HAS BEEN TURNED OFF IN ACCORDANCE WITH NOTE 1.

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER/TECHNICIAN SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.



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BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

**COVERING ELECTRICAL DESIGN**



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: E-001-NOTES.DWG  
DESIGN BY: KNL 3/5/2025  
DRAWN BY: CWS 3/7/2025  
REVIEWED BY: KNL 3/11/2025

SHEET TITLE

**AIRFIELD LIGHTING NOTES**

MAR 14, 2025 10:01 AM GROVE02594 1\23\JOBS\23A0001D\CAD\AIRPORT\SHEET\2025 CPS-5078 COMBINED SHEETS\IE-001-NOTES









**BI-STATE DEVELOPMENT**  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

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SHEET TITLE

TAXI GUIDANCE SIGN SCHEDULE - SHEET 2

**TAXI GUIDANCE SIGN SCHEDULE**

SIGN NUMBER	LOCATION	EXISTING		REMARKS
		SIDE A	SIDE B	
TGS-15	TAXIWAY B5 NORTH SIDE INTERSECTION WITH TAXIWAY B.			EXISTING SIGN TO BE RELOCATED. RECONNECT TO TAXIWAY B CIRCUIT 4. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KIT BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER.
TGS-16	TAXIWAY B EAST SIDE INTERSECTION WITH TAXIWAY B5.			EXISTING SIGN TO BE RELOCATED. RECONNECT TO TAXIWAY B CIRCUIT 4. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KITS BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER(S). THIS SIGN ARRAY HAS A 2 MODULE SIGN AND A 3 MODULE SIGN.
TGS-17	TAXIWAY B5 INTERSECTION WITH RUNWAY 30L-12R AT HOLD LINE.			EXISTING SIGN TO REMAIN IN PLACE. REWIRE SIGN TO BE POWERED FROM RUNWAY 12R-30L LIGHTING CIRCUIT. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KIT BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER.
TGS-17A	TAXIWAY B5 SOUTH SIDE INTERSECTION WITH TAXIWAY B.			EXISTING SIGN TO BE RELOCATED. RECONNECT TO TAXIWAY B CIRCUIT 4. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KIT BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER.
TGS-18	RUNWAY 30L INTERSECTION WITH TAXIWAY B5.			EXISTING SIGN TO REMAIN IN PLACE. REWIRE SIGN TO BE POWERED FROM RUNWAY 12R-30L LIGHTING CIRCUIT. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KIT BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER.
TGS-19	RUNWAY 12R INTERSECTION WITH TAXIWAY B5.			EXISTING SIGN TO REMAIN IN PLACE. REWIRE SIGN TO BE POWERED FROM RUNWAY 12R-30L LIGHTING CIRCUIT. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KIT BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER.
TGS-21	TAXIWAY B WEST SIDE INTERSECTION WITH TAXIWAY B5.			EXISTING SIGN TO BE RELOCATED. RECONNECT TO TAXIWAY B CIRCUIT 4. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KITS BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER(S). THIS SIGN ARRAY HAS A 2 MODULE SIGN AND A 3 MODULE SIGN.
TGS-23	TAXIWAY B5 INTERSECTION WITH RUNWAY 12L-30R AT HOLD LINE.			EXISTING SIGN TO REMAIN IN PLACE. REWIRE SIGN TO BE POWERED FROM RUNWAY 12L-30R LIGHTING CIRCUIT. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KIT BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER.
TGS-24	RUNWAY 30R INTERSECTION WITH TAXIWAY B5.			EXISTING SIGN TO REMAIN IN PLACE. REWIRE SIGN TO BE POWERED FROM RUNWAY 12L-30R LIGHTING CIRCUIT. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KIT BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER.
TGS-25	RUNWAY 12L INTERSECTION WITH TAXIWAY B5.			EXISTING SIGN TO REMAIN IN PLACE. REWIRE SIGN TO BE POWERED FROM RUNWAY 12L-30R LIGHTING CIRCUIT. PROVIDE NEW REPLACEMENT PANELS WITH LEGENDS TO MATCH EXISTING. PROVIDE LED UPGRADE KIT BY ORIGINAL EQUIPMENT MANUFACTURER. PROVIDE NEW FAA L-830-4 100W TRANSFORMER.

**NOTES:**

- THE TAXI GUIDANCE SIGNS IN THE SCHEDULE ARE EXISTING SIGNS SIZE 1, 18-IN. SIGN FACE WITH A 12-IN. LEGEND; STYLE 2, POWERED FROM A 4.8 TO 6.6 AMP SERIES LIGHTING CIRCUIT WITH QUARTZ OR INCANDESCENT LAMPS AND ARE MANUFACTURED BY LUMACURVE.
- PROVIDE TETHERS FOR EACH TAXI SIGN IN ACCORDANCE WITH FAA AC 150/5345-44L (OR LATEST ISSUE IN FORCE). SIGN TETHER ANCHOR HARD POINTS MUST BE PROVIDED ON ONE SIGN MOUNTING LEG ABOVE THE FRANGIBLE BREAKING POINT. TETHER ANCHOR HARD POINTS MUST BE PROVIDED SO THAT ONE END OF THE TETHER ATTACHES TO THE SIGN STRUCTURE, AND THE OTHER END ATTACHES BELOW THE FRANGIBLE POINT ON THE COUPLING TO EITHER ONE OF THE LEG MOUNTING BOLTS OR AN INDEPENDENT BOLT IN THE SIGN CONCRETE MOUNTING PAD. SIGNS THAT CONSIST OF MULTIPLE SEPARATE HOUSINGS (NOT CONNECTED TOGETHER IN A CONTINUOUS FRAME) MUST HAVE A MINIMUM OF ONE TETHER PER HOUSING. SIGNS THAT USE MULTIPLE MODULES CONNECTED TOGETHER IN A CONTINUOUS FRAME MUST USE A TETHER AT BOTH ENDS.
- PROVIDE A LEGEND PLATE/LABEL FOR EACH SIGN THAT NOTES THE RESPECTIVE POWER SOURCE. EXAMPLE: **"THIS SIGN IS CONNECTED TO LIGHTING CIRCUIT. CONFIRM AND DISCONNECT POWER SOURCE PRIOR TO WORKING ON THIS SIGN."** IDENTIFY THE RESPECTIVE AIRFIELD LIGHTING CIRCUIT FOR EACH SIGN. LOCATE ON SIGN ABOVE OR BELOW SIGN NUMBER LABEL.
- RUNWAY EXIT/TAXIWAY ENTRANCE SIGNS (TAXIWAY GUIDANCE SIGNS TO DEFINE THE THROAT OR ENTRANCE INTO THE INTERSECTING TAXIING ROUTE) OR RUNWAY EXIT/TAXIWAY ENTRANCE LIGHTS SHALL BE CONNECTED TO THE RESPECTIVE RUNWAY CIRCUIT TO BE ILLUMINATED WHEN THE RUNWAY EDGE LIGHTS ARE ON TO COMPLY WITH FAA AC 150/5340-18H, CHAPTER 1, PART 1.15 "SIGN OPERATION", AND/OR FAA AC 150/5340-30J PART 2.5.3.4.
- HOLDING POSITION SIGNS FOR RUNWAYS SHALL BE CONNECTED TO THE RESPECTIVE RUNWAY SERIES CIRCUIT TO BE ILLUMINATED WHEN THE ASSOCIATED RUNWAY LIGHTS ARE ILLUMINATED TO COMPLY WITH FAA AC150/5340-18H, CHAPTER 1, PART 1.15 "SIGN OPERATION".
- CONCRETE STEEL REINFORCEMENT SHALL BE TYPE ASTM A615 OR A706 GRADE 60 WELDED STEEL WIRE FABRIC SHALL CONFORM TO AASHTO M55 OR AASHTO M221. ALL REINFORCEMENT SHALL HAVE A 3" MINIMUM CONCRETE COVER. REINFORCEMENT MAY BE ADJUSTED TO MISS INTERFERENCES. CONCRETE SHALL CONFORM TO ITEM P-610 CONCRETE FOR MISCELLANEOUS STRUCTURES.
- SEE SPECIFICATION ITEM L-125 FOR ADDITIONAL REQUIREMENTS ON TAXI GUIDANCE SIGNS.
- SEE "AIRFIELD LIGHTING NOTES" SHEET FOR ADDITIONAL REQUIREMENT ON TAXI GUIDANCE SIGNS.
- CONTRACTOR SHALL TEST AND RECORD THE EARTH GROUND RESISTANCE FOR THE GROUND ROD AT EACH AIRFIELD LIGHT FIXTURE AND EACH TAXI GUIDANCE SIGN.
- FAA AC 150/5340-26C, PART 3.6.6 USE OF ORIGINAL EQUIPMENT MANUFACTURER (OEM) PART. NOTES THE FOLLOWING: "THE USE OF NON-OEM PARTS OR LAMPS IN FAA APPROVED EQUIPMENT IS STRONGLY DISCOURAGED. THE FAA HAS STRICT SPECIFICATIONS FOR APPROVAL OF ALL AIRPORT LIGHTING EQUIPMENT AND USE OF NON-OEM PARTS OR LAMPS IN SUCH EQUIPMENT OR SYSTEMS CAN RENDER THE EQUIPMENT TO BE FUNCTIONALLY NON-FAA APPROVED. THIS COULD POSSIBLY LEAD TO SERIOUS LIABILITY CONSEQUENCES IN CASE OF AN AIRCRAFT INCIDENT AT AN AIRPORT FOLLOWING THESE PRACTICES."

**TAXI GUIDANCE SIGN LEGEND**

TYPE L-858L(L) LOCATION SIGN - YELLOW LEGEND AND BORDER ON A BLACK BACKGROUND

TYPE L-858R(L) MANDATORY INSTRUCTION SIGN - BLACK OUTLINE ON OUTSIDE EDGE OF WHITE LEGEND ON A RED BACKGROUND

TYPE L-858Y(L) DIRECTION, DESTINATION, AND BOUNDARY SIGN - BLACK LEGEND ON A YELLOW BACKGROUND

BLANK - BLACK BACKGROUND

\* COORDINATE SIGN NUMBERING WITH AIRPORT DIRECTOR/MANAGER. EACH TAXI SIGN SHALL HAVE A TAG WITH ID NUMBER; 3" HIGH PERMANENT WHITE REFLECTIVE LETTERING/NUMBERING LOCATED ON THE EDGE OF THE SIGN.

**FOR BID**



**ST. LOUIS DOWNTOWN AIRPORT**

**BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

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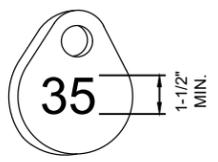
SHEET TITLE

**AIRFIELD LIGHT DETAILS**

A LIGHT BASE GROUND SHALL BE INSTALLED AT EACH STAKE MOUNTED LIGHT AND EACH TRANSFORMER BASE/LIGHT CAN ASSOCIATED WITH RUNWAY LIGHTS, TAXIWAY LIGHTS, RUNWAY DISTANCE REMAINING SIGNS, AND LIGHTED RUNWAY/TAXI GUIDANCE SIGNS. THE LIGHT BASE GROUND SHALL BE A #6 AWG BARE COPPER CONDUCTOR CONNECTED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE AND A 3/4-INCH DIAMETER BY 10-FEET LONG (MINIMUM) UL LISTED COPPER CLAD GROUND ROD.

**NOTES:**

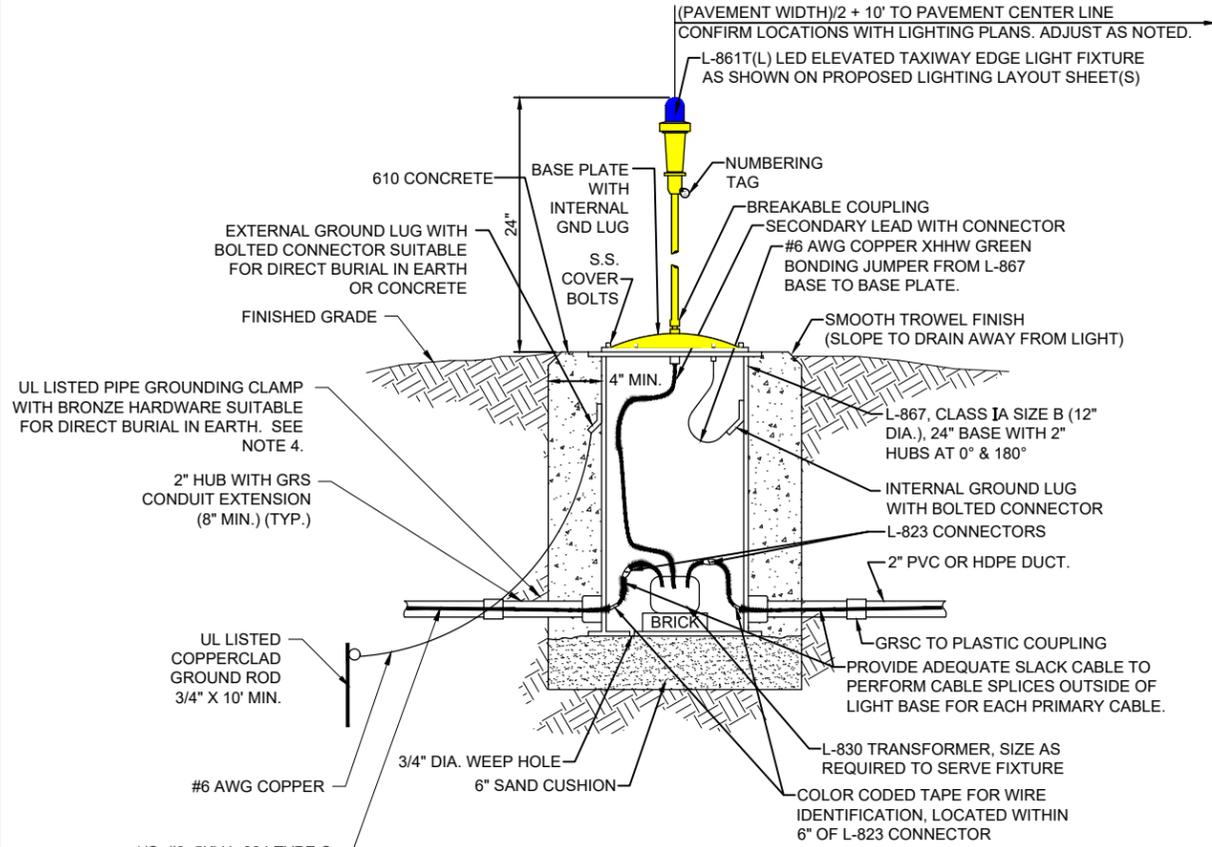
- SEE ELECTRICAL NOTES SHEETS.
- SEE "ELECTRICAL NOTES SHEET 2" AND "GROUNDING NOTES" SHEET FOR GROUNDING NOTES FOR AIRFIELD LIGHTING.
- SEE PROPOSED LIGHTING LAYOUT SHEET(S) FOR LIGHT LOCATIONS
- WHERE GROUND LUGS ARE NOT ACCESSIBLE ON BASE CANS, PROVIDE A UL LISTED PIPE GROUND CLAMP RATED FOR DIRECT BURIAL IN EARTH AND BOND TO THE METAL CONDUIT EXTENSION TO PROVIDE GROUND PATH TO LIGHT BASE.
- THE PROPOSED AIRFIELD LIGHT FIXTURES SHALL CONFORM TO ADVISORY CIRCULAR 150/5345-46 (CURRENT ISSUE(S) IN EFFECT) AND BE FAA APPROVED FOR TYPE L-861T(L) FOR TAXIWAY EDGE LIGHTS. AIRFIELD LIGHT FIXTURES SHALL HAVE LED (LIGHT EMITTING DIODE) ILLUMINATION AND SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF FAA ENGINEERING BRIEF NO. 67D LIGHT SOURCES OTHER THAN INCANDESCENT AND XENON FOR AIRPORT AND OBSTRUCTION LIGHTING FIXTURES.
- LIGHT BASE CANS FOR THE AIRFIELD LIGHT FIXTURES SHALL CONFORM TO THE REQUIREMENTS OF FAA AC 150/5345-42 (CURRENT ISSUE IN EFFECT), FOR TYPE L-867, CLASS IA, SIZE B (12 IN. NOMINAL DIAMETER) OR SIZE D (16 IN. NOMINAL DIAMETER), AND 24 IN. DEEP AND/OR AS DETAILED ON THE PLANS. EACH LIGHT BASE CAN SHALL INCLUDE INTERNAL AND EXTERNAL GROUND LUGS TO ACCOMMODATE THE RESPECTIVE APPLICATIONS. LIGHT BASE PLATES SHALL BE SIZED AND COMPATIBLE WITH THE RESPECTIVE LIGHT BASES AND LIGHT FIXTURES WITH STAINLESS STEEL BOLTS.
- PRIOR TO INSTALLING THE AIRFIELD LIGHT FIXTURES, APPLY AN OXIDE-INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS, BREAKABLE COUPLING, AND ALL PLACES WHERE METAL COMES INTO CONTACT WITH METAL.
- SERIES CIRCUIT ISOLATION TRANSFORMERS FOR THE AIRFIELD LIGHTING SHALL BE MANUFACTURED TO FAA SPECIFICATION AC 150/5345-47, (CURRENT EDITION IN EFFECT), AND SHALL BE FAA-APPROVED (ETL/INTERTEK TESTING SERVICES-CERTIFIED). SERIES CIRCUIT TRANSFORMER SHALL BE PROPERLY SIZED FOR THE RESPECTIVE AIRFIELD LIGHTING DEVICE, AND SHALL BE AS RECOMMENDED BY THE RESPECTIVE EQUIPMENT MANUFACTURER. CONFIRM PROPER TRANSFORMER SELECTION AND SIZING WITH THE RESPECTIVE EQUIPMENT MANUFACTURER.
- THE CONCRETE USED IN THE CONSTRUCTION OF THE BASES FOR THE AIRFIELD LIGHTING AND SPLICE CANS SHALL BE IN ACCORDANCE WITH ITEM 610 CONCRETE FOR MISCELLANEOUS STRUCTURES.
- IDENTIFICATION TAGS SHALL BE ATTACHED TO EACH AIRFIELD LIGHT FIXTURE.
- PER ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS ITEM 108, ITEM 125, AND FAA AC 150/5370-10H ITEM L-108 AND L-125, RUBBER AND PLASTIC ELECTRICAL TAPES SHALL BE SCOTCH ELECTRICAL TAPE NUMBERS 130C LINERLESS RUBBER SPLICING TAPE (2" WIDE) AND 88 (1.5" WIDE) RESPECTIVELY, AS MANUFACTURED THE MINNESOTA MINING AND MANUFACTURING COMPANY, OR EQUIVALENT.



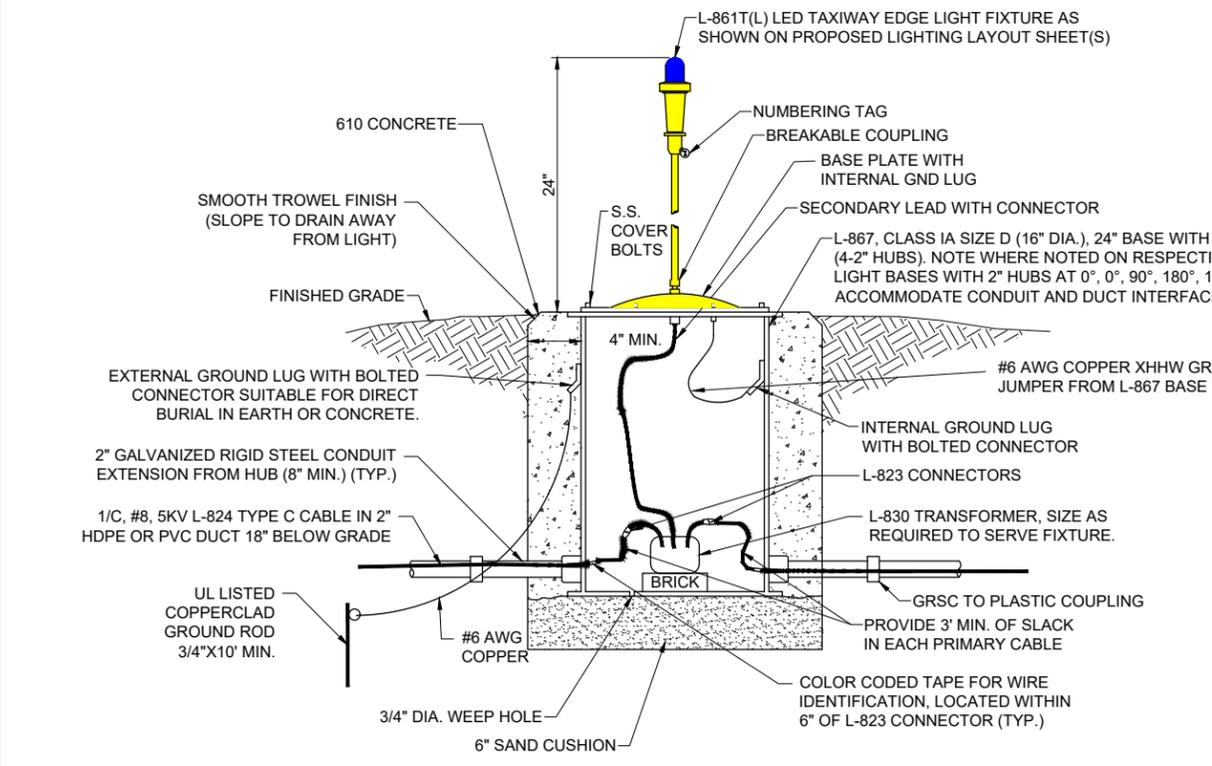
**NOTE:**

AFFIX NON-CORROSIVE, NON-BREAKABLE, TAG TO FIXTURE FACING RUNWAY/TAXIWAY WITH SET SCREW, WIRE TIE, OR METAL BAND. NUMERALS SHALL BE ENGRAVED FOR PERMANENT READABILITY. STAINLESS STEEL OR BRASS TAGS WITH 1/2" HIGH STAMPED LETTERING WILL ALSO BE ACCEPTABLE.

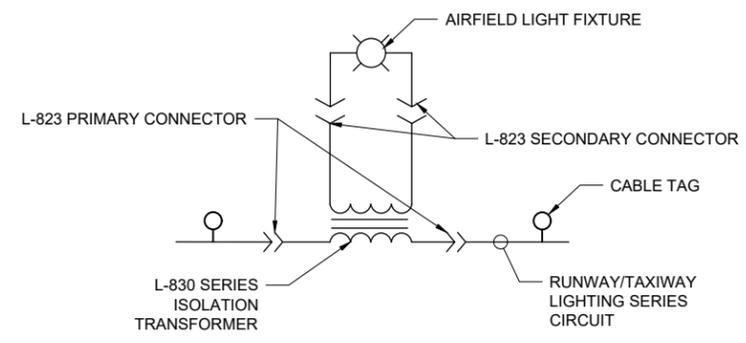
**NUMBERING TAG DETAIL**  
(NOT TO SCALE)



**MEDIUM INTENSITY ELEVATED AIRFIELD LIGHT - BASE MOUNTED**  
(NOT TO SCALE)



**MEDIUM INTENSITY TAXIWAY EDGE LIGHT - BASE MOUNTED WITH L-867D CAN**  
"NOT TO SCALE"



**LIGHTING CONNECTION SCHEMATIC**  
NOT TO SCALE

MAR 14, 2025 10:02 AM GROVE02594 1:23:JOBS023A0001D\CAD\AIRPORT\SHEET\0205 CPS-5078 COMBINED SHEETS\IE-501-DETL

**FOR BID**



**ST. LOUIS DOWNTOWN AIRPORT**

BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
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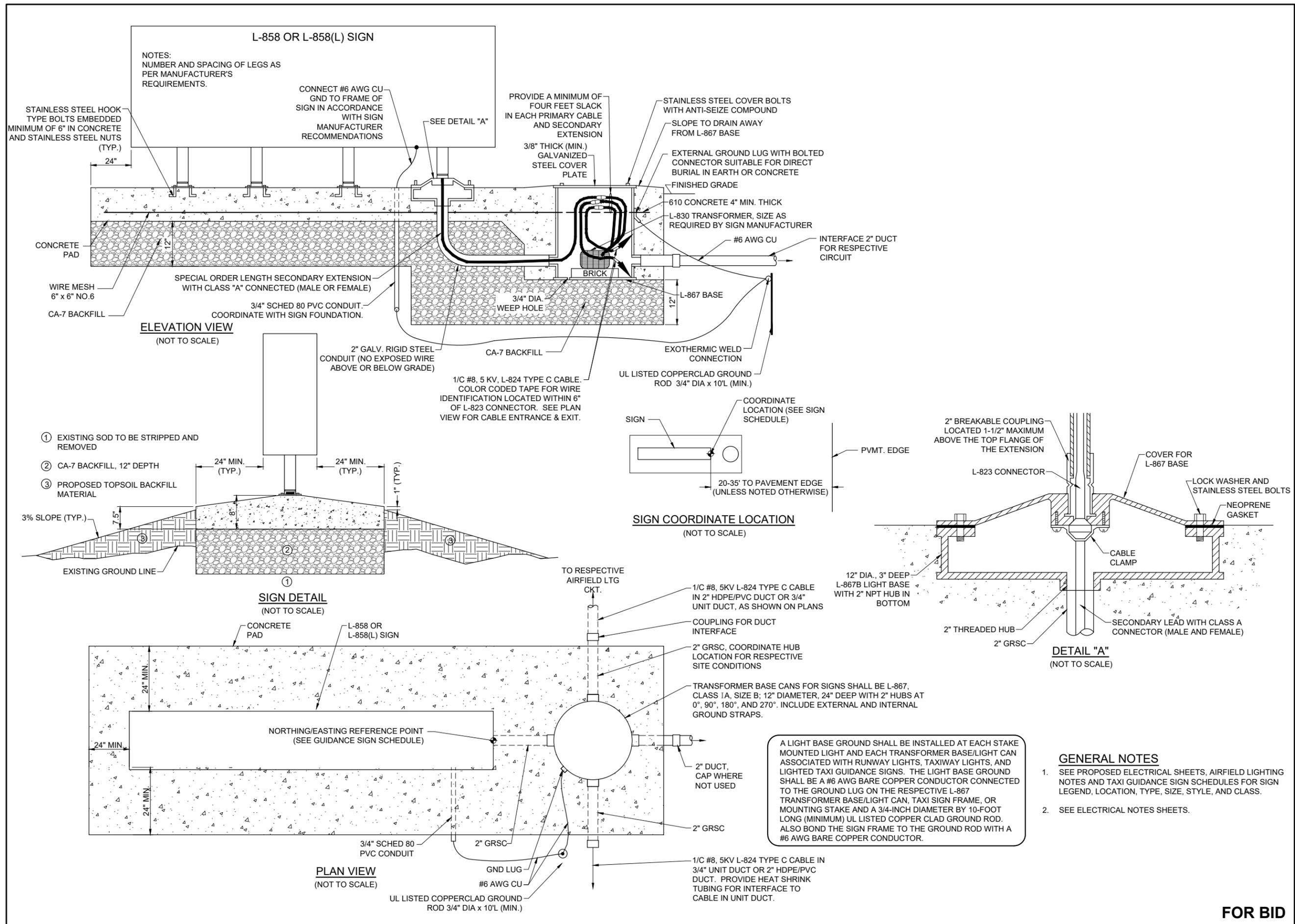
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SHEET TITLE

TAXI GUIDANCE SIGN  
DETAILS - SHEET 1



MAR 14, 2025 10:02 AM GROVE02594  
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**ST. LOUIS DOWNTOWN AIRPORT**

**BI-STATE DEVELOPMENT ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

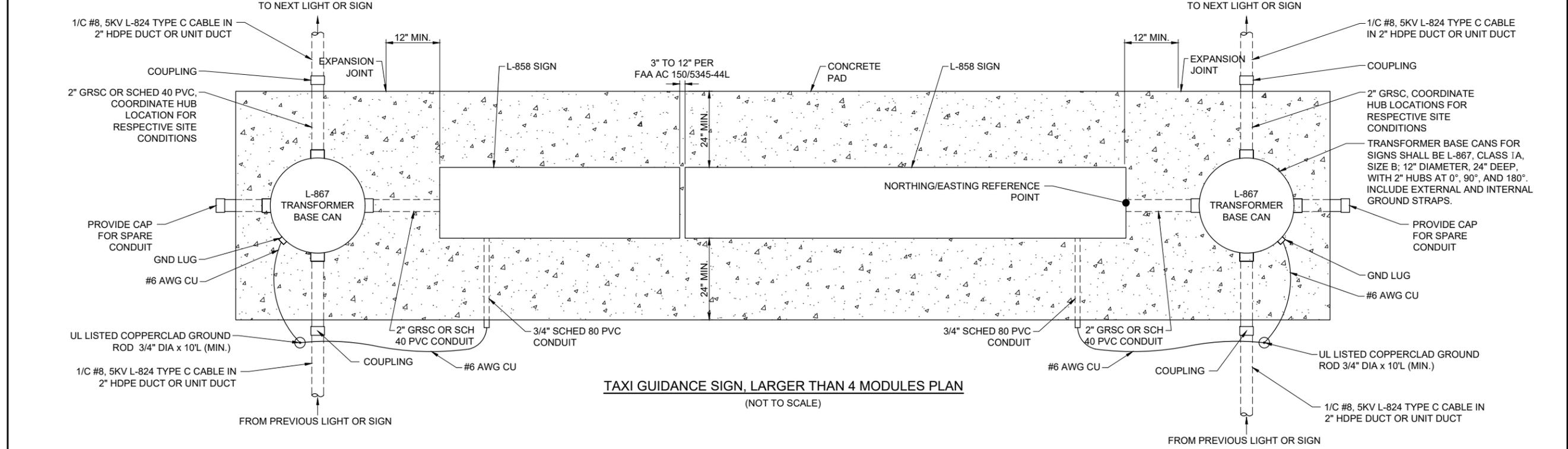
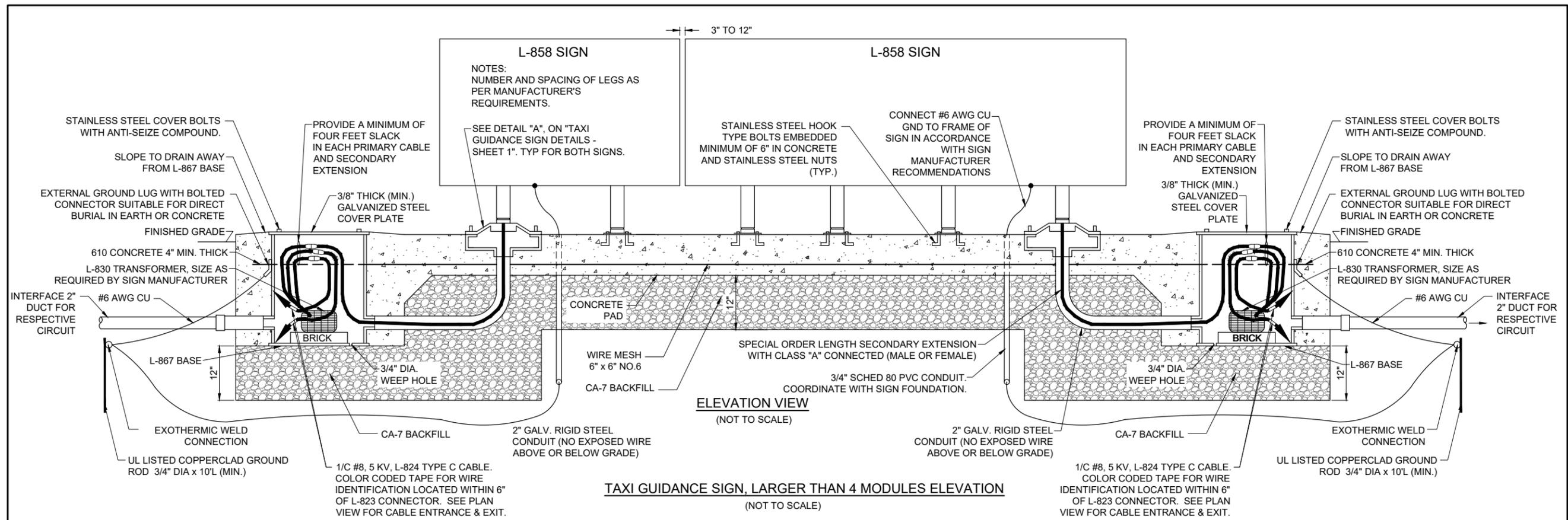
IDA NO.: CPS-5078 CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION	
		DES	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: E-503-DETL.DWG  
DESIGN BY: KNL 3/5/2025  
DRAWN BY: CWS 3/7/2025  
REVIEWED BY: KNL 3/11/2025

SHEET TITLE

TAXI GUIDANCE SIGN DETAILS - SHEET 2



**GENERAL NOTES**

- SEE PROPOSED ELECTRICAL SHEETS, AIRFIELD LIGHTING NOTES AND TAXI GUIDANCE SIGN SCHEDULES FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND CLASS.
- SEE ELECTRICAL NOTES SHEETS.

A LIGHT BASE GROUND SHALL BE INSTALLED AT EACH STAKE MOUNTED LIGHT AND EACH TRANSFORMER BASE/LIGHT CAN ASSOCIATED WITH RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS. THE LIGHT BASE GROUND SHALL BE A #2 AWG BARE COPPER CONDUCTOR CONNECTED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN, TAXI SIGN FRAME, OR MOUNTING STAKE AND A 3/4-INCH DIAMETER BY 10-FOOT LONG (MINIMUM) UL LISTED COPPER CLAD GROUND ROD.

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MAR 14, 2025 10:02 AM GROVE02594  
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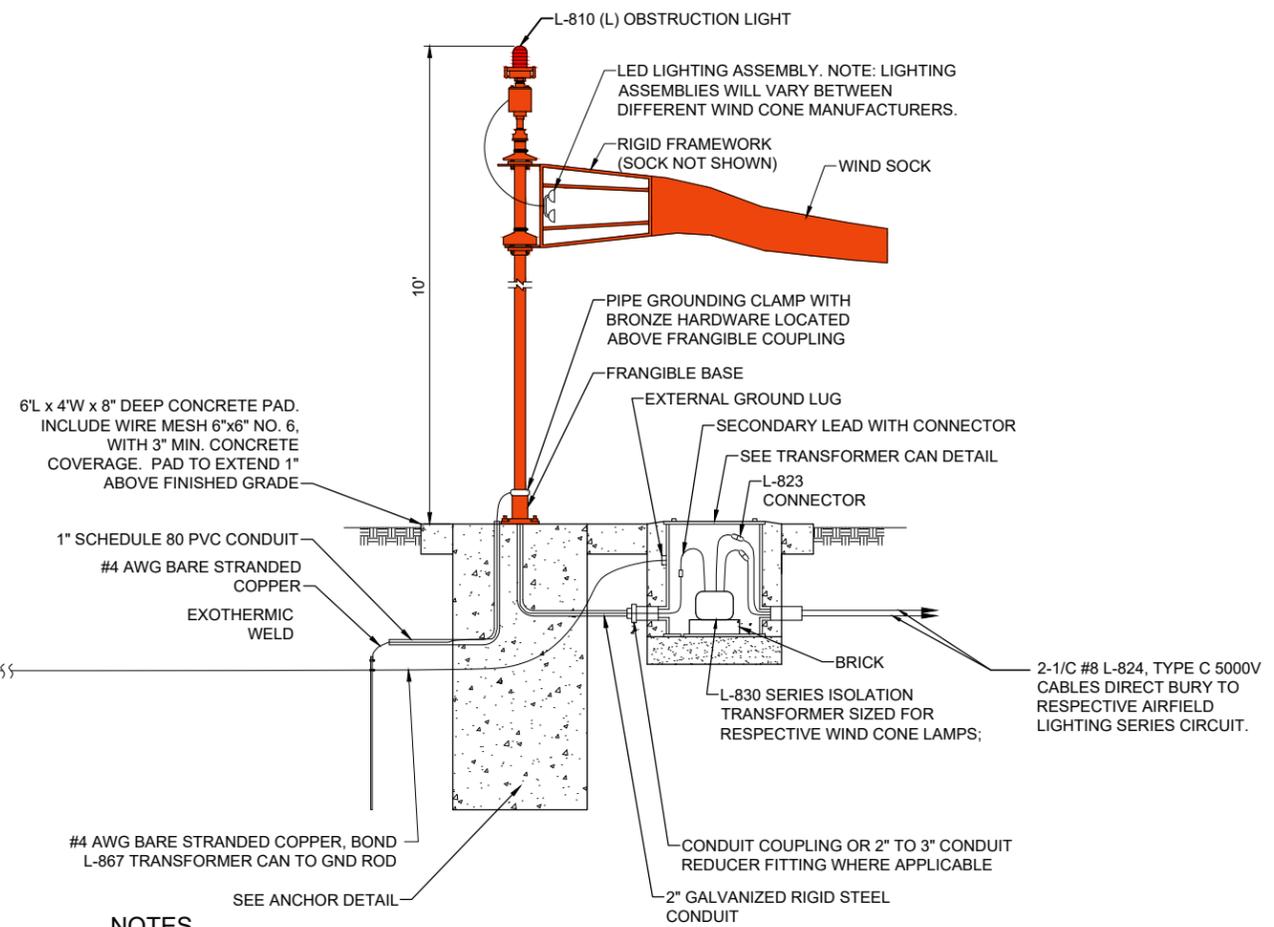
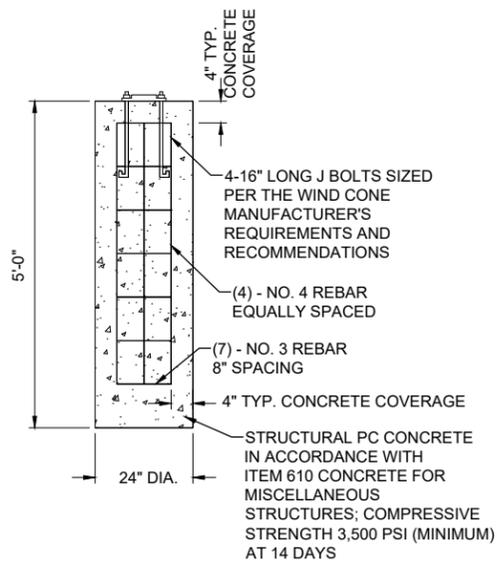
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SHEET TITLE

L-806 WIND CONE  
DETAILS



3/4" X 10' LONG UL LISTED COPPERCLAD GROUND ROD (TYP. FOR 2). GROUND RODS SHALL BE SPACED NOT LESS THAN ONE ROD LENGTH APART

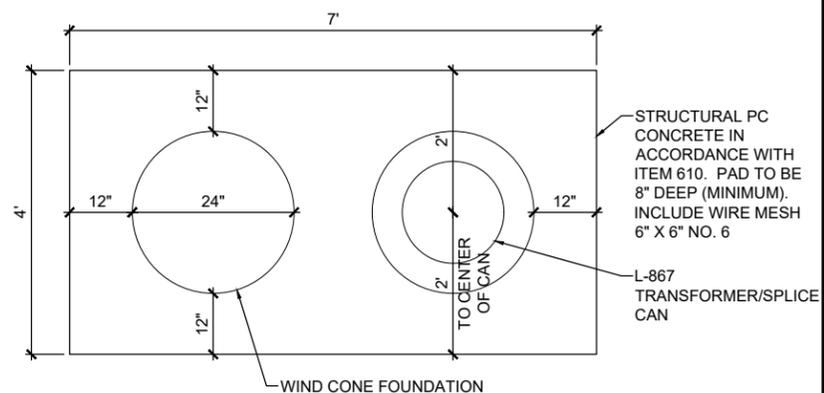
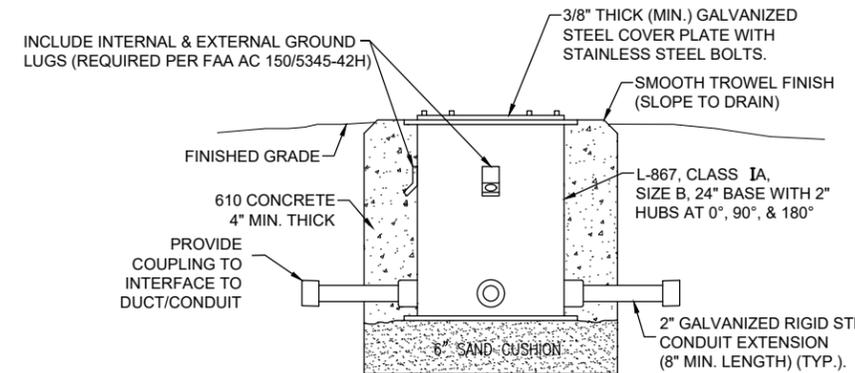
6L x 4W x 8" DEEP CONCRETE PAD. INCLUDE WIRE MESH 6"x6" NO. 6, WITH 3" MIN. CONCRETE COVERAGE. PAD TO EXTEND 1" ABOVE FINISHED GRADE

1" SCHEDULE 80 PVC CONDUIT  
#4 AWG BARE STRANDED COPPER  
EXOTHERMIC WELD

#4 AWG BARE STRANDED COPPER, BOND L-867 TRANSFORMER CAN TO GND ROD

**NOTES**

- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE OWNER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, INSTALLING, OR RECONNECTING THE RESPECTIVE AIRFIELD LIGHTING, NAVAID, OR OTHER DEVICE.
- SUPPLEMENTAL WIND CONES SHALL BE FAA APPROVED TYPE L-806(L) WITH LIGHT EMITTING DIODE ILLUMINATION, STYLE I-B (INTERNALLY LIGHTED), SIZE 1 (18-INCH DIAMETER BY 8 FEET LONG), AND SUITABLE FOR 6.6 AMP SERIES CIRCUIT POWER. WIND CONES SHALL INCLUDE CONSTANT-BRIGHTNESS SERIES CIRCUIT POWER ADAPTER. SEE SPECIAL PROVISION SPECS.
- THE RESPECTIVE RUNWAY LIGHTING CIRCUIT IS POWERED BY AN L-828, CLASS 1 - 6.6 AMP OUTPUT CURRENT, STYLE 2; 5 BRIGHTNESS STEPS CONSTANT CURRENT REGULATOR. COORDINATE WITH THE RESPECTIVE WIND CONE MANUFACTURER TO PROVIDE A COMPATIBLE AND PROPERLY SIZED SERIES ISOLATION TRANSFORMER FOR EACH WIND CONE.
- SUPPLEMENTAL L-806 WIND CONES WILL BE PAID FOR UNDER ITEM AR107508 L-806 WC 8" INTERNALLY LIT PER EACH. SPLICE/TRANSFORMER CANS FOR WIND CONE SERIES CIRCUIT TRANSFORMERS WILL BE INCIDENTAL TO THE RESPECTIVE WIND CONE PAY ITEM.
- REBAR SHALL CONFORM TO THE REQUIREMENTS OF ASTM A706 GRADE 60 OR ASTM A615 GRADE 6 AND SHALL BE MANUFACTURED FROM 100% DOMESTIC STEEL. WELDED WIRE FABRIC SHALL CONFORM TO AASHTO M55, ASTM A82, OR ASTM A185 AND SHALL BE MANUFACTURED FROM 100% DOMESTIC STEEL.
- FOR EACH GROUNDING ELECTRODE SYSTEM (GROUND ROD) THE CONTRACTOR SHALL TEST THE MADE ELECTRODE GROUNDING SYSTEM WITH A INSTRUMENT THAT IS SPECIFICALLY DESIGNED FOR TESTING GROUNDING SYSTEMS. TEST RESULTS SHALL BE RECORDED FOR EACH GROUNDING ELECTRODE SYSTEM. IF GROUND RESISTANCE EXCEEDS 25 OHMS, CONTACT THE PROJECT ENGINEER FOR FURTHER DIRECTION. COPIES OF THE GROUND SYSTEM TEST RESULTS SHALL BE FURNISHED TO THE PROJECT ENGINEER OF RECORD.
- RESTORE TURF AREAS AFFECTED BY WIND CONE INSTALLATION.



**INTERNALLY LIGHTED L806 WIND CONE (SERIES CIRCUIT TYPE)**

"NOT TO SCALE"

**FOR BID**

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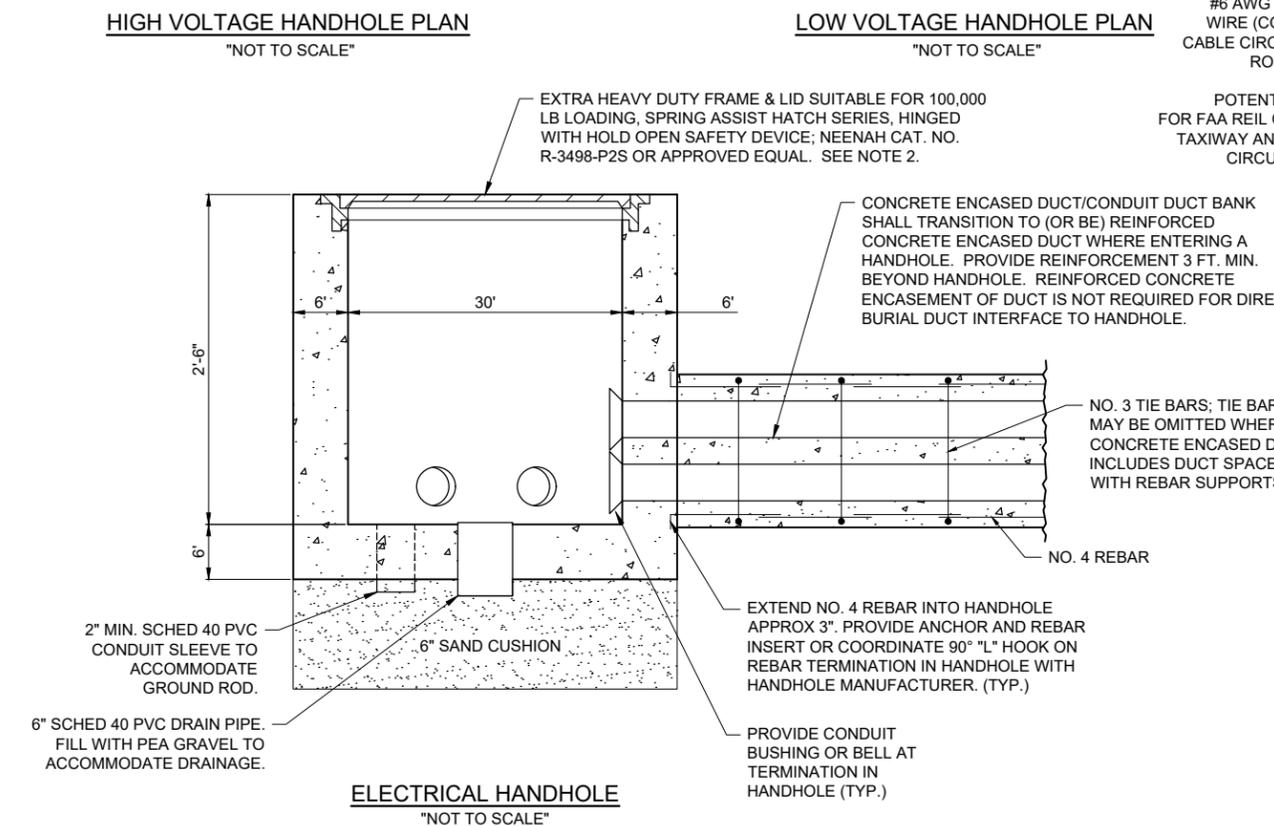
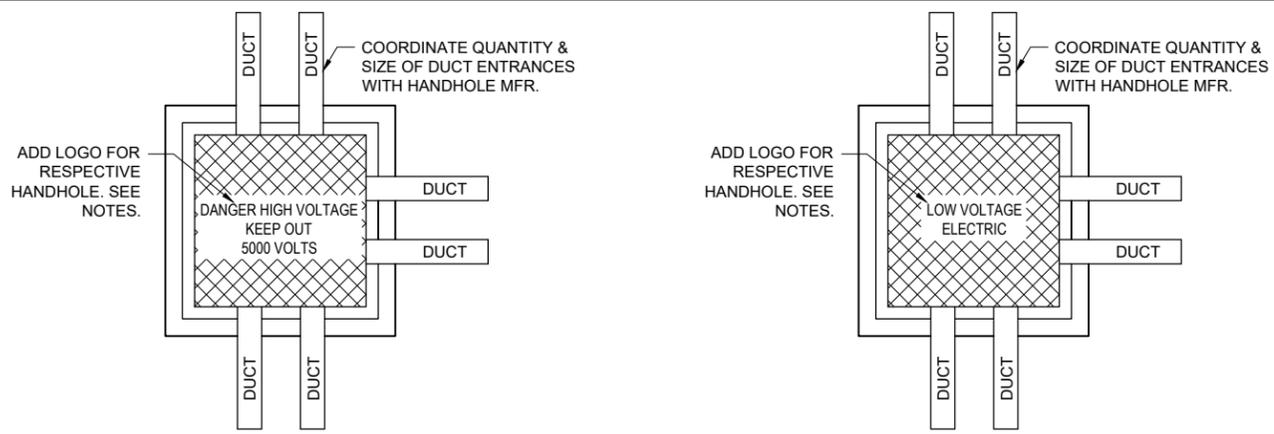
IDA NO.: CPS-5078  
CONTRACT NO.: SD064

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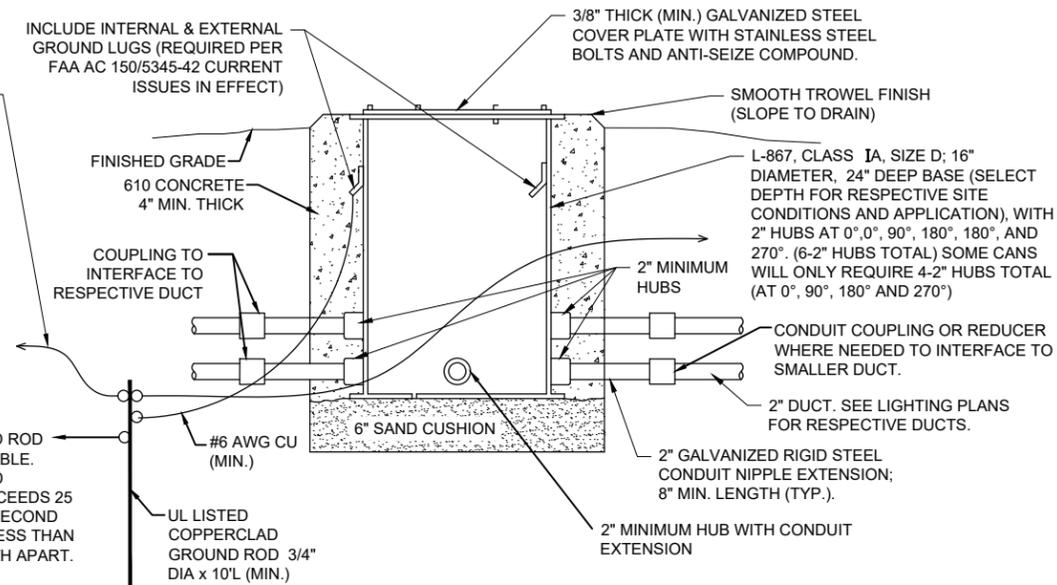
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SHEET TITLE

HANDHOLE AND SPLICE CAN DETAILS



#6 AWG BARE SOLID COPPER GUARD WIRE (COUNTERPOISE) FOR FAA REIL CABLE CIRCUITS. CONNECT TO GROUND ROD AT EACH SPLICE/JUNCTION CAN TO MAINTAIN EQUAL POTENTIAL COUNTERPOISE SYSTEM FOR FAA REIL CABLES. NOT REQUIRED FOR TAXIWAY AND RUNWAY LIGHTING SERIES CIRCUITS THAT DO NOT INCLUDE A COUNTERPOISE.



**NOTES FOR SPLICE CAN/JUNCTION CAN DETAIL:**

- SPLICE CANS SHALL CONFORM TO THE REQUIREMENTS OF FAA AC 150/5345-42 (CURRENT ISSUES IN EFFECT), FOR TYPE L-867, CLASS IA, SIZE D, (16 IN. NOMINAL DIAMETER), AND 24 IN. DEEP AND/OR AS DETAILED ON THE PLANS. EACH SPLICE CAN SHALL INCLUDE INTERNAL AND EXTERNAL GROUND LUGS TO ACCOMMODATE THE RESPECTIVE APPLICATIONS. SPLICE CANS AND/OR JUNCTION CANS SHALL HAVE GALVANIZED STEEL COVERS, 3/8-INCH THICK (MINIMUM), WITH STAINLESS STEEL BOLTS.
- FOR THE PURPOSE OF ENHANCING SAFETY, EACH BASE MUST HAVE INSTALLED, BY THE MANUFACTURER, AN INTERNAL AND EXTERNAL GROUND STRAP THAT IS AVAILABLE FOR THE PURPOSE OF ATTACHING A GROUND LUG THAT IS CONNECTED TO AN EARTH GROUND OR A SAFETY GROUND CONDUCTOR INSTALLED WITH THE RESPECTIVE CIRCUIT. FOR AIRPORT PROJECTS RECEIVING FEDERAL FUNDS THIS REQUIREMENT IS MANDATORY PER FAA AC 150/5345-42 (CURRENT ISSUES IN EFFECT).
- APPLY AN OXIDE-INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS, AND ALL PLACES WHERE METAL COMES INTO CONTACT WITH METAL.
- THE CONCRETE USED IN THE CONSTRUCTION OF THE BASES FOR THE AIRFIELD LIGHTING CANS SHALL BE IN ACCORDANCE WITH ITEM 610 STRUCTURAL PORTLAND CEMENT CONCRETE.
- LIDS FOR THE SPLICE CANS CONTAINING HIGH VOLTAGE AIRFIELD LIGHTING CABLES SHALL INCLUDE MINIMUM 1/2-INCH HIGH LETTERING LABELED "DANGER HIGH VOLTAGE KEEP OUT" TO COMPLY WITH 2020 NEC ARTICLE 300.45 "DANGER SIGNS", 2023 NEC ARTICLE 305.12 "DANGER SIGNS", AND 2020/2023 NEC ARTICLE 314.72(E) "SUITABLE COVERS". THIS WILL NEED TO BE COORDINATED WITH THE SPLICE CAN MANUFACTURER.
- LIDS FOR THE SPLICE CANS CONTAINING LOW VOLTAGE CABLES (RATED 600 VOLTS AND BELOW) WILL BE ACCEPTABLE TO USE BLANK COVERS.

**HANDHOLE NOTES:**

- LIDS FOR LOW VOLTAGE HANDHOLES (CONTAINING CIRCUITS RATED 600 VOLTS AND BELOW) SHALL BE LABELED "LOW VOLTAGE" OR "0V - 600V ELECTRIC". LIDS FOR HIGH VOLTAGE HANDHOLES CONTAINING AIRFIELD LIGHTING SERIES CIRCUIT WIRING SHALL BE LABELED "DANGER HIGH VOLTAGE KEEP OUT 5000 VOLTS" TO COMPLY WITH 2020 NEC ARTICLE 300.45 "DANGER SIGNS", 2023 NEC ARTICLE 305.12 "DANGER SIGNS", AND 2020/2023 NEC ARTICLE 314.30(D) "COVERS". COORDINATE LETTERING WITH MFR. HANDHOLES PROVIDED WITH THE WRONG LIDS SHALL HAVE THE LIDS REPLACED WITH THE CORRECT LIDS AT NO ADDITIONAL COST TO THE CONTRACT.
- ELECTRICAL HANDHOLE, FRAME & LID SHALL BE CAPABLE OF WITHSTANDING MINIMUM 100,000 POUND LOADS AS CALLED FOR IN FAA ADVISORY CIRCULAR AC 150/5320-6G APPENDIX B, ITEM B.2.4 DIRECT LOADING, 1.A. AIRPORT HANDHOLE FRAME & LID SHALL BE NEENAH CATALOG NO. R-3498-P2S, EAST JORDAN IRON WORKS CAT NO. 8096 OR APPROVED EQUAL.
- REINFORCEMENT SHALL BE #6 BARS AT 6" CENTERS BASE & WALLS EACH WAY.
- CONCRETE SHALL BE 5000 PSI AT 28 DAYS.
- HANDHOLES SHALL BE PRECAST. PRECAST MANUFACTURER MUST BE ON THE IDOT (ILLINOIS DEPARTMENT OF TRANSPORTATION) APPROVED LIST OF CERTIFIED PRECAST CONCRETE PRODUCERS.
- FRAMES AND LIDS (CASTINGS) SHALL BE MADE IN THE USA TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN PREFERENCES REQUIREMENTS.
- COORDINATE INSTALLATION OF HANDHOLES WITH RESPECTIVE FINISHED GRADE ELEVATION.
- ALL CORING, INTERFACE, AND LABOR ASSOCIATED WITH CONDUIT, DUCT, CABLE IN UNIT DUCT, AND/OR CABLE ENTRIES WILL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE HANDHOLE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- HANDHOLES WITH SIMILAR DIMENSIONS MEETING STRENGTH AND LOADING REQUIREMENTS WILL BE CONSIDERED.

**FOR BID**

MAR 14, 2025 10:02 AM GROVE02594 I:\23\JOBS\23A0001D\CAD\AIRPORT\1\SHEET\2025 CPS-5078 COMBINED SHEETS\E-504-DETL



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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3:  
CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

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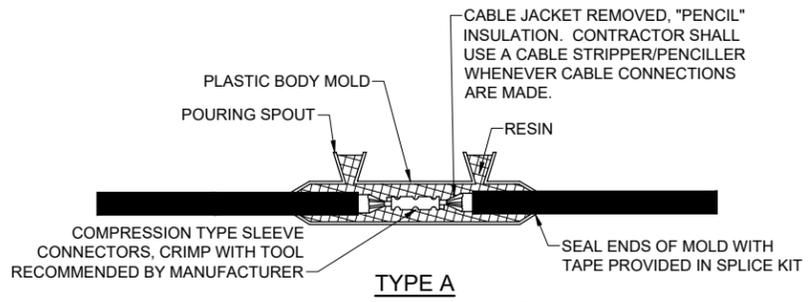
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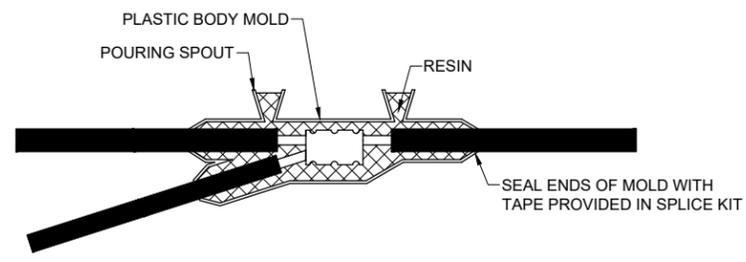
**AIRFIELD LIGHTING  
CABLE SPLICE  
DETAILS**

**NOTES:**

- SPLICE DETAILS ARE PROVIDED FOR NEW WORK AND TO ASSIST IN REPAIRS OF ACCIDENTAL OR UNEXPECTED INTERRUPTIONS AND/OR CUTS TO AIRFIELD LIGHTING CABLES.
- KEEP ON HAND A MINIMUM OF 10 SETS OF SPLICE KITS FOR L-823 CONNECTORS AND A MINIMUM OF 10 SETS OF TYPE A LOW VOLTAGE SPLICE KITS TO ACCOMMODATE REPAIRS.
- EVERY AIRFIELD LIGHTING CABLE SPICER SHALL BE QUALIFIED IN MAKING CABLE SPLICES AND TERMINATIONS ON CABLES RATED AT AND/OR ABOVE 5,000 VOLTS AC TO COMPLY WITH THE REQUIREMENTS OF FAA AC 150/5370-10H ITEM L-108.
- INSIDE DIAMETER OF RESPECTIVE CABLE CONNECTOR SHALL PROPERLY MATCH OUTSIDE DIAMETER OF CABLE.
- WHEN PREPARING CABLE FOR SPLICES, THE CONTRACTOR SHALL USE A CABLE STRIPPER/PENCILLER WHENEVER CABLE CONNECTIONS ARE MADE.
- WRAP ALL PRIMARY AND SECONDARY POWER CONNECTIONS WITH SUFFICIENT LAYERS OF HIGH VOLTAGE ELECTRICAL INSULATING TAPE (RUBBER SPLICING TAPE SUITABLE FOR PRIMARY ELECTRICAL INSULATION FOR SPLICING CABLE FROM 600 VOLTS TO 69,000 VOLTS) AND COVER WITH VINYL ELECTRICAL TAPE (ALL-WEATHER VINYL INSULATING TAPE SUITABLE FOR PROTECTIVE JACKETING FOR HIGH-VOLTAGE CABLE SPLICES AND REPAIRS) FOR FULL VALUE OF CABLE INSULATION VOLTAGE. PER ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS ITEM 108, ITEM 125, AND FAA AC 150/5370-10H ITEM L-108 AND L-125, HIGH VOLTAGE ELECTRICAL INSULATING TAPE SHALL BE 3M SCOTCH 130C LINERLESS RUBBER SPLICING TAPE (2 INCHES WIDE) OR APPROVED EQUIVALENT, AND VINYL ELECTRICAL TAPE SHALL BE 3M SCOTCH 88 (1.5 INCHES WIDE) OR APPROVED EQUIVALENT. TAPES MUST BE RATED SUITABLE FOR THE APPLICATION.
- PROVIDE CABLE TAGS TO IDENTIFY THE RESPECTIVE CIRCUITS ALL POINTS OF ACCESS INCLUDING L-867 BASES, L-868 BASES, HANDHOLES, MANHOLES, JUNCTION BOXES, AND WIREWAYS.
- CONNECTION OF CONDUCTORS MUST BE MADE BY USING CRIMP CONNECTORS AND A CRIMPING TOOL APPROVED BY THE CONNECTOR/LUG MANUFACTURER. THE TOOL MUST PRODUCE A COMPLETE CRIMP BEFORE IT CAN BE REMOVED. FOR THE L-823 CONNECTORS, THE CRIMPING TOOL USED MUST BE LISTED BY THE L-823 KIT MANUFACTURER. MAKE THE NUMBER AND TYPE OF CRIMPS PER THE KIT MANUFACTURER'S INSTRUCTIONS.

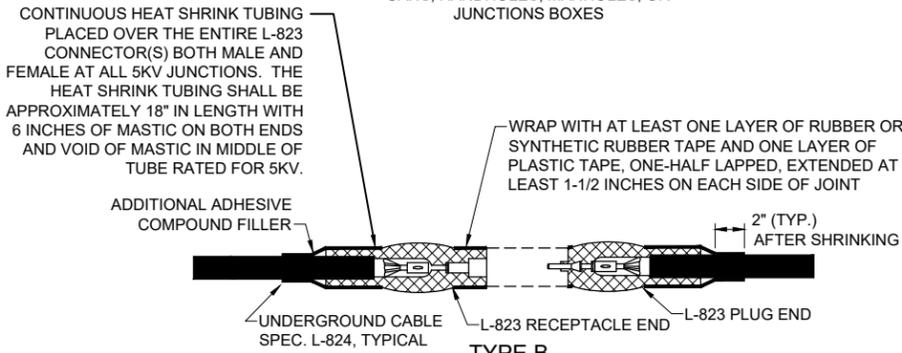


FOR SPLICES IN LOW VOLTAGE CABLE (600V) HOMERUNS FOR EXTENSIONS TO EXISTING LOW VOLTAGE CABLES ONLY. TYPE A SPLICES SHALL BE MADE IN SPLICE CANS, HANDHOLES, MANHOLES, OR JUNCTION BOXES

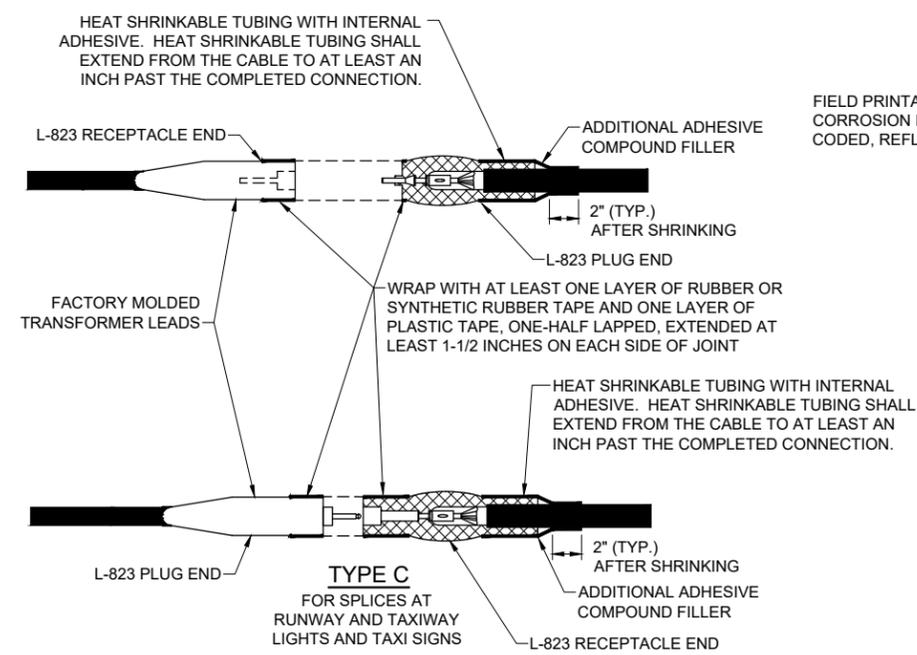


**LOW VOLTAGE UNDERGROUND TAP SPLICE**

FOR TAP SPLICES IN LOW VOLTAGE (600V) CABLE. SPLICES SHALL BE RATED AND LISTED SUITABLE FOR DIRECT BURIAL LOCATIONS. FOR SPLICES UP TO #2 AWG CONDUCTOR, SPLICES SHALL BE WYE RESIN TYPE POWER CABLE TAP SPLICE KIT SUITABLE FOR THE RESPECTIVE CABLES AND RESPECTIVE APPLICATION.

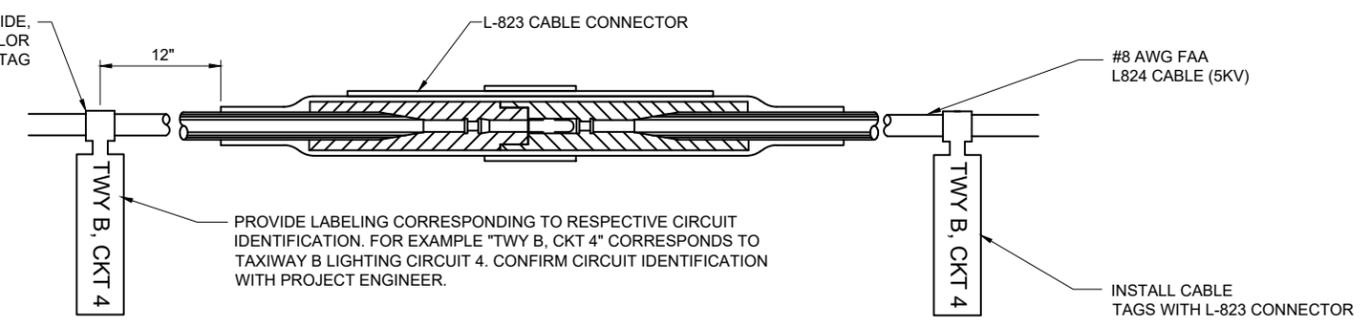


FOR SPLICES AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT AND FOR SPLICES IN HOMERUNS TO EXISTING CABLES



NOTES:  
INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.

**CABLE SPLICES**  
"NOT TO SCALE"



- CONTRACTOR SHALL PROVIDE CABLE CIRCUIT IDENTIFICATION MARKERS ATTACHED TO BOTH SIDES OF EACH CABLE CONNECTION.
- CABLE IDENTIFICATION TAGS SHALL BE FIELD PRINTABLE, DOUBLE SIDE, CORROSION RESISTANT, COLOR CODED, REFLECTIVE CABLE TAGS SUITABLE FOR THE RESPECTIVE ENVIRONMENT.
- THE CABLE SHALL THOROUGHLY BE CLEANED PRIOR TO THE INSTALLATION OF THE L-823 CONNECTOR KIT.
- ATTACH EACH CABLE TIE ENOUGH TO HOLD IN PLACE WITHOUT COMPRESSING EDGE OF CABLE TAG INTO CONDUCTOR. TRIM OFF EXCESS CABLE TIE.
- CABLE TAGS SHALL BE PROVIDED AT ALL POINTS OF ACCESS INCLUDING L-867 JUNCTION/SPLICE CANS, L-868 JUNCTION/SPLICE CANS, HANDHOLES, MANHOLES, JUNCTION BOXES, AND WIREWAYS.
- CABLE TAGS SHALL BE LABELED AS FOLLOWS FOR RESPECTIVE AIRFIELD LIGHTING CIRCUITS,  
RUNWAY 12R-30L LIGHTING: RWY 12R-30L  
RUNWAY 12L-30R LIGHTING: RWY 12L-30R  
RUNWAY 5-23 LIGHTING: RWY 5-23  
TAXIWAY A LIGHTING: TWY A  
TAXIWAY A6 & RUNUP AREA LIGHTING: TWY B, CKT 4  
TAXIWAY B CKT 1 LIGHTING: TWY B, CKT 1  
TAXIWAY B CKT 2 LIGHTING: TWY B, CKT 2  
TAXIWAY B CKT 3 LIGHTING: TWY B, CKT 3  
TAXIWAY B CKT 4 LIGHTING: TWY B, CKT 4  
TAXIWAY C LIGHTING: TWY C





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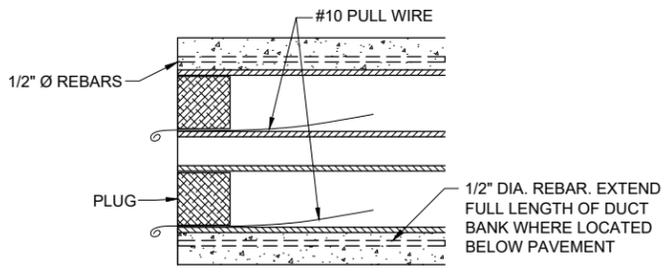
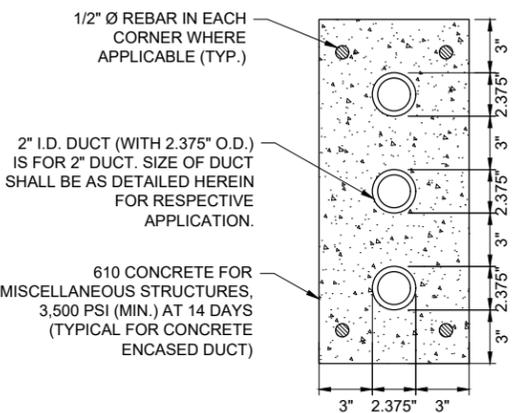
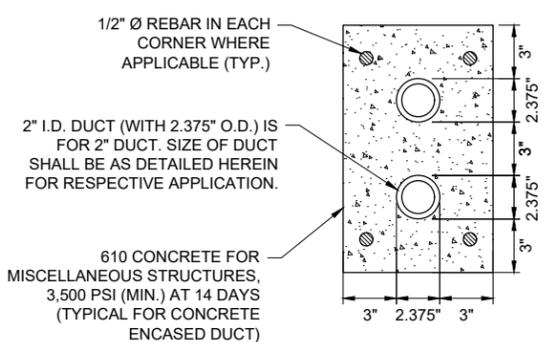
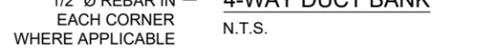
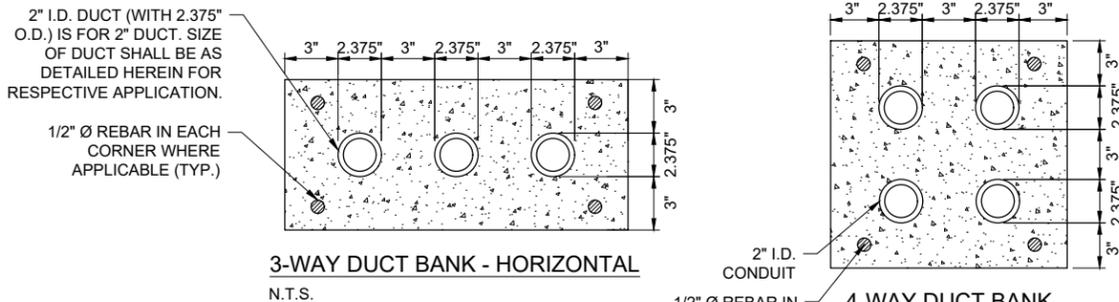

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SHEET TITLE

DUCT BANK DETAILS  
AND NOTES



**DUCT INSTALLATION NOTES**

- DIMENSIONS FOR CONCRETE COVERAGE AND SEPARATION BETWEEN DUCTS ARE MINIMUM.
- INCLUDE DUCT SPACERS AS MANUFACTURED BY UNDERGROUND DEVICES INC., CARLON, OR APPROVED EQUAL TO MAINTAIN PROPER SEPARATION OF CONDUITS.
- PROVIDE REBAR WHERE APPLICABLE TO ACCOMMODATE INTERFACE OF CONCRETE ENCASED DUCT BANKS TERMINATING IN HANDHOLE. PROVIDE REBAR REINFORCEMENT WHERE DUCT BANK IS LOCATED BELOW PAVEMENT. REBAR SHALL CONFORM TO THE REQUIREMENTS OF ASTM A706, GRADE 60, OR ASTM A615, GRADE 60.

**DUCT INSTALLATION NOTES**

- ALL ELECTRICAL EQUIPMENT AND MATERIALS SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE. THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, INTERTEK TESTING SERVICES VERIFICATION/ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
- CONTRACTOR SHALL KEEP A COPY OF THE LATEST NEC IN FORCE ON SITE AT ALL TIMES DURING CONSTRUCTION FOR USE AS A REFERENCE.
- CONTRACTOR SHALL COORDINATE WORK AND ANY POWER OUTAGES AND/OR SHUT DOWN OF SYSTEMS WITH THE RESPECTIVE FACILITY OWNER PERSONNEL AND THE AIRPORT MANAGER/DIRECTOR. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY INFORMATION FOR EXCAVATORS) FOR INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.
- ADJUSTMENTS TO DUCT BANK ROUTES MIGHT BE REQUIRED TO ACCOMMODATE EXISTING SITE CONDITIONS AND UNDERGROUND LINES AND UTILITIES. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL COORDINATE DUCT ROUTE ADJUSTMENTS WITH THE RESIDENT PROJECT REPRESENTATIVE AND THE AIRPORT MANAGER.
- CONTRACTOR SHALL LOCATE AND MARK ALL EXISTING CABLES, LINES, OR UTILITIES WITHIN 10 FT OF PROPOSED EXCAVATING/TRENCHING AREA. ANY CABLES, LINES, AND UTILITIES FOUND INTERFERING WITH PROPOSED EXCAVATION OR CABLE/TRENCHING SHALL BE HAND DUG AND EXPOSED. ANY DAMAGED CABLES OR OTHER UTILITIES SHALL BE IMMEDIATELY REPAIRED TO THE SATISFACTION OF THE RESIDENT PROJECT REPRESENTATIVE AT THE CONTRACTOR'S EXPENSE. THE RESIDENT PROJECT REPRESENTATIVE AND OWNER SHALL BE NOTIFIED IMMEDIATELY IF ANY CABLES OR OTHER UTILITIES ARE DAMAGED.
- PAYMENT FOR LOCATING AND MARKING UNDERGROUND UTILITIES AND CABLES WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE DUCT INSTALLATION.
- THE CONTRACTOR WILL DETERMINE IF THERE IS A CONFLICT BETWEEN THE INSTALLATION OF THE PROPOSED ELECTRICAL DUCTS AND ANY EXISTING UTILITIES. HE WILL MAKE ALL NECESSARY ADJUSTMENTS IN DEPTH OF INSTALLATION TO AVOID ANY AND ALL PROPOSED UNDERGROUND IMPROVEMENTS.
- CONDUITS FOR DIRECT BURIAL OR CONCRETE ENCASED DUCT BANK SHALL BE SCHEDULE 40 (MINIMUM) PVC CONDUIT, UL-LISTED, RATED FOR 90°C CABLE-CONFORMING TO NEMA STANDARD TC-2 AND UL 651, LISTED SUITABLE FOR UNDERGROUND USE EITHER DIRECT-BURIED OR ENCASED IN CONCRETE, OR SCHEDULE 40 (MINIMUM) HDPE CONDUIT, UL LISTED, CONFORMING TO NEMA STANDARD TC-7 AND UL 651B AND LISTED SUITABLE FOR UNDERGROUND USE; EITHER DIRECT BURY OR ENCASED IN CONCRETE.
- CONDUITS FOR DIRECTIONAL BORING SHALL BE SCHEDULE 40 PVC CONDUIT OR SCHEDULE 80 PVC CONDUIT, UL-LISTED, RATED FOR 90°C CABLE-CONFORMING TO NEMA STANDARD TC-2 AND UL 651 AND SUITABLE FOR DIRECTIONAL BORING INSTALLATION, SCHEDULE 80 HDPE CONDUIT, UL-LISTED, CONFORMING TO NEMA STANDARD TC-7 AND UL 651B AND SUITABLE FOR DIRECTIONAL BORING INSTALLATION, OR WALL TYPE SDR 13.5 OR SDR 11 HDPE CONDUIT MANUFACTURED IN ACCORDANCE WITH ASTM D-3350 (SPECIFICATION OF POLYETHYLENE PLASTICS PIPE AND FITTINGS MATERIALS) AND ASTM F2160 (STANDARD SPECIFICATION FOR SOLID WALL, HIGH-DENSITY POLYETHYLENE CONDUIT BASED ON CONTROLLED OUTSIDE DIAMETER), AND SUITABLE FOR DIRECTIONAL BORING INSTALLATION. PER NEC 300.5 (K), RACEWAYS INSTALLED USING DIRECTIONAL BORING EQUIPMENT SHALL BE APPROVED FOR THE PURPOSE.
- INSTALLATION OF CONDUIT AND DUCTS SHALL CONFORM TO ITEM 110 AIRPORT UNDERGROUND ELECTRICAL DUCT BANKS AND CONDUITS.
- DUCTS INSTALLED IN TRENCH SHALL BE INSTALLED 18 IN. MINIMUM BELOW GRADE IN TURF AREAS NOT SUBJECT TO FARMING. DUCTS LOCATED IN AREAS SUBJECT TO FARMING SHALL BE 42 IN. MINIMUM BELOW GRADE. MINIMUM DEPTH OF TOP OF DUCT ENCASEMENT SHALL BE 24" IN AREAS UNDER AIRFIELD PAVEMENTS. WHERE DETAILED ON THE PLANS OR WHERE REQUIRED TO AVOID OBSTRUCTIONS, DUCTS SHALL BE BURIED DEEPER.
- WHERE CONCRETE-ENCASED DUCT INTERFACES TO AN ELECTRICAL HANDHOLE OR MANHOLE, THE CONCRETE ENCASEMENT SHALL BE INSTALLED UP TO THE RESPECTIVE HANDHOLE OR MANHOLE. PROVIDE BUSHINGS OR BELLS AT CONDUIT TERMINATIONS IN ELECTRICAL HANDHOLES OR MANHOLES.
- UNDERGROUND DUCTS INSTALLED BY DIRECTIONAL-BORING METHOD SHALL BE INSTALLED IN A MANNER THAT WILL NOT DAMAGE ANY EXISTING UNDERGROUND UTILITIES, AND SHALL NOT DISTURB OR DAMAGE THE RESPECTIVE PAVEMENT OR ROADWAY SURFACE. DUCTS SHALL BE DIRECTIONAL-BORED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS. THE DUCTS WILL BE BORED AT A MINIMUM DEPTH OF 42 IN. BELOW THE RESPECTIVE PAVEMENT IT IS BEING BORED UNDER.
- A PULL WIRE SHALL BE INSTALLED IN EACH CONDUIT OR DUCT TO BE LEFT VACANT.
- HIGH VOLTAGE CIRCUITS (AIRFIELD LIGHTING 5000 VOLT SERIES CIRCUITS AND/OR OTHER CIRCUITS RATED ABOVE 600 VOLTS) AND LOW VOLTAGE CIRCUITS (RATED 600 VOLTS AND BELOW) SHALL NOT BE INSTALLED IN THE SAME RACEWAY, CONDUIT, DUCT, HANDHOLE, OR MANHOLE.
- CONTROL CABLES SHALL BE RUN IN SEPARATE DUCTS FROM POWER CABLES.
- HOMERUN CABLES FOR A RESPECTIVE CIRCUIT SHALL BE INSTALLED IN THE SAME RACEWAY OR DUCT.
- COORDINATE DUCT INTERFACE TO MANHOLES AND HANDHOLES. FIELD CUT OPENINGS FOR CONDUITS AND DUCTS TO INTERFACE TO MANHOLES AND/OR HANDHOLES. CUT WALL OF RESPECTIVE HANDHOLE OR MANHOLE WITH A TOOL DESIGNED FOR MATERIAL TO BE CUT. SIZE HOLES FOR RESPECTIVE DUCTS, CONDUITS, AND TERMINATION FITTINGS AND SEAL AROUND PENETRATIONS. ALL CORING, INTERFACE, CUTTING, AND SEALING WILL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE DUCT INSTALLATION AND/OR RESPECTIVE HANDHOLE/MANHOLE INSTALLATION.
- CONTRACTOR SHALL COORDINATE DUCT MARKING WITH AIRPORT.
- ALL POWER AND CONTROL CABLES IN HANDHOLES, MANHOLES, AND JUNCTION BOXES SHALL BE TAGGED TO IDENTIFY THE RESPECTIVE CABLE. A MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A MANHOLE; ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT. CABLE TAGS SHALL BE STAMPED BRASS TAGS OR OTHER WEATHERPROOF/WATERPROOF CORROSION RESISTANT MATERIAL.



**ST. LOUIS DOWNTOWN AIRPORT**

BI-STATE DEVELOPMENT  
**ST. LOUIS DOWNTOWN AIRPORT**  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

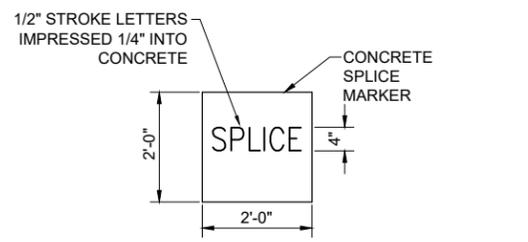
IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
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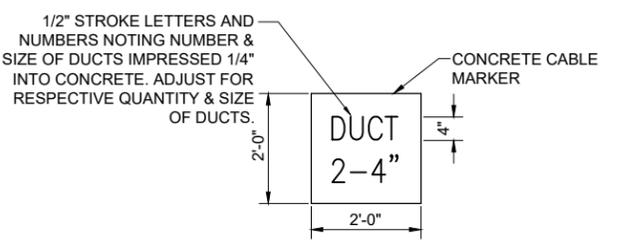
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PROJECT NO: 23A0001D  
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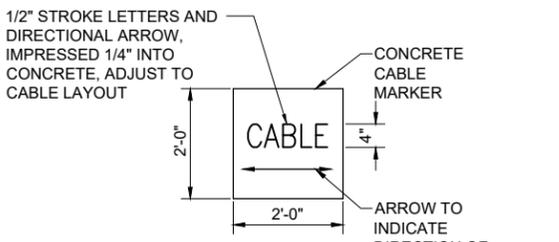
CABLE AND DUCT MARKER DETAILS



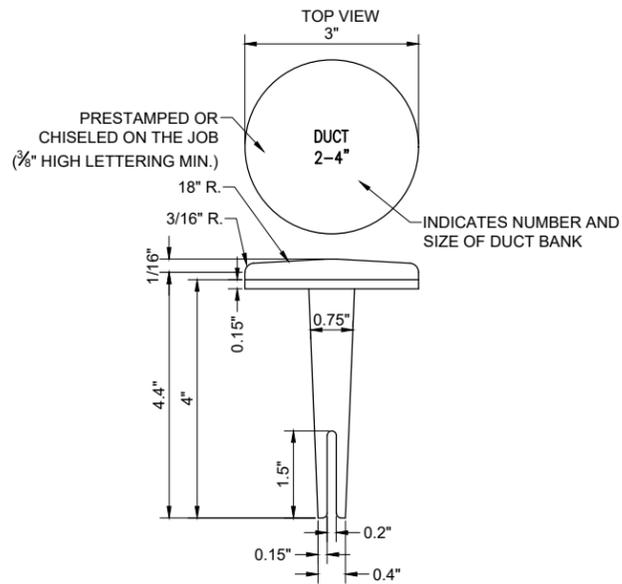
**TURF SPLICE MARKERS**  
"NOT TO SCALE"



**TURF DUCT MARKERS**  
"NOT TO SCALE"

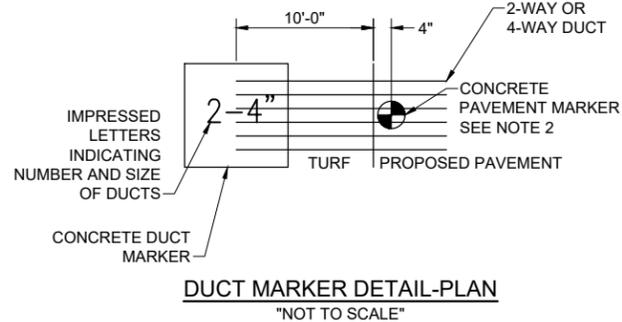


**TURF CABLE MARKERS**  
"NOT TO SCALE"

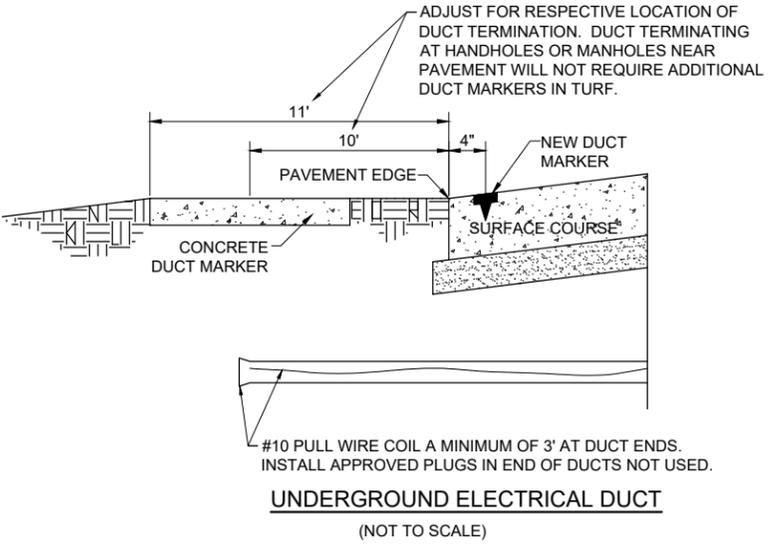


**BITUMINOUS PAVEMENT DUCT MARKERS**  
"NOT TO SCALE"

- NOTE:**
- TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE. MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH EPOXY GLUE
  - BRASS DUCT MARKERS ARE AVAILABLE FROM BERNTSEN INTERNATIONAL INC., P.O. BOX 8670, MADISON, WI. 53708-8670, PHONE: 1-877-959-8556, SURV-KAP, 3225 E. 47TH ST., TUCSON, AZ 85713, PHONE: (502)-622-6011, OR OTHER EQUIVALENT MANUFACTURERS.



**DUCT MARKER DETAIL-PLAN**  
"NOT TO SCALE"



**UNDERGROUND ELECTRICAL DUCT**  
(NOT TO SCALE)

**CABLE & DUCT MARKER NOTES:**

- THE COST OF ALL TURF AND PAVEMENT DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT. THE COST OF ALL CABLE MARKERS SHALL BE INCIDENTAL TO THE CABLE.
- BITUMINOUS PAVEMENT DUCT MARKER AND CONCRETE DUCT MARKER TO BE PROVIDED AT EACH END OF EACH DUCT AS SHOWN ON THE LOCATION PLAN. FOR CONCRETE PAVEMENT, THE LETTER "D" SHALL BE IMPRESSED IN THE PAVEMENT INSTEAD OF THE MARKER. THE LETTER SHALL BE INFORMED AS DESCRIBED IN NOTE 4.
- UNDERGROUND CABLE RUNS MUST BE IDENTIFIED BY CABLE MARKERS AT 200 FEET (61 M) MAXIMUM SPACING WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS MUST BE INSTALLED ABOVE THE CABLE. CABLE MARKERS ARE NOT REQUIRED FOR CABLE RUNS BETWEEN RUNWAY/TAXIWAY EDGE LIGHTS.
- CONCRETE CABLE MARKERS AND DUCT MARKERS SHALL HAVE LETTERS 4" HIGH, 3" WIDE WITH WIDTH OF STROKE 1/2" AND 1/4" DEEP. ALL LETTERS, NUMBERS AND ARROWS TO BE IMPRESSED.
- EMPLOY THE FOLLOWING METHODS WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED:
  - REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
  - INCREASE THE MARKER SIZE TO 30" X 30".
  - PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE
- TURF DUCT MARKERS ARE NOT REQUIRED AT PAVEMENT CROSSINGS WHERE DUCTS TERMINATE IN HANDHOLES, OR JUNCTION STRUCTURES.
- LOCATION OF ALL DIRECT EARTH BURIAL UNDERGROUND CABLE SPLICE/CONNECTIONS, EXCEPT THOSE AT ISOLATION TRANSFORMERS, MUST BE IDENTIFIED BY SPLICE MARKERS. SPLICE MARKERS MUST BE PLACED ABOVE THE SPLICE/CONNECTIONS. DIRECT EARTH BURIAL UNDERGROUND CABLE SPLICES SHALL BE AVOIDED WHERE POSSIBLE. CABLE SPLICES SHALL BE LOCATED IN SPLICE CANS, LIGHT BASES, HANDHOLES, MANHOLES, OR OTHER JUNCTION STRUCTURES UNLESS OTHERWISE APPROVED BY THE PROJECT ENGINEER OF RECORD.
- THE CABLE AND SPLICE MARKERS MUST IDENTIFY THE CIRCUITS TO WHICH THE CABLES BELONG. FOR EXAMPLE: TWY A, TWY B.
- LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS MUST BE IDENTIFIED BY DUCT MARKERS.









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DOWNTOWN AIRPORT**

**BI-STATE DEVELOPMENT  
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6100 Archview Drive  
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COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025

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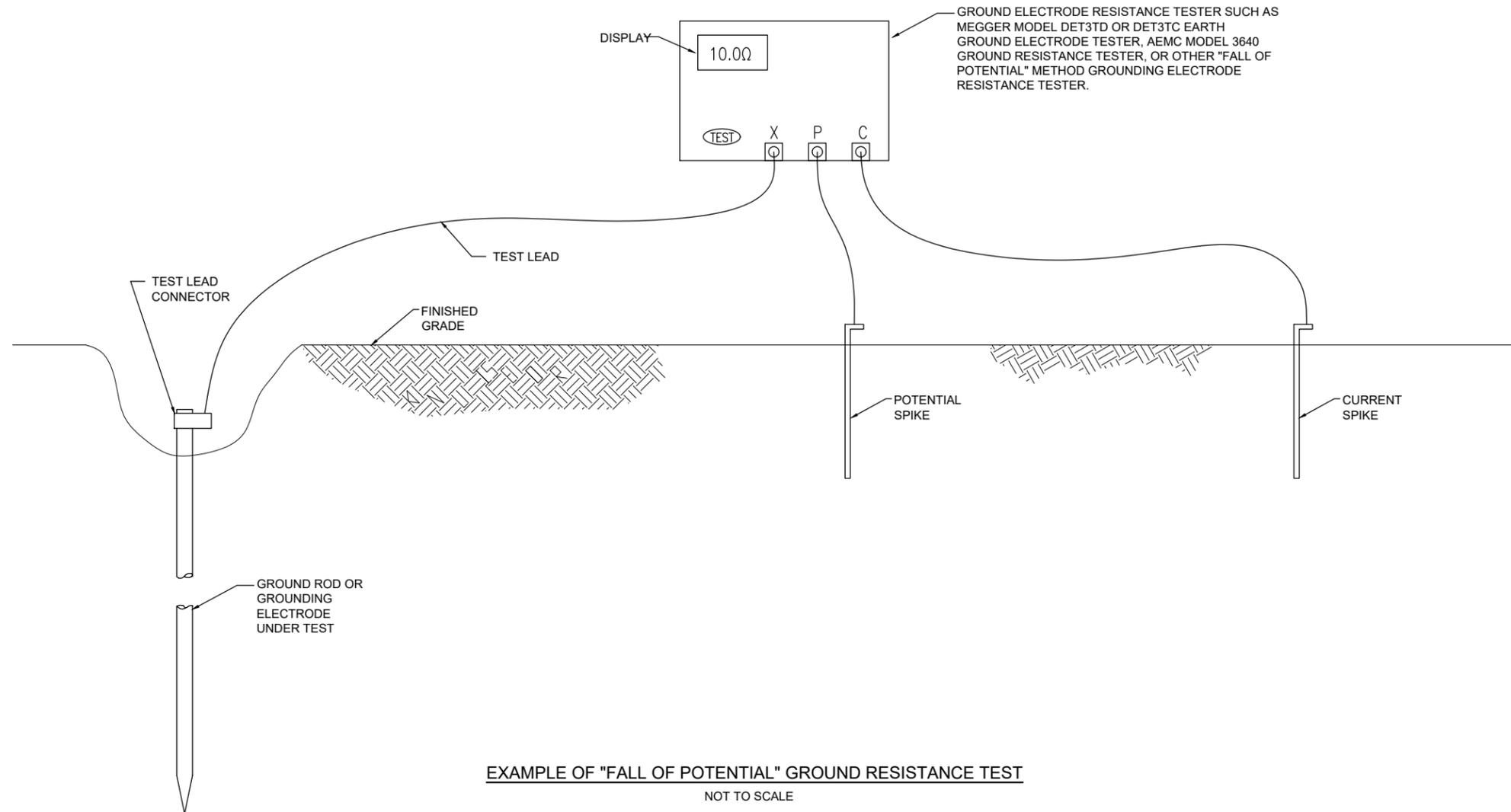
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SHEET TITLE

**GROUND  
RESISTANCE  
TESTING DETAILS**



**EXAMPLE OF "FALL OF POTENTIAL" GROUND RESISTANCE TEST**

NOT TO SCALE

**NOTES**

- CONTRACTOR SHALL TEST AND RECORD THE RESISTANCE FOR EACH MADE ELECTRODE GROUND ROD/GROUND FIELD/GROUND RING WITH AN INSTRUMENT SPECIFICALLY DESIGNED FOR TESTING GROUNDING ELECTRODE SYSTEMS. IF GROUND RESISTANCE EXCEEDS 25 OHMS, CONTACT THE PROJECT ENGINEER OF RECORD; KEVIN LIGHTFOOT FOR FURTHER DIRECTIONS. COPIES OF GROUND ROD TEST RESULTS SHALL BE FURNISHED TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN, AND THE PROJECT ENGINEER OF RECORD.
- FOR EACH AIRFIELD LIGHT FIXTURE, TAXI GUIDANCE SIGN, SPLICE CAN AND NAVAID THE CONTRACTOR SHALL TEST THE MADE ELECTRODE GROUND SYSTEM WITH AN INSTRUMENT SPECIFICALLY DESIGNED FOR TESTING GROUND SYSTEMS. TEST RESULTS SHALL BE RECORDED FOR EACH AIRFIELD LIGHT FIXTURE, TAXI GUIDANCE SIGN, AND NAVAIDS INSTALLATION. IF GROUND RESISTANCE EXCEEDS 25 OHMS, CONTACT THE PROJECT ENGINEER OF RECORD FOR FURTHER DIRECTIONS. ALSO REFER TO EOR-062-047643 FOR ADDITIONAL INFORMATION ON GROUNDING REQUIREMENTS WHERE APPLICABLE. COPIES OF THE GROUND SYSTEM TEST RESULTS SHALL BE FURNISHED TO THE RESIDENT ENGINEER / RESIDENT TECHNICIAN, AND THE PROJECT ENGINEER OF RECORD.
- GROUND RESISTANCE TEST SHALL BE CONDUCTED IN ACCORDANCE WITH THE RESPECTIVE GROUND ELECTRODE RESISTANCE TESTING EQUIPMENT MANUFACTURER'S INSTRUCTIONS.
- RECORD SITE CONDITIONS DURING TESTS.
- "FALL OF POTENTIAL" TYPE GROUND ELECTRODE RESISTANCE TESTER IS RECOMMENDED FOR TESTING INDIVIDUAL STAND ALONE GROUND RODS.

**FOR BID**



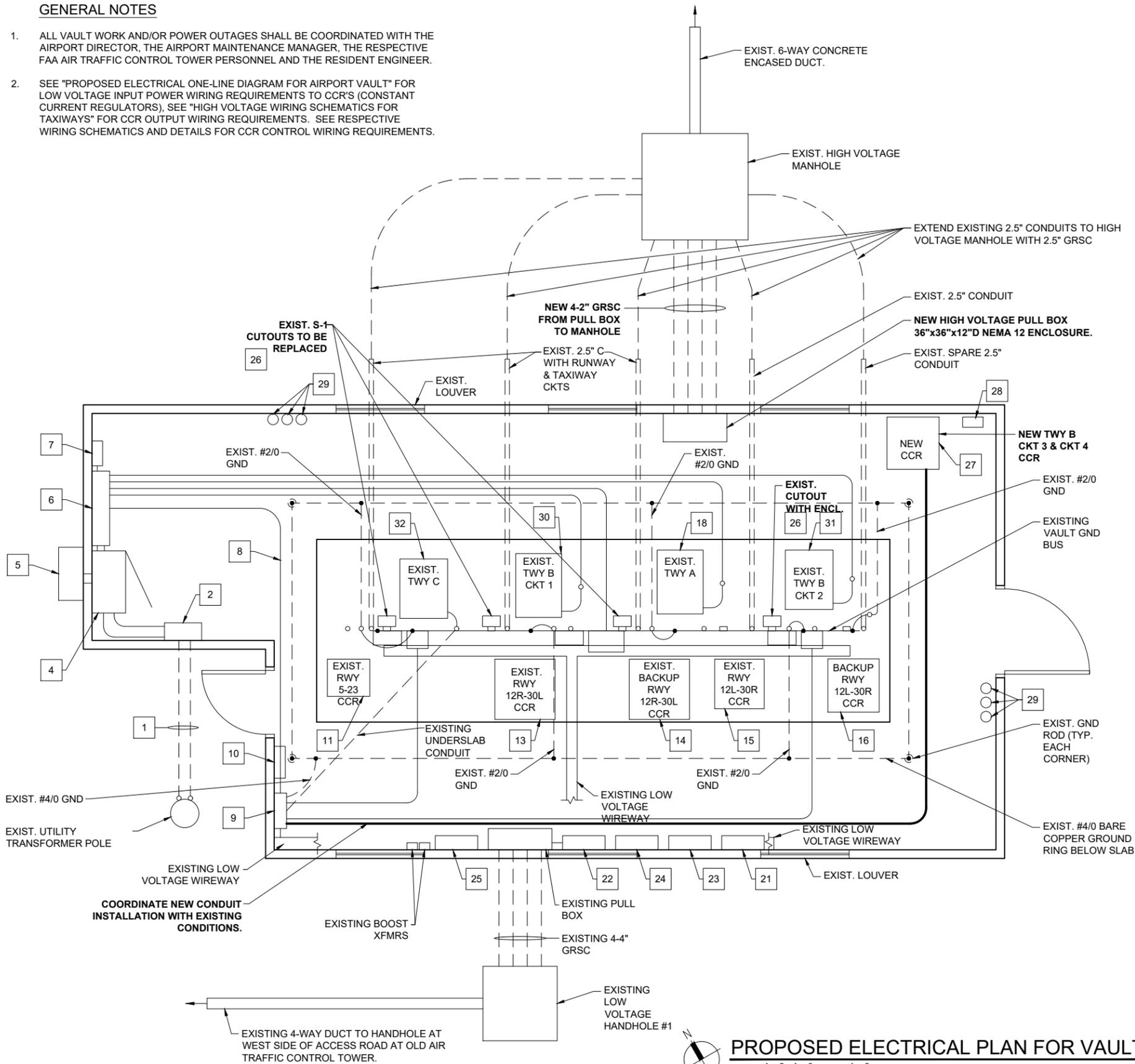


**GENERAL NOTES**

1. ALL VAULT WORK AND/OR POWER OUTAGES SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR, THE AIRPORT MAINTENANCE MANAGER, THE RESPECTIVE FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AND THE RESIDENT ENGINEER.
2. SEE "PROPOSED ELECTRICAL ONE-LINE DIAGRAM FOR AIRPORT VAULT" FOR LOW VOLTAGE INPUT POWER WIRING REQUIREMENTS TO CCR'S (CONSTANT CURRENT REGULATORS), SEE "HIGH VOLTAGE WIRING SCHEMATICS FOR TAXIWAYS" FOR CCR OUTPUT WIRING REQUIREMENTS. SEE RESPECTIVE WIRING SCHEMATICS AND DETAILS FOR CCR CONTROL WIRING REQUIREMENTS.

**KEYED NOTES**

- 1 EXIST. UNDERGROUND 800 AMP, 120/240 VAC, 1PH, 3W SERVICE;
- 2 EXIST. 800 AMP, 2 POLE CIRCUIT BREAKER SERVICE ENTRANCE DISCONNECT SWITCH.
- 3 RESERVED.
- 4 EXIST. 800 AMP, 120/240 VAC, 1 PHASE AUTO TRANSFER SWITCH IN A NEMA 12 ENCLOSURE.
- 5 EXIST. 800 AMP, 120/240 VAC, 1 PHASE PORTABLE GENERATOR CONNECTION BOX.
- 6 EXIST. MAIN DISTRIBUTION PANELBOARD "A".
- 7 EXIST. TRANSIENT VOLTAGE SURGE SUPPRESSOR.
- 8 EXIST. 400 AMP FEEDER FROM PANEL A TO PANEL B.
- 9 EXIST. DISTRIBUTION PANELBOARD "B".
- 10 EXIST. TVSS.
- 11 EXIST. 4KW RWY 5-23 CCR.
- 12 RESERVED.
- 13 EXIST. RWY 12R-30L CCR.
- 14 EXIST. RWY 12R-30L BACKUP CCR.
- 15 EXIST. 7.5KW RWY 12L-30R CCR; PRIMARY CCR FOR RWY 12L-30R.
- 16 EXIST. 10KW RWY 12R-30L CCR; BACKUP UNIT FOR RWY 12L-30R CCR.
- 17 RESERVED
- 18 EXIST. 30KW TWY A CCR.
- 19 RESERVED
- 20 RESERVED
- 21 EXIST. TRANSFER RELAY PANEL.
- 22 EXIST. TERMINAL PANEL FOR 50 PAIR CONTROL CABLE.
- 23 EXIST. 48VDC PILOT RELAY PANEL #1.
- 24 EXIST. 48VDC PILOT RELAY PANEL #2.
- 25 EXIST. LIGHTING CONTACTOR PANEL.
- 26 REMOVE EXISTING S-1 CUTOUT AND FURNISH AND INSTALL NEW TYPE S-1 CUTOUT IN EXISTING ENCLOSURE. REMOVE EXISTING WIRING AND REPLACE WITH NEW WIRING FROM CCR TO CUTOUT. (TYP EACH TAXIWAY CCR)
- 27 NEW TAXIWAY B CIRCUIT 3 AND CIRCUIT 4 CCR. INTERFACE CONTROL WIRING TO EXISTING TAXIWAY "B" CIRCUIT 2 CCR SUCH THAT TAXIWAY B CIRCUIT 3 AND CIRCUIT 4 LIGHTING IS CONTROLLED BY L-821 PANEL AT ATCT BY TAXIWAY "B" CONTROL SELECTOR SWITCH.
- 28 NEW S-1 CUTOUT PAIR (TWO CUTOUTS) WITH NEMA 12 ENCLOSURE FOR TAXIWAY "B" CIRCUIT 3 LIGHTING AND TAXIWAY "B" CIRCUIT 4 LIGHTING. CUTOUTS SHALL BE WIRED TO POWER TWO SERIES CIRCUITS CONNECTED TO ONE CCR.
- 29 PROVIDE TWO 10LB UL RATING 10B:C CARBON DIOXIDE FIRE EXTINGUISHERS FOR VAULT LOCATE NEAR EACH EXIT. INCLUDE PLACARD/SIGN "FOR ELECTRICAL FIRES" WITH ARROW POINTING TO EXTINGUISHER. PROVIDE TWO 10 LB UL RATING 1A: 10B:C HALOTRON FIRE EXTINGUISHERS LOCATE ADJACENT TO EACH CARBON DIOXIDE FIRE EXTINGUISHER. INCLUDE PLACARD/SIGN "FOR ELECTRICAL FIRES" WITH ARROW POINTING TO EXTINGUISHER. PROVIDE TWO 10LB UL RATING 4A:80B:C ABC DRY CHEMICAL FIRE EXTINGUISHERS. LOCATE ADJACENT TO OTHER FIRE EXTINGUISHERS. INCLUDE PLACARD/SIGN "FOR BUILDING FIRES" WITH ARROW POINTING TO ABC DRY CHEMICAL FIRE EXTINGUISHER.
- 30 EXISTING 30KW TWY B CKT 1 CCR TO BE REPLACED WITH A NEW 15KW L-829 CCR. RELOCATE EXISTING CCR TO STORAGE.
- 31 EXISTING 15KW TWY B CKT 2 CCR TO BE REPLACED WITH A NEW 15KW L-829 CCR. RELOCATE EXISTING TWY B, CKT 2 CCR FOR USE AS TWY C CCR.
- 32 EXISTING 7.5KW TWY C CCR TO BE REPLACED WITH EXISTING TWY B CKT 2 CCR. RELOCATE EXISTING TWY C CCR FOR USE AS BACKUP CCR FOR RWY 5-23.



**PROPOSED ELECTRICAL PLAN FOR VAULT**  
 0 1'-4" 2'-8" 5'-4"  
 HALF SIZE SCALE: 3/16" = 1'-0"  
 FULL SIZE SCALE: 3/8" = 1'-0"



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 www.hanson-inc.com  
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 1525 South Sixth Street  
 Springfield, Illinois 62703-2886  
 Telephone: 217.788.2450  
 Fax: 217.788.2503



COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
 CONTRACT NO.: SD064

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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PROPOSED ELECTRICAL PLAN FOR VAULT

**FOR BID**

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BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
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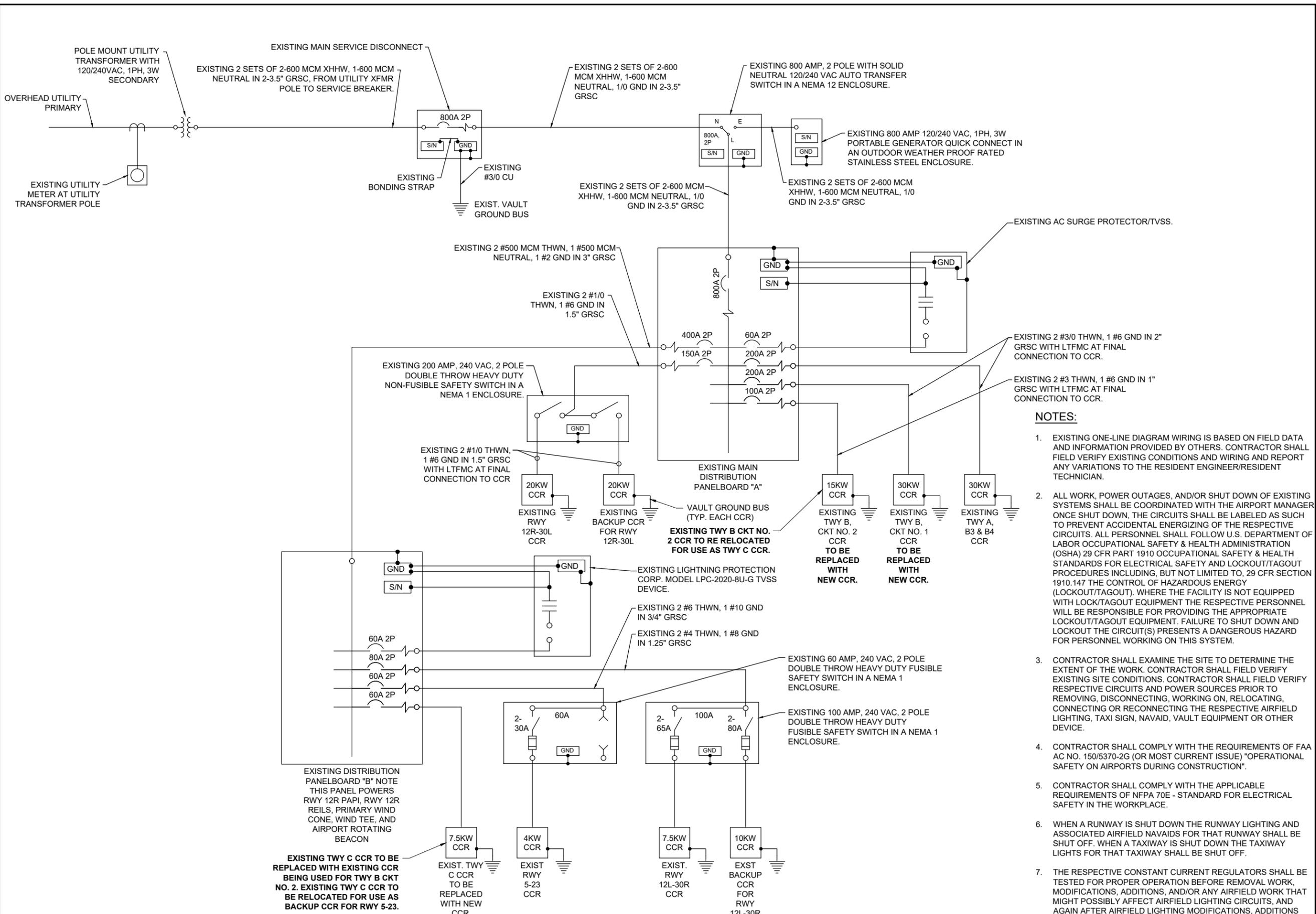
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SHEET TITLE

EXISTING  
ELECTRICAL  
ONE-LINE DIAGRAM  
FOR AIRPORT VAULT



**NOTES:**

- EXISTING ONE-LINE DIAGRAM WIRING IS BASED ON FIELD DATA AND INFORMATION PROVIDED BY OTHERS. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS AND WIRING AND REPORT ANY VARIATIONS TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN.
- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT). WHERE THE FACILITY IS NOT EQUIPPED WITH LOCK/TAGOUT EQUIPMENT THE RESPECTIVE PERSONNEL WILL BE RESPONSIBLE FOR PROVIDING THE APPROPRIATE LOCKOUT/TAGOUT EQUIPMENT. FAILURE TO SHUT DOWN AND LOCKOUT THE CIRCUIT(S) PRESENTS A DANGEROUS HAZARD FOR PERSONNEL WORKING ON THIS SYSTEM.
- CONTRACTOR SHALL EXAMINE THE SITE TO DETERMINE THE EXTENT OF THE WORK. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, CONNECTING OR RECONNECTING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, VAULT EQUIPMENT OR OTHER DEVICE.
- CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2G (OR MOST CURRENT ISSUE) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E - STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.
- WHEN A RUNWAY IS SHUT DOWN THE RUNWAY LIGHTING AND ASSOCIATED AIRFIELD NAVAIDS FOR THAT RUNWAY SHALL BE SHUT OFF. WHEN A TAXIWAY IS SHUT DOWN THE TAXIWAY LIGHTS FOR THAT TAXIWAY SHALL BE SHUT OFF.
- THE RESPECTIVE CONSTANT CURRENT REGULATORS SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, ADDITIONS, AND/OR ANY AIRFIELD WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING CIRCUITS, AND AGAIN AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS AND/OR UPGRADES.

**EXISTING ELECTRICAL ONE-LINE DIAGRAM FOR AIRPORT VAULT**

**FOR BID**

MAR 14, 2025 10:05 AM GROVE02594 1:23:JOBS023A0001D\CAD\AIRPORT\1\SHEET\2025 CPS-5078 COMBINED SHEETS\IE-601





BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

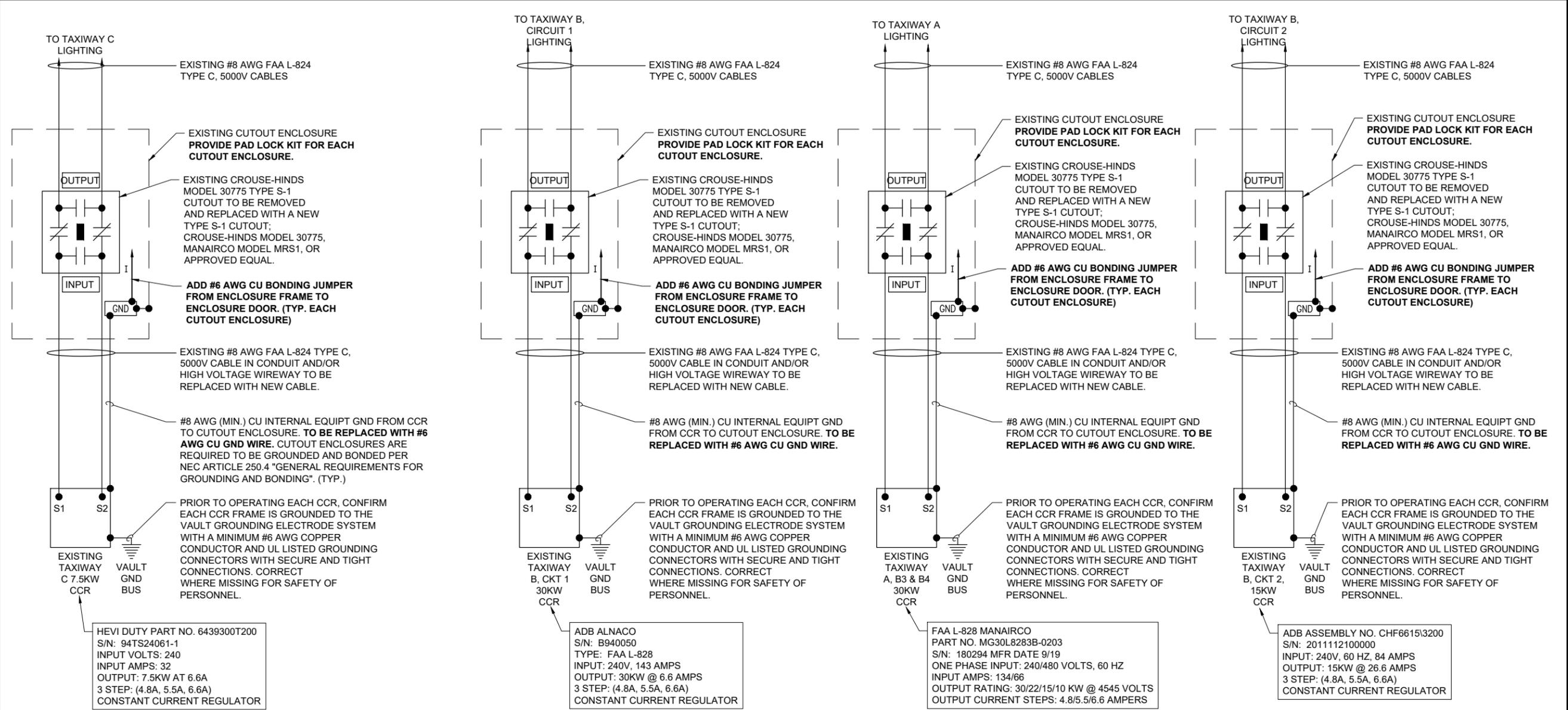
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: E-603-SCHM.DWG  
DESIGN BY: KNL 3/5/2025  
DRAWN BY: CWS 3/7/2025  
REVIEWED BY: KNL 3/11/2025

SHEET TITLE

EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS

**FOR BID**



EXISTING HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS

**NOTES**

- KEEP ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT PROJECT REPRESENTATIVE. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT). WHERE THE FACILITY IS NOT EQUIPPED WITH LOCKOUT/TAGOUT EQUIPMENT THE RESPECTIVE PERSONNEL WILL BE RESPONSIBLE FOR PROVIDING THE APPROPRIATE LOCKOUT/TAGOUT EQUIPMENT. WHERE EXISTING ELECTRICAL EQUIPMENT DOES NOT HAVE FEATURES FOR LOCKOUT/TAGOUT THE RESPECTIVE PERSONNEL WILL BE RESPONSIBLE FOR PROVIDING THE APPROPRIATE LOCKOUT/TAGOUT EQUIPMENT AND MEASURES TO ENSURE THE COMPLIANCE WITH OSHA LOCKOUT/TAGOUT PROCEDURES. FAILURE TO SHUT DOWN AND LOCKOUT THE CIRCUIT PRESENTS A DANGEROUS HAZARD FOR PERSONNEL WORKING ON THE SYSTEM. COMPLIANCE WITH LOCKOUT/TAGOUT PROCEDURES AND ALL OTHER SAFETY PROCEDURES AND REQUIREMENTS ARE THE RESPONSIBILITY OF EACH INDIVIDUAL, THE CONTRACTOR, THE RESPECTIVE MAINTENANCE PERSONNEL, AND ANY OTHER PERSONNEL WORKING ON THE EQUIPMENT OR ELECTRICAL SYSTEM.
- EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS.
- VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES FOR RESPECTIVE SYSTEMS PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, VAULT EQUIPMENT, OR OTHER DEVICES. THE CONTRACTOR WILL NEED TO EXERCISE CAUTION WHEN WORKING IN THE VAULT AND ON THE AIRFIELD. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE PROJECT ENGINEER OF RECORD AND THE RESIDENT PROJECT REPRESENTATIVE. CONTRACTOR SHALL FOLLOW LOCKOUT/TAGOUT PROCEDURES TO COMPLY WITH OSHA REQUIREMENTS.
- IDENTIFY EACH RESPECTIVE CIRCUIT PRIOR TO PERFORMING WORK ON THAT CIRCUIT. CONTRACTOR SHALL ARRANGE TO SHUTOFF, DISCONNECT, AND LOCKOUT/TAGOUT CIRCUITS WHEN WORKING ON THE RESPECTIVE AIRFIELD LIGHTING SYSTEMS FOR SAFETY OF PERSONNEL.
- NOTE: SOME OF THE EXISTING TAXIWAY LIGHTING CIRCUITS HAVE BEEN OBSERVED TO BE IN VERY POOR TO DANGEROUS CONDITIONS. EXERCISE CAUTION AND SAFETY PROCEDURES WHEN WORKING ON AIRFIELD LIGHTING SYSTEMS.

- NEVER REMOVE OR INSERT A CUTOUT WITH THE CIRCUIT ENERGIZED. SHUTOFF CIRCUITS PRIOR TO PULLING OR INSERTING A SERIES PLUG CUTOUT.
- THE RESPECTIVE PERSONNEL PERFORMING AIRFIELD LIGHTING WORK, VAULT WORK, AND/OR TESTS SHALL BE FAMILIAR WITH, AND QUALIFIED TO WORK ON, 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT. NEC DEFINES A QUALIFIED PERSON AS "ONE WHO HAS SKILLS AND KNOWLEDGE RELATED TO THE CONSTRUCTION AND OPERATION OF THE ELECTRICAL EQUIPMENT AND INSTALLATIONS AND HAS RECEIVED SAFETY TRAINING TO RECOGNIZE AND AVOID THE HAZARDS INVOLVED."
- EXERCISE CAUTION, PRACTICE SAFETY, AND DISCONNECT THE SERIES CIRCUITS FROM THE RESPECTIVE CONSTANT CURRENT REGULATORS, AS APPLICABLE WHEN PERFORMING WORK ON THE AIRFIELD LIGHTING OR WORK THAT MIGHT AFFECT THE AIRFIELD LIGHTING. CONTRACTOR SHALL MAKE NECESSARY ARRANGEMENTS TO DISCONNECT POWER AND LOCKOUT CIRCUITS FOR PROTECTION OF PERSONNEL.
- OVERSEE AND CONDUCT TESTS FOR AREAS OF WORK WHERE THE RESPECTIVE CIRCUITS MIGHT BE AFFECTED. MEGGER TEST AND RECORD EXISTING SERIES CIRCUITS (WITH A CABLE INSULATION TESTER) PRIOR TO CABLE WORK OR ANY OTHER WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING SYSTEMS, AND AGAIN AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, UPGRADES AND/OR OTHER WORK HAS BEEN COMPLETED. PROVIDE 5KV INSULATION TESTER FOR 5,000 VOLT SERIES CIRCUIT CABLES. ALSO TEST AND RECORD SERIES CIRCUIT LOOP RESISTANCE WITH AN OHMMETER. PROVIDE COPY OF TEST RESULTS TO THE ENGINEER OF RECORD (EOR) WITHIN 5 DAYS OF CONDUCTING TESTS.
- RESPECTIVE CCR'S SHALL BE TESTED FOR PROPER OPERATION BEFORE REMOVAL WORK, MODIFICATIONS, ADDITIONS AND/OR ANY AIRFIELD WORK THAT MIGHT AFFECT LIGHTING CIRCUITS AND AGAIN AFTER THE AIRFIELD WORK AND ADDITIONS HAVE BEEN COMPLETED. CONTRACTOR SHALL TEST AND RECORD THE INPUT CURRENT AND OUTPUT CURRENT FOR EACH CONSTANT CURRENT REGULATOR IN THE AUTOMATIC AND MANUAL MODES OF OPERATION. PROVIDE A TRUE RMS AMMETER FOR CURRENT MEASUREMENTS. CONTRACTOR SHALL REPORT CONCERNS AND/OR DEFICIENCIES TO THE RESIDENT PROJECT REPRESENTATIVE AND THE ENGINEER OF RECORD (EOR). WRITTEN TEST RESULTS SHALL BE PROVIDED TO THE RESIDENT PROJECT REPRESENTATIVE AND THE ENGINEER OF RECORD (EOR).

**LEGEND**

- "I" DENOTES PLUG CUTOUT WITH PLUG INSERTED
- "P" DENOTES PLUG CUTOUT WITH PLUG PULLED
- "CCR" DENOTES CONSTANT CURRENT REGULATOR

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RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

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ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: E-604.DWG  
DESIGN BY: KNL 3/5/2025  
DRAWN BY: CWS 3/7/2025  
REVIEWED BY: KNL 3/11/2025

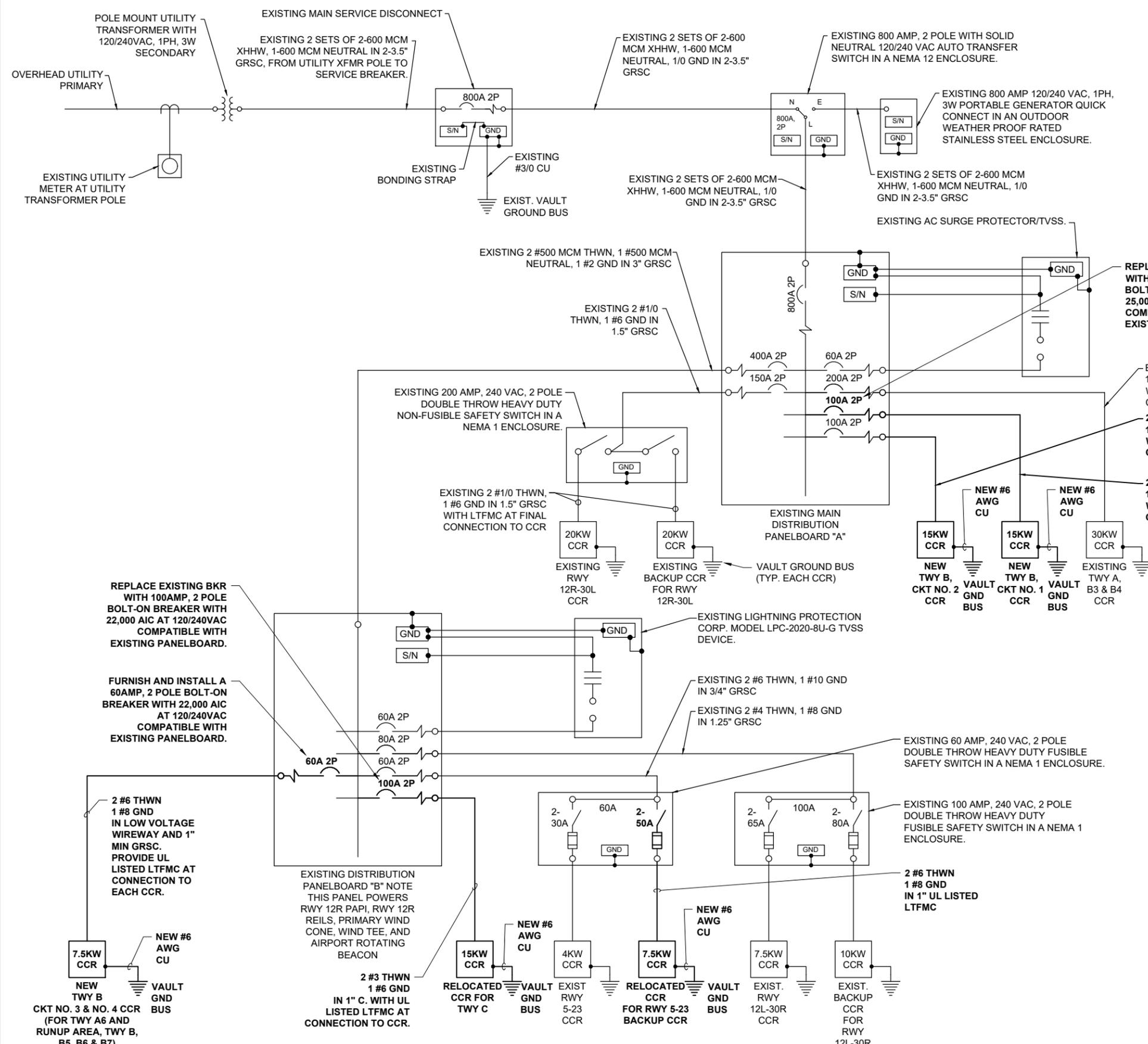
SHEET TITLE

PROPOSED ELECTRICAL ONE-LINE DIAGRAM FOR AIRPORT VAULT

**NOTES:**

- ALL VAULT WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- ALL ELECTRICAL EQUIPMENT AND MATERIALS SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, INTERTEK TESTING SERVICES VERIFICATION/ETL LISTING, (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
- ALL CONDUCTORS/WIRING SHALL BE COPPER.
- COLOR CODE PHASE AND NEUTRAL CONDUCTOR INSULATION FOR NO. 4 AWG OR SMALLER. PROVIDE COLORED INSULATION OR COLORED MARKING TAPE FOR PHASE AND NEUTRAL CONDUCTORS FOR NO. 3 AWG AND LARGER. INSULATED GROUND CONDUCTORS SHALL HAVE GREEN COLORED INSULATION FOR ALL CONDUCTOR AWG AND/OR KCMIL TO COMPLY WITH NEC 250.119. NEUTRAL CONDUCTORS SHALL HAVE WHITE COLORED INSULATION FOR NO. 6 AWG AND SMALLER TO MEET THE REQUIREMENTS OF NEC 200.6. STANDARD COLORS FOR POWER WIRING AND BRANCH CIRCUITS SHALL BE AS FOLLOWS:  

120/240 VAC, 1 PHASE, 3 WIRE	
PHASE A	BLACK
PHASE B	RED
NEUTRAL	WHITE
GROUND	GREEN
- CONTRACTOR SHALL CONFIRM POWER REQUIREMENTS WITH THE ACTUAL NAMEPLATE ON EACH CONSTANT CURRENT REGULATOR (OR OTHER RESPECTIVE EQUIPMENT) AND ADJUST CIRCUIT BREAKER, FUSES, WIRE SIZES & CONDUIT SIZES TO CONFORM WITH NEC & MANUFACTURER'S RECOMMENDATIONS WHERE APPLICABLE. WIRE SIZES SHOWN ON THE PLANS ARE MINIMUM.
- HIGH VOLTAGE CIRCUITS (AIRFIELD LIGHTING 5000 VOLT SERIES CIRCUITS OR OTHER CIRCUITS RATED ABOVE 600 VOLTS) AND LOW VOLTAGE CIRCUITS (RATED 600 VOLTS AND BELOW) SHALL NOT BE INSTALLED IN THE SAME WIREWAY, CONDUIT, HANDHOLE, JUNCTION BOX, OR RACEWAY.
- LTFMC DENOTES LIQUID TIGHT FLEXIBLE METAL CONDUIT UL LISTED, SUNLIGHT RESISTANT, & SUITABLE FOR GROUNDING. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LTFMC THAT IS NOT UL LISTED. CONFIRM LTFMC BEARS THE UL LABEL PRIOR TO INSTALLATION.
- EQUIPMENT AND MATERIALS NOT LABELED AS "EXISTING" ARE NEW.



**PROPOSED ELECTRICAL ONE-LINE DIAGRAM FOR AIRPORT VAULT**

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ISSUE: MARCH 14, 2025

PROJECT NO: 23A0001D

CAD FILE: E-610.DWG

DESIGN BY: KNL 3/5/2025

DRAWN BY: CWS 3/7/2025

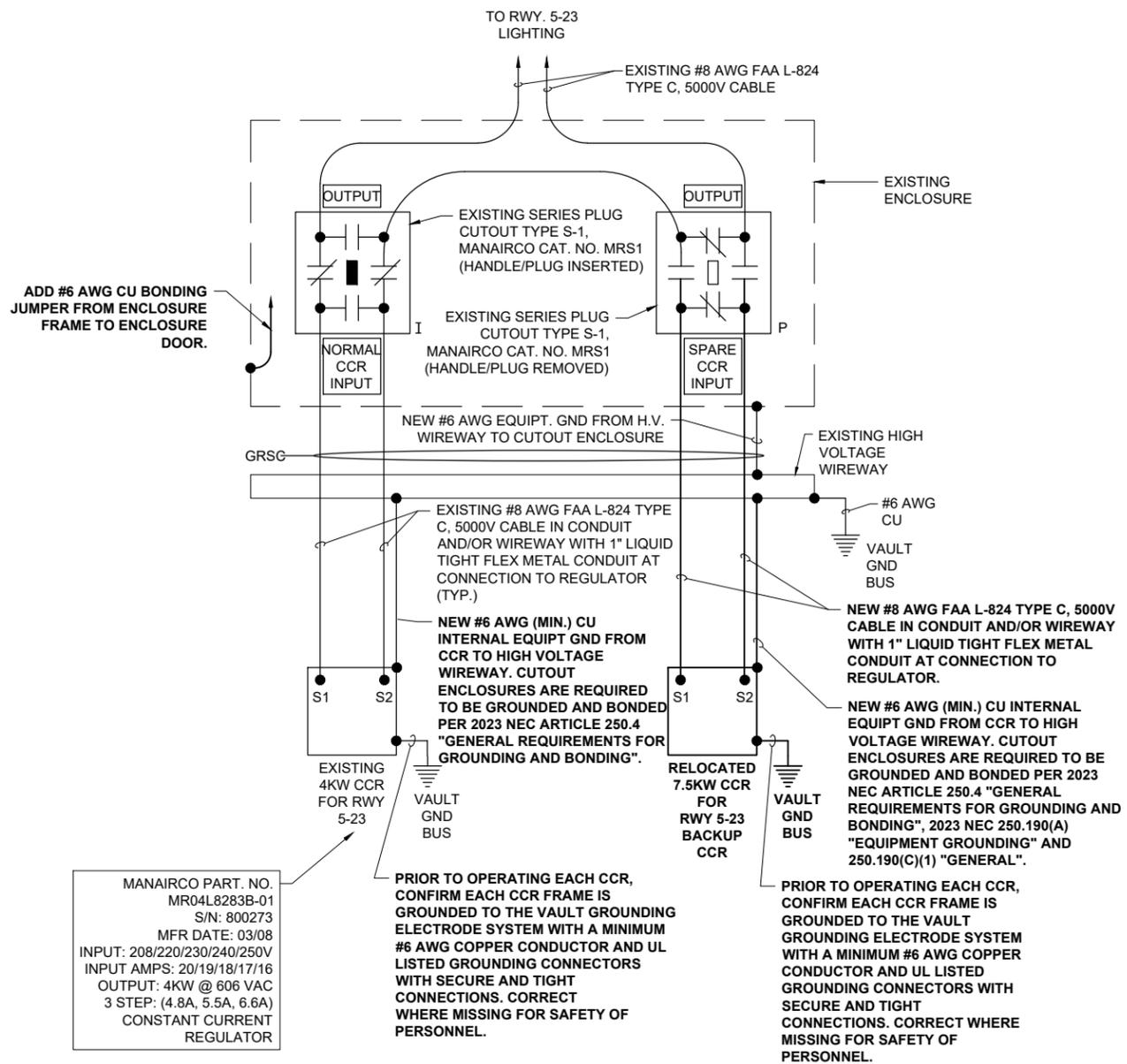
REVIEWED BY: KNL 3/11/2025

SHEET TITLE

PROPOSED HIGH VOLTAGE WIRING SCHEMATIC FOR RUNWAY 5-23

**NOTES:**

1. KNOW RESPECTIVE CIRCUITS AND POWER SOURCES FOR RESPECTIVE SYSTEMS PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, VAULT EQUIPMENT, OR OTHER DEVICES. THE CONTRACTOR WILL NEED TO EXERCISE CAUTION WHEN WORKING IN THE VAULT AND ON THE AIRFIELD. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE PROJECT ENGINEER OF RECORD AND THE RESIDENT PROJECT REPRESENTATIVE. CONTRACTOR SHALL FOLLOW LOCKOUT/TAGOUT PROCEDURES FOR SAFETY PERSONNEL.
2. EQUIPMENT AND MATERIALS NOT LABELED AS EXISTING ARE NEW.
3. VERIFY EACH CUTOUT IS PROVIDED WITH LEGEND PLATES TO IDENTIFY THE RESPECTIVE CUTOUT INPUT AND OUTPUT.
4. INCLUDE ADEQUATE WORKING SPACE IN FRONT OF EACH CUTOUT ENCLOSURE TO MEET NEC CLEARANCE REQUIREMENTS.
5. NOTE SOME OF THE EXISTING RUNWAY LIGHTING CIRCUITS HAVE BEEN OBSERVED TO BE IN VERY POOR TO DANGEROUS CONDITION. DANGEROUS CONDITION INDICATES LIGHTING FAILURES, GROUND FAULTS, TRANSFORMER FAILURES, BAD CONNECTIONS, UNSAFE CONDITIONS, RISK OF ELECTRIC SHOCK, INJURY AND/OR DEATH, AND OTHER FAILURE CONDITIONS CAN BE EXPECTED OR PRESENTLY EXIST. PERSONNEL ARE AT RISK OF ELECTRIC SHOCK, INJURY, AND DEATH WHEN ON THE AIRFIELD AREAS THAT CONTAIN THE LIGHTING SYSTEMS. THE AIRFIELD LIGHTING SERIES CIRCUIT CABLES ARE UNSAFE, AND THAT BEING NEAR ACTIVE CIRCUITS WITH STANDING WATER ON THE GROUND COULD BE LIFE-THREATENING. CAUTION NEEDS TO BE EXERCISED WHEN WORKING ON OR AROUND THESE CIRCUITS. THE AIRFIELD LIGHTING SERIES CIRCUITS ARE IN SUCH BAD CONDITION THAT THEY ARE A SAFETY HAZARD TO PERSONNEL WORKING ON THE AIRFIELD, AND SAFETY MEASURES NEED TO BE PERFORMED WHEN WORKING AROUND THESE CIRCUITS. SHUT OFF AND LOCK OFF THE RESPECTIVE AIRFIELD LIGHTING CIRCUITS WHEN WORKING IN AREAS THAT HAVE AIRFIELD LIGHTING CIRCUITS.
6. NEVER REMOVE OR INSERT A CUTOUT WITH THE CIRCUIT ENERGIZED. SHUTOFF CIRCUITS PRIOR TO PULLING OR INSERTING A SERIES PLUG CUTOUT.
7. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6, SUITABLE FOR GROUNDING AND SUNLIGHT RESISTANT. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS NOT U.L. LISTED. CONFIRM LIQUID TIGHT FLEXIBLE METAL CONDUIT BEARS THE UL LABEL PRIOR TO INSTALLING IT.
8. INSTALL UL LISTED FIRE STOP MATERIAL AT EACH CONDUIT ENTRY AND EXIT TO EACH RESPECTIVE CUTOUT ENCLOSURE (EXISTING AND NEW).
9. SERIES CIRCUIT DISCONNECTS/CUTOUTS ARE REQUIRED TO ACCOMMODATE MAINTENANCE PROCEDURES AS NOTED IN FAA AC 150/5340-26C AND IN ACCORDANCE WITH FAA AC 150/5340-30J, PART 3.5.5 CONSTANT CURRENT REGULATORS (CCRS). SERIES PLUG CUTOUTS SHALL BE TYPE S-1, RATED 5000 VOLTS, 20-AMP. SERIES PLUG CUTOUTS SHALL BE RATED SUITABLE FOR NORMAL OPERATIONS WITH HANDLE REMOVED OR HANDLE INSERTED. CUTOUTS SHALL DISCONNECT THE INPUT FROM THE OUTPUT, SHORT TERMINALS, AND SHORT THE OUTPUT TERMINALS WHEN THE HANDLE/PLUG IS REMOVED. SERIES PLUG CUTOUTS SHALL BE CROUSE-HINDS CAT. NO. 30775. OR APPROVED EQUAL THE RESPECTIVE MANUFACTURER SHALL CERTIFY IN WRITING THAT THEIR CUTOUT IS SUITABLE AND RATED FOR THE RESPECTIVE APPLICATION.
10. EACH REGULATOR FRAME SHALL BE BONDED TO VAULT GROUND BUS WITH A DEDICATED #6 AWG BONDING JUMPER.
11. OTHER PROJECTS MAY BE UNDER CONSTRUCTION DURING THIS PROJECT. COORDINATE WORK WITH OTHER CONTRACTORS.
12. RESPECTIVE LOW VOLTAGE WIRING SHALL ENTER RESPECTIVE CCR AT THE LOW VOLTAGE SECTION. HIGH VOLTAGE WIRING SHALL ENTER THE RESPECTIVE CCR AT THE HIGH VOLTAGE SECTION. MAINTAIN SEPERATION OF HIGH VOLTAGE WIRING (AIRFIELD LIGHTING 5000 VOLTS SERIES CIRCUITS AND/OR OTHER CIRCUITS RATED ABOVE 600 VOLTS) FROM LOW VOLTAGE WIRING (RATED 600 VOLTS AND BELOW) TO COMPLY WITH NEC 300.3(C)(2). HIGH VOLTAGE AND LOW VOLTAGE WIRING SHALL NOT BE INSTALLED IN THE SAME RACEWAY, CONDUIT, WIREWAY, PULL BOX, SPLICE CAN, HANDHOLE, OR MANHOLE.



**PROPOSED HIGH VOLTAGE WIRING SCHEMATIC FOR RUNWAY 5-23**

**LEGEND**

"I" DENOTES PLUG CUTOUT WITH PLUG INSERTED

"P" DENOTES PLUG CUTOUT WITH PLUG PULLED

"CCR" DENOTES CONSTANT CURRENT REGULATOR

MANAIRCO PART. NO. MR04L8283B-01  
S/N: 800273  
MFR DATE: 03/08  
INPUT: 208/220/230/240/250V  
INPUT AMPS: 20/19/18/17/16  
OUTPUT: 4KW @ 606 VAC  
3 STEP: (4.8A, 5.5A, 6.6A)  
CONSTANT CURRENT REGULATOR

PRIOR TO OPERATING EACH CCR, CONFIRM EACH CCR FRAME IS GROUNDED TO THE VAULT GROUNDING ELECTRODE SYSTEM WITH A MINIMUM #6 AWG COPPER CONDUCTOR AND UL LISTED GROUNDING CONNECTORS WITH SECURE AND TIGHT CONNECTIONS. CORRECT WHERE MISSING FOR SAFETY OF PERSONNEL.

PRIOR TO OPERATING EACH CCR, CONFIRM EACH CCR FRAME IS GROUNDED TO THE VAULT GROUNDING ELECTRODE SYSTEM WITH A MINIMUM #6 AWG COPPER CONDUCTOR AND UL LISTED GROUNDING CONNECTORS WITH SECURE AND TIGHT CONNECTIONS. CORRECT WHERE MISSING FOR SAFETY OF PERSONNEL.



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PROJECT NO: 23A0001D

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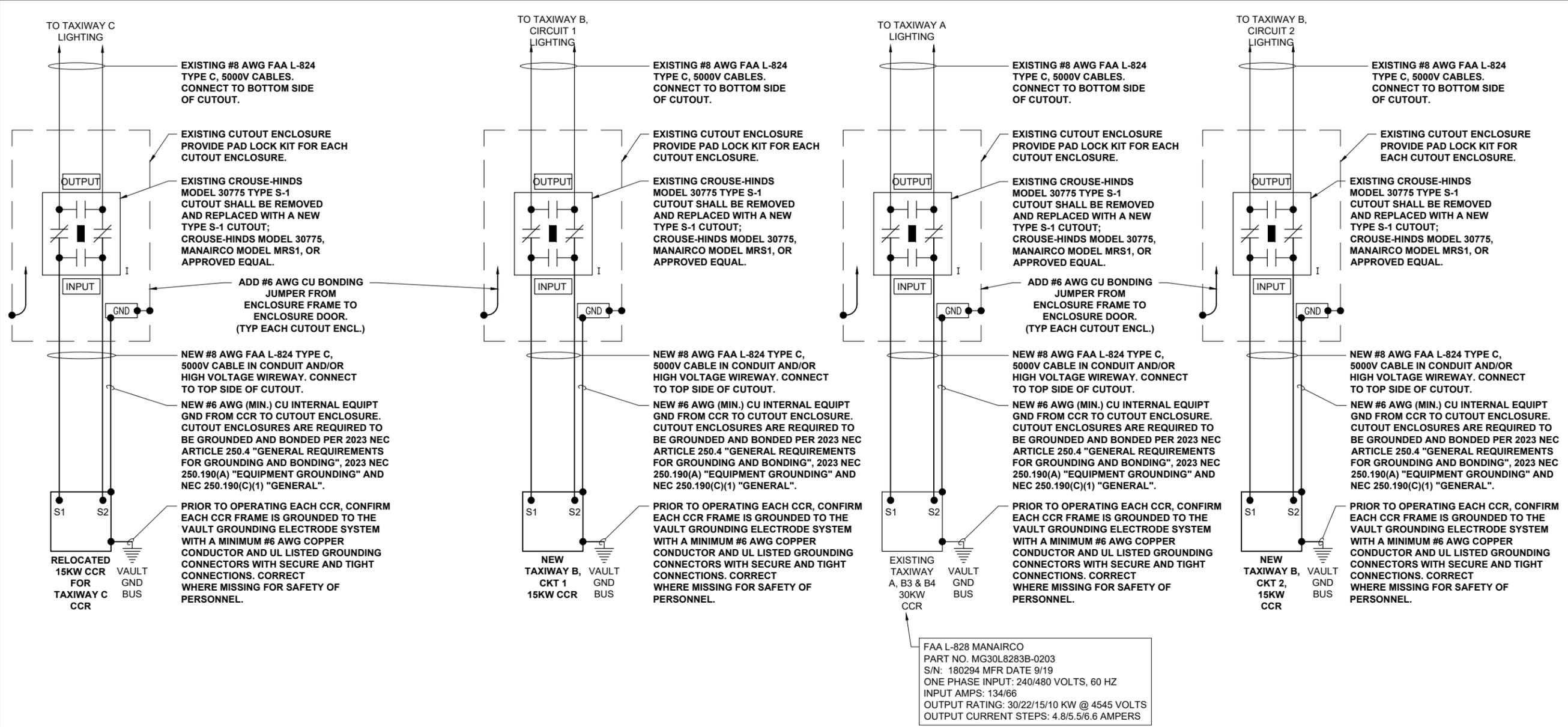
DESIGN BY: KNL 3/5/2025

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SHEET TITLE

**PROPOSED HIGH  
VOLTAGE WIRING  
SCHEMATIC FOR  
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**PROPOSED HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAYS**

**NOTES:**

- KNOW RESPECTIVE CIRCUITS AND POWER SOURCES FOR RESPECTIVE SYSTEMS PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, VAULT EQUIPMENT, OR OTHER DEVICES. THE CONTRACTOR WILL NEED TO EXERCISE CAUTION WHEN WORKING IN THE VAULT AND ON THE AIRFIELD. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE PROJECT ENGINEER OF RECORD AND THE RESIDENT PROJECT REPRESENTATIVE. CONTRACTOR SHALL FOLLOW LOCKOUT/TAGOUT PROCEDURES FOR SAFETY PERSONNEL.
- EQUIPMENT AND MATERIALS NOT LABELED AS EXISTING ARE NEW.
- VERIFY EACH CUTOUT IS PROVIDED WITH LEGEND PLATES TO IDENTIFY THE RESPECTIVE CUTOUT INPUT AND OUTPUT.
- INCLUDE ADEQUATE WORKING SPACE IN FRONT OF EACH CUTOUT ENCLOSURE TO MEET NEC CLEARANCE REQUIREMENTS.
- NOTE THE EXISTING TAXIWAY LIGHTING CIRCUITS HAVE BEEN OBSERVED TO BE IN VERY POOR TO DANGEROUS CONDITION. DANGEROUS CONDITION INDICATES LIGHTING FAILURES, GROUND FAULTS, TRANSFORMER FAILURES, BAD CONNECTIONS, UNSAFE CONDITIONS, RISK OF ELECTRIC SHOCK, INJURY AND/OR DEATH, AND OTHER FAILURE CONDITIONS CAN BE EXPECTED OR PRESENTLY EXIST. PERSONNEL ARE AT RISK OF ELECTRIC SHOCK, INJURY, AND DEATH WHEN ON THE AIRFIELD AREAS THAT CONTAIN THE LIGHTING SYSTEMS. THE AIRFIELD LIGHTING SERIES CIRCUIT CABLES ARE UNSAFE, AND THAT BEING NEAR ACTIVE CIRCUITS WITH STANDING WATER ON THE GROUND COULD BE LIFE-THREATENING. CAUTION NEEDS TO BE EXERCISED WHEN WORKING ON OR AROUND THESE CIRCUITS. THE AIRFIELD LIGHTING SERIES CIRCUITS ARE IN SUCH BAD CONDITION THAT THEY ARE A SAFETY HAZARD TO PERSONNEL WORKING ON THE AIRFIELD, AND SAFETY MEASURES NEED TO BE PERFORMED WHEN WORKING AROUND THESE CIRCUITS. SHUT OFF AND LOCK OFF THE RESPECTIVE AIRFIELD LIGHTING CIRCUITS WHEN WORKING IN AREAS THAT HAVE AIRFIELD LIGHTING CIRCUITS.
- NEVER REMOVE OR INSERT A CUTOUT WITH THE CIRCUIT ENERGIZED. SHUTOFF CIRCUITS PRIOR TO PULLING OR INSERTING A SERIES PLUG CUTOUT.
- LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6, SUITABLE FOR GROUNDING AND SUNLIGHT RESISTANT. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS NOT UL LISTED. CONFIRM LIQUID TIGHT FLEXIBLE METAL CONDUIT BEARS THE UL LABEL PRIOR TO INSTALLING IT.
- INSTALL UL LISTED FIRE STOP MATERIAL AT EACH CONDUIT ENTRY AND EXIT TO EACH RESPECTIVE CUTOUT ENCLOSURE (EXISTING AND NEW).
- SERIES CIRCUIT DISCONNECTS/CUTOUPS ARE REQUIRED TO ACCOMMODATE MAINTENANCE PROCEDURES AS NOTED IN FAA AC 150/5340-26C AND IN ACCORDANCE WITH FAA AC 150/5340-30J, PART 3.5.5 CONSTANT CURRENT REGULATORS (CCRS). SERIES PLUG CUTOUPS SHALL BE TYPE S-1, RATED 5000 VOLTS, 20-AMP, SERIES PLUG CUTOUPS SHALL BE RATED SUITABLE FOR NORMAL OPERATIONS WITH HANDLE REMOVED OR HANDLE INSERTED. CUTOUPS SHALL DISCONNECT THE INPUT FROM THE OUTPUT, SHORT TERMINALS, AND SHORT THE OUTPUT TERMINALS WHEN THE HANDLE/PLUG IS REMOVED. SERIES PLUG CUTOUPS SHALL BE CROUSE-HINDS CAT. NO. 30775, MANAIRCO CAT. NO. MRS1, OR APPROVED EQUAL THE RESPECTIVE MANUFACTURER SHALL CERTIFY IN WRITING THAT THEIR CUTOUP IS SUITABLE AND RATED FOR THE RESPECTIVE APPLICATION.
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- OTHER PROJECTS MAY BE UNDER CONSTRUCTION DURING THIS PROJECT. COORDINATE WORK WITH OTHER CONTRACTORS.
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**LEGEND**

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"P"	DENOTES PLUG CUTOUT WITH PLUG PULLED
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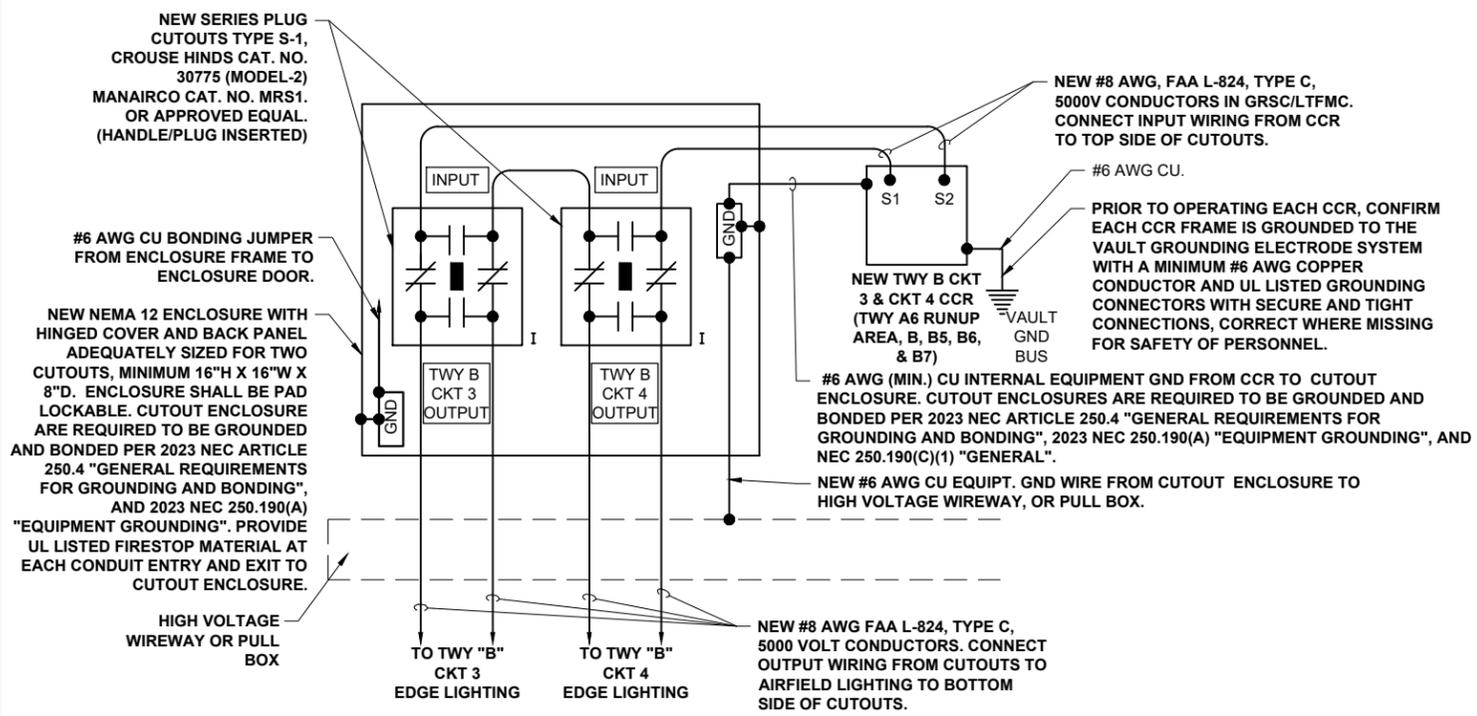
ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
CAD FILE: E-606.DWG  
DESIGN BY: KNL 3/5/2025  
DRAWN BY: CWS 3/7/2025  
REVIEWED BY: KNL 3/11/2025

SHEET TITLE

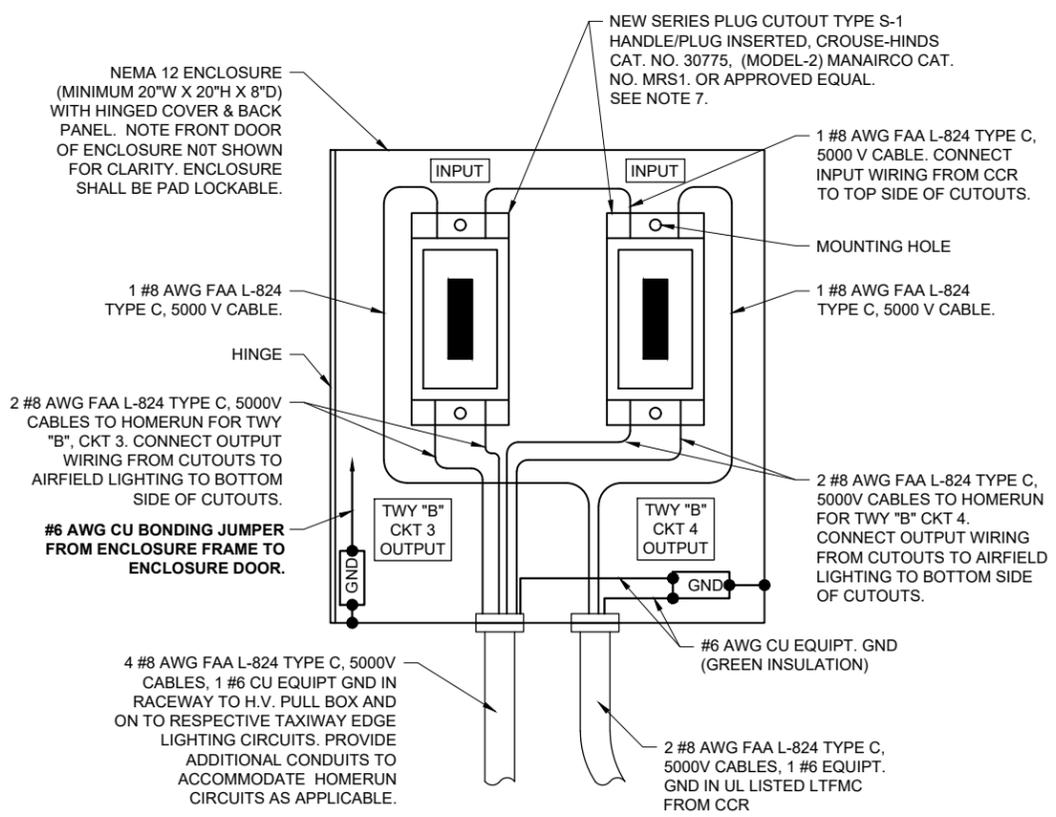
PROPOSED HIGH  
VOLTAGE WIRING  
SCHEMATIC - TWY B  
CKTS 3 & 4

**NOTES**

1. PROVIDE PHENOLIC ENGRAVED LEGEND PLATES FOR EACH CONSTANT CURRENT REGULATOR NOTING THE REGULATOR DESIGNATION AND THE RUNWAY AND/OR TAXIWAY SERVED.
2. EACH PLUG CUTOUT CABINET SHALL BE FURNISHED WITH A PHENOLIC ENGRAVED LEGEND PLATE THAT IDENTIFIES THE RESPECTIVE CIRCUIT OR REGULATOR. INCLUDE AN ADDITIONAL LEGEND PLATE LABELED "CAUTION OPERATE CUTOUTS WITH CCR SHUT OFF". FURNISH & INSTALL A WARNING LABEL FOR CUTOUT ENCLOSURE TO WARN PERSONS OF POTENTIAL ARC FLASH HAZARDS, PER THE REQUIREMENTS OF NEC 110.16 "ARC-FLASH HAZARD WARNING". PROVIDE PHENOLIC ENGRAVED LEGEND PLATES FOR EACH CUTOUT TO IDENTIFY THE RESPECTIVE CUTOUT INPUT CONNECTION AND THE RESPECTIVE CUTOUT OUTPUT CONNECTION.
3. PROVIDE PHENOLIC ENGRAVED LEGEND PLATES FOR EACH CUTOUT TO IDENTIFY THE RESPECTIVE CUTOUT INPUT CONNECTION AND THE RESPECTIVE CUTOUT OUTPUT CONNECTION.
4. BOND EACH REGULATOR FRAME TO VAULT GROUND BUS WITH A DEDICATED #6 AWG COPPER BONDING JUMPER.
5. PROVIDE ADEQUATE WORKING SPACE IN FRONT OF EACH CUTOUT ENCLOSURE TO MEET NEC CLEARANCE REQUIREMENTS.
6. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6, SUITABLE FOR GROUNDING AND SUNLIGHT RESISTANT. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS NOT UL LISTED. CONFIRM LIQUID TIGHT FLEXIBLE METAL CONDUIT BEARS THE UL LABEL PRIOR TO INSTALLING IT.
7. SERIES PLUG CUTOUTS SHALL BE TYPE S-1, RATED 5000 VOLTS, 20-AMP. SERIES PLUG CUTOUTS SHALL BE RATED SUITABLE FOR NORMAL OPERATION WITH HANDLE REMOVED OR HANDLE INSERTED. CUTOUTS SHALL DISCONNECT THE INPUT FROM THE OUTPUT, SHORT THE INPUT TERMINALS, AND SHORT THE OUTPUT TERMINALS WHEN THE HANDLE/PLUG IS REMOVED. SERIES PLUG CUTOUTS FOR TAXIWAY B LIGHTING CIRCUITS 3 & 4 SHALL BE WIRED TO POWER TWO SEPARATE SERIES LIGHTING CIRCUITS FROM ONE CCR. SERIES CIRCUIT PLUG CUTOUTS SHALL BE SUITABLE FOR NORMAL OPERATION WITH HANDLE PLUG REMOVED TO ACCOMMODATE OPERATING ONE TWY LTG CKT WHILE THE OTHER TWY LTG CKT IS OFF. SERIES PLUG CUTOUTS SHALL BE CROUSE-HINDS CAT. NO. 30775, MANAIRCO CAT. NO. MRS1, HUGHEY & PHILLIPS CAT. NO. MRS1 OR APPROVED EQUAL. THE RESPECTIVE MANUFACTURER SHALL CERTIFY IN WRITING THAT THEIR CUTOUT IS SUITABLE AND RATED FOR THE RESPECTIVE APPLICATION.
8. MAINTAIN SEPARATION OF HIGH VOLTAGE WIRING (AIRFIELD LIGHTING 5000 VOLT SERIES CIRCUITS AND/OR OTHER CIRCUITS RATED ABOVE 600 VOLTS) FROM LOW VOLTAGE WIRING (RATED 600 VOLTS AND BELOW) TO COMPLY WITH NEC 300.3(C)(2). HIGH VOLTAGE AND LOW VOLTAGE WIRING SHALL NOT BE INSTALLED IN THE SAME RACEWAY, CONDUIT, WIREWAY, PULL BOX, SPLICE CAN, HANDHOLE, OR MANHOLE.
9. LOW VOLTAGE WIRING SHALL ENTER THE RESPECTIVE CCR AT THE LOW VOLTAGE SECTION. HIGH VOLTAGE WIRING SHALL ENTER THE RESPECTIVE CCR AT THE HIGH VOLTAGE SECTION.
10. PROVIDE UL LISTED FIRE STOP MATERIAL AT EACH CONDUIT ENTRY AND EXIT TO EACH RESPECTIVE CUTOUT ENCLOSURE.



**PROPOSED HIGH VOLTAGE WIRING SCHEMATIC FOR TAXIWAY B, CKTS 3 & 4 LIGHTING**  
NOT TO SCALE



**SERIES PLUG CUTOUT MOUNTING DETAIL FOR  
TAXIWAY "B" CIRCUITS 3 & 4**  
NOT TO SCALE

**LEGEND**

- "I" DENOTES PLUG CUTOUT WITH PLUG INSERTED
- "P" DENOTES PLUG CUTOUT WITH PLUG PULLED
- "CCR" DENOTES CONSTANT CURRENT REGULATOR

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**FOR BID**



**ST. LOUIS DOWNTOWN AIRPORT**

BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

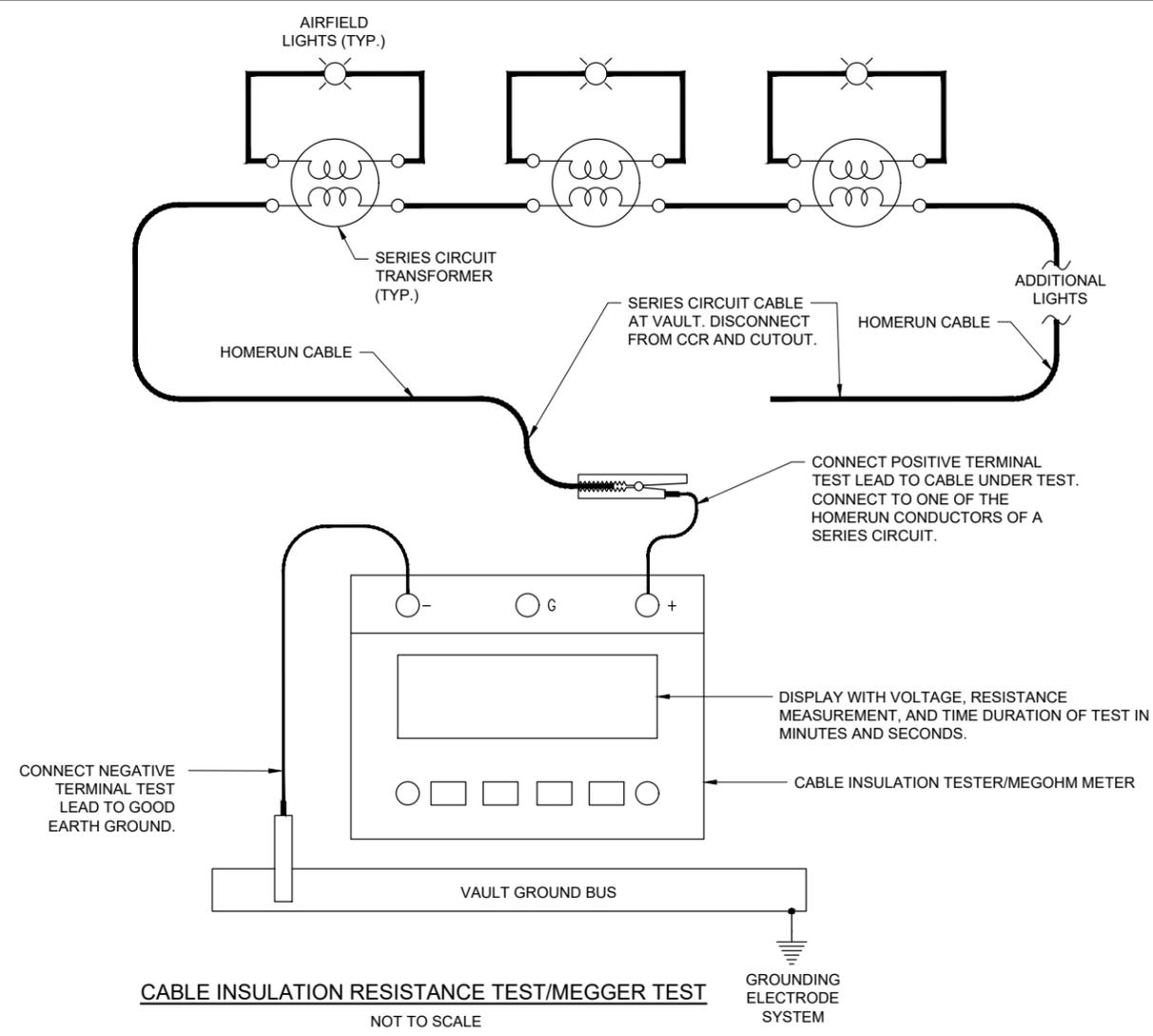
IDA NO.: CPS-5078  
CONTRACT NO.: SD064

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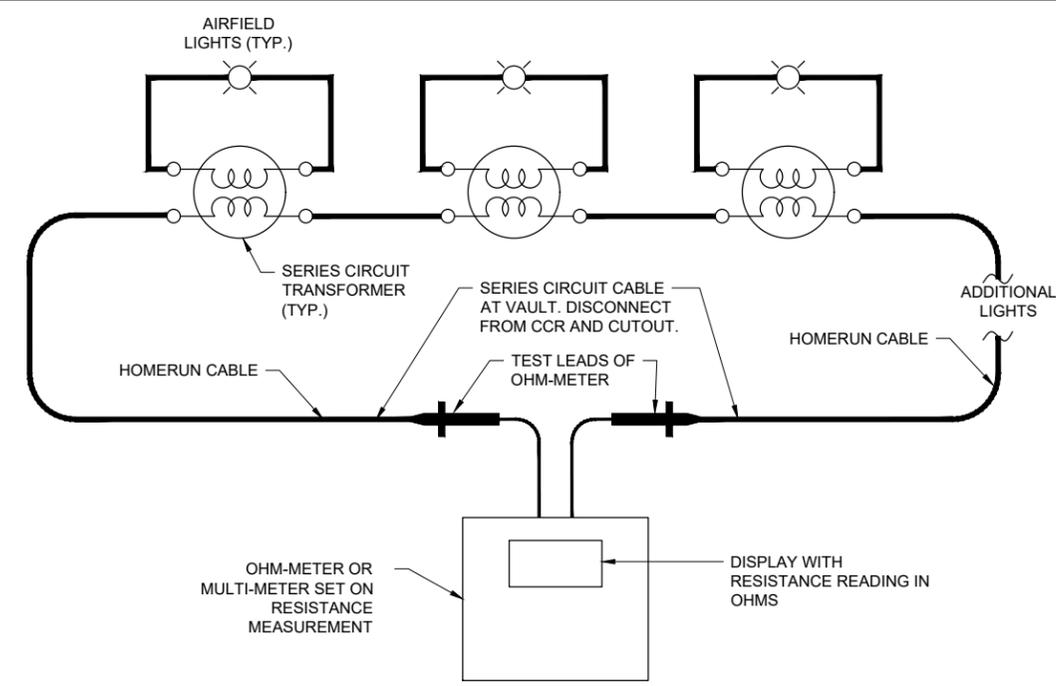
**SERIES CIRCUIT CABLE TESTING DETAILS**



**CABLE INSULATION RESISTANCE TEST/MEGGER TEST**  
NOT TO SCALE

**CABLE INSULATION RESISTANCE TEST (MEGGER TEST) NOTES**

- PRIOR TO BEGINNING EXCAVATIONS, AIRFIELD LIGHTING MODIFICATIONS, CABLE INSTALLATION, AND/OR ANY OTHER WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING CIRCUITS, ALL EXISTING SERIES CIRCUIT LIGHTING CABLES SHALL BE MEGGER TESTED WITH AN INSULATION RESISTANCE TESTER AND RECORDED AT THE RESPECTIVE AIRPORT ELECTRICAL VAULT.
- AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, UPGRADES, AND/OR OTHER WORK AND ADDITIONS HAVE BEEN COMPLETED ALL EXISTING SERIES CIRCUIT LIGHTING CABLES SHALL BE MEGGER TESTED WITH AN INSULATION RESISTANCE TESTER AND RECORDED AT THE RESPECTIVE AIRPORT ELECTRICAL VAULT.
- THE CONTRACTOR IS RESPONSIBLE TO EMPLOY THE SERVICES OF PERSONNEL QUALIFIED, FAMILIAR WITH, AND TRAINED TO PERFORM THE RESPECTIVE TESTS, AND QUALIFIED TO WORK ON 5000 VOLT AIRFIELD LIGHTING SERIES CIRCUITS, CONSTANT CURRENT REGULATORS, AND ASSOCIATED AIRPORT ELECTRICAL VAULT EQUIPMENT.
- INSULATION RESISTANCE TESTING EQUIPMENT FOR USE WITH 5,000 VOLT SERIES CIRCUIT CABLES SHALL USE AN INSULATION RESISTANCE TESTER CAPABLE OF TESTING THE CABLES AT 5,000 VOLTS. OLDER SERIES CIRCUIT CABLES AND/OR CABLES IN POOR CONDITION MAY REQUIRE THE TEST VOLTAGE TO BE PERFORMED AT A VOLTAGE LOWER THAN 5,000 VOLTS (EXAMPLE 1,000 VOLTS, 500 VOLTS, OR LESS THAN 500 VOLTS). THE RESPECTIVE TEST VOLTAGE SHALL BE RECORDED FOR EACH CABLE INSULATION RESISTANCE TEST RESULT.
- INSULATION RESISTANCE TESTING EQUIPMENT FOR USE WITH 600 VOLT RATED CABLES SHALL USE A 500 VOLT INSULATION RESISTANCE TESTER. THE RESPECTIVE TEST VOLTAGE SHALL BE RECORDED FOR EACH CABLE INSULATION RESISTANCE TEST RESULT.
- IT IS RECOMMENDED TO USE THE SAME INSULATION RESISTANCE TEST EQUIPMENT THROUGHOUT THE PROJECT TO ENSURE RELIABLE COMPARATIVE READINGS AT THE BEGINNING OF THE PROJECT AND AT THE COMPLETION OF THE PROJECT.
- DISCONNECT THE AIRFIELD LIGHTING SERIES CIRCUIT CABLES FROM THE CONSTANT CURRENT REGULATOR WHEN PERFORMING CABLE INSULATION RESISTANCE TESTS (MEGGER TESTS). TEST THE CABLES THAT GO TO THE AIRFIELD FOR THE RESPECTIVE AIRFIELD LIGHTING SERIES CIRCUIT. CONNECT THE CABLE INSULATION RESISTANCE TESTER TO ONE OF THE AIRFIELD LIGHTING SERIES CIRCUIT CABLES AND TO A GOOD GROUND IN THE AIRPORT ELECTRICAL VAULT SUCH AS THE AIRPORT VAULT GROUND BUS. CONDUCT THE CABLE INSULATION RESISTANCE TEST ON EACH RESPECTIVE CABLE FOR NOT LESS THAN 90 SECONDS. RECORD THE TEST RESULTS AT THE END OF THE TIME DURATION FOR THE TEST.
- FAA ADVISORY CIRCULAR 150/5340-26C MAINTENANCE OF AIRPORT VISUAL AID FACILITIES PROVIDES GUIDANCE ON INSULATION RESISTANCE TESTS. ALSO REFER TO THE USER MANUAL FOR THE RESPECTIVE CABLE INSULATION RESISTANCE TESTER. REASONABLY NEW SERIES CIRCUIT CABLES AND TRANSFORMERS WITH GOOD CONNECTIONS SHOULD READ 500 MEGA-OHMS TO 1,000 MEGA-OHMS OR HIGHER. THE READINGS SHOULD DECREASE WITH AGE. THE RESISTANCE VALUE DECLINES OVER THE SERVICE LIFE OF THE CIRCUIT; A 10-20 PERCENT DECLINE PER YEAR MAY BE CONSIDERED NORMAL. A YEARLY DECLINE OF 50 PERCENT (4 PERCENT MONTHLY) OR GREATER INDICATES THE EXISTENCE OF A PROBLEM, SUCH AS A HIGH RESISTANCE GROUND, SERIOUS DETERIORATION OF THE CIRCUIT INSULATION, LIGHTNING DAMAGE, BAD CONNECTIONS, BAD SPLICES, CABLE INSULATION DAMAGE, OR OTHER FAILURE. FAA ADVISORY CIRCULAR 150/5340-26C NOTES "GENERALLY SPEAKING, ANY CIRCUIT THAT MEASURES LESS THAN 1 MEGOHM IS CERTAINLY DESTINED FOR RAPID FAILURE." AIRFIELD LIGHTING SERIES CIRCUITS WITH CABLE INSULATION READINGS OF LESS THAN 1 MEGOHM ARE NOT UNCOMMON FOR OLDER CIRCUITS THAT ARE 20 YEARS OR MORE OF AGE.
- BASED ON INFORMATION IN FAA AC NO. 150/5340-26C MAINTENANCE OF AIRPORT VISUAL AID FACILITIES, THE CABLE INSULATION RESISTANCE VALUE INEVITABLY DECLINES OVER THE SERVICE LIFE OF THE CIRCUIT; A 10-20 PERCENT DECLINE PER YEAR MAY BE CONSIDERED NORMAL. IN THE EVENT THAT THE CABLE INSULATION RESISTANCE READINGS HAVE DECLINED MORE THAN 2 PERCENT PER MONTH IT MIGHT INDICATE CABLE DAMAGE DUE TO LIGHTNING OR DAMAGE AS A RESULT OF CONTRACTOR OPERATIONS. WHERE THE CABLE INSULATION RESISTANCE READINGS HAVE DECLINED MORE THAN 2 PERCENT PER MONTH OVER THE PROJECT CONSTRUCTION DURATION AS A RESULT OF CONTRACTOR OPERATIONS, CONTRACTOR WILL NEED TO INVESTIGATE, ADDRESS, AND REPAIR THE RESPECTIVE CABLE CIRCUITS.



**MEASURE RESISTANCE OF SERIES CIRCUIT LOOP.**  
NOT TO SCALE

**SERIES CIRCUIT LOOP RESISTANCE MEASUREMENT NOTES**

- PRIOR TO BEGINNING EXCAVATIONS, AIRFIELD LIGHTING MODIFICATIONS, CABLE INSTALLATION, AND/OR ANY OTHER WORK THAT MIGHT POSSIBLY AFFECT AIRFIELD LIGHTING CIRCUITS, THE RESPECTIVE SERIES CIRCUIT CABLE LOOPS SHALL HAVE THE RESISTANCE MEASURED WITH AN OHMMETER AND RECORDED FOR EACH CIRCUIT AT THE VAULT.
- AFTER AIRFIELD LIGHTING MODIFICATIONS, ADDITIONS, UPGRADES, AND/OR OTHER WORK AND ADDITIONS HAVE BEEN COMPLETED THE RESPECTIVE SERIES CIRCUIT CABLE LOOPS SHALL HAVE THE RESISTANCE MEASURED WITH AN OHMMETER AND RECORDED FOR EACH CIRCUIT AT THE VAULT.
- ALL EXISTING SERIES CIRCUIT CABLE LOOPS SHALL HAVE THE RESISTANCE MEASURED WITH AN OHMMETER AND RECORDED FOR EACH CIRCUIT AT THE VAULT. THE RESISTANCE OF THE SERIES CIRCUIT LOOP WITH CONNECTIONS USING #8 AWG COPPER CONDUCTOR SHOULD BE APPROXIMATELY 0.8 TO 1 OHM PER THOUSAND FEET OF CABLE LENGTH. THE RESISTANCE OF THE SERIES CIRCUIT LOOP WITH CONNECTIONS USING #6 AWG COPPER CONDUCTOR SHOULD BE APPROXIMATELY 0.5 TO 0.7 OHM PER THOUSAND FEET OF CABLE LENGTH. THE NUMBER OF SERIES CIRCUIT TRANSFORMERS AND CONNECTIONS WILL AFFECT THE OVERALL RESISTANCE OF THE SERIES CIRCUIT LOOP AND THEREFORE THE MEASUREMENTS MIGHT BE SLIGHTLY HIGHER THAN THE CALCULATED RESISTANCE FOR THE RESPECTIVE LENGTH OF CABLE.

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**FOR BID**





ST. LOUIS  
DOWNTOWN AIRPORT

BI-STATE DEVELOPMENT  
ST. LOUIS DOWNTOWN AIRPORT  
6100 Archview Drive  
Cahokia Heights, Illinois 62206

COVERING ELECTRICAL DESIGN



DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT  
TAXIWAY B, PHASE 3:  
CONSTRUCT FROM  
TAXIWAY B5 TO B7  
AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 14, 2025  
PROJECT NO: 23A0001D  
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DESIGN BY: KNL 3/5/2025  
DRAWN BY: CWS 3/7/2025  
REVIEWED BY: KNL 3/11/2025

SHEET TITLE

LEGEND PLATE AND  
SIGNAGE  
SCHEDULES - 2

**WARNING**

**Arc Flash and Shock Hazard  
Appropriate PPE Required**

NOMINAL VOLTAGE: 120/240 VAC,  
SINGLE-PHASE, 3-WIRE  
ARC FLASH BOUNDARY: 19 INCHES  
ARC FLASH PPE CATEGORY: 1

Refer to NFPA 70E for minimum PPE Requirements

ARC FLASH AND SHOCK HAZARD RISK LABEL

EXAMPLE OF ARC FLASH AND SHOCK HAZARD RISK LABEL FOR 120/240 VAC, SINGLE-PHASE, 3-WIRE PANELBOARD OR OTHER EQUIPMENT WHERE THE MAXIMUM AVAILABLE FAULT CURRENT IS LESS THAN 25,000 AMPS.

PROVIDE THESE LABELS FOR THE FOLLOWING EQUIPMENT:

1. VAULT MAIN SERVICE DISCONNECT.
2. VAULT TRANSFER SWITCH.
3. VAULT MAIN DISTRIBUTION PANEL "A".
4. VAULT MAIN DISTRIBUTION PANEL "B".
5. CONTROL PANEL FOR AIRFIELD NAVAIDS.
6. EACH RELAY INTERFACE PANEL FOR CCRS.
7. DOUBLE THROW SAFETY SWITCH FOR RUNWAY 5-23 CCRS.
8. DOUBLE THROW SAFETY SWITCH FOR RUNWAY 12L-30R CCRS.
9. DOUBLE THROW SAFETY SWITCH FOR RUNWAY 12R-30L CCRS.

NOTES:

1. SIGNS AND LABELS SHALL BE WEATHERPROOF AND/OR SUITABLE FOR THE RESPECTIVE ENVIRONMENT. SECURE WITH WEATHERPROOF ADHESIVE AND MACHINE SCREWS.
2. FURNISH & INSTALL A WEATHERPROOF WARNING LABEL FOR EACH SAFETY SWITCH, PANELBOARD, LOAD CENTER, CUTOUT, & CONTROL PANEL TO WARN PERSONS OF POTENTIAL ELECTRIC ARC FLASH HAZARDS, PER THE REQUIREMENTS OF NEC 110.16 "ARC-FLASH HAZARD WARNING".
3. FAULT CURRENT INFORMATION TO BE PROVIDED BY SERVING ELECTRIC UTILITY COMPANY OR FROM DATA OBTAINED FROM UTILITY TRANSFORMER NAMEPLATE. CONTACT PROJECT ENGINEER TO CONFIRM FAULT CURRENT CALCULATIONS.
4. CONTRACTOR SHALL PROVIDE APPROPRIATE LABELS ON ELECTRICAL EQUIPMENT, IN ACCORDANCE WITH NFPA 70E ARTICLE 130 WORK INVOLVING ELECTRICAL HAZARDS, PART 130.5 ARC FLASH RISK ASSESSMENT, (H) EQUIPMENT LABELING. WHERE MAXIMUM CALCULATED FAULT CURRENT EXCEEDS 25,000 AMPS CONTACT PROJECT ENGINEER.

"DANGER HIGH VOLTAGE KEEP OUT" LABELS, MARKINGS, AND/OR SIGNS ARE REQUIRED FOR EQUIPMENT RATED OVER 1000 VOLTS AC IN ACCORDANCE WITH THE FOLLOWING:

- 2020/2023 NEC 110.34(C) "LOCKED ROOMS OR ENCLOSURES".
- 2020 NEC 300.45 "DANGER SIGNS".
- 2023 NEC 305.12 "DANGER SIGNS".
- 2020/2023 NEC 314.72(E) "SUITABLE COVERS".
- 2020 NEC 490.35 (A) "HIGH-VOLTAGE EQUIPMENT".
- 2023 NEC 495.35 (A) "HIGH-VOLTAGE EQUIPMENT".
- AC 150/5340-26C "MAINTENANCE OF AIRPORT VISUAL AID FACILITIES".



"DANGER - HIGH VOLTAGE UNAUTHORIZED PERSONNEL KEEP OUT" SIGN

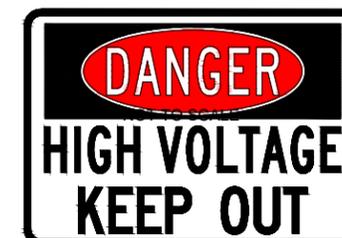
PROVIDE WEATHERPROOF WARNING SIGN FOR EACH DOOR TO AIRPORT ELECTRICAL VAULT LABELED "DANGER - HIGH VOLTAGE UNAUTHORIZED PERSONNEL KEEP OUT" PER THE REQUIREMENTS OF NEC 110.34 (C). SIGN SHALL BE APPROXIMATELY 10"H X 14"W OR 14" H X 20" W. NEW SIGNS SHALL REPLACE EXISTING SIGNS ON VAULT DOORS



CCR CHECK SIGN

NOT TO SCALE

PROVIDE ONE SIGN FOR EACH INTERIOR DOOR AT THE VAULT.



"DANGER - HIGH VOLTAGE KEEP OUT" SIGN

NOT TO SCALE

FURNISH AND INSTALL "DANGER - HIGH VOLTAGE KEEP OUT" LABELS/SIGNS FOR HIGH VOLTAGE SECTION OF CONSTANT CURRENT REGULATORS, SERIES CIRCUIT DISCONNECT/CUTOUT ENCLOSURES, HIGH VOLTAGE WIREWAYS, AND HIGH VOLTAGE PULL BOXES. LABELS SHALL BE MINIMUM 3.5"H X 5" W.

**FOR BID**



*Kevin N. Lightfoot*

DATE SIGNED: 3/14/2025 LICENSE EXPIRES: 11/30/2025

RELOCATE/RECONSTRUCT TAXIWAY B, PHASE 3: CONSTRUCT FROM TAXIWAY B5 TO B7 AND B1 INTERSECTION

IDA NO.: CPS-5078  
CONTRACT NO.: SD064

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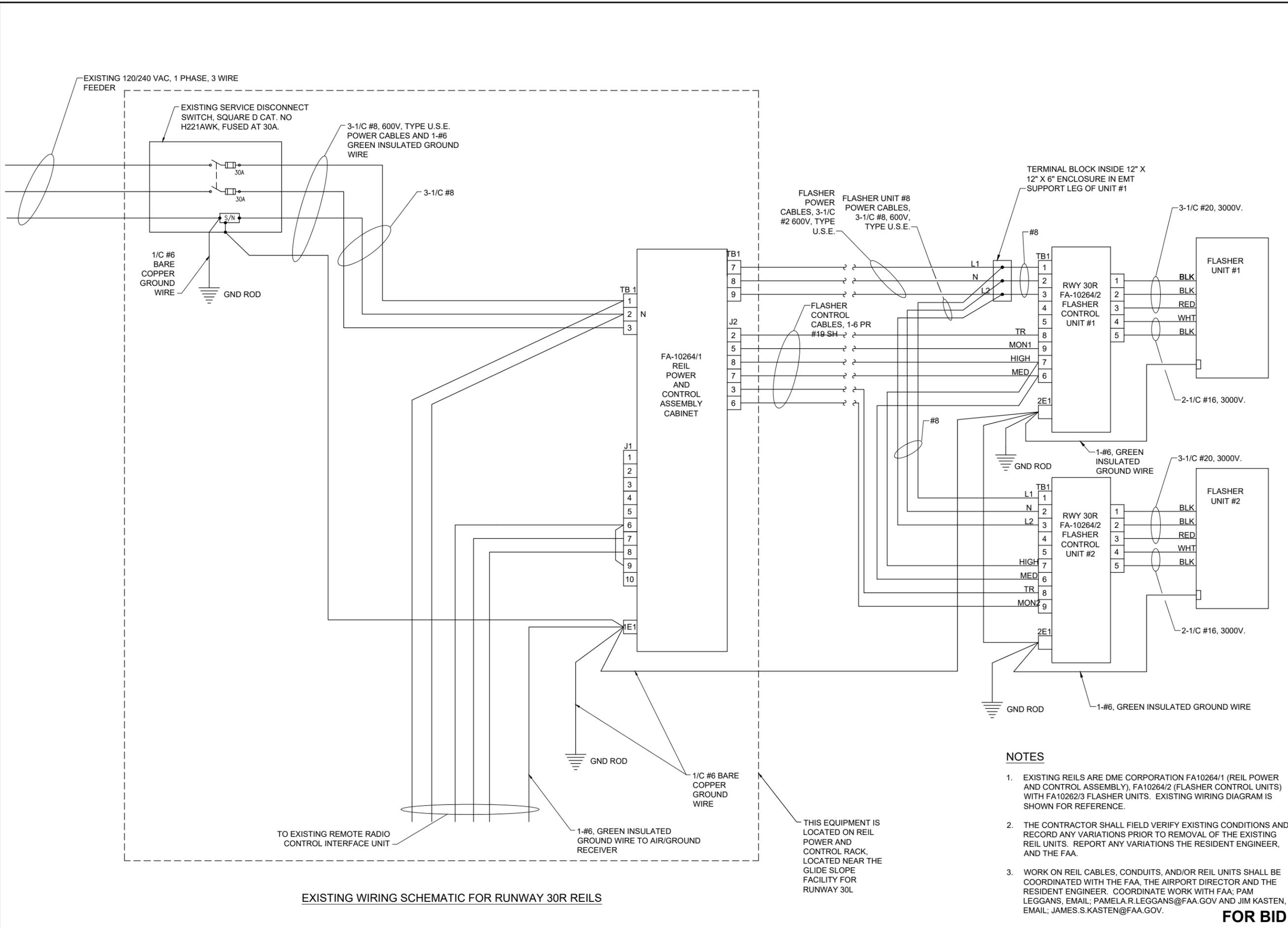
DESIGN BY: KNL 3/5/2025

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REVIEWED BY: KNL 3/11/2025

SHEET TITLE

EXISTING WIRING SCHEMATIC FOR RUNWAY 30R REILS



EXISTING WIRING SCHEMATIC FOR RUNWAY 30R REILS

**NOTES**

- EXISTING REILS ARE DME CORPORATION FA10264/1 (REIL POWER AND CONTROL ASSEMBLY), FA10264/2 (FLASHER CONTROL UNITS) WITH FA10262/3 FLASHER UNITS. EXISTING WIRING DIAGRAM IS SHOWN FOR REFERENCE.
- THE CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS AND RECORD ANY VARIATIONS PRIOR TO REMOVAL OF THE EXISTING REIL UNITS. REPORT ANY VARIATIONS THE RESIDENT ENGINEER, AND THE FAA.
- WORK ON REIL CABLES, CONDUITS, AND/OR REIL UNITS SHALL BE COORDINATED WITH THE FAA, THE AIRPORT DIRECTOR AND THE RESIDENT ENGINEER. COORDINATE WORK WITH FAA; PAM LEGGANS, EMAIL; PAMELA.R.LEGGANS@FAA.GOV AND JIM KASTEN, EMAIL; JAMES.S.KASTEN@FAA.GOV.

**FOR BID**

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