

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	2010-040RS	WILL	20	1
		ILLINOIS	CONTRACT NO. 60K97	

D-91-625-10



PROPOSED HIGHWAY PLANS

FAP RTE 607: US 6 / US 30 (CASS AVE)
MICHIGAN ST./MAYOR ART SCHULTZ DRIVE
TO 0.05 MI. E OF CASSEDAY AVE.
SECTION: 2010-040 RS
RESURFACING
WILL COUNTY

FOR INDEX OF SHEETS, SEE SHEET NO. 2

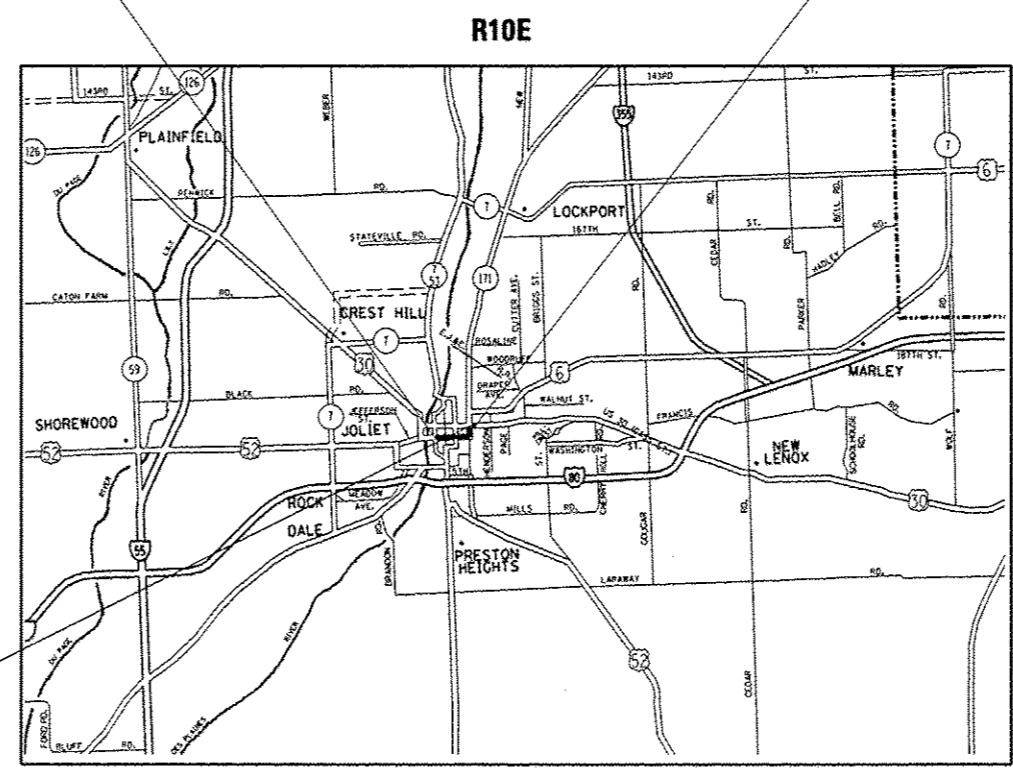
IMPROVEMENT IS LOCATED
IN THE CITY OF JOLIET

IMPROVEMENT BEGINS
STA 23+37.00

IMPROVEMENT ENDS
STA 58+00

TRAFFIC DATA

2011 ADT = 14100
POSTED SPEED = 30-40 MPH



OMISSIONS OF RESURFACING:
STA 49+78 to STA 52+15

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER BORO, ROBERT (847) 705 - 4237
PROJECT MANAGER RAYYAN, ISSAM (847) 705 - 4178

GROSS LENGTH OF IMPROVEMENT = 3463 FT = 0.656 MILES

CONTRACT NO. 60K97

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
SUBMITTED DECEMBER 12, 2012
[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 22, 2013
John D. Baranzelli, PE, Ia
ENGINEER OF DESIGN AND ENVIRONMENT
March 22, 2013
Omer Osman, PE, Ia
DIRECTOR OF HIGHWAYS, CHIEF/ENGINEER

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

LIST OF STATE STANDARDS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF JOLIET.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BEFORE BEGINING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND REVISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING, EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

THE ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC OPERATIONS ENGINEER AT (847) 705-4411 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

SIDEWALK REMOVAL AND P.C.C. SIDEWALK 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS
6-7	ROADWAY & PAVEMENT MARKING PLANS
8	EXISTING DETECTOR LOOP PLAN
9	DRIVEWAY DETAILS (BD-2)
10	FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)
11	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
12	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
13	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
14	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
15	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
16	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
17	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
18	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
19	ARTERIAL ROAD INFORMATION SIGNING (TC-22)
20	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-7)

STANDARD NO.	DESCRIPTION
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701427-01	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS < 40 MPH
701601-08	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-08	URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES

EXISTING PAVEMENT MARKINGS SHALL BE TEMPORARILY COVERED PRIOR TO APPLICATION OF THE DECK SURFACE TREATMENT, TO PREVENT THE MATERIAL FROM BEING APPLIED TO THE MARKINGS. THE TEMPORARY COVERING SHALL BE REMOVED AFTER APPLICATION OF THE DECK SURFACE TREATMENT AND PRIOR TO OPENING TO TRAFFIC. COST IS INCLUDED WITH THE DECK SURFACE TREATMENT SPECIFIED.

THE CONTRACTOR SHALL CONTACT THE CITY OF JOLIET, DEPARTMENT OF PUBLIC WORKS AT (815) 724-4200, FOR ALL LOOP DETECTOR REPLACEMENT INFORMATION PRIOR TO BEGINNING OF WORK.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES, THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

FILE NAME *	USER NAME * gorengautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 6 /US 30 (CASS AVE) (MICHIGAN ST./MAYOR ART SCHULTZ DR. TO EAST OF CASSEDAVE AVE) INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwwork\pwwork\gorengautab\d0252964	D162510-shit-plan.dgn	DRAWN -	REVISED -			607	2010-040RS	WILL	20	2	
	PLOT SCALE = 1/8"=1'-0"	CHECKED -	REVISED -			CONTRACT NO. 60K97					
	PLOT DATE = 12/17/2012	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

SUMMARY OF QUANTITIES			URBAN 100% STATE TOTAL QUANTITIES		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	0005						
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	15	15					
25200110	SODDING, SALT TOLERANT	SO YD	15	15					
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	4	4					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	13	13					
40600300	AGGREGATE (PRIME COAT)	TON	66	66					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	25	25					
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	615	615					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	232	232					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	13.3	13.3					
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	0.5	0.5					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1386	1386					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	38	38					
44000154	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"	SO YD	1600	1600					
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	14900	14900					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	4	4					

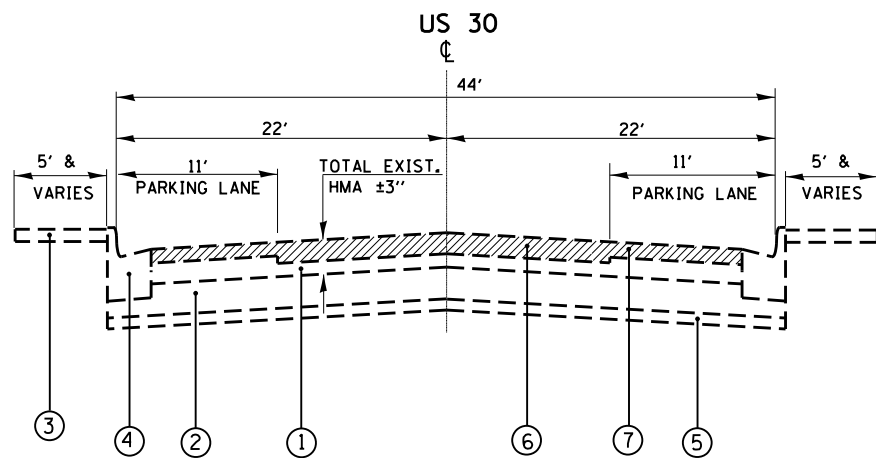
SUMMARY OF QUANTITIES			URBAN 100% STATE TOTAL QUANTITIES		CONSTRUCTION TYPE CODE 0005				
CODE NO	ITEM	UNIT	0005						
44000600	SIDEWALK REMOVAL	SO FT	38	38					
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SO YD	79	79					
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	50	50					
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	12.5	12.5					
50300300	PROTECTIVE COAT	SO YD	21	21					
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1					
60266600	VALVE BOXES TO BE ADJUSTED	EACH	5	5					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	5	5					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3					
67100100	MOBILIZATION	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	895	895					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	181.5	181.5					

FILE NAME: <i>16</i>	USER NAME: <i>gorenca/00</i>	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 6 / US 30 (CASS AVE) (MICHIGAN ST/MAYOR ART SCHULTZ DR. TO EAST OF CASSEDAY AVE) SUMMARY OF QUANTITIES	F.A.P. RTE. 607	SECTION 2040-0406	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 3	
PLT SCALE: 1/4" = 100'	PLT DATE: 12/17/2012	DRAWN: -	REVISED: -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 60K97		
		CHECKED: -	REVISED: -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
		DATE: -	REVISED: -								

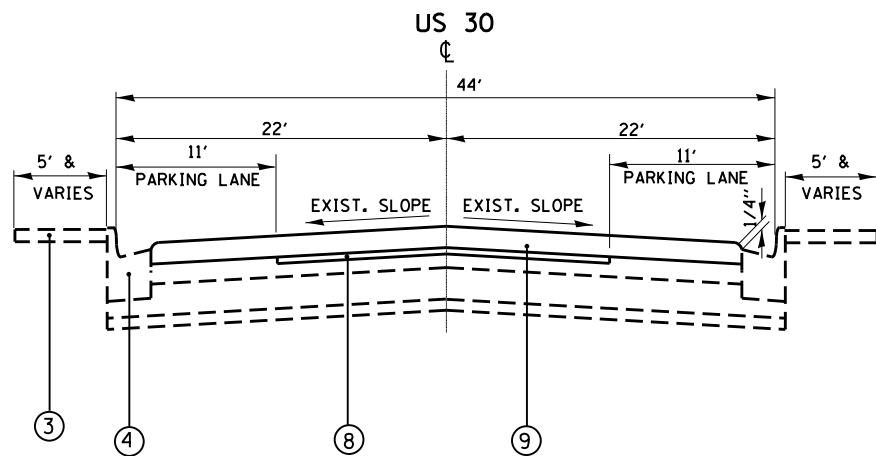
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0005					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0005				
CODE NO	ITEM	UNIT	100% STATE TOTAL QUANTITIES	0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	5790	5790					X 88600600	DETECTOR LOOP REPLACEMENT	FOOT	500	500				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	880	880					X 89502376	REBUILD EXISTING HANDHOLE	EACH	2	2				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	130	130					X4404700	SIDEWALK REMOVAL (SPECIAL)	SO FT	23	23				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	860	860					X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	50	50				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	330	330					Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	95	95				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	305	305					Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	128.5	128.5				
X 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	181.5	181.5													
X 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5790	5790													
X 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	880	880													
X 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	130	130													
X 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	860	860													
X 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	330	330													
X 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	205	205													
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	110	110													

* Specialty Items

FILE NAME :	USER NAME : gorenqat08	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 6 / US 30 (CASS AVE) (MICHIGAN ST/MAYOR ART SCHULTZ DR. TO EAST OF CASSEDAY AVE) SUMMARY OF QUANTITIES	F.A. R.T.E. 607	SECTION 2010-040R5	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 4
CONTRACT NO. 60K97	SCALE:	SHEET NO. OF SHEETS	STA. TO STA.			FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT			
DESIGNED -	REVISED -	CHECKED -	REVISED -			DATE -	REVISED -			
PLOT SCALE : 100,000' / 1" = 100'	PLOT DATE : 12/17/2012									



TYPICAL EXISTING CROSS SECTION
US ROUTE 30 (JEFFERSON ST)
STA. 23+37 TO STA. 30+88

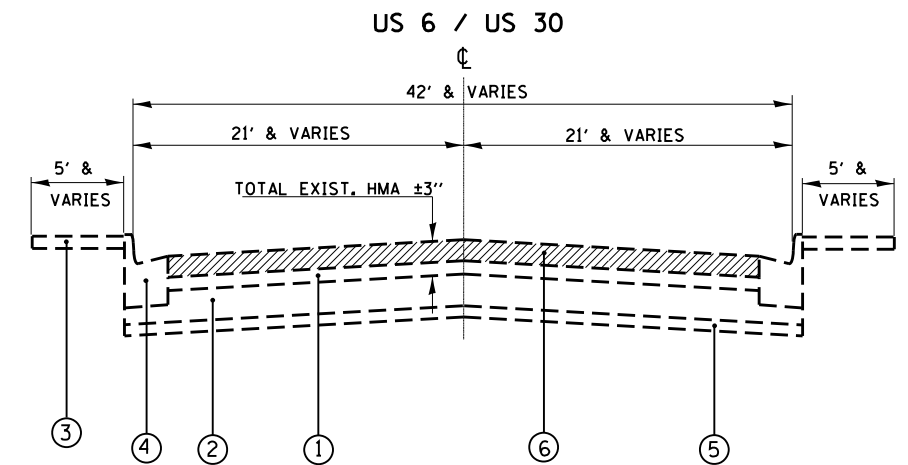


PROPOSED TYPICAL CROSS SECTION
US ROUTE 30 (JEFFERSON ST)
STA. 23+37 TO STA. 30+88

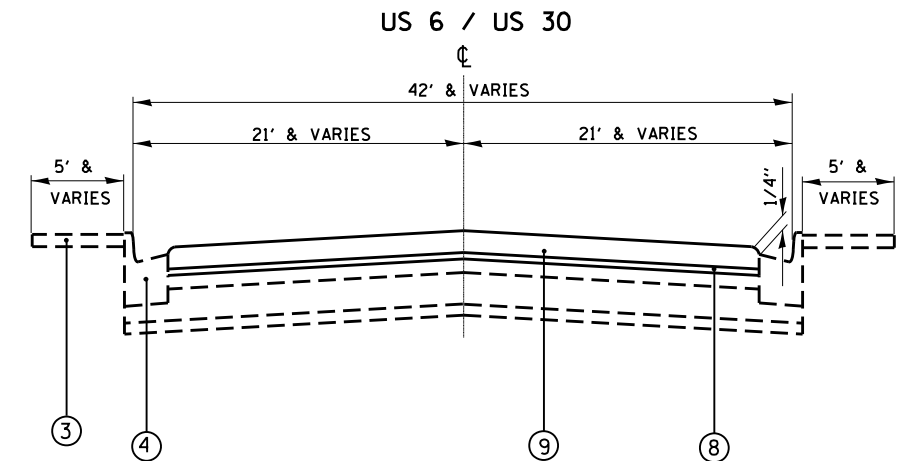
LEGEND

- ① EXISTING HMA SURFACE COURSE, ±3"
- ② EXISTING P.C.C. BASE COURSE ±10"
- ③ EXISTING SIDEWALK
- ④ EXISTING COMBINATION CONCRETE CURB & GUTTER
- ⑤ EXIST. SUB-BASE GRANULAR MATERIAL, ±4"
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 "
- ⑦ PROPOSED HMA SURFACE REMOVAL, 1 1/4"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (M M), IL -4.75, N 50, 3/4"
- ⑨ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

THE CONTRACTOR SHALL PATCH
FIRST BEFORE MILLING



TYPICAL EXISTING CROSS SECTION
US 6 (COLLINS ST) / US 30 (JEFFERSON & CASS ST)
STA. 30+88 TO STA. 58+00



PROPOSED TYPICAL CROSS SECTION
US 6 (COLLINS ST) / US 30 (JEFFERSON & CASS ST)
STA. 30+88 TO STA. 58+00

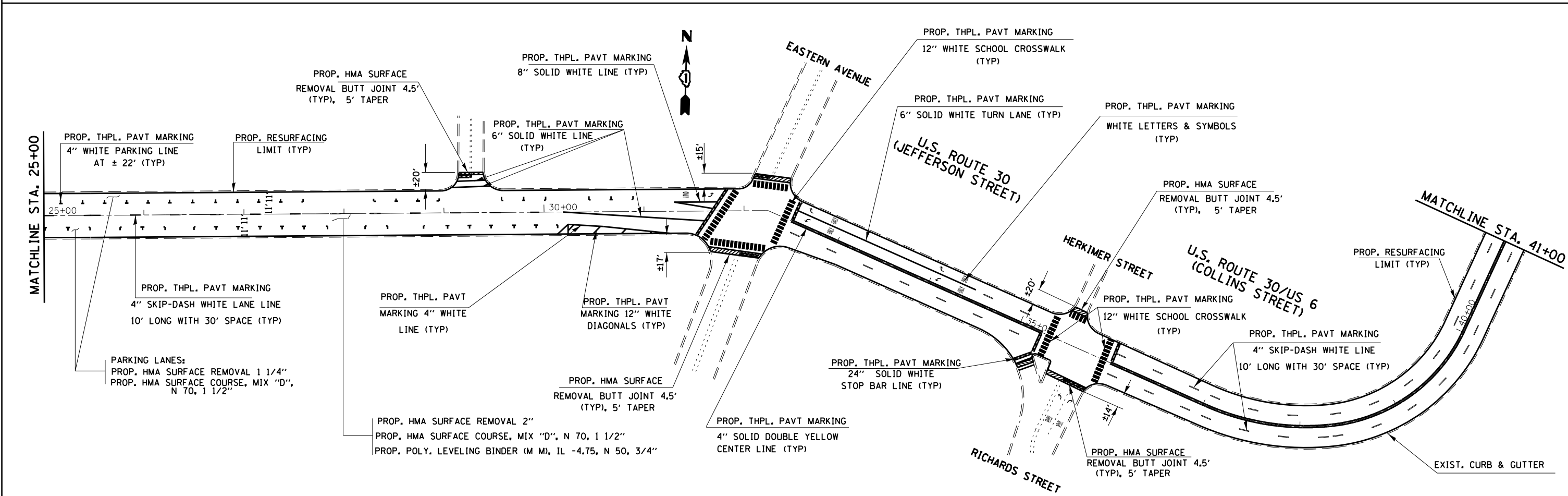
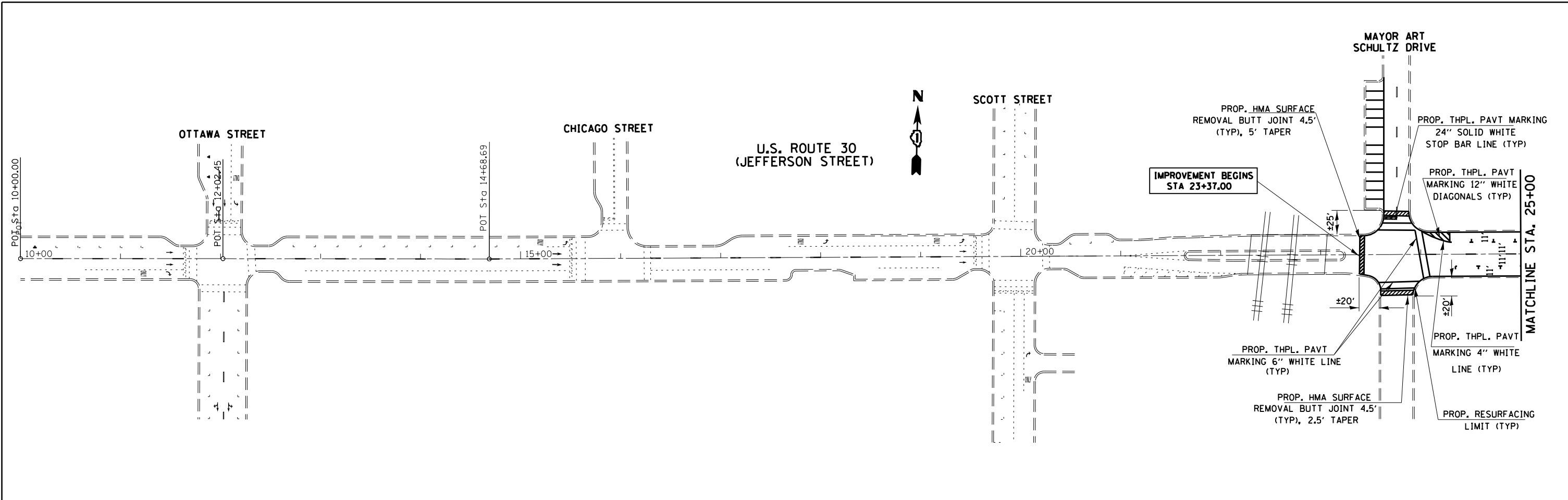
HMA MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS & Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70, (IL 9.5 mm)	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (M M), IL -4.75, N50	3.5% @ 50 GYR
PATCHING	
CLASS D PATCHES, (HMA BINDER IL-19mm)	4% @ 70 GYR
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR
DRIVEWAY	
HMA SURFACE COURSE, MIX D, N50, (IL 9.5 mm); 2"	4% @ 50 GYR
HMA BASE COURSE (HMA BINDER IL -19mm); CE -8"	4% @ 50 GYR

NOTE:

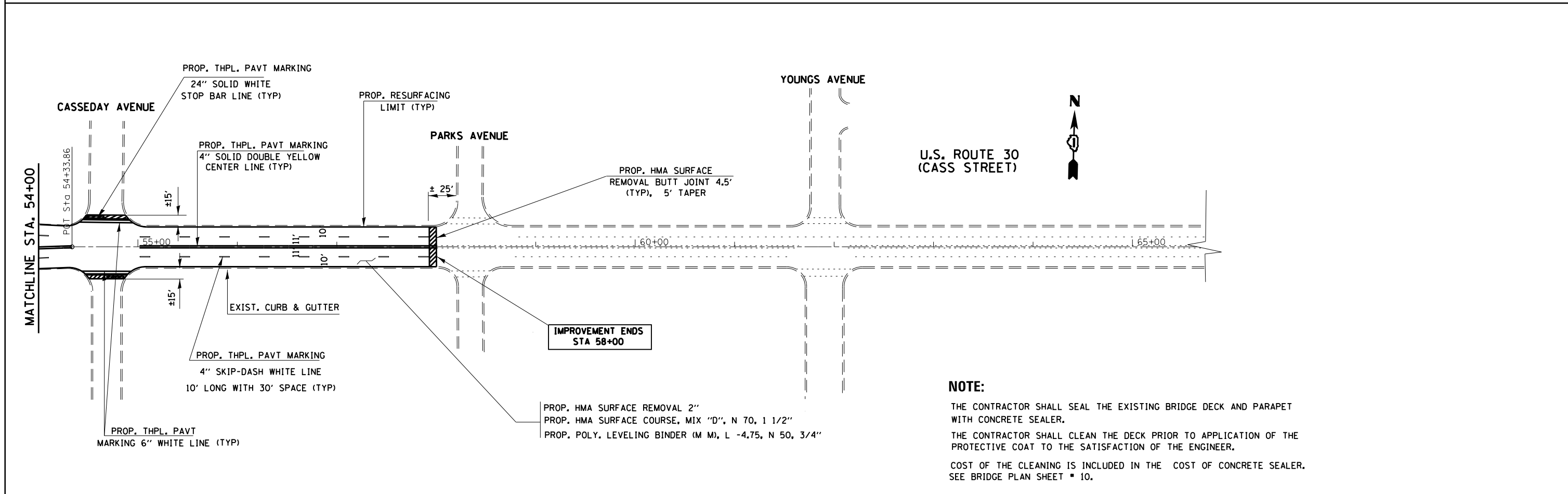
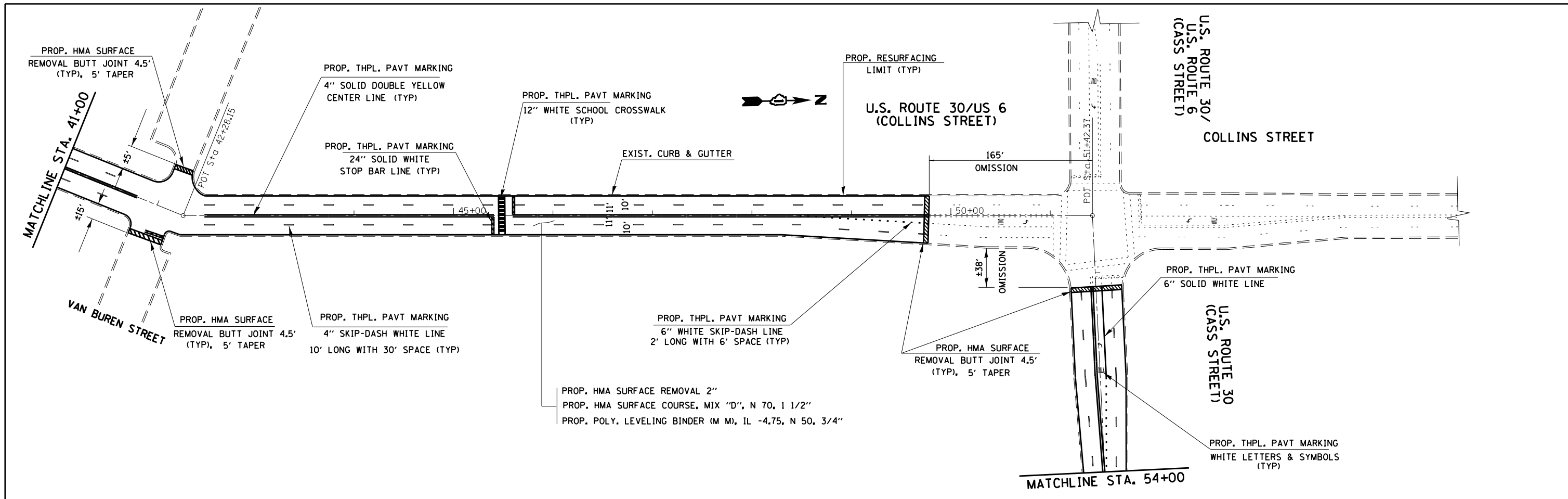
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ. YD. / IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 76 -22" AND FOR NON POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

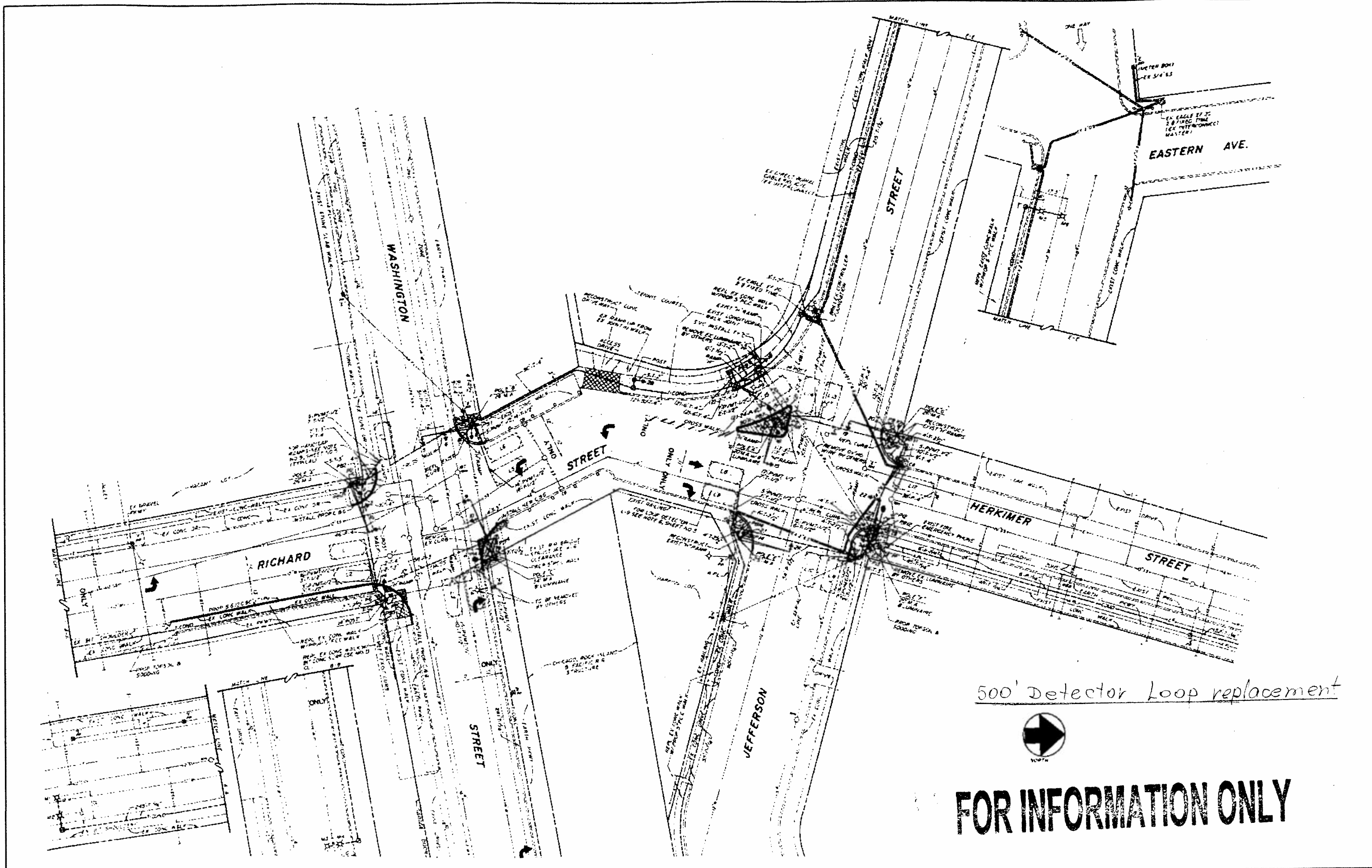


FILE NAME = D162510-sht-plan.dgn	USER NAME = gorengautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 6 /US 30 (CASS AVE) (MICHIGAN ST/MAYOR ART SCHULTZ DR. TO EAST OF CASSEDAY AVE) PROPOSED ROADWAY & PAVEMENT MARKING PLAN		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	607	2010-040RS	WILL	20
PLOT DATE = 12/17/2012	DATE -	REVISED -	REVISED -											



NOTE:
 THE CONTRACTOR SHALL SEAL THE EXISTING BRIDGE DECK AND PARAPET WITH CONCRETE SEALER.
 THE CONTRACTOR SHALL CLEAN THE DECK PRIOR TO APPLICATION OF THE PROTECTIVE COAT TO THE SATISFACTION OF THE ENGINEER.
 COST OF THE CLEANING IS INCLUDED IN THE COST OF CONCRETE SEALER. SEE BRIDGE PLAN SHEET # 10.

FILE NAME = D162510-sht-plan.dgn	USER NAME = gorengautab	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 6 / US 30 (CASS AVE) (MICHIGAN ST./MAYOR ART SCHULTZ DR. TO EAST OF CASSEDAY AVE) PROPOSED ROADWAY & PAVEMENT MARKING PLAN				F.A.P. RTE. 607	SECTION 2010-040RS	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 7
PLOT SCALE = 100.3282' / in.	CHECKED -	REVISED -	REVISED -		SCALE: 1" = 50'	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60K97		
PLOT DATE = 12/17/2012	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT								

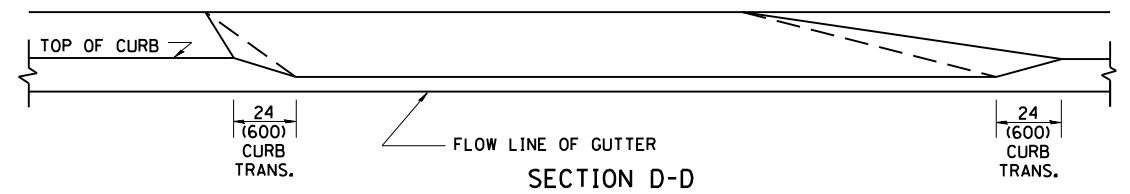
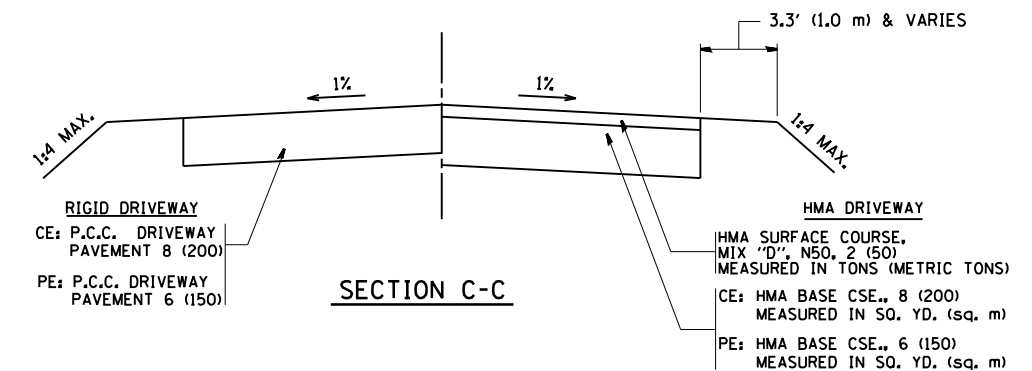
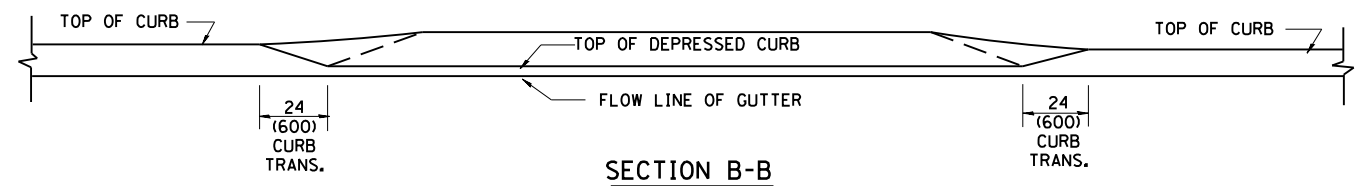
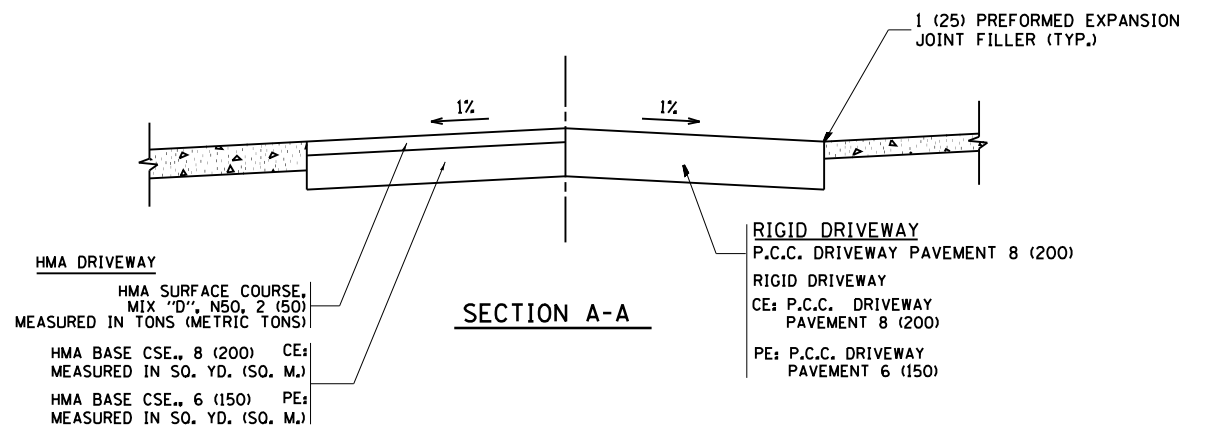
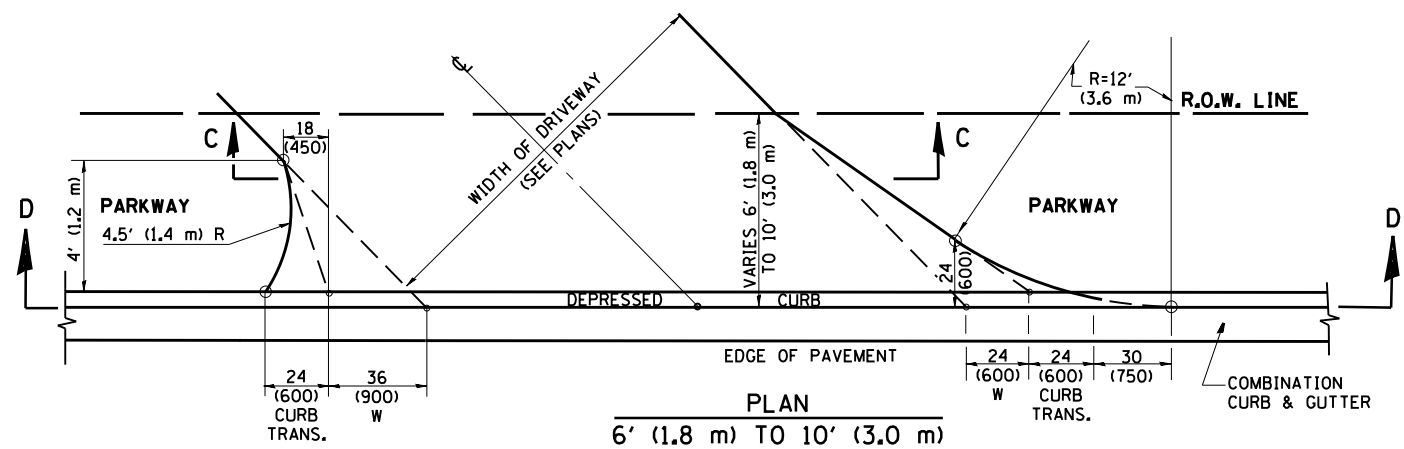
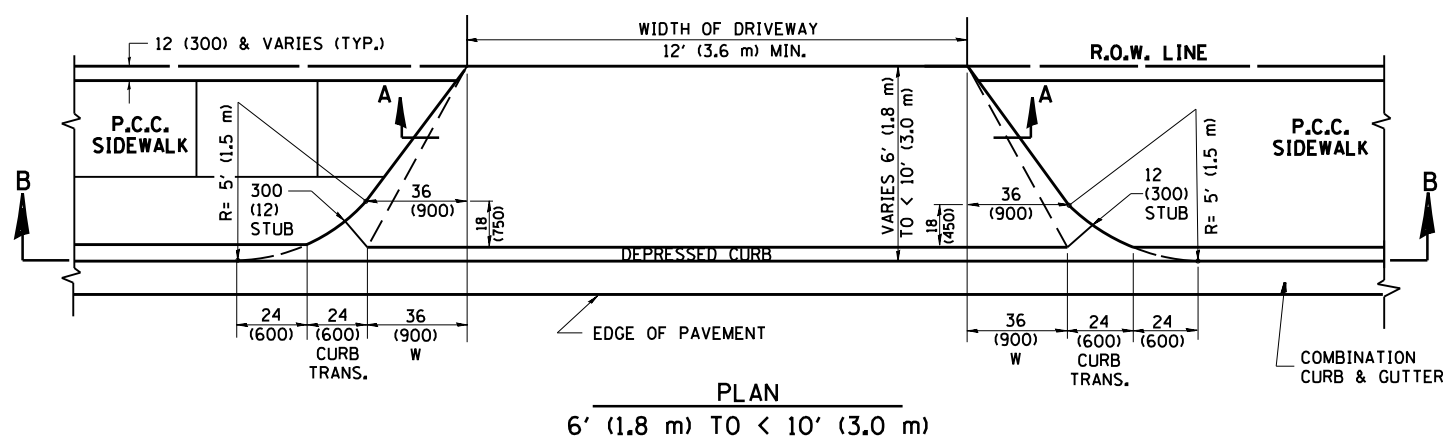
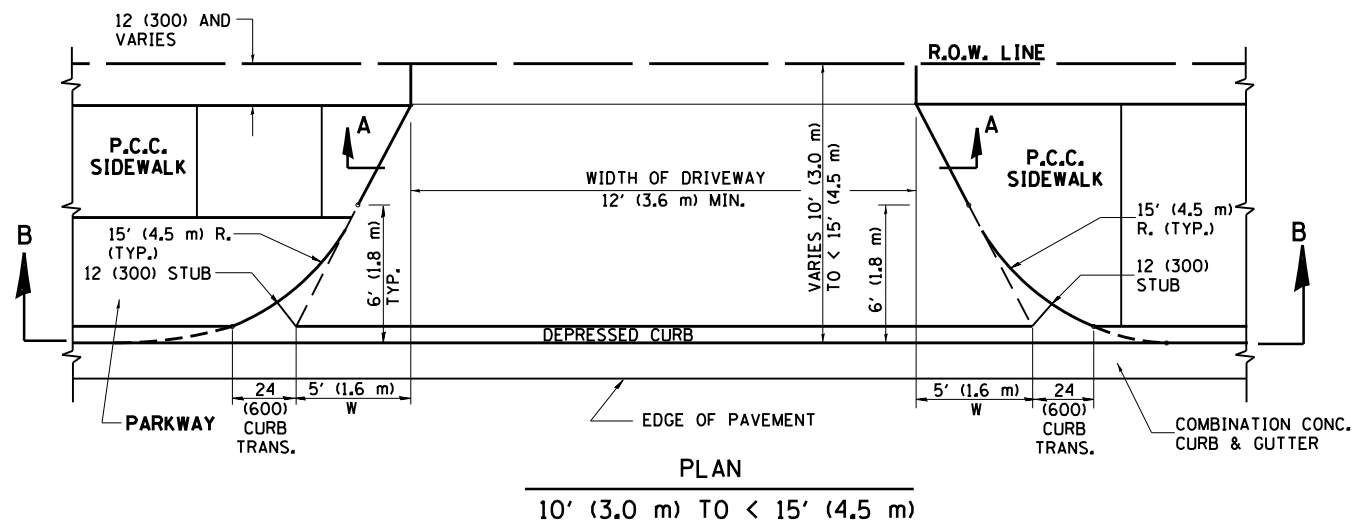


500' Detector Loop replacement



FOR INFORMATION ONLY

FILE NAME : c:\p\work\p1dat\gorenauteb\0252764	USER NAME : gorenauteb	DESIGNED : DRAWN :	REVISED : REVISED :	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 6 /US 30 (CASS AVE) (EAST OF SCOTT STREET TO EAST OF WALNUT STREET) EXISTING DETECTOR LOOP PLAN				F.A.P. RTE. 607	SECTION 2010-040RS	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 8
PLOT SCALE : 1/8"=1'-0"		CHECKED :	REVISED :		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			
PLOT DATE : 10/11/2012		DATE :	REVISED :										



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

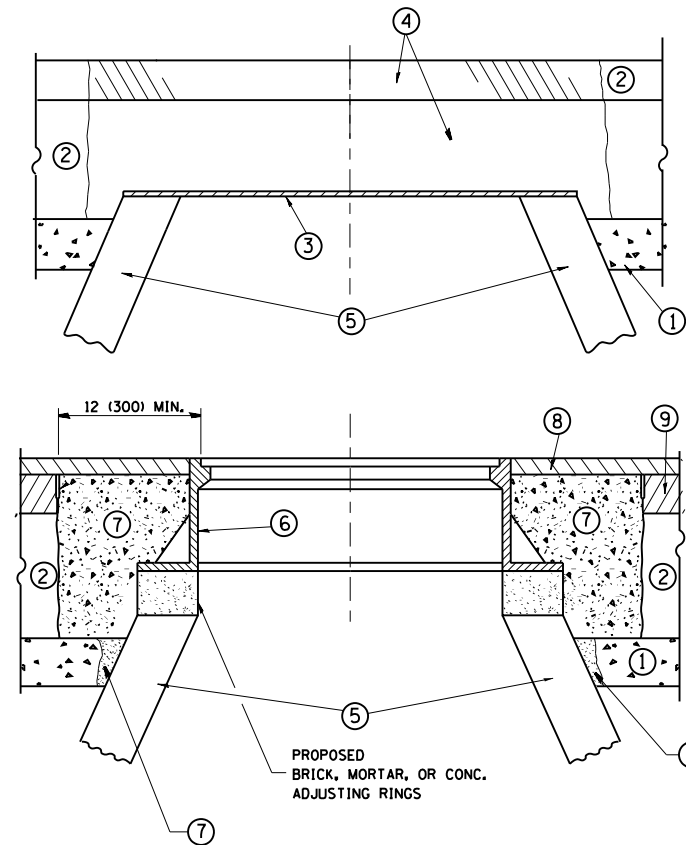
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = gorengautab	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
et:\pwork\pwork\gorengautab\d0252964\DistStd.dgn		DRAWN -	REVISED - P. LOFLEUR 04-15-03
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 12/17/2012	DATE - 11-06-95	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 607	SECTION 2010-040RS	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 9
BD400-02 (BD-02)		CONTRACT NO. 60K97		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

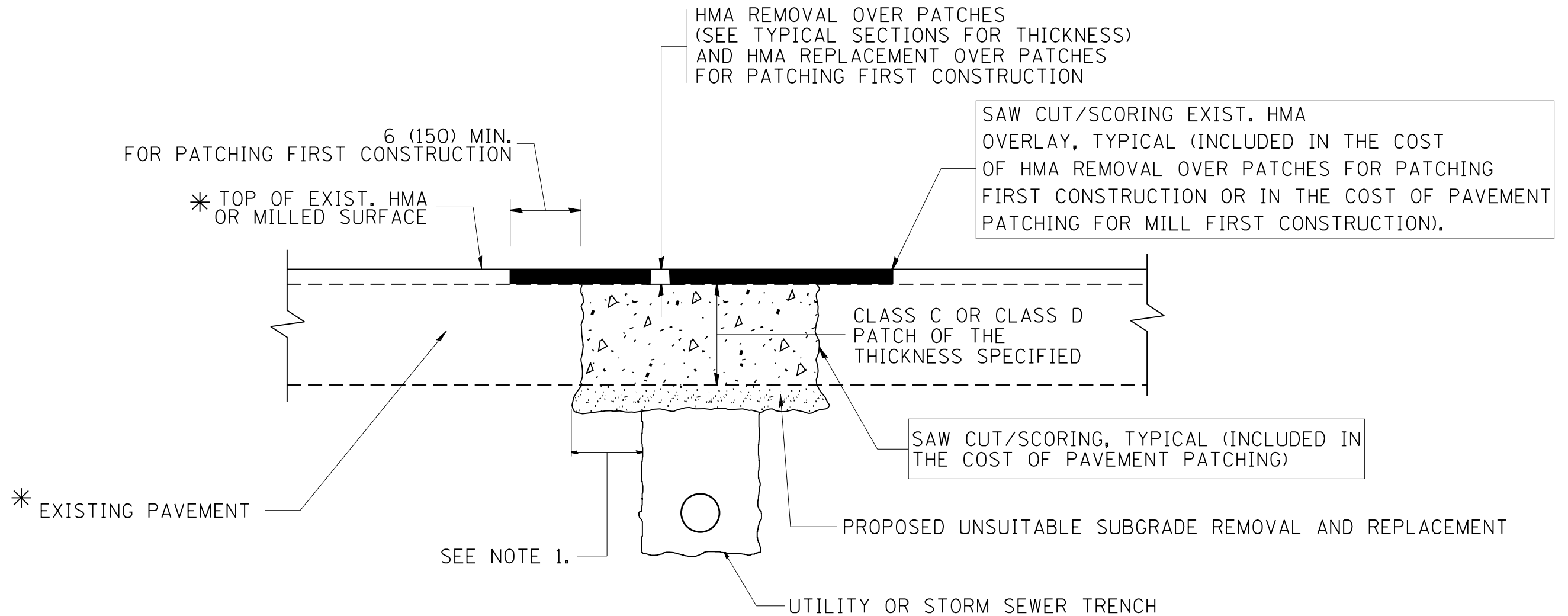
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gorengautab	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/17/2012	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	2010-040RS	WILL	20	10
BD600-03 (BD-8)		CONTRACT NO. 60K97		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

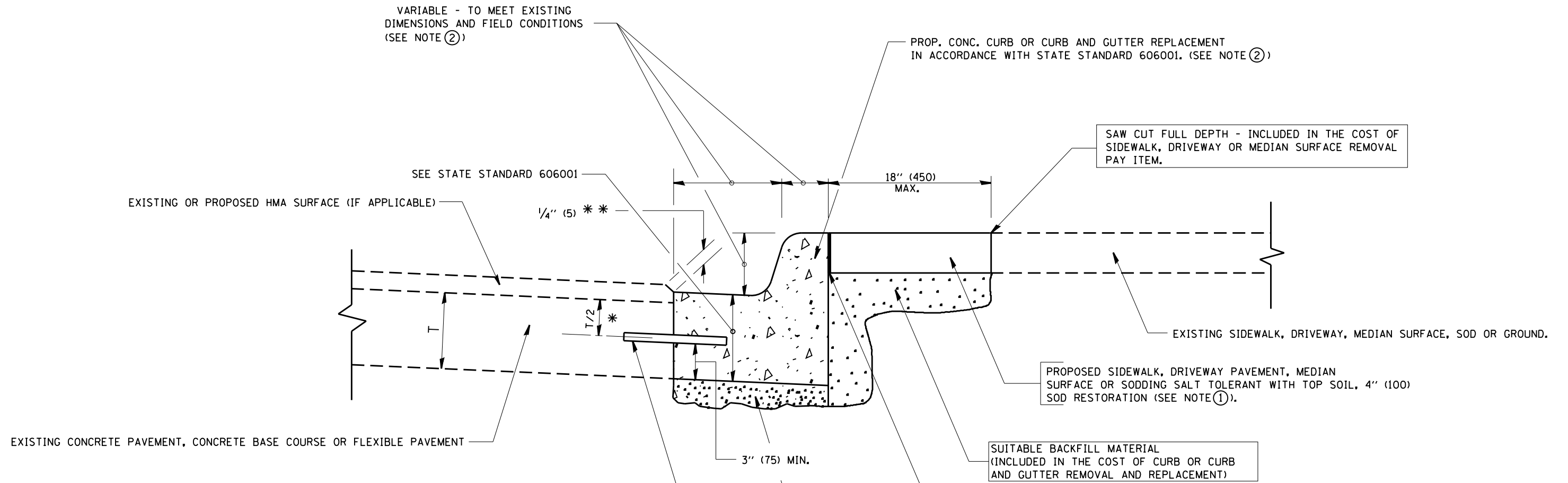
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\gorengautab\d0252964\DistStd.dgn		DRAWN -	REVISED - R. BORO 01-01-07					607	2010-040RS	WILL	20	11
	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 60K97				
	PLOT DATE = 12/17/2012	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
 - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

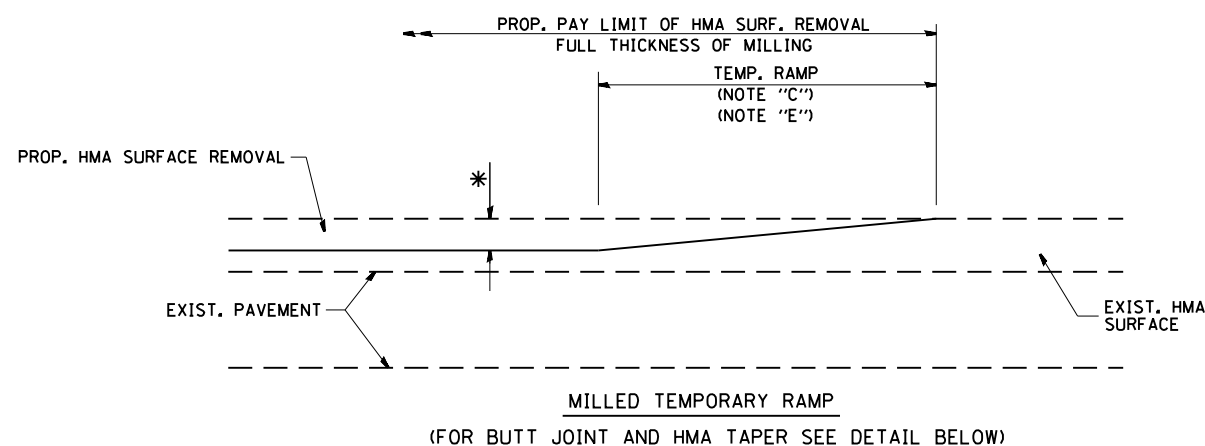
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

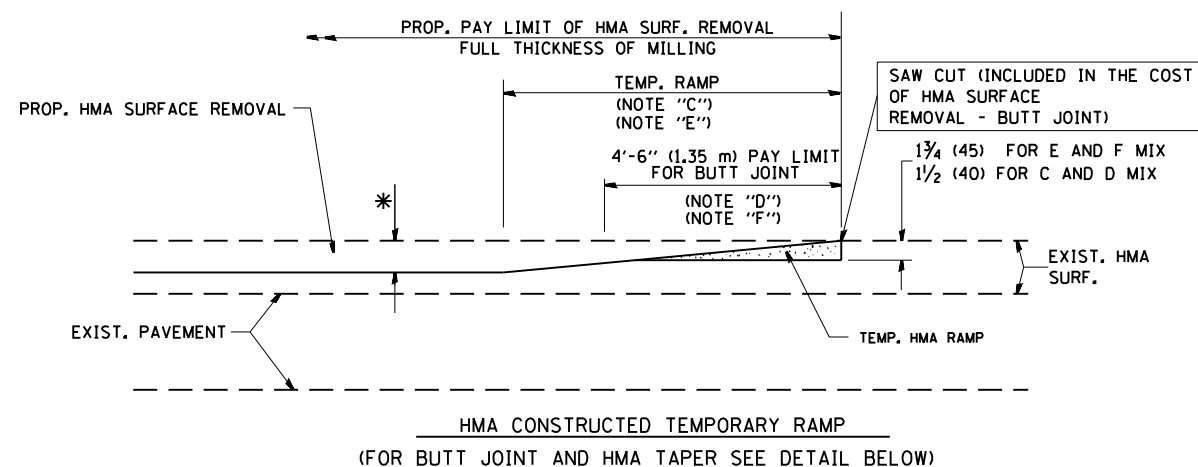
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A. RTE. 607	SECTION 2010-040RS	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 12		
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PLOT DATE = 12/17/2012	DATE - 03-11-94	REVISED - R. BORO 12-15-09				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

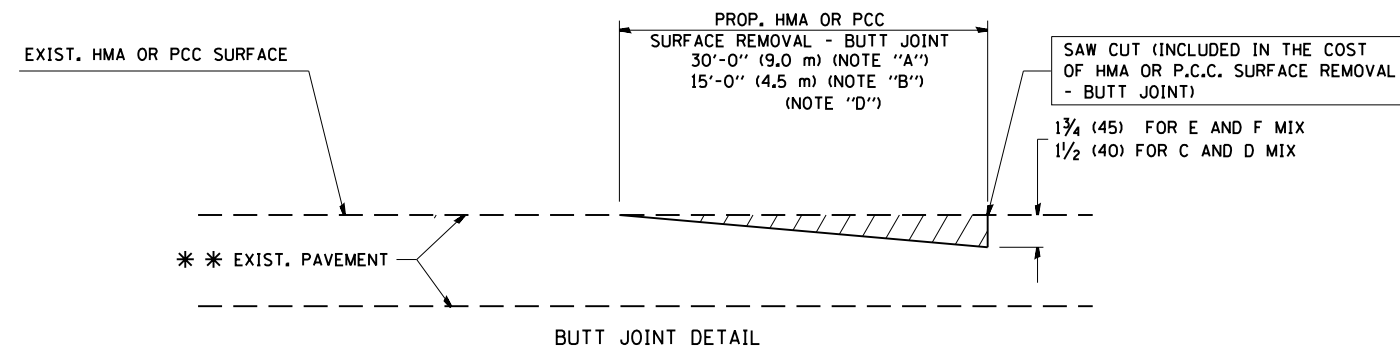


OPTION 1

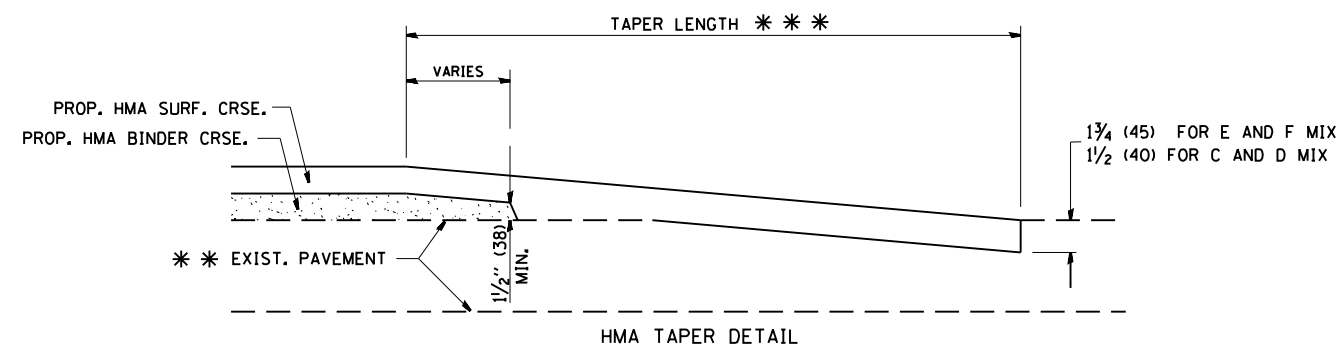


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

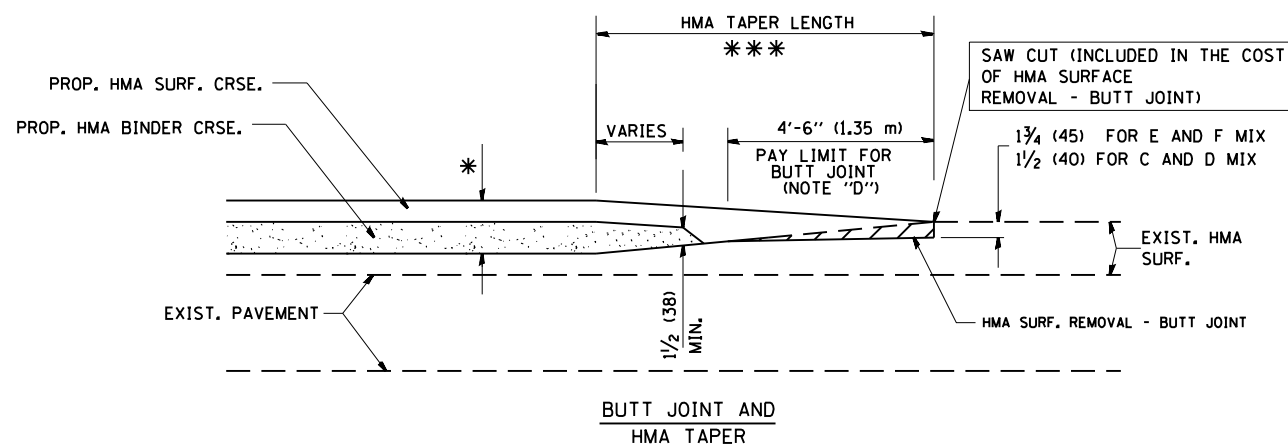
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



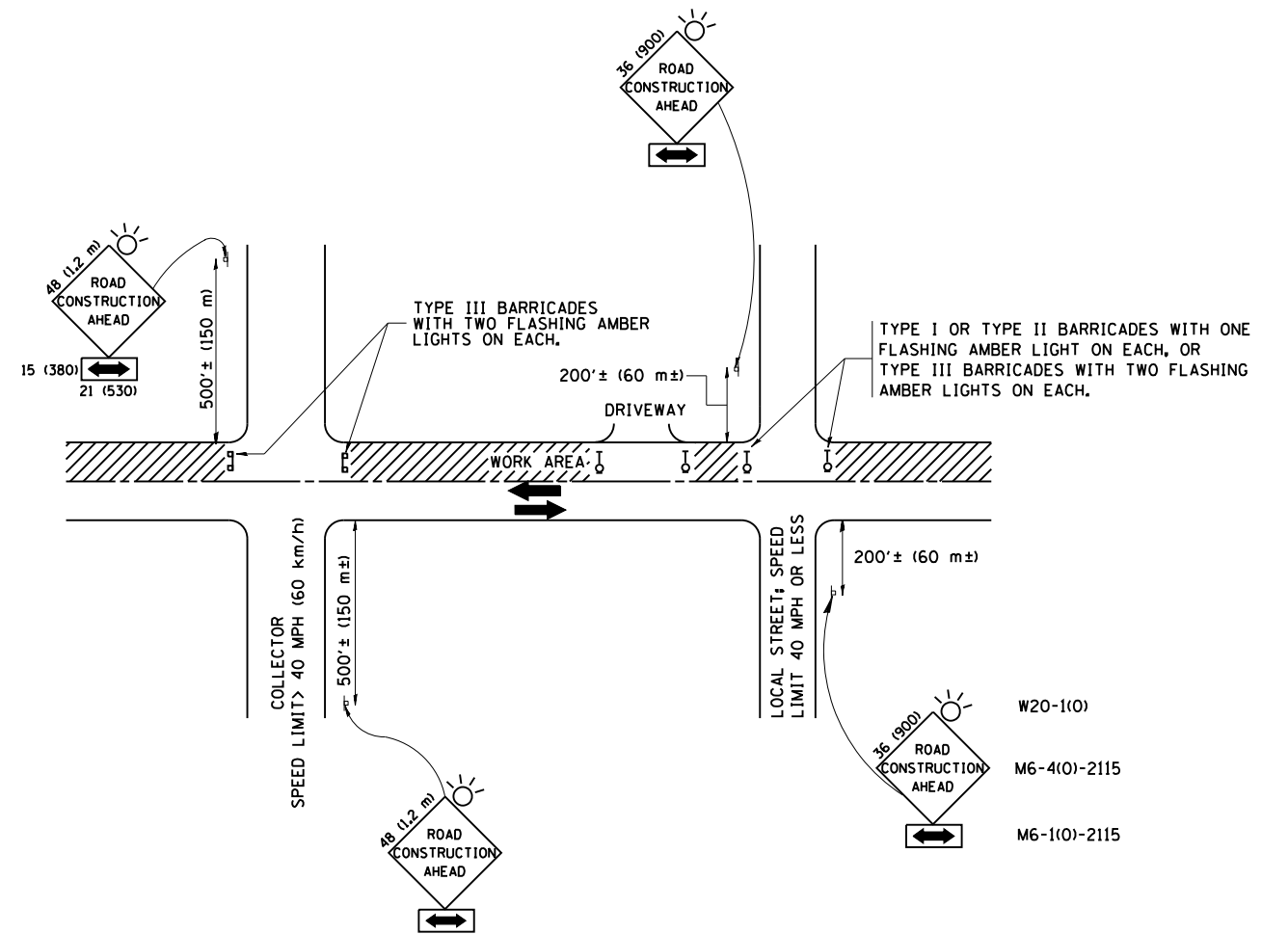
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = gorengautab	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 12/17/2012	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	2010-040RS	WILL	20	13
BD400-05 BD32		CONTRACT NO. 60K97		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

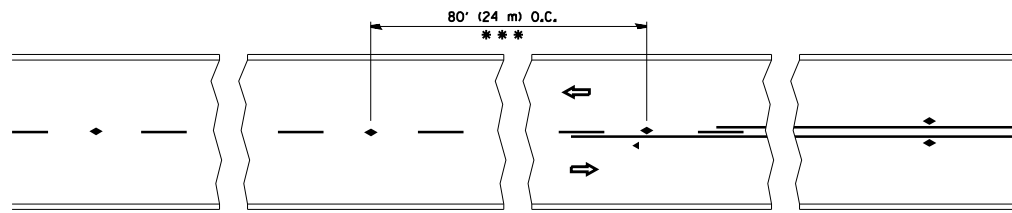
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 12/17/2012	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

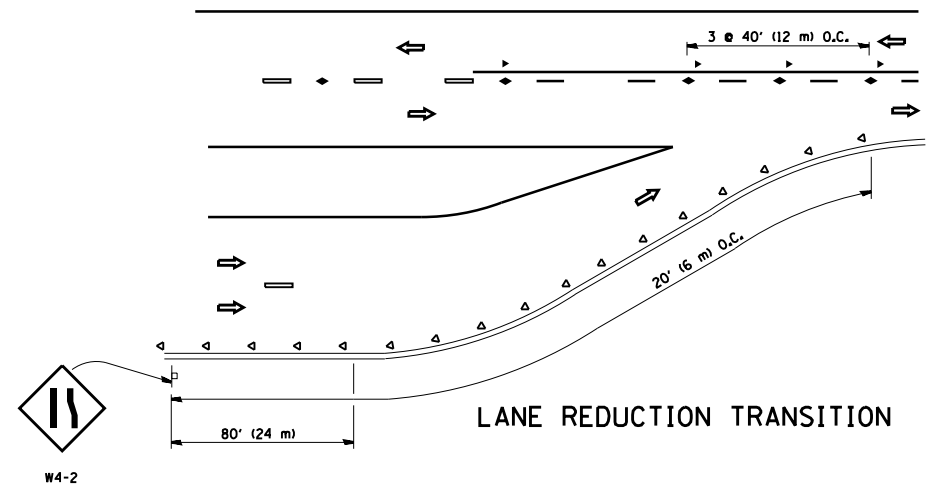
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F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10		CONTRACT NO. 60K97		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

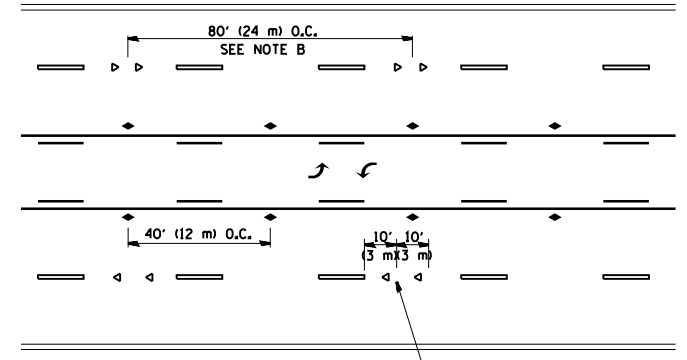


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

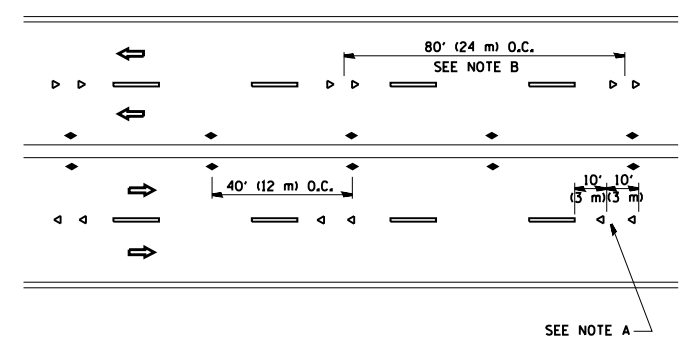
TWO-LANE/TWO-WAY



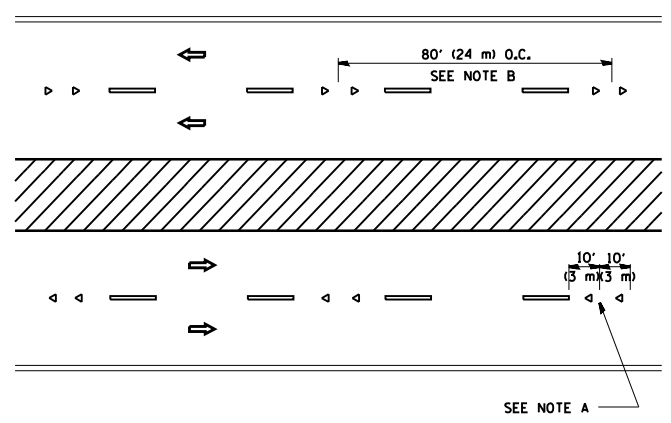
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

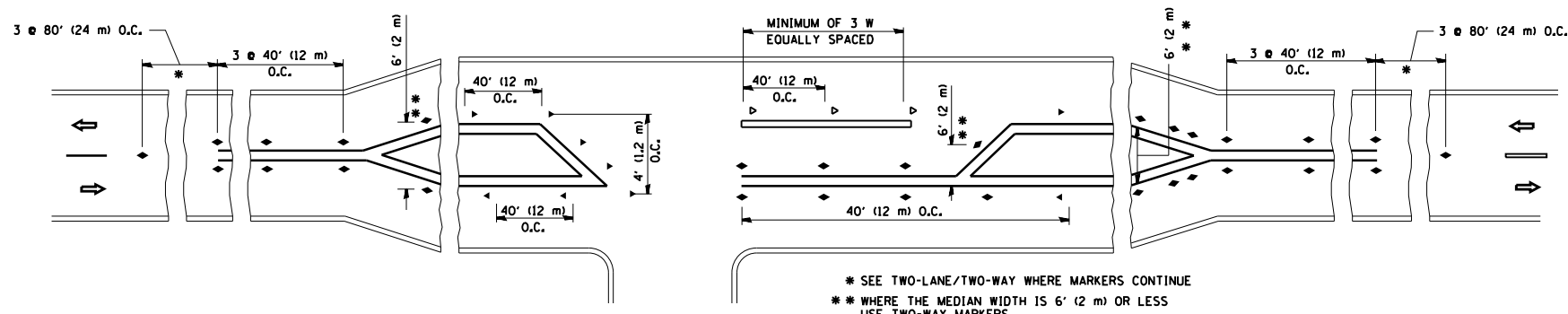
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

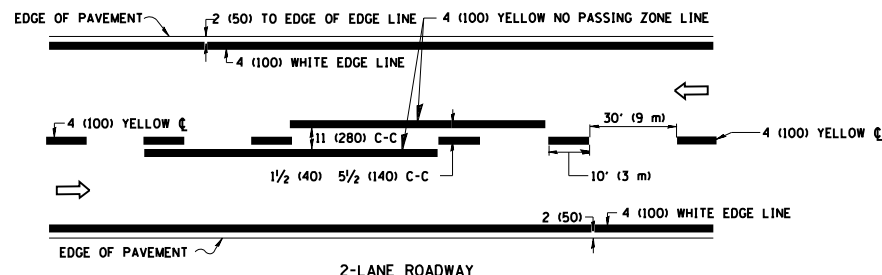
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



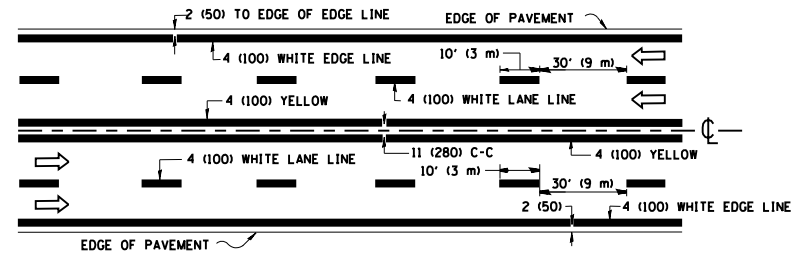
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

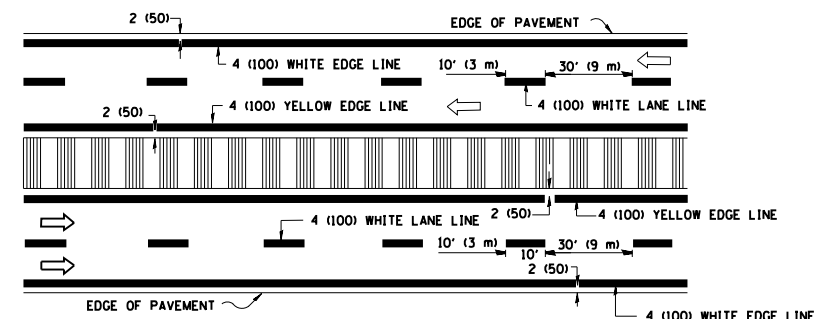
FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\gorengautab\d0252964\DistStd.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			607	2010-040RS	WILL	20	15
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 60K97	
	PLOT DATE = 12/17/2012	DATE -	REVISED - C. JUCIUS 09-09-09								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



2-LANE ROADWAY



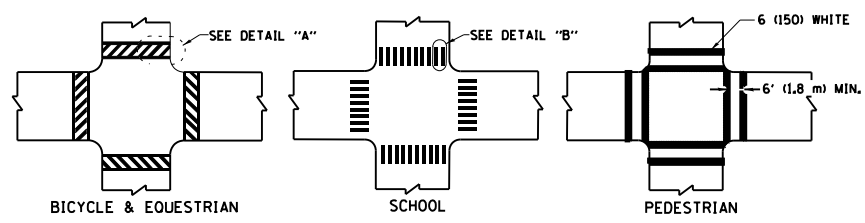
MULTI-LANE UNDIVIDED



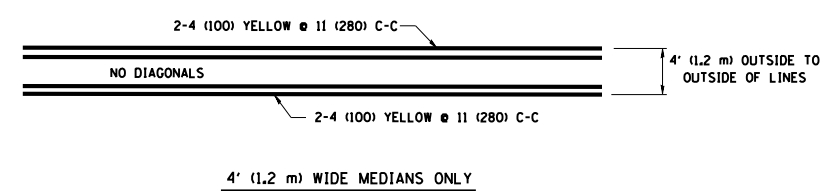
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

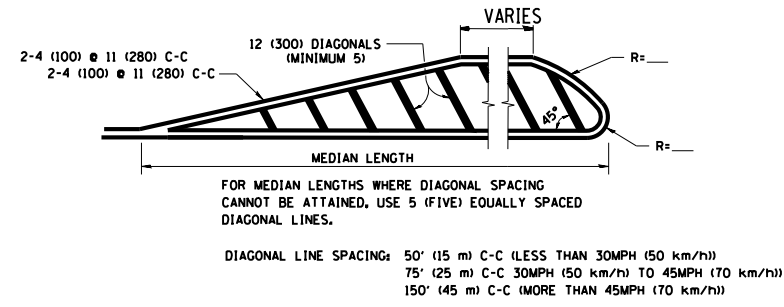
TYPICAL LANE AND EDGE LINE MARKING



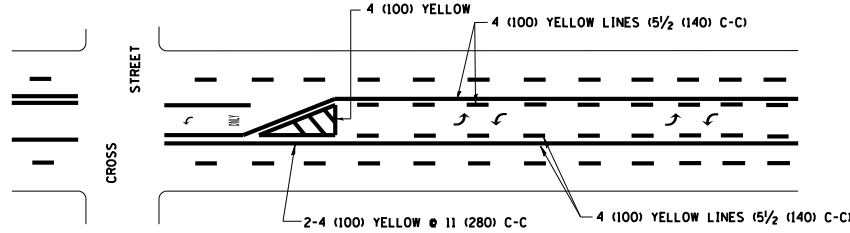
TYPICAL CROSSWALK MARKING



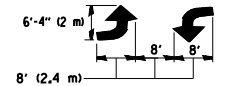
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

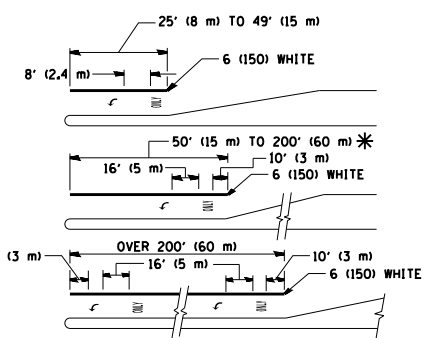


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL LEFT (OR RIGHT) TURN LANE

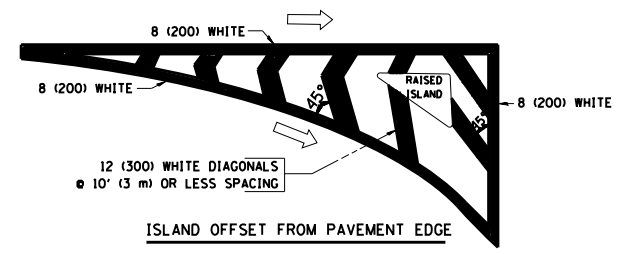
TYPICAL PAINTED MEDIAN MARKING



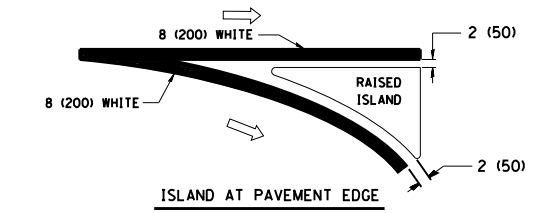
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

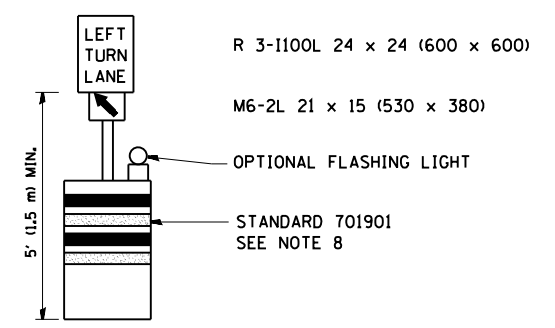
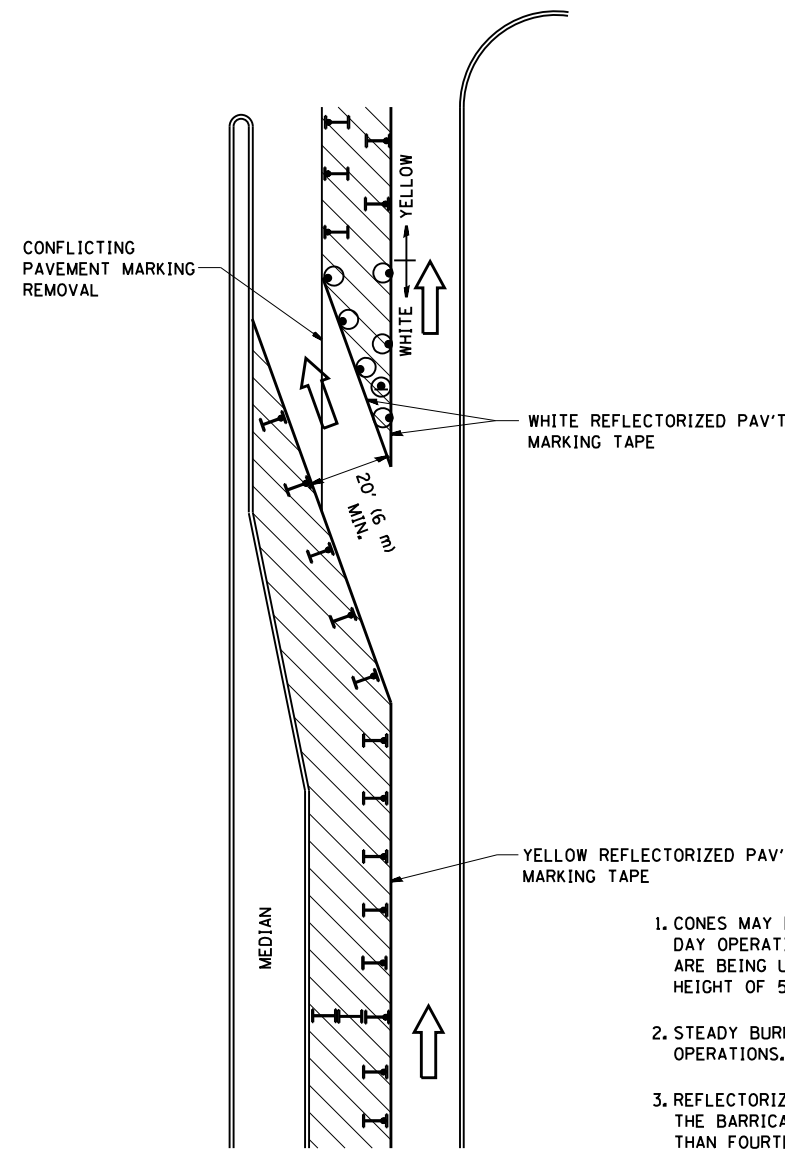
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = gorengautab	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
et:\pwork\pwork\dor\gorengautab\d0252964\DistStd.dgn	DistStd.dgn	DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2012	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.R. RTE. 607	SECTION 2010-040RS	COUNTY WILL	TOTAL SHEETS 20	SHEET NO. 16
TC-13		CONTRACT NO. 60K97		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

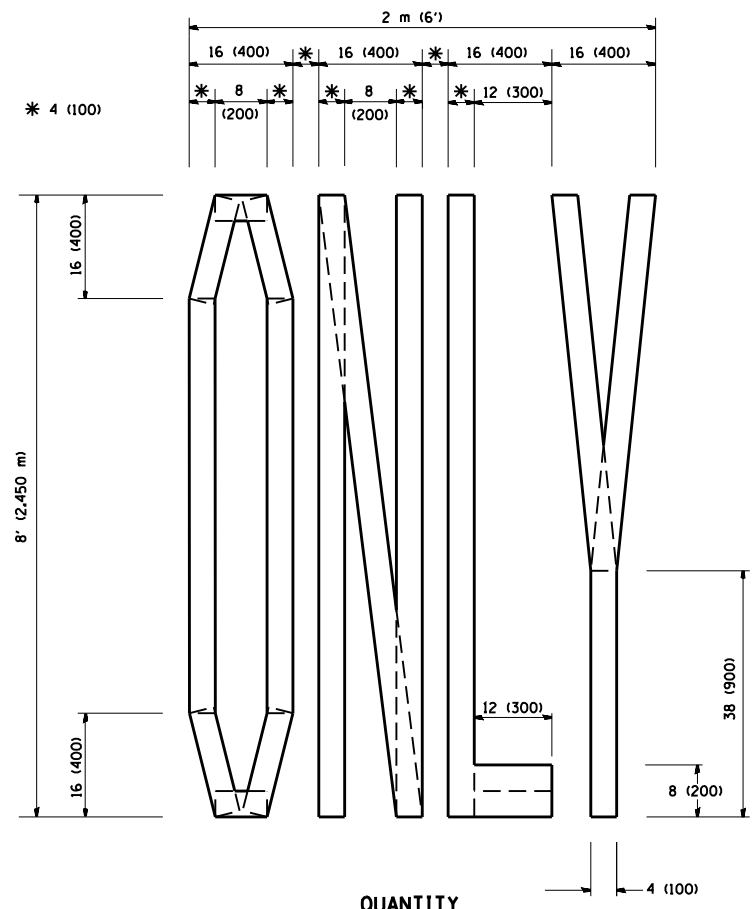
- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = gorengautab	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
et:\pwork\work\pwork\gorengautab\d0252964\DistStd.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 12/17/2012	REVISED - T. RAMMACHER 01-06-00	REVISED -

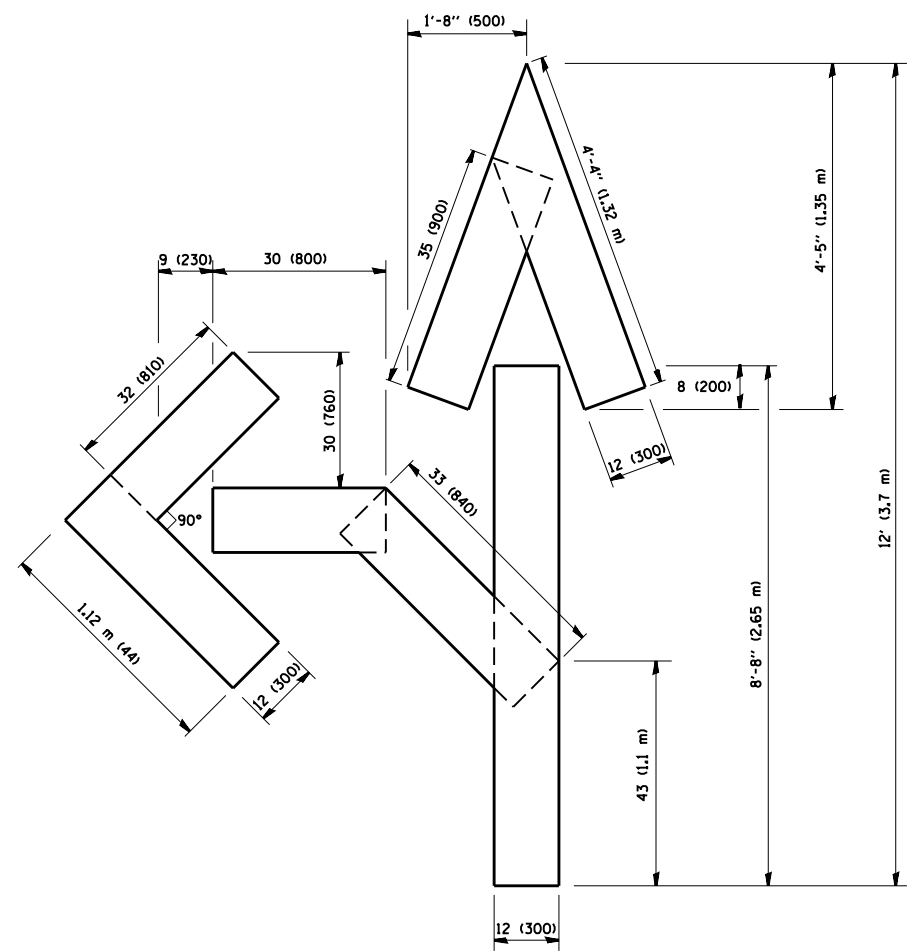
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

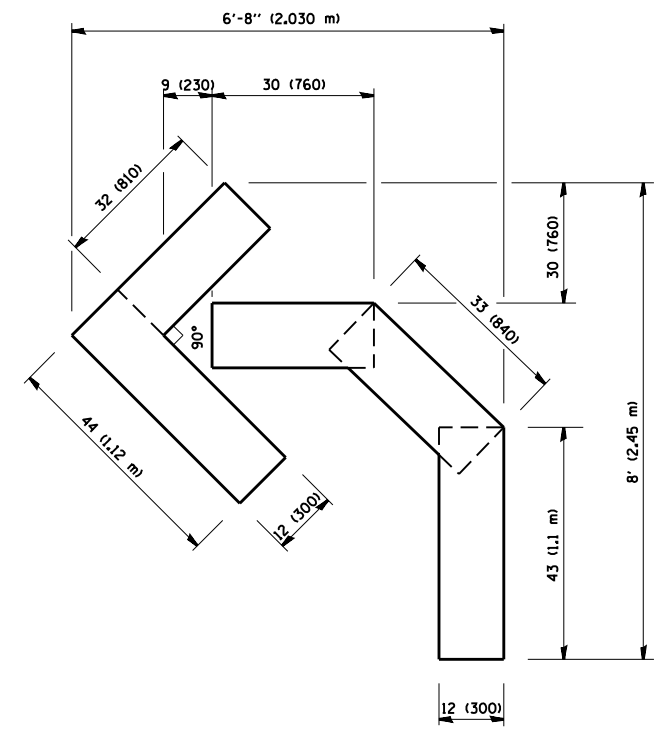
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	2010-040RS	WILL	20	17
TC-14		CONTRACT NO. 60K97		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

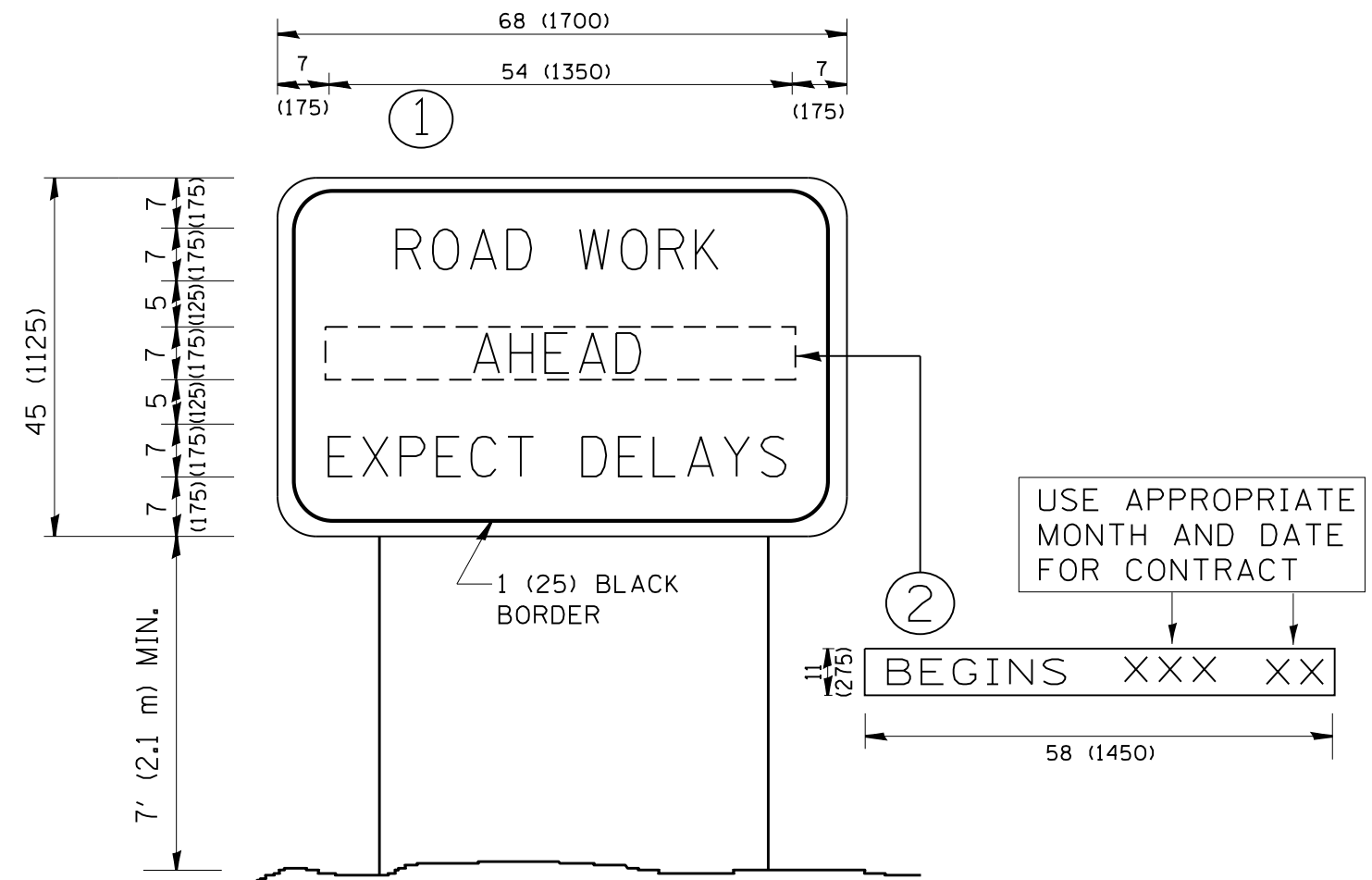
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -T, RAMMACHER 06-05-96
ct:\pwork\work\pwork\gorengautab\d0252964\DistStd.dgn		DRAWN -	REVISED -T, RAMMACHER 11-04-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -T, RAMMACHER 03-02-98
	PLOT DATE = 12/17/2012	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	2010-040RS	WILL	20	18
TC-16		CONTRACT NO. 60K97		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED - R. MIRS 09-15-97
et:\pwork\pwork\gorengautab\d0252964\DistStd.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 12/17/2012	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

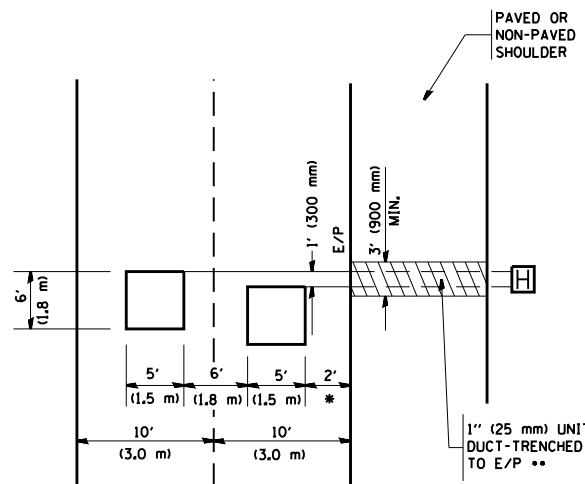
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	2010-040R5	WILL	20	19
TC-22		CONTRACT NO. 60K97		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

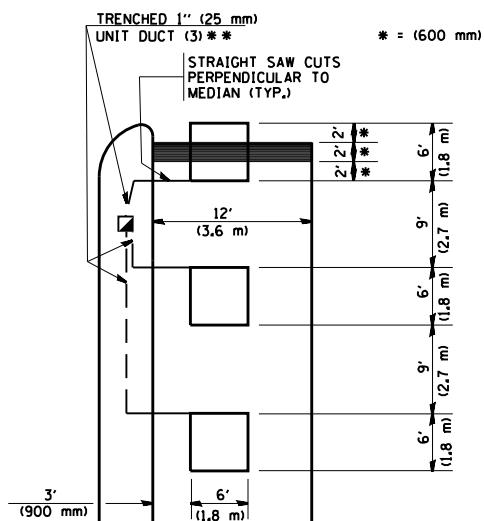


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

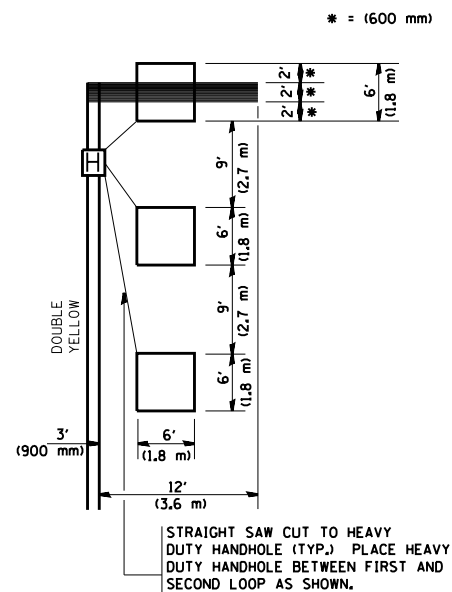
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

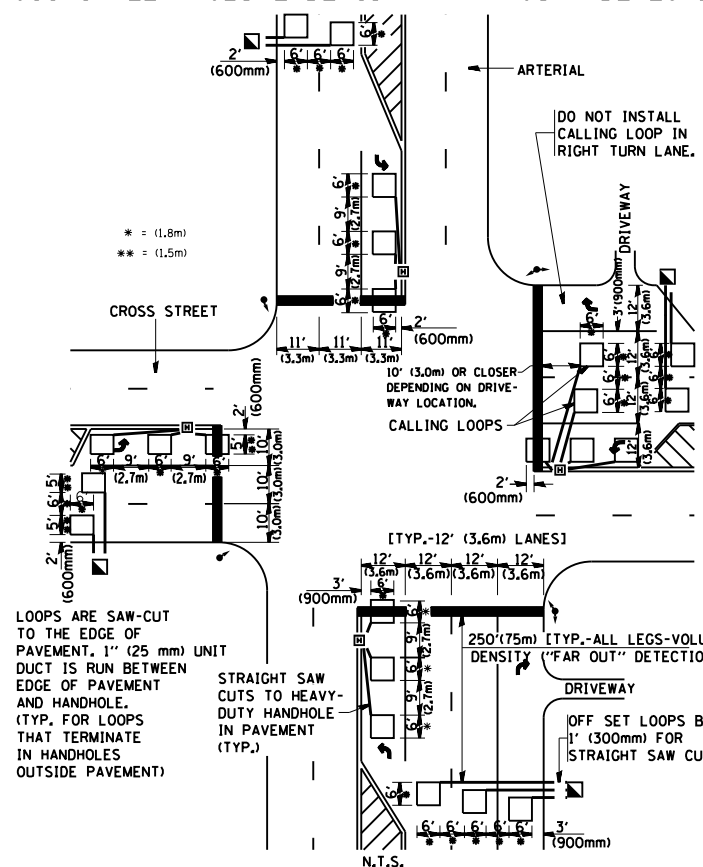
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

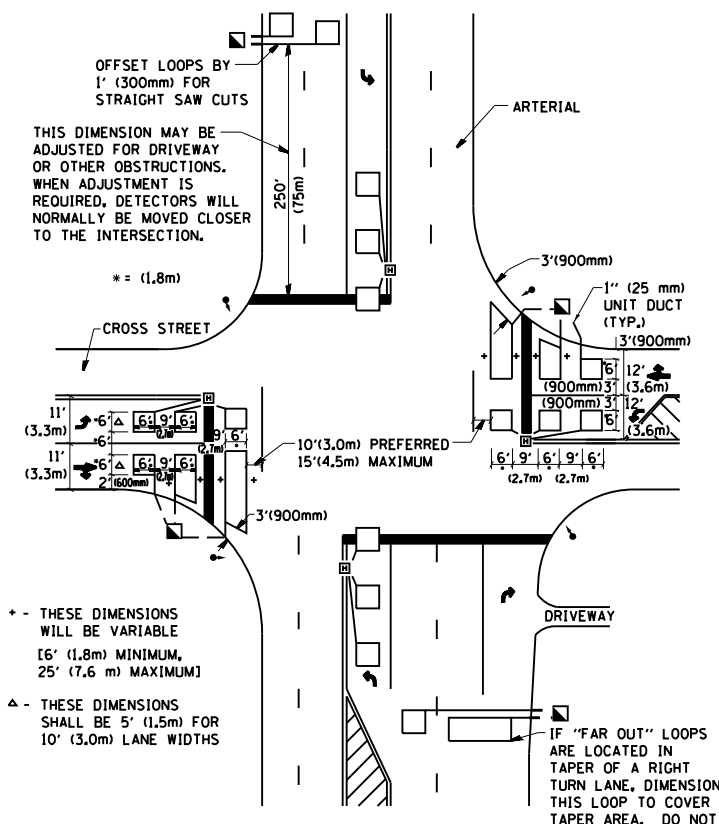
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -
et:\pwork\pwork\dot\gorengautab\d0252964\DistStd.dgn		DRAWN -	REVISED -
		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	2010-040RS	WILL	20	20
	TS-07			CONTRACT NO. 60R97
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				