

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

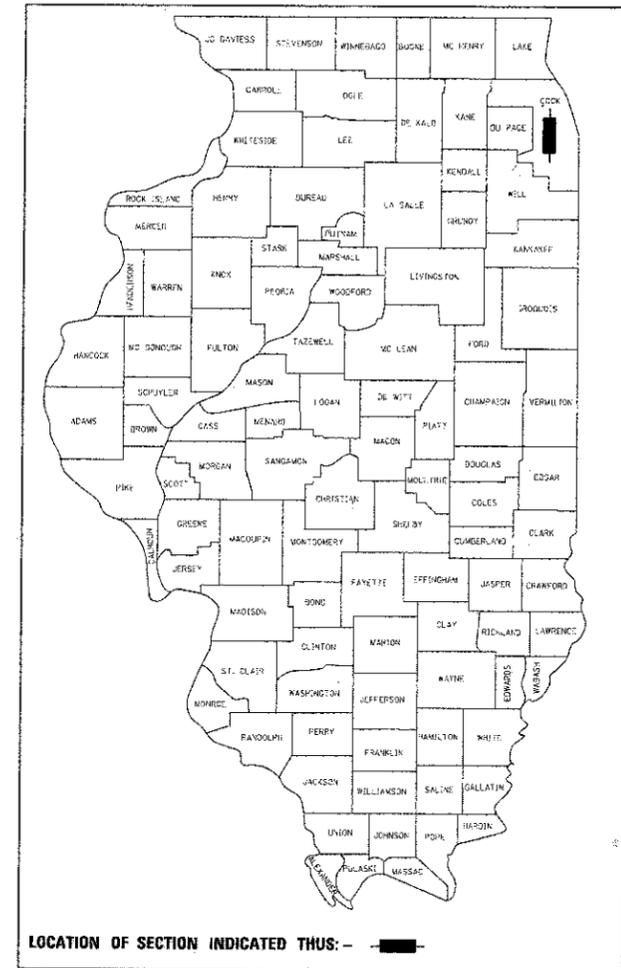
**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**

F.A.U. 2539 (ROY AVENUE) AND F.A.U. 1006 (ROBERTA AVENUE)  
SOFFEL AVENUE TO NORTH AVENUE  
ROADWAY RESURFACING  
Section No.: 12-00080-00-RS  
Project No. M-4003(126)  
CITY OF NORTHLAKE  
COOK COUNTY  
JOB NO.: C-91-129-13

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	12-00080-00-RS	COOK	18	1

ILLINOIS FED. AID PROJECT

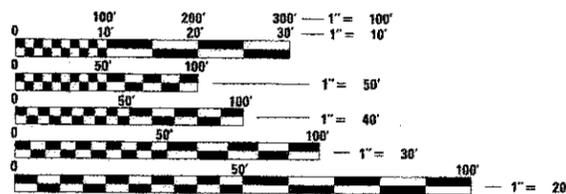
CONTRACT NO. 63791



INDEX OF SHEETS

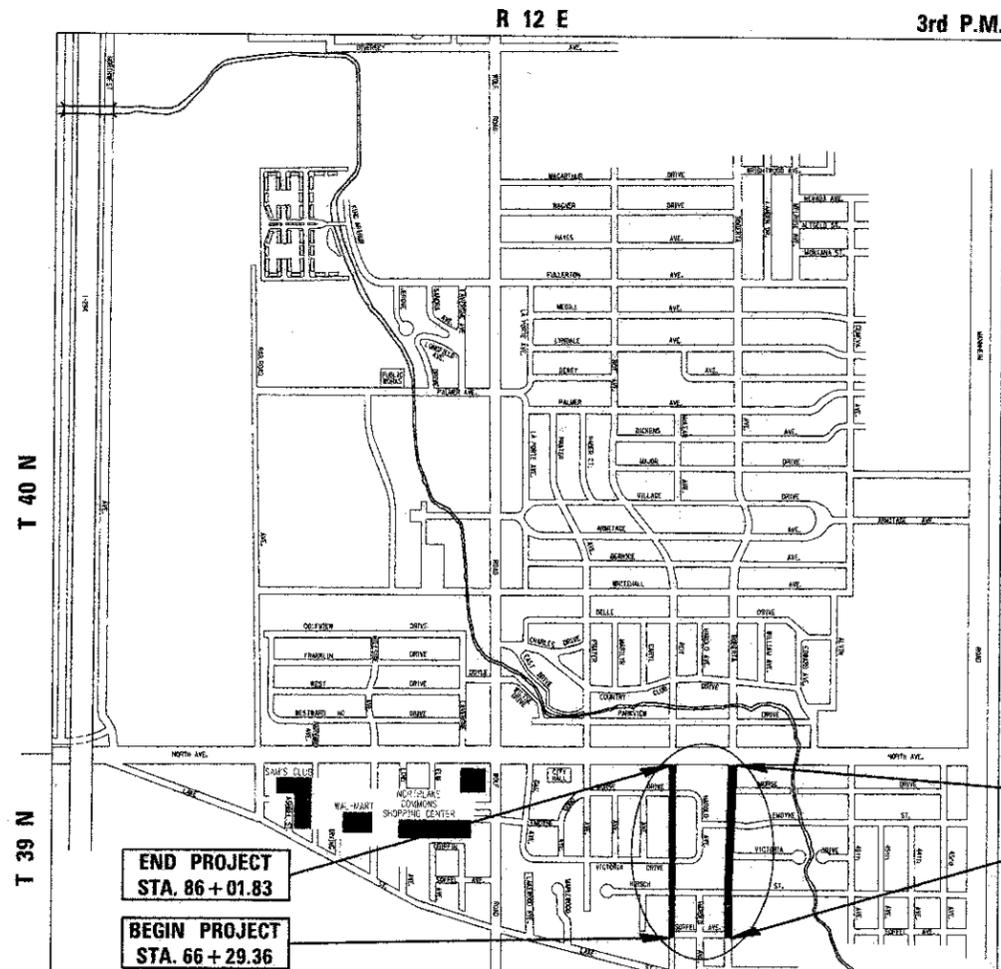
SHEET NO.	DESCRIPTION
1	COVER SHEET
2	GENERAL NOTES AND HIGHWAY STANDARDS
3	SCHEDULES OF QUANTITIES
4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS
6 - 11	ROADWAY PLANS
12	DETECTOR LOOP REPLACEMENT PLAN
13	CONSTRUCTION DETAILS
14	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
15	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
16	BUTT JOINT AND HMA TAPER DETAILS
17	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
18	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

TRAFFIC DATA  
ROY AVENUE AND ROBERTA AVENUE  
CLASSIFICATION - RESIDENTIAL COLLECTOR  
ADT (2011) = 1300  
SPEED LIMIT = 25 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123



LOCATION MAP - PROVISO TOWNSHIP  
GROSS LENGTH OF PROJECT = 3,951 LINEAL FEET (0.75 MI.)  
NET LENGTH OF PROJECT = 3,869 LINEAL FEET (0.73 MI.)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED *[Signature]* 2/14/2013  
MAYOR  
CITY OF NORTHLAKE

PASSED FEBRUARY 20, 2013  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW February 21, 2013

*[Signature]*  
DEPUTY DIRECTOR OF HIGHWAYS  
REGION ONE ENGINEER



February 21, 2013  
*[Signature]*  
ANDREW M. PUFUNDT  
ILLINOIS REGISTRATION No. 062-061729 ENGINEER  
EXPIRATION DATE: 11/30/13

CHRISTOPHER B. BURKE ENGINEERING LTD.  
9575 West Higgins Road, Suite 600  
Rosemont, Illinois 60018  
(847) 823-0500

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406, SCHAMBERG, IL

**GENERAL NOTES**

**SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS**

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2013; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", (SSTCI); THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2009 SIXTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE LAWS AND GOVERNMENT AGENCY REGULATIONS AND RULES; AUTHORITIES HAVING JURISDICTION; OSHA REGULATIONS AND RULES; AND ANY APPLICABLE RULES AND REGULATIONS OF THE STATE OF ILLINOIS OR COOK COUNTY AGENCIES. FURTHERMORE, AND AS RELATED TO THE WORK, THE CONTRACTOR SHALL GIVE NOTICES AND COMPLY WITH APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS, AND LAWFUL ORDERS OF ALL PUBLIC AUTHORITIES BEARING ON THE SAFETY OF PERSONS OR PROPERTY OR THEIR PROTECTION FROM DAMAGE, INJURY OR LOSS.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

**UTILITIES**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE. AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

**STAKING**

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED, AND SHALL BE AS INDICATED ON THE PLANS. ELEVATIONS SHOWN AT POINT OF CURVE, ETC. IS EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

**WATER, STORM SEWER AND SANITARY SEWER**

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE COMPLETED PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT THEIR EXPENSE.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS WITHOUT PRIOR AUTHORIZATION FROM THE CITY WATER DEPARTMENT. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

**GENERAL NOTES**

1. THE CITY OF NORTHLAKE SHALL BE NOTIFIED BY THE CONTRACTOR IN WRITING AT LEAST (3) FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION.
2. THE CONTRACTOR SHALL PROVIDE ALL NECESSARY PROTECTION FOR EXISTING UTILITIES IN CONFORMANCE WITH THE AFFECTED UTILITY COMPANIES REQUIREMENTS AS MAY BE REQUIRED TO PERFORM THE WORK OF THIS CONTRACT.
3. BEFORE BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE LINE AND GRADES SHOWN ON THE CONTRACT DRAWINGS. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONTRACT DRAWINGS, THE CONTRACTOR SHALL IMMEDIATELY REPORT SAME TO THE ENGINEER PRIOR TO PERFORMING WORK. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK AS REQUIRED.
4. THE CONTRACT DOCUMENTS ARE NOT INTENDED TO SHOW EVERY AND ALL DETAILS OF WORK TO BE PERFORMED OR EQUIPMENT TO BE SUPPLIED. THE INTENT OF THE CONTRACT DOCUMENTS IS TO ILLUSTRATE THE CONCEPTUAL DESIGN AND LAYOUT. THE CONTRACTOR SHALL BE KNOWLEDGEABLE AND REGULARLY ENGAGED IN THE TYPE OF WORK DESCRIBED BY THESE CONTRACT DOCUMENTS, AND SHALL BE RESPONSIBLE FOR UNDERSTANDING THEIR INTENT. ADDITIONAL WORK TO BE PERFORMED OR ITEM OF EQUIPMENT TO BE SUPPLIED WHICH IS NOT SPECIFICALLY CALLED FOR BY THESE CONTRACT DOCUMENTS BUT WHICH IS NECESSARY TO PROVIDE A COMPLETE AND SUCCESSFUL WORKING SYSTEM SHALL BE INCLUDED IN THE COST OF THE CONTRACT AT NO ADDITIONAL COST TO THE OWNER.
5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ALL MATERIAL QUANTITIES AND APPRAISE HIMSELF/HERSELF OF ALL CONDITIONS. NO CLAIMS FOR ADDITIONAL COMPENSATION FOR INDIVIDUAL PAY ITEMS WILL BE RECOGNIZED DUE TO THE CONTRACTOR'S FAILURE TO UNDERSTAND THE SCOPE OF WORK.
6. THE WORK PERFORMED UNDER THIS CONTRACT SHALL IN NO WAY INTERFERE WITH THE NORMAL OPERATION OF ANY EXISTING UTILITY SERVICE. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ITEMS OF EQUIPMENT REQUIRED TO MAINTAIN SUCH NORMAL OPERATION AT NO ADDITIONAL COST TO THE OWNER. THE COST ASSOCIATED FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE CONTRACT.
7. CERTAIN INFORMATION SHOWN ON THESE DRAWINGS HAS BEEN OBTAINED FROM DRAWINGS OF RECORD. CONTRACTOR SHALL VERIFY SUCH INFORMATION PRIOR TO ACTUAL START OF WORK. WHERE DISCREPANCIES ARE DISCOVERED THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER. FAILURE BY THE CONTRACTOR TO IMMEDIATELY NOTIFY THE ENGINEER OF SUCH DISCREPANCIES SHALL RESULT IN THE CONTRACTOR BEARING THE FULL BURDEN OF ALL RISKS/COSTS ATTRIBUTED TO THE DISCOVERED DISCREPANCY.
8. SOIL EROSION PROTECTION SHALL BE IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL. ALL DISTURBED AREAS (NOT IMPERVIOUS IN NATURE) SHALL BE FINE GRADED, TOP SOIL RESTORED (MIN 4 INCHES) AND SEED/MULCH APPLIED UNLESS OTHERWISE SPECIFIED ON THE PLANS.
9. ALL REMOVAL OR EXCAVATION ITEMS BEING DISPOSED OF AT AN UNCONTAMINATED SOIL FILL OPERATION OR CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) FILL SITE SHALL MEET THE REQUIREMENTS OF PUBLIC ACT 96-1416. ALL COSTS ASSOCIATED WITH MEETING THESE REQUIREMENTS SHALL BE INCLUDED IN THE UNIT PRICE COST FOR THE ASSOCIATED REMOVAL OR EXCAVATION ITEMS IN THE CONTRACT. THESE COSTS SHALL INCLUDE BUT ARE NOT LIMITED TO ALL REQUIRED TESTING, LAB ANALYSIS, CERTIFICATION BY A LICENSED PROFESSIONAL ENGINEER, AND STATE OR LOCAL TIPPING FEES.
10. THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK ADJACENT TO IL 64 (NORTH AVENUE).

**MISCELLANEOUS**

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM AGGREGATE FOR TEMPORARY ACCESS.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED TO REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

TREE ROOT PRUNING, COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, DRIVEWAY PAVEMENT REMOVAL, PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH, PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH AND 7 INCH, AND CLASS D PATCHES 6" ARE NOT SHOWN ON THE PLANS BUT WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE DISABLED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, COMMERCIAL DRIVEWAYS, AND ALLEYS AS DIRECTED BY THE ENGINEER (SEE IDOT STD. 424001 INCLUDED IN THE SPECIFICATIONS).

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES. THIS WORK SHALL BE PERFORMED PER ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.

PER ARTICLE 107.20, MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS AS DIRECTED BY THE ENGINEER.

PROPOSED CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO EXISTING CURB AND GUTTER OVER A LENGTH OF 5 FEET. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

ALL UNDERGROUND, DRIVEWAY, CONCRETE, AND LANDSCAPE RESTORATION WORK IS TO BE COMPLETED BEFORE THE SURFACE COURSE CAN BE INSTALLED.

THE HMA SURFACE COURSE USED TO REPLACE DRIVEWAYS WILL BE PAID FOR PER TON.

PROTECTIVE COAT FOR ALL CONCRETE SURFACES SHALL BE CONSIDERED INCLUDED IN THE COST OF THE RESPECTIVE PAY ITEM.

ANY DEFACED WORK SHALL BE CORRECTED OR REPLACED BY THE CONTRACTOR AT HIS SOLE EXPENSE PRIOR TO FINAL PAYMENT. THE CITY WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE PER CONDITIONS OF ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS.

FOR HOT-MIX ASPHALT SURFACE REMOVAL, THE EXISTING ASPHALT SURFACE SHALL BE REMOVED TO THE DEPTH SPECIFIED. THE GRINDINGS SHALL BE REMOVED FROM THE SITE AND THE SURFACE MECHANICALLY BROOMED UNTIL THE SURFACE IS COMPLETELY FREE OF ANY LOOSE MATERIAL AND DEBRIS. GRINDING OF THE EXISTING CONCRETE BASE MAY BE REQUIRED TO ESTABLISH THE SPECIFIED DEPTH. CONCRETE GRINDING SHALL BE CONSIDERED INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL.

CLASS D PATCHES SHALL CONSIST OF REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT AT LOCATIONS DIRECTED BY THE ENGINEER. AFTER MILLING IS COMPLETE, THE EXISTING PAVEMENT INCLUDING THE BASE AND HMA SURFACE SHALL BE REMOVED TO A DEPTH OF SIX (6) INCHES AND REPLACED WITH SIX (6) INCHES OF HMA MIX. THE SURFACE OF THE PATCH SHALL MEET THE SURFACE OF THE GROUND OF THE HOT-MIX ASPHALT SURFACE REMOVAL. ALL HOLES, SOFT PLACES AND OTHER DEFECTS IN THE SUBBASE OR SUBGRADE SHALL BE CORRECTED BY THE CONTRACTOR BY REMOVING THE UNSUITABLE MATERIAL, ADDING MORE HMA MIX AS SPECIFIED IN SECTION 406 OF THE STANDARD SPECIFICATIONS.

FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EXPOXY COATED, UNLESS NOTED ON THE PLAN.

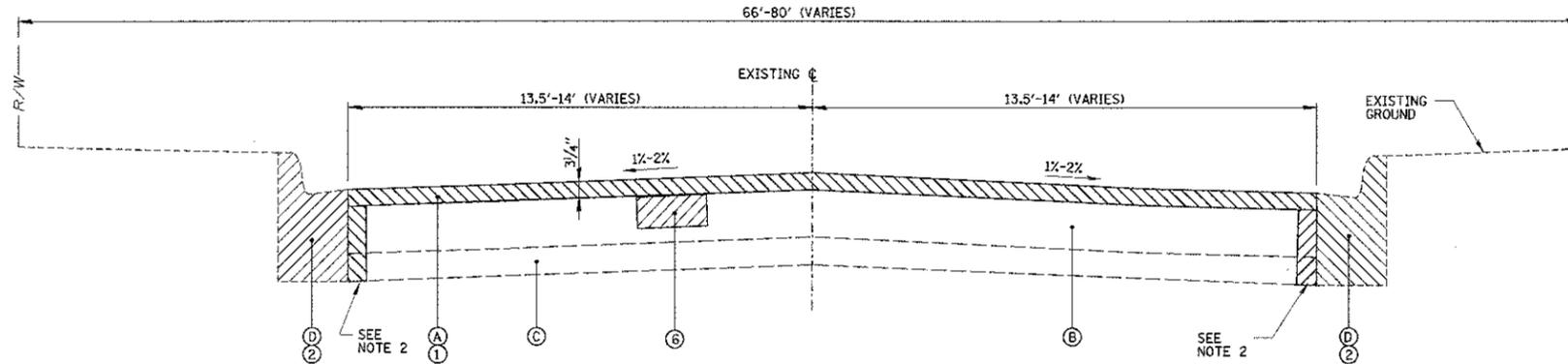
THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AS WELL AS SUPERVISION / DIRECTION AND MEANS / METHODS OF CONSTRUCTION.

**HIGHWAY STANDARDS**

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALK
424006-01	DIAGONAL CURB RAMPS FOR SIDEWALKS
424021-01	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN TYPE A
602501-02	VALVE VAULT TYPE A
604001-03	FRAME AND LIDS TYPE 1
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
780001-03	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTOR LOOP
B. L. R. 17-4	TRAFFIC CONTROL DEVICES - DAY LABOR CONSTRUCTION
B. L. R. 18-5	TRAFFIC CONTROL DEVICES - DAY LABOR MAINTENANCE

FILE NAME =	USER NAME = edtdob	DESIGNED - AMP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES AND HIGHWAY STANDARDS</b>	F.A.U. R.T.E. = 2539	SECTION = 12-00080-00-RS	COUNTY = COOK	TOTAL SHEETS = 18	SHEET NO. = 2	CONTRACT NO. 63791
N:\NORTHLAKE\9400320C12\G\civil\gnote1.dwg	roy.dci22.sh1	DRAWN - EDT	REVISED -								
PLOT SCALE = 1"	CHECKED - MEW	REVISED -	REVISED -								
PLOT DATE = 2/20/2013	DATE - 2/4/13	REVISED -	REVISED -								
SCALE:						SHEET NO.	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT





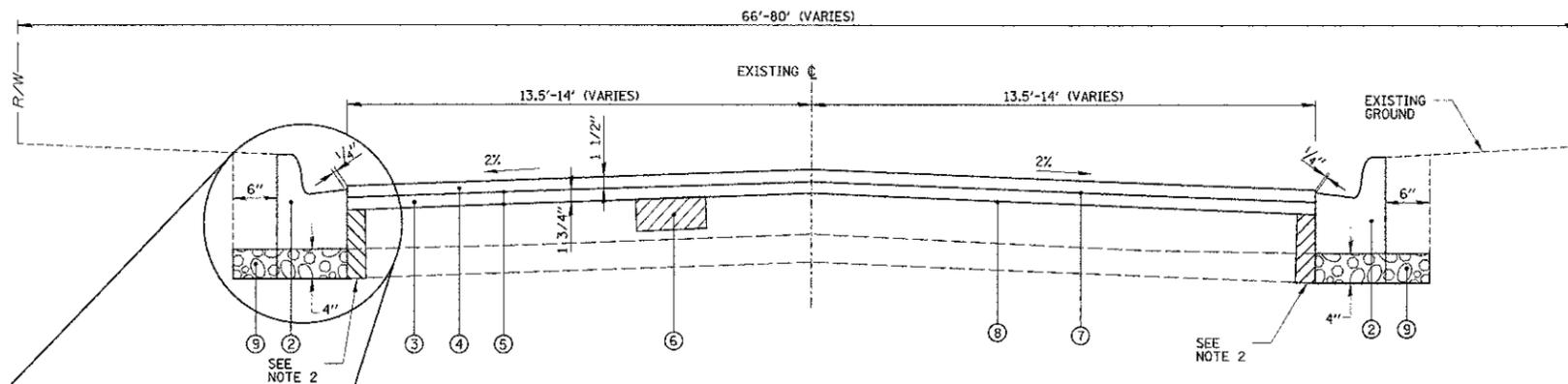
EXISTING TYPICAL SECTION  
 STA. 66+29.36 TO STA. 86+01.83, ROY AVENUE  
 STA. 106+48.15 TO STA. 126+27.63, ROBERTA AVENUE  
 NOT TO SCALE

**LEGEND**

- (A) EXISTING ASPHALT PAVEMENT (+/- 3" TO 10.5")
- (B) EXISTING HMA OR P.C.C. BASE (+/- 0" TO 11.5")
- (C) EXISTING AGGREGATE BASE (+/- 0" TO 11.5")
- (D) EXISTING COMBINATION CURB AND GUTTER (TYPE VARIES - TYPE B-6.12 OR M-4.12)
- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"
- (2) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY ENGINEER) (SEE DETAIL SHEET 14)
- (3) LEVELING BINDER (MACHINE METHOD), N50 - 1 3/4" MIN.
- (4) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (5) BITUMINOUS MATERIALS (PRIME COAT)
- (6) CLASS D PATCHES, TYPE VARIES, 6"
- (7) AREA REFLECTIVE CRACK CONTROL TREATMENT
- (8) AGGREGATE (PRIME COAT)
- (9) SUBBASE GRANULAR MATERIAL, TYPE B (INCLUDED IN COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (SEE DETAIL SHEET 14)

**NOTES**

1. CONTRACTOR SHALL MILL PAVEMENT BEFORE PATCHING.
2. ANY PAVEMENT REMOVAL REQUIRED FOR CONSTRUCTION OF COMBINATION CONCRETE CURB AND GUTTER SHALL BE REPLACED WITH P.C.C. (CLASS S1) AND SHALL BE VIBRATED IN PLACE. COST FOR PAVEMENT REMOVAL AND P.C.C. (CLASS S1) SHALL BE INCLUDED IN UNIT PRICE FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
3. SEE CURB AND GUTTER REMOVAL AND REPLACEMENT DETAIL REGARDING PAYMENT FOR LANDSCAPE RESTORATION (SHEET 14).
4. THE EXISTING PAVEMENT CROSS SLOPE IS APPROXIMATELY 1%. ADDITIONAL QUANTITY OF LEVELING BINDER HAS BEEN INCLUDED IN THE CONTRACT TO RAISE THE ROADWAY CROWN TO ESTABLISH A 2% CROSS SLOPE. GRINDING AND PAVING OPERATIONS SHALL BE ADJUSTED ACCORDINGLY.



PROPOSED TYPICAL SECTION  
 STA. 66+29.36 TO STA. 86+01.83, ROY AVENUE  
 STA. 106+48.15 TO STA. 126+27.63, ROBERTA AVENUE  
 NOT TO SCALE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE ITEM	AIR VOIDS @ Ndes
<b>PAVEMENT RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 1 1/2"	4% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"	4% @ 50 GYR
LEVELING BINDER (MACHINE METHOD), N50, 1 1/2"	4% @ 50 GYR
<b>DRIVEWAYS</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 3" (2 LIFTS)	4% @ 50 GYR
<b>PATCHING</b>	
CLASS D PATCHES, 6" (SPECIAL) (HMA BINDER IL-19 MM) NOTE: SAW CUT PATCHES PRIOR TO REMOVAL	4% @ 70 GYR

**NOTES:**

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG TO -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

**SCHEDULES OF QUANTITIES**

**COMBINATION CONCRETE CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

ROY AVENUE			
FROM STA.	TO STA.	L OR R	LENGTH (FT)
AT ENGINEER'S DIRECTION			73
67+00	67+15	L	15
67+05	67+20	R	15
68+45	68+66	L	21
68+90	69+02	R	12
69+52	70+31	R	79
70+02	70+32	L	30
70+85	71+37	L	110
71+15	71+38	R	61
71+61	72+05	L	69
71+65	72+20	R	84
73+17	73+36	R	19
74+67	74+97	L	49
74+70	74+97	R	31
75+22	75+55	L	44
75+22	75+55	R	41
77+00	77+10	L	10
77+30	77+70	R	40
77+65	78+09	L	44
77+93	78+70	R	77
78+50	79+34	L	84
79+04	79+29	R	25
80+07	80+28	L	21
80+20	80+58	R	38
80+86	80+94	R	8
TOTAL =			1100

ROBERTA AVENUE			
FROM STA.	TO STA.	L OR R	LENGTH (FT)
AT ENGINEER'S DIRECTION			61
110+43	110+80	L	37
111+32	111+63	R	76
111+35	111+60	L	38
111+90	112+30	L	52
113+10	113+80	L	70
113+13	114+17	R	104
115+19	115+39	L	20
115+27	115+45	R	35
115+50	115+80	L	30
115+70	115+90	R	50
117+40	117+74	L	34
118+65	118+90	L	31
118+65	118+90	R	30
119+15	119+35	L	31
119+15	119+35	R	32
119+74	122+00	L	226
120+58	120+80	R	22
121+25	121+59	R	34
122+00	122+85	L	85
122+40	122+65	R	56
122+90	123+10	R	53
123+50	123+67	L	17
124+38	124+46	R	8
124+97	125+27	R	30
125+30	125+53	L	23
TOTAL =			1285

NOTE: LIMITS OF CURB REMOVAL AND REPLACEMENT ARE APPROXIMATE AND TO BE MARKED IN THE FIELD BY THE ENGINEER. LIMITS ARE SUBJECT TO CHANGE.

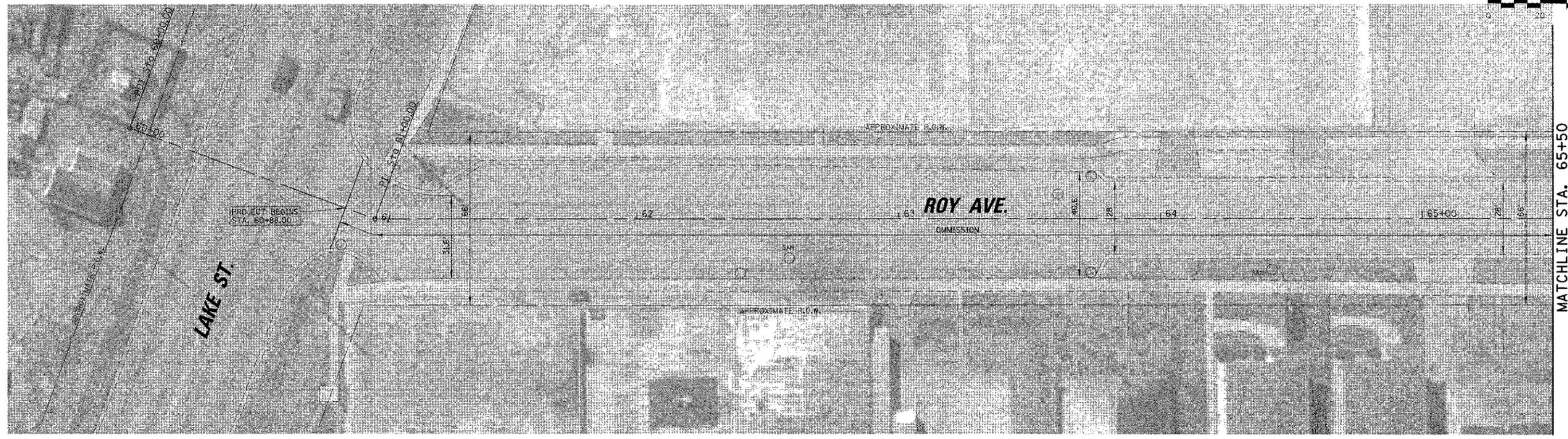
**DRIVEWAY REMOVAL AND REPLACEMENT**

ROY AVENUE			
AT STA.	L OR R	TYPE	AREA (SY)
AT ENGINEER'S DIRECTION			30
77+90	L	ASPHALT	23
78+65	L	ASPHALT	14
79+15	R	PCC	21
79+20	L	ASPHALT	12
TOTAL =			100

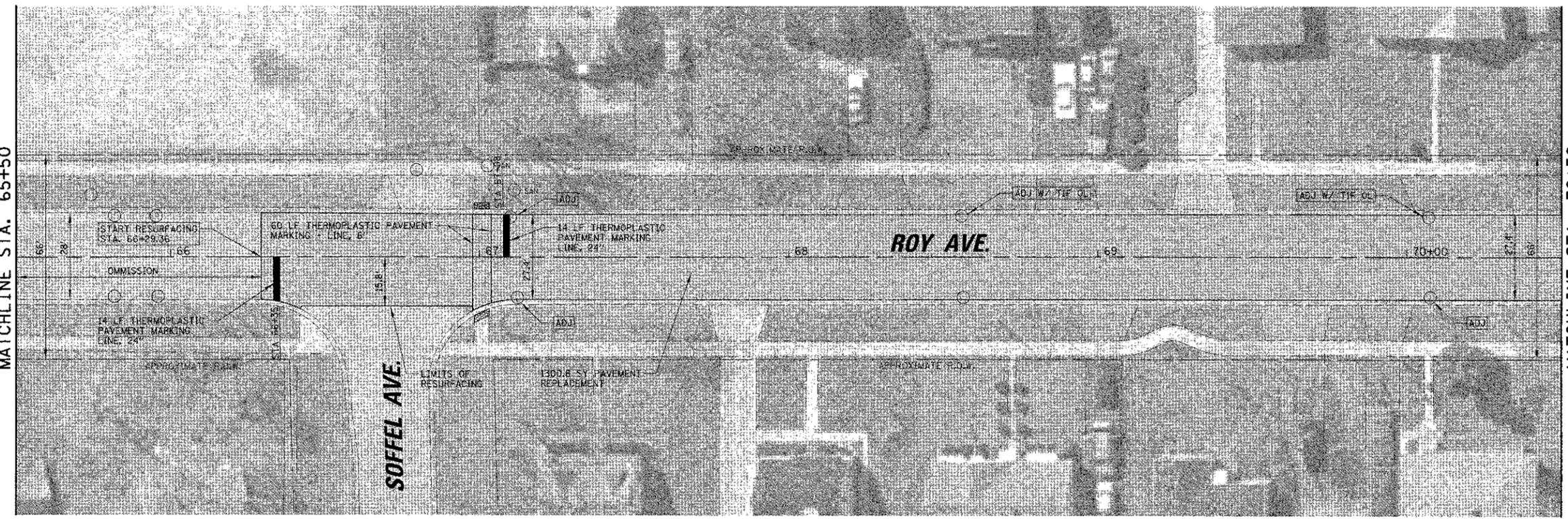
  

ROBERTA AVENUE			
AT STA.	L OR R	TYPE	AREA (SY)
AT ENGINEER'S DIRECTION			32
110+65	L	ASPHALT	13
113+30	L	ASPHALT	15
113+35	R	ASPHALT	9
113+65	L	ASPHALT	10
114+05	R	ASPHALT	17
115+60	L	ASPHALT	8
120+05	L	ASPHALT	10
120+70	L	ASPHALT	10
121+35	L	ASPHALT	9
122+40	L	ASPHALT	10
122+60	L	ASPHALT	7
TOTAL =			150

NOTE: AREAS FOR DRIVEWAY REMOVAL AND REPLACEMENT ARE APPROXIMATE AND TO BE MARKED IN THE FIELD BY THE ENGINEER. LIMITS ARE SUBJECT TO CHANGE.



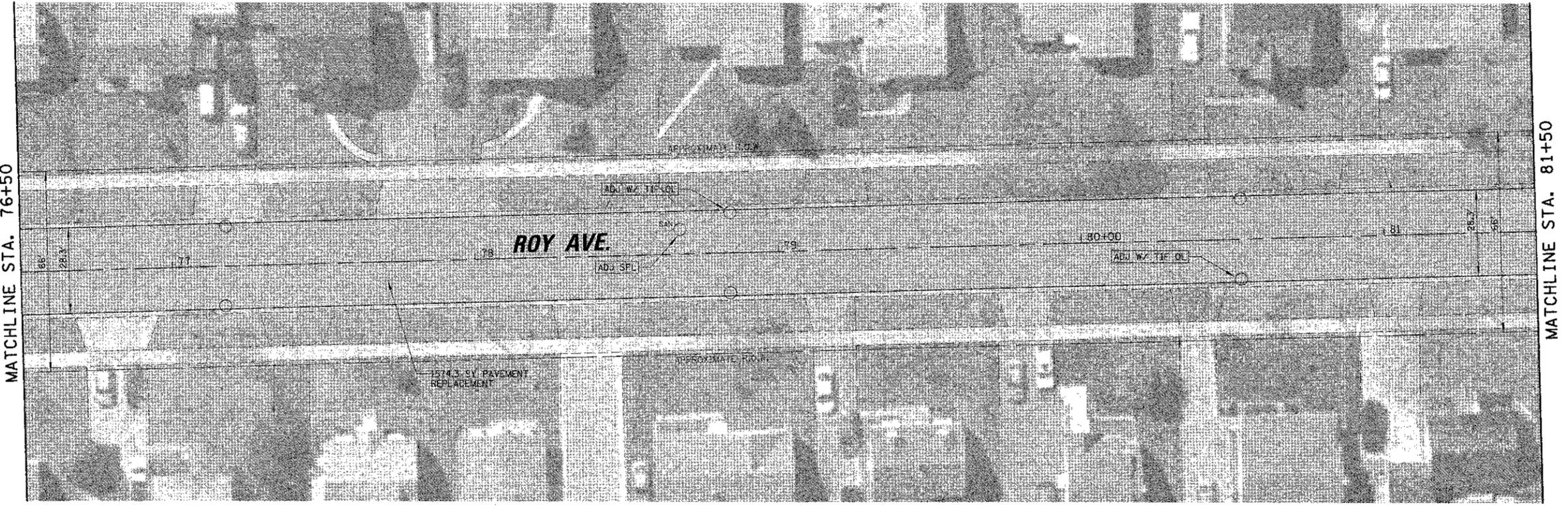
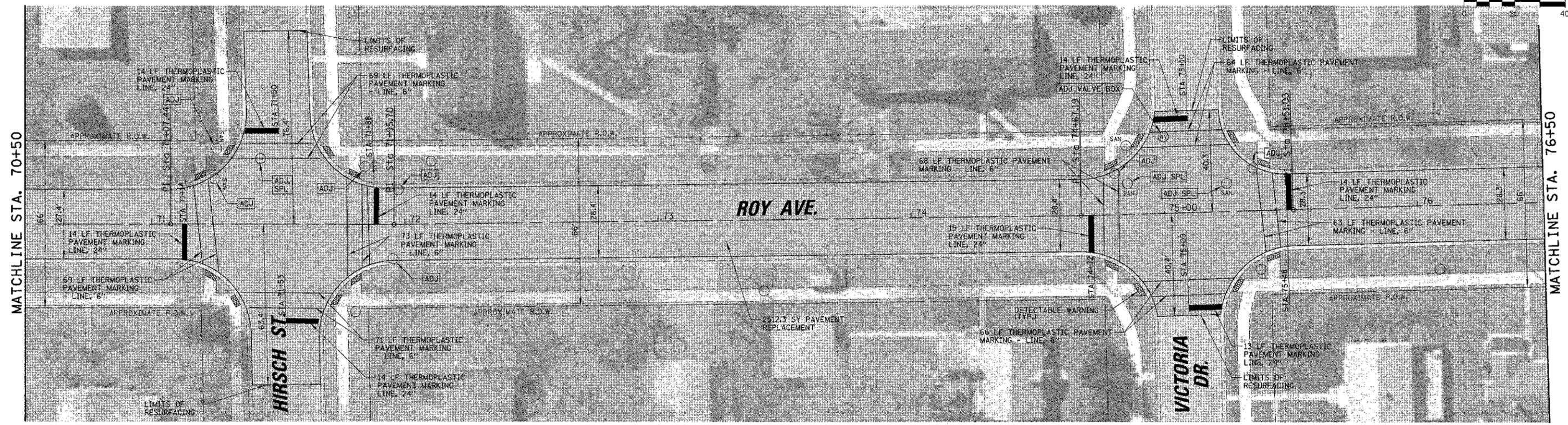
MATCHLINE STA. 65+50



MATCHLINE STA. 65+50

MATCHLINE STA. 70+50

FILE NAME =	USER NAME = edtoday	DESIGNED - AMP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLANS ROY AVENUE</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
h:\NDRT\LAKE\9482320\122\civ\1\p\ri.roy	0122.sht	DRAWN - EDT	REVISED -			2539	12-00080-00-RS	COOK	18	6	
PLOT SCALE = 20'		CHECKED - MEW	REVISED -			CONTRACT NO. 63791					
PLOT DATE = 2/28/2013		DATE - 2/4/13	REVISED -			ILLINOIS FED. AID PROJECT					
SCALE: 1" = 20'						SHEET NO. OF SHEETS STA. TO STA.					

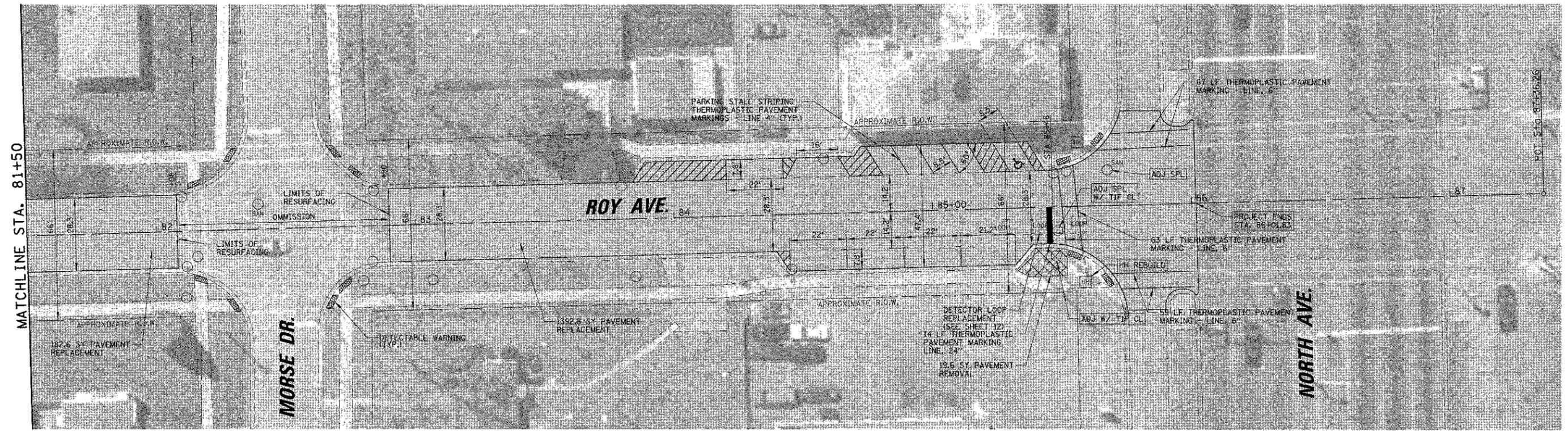
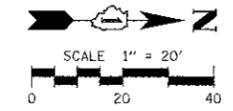


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\\NORTH\AKEN\9488320C\22\6\vi\p\h2_ro_	XC122.sht	DRAWN - EDT	REVISED -
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	PLOT DATE = 2/28/2013	DATE - 2/4/13	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROADWAY PLANS ROY AVENUE				
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	12-00080-00-RS	COOK	18	7
CONTRACT NO. 63791				
ILLINOIS FED. AID PROJECT				



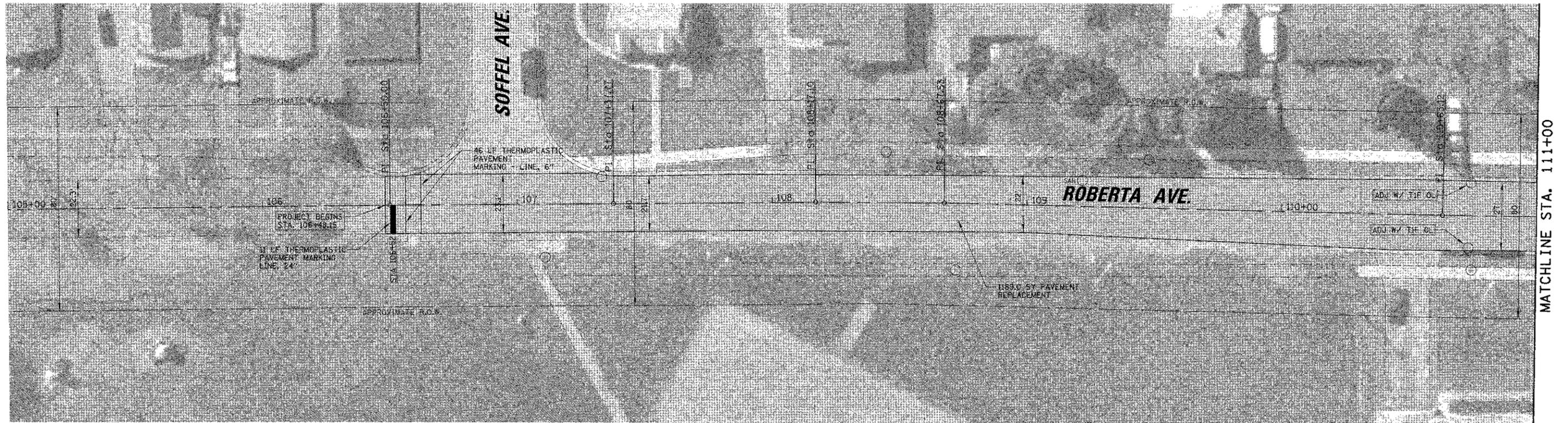
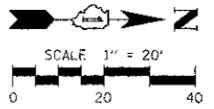
NOTE:  
 1. HOT-MIX ASPHALT SURFACE REMOVAL DEPTH WITHIN NORTH AVE. R.O.W. SHALL BE ADJUSTED TO 2 1/4". GRINDING OPERATIONS MAY INCLUDE PORTION OF EXISTING CONCRETE BASE. PROPOSED PAVEMENT SECTION SHALL BE HMA LEVELING BINDER, 3/4" AND HMA SURFACE COURSE, 1 1/2".

FILE NAME =	USER NAME = edctoda	DESIGNED - AMP	REVISED -
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	PLT DATE = 2/20/2013	DATE - 2/4/13	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>ROADWAY PLANS ROY AVENUE</b>				
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS
2539	12-00080-00-RS	COOK	18 / 8
			CONTRACT NO. 63791
ILLINOIS FED. AID PROJECT			



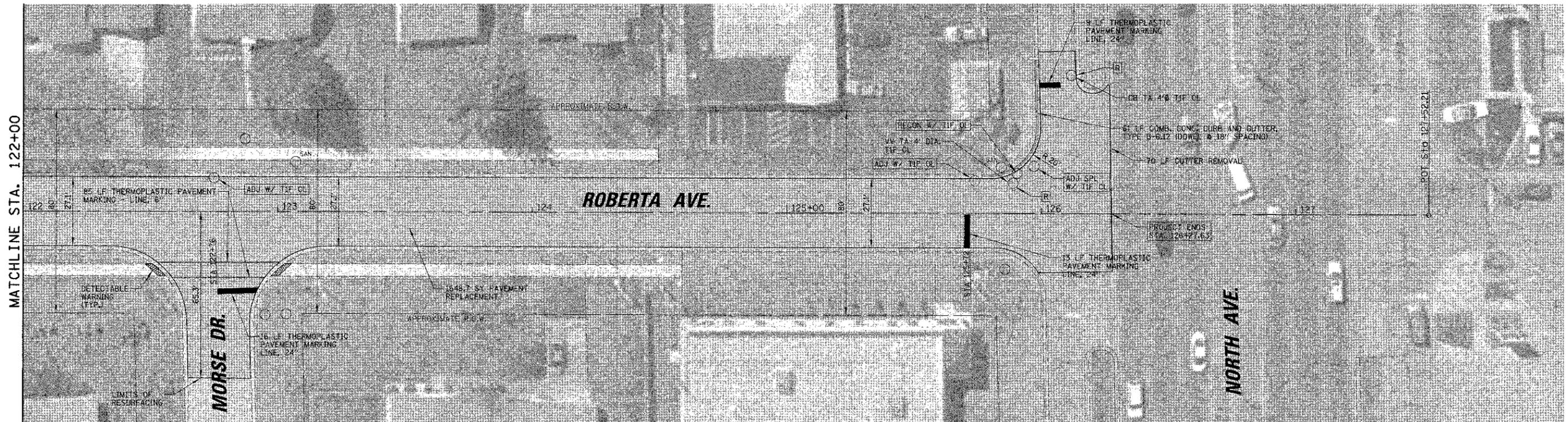
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	PLGT DATE = 2/28/2013	DATE - 2/4/13	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>ROADWAY PLANS ROBERTA AVENUE</b>			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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				CONTRACT NO. 63791
ILLINOIS FED. AID PROJECT				

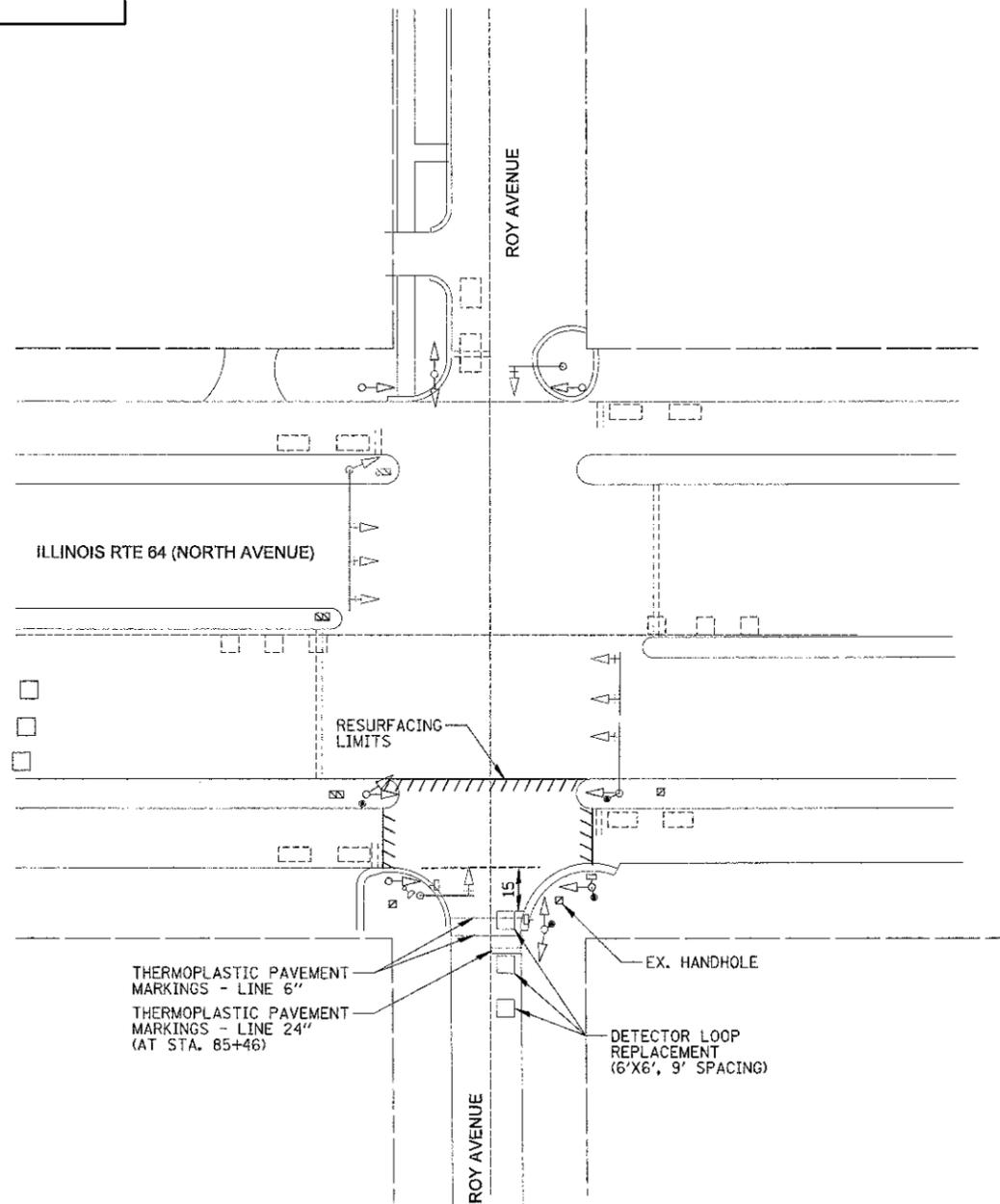
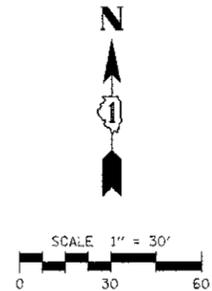




- NOTES:
- HOT-MIX ASPHALT SURFACE REMOVAL DEPTH WITHIN NORTH AVE, R.O.W. SHALL BE ADJUSTED TO 2 1/4". GRINDING OPERATIONS MAY INCLUDE PORTION OF EXISTING CONCRETE BASE. PROPOSED PAVEMENT SECTION SHALL BE HMA LEVELING BINDER, 3/4" AND HMA SURFACE COURSE, 1 1/2".
  - CONTRACTOR TO DETERMINE CUTS AND SIZES OF PIPE FOR PROPOSED REPLACEMENT STRUCTURES NEAR THE INTERSECTION OF ROBERTA AVENUE AND THE NORTH AVENUE FRONTAGE ROAD.

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLANS ROBERTA AVENUE</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
NONDRIT-LAKE\948032DC122\City\3\rob	ca_DEC122.sht	DRAWN -	REVISED -					2539	12-00080-00-RS	COOK	18	11
	PLCT SCALE = 20'	CHECKED -	REVISED					CONTRACT NO. 63791				
	PLCT DATE = 2/26/2013	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
				SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.			

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.



**TRAFFIC SIGNAL LEGEND**

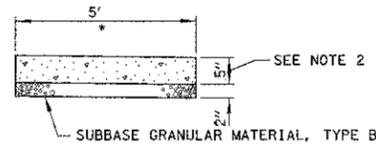
ITEM	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		▲
SIGNAL HEAD		▲▲
DETECTOR LOOP	□	□

**THIS PLAN IS FOR THE SOLE PURPOSE OF  
DETECTOR LOOP REPLACEMENT**

**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
88600600	175	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME =	USER NAME = wdscde	DESIGNED - AMP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
RYNDORTH-LAKE\9402320122\Civil\Loop-Detector_DC122.dwg		DRAWN - EDT	REVISED -			2539	12-00080-00-RS	COOK	18	12	
PL07 SCALE = 30'		CHECKED - MEW	REVISED -			CONTRACT NO. 63791					
PL07 DATE = 2/20/2013		DATE - 2/4/13	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	

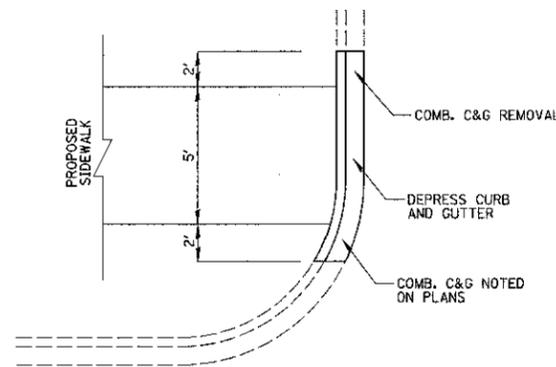


CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS

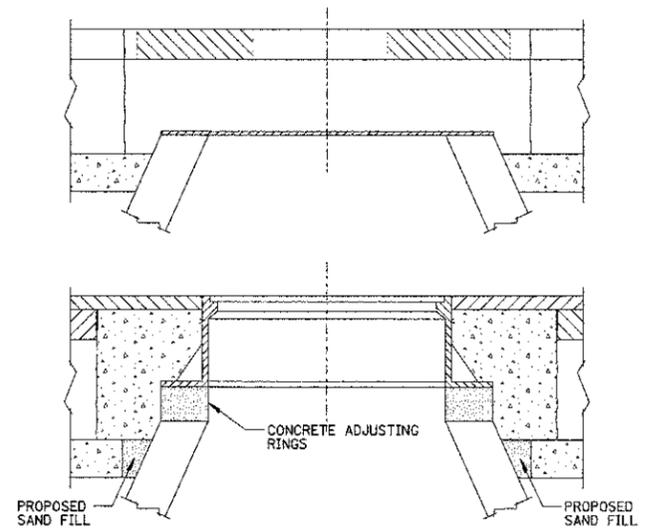
NOTES:

1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK TO MEET CURRENT ADA REQUIREMENTS SHALL BE INCLUDED IN THE COST FOR P.C.C. SIDEWALK.
2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
3. ALL LANDSCAPE RESTORATION (TOPSOIL, SEEDING, CLASS I, MULCH METHOD 3) SHALL BE INCLUDED IN COST FOR P.C.C. SIDEWALK.

P.C.C. SIDEWALK DETAIL



PROPOSED DEPRESSED CURB & GUTTER  
EXISTING CURB & GUTTER NOT DEPRESSED  
NOT TO SCALE



CONSTRUCTION PROCEDURES

STAGE I (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1" THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE II (AFTER PAVEMENT MILLING)

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

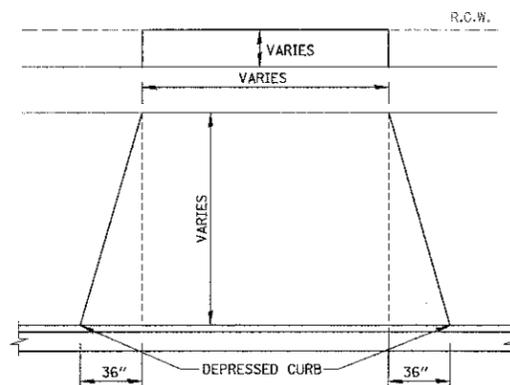
BASIS OF PAYMENT

DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL).

NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL)



DETAIL OF DRIVEWAY

THIS TYPICAL DRIVE LAYOUT IS FOR BOTH CONCRETE AND ASPHALT DRIVES

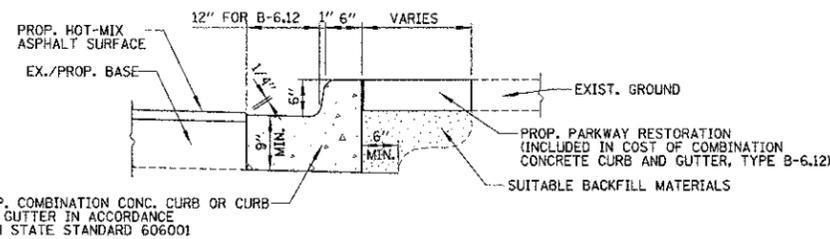
- P.C.C. DRIVE - 7" P.C.C. DRIVEWAY PAVEMENT  
2" SUB-BASE GRANULAR MATERIAL, TYPE B
- ASPHALT DRIVE - 2" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50  
2 1/4" HOT-MIX ASPHALT BASE COURSE  
2" SUB-BASE GRANULAR MATERIAL, TYPE B

NOTES:

1. PROPOSED SIDEWALK THROUGH DRIVEWAY SHALL BE 7 INCHES OF PORTLAND CEMENT CONCRETE ON 2 INCH SUB-BASE GRANULAR MATERIAL, TYPE B.

2. THE HMA SURFACE COURSE USED TO REPLACE DRIVEWAYS WILL BE PAID FOR PER TON. THE NUMBER OF DRIVEWAYS TO BE REPLACED WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

3. ALL LANDSCAPE RESTORATION (TOPSOIL, SEEDING, CLASS I, MULCH METHOD 3) SHALL BE INCLUDED IN COST FOR DRIVEWAY REPLACEMENT.



ALL REQUIRED EARTH EXCAVATION AND SUITABLE BACKFILL TO CONSTRUCT COMBINATION CONCRETE CURB AND GUTTER WILL BE INCLUDED IN THE COST FOR COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

1. MINIMUM 4" THICK SUB-BASE GRANULAR MATERIAL SHALL BE PROVIDED UNDER NEW CURB AND GUTTER TO 6" BEYOND BACK OF CURB.

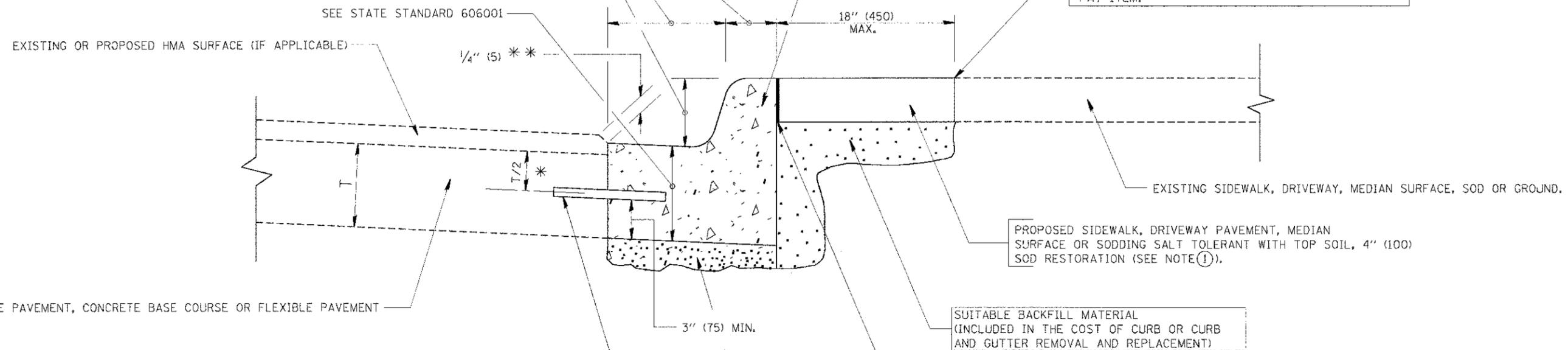
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12  
NOT TO SCALE

FILE NAME =	USER NAME = edcode	DESIGNED - AMP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CONSTRUCTION DETAILS</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
M:\NDHT\LAKE\9408320CJ22\civil\est1_feb	roy_dc122.sht	DRAWN - EDT	REVISED -			2539	12-00080-00-RS	COOK	18	13	
	PLOT SCALE = 20'	CHECKED - MEW	REVISED -			CONTRACT NO. 63791					
	PLOT DATE = 2/28/2013	DATE - 2/4/13	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

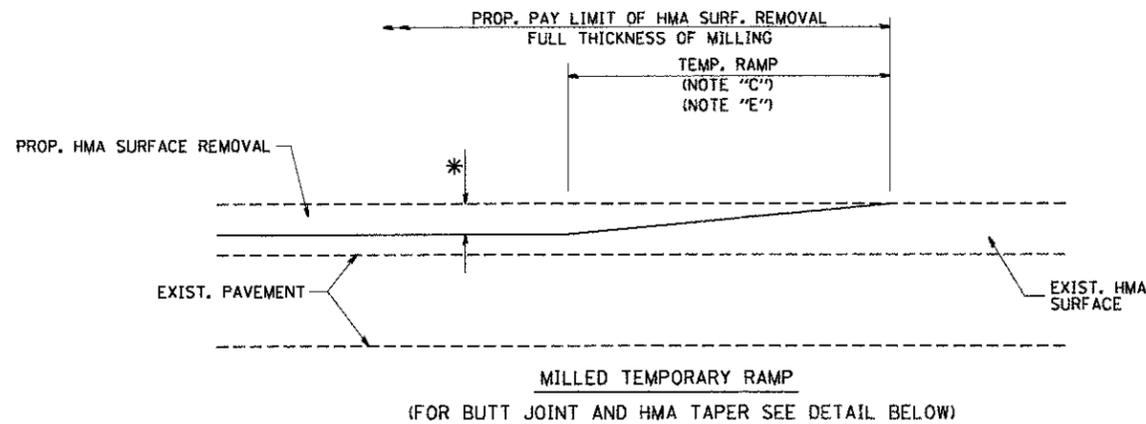
**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

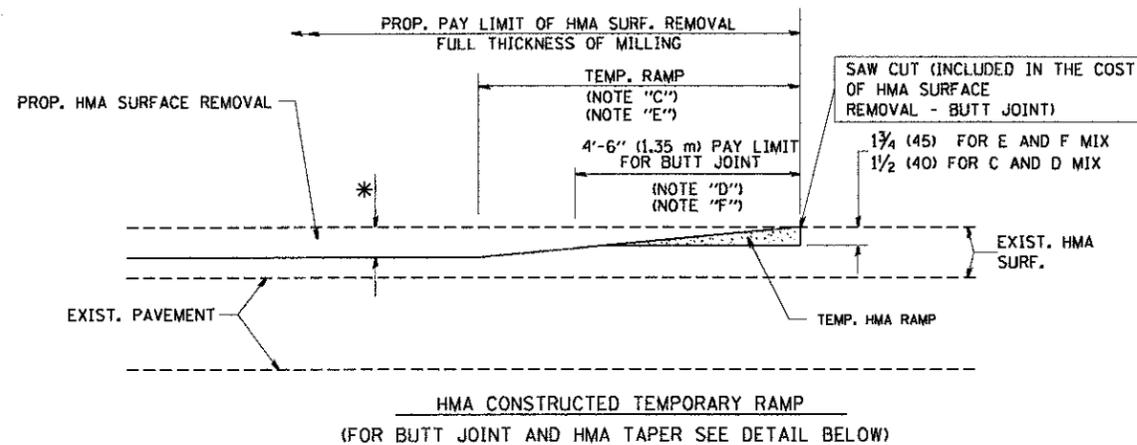
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - A. ABBAS 03-21-97			2539	12-00080-00-RS	COOK	18	14	
		CHECKED -	REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		CONTRACT NO. 63791			
		DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	

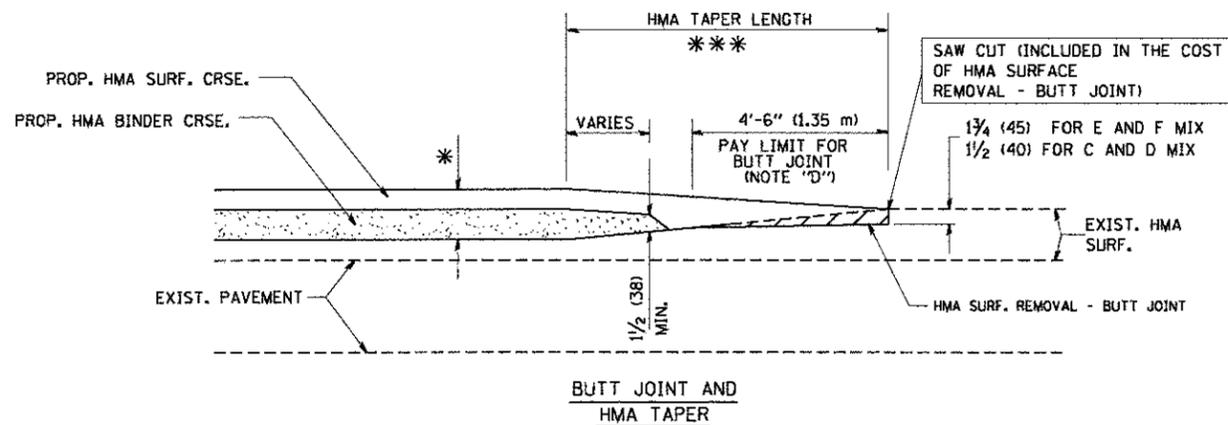




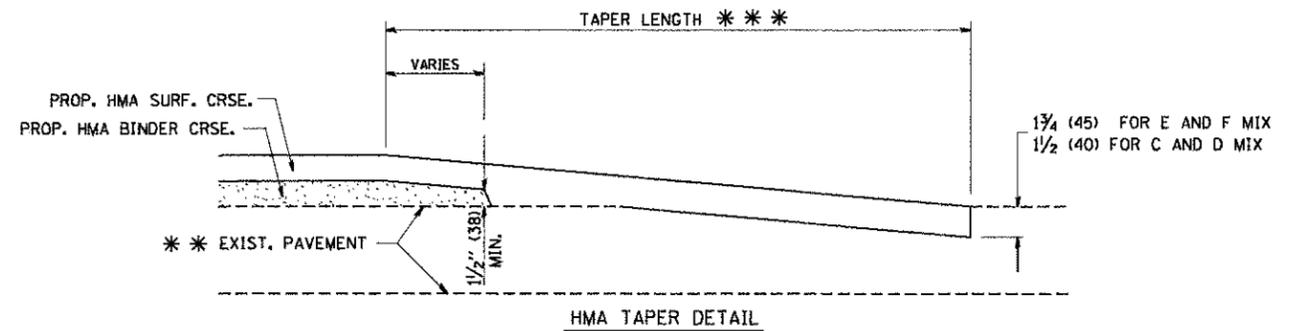
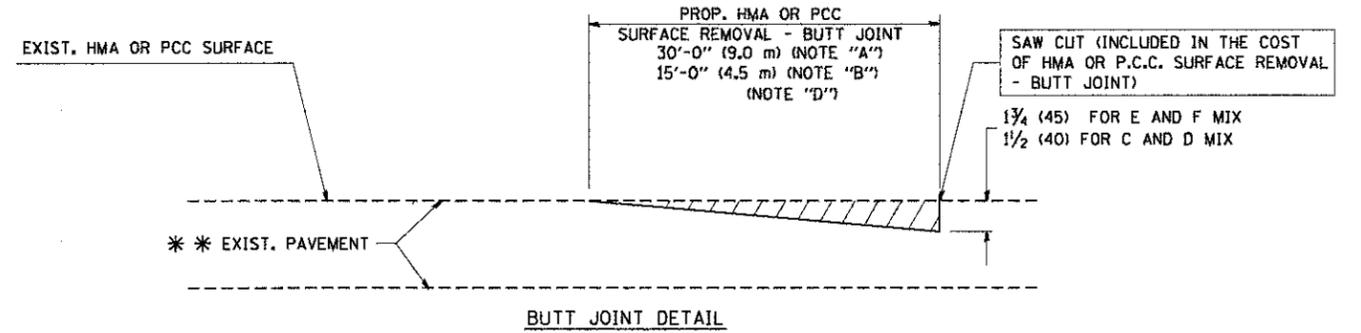
**OPTION 1**



**OPTION 2  
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

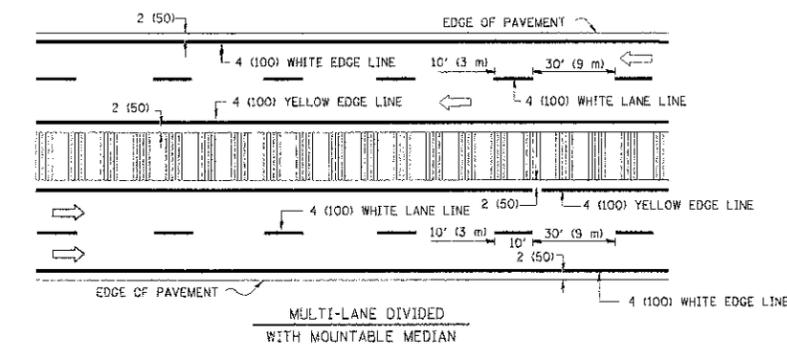
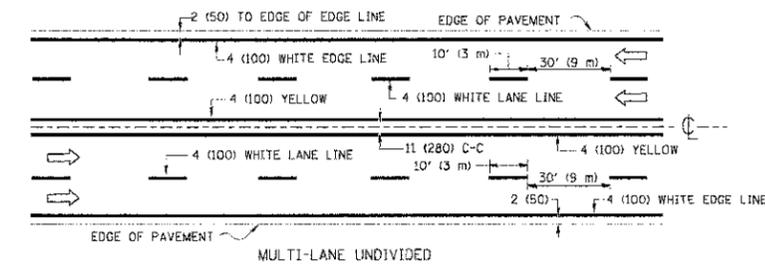
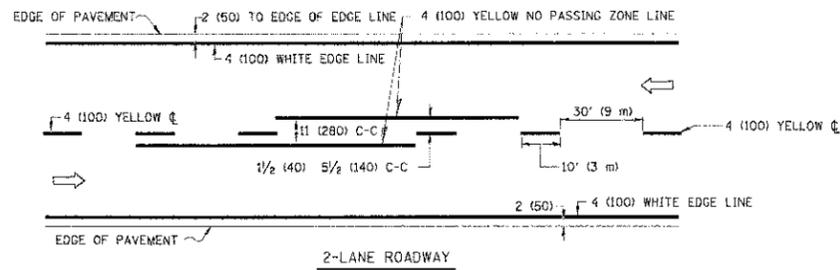
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distrs\22x34\bd32.dgn	USER NAME = gegltonabt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2000	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

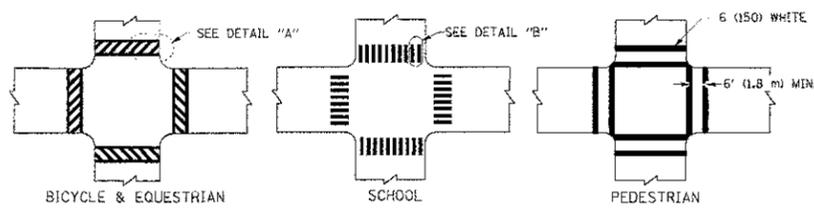
<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	12-00080-00-RS	COOK	18	16
BD400-05 BD32		CONTRACT NO. 63791		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

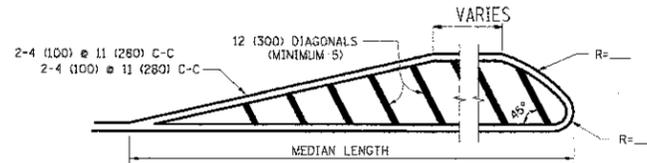
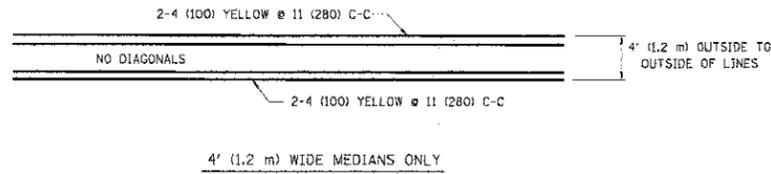


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



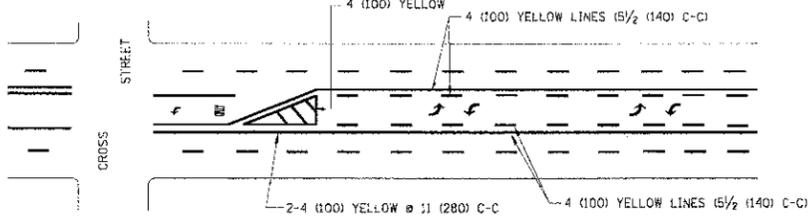
TYPICAL CROSSWALK MARKING



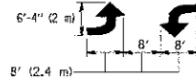
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

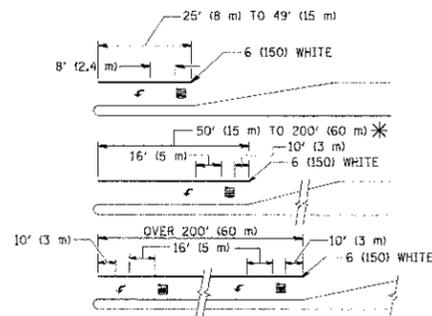


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

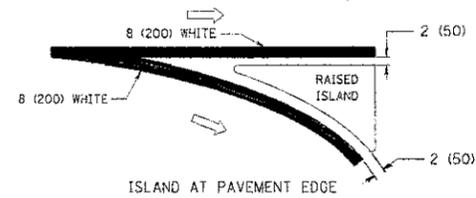
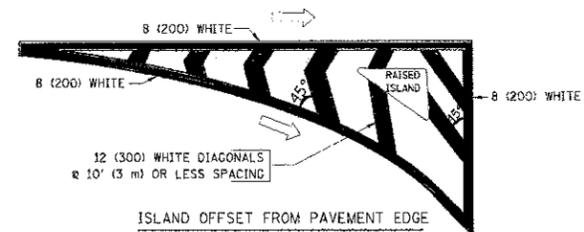


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18" (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

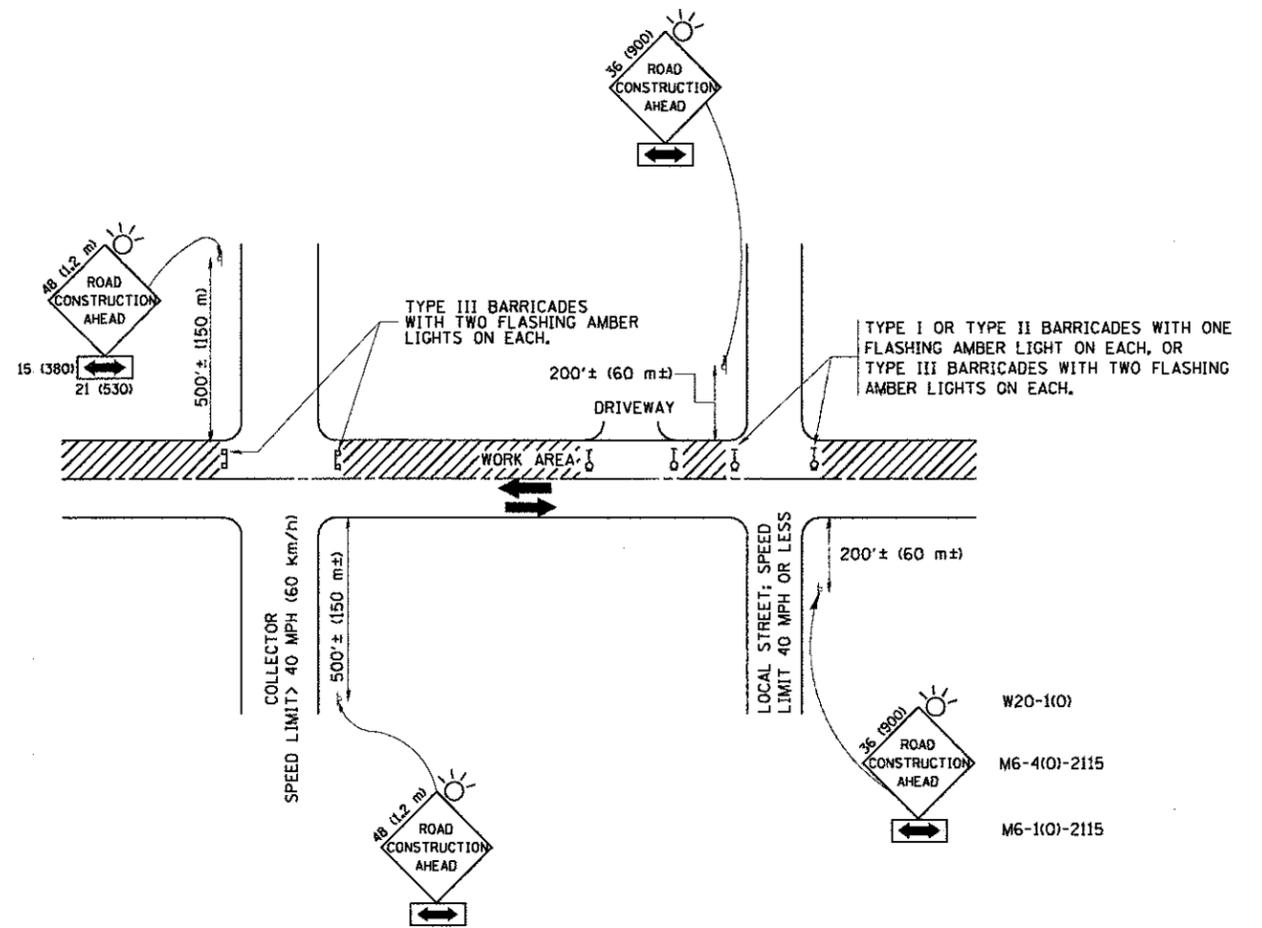
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = d-jvakaegn	DESIGNED EVERS	REVISED -7, RAMMACHNER 10-27-94
	DESIGN	DRAWN	REVISED -C, JUCIUS 09-09-09
	PLD: 56PLR = 50,200' / IN.	CHECKED	REVISED -
	PLD DATE = 9/9/2009	DATE	03-19-90
		REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS		F.A.U. RTE. 2539	SECTION 12-00080-00-RS	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 17
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	CONTRACT NO. 63791		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (MG-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\datastd\22x34\td19.dgn	USER NAME = gagliemobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</b>				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - A. HOUSEH 03-06-96		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	2539	12-00080-00-RS	COOK	18 18
		PLOT SCALE = 50.0000' / IN.	REVISED - A. HOUSEH 10-15-96									<b>TC-10</b>		CONTRACT NO. 63791
		PLOT DATE = 1/4/2028	REVISED - T. RAMMACHER 01-06-00									FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
		DATE - 06-89												