

**GENERAL NOTES:**

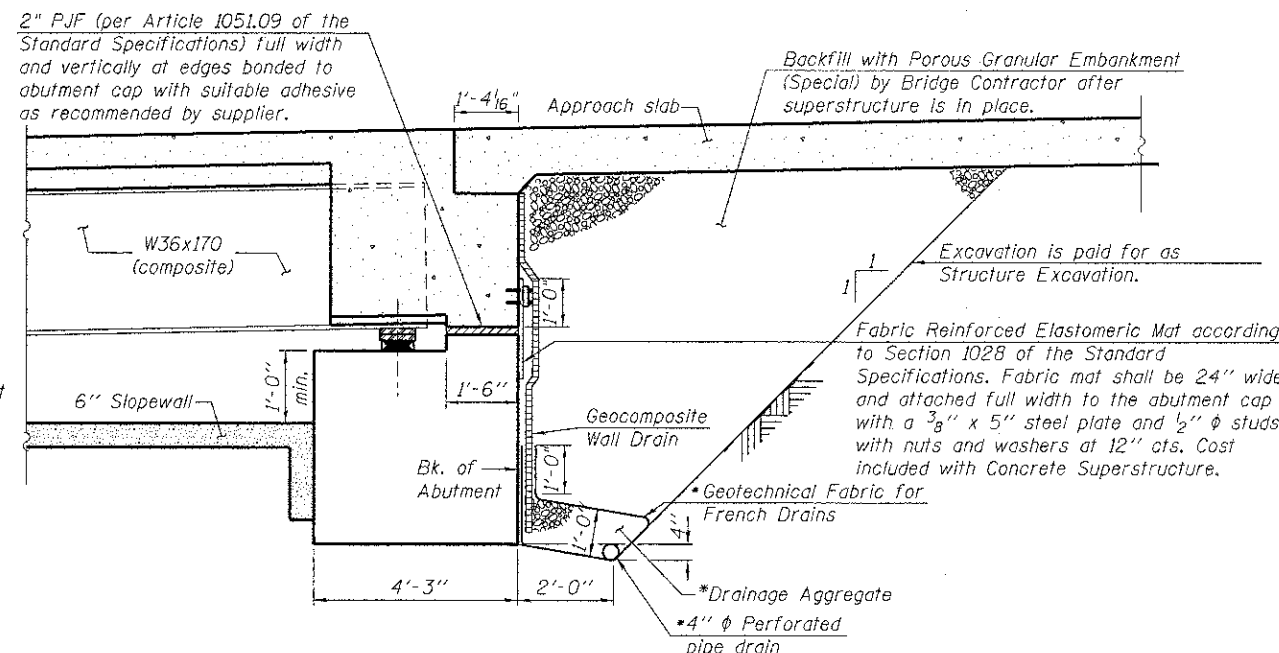
- No field welding is permitted except as specified in the contract documents.
- The Contractor shall test the existing welds by nondestructive methods within 2 ft. of the end of the existing cover plates for cracks after removal of the existing concrete deck. Dye penetrant (PT), magnetic particle (MT), or other approved testing method shall be performed by qualified personnel approved by the Engineer. If cracks are found, report them to the Bureau of Bridges and Structures for disposition. The cost of testing is included in Removal of Existing Concrete Deck. The cost of crack repair, if necessary, will be paid for according to Article 109.04 of the Standard Specifications.
- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of existing concrete. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Bearing seat surfaces shall be constructed or adjusted to the designated elevations within tolerance of 1/8 in. (0.01 ft.). Adjustments shall be made either by grinding the surface or by shimming the bearings.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- The Organic Zinc Rich Primer / Epoxy / Urethane Paint System shall be used for painting of new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception of the exterior surface and the bottom of the bottom flange of fascia beams, masked off construction surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat for all interior surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Reddish Brown, Munsell No. 2.5YR 3/4.
- If the Contractor chooses to alter the temporary cantilevered sheet piling design requirements shown on the plans, a design submittal including plan details and calculations will be required for review and acceptance by the Engineer.
- Cleaning and painting of the existing structural steel shall be as specified in the special provisions for "Cleaning and Painting Existing Steel Structures". All existing steel shall be cleaned per Near White Blast Cleaning - SSPC-SP10. All existing steel shall be painted according to the requirements of Paint System 1 - OZ/E/U. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Reddish Brown, Munsell No. 2.5YR 3/4.
- All new fasteners shall be high strength bolts. Holes shall be subpunched or subdrilled 11/16" dia. and reamed in the field to 13/16" dia. for 3/4" dia. bolts, unless otherwise noted. Holes for splice between existing and proposed beams shall be subpunched or subdrilled 13/16" dia. and reamed in the field to 15/16" dia. for 7/8" dia. bolts, unless otherwise noted.
- Hardwood timbers shall be installed tightly between the top and bottom flange of beams D-F at the East and West Abutments as close as possible to the center of bearing. These should be placed prior to Stage I traffic being shifted. Cost to be included with "Removal of Existing Concrete Deck".
- Permit Load refers to the combination of a 120 kip vehicle with a 80 kip vehicle present in other lanes and in front and behind the 120,000 lb. vehicle. For vehicle axle weight and spacing, see detail on this sheet.
- Slipforming of the parapets is not allowed.
- Complete original and rehabilitation plans are available upon request from the Will County Department of Highways.
- Current Ratings on File for Existing Structure  
Inventory: HS 5.7  
Operating: HS 9.7  
Live Load Restrictions: 15 Tons  
Inventory and Operating Ratings and Live Load Restrictions are provided for information only. Inventory and Operating Ratings are based on HS loading and configuration. Live Load Restrictions are based on Illinois legal loads and configurations. The Ratings and Live Load Restrictions are not necessarily representative of capacities to support the Contractor's equipment.
- The Contractor is advised that the existing structure contains members that are in a deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the existing structure when developing construction procedures for the partial removal of the structure.

**INDEX OF SHEETS**

- S-1 General Plan & Elevation
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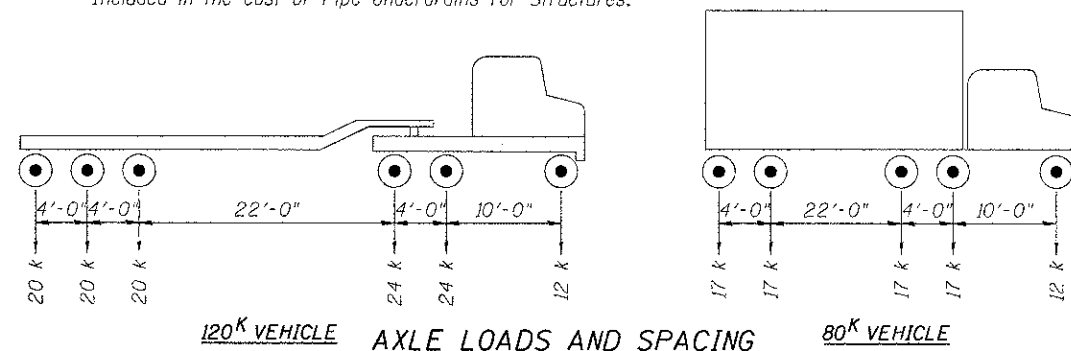
**NOTES:**

- All drainage system components shall run under the wingwall footings and shall extend to 2'-0" from the face of each wingwall except an outlet pipe shall connect with a 4" pipe drain. The pipe drain shall extend to the toe of slope where it shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).
- Porous Granular Embankment and Pipe Underdrains for Structures are billed with the abutments on sheets S-21 and S-22.



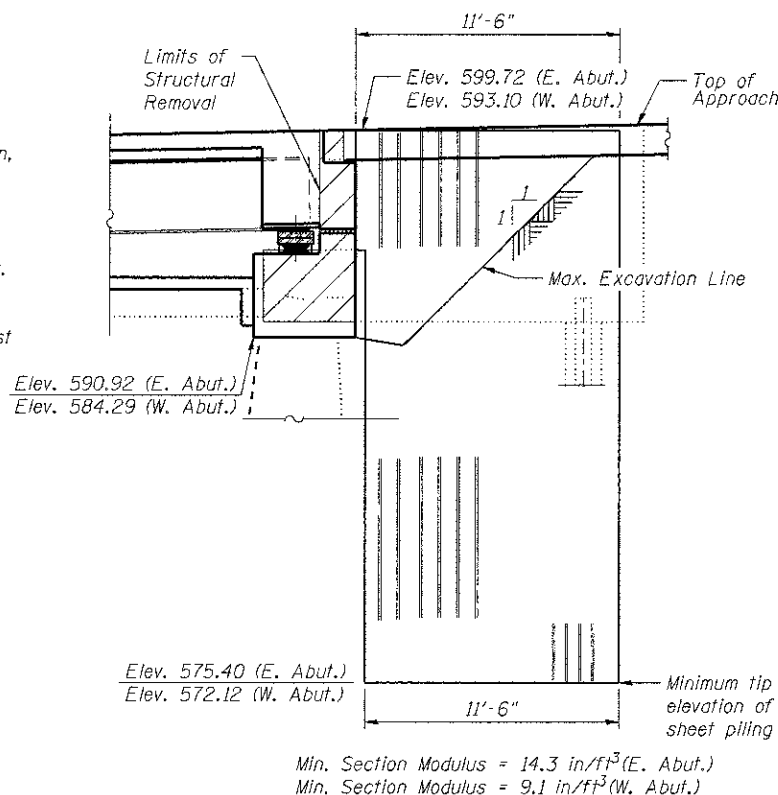
**DRAINAGE DETAILS**

(Horiz. dim. @ Rt. L's)  
\*Included in the cost of Pipe Underdrains for Structures.



**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd		85.6	85.6
Removal Of Existing Concrete Deck	Each	1		1
Structure Excavation	Cu Yd		168	168
Concrete Structures	Cu Yd		121.3	121.3
Concrete Superstructure	Cu Yd	696.5		696.5
Bridge Deck Grooving	Sq Yd	1926		1926
Protective Coat	Sq Yd	2245		2245
Furnishing And Erecting Structural Steel	Pound	238210		238210
Stud Shear Connectors	Each	6996		6996
Cleaning And Painting Structural Steel, Location 1	L Sum	1		1
Reinforcement Bars, Epoxy Coated	Pound	144860	14380	159240
Bar Splicers	Each	827	114	941
Slope Wall 6 Inch	Sq Yd		714	714
Name Plates	Each	1		1
Elastomeric Bearing Assembly, Type I	Each	40		40
Anchor Bolts, 1"	Each	80		80
Geocomposite Wall Drain	Sq Yd		132	132
Porous Granular Embankment, Special	Cu Yd		210	210
Jack And Remove Existing Bearings	Each	10		10
Containment And Disposal Of Lead Paint Cleaning Residues	L Sum	1		1
Drainage Scuppers, DS-33	Each	8		8
Temporary Sheet Piling	Sq Ft		521	521
Pipe Underdrains For Structures 4"	Foot		180	180
Structural Steel Removal	Pound	215780		215780



**TEMPORARY SHEET PILING**

Min. Section Modulus = 14.3 in<sup>3</sup>/ft<sup>3</sup> (E. Abut.)  
Min. Section Modulus = 9.1 in<sup>3</sup>/ft<sup>3</sup> (W. Abut.)

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USER NAME = rdonley	DESIGNED - MHT	REVISED -
PLOT SCALE = 0.883333 / 1 in.	CHECKED - SMY	REVISED -
PLOT DATE = 1/28/2013	DRAWN - SRG	REVISED -
	CHECKED - BWS	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, INDEX OF SHEETS AND BILL OF MATERIAL  
STRUCTURE NO. 099-3031**

SHEET NO. S-2 OF S-24 SHEETS

F.A.I. RTE. 298	SECTION 04-0069-18-BR	COUNTY WILL	TOTAL SHEETS 51	SHEET NO. 23
CONTRACT NO. 63803			ILLINOIS FED. AID PROJECT	