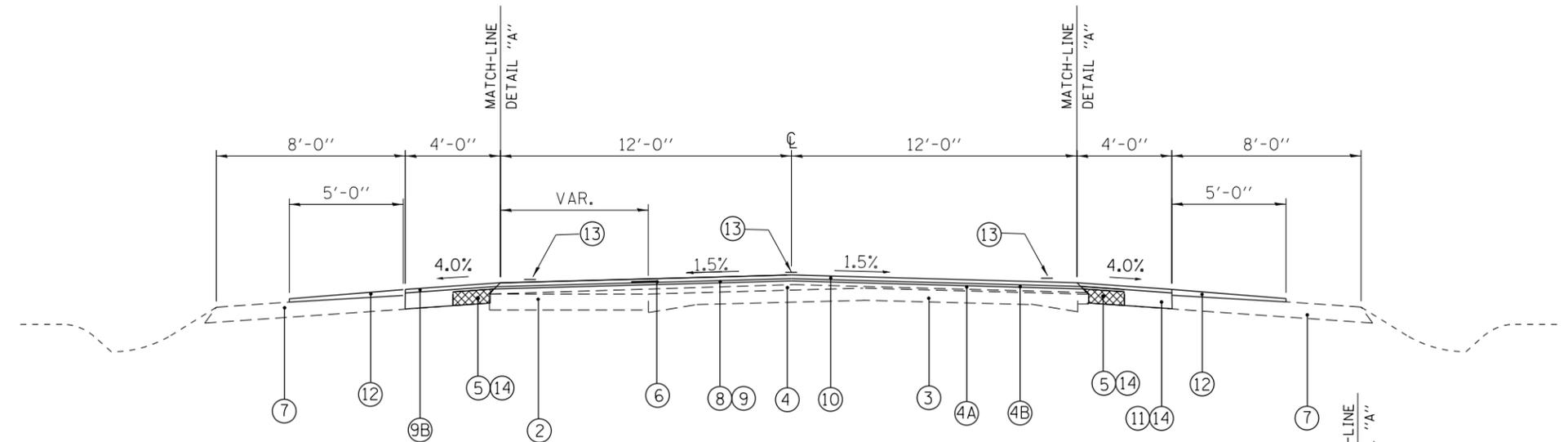


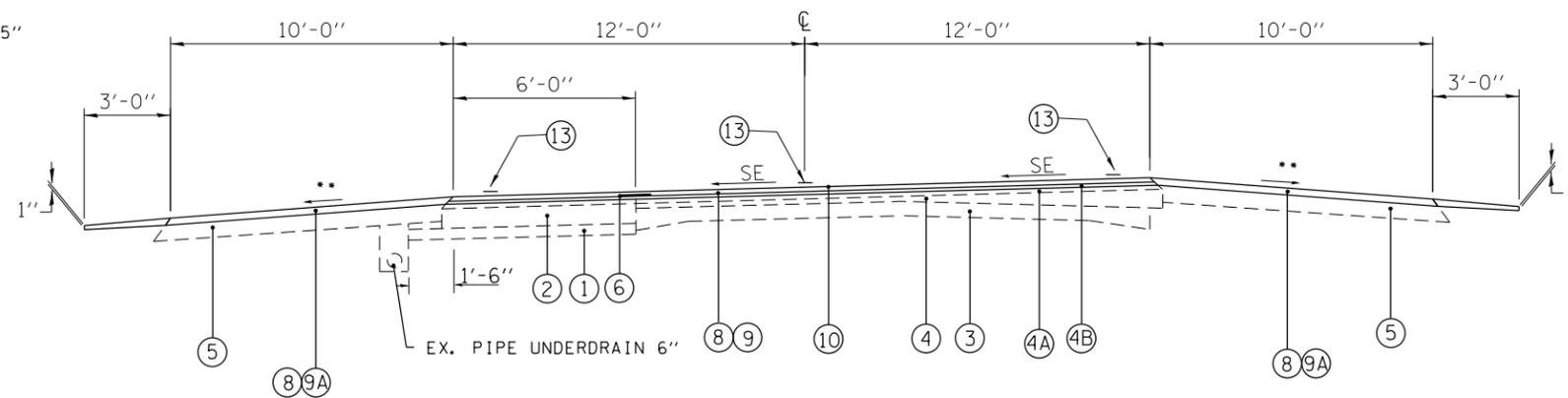
LEGEND

- ① EXISTING AGGREGATE SUB-BASE, 4"
- ② EXISTING PCC BASE COURSE / WIDENING, 8"
- ③ EXISTING PCC PAVEMENT (9" - 6" - 9")
- ④ EXISTING HMA BINDER COURSE (VAR. DEPTH) - 4 1/2" (+/- 1")
- ④A EXISTING HMA LEVELING BINDER
- ④B EXISTING HMA SURFACE
- ⑤ EXISTING HMA SHOULDER, 8"
- ⑥ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑧ PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑨ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, 3/4"
- ⑨A PROPOSED HMA SHOULDERS, 2 1/4"
- ⑨B PROPOSED HMA SHOULDERS, 1 1/2"
- ⑩ PROPOSED HMA SURFACE COURSE MIX "C", N70, 1 1/2"
- ⑪ PROPOSED HMA SHOULDERS, 6 1/2"
- ⑫ PROPOSED AGGREGATE SHOULDERS, TYPE B
- ⑬ PROPOSED MODIFIED URETHANE PAVEMENT MARKING - LINE 5"
- ⑭ PROPOSED EXCAVATE AND GRADE EXISTING SHOULDER

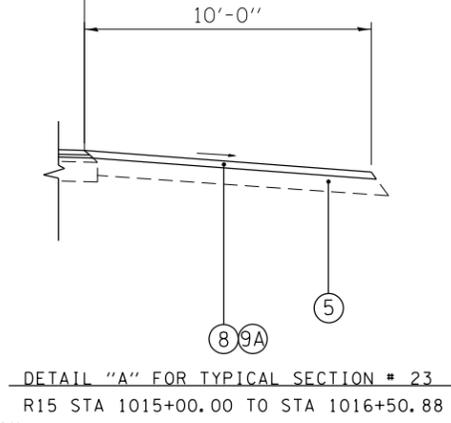


TYPICAL SECTION # 23 (FAP 67 - IL 125)
 R14 STA 912+31.30 TO STA 918+19.03 (BK)
 R15 STA 918+18.96 (AH) TO STA 1015+00.00

STATION EQUATION
 STA 918+19.03 R14 BK =
 STA 918+18.96 R15 AH



TYPICAL SECTION # 24 (FAP 67 - IL 125)
 R15 STA 1016+50.88 TO STA 1021+07.00



DETAIL "A" FOR TYPICAL SECTION # 23
 R15 STA 1015+00.00 TO STA 1016+50.88

** WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0.0% AND 4.0%, THE SHOULDER SLOPE SHALL BE 4.0%. WHEN THE SE RATE OF PAVEMENT EXCEEDS 4.0%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER DOES NOT EXCEED 8.0% ON THE HIGH SIDE SHOULDER, AND MATCHES THE SE ON THE LOW SIDE SHOULDER.

FILE NAME =	USER NAME = sparksgw	DESIGNED - EK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 125 TYPICAL SECTIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Plot Scale = 100.0000' / in.	CHECKED - JM	REVISED -					67	.	CASS	47	23
	PLOT DATE = Oct-17-2012 12:27:33PM	DATE -	REVISED -		SCALE: NTS SHEET 12 OF 13 SHEETS STA. TO STA.			CONTRACT NO. 72F69				
ILLINOIS FED. AID PROJECT												