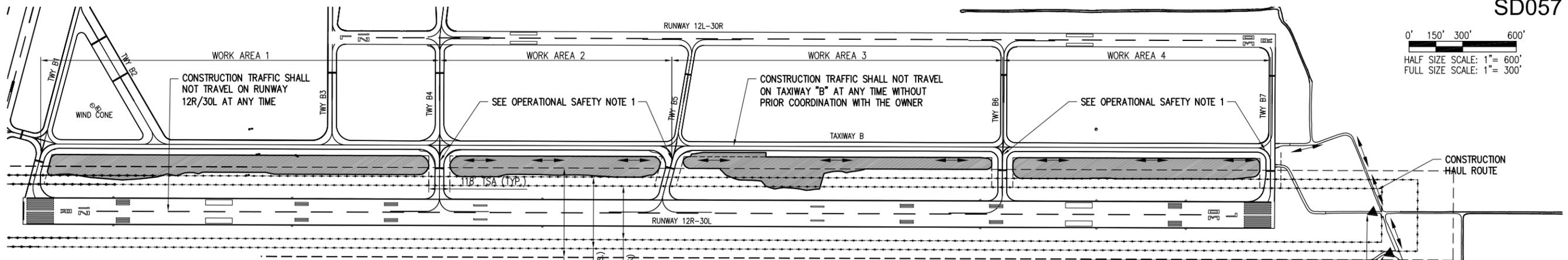


0' 150' 300' 600'
 HALF SIZE SCALE: 1" = 600'
 FULL SIZE SCALE: 1" = 300'



OPERATIONAL SAFETY NOTES

1. FLAGPERSONS AND/OR ESCORTS WITH RADIOS SHALL BE REQUIRED TO CONTROL VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. NO CONSTRUCTION PERSONNEL/EQUIPMENT ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR TAXIWAY SAFETY AREA (TSA) WHEN PAVEMENTS ARE OPEN TO AIRCRAFT TRAFFIC. PAVEMENTS ARE TO BE KEPT FREE OF DEBRIS AT ALL TIMES. ANY DAMAGE TO PAVEMENTS BY THE CONTRACTOR'S FORCES SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONSTRUCTION PERSONNEL WILL BE REQUIRED TO ATTEND AIRFIELD DRIVER SAFETY TRAINING (APPROXIMATELY 1 HOUR) AT THE AIRPORT PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES.
3. DURING WORK IN THIS PHASE, THE OWNER WILL ISSUE A NOTAM TO TEMPORARILY REDUCE THE WIDTH OF THE RUNWAY 12R/30L SAFETY AREA (RSA) FROM 500' WIDE TO 300' WIDE (B-III AIRCRAFT, ≥3/4 MI. VIS.) ANY WORK REQUIRED WITHIN 150' OF THE RUNWAY 12R/30L CENTERLINE WILL REQUIRE THE RUNWAY TO BE SHUTDOWN TO AIRCRAFT TRAFFIC. WORK REQUIRED WITHIN THE REDUCED RUNWAY SAFETY AREA MAY BE REQUIRED TO BE SCHEDULED AROUND CERTAIN PEAK TRAFFIC TIMES, AND MUST BE COORDINATED WITH THE OWNER IN ADVANCE.
4. THE CONTRACTOR SHALL LOCATE THE REDUCED RSA LIMITS FOR THIS WORK AREA AT THE START OF CONSTRUCTION AND WILL PLACE FLAGGED LATHE EVERY 150' ALONG IT WITHIN THE WORK AREA LIMITS. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN THE RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THESE LATHE LINES DURING CONSTRUCTION.
5. THE OWNER RESERVES THE RIGHT TO ALLOW SPECIFIC CRITICAL AIRCRAFT (C-II) OPERATIONS DURING CONSTRUCTION THAT WILL REQUIRE THE STANDARD RSA WIDTH OF 500' TO BE TEMPORARILY REINSTATED. IN THESE INSTANCES, THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY MOVE THEIR PERSONNEL AND EQUIPMENT OUTSIDE OF THE STANDARD RSA LIMITS UNTIL THE AIRCRAFT OPERATION IS COMPLETED (ESTIMATED MAXIMUM 30 MINUTES). THE CONTRACTOR WILL BE GIVEN AS MUCH ADVANCE NOTICE AS POSSIBLE FOR EACH OCCURRENCE. ANTICIPATED FREQUENCY OF THIS OCCURRENCE IS 5 PER WEEK, ON AVERAGE, HOWEVER THE OWNER WILL ATTEMPT TO SCHEDULE CRITICAL AIRCRAFT OPERATIONS OUTSIDE OF THE NORMAL DAILY CONSTRUCTION SCHEDULE AS BEST AS POSSIBLE TO AVOID INTERRUPTIONS TO CONSTRUCTION PROGRESS.
6. IF WEATHER AND ATMOSPHERIC CONDITIONS RESULT IN A LESS THAN 3/4-MILE VISIBILITY, THE OWNER RESERVES THE RIGHT TO MODIFY THE TEMPORARY RUNWAY SAFETY AREA TO A 400' WIDTH IN ORDER TO ALLOW CONTINUED AIRCRAFT OPERATIONS ON THE RUNWAY (B-III AIRCRAFT, <3/4 MI. VIS.) IF THIS SCENARIO OCCURS, ANY WORK REQUIRED WITHIN 200' OF THE RUNWAY 12R/30L CENTERLINE WILL REQUIRE THE RUNWAY TO BE SHUTDOWN TO AIRCRAFT TRAFFIC. WORK REQUIRED WITHIN THE REDUCED RUNWAY SAFETY AREA MAY BE REQUIRED TO BE SCHEDULED AROUND CERTAIN PEAK TRAFFIC TIMES, AND MUST BE COORDINATED WITH THE OWNER IN ADVANCE.

OVERALL PROJECT VIEW

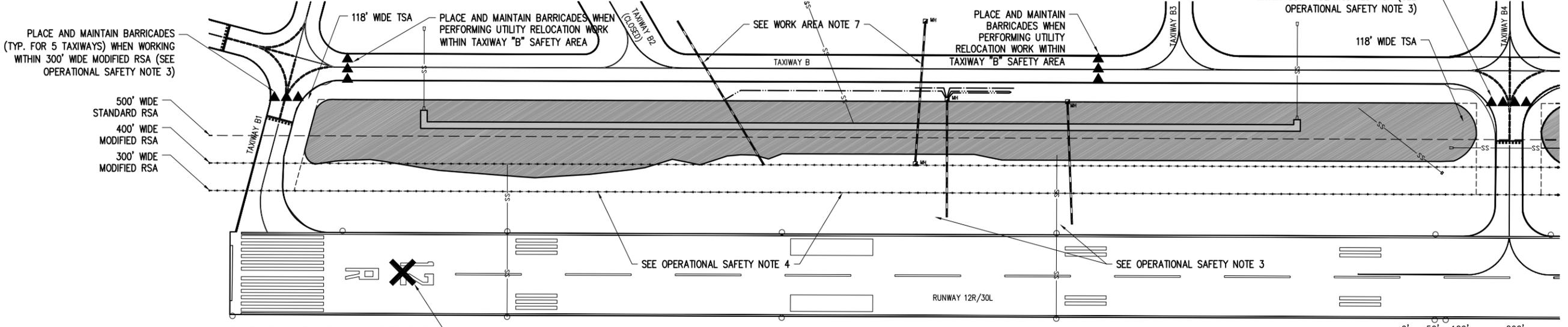
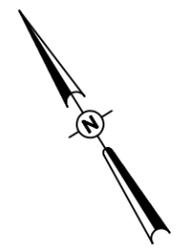
500' STANDARD RSA (C-III AIRCRAFT)
 400' MODIFIED RSA (B-III AIRCRAFT, < 3/4 MI. VIS.)
 300' MODIFIED RSA (B-III AIRCRAFT, ≥3/4 MI. VIS.)

WORK AREA 1 NOTES

1. THE WORK ITEMS TO BE COMPLETED IN THIS AREA INCLUDE UNCLASSIFIED EXCAVATION, CONSTRUCTION OF A CONCRETE PAVED DITCH, MISCELLANEOUS DRAINAGE ITEMS, REROUTING OF UTILITY LINES, SEEDING/MULCHING AND EROSION CONTROL.
2. THE CONTRACTOR SHALL HAVE A MAXIMUM OF 30 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK IN THIS AREA.
3. CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
4. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
5. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROAD, EQUIPMENT PARKING AREA, AND GATE ARE TO BE LEFT IN PLACE IN THEIR PRE-CONSTRUCTION CONDITION.
6. ANY UTILITIES NOTED TO BE REROUTED AS PART OF THIS WORK AREA SHALL BE COMPLETED WITHIN NORMAL CONSTRUCTION HOURS SUCH THAT NO SINGLE UTILITY LINE IS OUT OF SERVICE OVERNIGHT, UNLESS APPROVAL IS OBTAINED FROM THE OWNER PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND RESIDENT ENGINEER IN ADVANCE IN ORDER TO PROPERLY SHUTDOWN THE UTILITIES FOR REROUTING.
7. THE PROPOSED WORK ASSOCIATED WITH RELOCATION OF ANY PHONE COMPANY LINES SHALL BE CLOSELY COORDINATED AT THE BEGINNING OF THE PROJECT IN ORDER TO SCHEDULE THE APPROPRIATE CONSTRUCTION ITEMS TO RUN CONSECUTIVELY BETWEEN THE CONTRACTOR AND PHONE COMPANY.
8. THE CONTRACTOR SHALL MAINTAIN NATURAL WATER FLOW THROUGHOUT CONSTRUCTION TO ALLOW FOR COMPLETE REALIGNMENT OF THE DITCH. CONTRACTOR SHALL SUBMIT A WATER/SITE MAINTENANCE PLAN TO THE OWNER FOR REVIEW AND APPROVAL PRIOR TO BEGINNING CONSTRUCTION.
9. THE COSTS FOR ALL ITEMS ASSOCIATED WITH THIS SAFETY PLAN ARE TO BE INCLUDED IN THE COST OF OTHER WORK ITEMS, UNLESS OTHERWISE NOTED WITHIN THE PLAN SET.

LEGEND

- [Solid Line] EXISTING IMPROVEMENTS
- [Dashed Line] WORK AREA LIMITS
- [Cross-hatch] PROPOSED EQUIPMENT PARKING AREA
- [Diagonal-hatch] PROPOSED EARTHWORK STOCKPILE AREA
- [Line with X] EXISTING FENCE
- [Triangle] PROPOSED BARRICADES
- [Arrow] PROPOSED HAUL ROUTE
- [T-shape] CONSTRUCTION SIGN:
 - [Diamond with A] "CONSTRUCTION TRAFFIC →"
 - [Diamond with B] "← CONSTRUCTION TRAFFIC"



WORK AREA 1 VIEW

0' 50' 100' 200'
 HALF SIZE SCALE: 1" = 200'
 FULL SIZE SCALE: 1" = 100'

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REVISION	DATE

SAINT LOUIS DOWNTOWN AIRPORT
 A Division of Bi-State Development Agency
 BLOCK GRANT PROJ.: 3-17-0039-B29
 IL PROJ.: CPS-4210

Hanson Project No.	11A0190
Filename	C-005-SFY.dwg
Scale	AS SHOWN
Date	03/08/13
LAYOUT	BSS 01/31/13
DRAWN	MLH 02/01/13
REVIEWED	BSS 03/08/13

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 Springfield, Illinois 62703-2886

GRADE DITCH PARALLEL TO MAIN RUNWAY
 PROPOSED CONSTRUCTION SAFETY PLAN - WORK AREA 1