

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567 2569	18-00119-00-RS	DUPAGE	37	1
ILLINOIS CONTRACT NO.			61F50	

FOR INDEX OF SHEETS AND
HIGHWAY STANDARDS
SEE SHEET 2

04-26-2019 LETTING ITEM 135

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAU ROUTE 2567 (BLANCHARD STREET)
HAWKINS CIRCLE TO PRESIDENT STREET
FAU ROUTE 2569 (PRESIDENT STREET)
BLANCHARD STREET TO IL ROUTE 38 (ROOSEVELT ROAD)
RESURFACING, CURB AND SIDEWALK
SECTION: 18-00119-00-RS
PROJECT: BDF3(494)
CITY OF WHEATON
DUPAGE COUNTY
JOB NO: C-91-146-18



LOCATION OF SECTION INDICATED THIS: - [Symbol] -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED Nov 26, 2018
Paul Hedner
CITY OF WHEATON, DIRECTOR OF ENGINEERING

PASSED 3/8/2019
Scott McQuinn
DISTRICT ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR
LIMITED REVIEW MARCH 8, 2019
Anthony J. Drangley/CAS
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PROFESSIONAL ENGINEER'S CERTIFICATION

I HEREBY CERTIFY THAT THIS SUBMISSION WAS PREPARED UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS 26th DAY OF November, 2018.



JAMES M. YURATOVAC
ILLINOIS REG. PROF. ENGINEER NO.062-060059 EXPIRATION DATE 11-30-2019

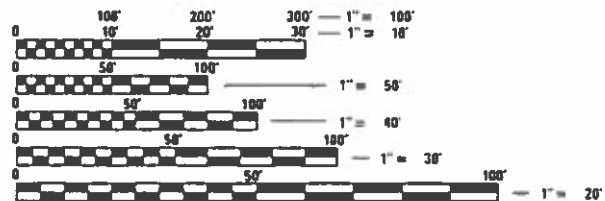
PROGRAM AND OFFICE ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

BLANCHARD STREET

DESIGN DESIGNATION: MAJOR COLLECTOR
SPEED LIMIT = 30 MPH
TRAFFIC = 10,500 ADT (2016)

PRESIDENT STREET

DESIGN DESIGNATION: MAJOR COLLECTOR
SPEED LIMIT = 30 MPH
TRAFFIC = 7,500 ADT (2016)

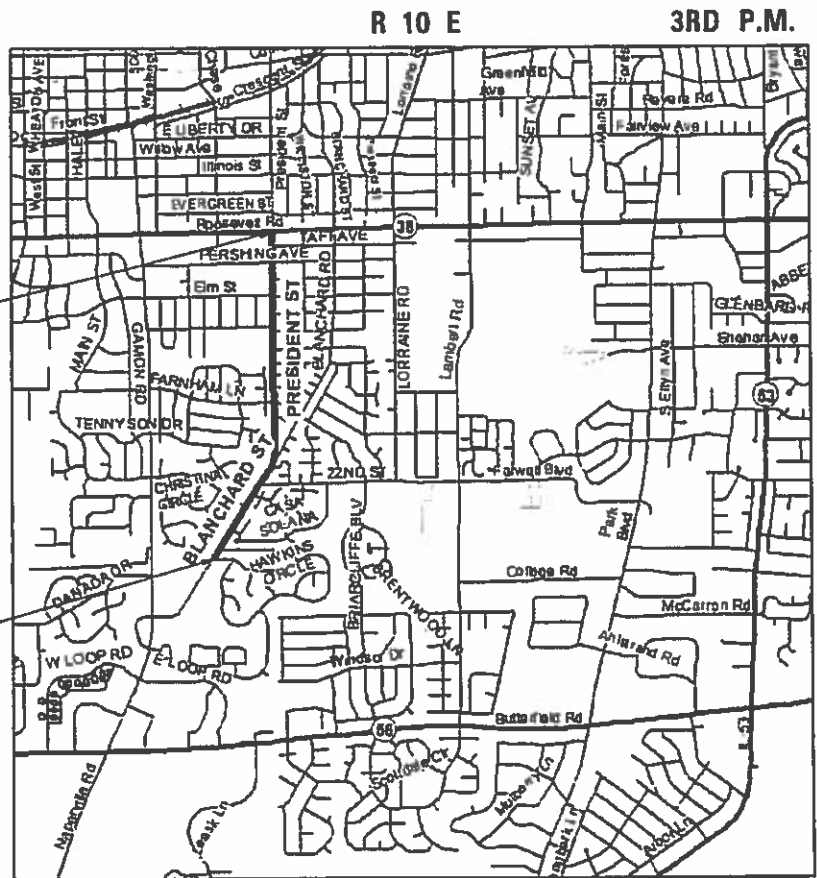


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT MANAGER: JIM YURATOVAC

CONTRACT NO. 61F50



MILTON TOWNSHIP
LOCATION MAP
N.T.S.

BLANCHARD STREET/PRESIDENT STREET GROSS LENGTH = 7128 FT. = 1.35 MILES
BLANCHARD STREET/PRESIDENT STREET NET LENGTH = 7128 FT. = 1.35 MILES



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DISTRICT 1 DETAILS

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BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
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TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

HIGHWAY STANDARDS

000001-07	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-04	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-05	MID-BLOCK CURB RAMPS FOR SIDEWALKS
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701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
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701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
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780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES:

- 1 ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, APRIL 1, 2016 AND SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS 2019.
- 2 ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- 3 THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THEIR AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- 4 THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVEL BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE PAVED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL.
- 5 THE CONTRACTOR SHALL USE 2 CHANGEABLE MESSAGE SIGNS AT LOCATIONS TO BE DETERMINED BY THE ENGINEER FOR A PERIOD FROM ONE WEEK PRIOR TO THE START OF CONSTRUCTION TO THE CONCLUSION OF THE PROJECT.
- 6 THE CONTRACTOR SHALL ENSURE ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
- 7 THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO THE USE OF WATER.
- 8 IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SELECT DUMPING SITES. HE/SHE SHALL PROVIDE A LIST OF THESE SITES TO THE ENGINEER FOR HIS/HER EXAMINATION AND GENERAL INFORMATION.
- 9 ALL EXCESS MATERIAL (BROKEN CONCRETE, ASPHALT, CULVERT PIPE, WASTE ROADWAY EXCAVATION, AND SURPLUS MATERIALS FROM UTILITY TRENCHES) SHALL BE WASTED OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY.
- 10 ALL PROPERTY AND SURFACE STRUCTURES WITHIN THE RIGHT-OF-WAY SHALL BE PROTECTED DURING CONSTRUCTION OPERATIONS UNLESS THE ENGINEER DIRECTS REMOVAL FOR PURPOSES RELATED TO CONSTRUCTION UNDER THIS CONTRACT. ANY FENCES, POLES, FLAGSTONE, DECORATIVE STONE, SPECIAL LANDSCAPING, OR OTHER MAN MADE SURFACE IMPROVEMENT WHICH IS REMOVED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY HIM TO ITS ORIGINAL CONDITION AFTER THE CONSTRUCTION ACTIVITIES ARE COMPLETED.
- 11 ALL FRAMES, GRATES, LIDS, FIRE HYDRANTS, AND VALVE BOXES WHICH ARE REMOVED AND ARE TO BE ABANDONED SHALL REMAIN THE PROPERTY OF THE CITY OF WHEATON. UPON REMOVAL FROM THEIR WORKING LOCATIONS, THEY ARE TO BE STOCKPILED AT AN ON-SITE LOCATION DETERMINED BY THE ENGINEER WHERE THEY WILL BE PICKED UP BY CITY PERSONNEL AT THE CONCLUSION OF WORK ON THAT STREET.
- 12 WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE AND PUBLIC DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO ACCEPT ALL STORM WATER THAT WILL BE DELIVERED BY THESE DRAINS AND BASINS AND SHALL DISCHARGE THE SAME. IF NECESSARY, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT AND TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF WATER WHICH IS RECEIVED FROM ALL TEMPORARY CONNECTIONS. THESE TEMPORARY FACILITIES SHALL BE MAINTAINED UNTIL ALL PERMANENT CONNECTIONS ARE COMPLETED.
- 13 EROSION AND SEDIMENT CONTROL MEASURES SHALL BE FOLLOWED WHEN EXISTING CURB AND GUTTER IS REMOVED AND EXISTING DRAINAGE STRUCTURES ARE TO REMAIN ACTIVE. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE DETAIL PROVIDED IN THE CONTRACT DOCUMENTS.
- 14 THE THICKNESS OF THE HMA SHOWN ON THE PLANS IS NOMINAL. DEVIATIONS MAY OCCUR IN THE FIELD DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE UPON WHICH THE HMA IS BEING PLACED.
- 15 THE LOCATIONS AND ELEVATIONS OF THE VARIOUS UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE NOT TO BE TAKEN AS EXACT. THE CONTRACTOR SHALL EXERCISE CARE WHEN CONDUCTING CONSTRUCTION OPERATIONS NEAR UTILITIES TO PREVENT DAMAGE. THE FAILURE OF A UTILITY COMPANY TO ACCURATELY LOCATE THEIR UTILITY DOES NOT FREE THE CONTRACTOR FROM RESPONSIBILITY. THE MAJOR CONCERN OF THE CITY OF WHEATON IS PUBLIC SAFETY.
- 16 THE CONTRACTOR SHALL COOPERATE WITH THE CITY IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE CITY MAY WANT TO PERFORM DURING THE CONTRACTOR'S OPERATIONS.

- 17 THE CONTRACTOR SHALL HAVE LINE AND/OR FORMS SET A MINIMUM OF FOUR WORKING HOURS PRIOR TO THE SCHEDULED ARRIVAL OF CONCRETE ON SITE FOR THE PLACEMENT OF CURB AND GUTTER, DRIVEWAYS, AND SIDEWALK TO ALLOW THE ENGINEER TIME TO CHECK LINE AND GRADE.
- 18 ALL RADII FOR PROPOSED COMBINATION CONCRETE CURB AND GUTTER SHALL BE PLACED AT THE EXISTING DIMENSION UNLESS OTHERWISE INDICATED ON THE PROJECT PLANS OR AS DIRECTED BY THE ENGINEER. ELEVATIONS SHOWN AT POINT ON THE CURB INDICATED FLOW LINE ELEVATIONS UNLESS NOTED OTHERWISE.
- 19 WHEN WATER SERVICE BOXES FALL WITHIN THE LIMITS OF CONCRETE FLATWORK, THE CONTRACTOR SHALL PROVIDE EITHER A SECTION OF 4" POLY VINYL CHLORIDE (PVC) PIPE OR 4" HIGH DENSITY POLYETHYLENE (HDPE) PIPE TO SLEEVE THE BOX. THE SLEEVE SHALL EITHER BE REMOVED OR TRIMMED TO MATCH THE FINISHED CONCRETE GRADE LEVEL. THE FINAL RESULT SHALL BE A SMOOTH FINISHED "BOX-OUT" AROUND THE SERVICE BOX WHICH SHALL FACILITATE EASY REMOVAL OF THE CAP AT MEET THE FINISHED GRADE. ALL WATER SERVICE BOXES WITHIN PAVEMENT RESURFACING SHALL BE ADJUSTED TO MEET THE FINISHED GRADE LEVEL.
- 20 WHEN A SEWER STRUCTURE FALLS WITHIN THE LIMITS OF A CONCRETE DRIVEWAY, THE CONTRACTOR SHALL PLACE EXPANSION MATERIAL IN A BOX APPROXIMATELY EIGHTEEN (18") INCHES FROM THE CENTER OF THE LID FORMING A SQUARE "BOX-OUT" AROUND THE FRAME. THE RESULT SHALL BE A CONDITION THAT WILL ALLOW FOR THE REMOVAL OF THE SQUARE OF CONCRETE FROM THE DRIVEWAY FOR THE PURPOSE OF REPLACING THE FRAME WITHOUT DAMAGING THE REMAINDER OF THE DRIVEWAY PAVEMENT.
- 21 ALL CONSTRUCTION PERSONNEL SHALL BE REQUIRED TO WEAR AN ANSI/ISEA 107-2015 COMPLIANT SAFETY VEST AT ALL TIMES WHILE ON THE CONSTRUCTION SITE.
- 22 WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- 23 BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD THE LOCATIONS OF ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE REESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- 24 IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- 25 THE CONTRACTOR SHALL PROVIDE SAFE AND ORDERLY PASSAGE FOR TRAFFIC AND PEDESTRIANS WHERE CONSTRUCTION OPERATIONS IMPACT PUBLIC THOROUGHFARES AND ADJACENT PROPERTY. THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- 26 ALL SIDEWALK CURB RAMPS SHALL BE COMPLIANT WITH ALL APPLICABLE ADA STANDARDS AND INCLUDED DETAILS. INDIVIDUAL DESIGN DETAILS HAVE BEEN PROVIDED FOR ALL LOCATIONS WITH AN EXISTING SLOPE OVER 5%.
- 27 A NOMINAL QUANTITY HAS BEEN INCLUDED FOR THE FOLLOWING PAY ITEMS:
- EARTH EXCAVATION
- REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- AGGREGATE SUBGRADE IMPROVEMENT
- CLASS D PATCHES
- 28 ALL UTILITY STRUCTURES SHALL REMAIN ACCESSIBLE DURING CONSTRUCTION. WHEN UTILITY STRUCTURES ARE TO BE ADJUSTED UNDER TRAFFIC, TEMPORARY RAMPS SHALL BE CONSTRUCTED AND MAINTAINED AT BOTH UPSTREAM AND DOWNSTREAM ENDS OF THE UTILITY CASTING IN ACCORDANCE WITH ARTICLE 406.08.

GENERAL NOTES - SEWERS:

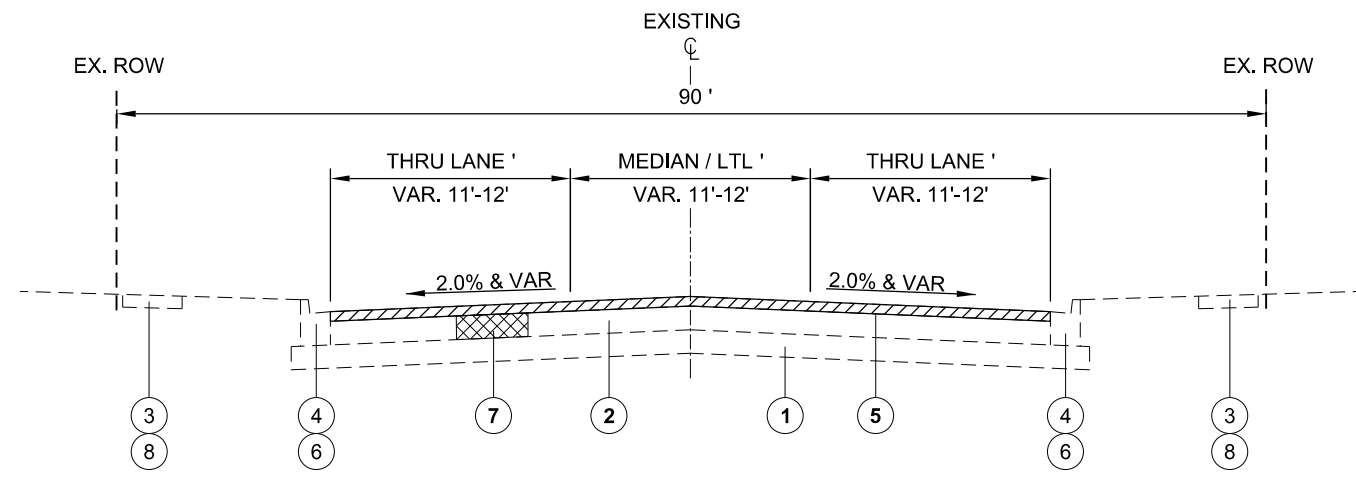
- 29 ALL FRAMES WITH CLOSED LIDS BEING FURNISHED FOR THIS PROJECT AS CONSTRUCTION, ADJUSTMENT, OR RECONSTRUCTION OF ANY MANHOLE, CATCH BASIN, INLET, OR WATER VALVE VAULT SHALL HAVE CAST INTO THE LID ONE OR MORE OF THE FOLLOWING WORDS:
- "STORM" - FOR STORM SEWER MANHOLE LIDS
- "SANITARY" - FOR SANITARY MANHOLE LIDS
- "WATER" - FOR WATER SYSTEM STRUCTURE LIDS
- "FISH" SYMBOL DIRECTLY ON THE CURB BACK - FOR CURB INLET FRAMES
- 30 IF AT ANY TIME DURING THE CONSTRUCTION OF THIS PROJECT LOOSE MATERIAL IS DEPOSITED INTO THE FLOW LINE OF A SEWER STRUCTURE IN SUCH A WAY AS TO RESTRICT OR OBSTRUCT THE NATURAL FLOW OF WATER IN THE STRUCTURE, THE MATERIAL SHALL BE REMOVED BEFORE THE CLOSE OF THE WORKING DAY DURING WHICH IT WAS DEPOSITED. AT THE CONCLUSION OF CONSTRUCTION ACTIVITIES, AND BEFORE THE ENGINEER ACCEPTS THE PROJECT, ALL SEWERS AND SEWER STRUCTURES THAT FALL WITHIN THE LIMITS OF THE PROJECT SHALL BE FREE OF CONSTRUCTION DEBRIS AND LOOSE MATERIAL.

SUMMARY OF QUANTITIES

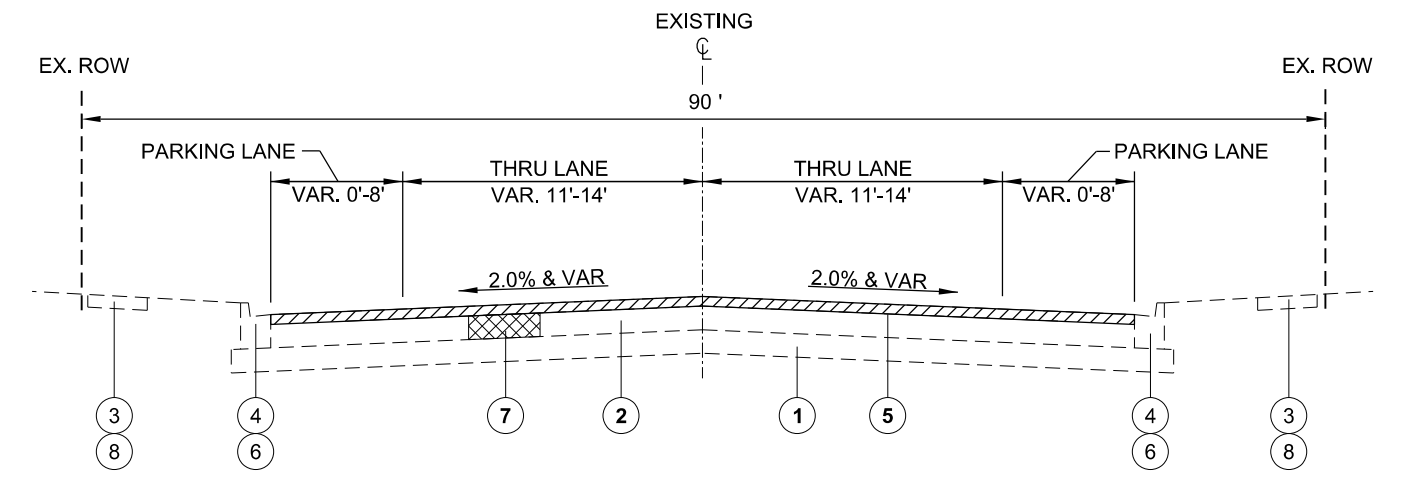
CODE NO.	ITEM	UNIT	TOTAL QTY. CONSTR. CODE 0005 STU 70/30
20101200	TREE ROOT PRUNING	EACH	10
20200100	EARTH EXCAVATION	CU YD	20
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	20
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	524
25200110	SODDING, SALT TOLERANT	SQ YD	524
25200200	SUPPLEMENTAL WATERING	UNIT	17
28000510	INLET FILTERS	EACH	61
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	572
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	20
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	20171
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1673
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	386
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3347
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	11097
42400800	DETECTABLE WARNINGS	SQ FT	642
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	29882
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1906

CODE NO.	ITEM	UNIT	TOTAL QTY. CONSTR. CODE 0005 STU 70/30
44000600	SIDEWALK REMOVAL	SQ FT	11239
44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD	149
44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	299
44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	448
44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	598
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	8
60255500	MANHOLES TO BE ADJUSTED	EACH	6
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	2
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	16
60260600	INLETS TO BE ADJUSTED WITH NEW TYPE 4 FRAME AND GRATE	EACH	1
60261300	INLETS TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	3
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1893
67100100	MOBILIZATION	L SUM	1
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	130
70300100	SHORT TERM PAVEMENT MARKING	FOOT	6218

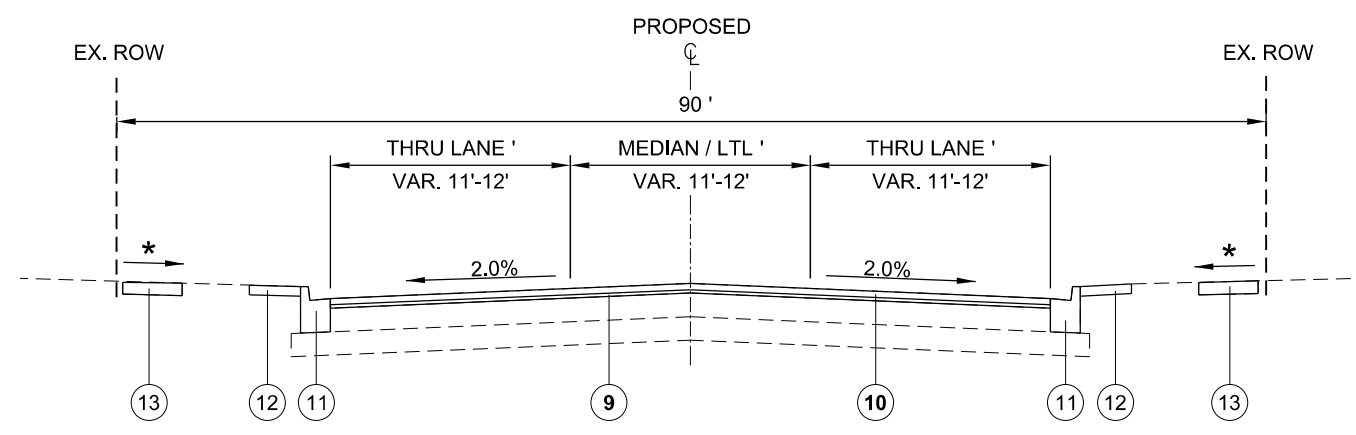
* SPECIALTY ITEMS



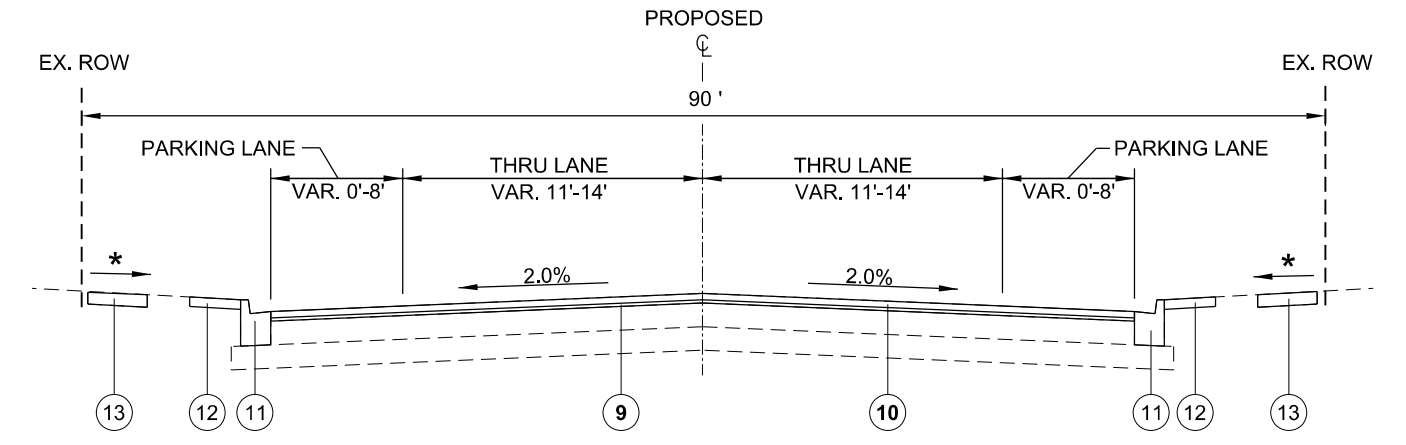
EXISTING TYPICAL SECTION
BLANCHARD STREET
STA. 11+85 TO STA. 37+00
PRESIDENT STREET
STA. 37+00 TO STA. 56+69



EXISTING TYPICAL SECTION
PRESIDENT STREET
STA. 56+69 TO STA. 81+00



PROPOSED TYPICAL SECTION
BLANCHARD STREET
STA. 11+85 TO STA. 37+00
PRESIDENT STREET
STA. 37+00 TO STA. 56+69



PROPOSED TYPICAL SECTION
PRESIDENT STREET
STA. 56+69 TO STA. 81+00

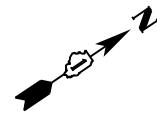
- LEGEND**
- ① EX SUB-BASE GRAN MATL, THICKNESS VARIES
 - ② EX HMA PAVEMENT, 13"
 - ③ EX PCC SIDEWALK
 - ④ EX COMB. CONC. CURB AND GUTTER
 - ⑤ HMA SURFACE REMOVAL, 3"
 - ⑥ COMB. CURB AND GUTTER REMOVAL (SEE NOTE 1)
 - ⑦ PR CLASS D PATCHES, 7" (SEE NOTE 1)
 - ⑧ PR SIDEWALK REMOVAL (SEE NOTE 1)
 - ⑨ PR. POLY. LEVELING BINDER (MM), IL-4.75, N50, 1"
 - ⑩ PR. HMA SURFACE CRS, MIX "D", N70, 2"
 - ⑪ PR COMB CONC CURB AND GUTTER, TY B-6.12 (SEE NOTE 1)
 - ⑫ PR SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 4" (SEE NOTE 1)
 - ⑬ PR. P.C.C. SIDEWALK, 5" (SEE NOTE 1)

- LEGEND**
- REMOVAL ITEMS

- NOTES:**
1. LOCATIONS WILL BE SPECIFIED BY THE ENGINEER IN THE FIELD DURING CONSTRUCTION
 2. PR AGG BASE COURSE IS INCLUDED IN THE PCC SIDEWALK PAY ITEM. SEE PROJECT SPECIFICATIONS FOR DETAILS.
- * 1.5% DESIRABLE, 2% MAXIMUM

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
ROADWAY RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% @ 70 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 Gyr.
CLASS D PATCHING	
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	4% @ 70 Gyr.

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIALS PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
4. THE CONTRACTOR SHALL MILL BEFORE PATCHING.



POT. STA. 10+00.00

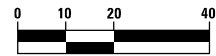
10+00

BLANCHARD STREET

PROJECT BEGINS
STA. 11+84

PR. P.C.C. SIDEWALK, 5"
45 SQFT
PR. DETECTABLE WARNINGS
10 SQFT
PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 12 FT

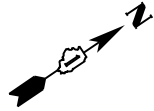
MATCH LINE STA. 15+00



HORIZONTAL SCALE IN FEET

NOTES:

- FOR SIDEWALK DETAILS, SEE SHEETS 20-29
- ANY NECESSARY TYPE B CURB FOR MEETING ADA RAMP GRADES AT THE BACK OF SIDEWALK SHALL BE POURED MONOLITHICALLY WITH THE SIDEWALK AND INCLUDED IN THE COST OF THE SIDEWALK.
- A MINIMUM TRANSITION OF 18" WILL BE USED FROM DEPRESSED CURB TO FULL CURB HEIGHT (TYP).
- ALL STRUCTURE ADJUSTMENTS SHALL BE WITH TYPE 1 CLOSED LID, UNLESS OTHERWISE SPECIFIED.
- PRIOR TO ADJUSTMENT OF ANY DUPAGE WATER COMMISSION STRUCTURES, THE CONTRACTOR SHALL CONTACT DWC AT 630-834-0100 A MINIMUM OF 48 HOURS IN ADVANCE OF THE WORK. DWC WILL SUPPLY A NEW FRAME AND LID IF THEY DETERMINE IT SHOULD BE REPLACED.



PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 11 FT
PR. DETECTABLE WARNINGS
9 SQFT
PR. P.C.C. SIDEWALK, 5"
223 SQFT
PR. DETECTABLE WARNINGS
9 SQFT
PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 10 FT

HAWKINS CIRCLE

PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 10 FT
PR. DETECTABLE WARNINGS
9 SQFT
PR. P.C.C. SIDEWALK, 5"
245 SQFT
PR. DETECTABLE WARNINGS
9 SQFT
PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 10 FT

TARA COURT

PR. DETECTABLE WARNINGS
10 SQFT
PR. P.C.C. SIDEWALK, 5"
73 SQFT

PR. P.C.C. SIDEWALK, 5"
55 SQFT
PR. DETECTABLE WARNINGS
9 SQFT

LEGEND

- HMA SURFACE REMOVAL, 3"
PR. POLY. LEVELING BINDER (MM), IL-4.75, N50, 1"
PR. HMA SURFACE COURSE, MIX "D", N70, 2"
- PR. HMA SURFACE REMOVAL- BUTT JOINT
- PR. STRUCTURE TO BE ADJUSTED
• SEE NOTE 5
- PR. STRUCTURE TO BE ADJUSTED WITH NEW FRAME
AND GRATE OR LID (OF TYPE SPECIFIED)

MATCH LINE STA. 15+00

15+00

BLANCHARD STREET

PR. P.C.C. SIDEWALK, 5"
36 SQFT
PR. DETECTABLE WARNINGS
9 SQFT

PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 12 FT

MATCH LINE STA. 21+00

18

19

20+00

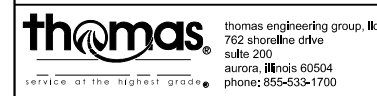
CASA SOLANA DRIVE

PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 18 FT
PR. DETECTABLE WARNINGS
15 SQFT
PR. P.C.C. SIDEWALK, 5"
144 SQFT

PR. DETECTABLE WARNINGS
10 SQFT
PR. P.C.C. SIDEWALK, 5"
48 SQFT



HORIZONTAL SCALE IN FEET



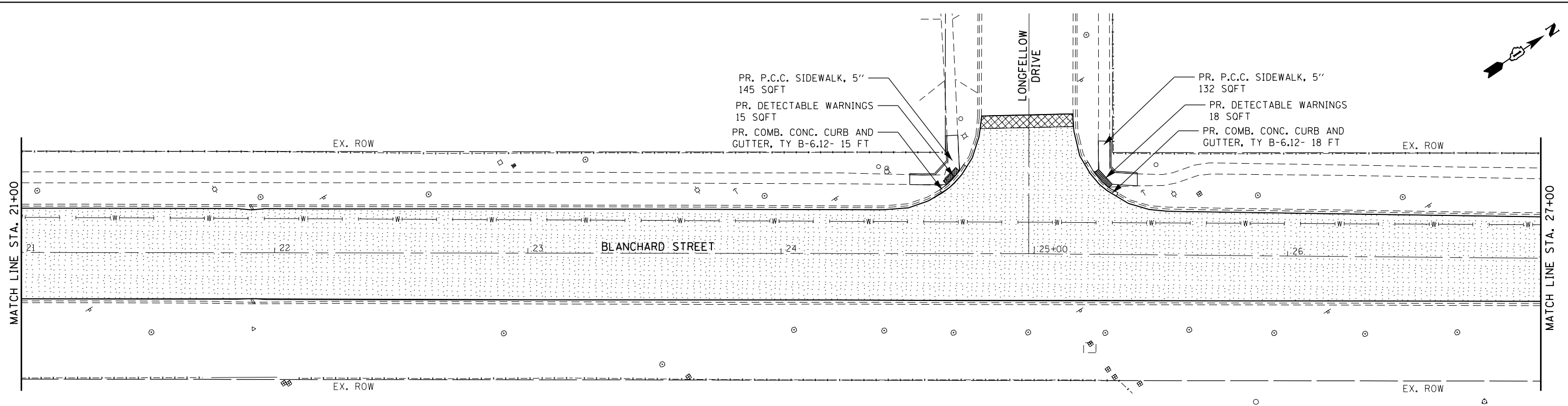
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLANS





SCALE: SHEET OF SHEETS STA. TO STA.

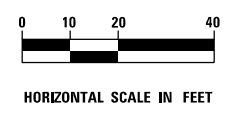
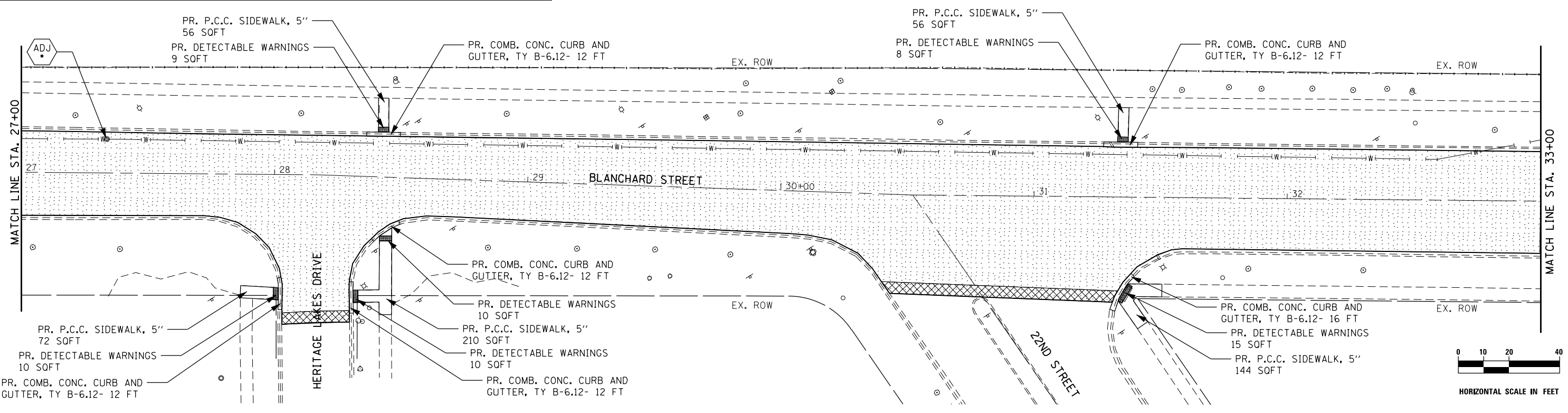
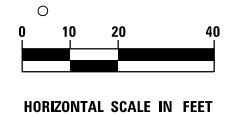
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CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



- NOTES:
- FOR SIDEWALK DETAILS, SEE SHEETS 20-29
 - ANY NECESSARY TYPE B CURB FOR MEETING ADA RAMP GRADES AT THE BACK OF SIDEWALK SHALL BE POURED MONOLITHICALLY WITH THE SIDEWALK AND INCLUDED IN THE COST OF THE SIDEWALK.
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 - ALL STRUCTURE ADJUSTMENTS SHALL BE WITH TYPE 1 CLOSED LID, UNLESS OTHERWISE SPECIFIED.
 - PRIOR TO ADJUSTMENT OF ANY DUPAGE WATER COMMISSION STRUCTURES, THE CONTRACTOR SHALL CONTACT DWC AT 630-834-0100 A MINIMUM OF 48 HOURS IN ADVANCE OF THE WORK. DWC WILL SUPPLY A NEW FRAME AND LID IF THEY DETERMINE IT SHOULD BE REPLACED.

LEGEND

	HMA SURFACE REMOVAL, 3" PR. POLY. LEVELING BINDER (MM), IL-4.75, N50, 1" PR. HMA SURFACE COURSE, MIX "D", N70, 2"		PR. STRUCTURE TO BE ADJUSTED • SEE NOTE 5
	PR. HMA SURFACE REMOVAL- BUTT JOINT		PR. STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND GRATE OR LID (OF TYPE SPECIFIED)



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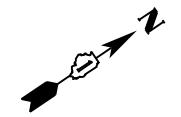
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

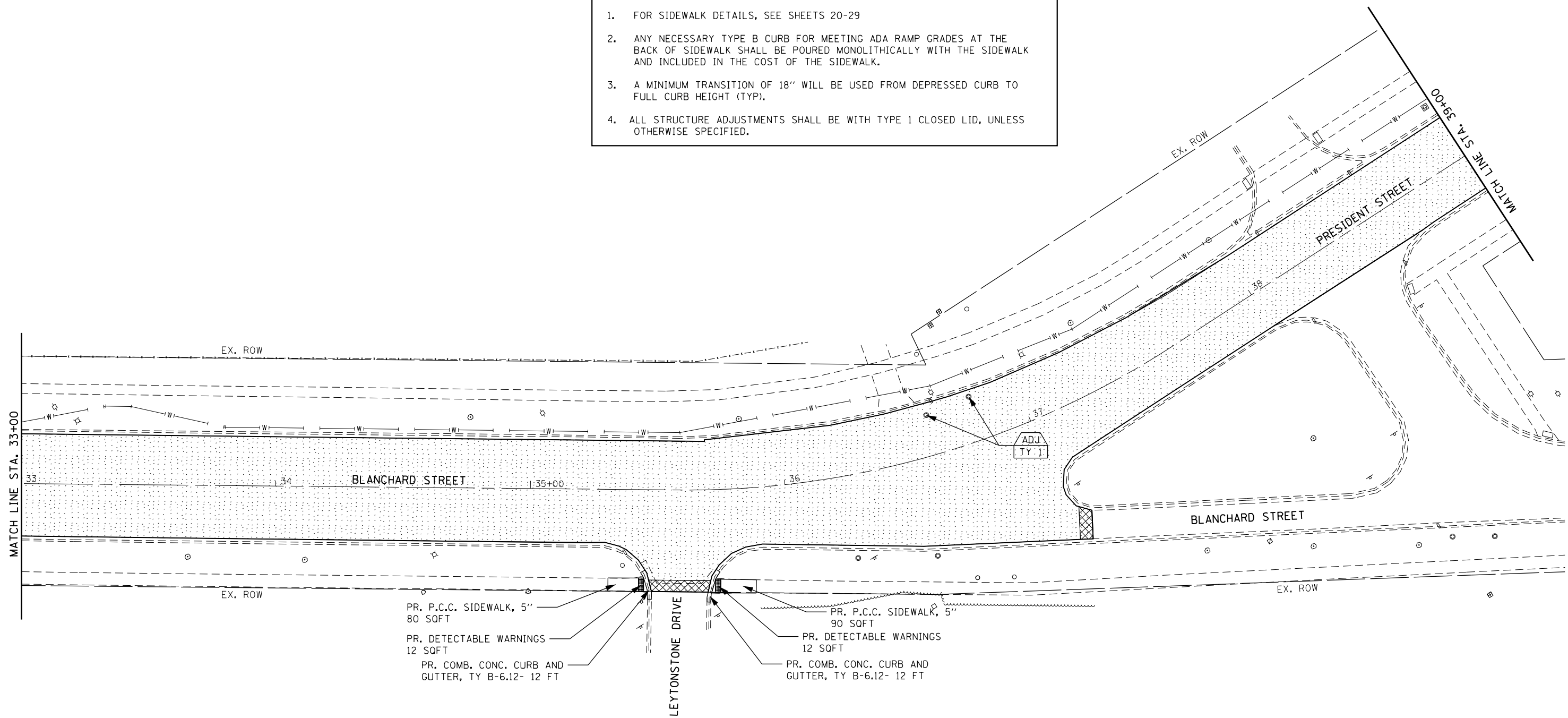
ROADWAY PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2567/2569	SECTION 18-00119-00-R5	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 7
CONTRACT: 61F50				
ILLINOIS FED. AID PROJECT				



- NOTES:
1. FOR SIDEWALK DETAILS, SEE SHEETS 20-29
 2. ANY NECESSARY TYPE B CURB FOR MEETING ADA RAMP GRADES AT THE BACK OF SIDEWALK SHALL BE POURED MONOLITHICALLY WITH THE SIDEWALK AND INCLUDED IN THE COST OF THE SIDEWALK.
 3. A MINIMUM TRANSITION OF 18" WILL BE USED FROM DEPRESSED CURB TO FULL CURB HEIGHT (TYP).
 4. ALL STRUCTURE ADJUSTMENTS SHALL BE WITH TYPE 1 CLOSED LID, UNLESS OTHERWISE SPECIFIED.



PR. P.C.C. SIDEWALK, 5"
80 SOFT

PR. DETECTABLE WARNINGS
12 SOFT

PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 12 FT

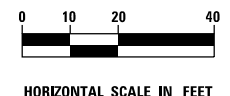
PR. P.C.C. SIDEWALK, 5"
90 SOFT

PR. DETECTABLE WARNINGS
12 SOFT

PR. COMB. CONC. CURB AND
GUTTER, TY B-6.12- 12 FT

LEGEND

	HMA SURFACE REMOVAL, 3" PR. POLY. LEVELING BINDER (MM), IL-4.75, N50, 1" PR. HMA SURFACE COURSE, MIX "D", N70, 2"		PR. STRUCTURE TO BE ADJUSTED
	PR. HMA SURFACE REMOVAL- BUTT JOINT		PR. STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND GRATE OR LID (OF TYPE SPECIFIED)



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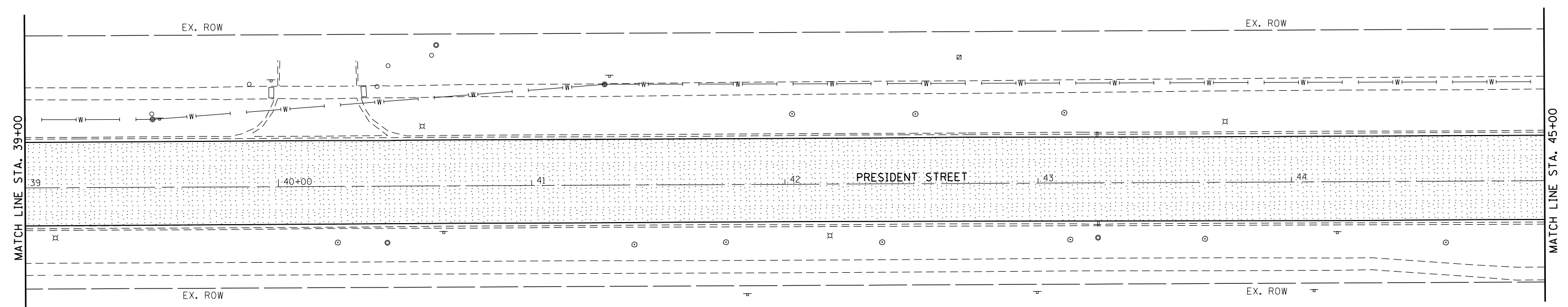
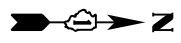
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

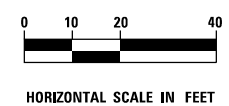
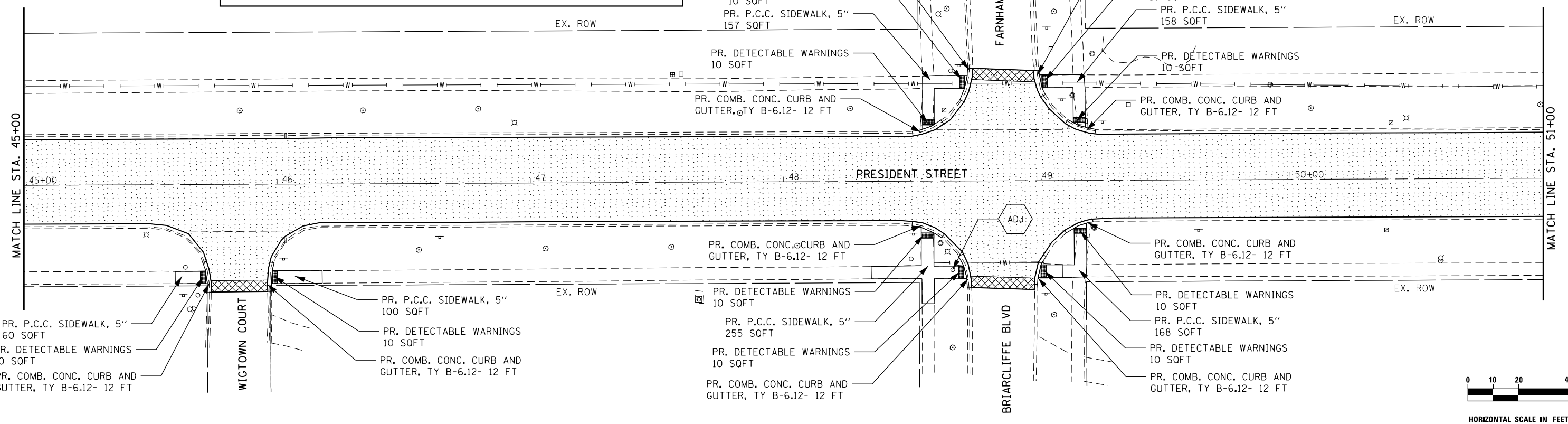
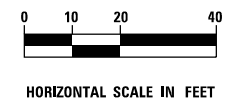
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CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



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LEGEND

	HMA SURFACE REMOVAL, 3" PR. POLY. LEVELING BINDER (MM), IL-4.75, N50, 1" PR. HMA SURFACE COURSE, MIX "D", N70, 2"		PR. STRUCTURE TO BE ADJUSTED
	PR. HMA SURFACE REMOVAL- BUTT JOINT		PR. STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND GRATE OR LID (OF TYPE SPECIFIED)



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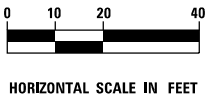
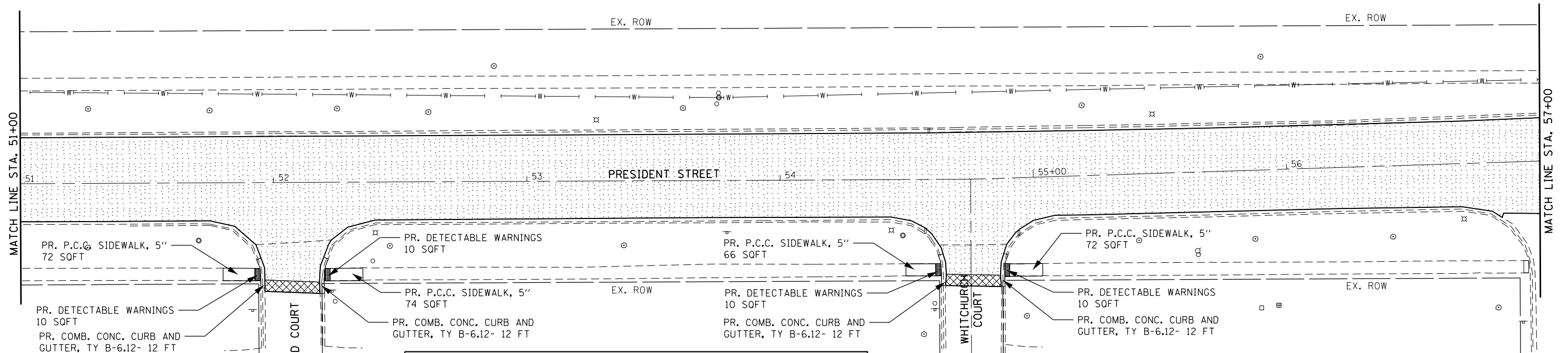
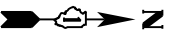
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLANS

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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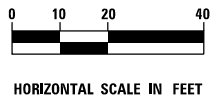
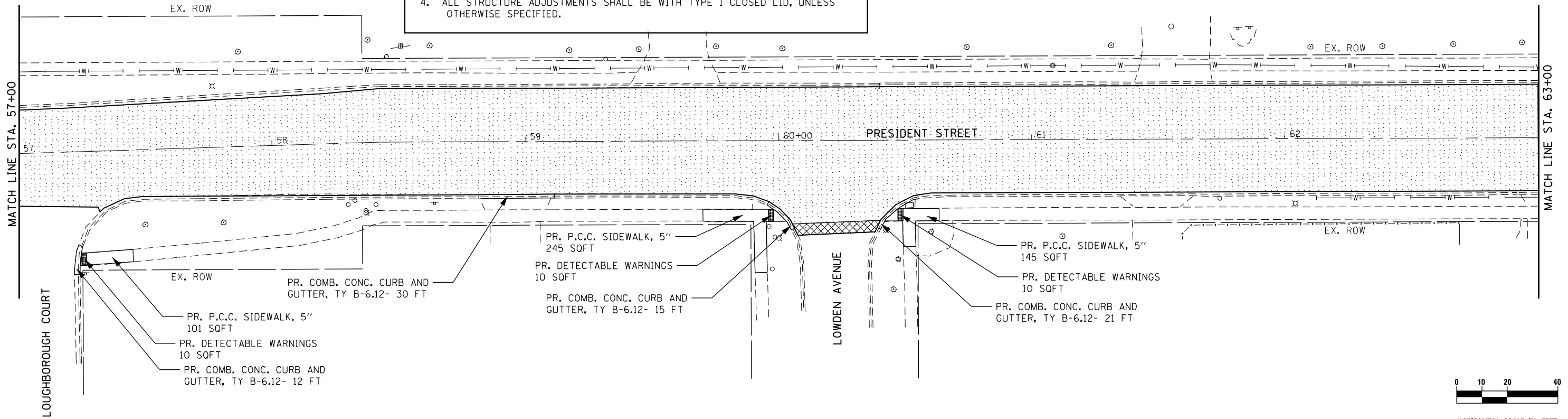
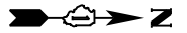
F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 9
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



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LEGEND

- HMA SURFACE REMOVAL, 3" PR. POLY. LEVELING BINDER (MM), IL-4.75, N50, 1" PR. HMA SURFACE COURSE, MIX "D", N70, 2"
- PR. HMA SURFACE REMOVAL- BUTT JOINT
- PR. STRUCTURE TO BE ADJUSTED
- PR. STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND GRATE OR LID (OF TYPE SPECIFIED)



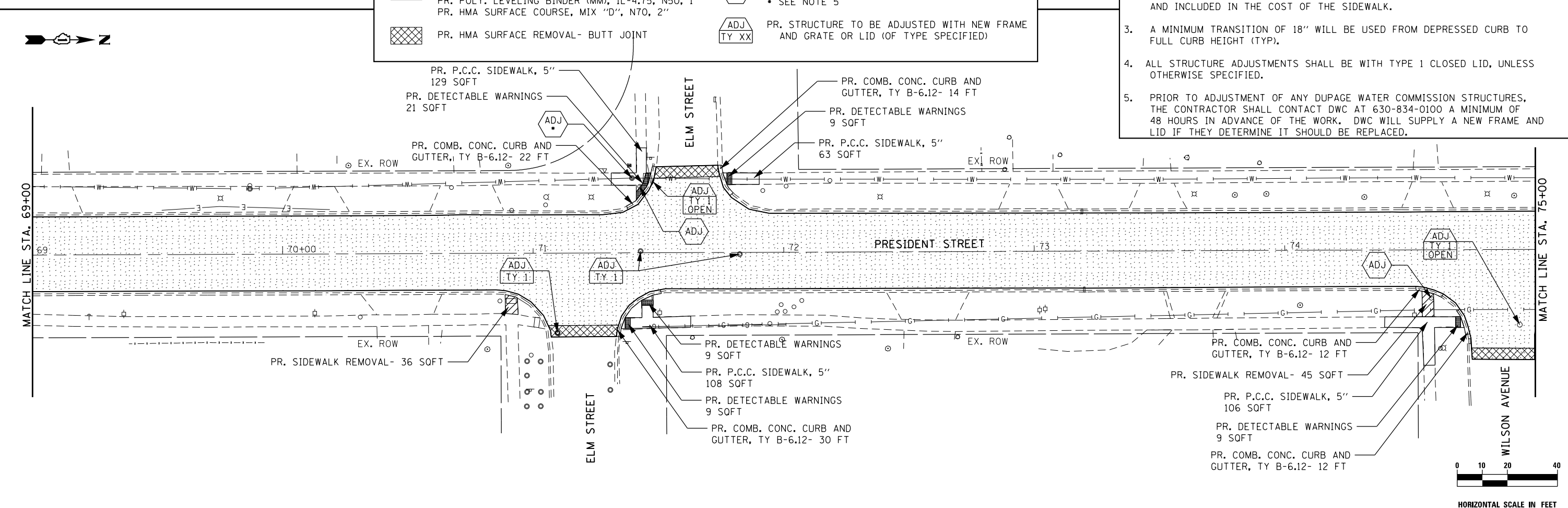
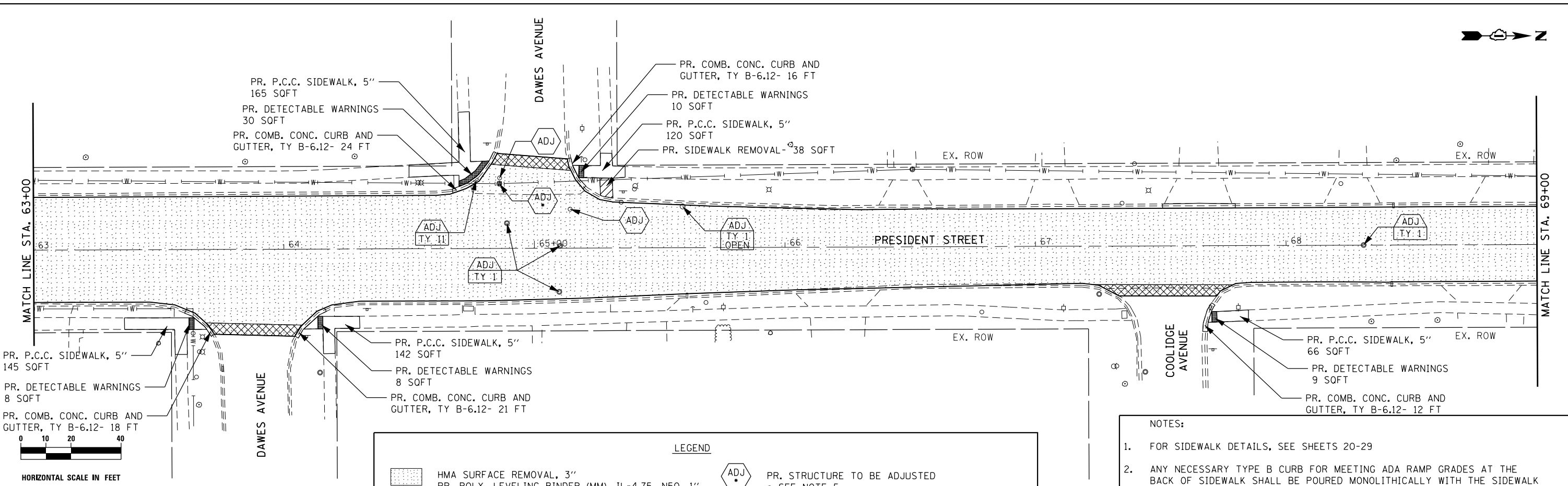
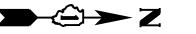
thomas thomas engineering group, llc
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 phone: 855-533-1700

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	DATE -	REVISED -






**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY PLANS	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 10
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



LEGEND

	HMA SURFACE REMOVAL, 3"		PR. STRUCTURE TO BE ADJUSTED • SEE NOTE 5
	PR. POLY. LEVELING BINDER (MM), IL-4.75, N50, 1" PR. HMA SURFACE COURSE, MIX "D", N70, 2"		PR. STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND GRATE OR LID (OF TYPE SPECIFIED)
	PR. HMA SURFACE REMOVAL- BUTT JOINT		

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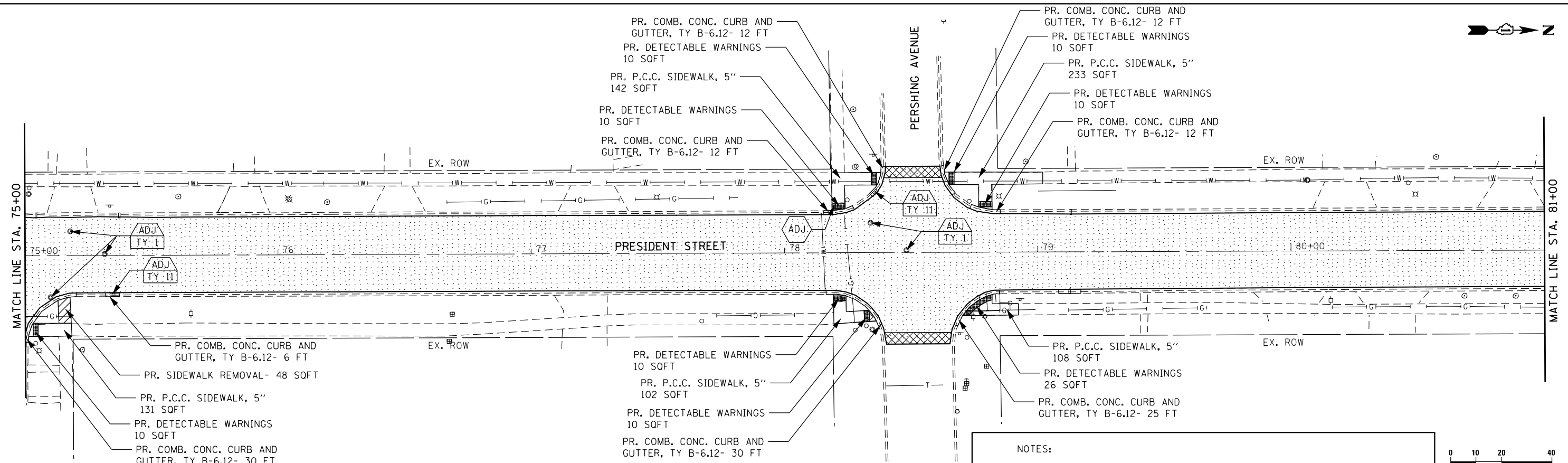
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

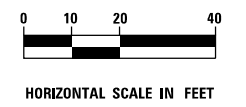
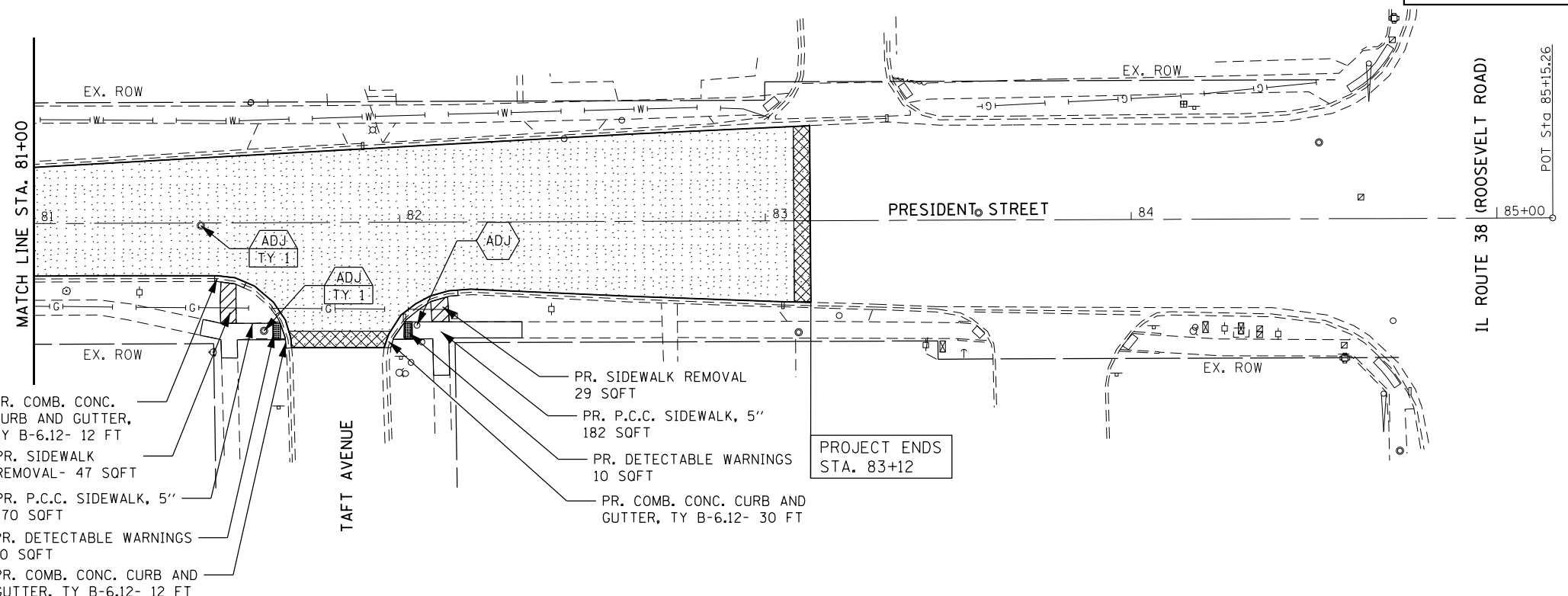
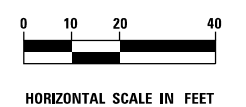
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CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



LEGEND

	HMA SURFACE REMOVAL, 3"		PR. STRUCTURE TO BE ADJUSTED
	PR. POLY. LEVELING BINDER (MM), IL-4.75, N50, 1"		PR. STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND GRATE OR LID (OF TYPE SPECIFIED)
	PR. HMA SURFACE COURSE, MIX "D", N70, 2"		
	PR. HMA SURFACE REMOVAL- BUTT JOINT		

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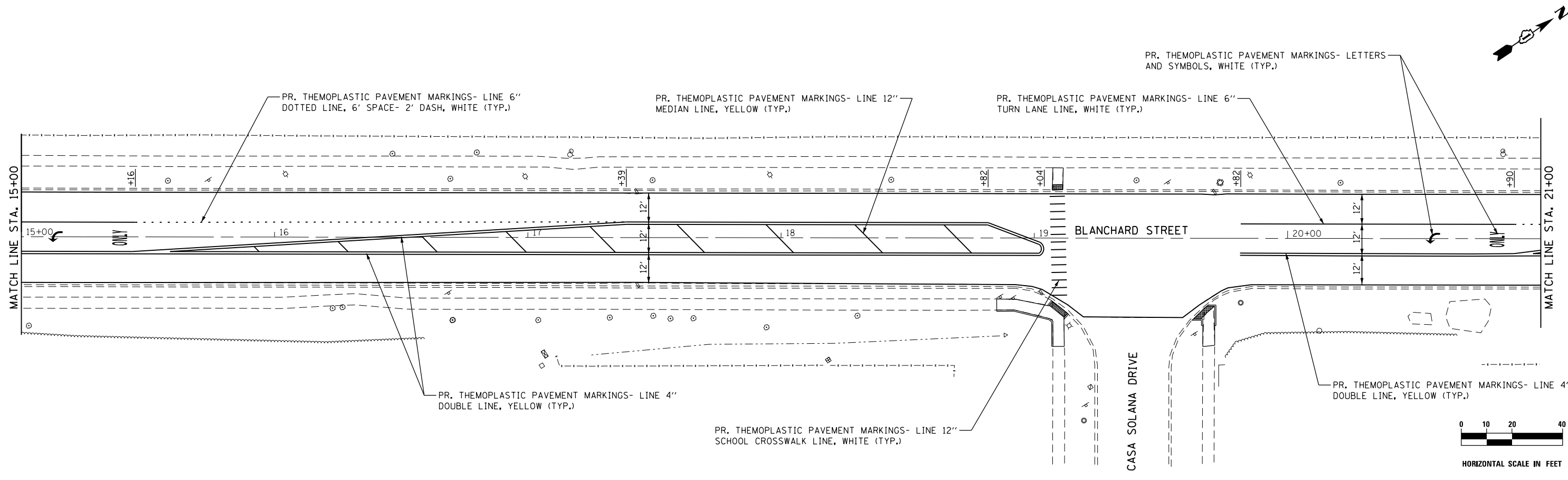
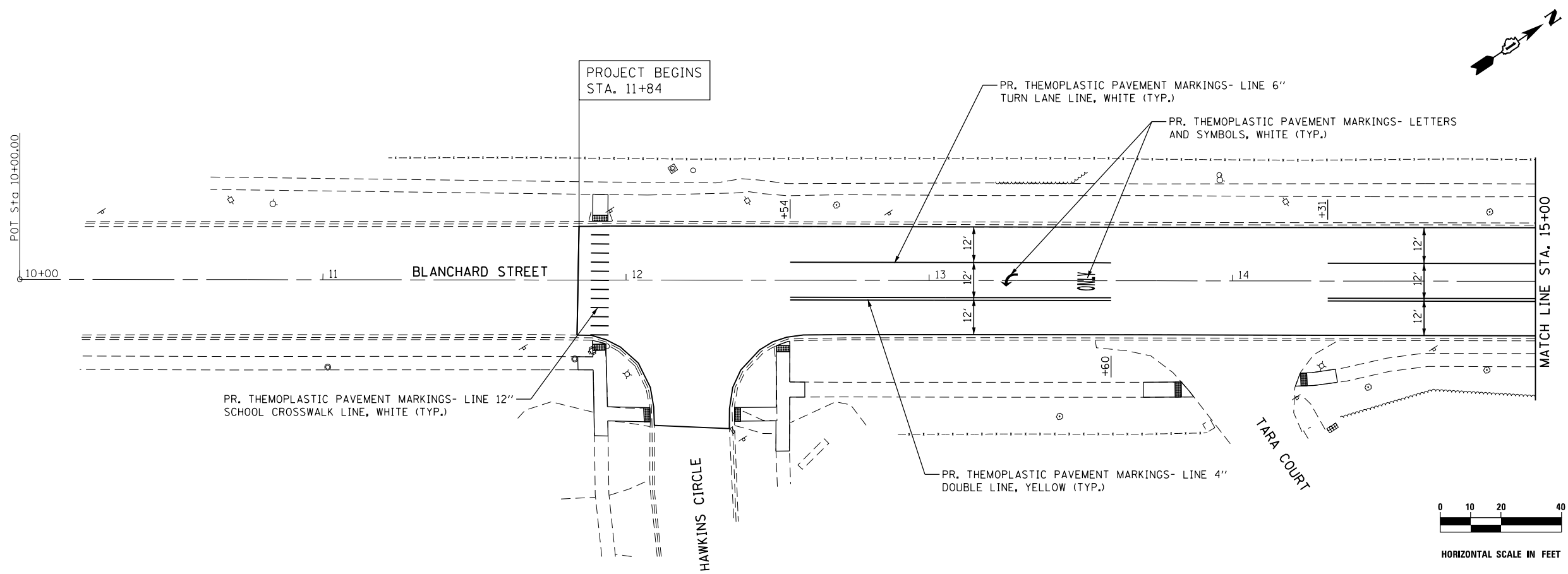
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DRAWN -	REVISOR -	
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PLOT DATE = 12/21/2018	DATE -	REVISOR -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY PLANS

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY	TOTAL SHEETS 37	SHEET NO. 12
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



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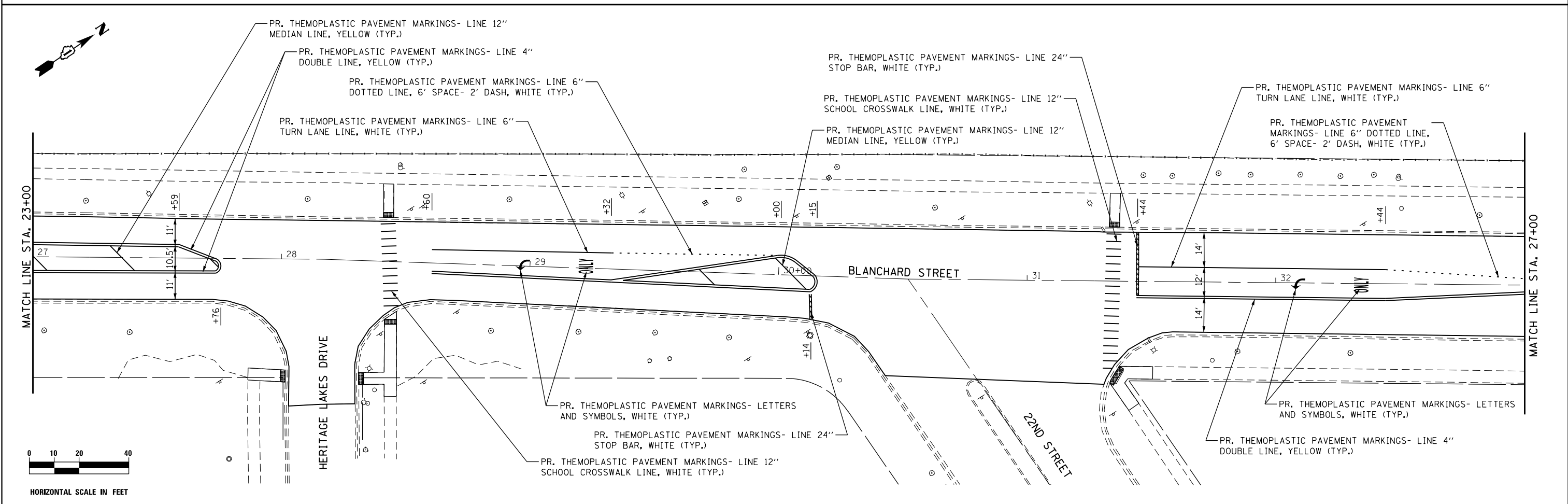
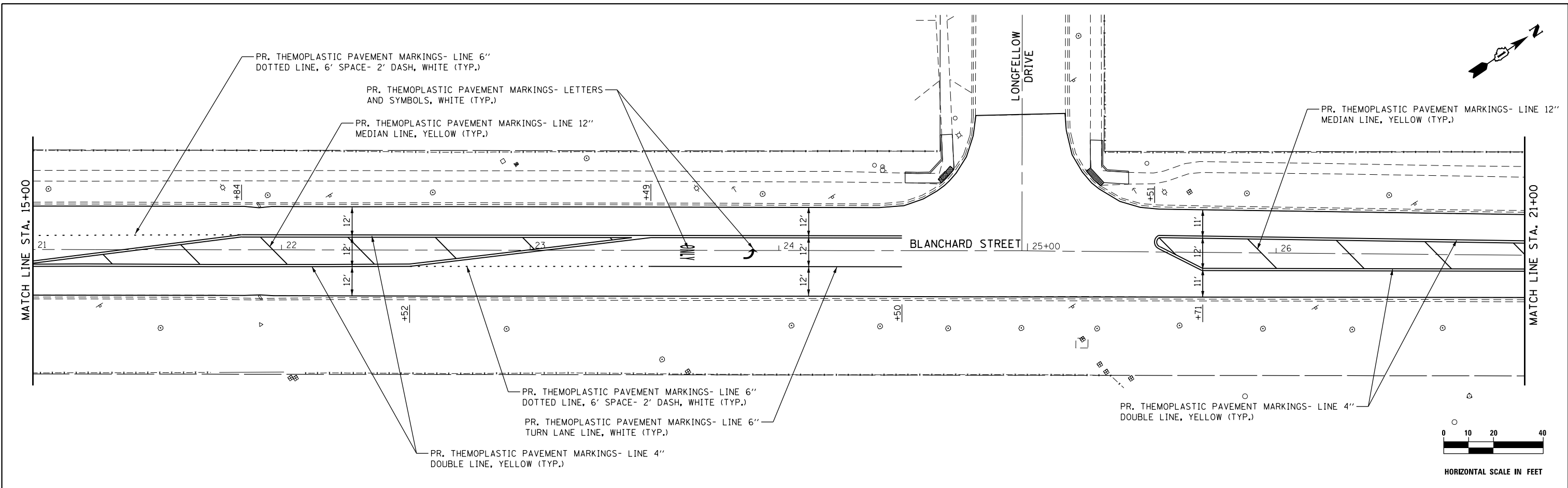
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DRAWN -	REVISED -	
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DATE -	REVISED -	
PLOT SCALE = 40.0000' / in.		
PLOT DATE = 12/21/2018		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLANS

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.U. RTE. 2567 2569	SECTION 18-00119-00-R5	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 13
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



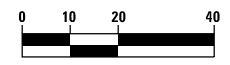
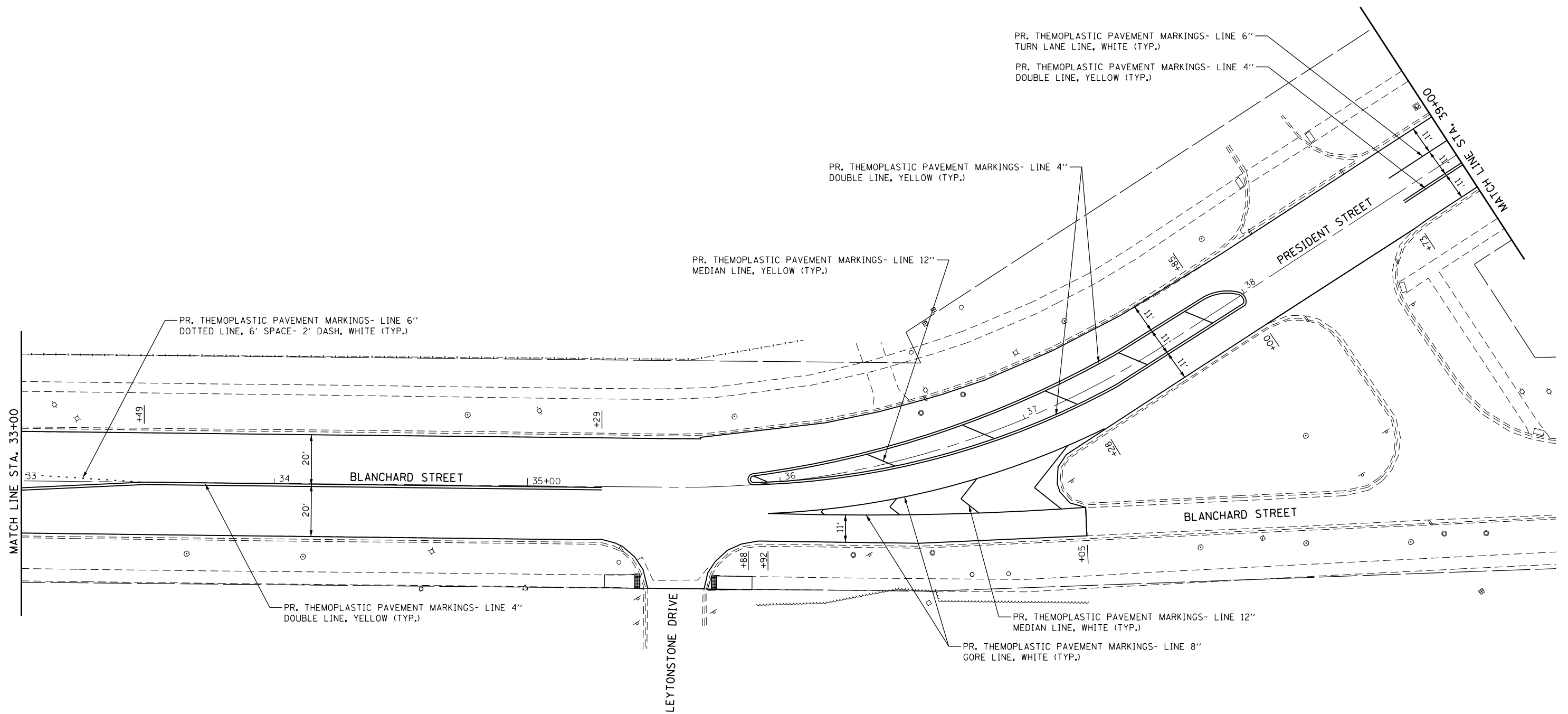
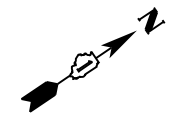
thomas thomas engineering group, llc
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PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLANS	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2567 2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 14
CONTRACT: 61F50				
ILLINOIS FED. AID PROJECT				



HORIZONTAL SCALE IN FEET

thomas
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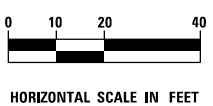
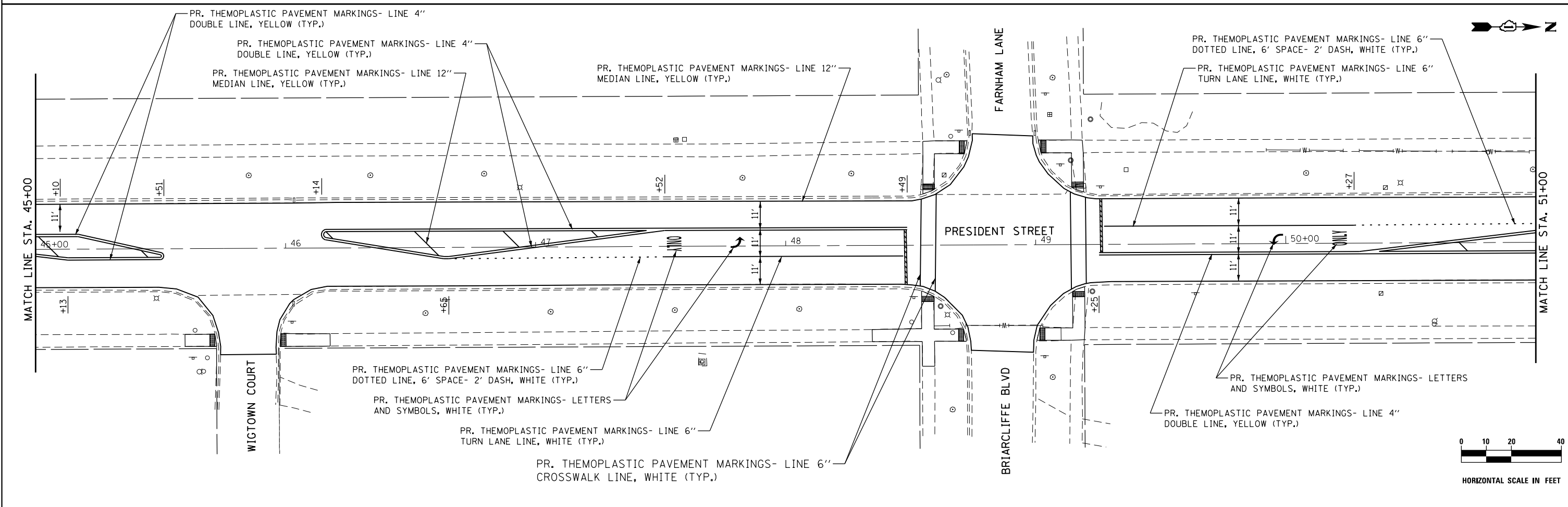
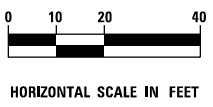
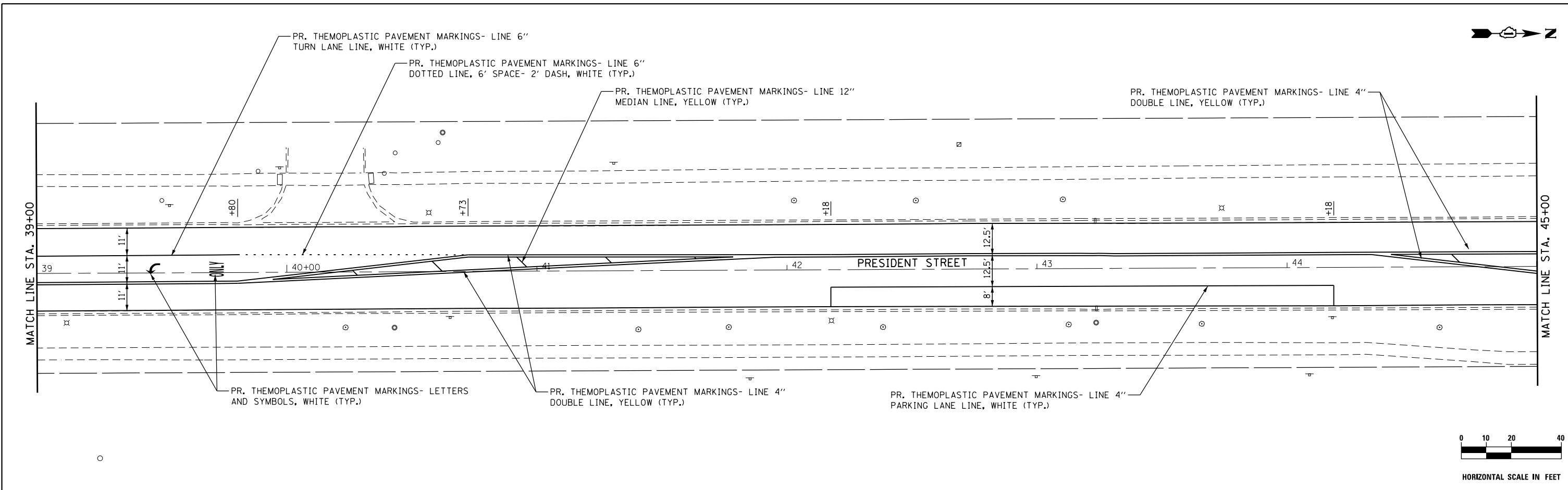
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DRAWN -	REVISOR -	
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PLOT DATE = 12/21/2018	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLANS

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.U. RTE. 2567 2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 15
CONTRACT: 61F50				
ILLINOIS FED. AID PROJECT				



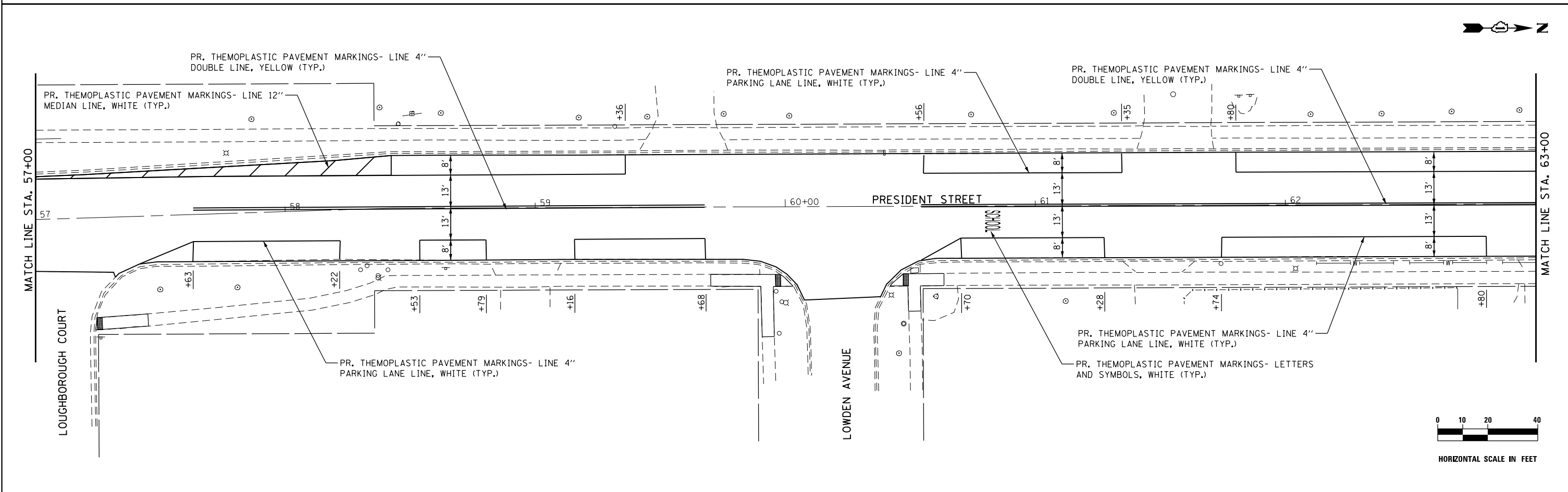
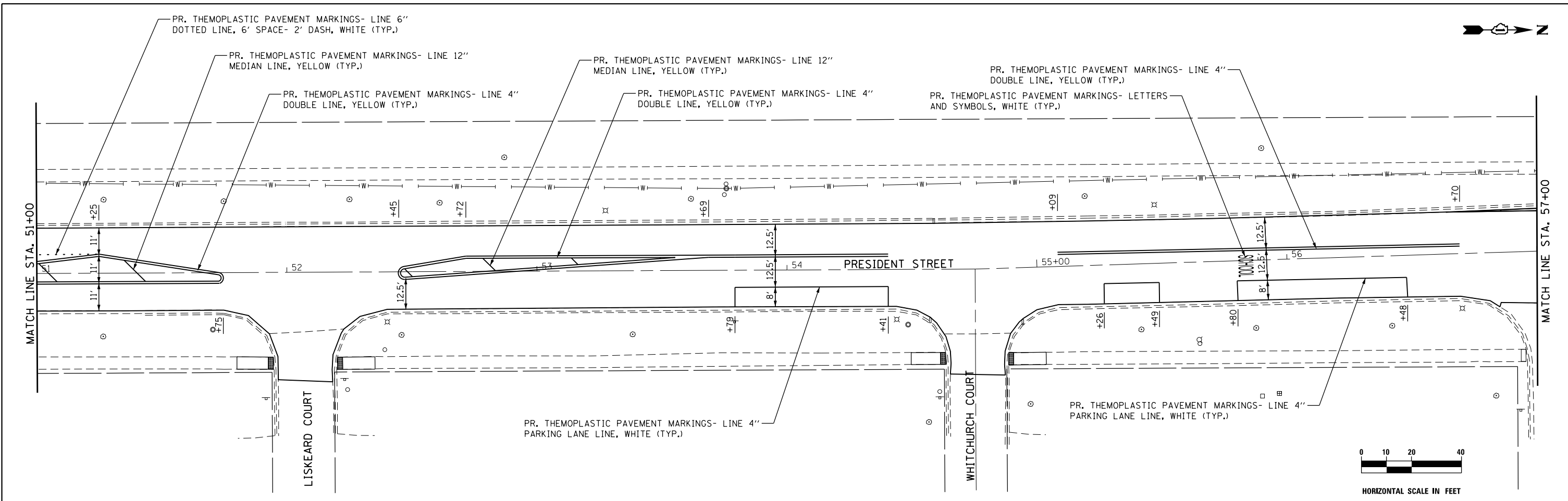
thomas thomas engineering group, llc
 762 shoreline drive
 suite 200
 aurora, illinois 60504
 phone: 855-533-1700

USER NAME = jamesy	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLANS	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2567 2569	SECTION 18-00119-00-R5	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 16
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



thomas thomas engineering group, llc
762 shoreline drive
suite 200
aurora, illinois 60504
phone: 855-533-1700

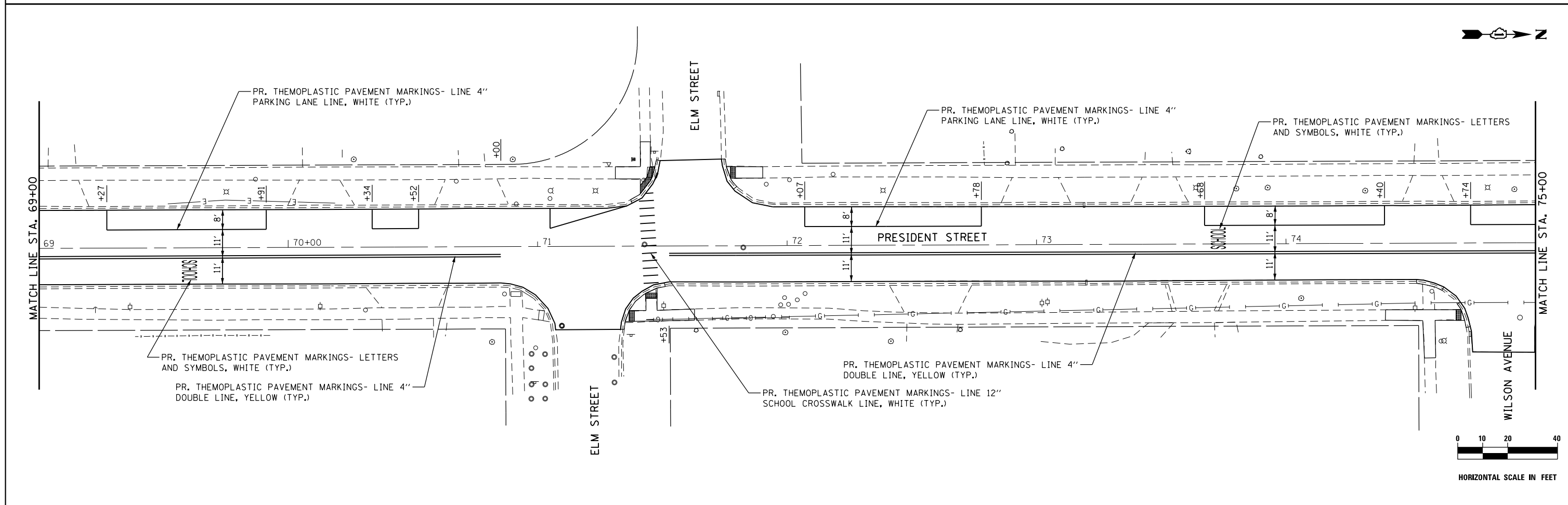
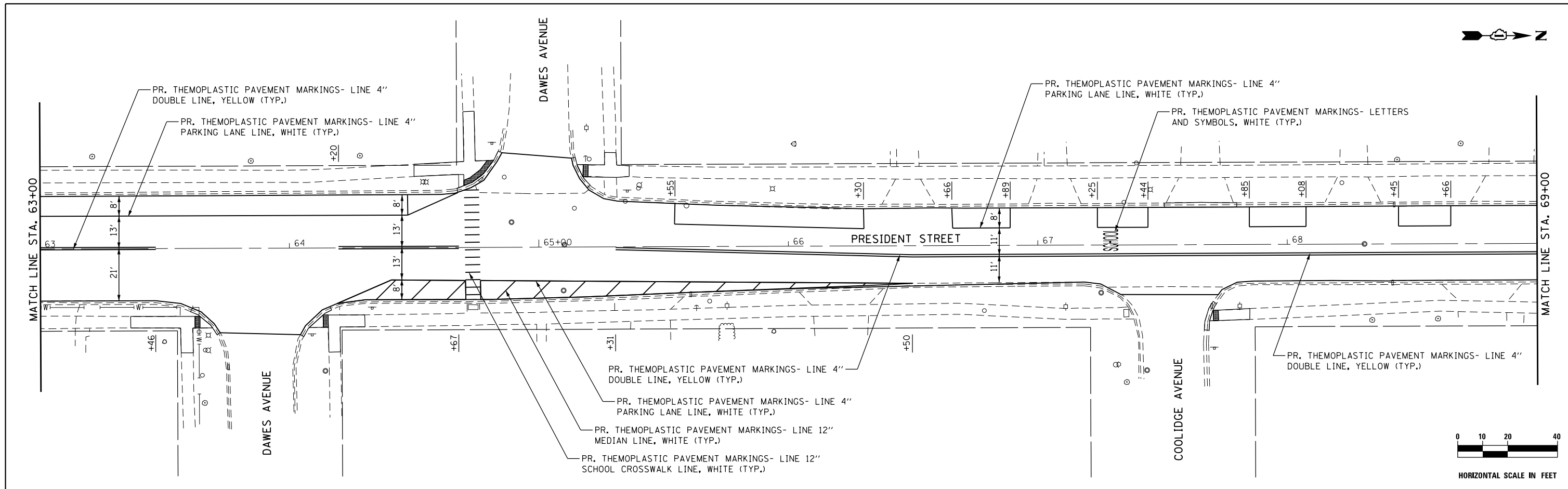
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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

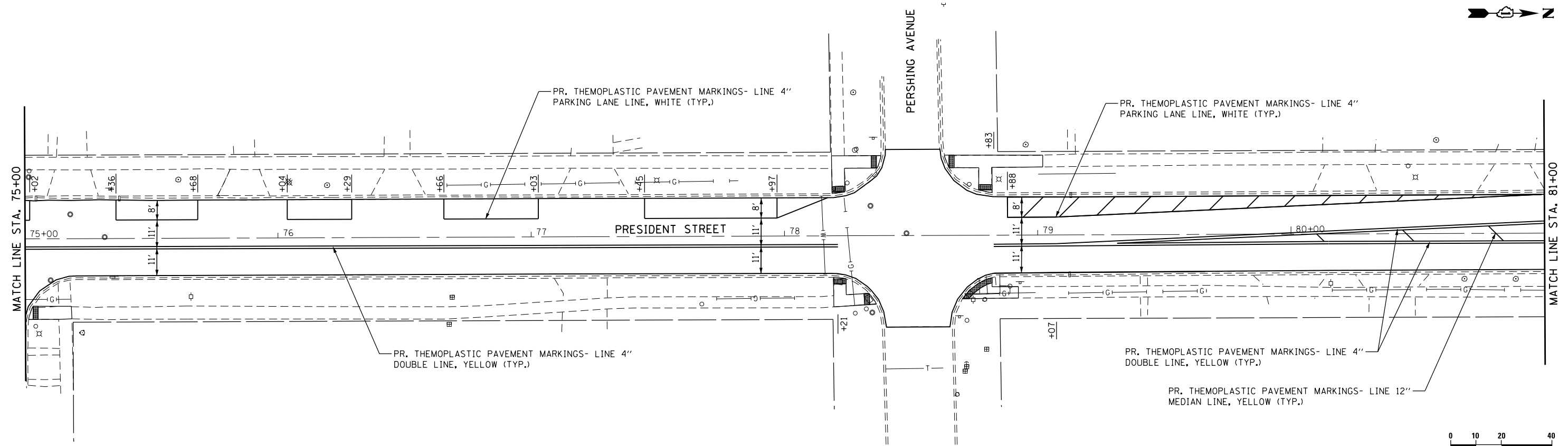
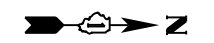
PAVEMENT MARKING PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

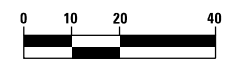
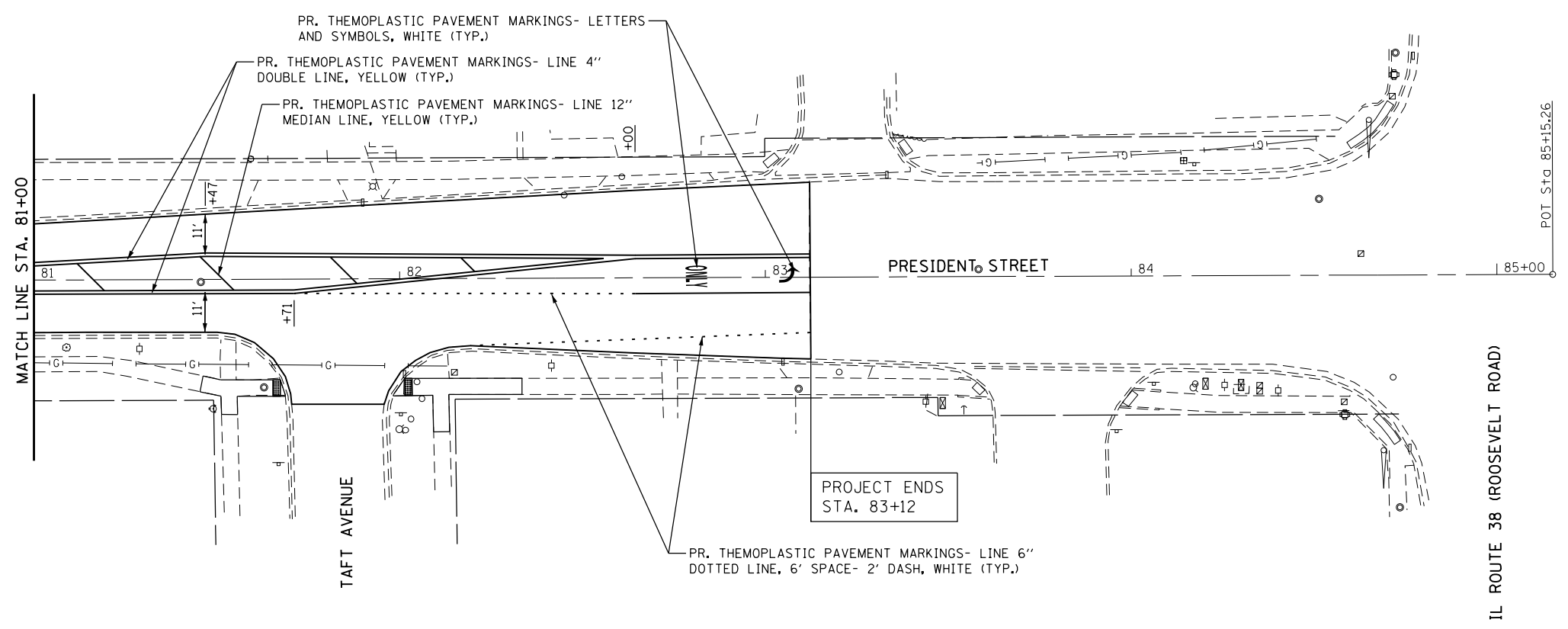
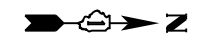
F.A.U. RTE. 2567/2569	SECTION 18-00119-00-R5	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 17
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



thomas engineering group, llc 762 shoreline drive suite 200 aurora, illinois 60504 phone: 855-533-1700	USER NAME = jamesj	DESIGNED -	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLANS			F.A.U. RTE. 2557/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 18
	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -					CONTRACT: 61F50 61F50				
	PLOT DATE = 12/21/2018	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
					SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	



HORIZONTAL SCALE IN FEET



HORIZONTAL SCALE IN FEET

thomas
 thomas engineering group, llc
 762 shoreline drive
 suite 200
 aurora, illinois 60504
 phone: 855-533-1700

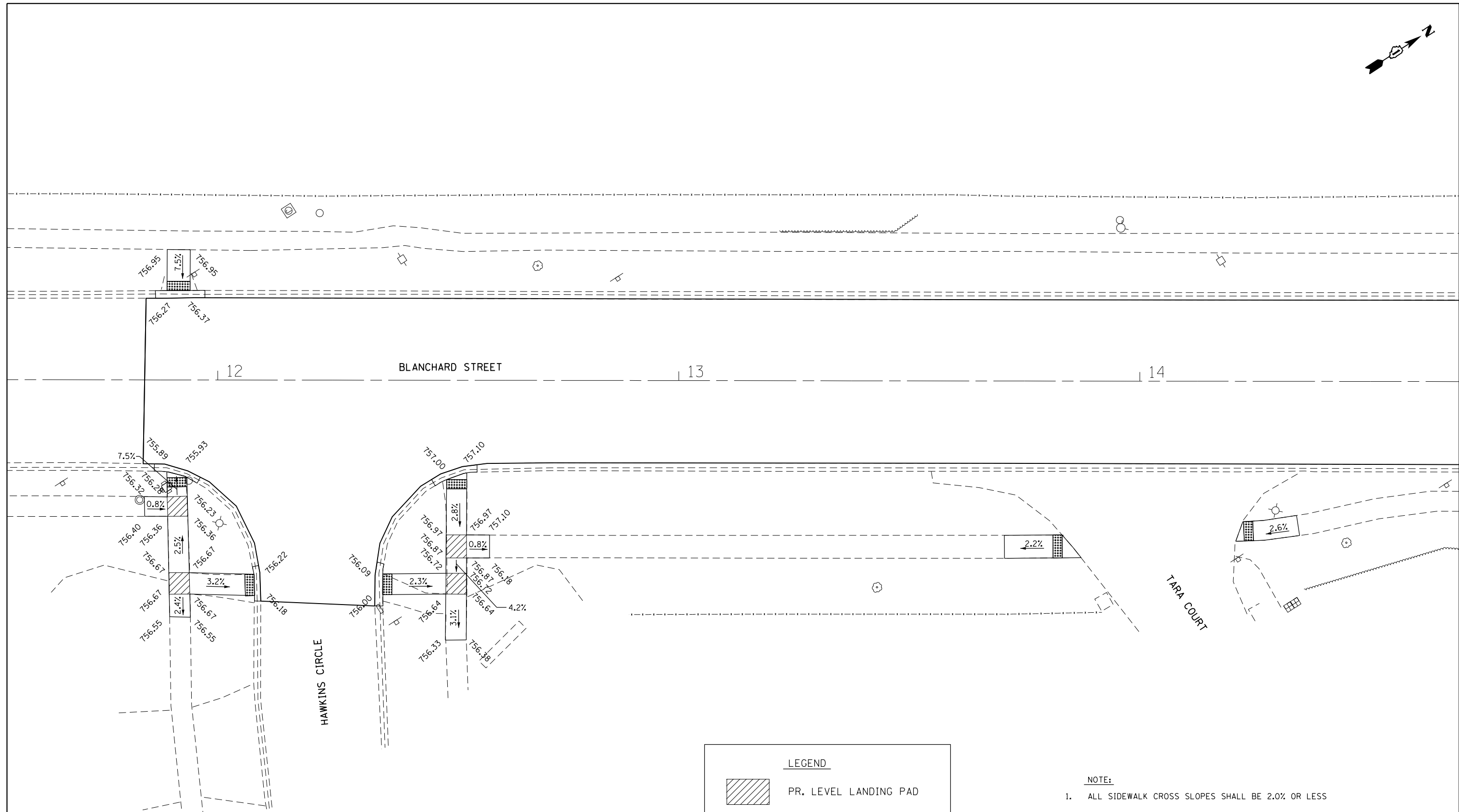
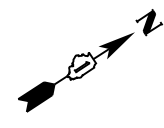
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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**


PAVEMENT MARKING PLANS


SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 19
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	

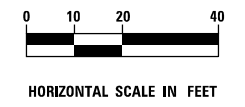


LEGEND

 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

NOTE:
 1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



thomas
 thomas engineering group, llc
 762 shoreline drive
 suite 200
 aurora, illinois 60504
 phone: 855-533-1700

USER NAME = jamesy	DESIGNED -	REVISED
DRAWN -	REVISOR -	
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 12/21/2018	DATE -	REVISOR -


**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**


SIDEWALK PLANS

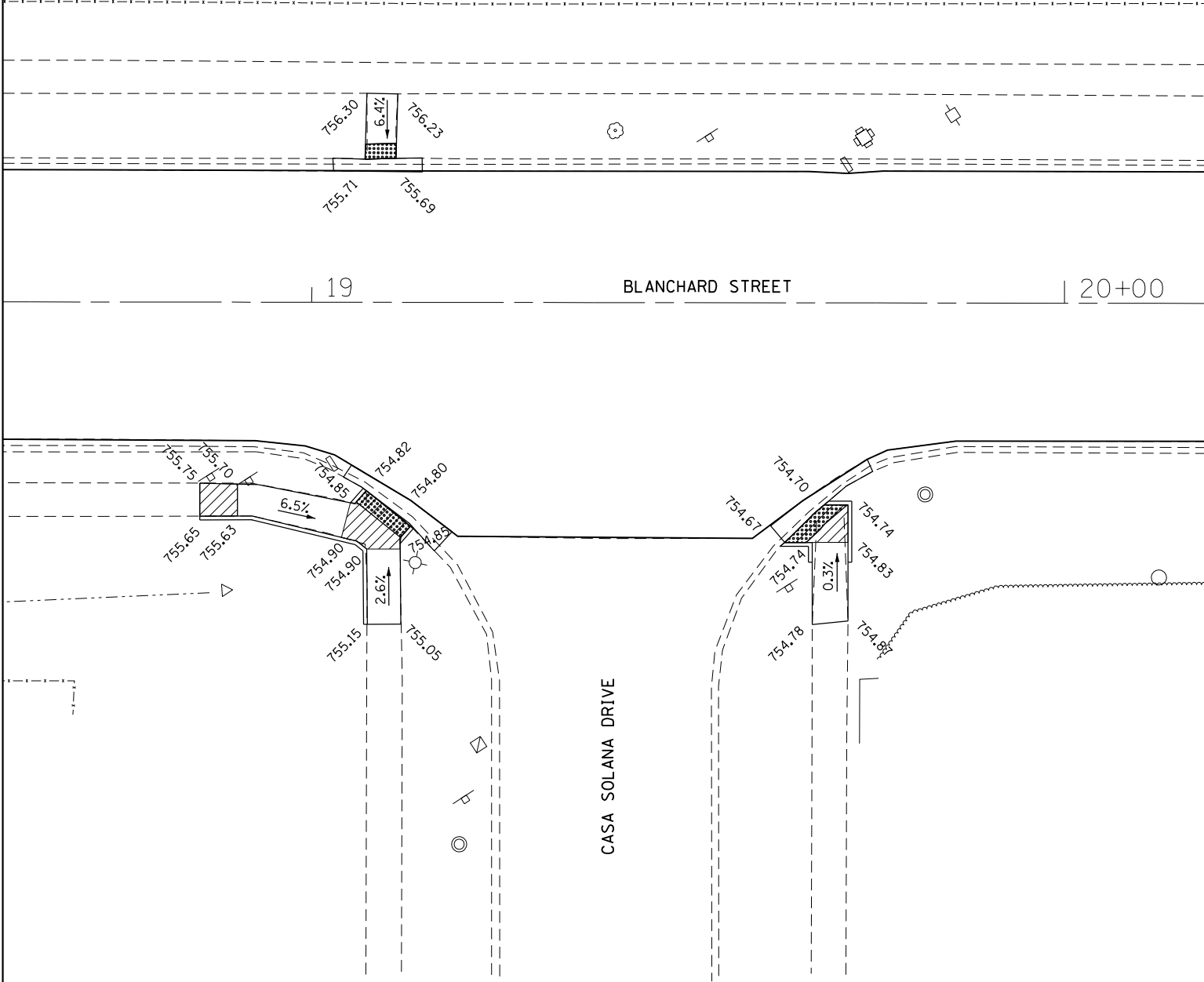
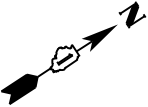
SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567 2569	18-00119-00-RS	DuPAGE	37	20
CONTRACT: 61F50				
ILLINOIS FED. AID PROJECT				

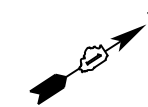
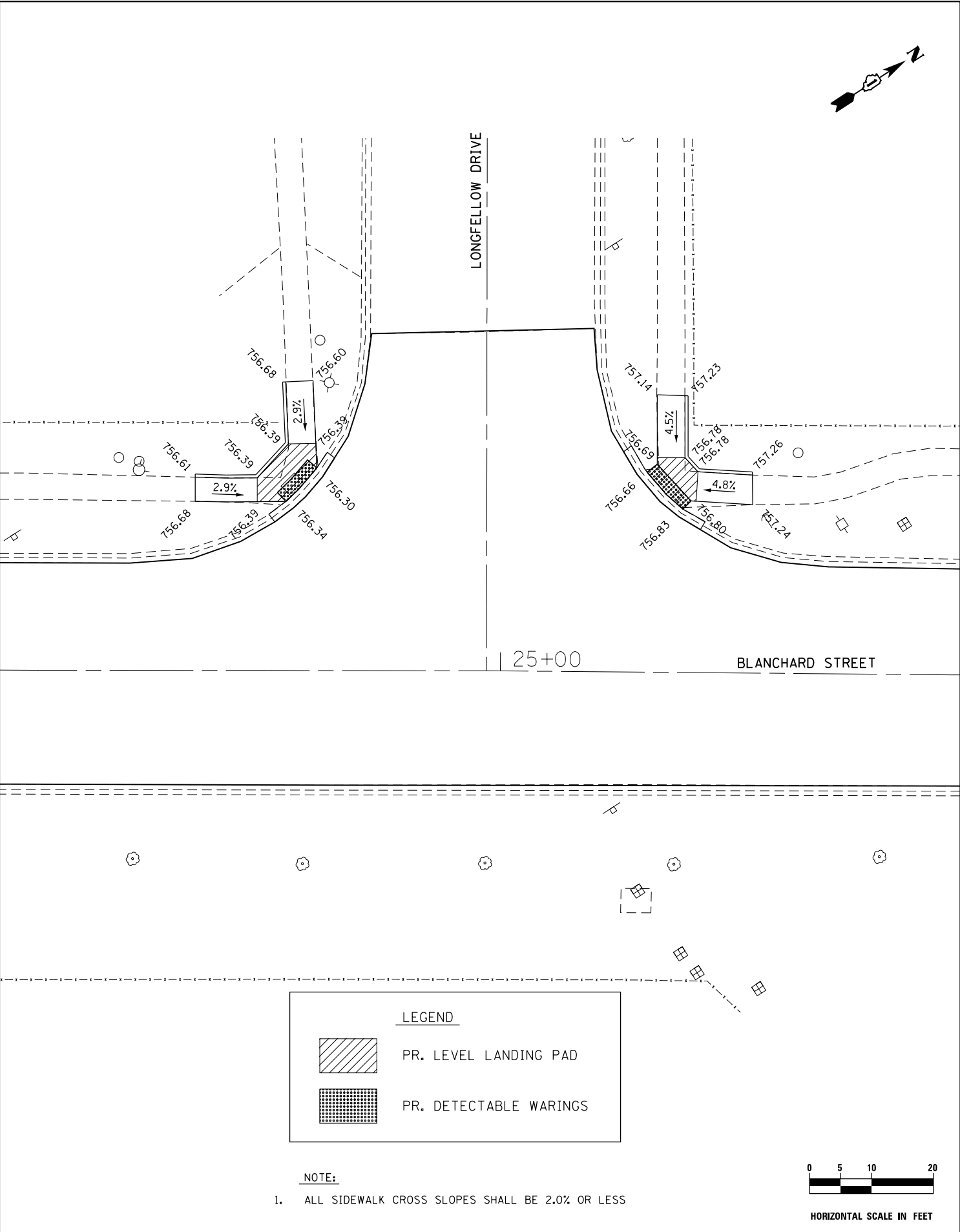
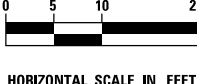
LEGEND

 PR. LEVEL LANDING PAD

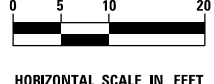
 PR. DETECTABLE WARINGS



NOTE:
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aurora, illinois 60504
phone: 855-533-1700

USER NAME = jamesy	DESIGNED -	REVISED -
DRAWN -	CHECKED -	REVISED -
PLOT SCALE = 20.0000' / in.	DATE -	REVISED -
PLOT DATE = 12/21/2018		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**


SIDEWALK PLANS				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.


F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 21
CONTRACT: 61F50				
ILLINOIS FED. AID PROJECT				

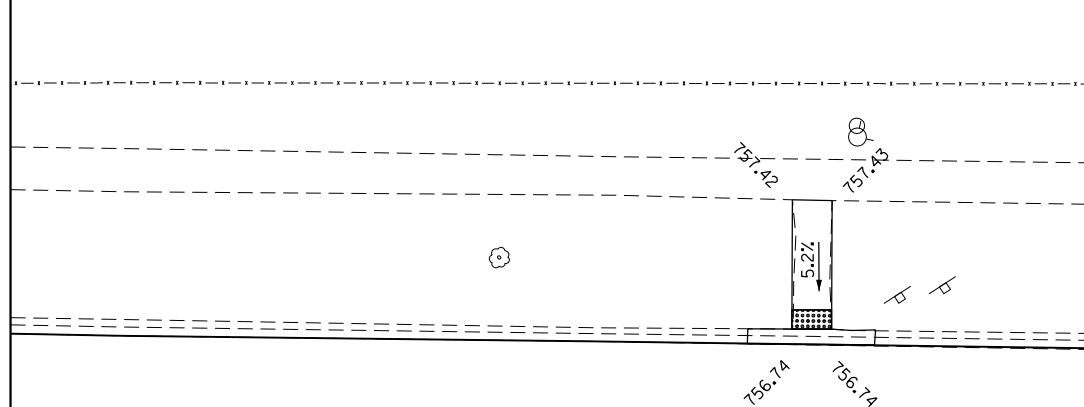
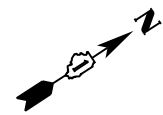
NOTE:

- ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS

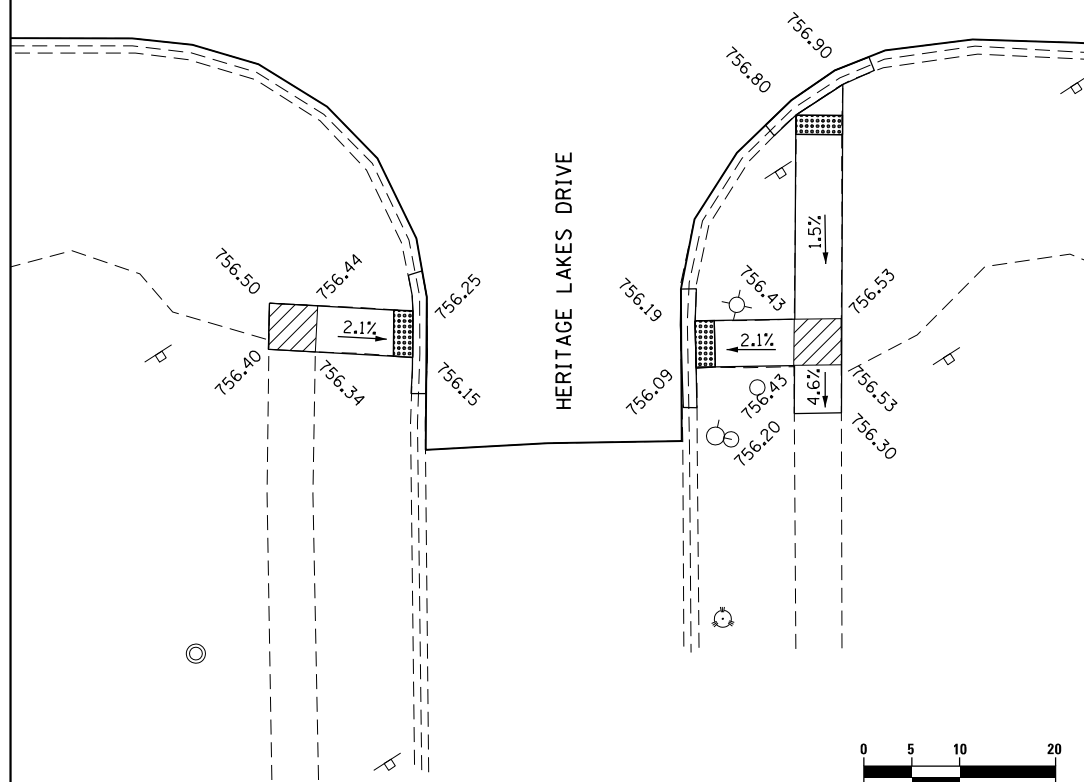
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 PR. LEVEL LANDING PAD

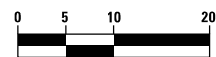
 PR. DETECTABLE WARINGS



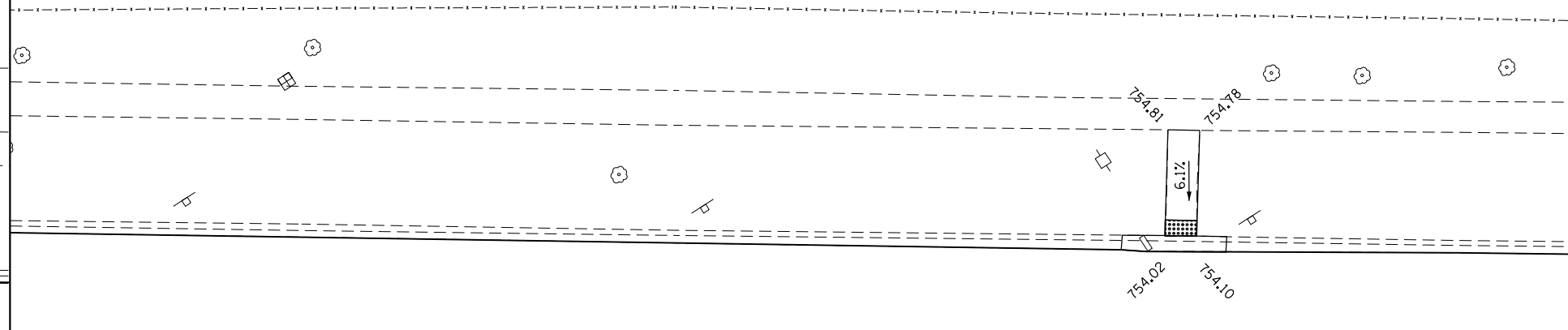
BLANCHARD STREET | 28



HERITAGE LAKES DRIVE

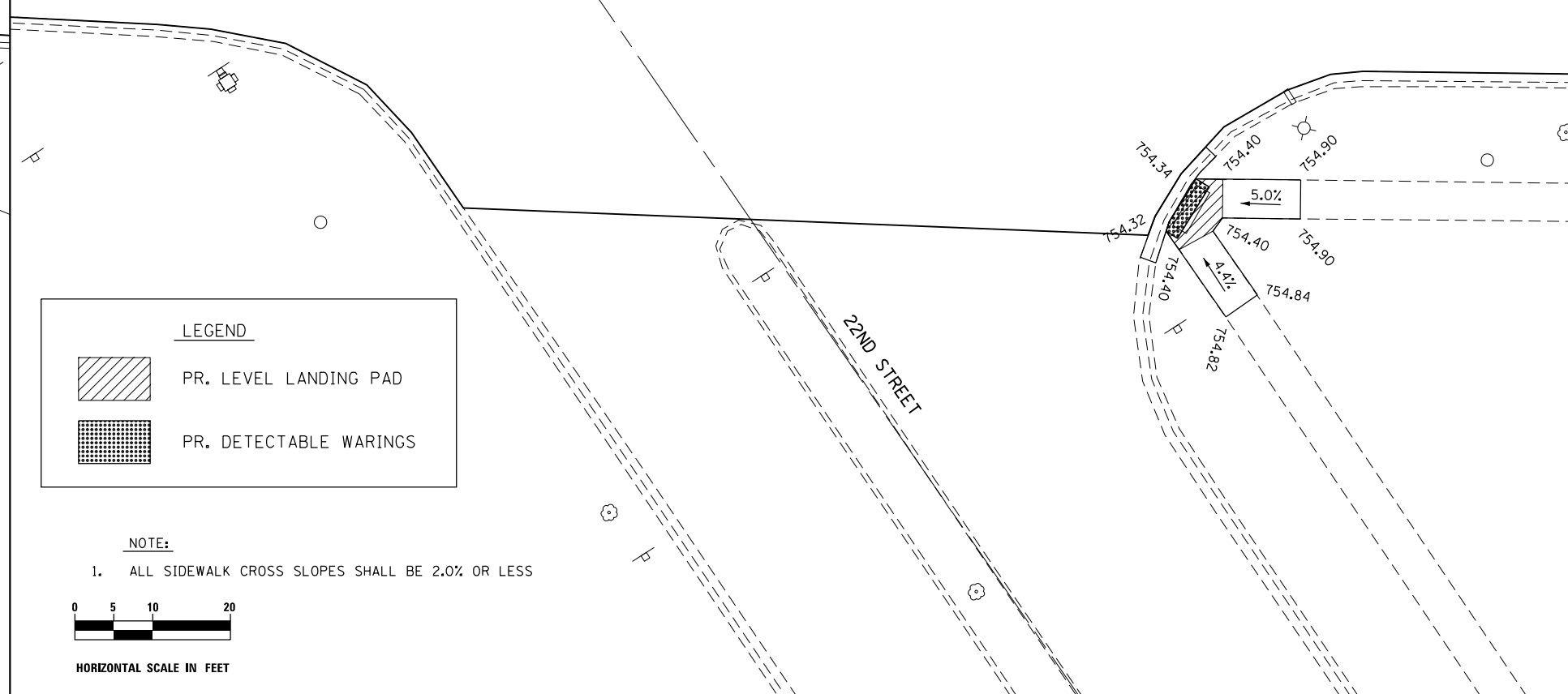


HORIZONTAL SCALE IN FEET



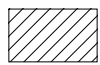
| 30+00 | 31

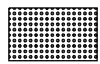
BLANCHARD STREET



22ND STREET

LEGEND

 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

NOTE:

- ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



HORIZONTAL SCALE IN FEET

thomas thomas engineering group, llc
762 shoreline drive
suite 200
aurora, illinois 60504
phone: 855-533-1700

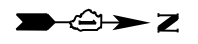
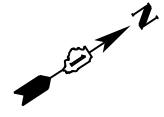
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DRAWN -	CHECKED -	REVISED -
PLOT SCALE = 20.0000' / in.	DATE -	REVISED -
PLOT DATE = 12/21/2018		

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -


**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**


SIDEWALK PLANS	
SCALE:	SHEET OF SHEETS STA. TO STA.

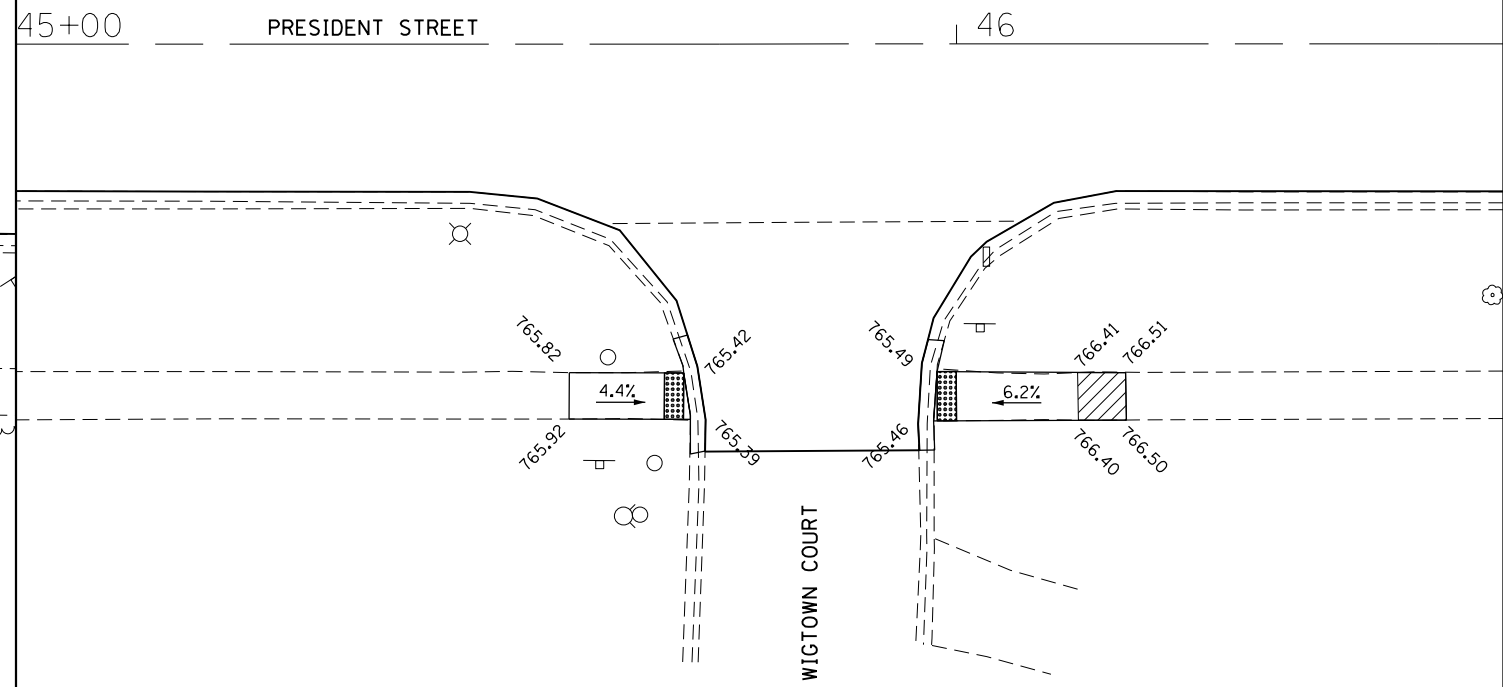
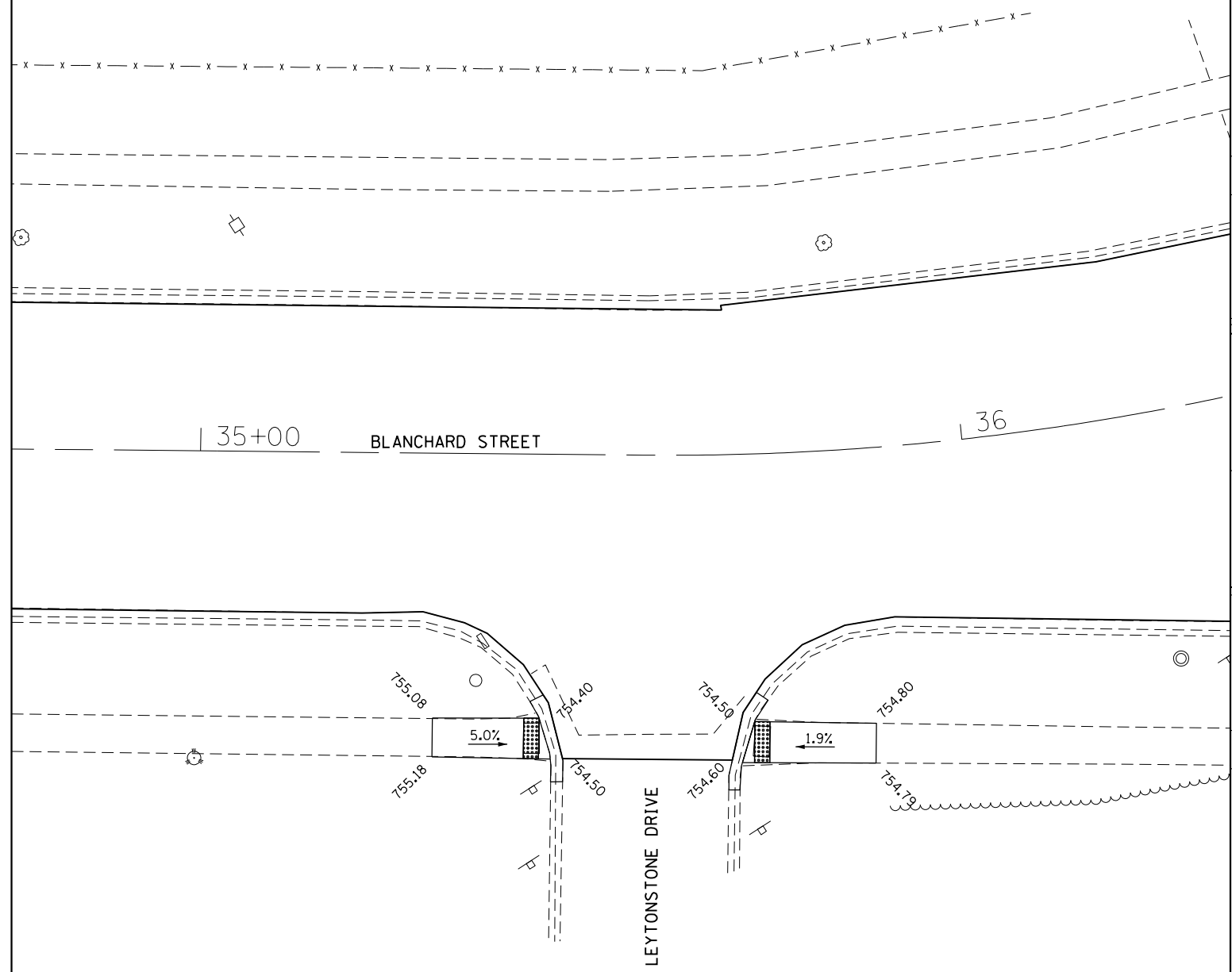
F.A.U. RTE. 2567 2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 22
CONTRACT: 61F50				
ILLINOIS FED. AID PROJECT				




LEGEND


 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

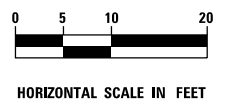


LEGEND

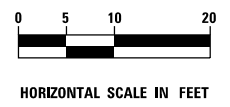
 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

NOTE:
1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



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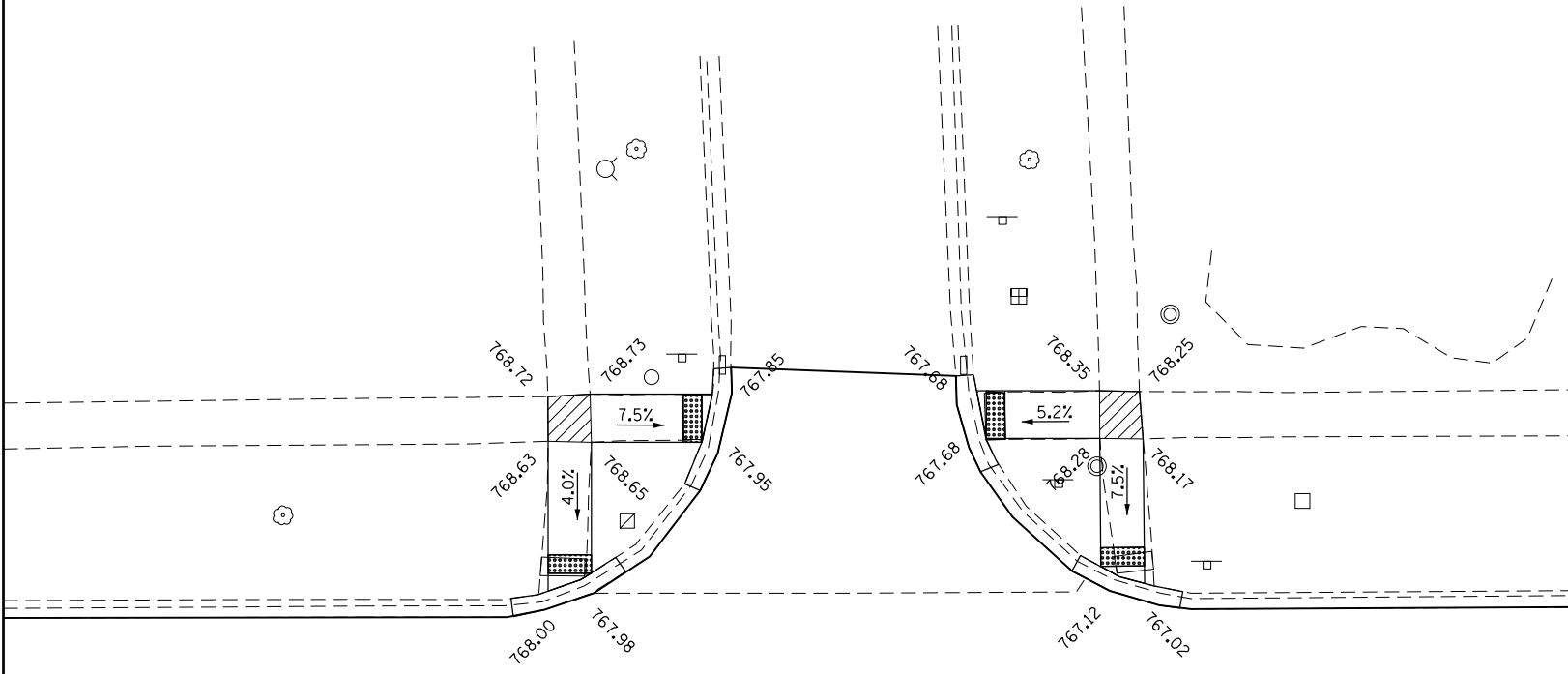
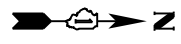
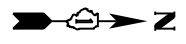
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PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

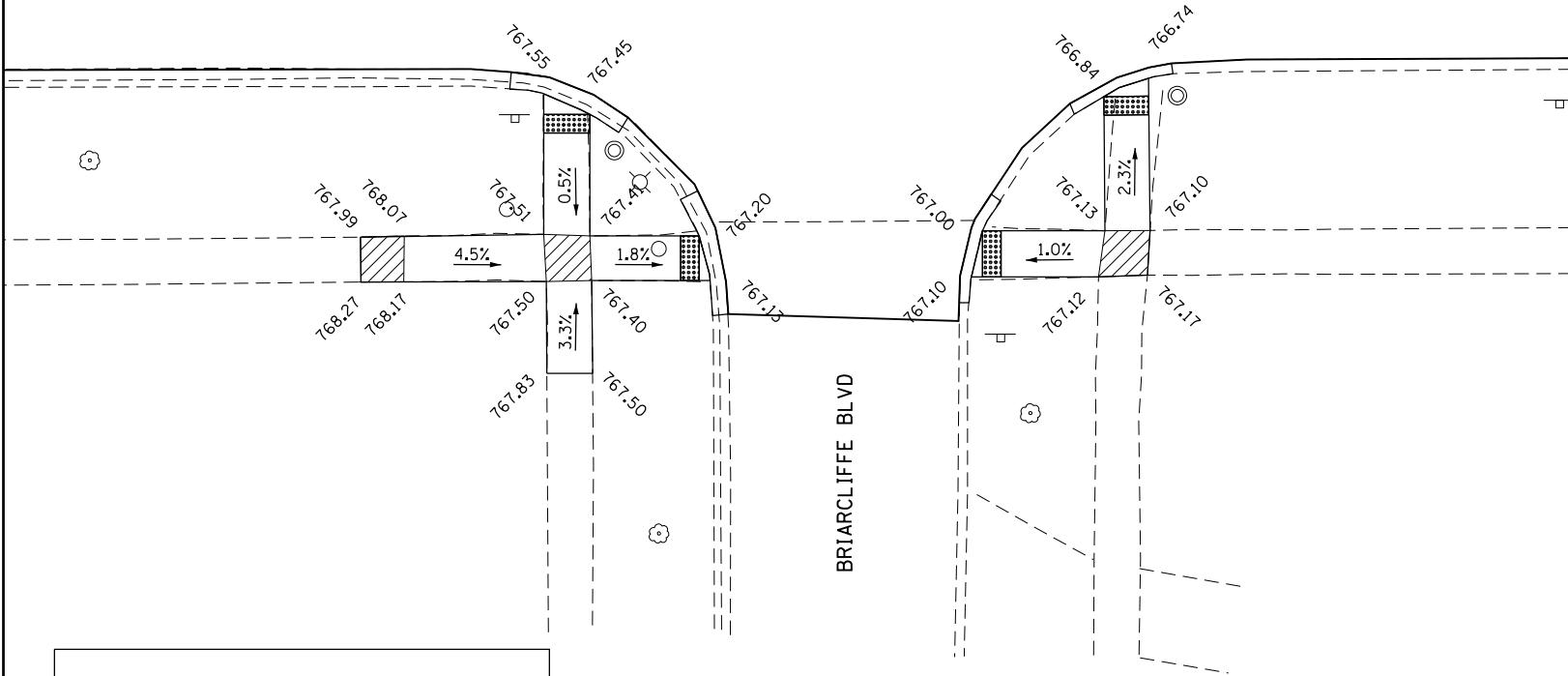
SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.


F.A.U. RTE. 2567 2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 23
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	




48 PRESIDENT STREET 49 PRESIDENT STREET

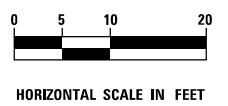


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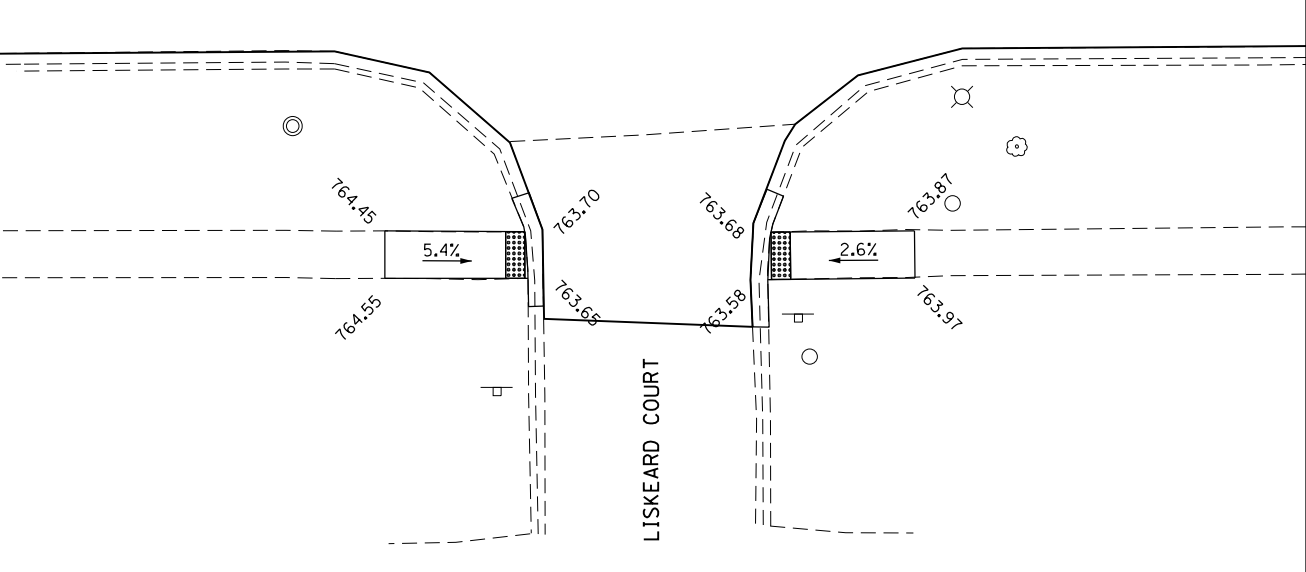
 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS


NOTE:
1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS




PRESIDENT STREET 52

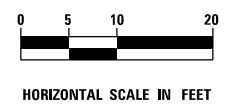


LEGEND

 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

NOTE:
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thomas thomas engineering group, llc
762 shoreline drive
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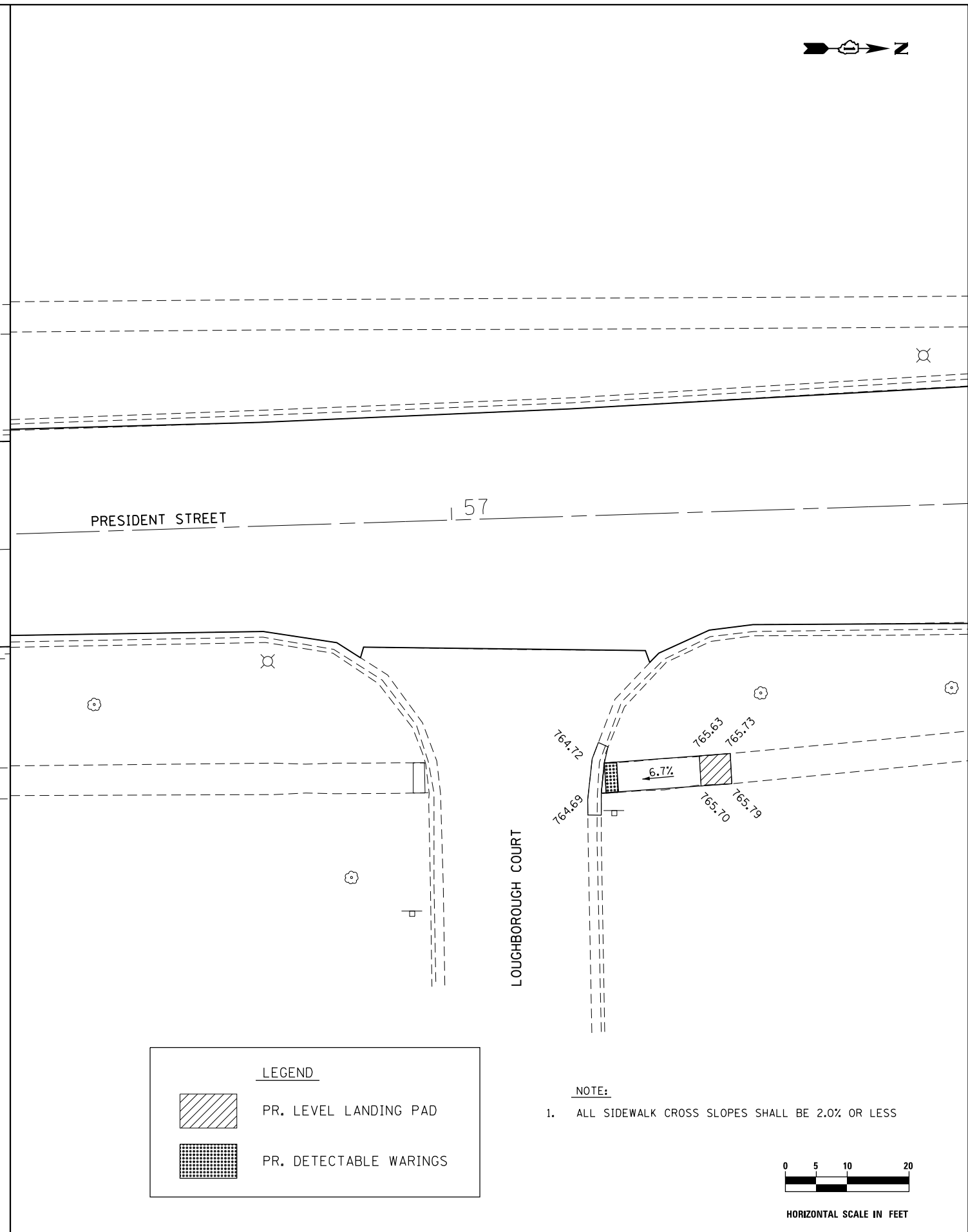
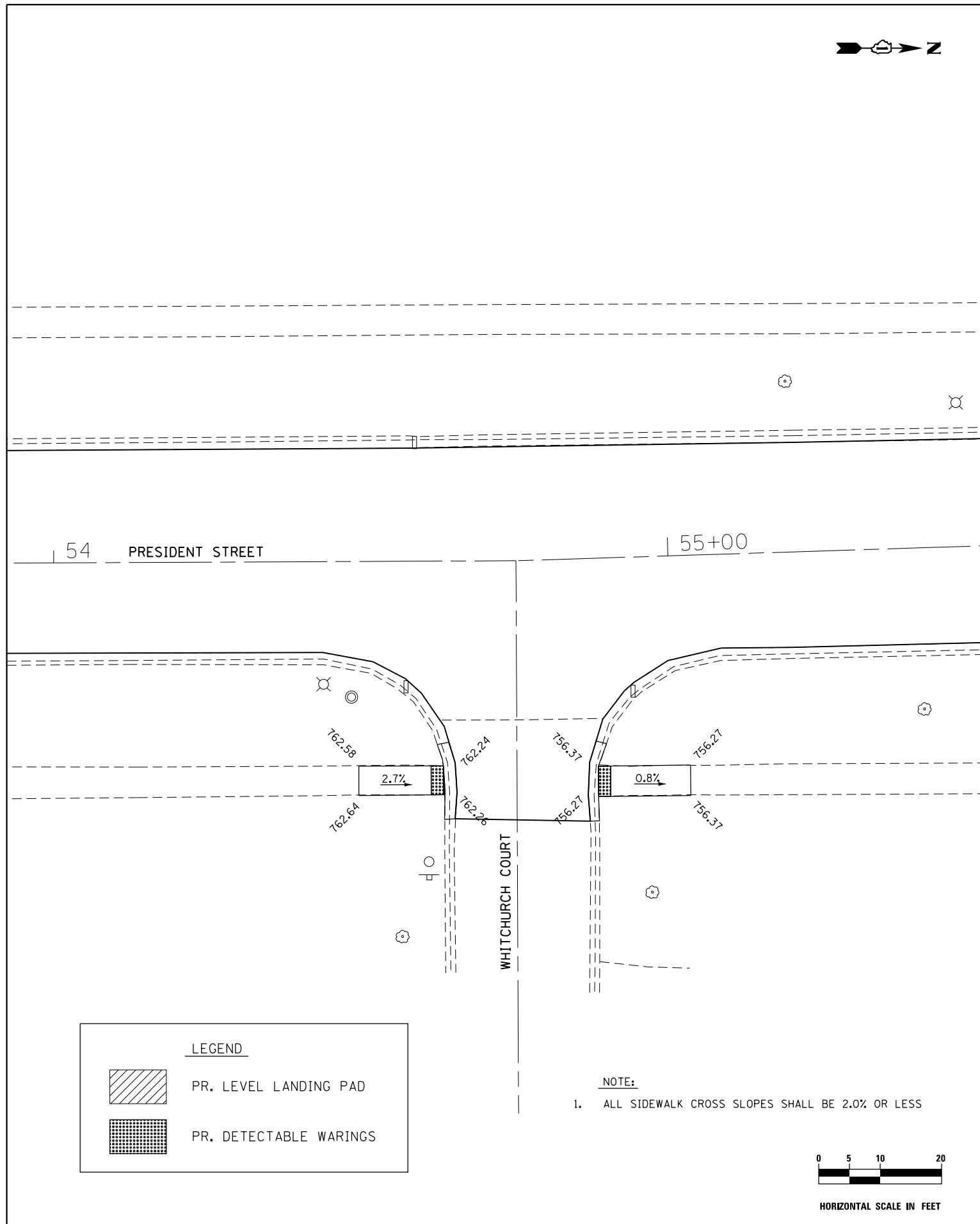
USER NAME = jamesy	DESIGNED -	REVISED
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK PLANS

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 24
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



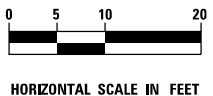
LEGEND

PR. LEVEL LANDING PAD

PR. DETECTABLE WARINGS

NOTE:

1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



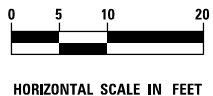
LEGEND

PR. LEVEL LANDING PAD

PR. DETECTABLE WARINGS

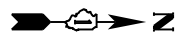
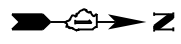
NOTE:

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



USER NAME = jamesj	DESIGNED -	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567 2569	18-00119-00-RS	DuPAGE	37	25
CONTRACT: 61F50				
ILLINOIS FED. AID PROJECT				





LEGEND

 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

LEGEND

 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

PRESIDENT STREET

60+00

PRESIDENT STREET

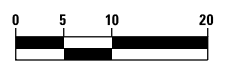
64

LOWDEN AVENUE

DAWES AVENUE

NOTE:

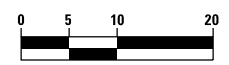
- 1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



HORIZONTAL SCALE IN FEET

NOTE:

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HORIZONTAL SCALE IN FEET

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phone: 855-533-1700

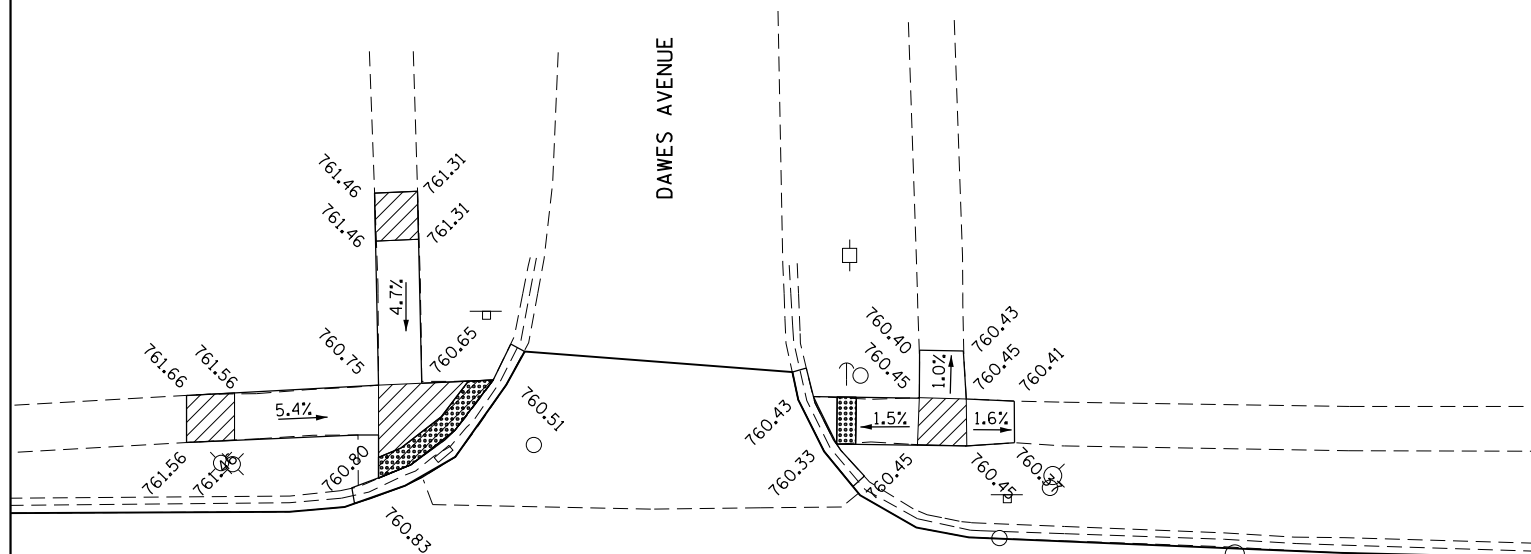
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PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 26
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	

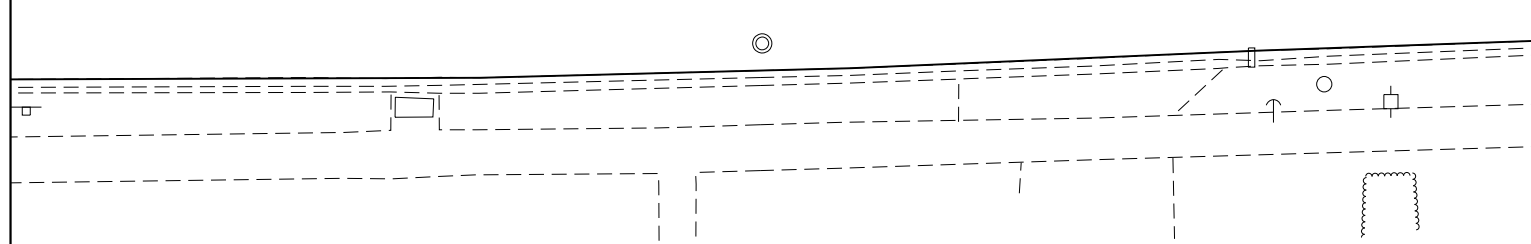


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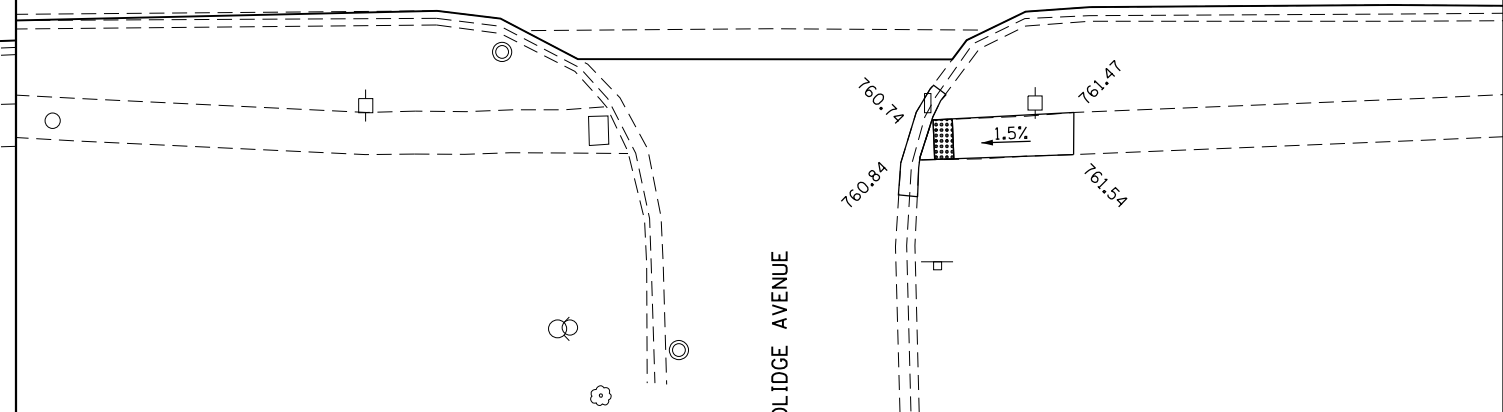
PR. LEVEL LANDING PAD

PR. DETECTABLE WARINGS

PRESIDENT STREET | 65+00

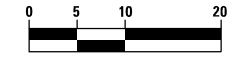


67 | PRESIDENT STREET | 68



NOTE:

1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



HORIZONTAL SCALE IN FEET

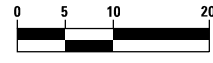
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PR. LEVEL LANDING PAD

PR. DETECTABLE WARINGS

NOTE:

1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS

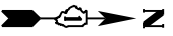


HORIZONTAL SCALE IN FEET


USER NAME = jamesj	DESIGNED -	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -


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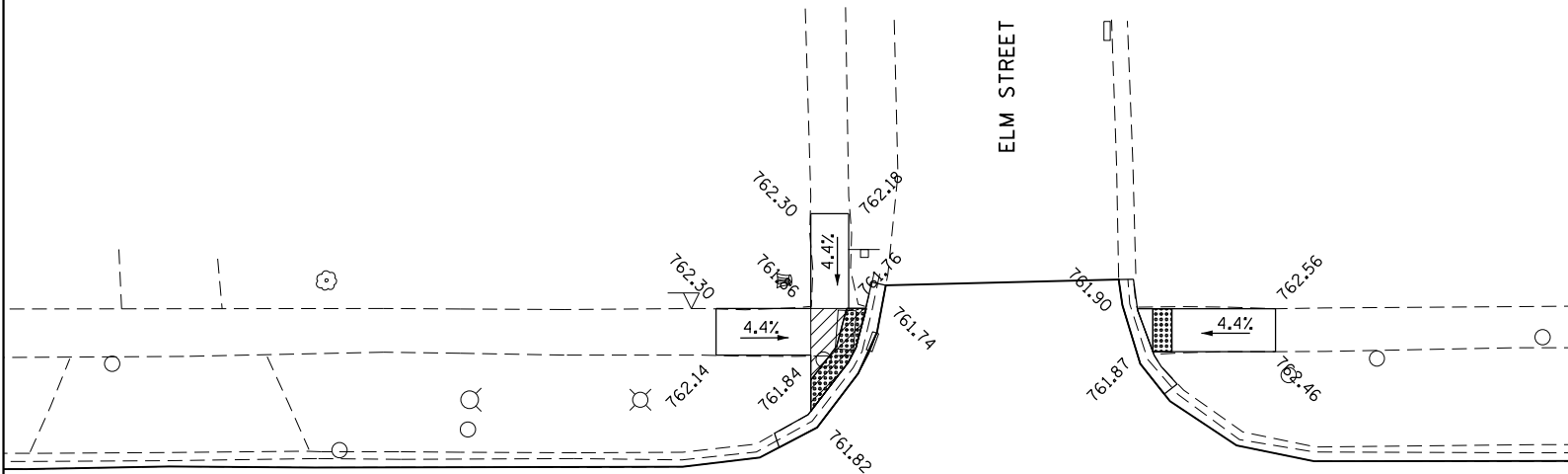
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CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



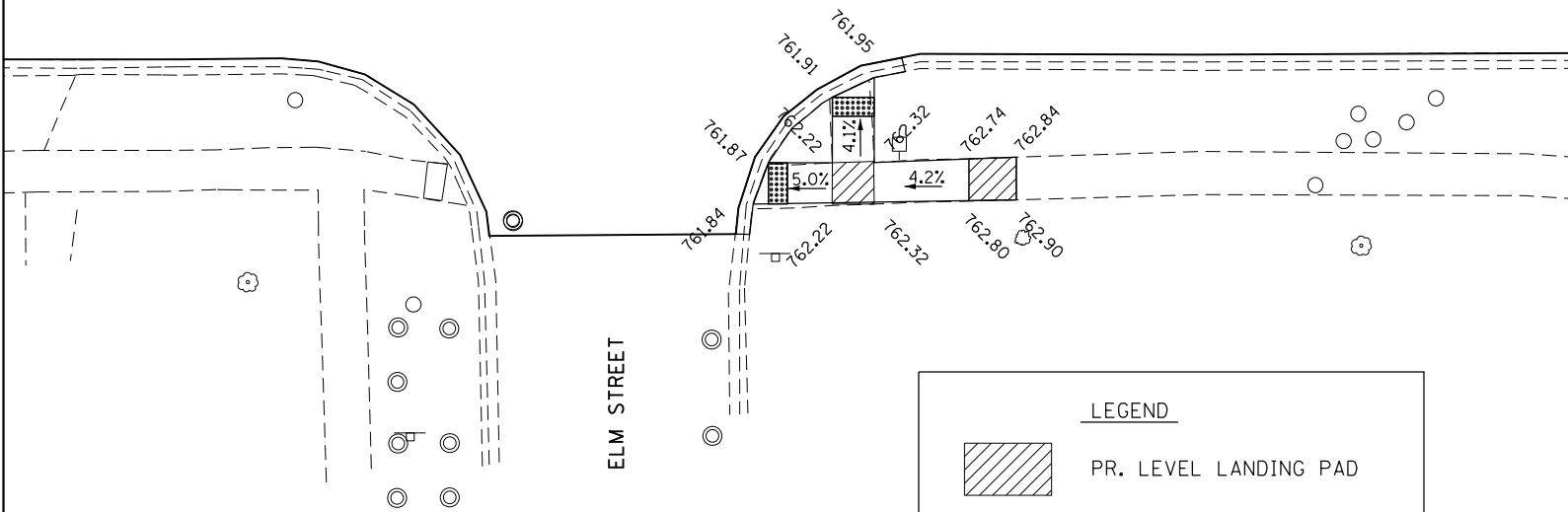
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 PR. LEVEL LANDING PAD


 PR. DETECTABLE WARINGS




71 PRESIDENT STREET 72



LEGEND

 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

NOTE:

- 1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



HORIZONTAL SCALE IN FEET

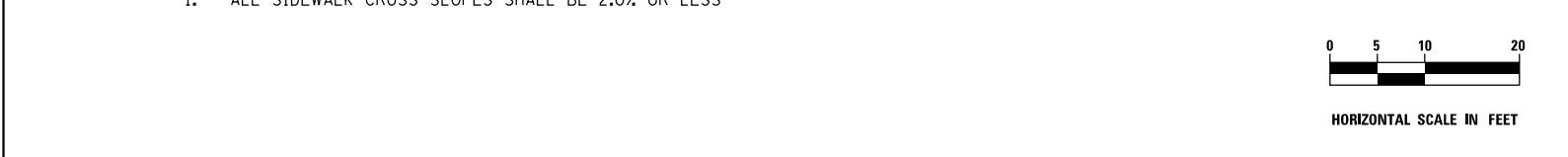
thomas
 thomas engineering group, llc
 762 shoreline drive
 suite 200
 aurora, illinois 60504
 phone: 855-533-1700

USER NAME = jamesj	DESIGNED -	REVISED
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.



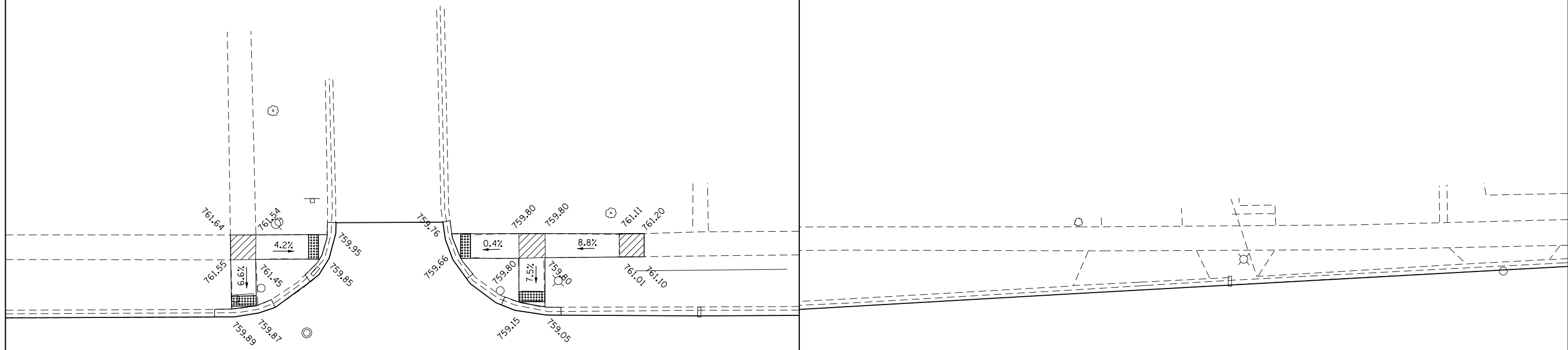
NOTE:

- 1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



HORIZONTAL SCALE IN FEET

F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 28
CONTRACT: 61F50			ILLINOIS FED. AID PROJECT	



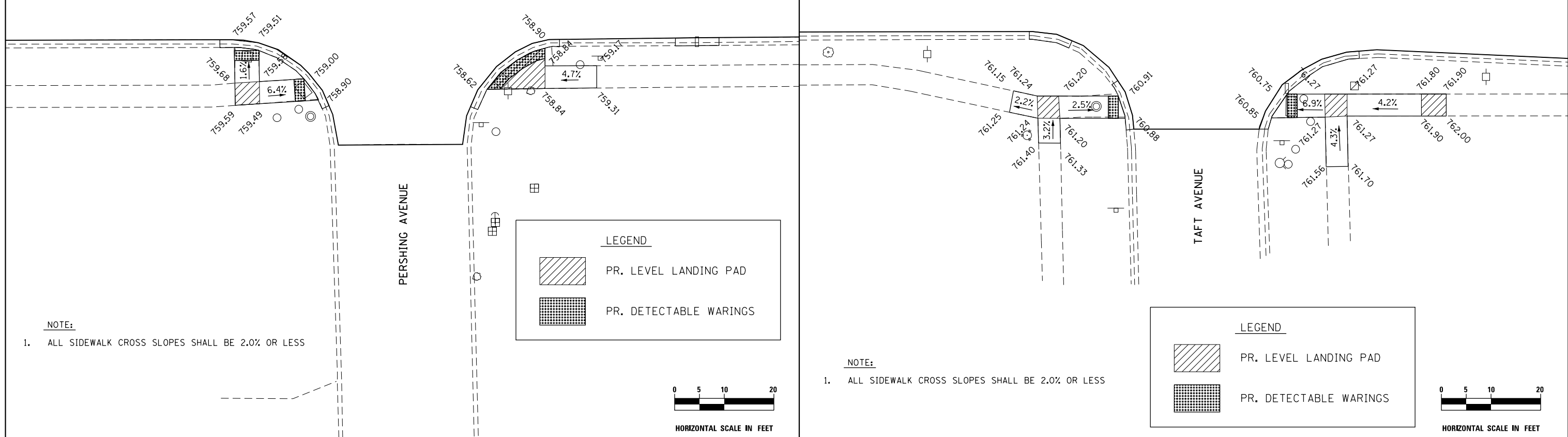
PRESIDENT STREET | 78

79

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
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


PERSHING AVENUE


TAFT AVENUE


LEGEND

 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

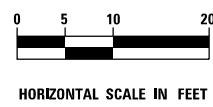
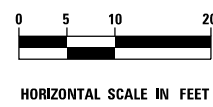
LEGEND

 PR. LEVEL LANDING PAD

 PR. DETECTABLE WARINGS

NOTE:
1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS

NOTE:
1. ALL SIDEWALK CROSS SLOPES SHALL BE 2.0% OR LESS



thomas
service at the highest grade

thomas engineering group, llc
762 shoreline drive
suite 200
aurora, illinois 60504
phone: 855-533-1700

USER NAME = jamesj	DESIGNED -	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

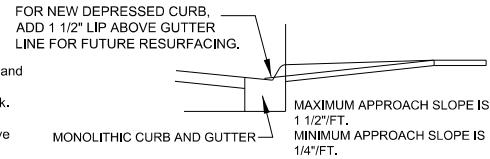
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F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 29
CONTRACT: 61F50				
ILLINOIS FED. AID PROJECT				

DRIVE APPROACH & SIDEWALK

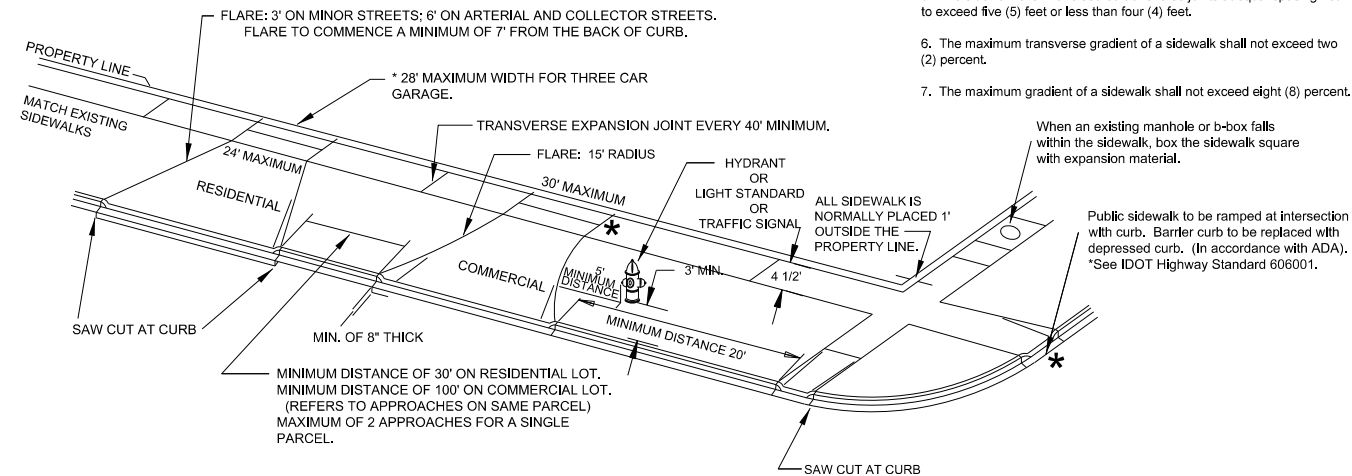
GENERAL STANDARDS

- All Portland Cement Concrete shall conform to Illinois Department of Transportation Class SI mix.
- Where new concrete work meets or abuts any existing concrete structures, the existing concrete shall be saw cut to a straight and clean edge and expansion material placed between the new and existing work.
- Expansion material is also required between new curb and new concrete approaches; new sidewalk and new concrete driveways; and new curb and new sidewalk.
- A culvert will be required where the street does not have curb and gutter.
- Replacement of barrier curb and gutter with depressed curb and gutter to be with a single pour. Gutter shall be a minimum of 8" thick.
- Wire mesh is not required.
- Pea gravel is not allowed as fill or base material.
- All curb replacements shall be a minimum of 5' in length.



** See Note 6

- COVER OVER CULVERT SHALL BE A MINIMUM OF 5' OR 1/2 CULVERT DIAMETER, OR WHICHEVER IS LARGER.
- MINIMUM CULVERT SIZE: 10" DIAMETER.



RESIDENTIAL APPROACH

- Shall be 5" thick, Portland Cement Concrete, Class SI
- 1 1/2" Bituminous Concrete, on an 8" thick aggregate base
- Brick pavers shall be constructed in accordance with the manufacturer's specifications. Manufacturer's specifications must be submitted as part of the permit application.

COMMERCIAL APPROACH

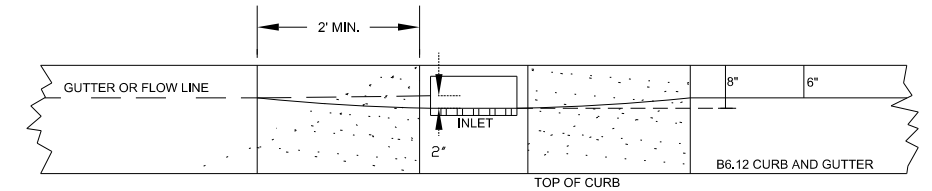
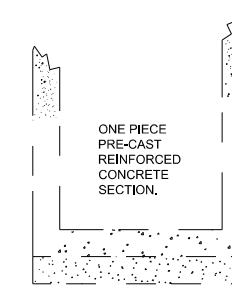
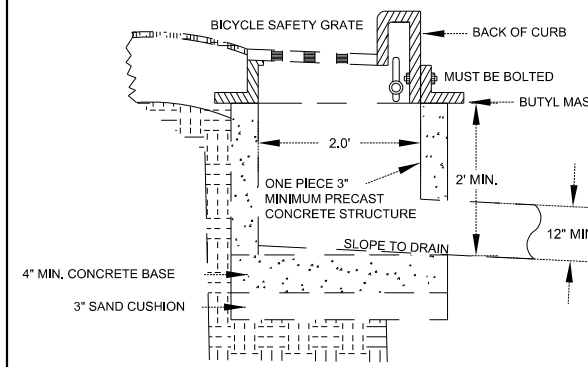
- 6" thick, Portland Cement Concrete, Class SI (Bituminous is not permitted)

SIDEWALK

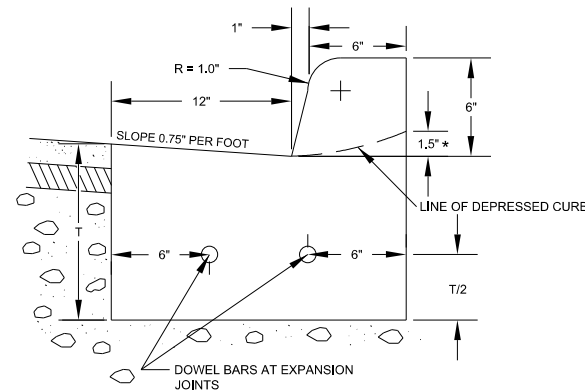
- 4" thick, Portland Cement Concrete on a compacted crushed aggregate or bank run gravel base.
- 5" thick through Residential Approaches
- 6" thick through Commercial Approaches
- Walk shall be no lower than the centerline of the street.
- The sidewalk shall have scored transverse joints at equal spacing not to exceed five (5) feet or less than four (4) feet.
- The maximum transverse gradient of a sidewalk shall not exceed two (2) percent.
- The maximum gradient of a sidewalk shall not exceed eight (8) percent.

TRANSITION CURB AT INLET OR C.B.

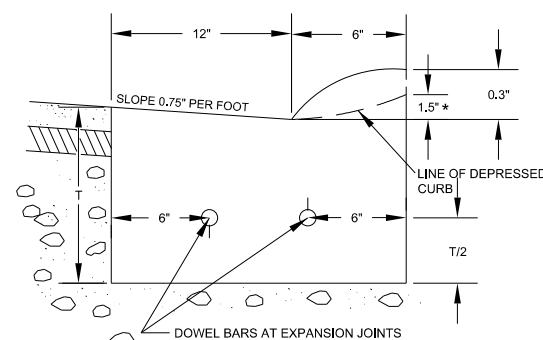
- A MAXIMUM ADJUSTMENT OF 8" USING 2 (TWO) RUBBER RINGS IS ALLOWED. A MINIMUM OF 1 (ONE) RUBBER RING SHOULD BE INSTALLED BETWEEN THE FRAME AND STRUCTURE
- BRICKS ARE NOT PERMITTED FOR ADJUSTMENTS.
- FOR ROLLED AND/OR BARRIER CURB USE NEENAH R-3010 OR EJIW 7045Z FRAME, 7040M1 GRATE AND 7050T1 CURB BACK OR US FOUNDRY 5132.
- ALL JOINTS BETWEEN BARREL SECTION, RISER, AND CASTING SHALL BE SEALED WITH FLEXIBLE BUTYL MASTIC MATERIAL 2-1/4" WIDE AND 3/8" THICK OR ENGINEER APPROVED SUBSTITUTE. MORTAR IS NOT ALLOWED.
- THE GRATE SHALL BE A BICYCLE SAFETY GRATE.
- REFERENCE: STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, MAY 1996, SECTION 33.
- ANY SUBSTITUTIONS MUST BE APPROVED BY THE ENGINEER.
- 2' INSIDE DIAMETER TO BE USED AT END OF PIPE RUN ONLY. IN ALL INSTANCES WHERE 2 OR MORE PIPES ENTER OR EXIT C.B., 4' INSIDE DIAMETER STRUCTURE MUST BE USED.
- MINIMUM PIPE DIAMETER IS 12" ON 2' STRUCTURE.
- ANY NEW FRAME BACKS MUST HAVE STAMPED FISH SYMBOL PER NPDES REQUIREMENTS.



BARRIER CURB AND GUTTER TYPE B 6.12

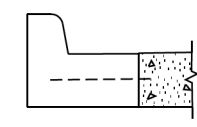
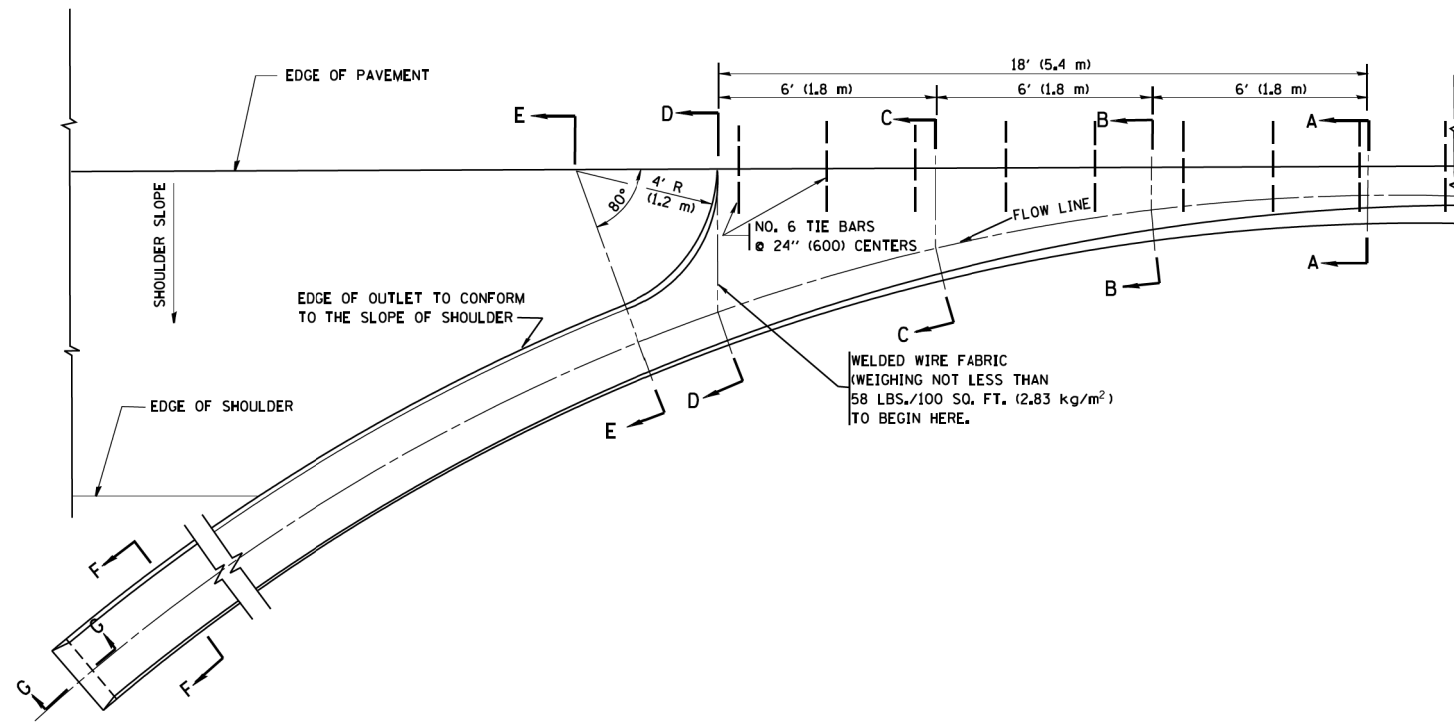


TYPE M 3.12



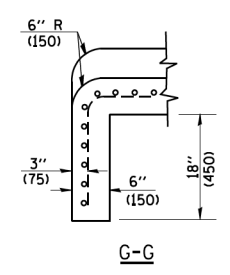
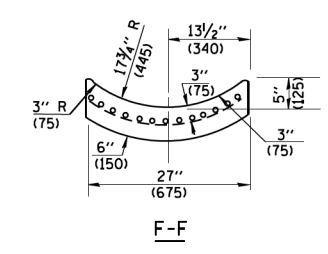
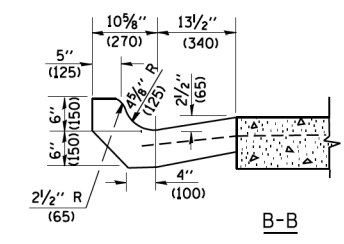
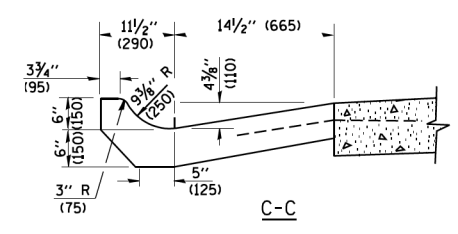
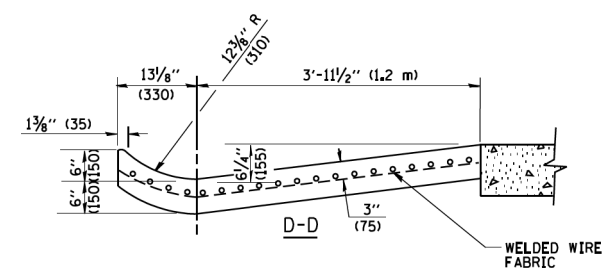
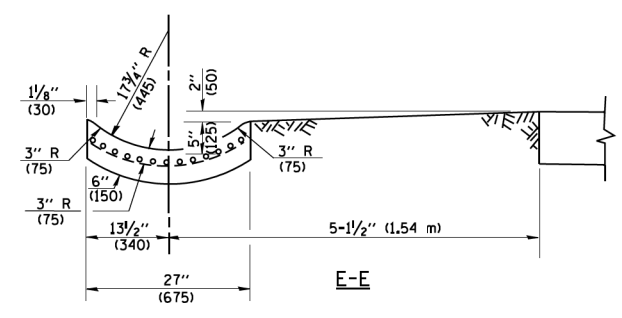
T = THICKNESS OF PAVEMENT
* = 1/2" AT ADA RAMP LOCATIONS

- NOTE:
- TWO NO.6 STEEL DOWEL BARS WITH CAPS TO BE USED AT ALL EXPANSION JOINTS 100 ft. INTERVALS.
 - CONSTRUCTION JOINTS AT 25 ft. INTERVALS.
 - THREE 3/8" RE-BARS AT ALL TRENCH CROSSINGS. BARS TO EXTEND TWO FEET BEYOND EDGE OF TRENCH AND HAVE A MINIMUM LENGTH OF 10 FT.
 - THE BASE COURSE WILL BE OF A DEPTH SUFFICIENT TO BRING THE CURB AND GUTTER TO THE PROPOSED GRADE.
 - THE BASE COURSE LOCATED UNDER THE CURB AND GUTTER WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR COMBINATION CONCRETE CURB AND GUTTER OF THE TYPE SPECIFIED.
 - THE HEIGHT OF DEPRESSED CURB ADJACENT TO ADA RAMP LOCATIONS SHALL BE 1/2'.
 - SEE STANDARD 606001-06.



A-A *

* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



GENERAL NOTES

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO.6) AT 24\" (600) CENTERS UNLESS OTHERWISE SHOWN.

IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6' (1.8 m) FOR EACH 1% INCREASE IN GRADE.

QUANTITIES

FOR SECTION A-A TO E-E AND CURTAIN WALL =
 1.25 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 9\" (225) PAV'T.
 1.27 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 10\" (250) PAV'T.
 FOR SECTION F-F =
 0.045 CU. YDS. (0.03 m³) CLASS SI CONCRETE PER FT. (m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststa\22x34\bd03.dgn	USER NAME = gegl1enobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OUTLET FOR CONCRETE CURB AND GUTTER			F.A.U. RTE. 2567 2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 31
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. SHAH 10-25-94		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			BD600-01 (BD-03) CONTRACT NO. 61F50				
	PLOT DATE = 1/4/2008	DATE - 08-04-86	REVISED - E. GOMEZ 12-21-00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
			REVISED -									

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

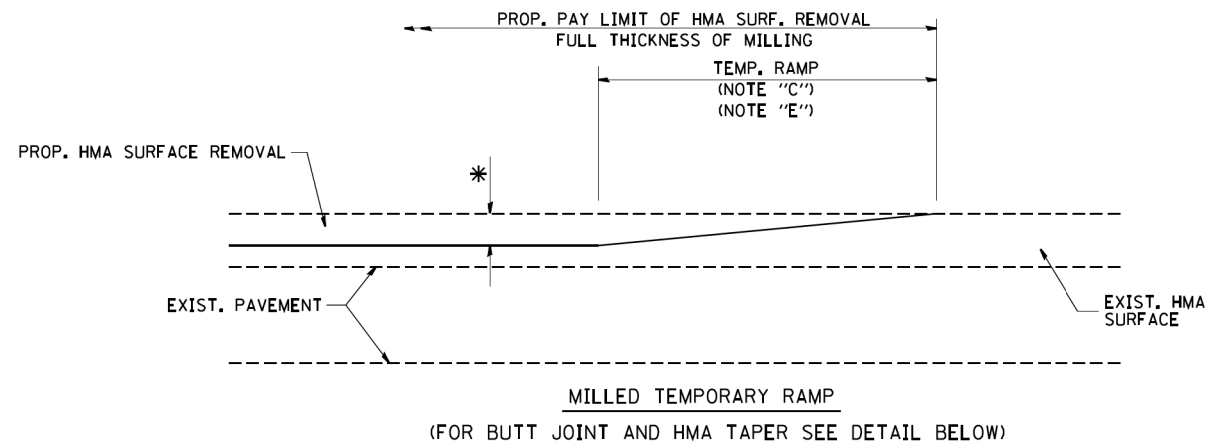
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

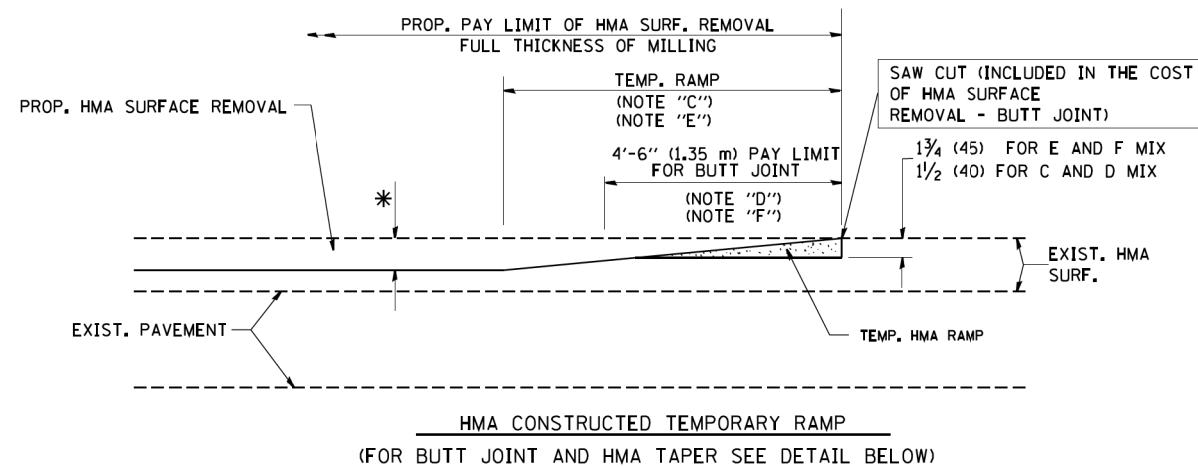
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = d-ivkosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\p\work\p\dot\drivkosgn\d0108315\bc24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			2567	18-00119-00-RS	DuPAGE	37	33	
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 61F50			
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

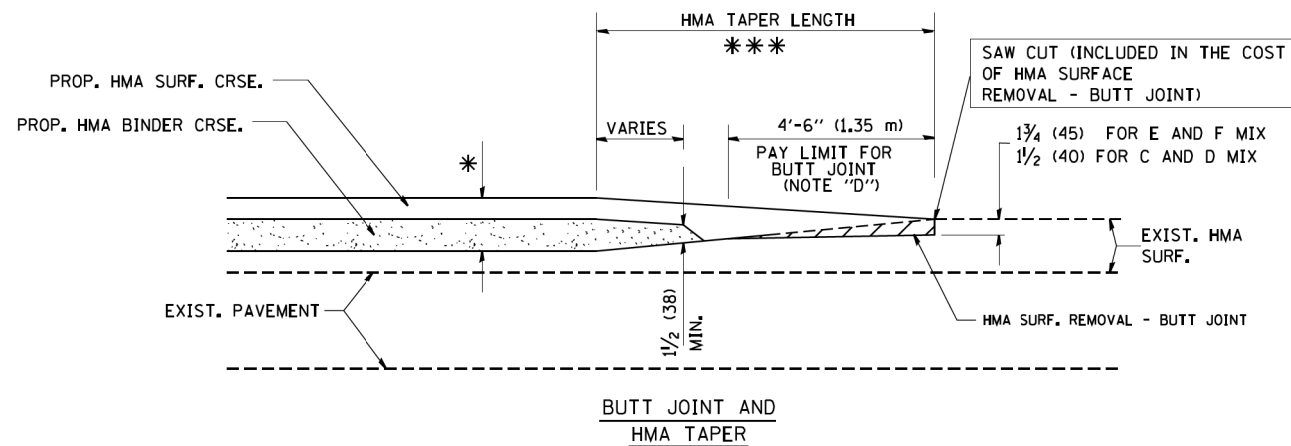


OPTION 1

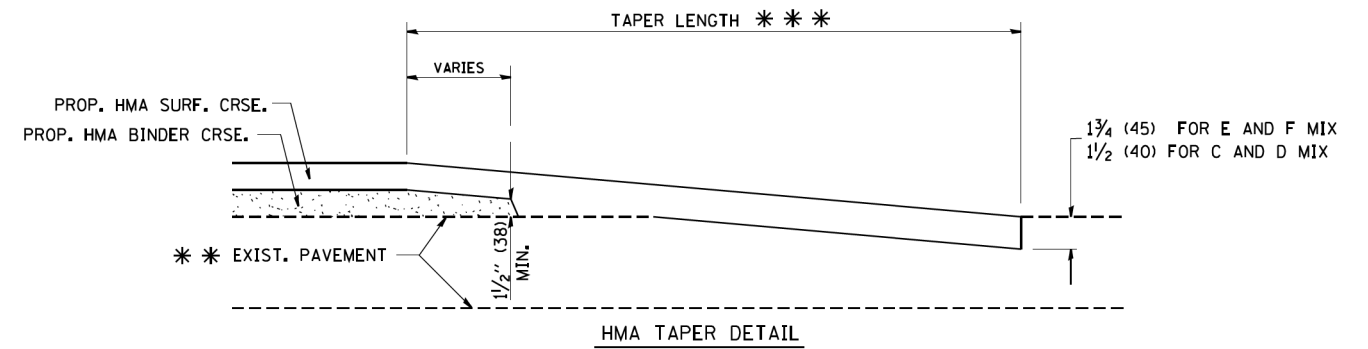
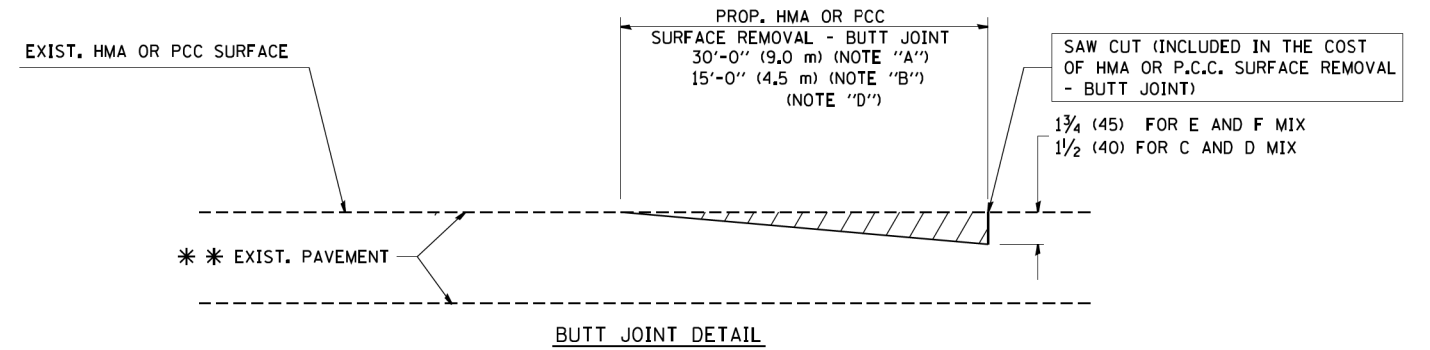


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd32.dgn

USER NAME = gegl1enobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

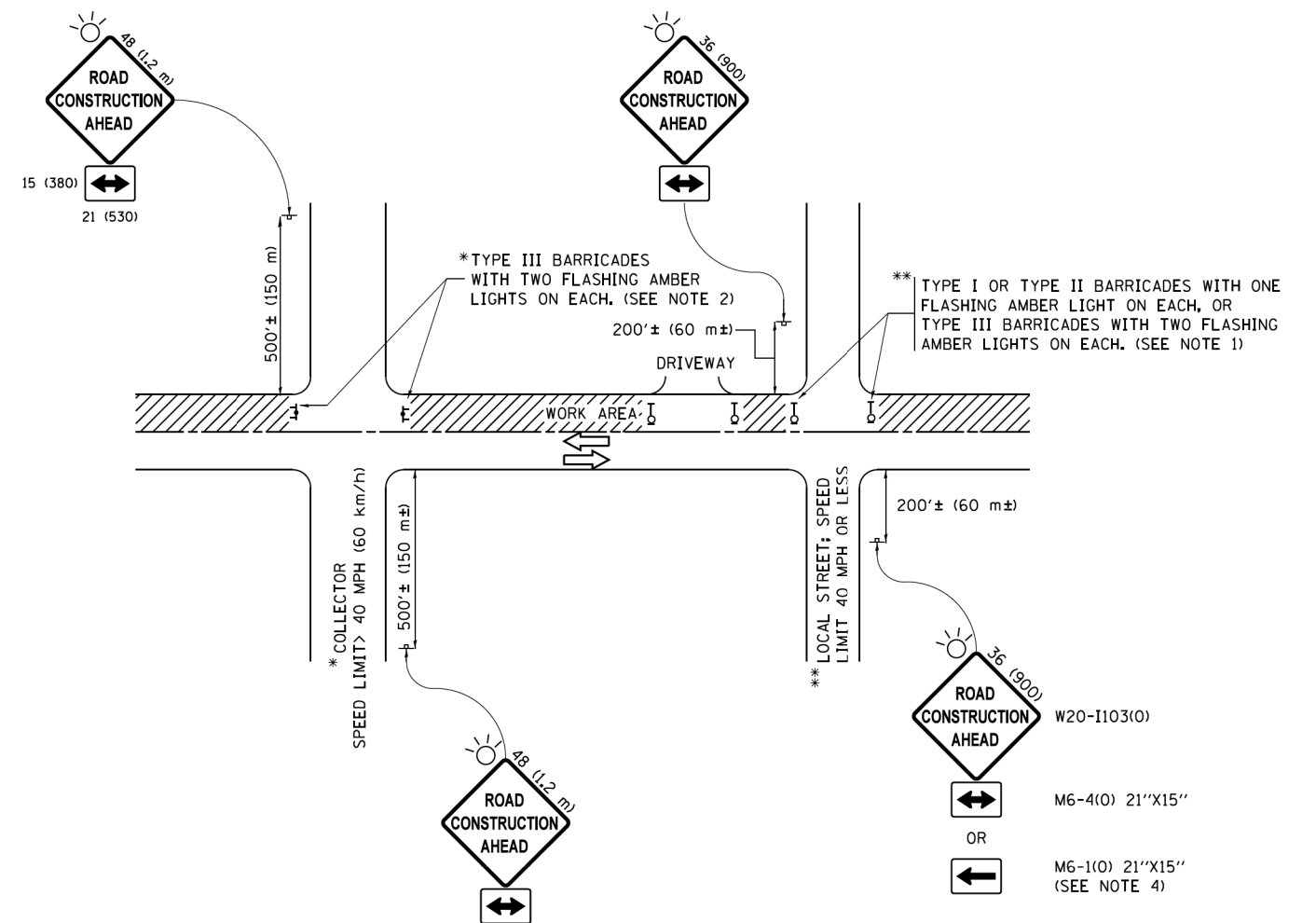
REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 34
BD400-05 BD32		CONTRACT NO. 61F50		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

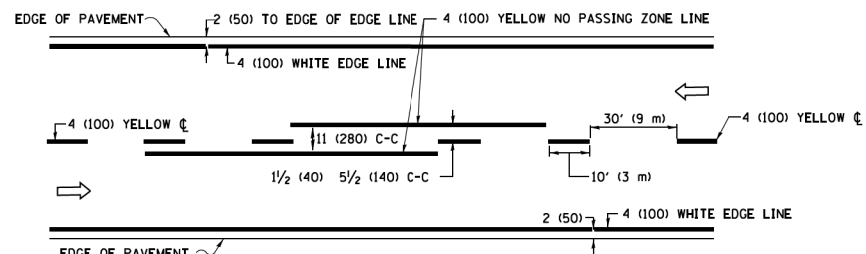
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

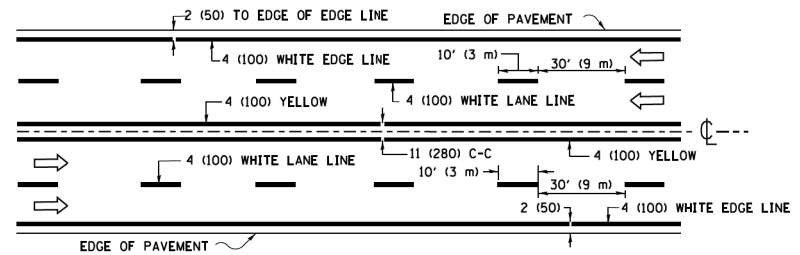
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

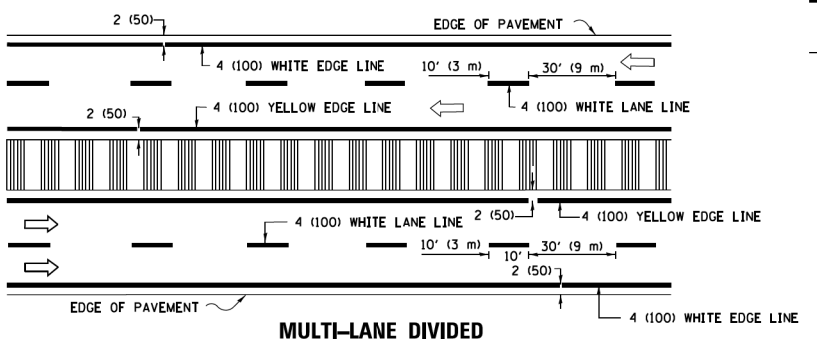
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TC-10		CONTRACT NO. 61F50		
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

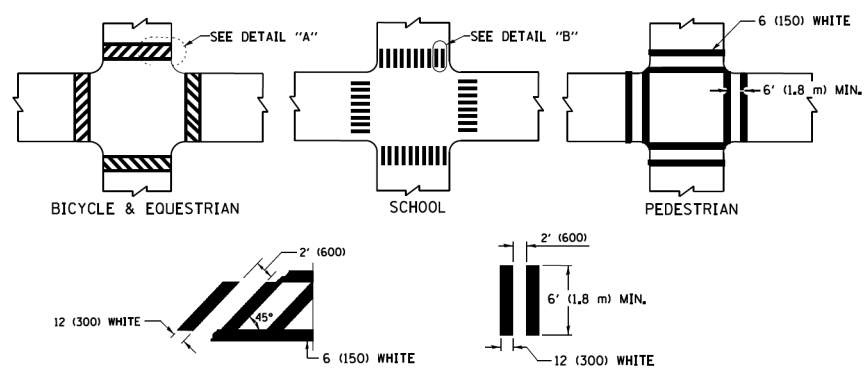


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

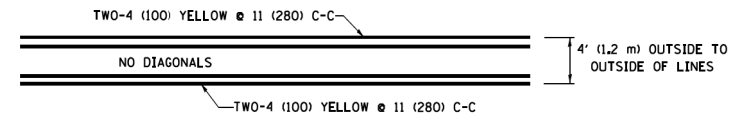


DETAIL "A"

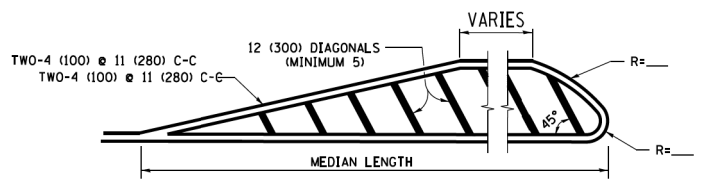
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



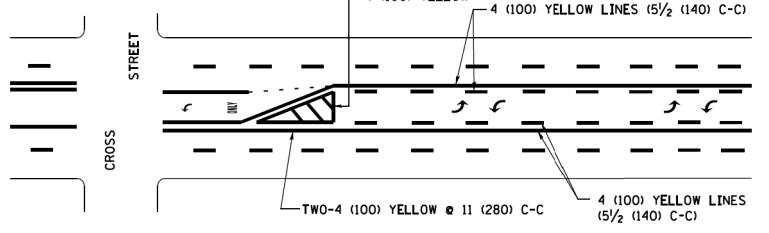
4' (1.2 m) WIDE MEDIANS ONLY



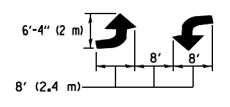
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

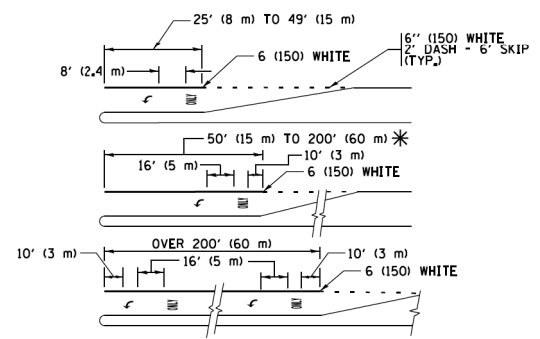


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

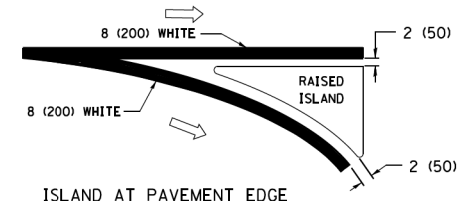
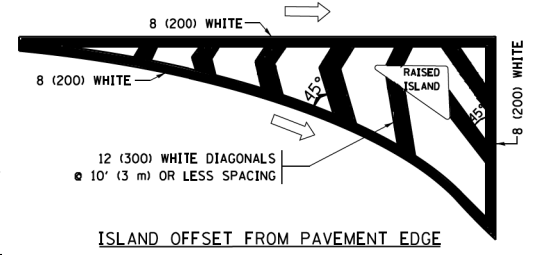


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

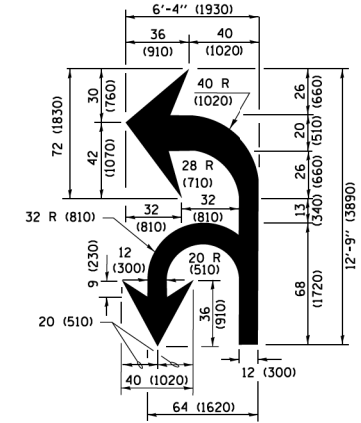
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

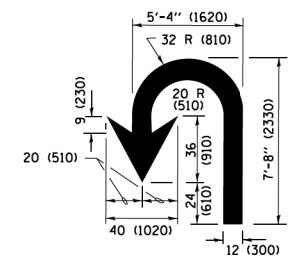
TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS; 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

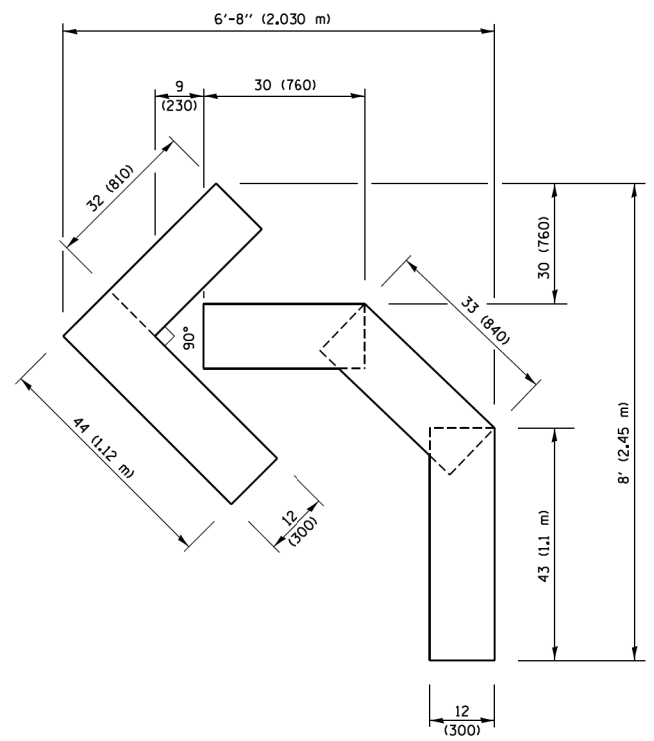
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

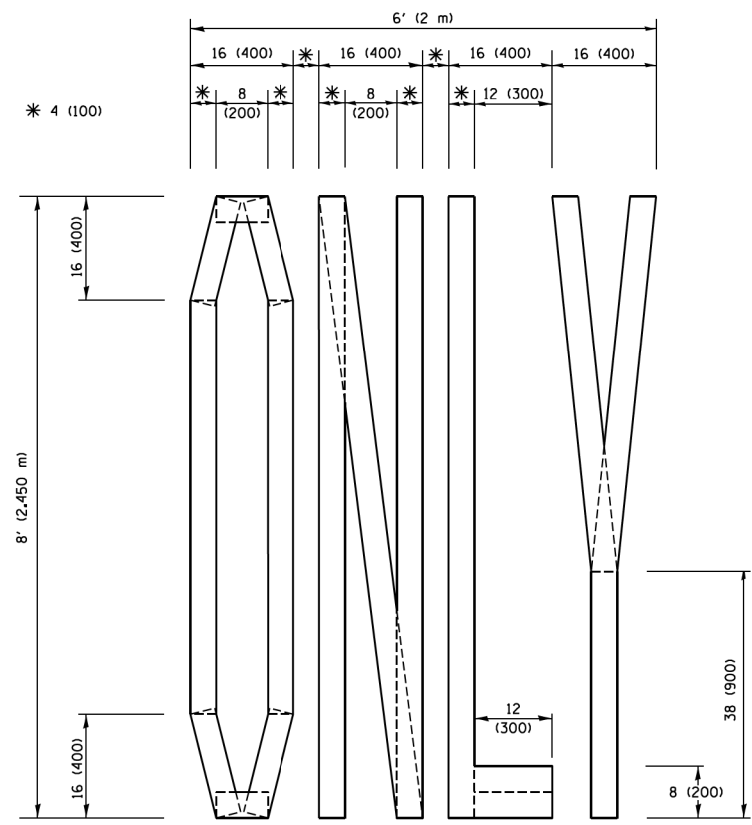
**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

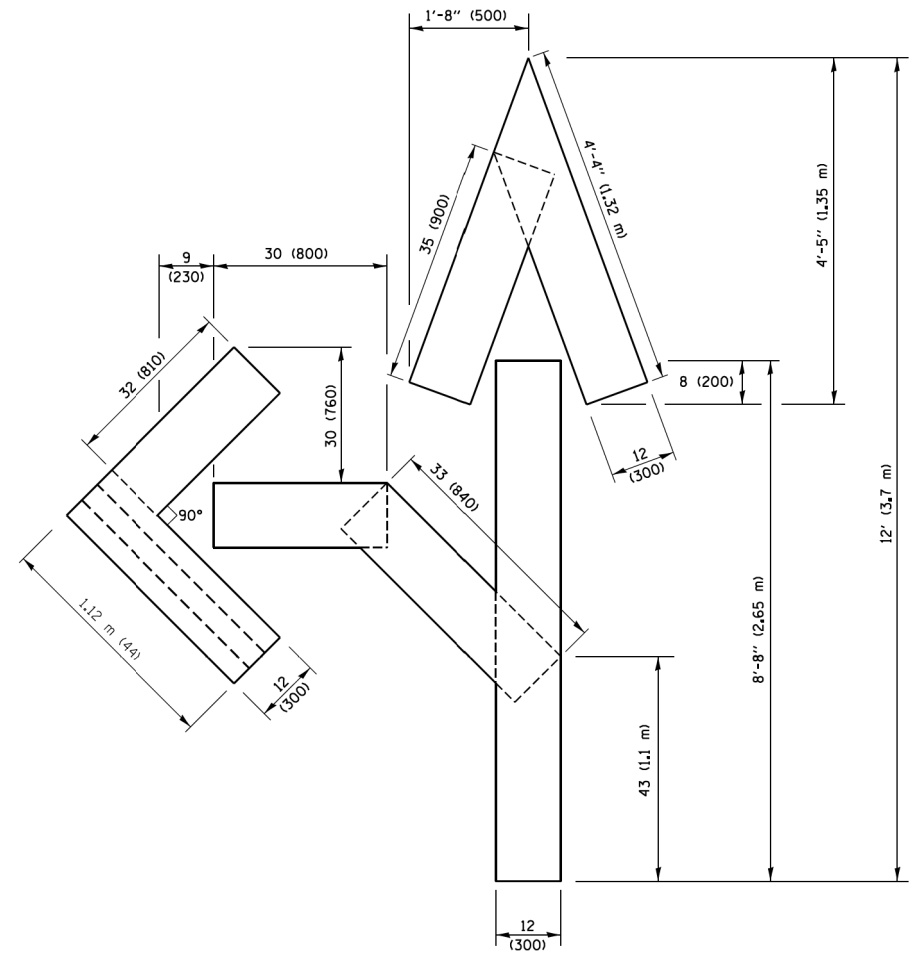
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567 2569	18-00119-00-RS	DuPAGE	37	35
TC-13		CONTRACT NO. 61F50	ILLINOIS FED. AID PROJECT	



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

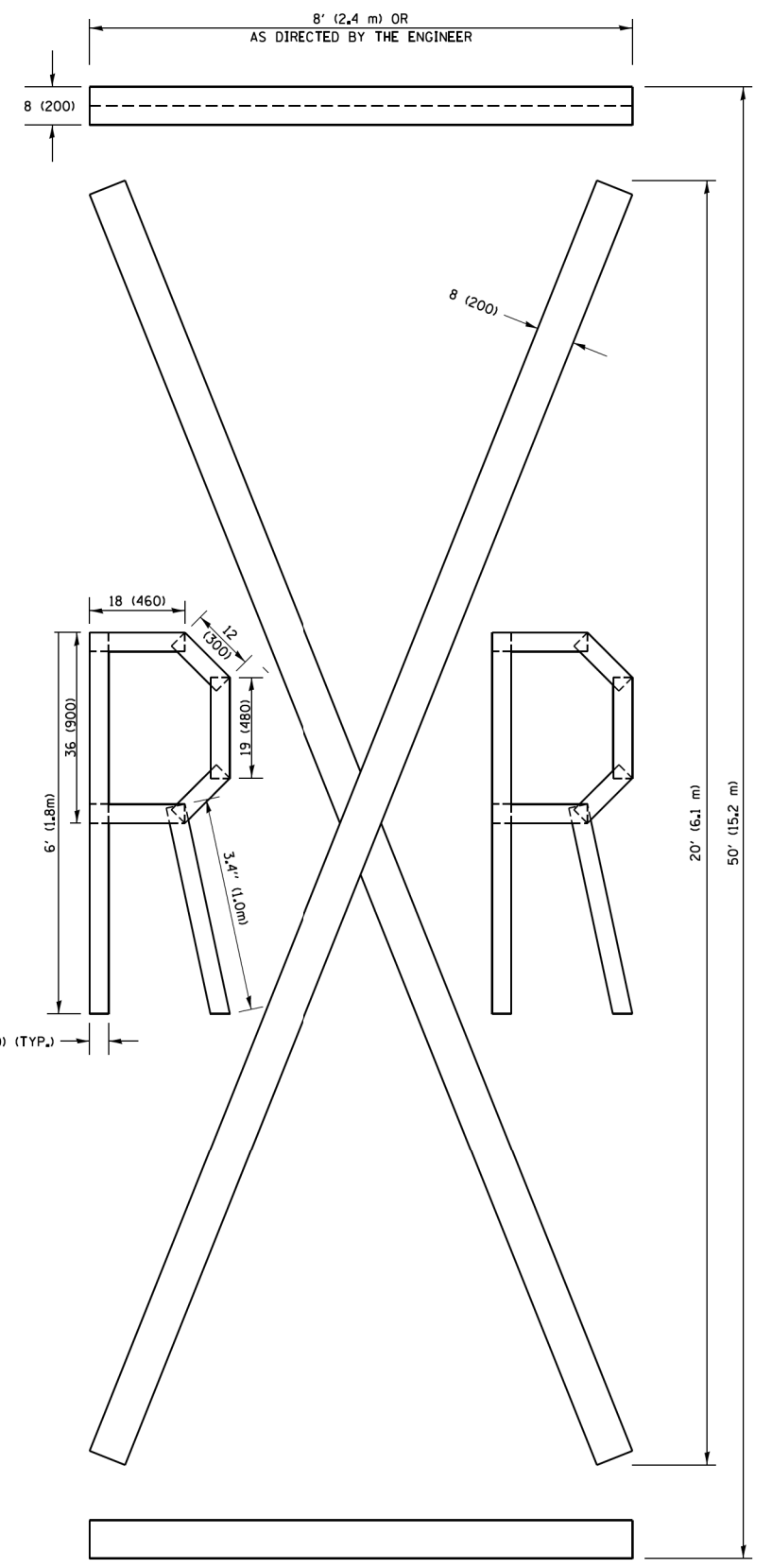


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
pw\1\084E81D\INTEG\illinois.gov\FWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\CADD\cadd\CAD\Sheets\to16.dgn		DRAWN -	REVISED -E. GOMEZ 08-28-00
PLOT SCALE = 50,0000' / 1"		CHECKED -	REVISED -E. GOMEZ 08-28-00
PLOT DATE = 9/15/2016		DATE -	REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE. 2567/2569	SECTION 18-00119-00-RS	COUNTY DuPAGE	TOTAL SHEETS 37	SHEET NO. 37
TC-16		CONTRACT NO. 61F50		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				