

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	1

DISTRICT ONE-PLAN PREPARATION ENGINEER K. ENG / LONG TRAN (847) 705-4240

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**  
F.A.P. 336: IL 31 (FRONT STREET)  
BULL VALLEY RD. TO MCCULLOM LAKE RD.  
SECTION: 112 RS-5  
RESURFACING (MAINTENANCE)  
MCHENRY COUNTY  
C-91-426-06

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT IS LOCATED IN THE CITY OF MCHENRY

D-91-426-06

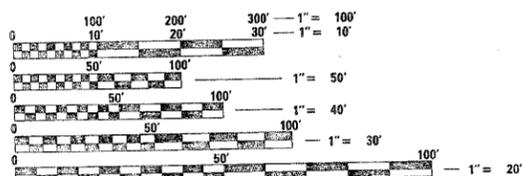
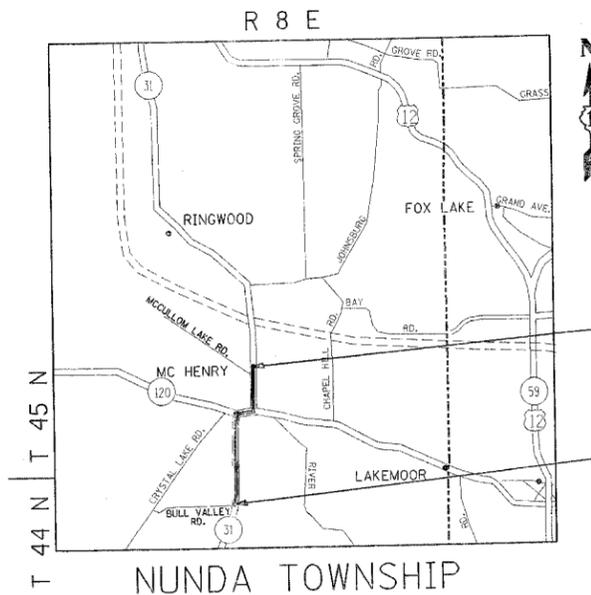


OMISSION:  
STA. 68+08 TO 137+30  
STA. 151+16 TO 169+96

TRAFFIC DATA  
SPEED LIMIT: 30-45 MPH  
2005 ADT: 18,100

IMPROVEMENT ENDS  
STA. 173+71

IMPROVEMENT BEGINS  
STA. 31+92



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

CONTRACT NO. 60B77

GROSS LENGTH OF IMPROVEMENT = 14,179 FEET (2.7 MILES)  
NET LENGTH OF IMPROVEMENT = 5,377 FEET (1.02 MILES)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED March 6 2007  
Dione O'Keefe  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 23 2007  
Eric E. Harshbarger  
INTERIM ENGINEER OF DESIGN AND ENVIRONMENT

March 23, 2007  
Milton R. Sear  
DIRECTOR OF HIGHWAY, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	2
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

CONTRACT NO. 60877

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4-6	TYPICAL SECTIONS
7-18	ROADWAY & PAVEMENT MARKING PLANS
19-20	DETECTOR LOOP REPLACEMENT PLANS
21	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT
22	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
23	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS
24	TRAFFIC CONTROL AND PROTECTION FOR SIDES ROADS, INTERSECTIONS AND DRIVEWAYS
25	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
26	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
27	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
28	PAVEMENT MARKING LETTER AND SYMBOLS FOR TRAFFIC STAGING
29	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
30	SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS
31	TEMPORARY INFORMATION SIGNING
32-33	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAIL
34	DETAILS FOR FRAME AND LIPS ADJUSTMENT WITH MILLING

STATE STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-02	CLASS C AND D PATCHES
606001-03	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB & GUTTER
701201-02	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >45MPH
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-01	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45MPH
701311-02	LANE CLOSURE 2L, 2W MOVING OPERATIONS-DAY ONLY FOR SPEEDS>= 45MPH
702001-06	TRAFFIC CONTROL DEVICES

GENERAL NOTES:

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OR BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF MCHENRY.
- THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- ALL HOT-MIX ASPHALT PAVEMENT PATCHING SHALL BE CLASS D.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND HOT MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT MS. DEBBIE HANLON, AT (847) 438-2300 A MINIMUM OF 72 HOURS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- 3 METER (10 FEET) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATING PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURE.
- THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION <b>IL 31 (FRONT ST.)</b> <b>INDEX OF SHEETS STATE STANDARDS AND GENERAL NOTES</b>
NAME	DATE	
		SCALE: VERT. HORIZ. DATE 3/6/2007
DRAWN BY AA		CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	3
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	
D-91-426-06				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		URBAN 1000 100% STATE				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	20.2	20.2				
40600300	AGGREGATE (PRIME COAT)	TON	102	102				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15.1	15.1				
40600895	CONSTRUCTING TEST STRIP	EACH	1	1				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	297	297				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	92	92				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2464	2464				
42001300	PROTECTIVE COAT	SQ YD	14	14				
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	6378	6378				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	22096	22096				
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	50	50				
44002227	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 6 3/4"	SQ YD	242	242				
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	180	180				
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	36	36				
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	26	26				
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	5				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				
67100100	MOBILIZATION	L SUM	1	1				
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1				
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1				
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	4761	4761				
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	624	624				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	17460	17460				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2288	2288				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		URBAN 1000 100% STATE				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	681	681				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1090	1090				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	202	202				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	529	529				
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	624	624				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17460	17460				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2288	2288				
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	681	681				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1090	1090				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	202	202				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	344	344				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	81	81				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	449	449				
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4				
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	956	956				

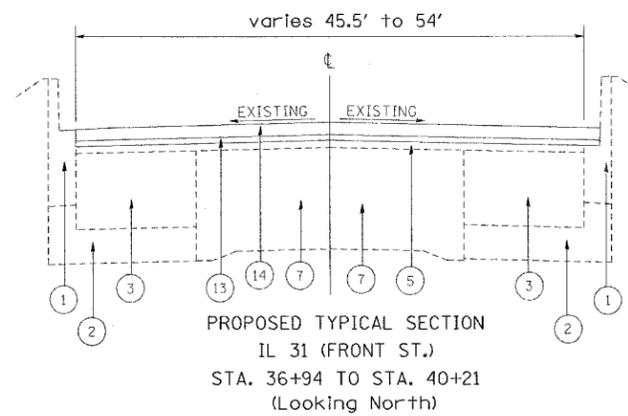
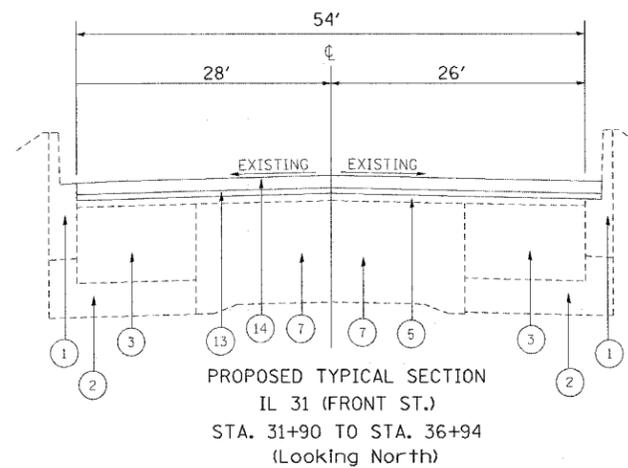
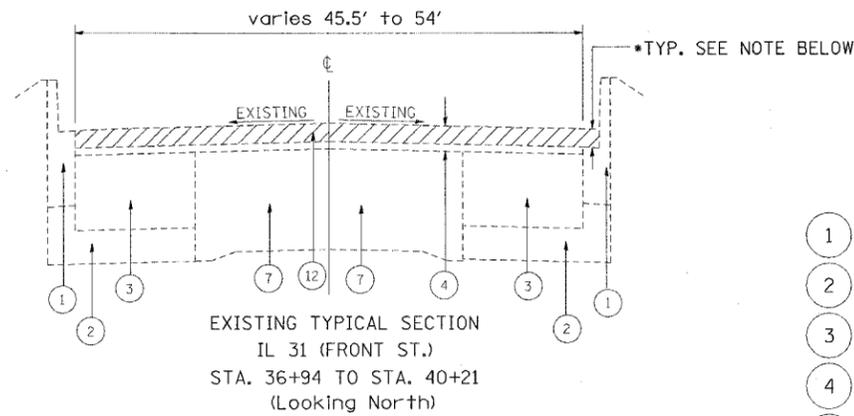
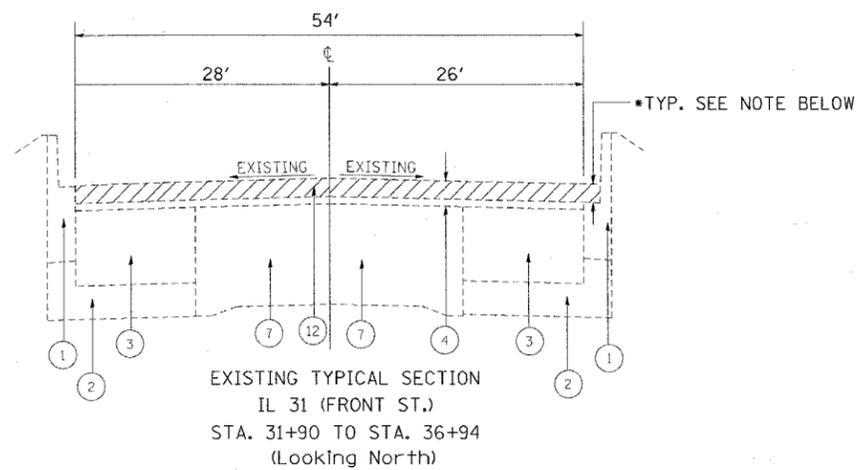
\* SPECIALTY ITEMS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
SUMMARY OF QUANTITIES  
IL 31 (FRONT ST.)

3/6/2007

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	4
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



**LEGEND**

- 1 EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B6.24 (MODIFIED)
- 2 EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B (4")
- 3 EXISTING PC CONC. BASE COURSE 8" ±
- 4 EXISTING HOT-MIX ASPHALT OVERLAY VARIES 6 3/4" ±
- 5 EXISTING HOT-MIX ASPHALT OVERLAY AFTER MILLING
- 6 EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B6.12
- 7 EXISTING PC CONC. BASE COURSE 9" ±
- 8 EXISTING BITUMINOUS BASE COURSE 10 3/4"
- 9 EXISTING BITUMINOUS CONCRETE PARKING (3" & VAR.)
- 10 EXISTING BITUMINOUS CONC. BASE COURSE WIDENING 9"
- 11 EXISTING RAISED CONCRETE MEDIAN
- 12 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- 13 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- 14 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- 15 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"

**MIXTURE REQUIREMENTS**

MIXTURE USES	PAVEMENT RESURFACING		PATCHING	
	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	HOT-MIX ASPHALT SURFACE, MIX "D", N70	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (BINDER IL-19MM)	CLASS D PATCHES (BINDER IL-19MM)
AC/PG:	SBS/SBR PG 76-28/22	PG 64-22	PG 64-22/58-22	PG 64-22/58-22
DESIGN AIR VOIDS	4.0% @ 50 GYR	4.0% @ 70 GYR	4% @ 70 GYR	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIX QUANTITIES IS 112 LBS/SQYD/IN WHEN RAP EXCEEDS 20%. THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

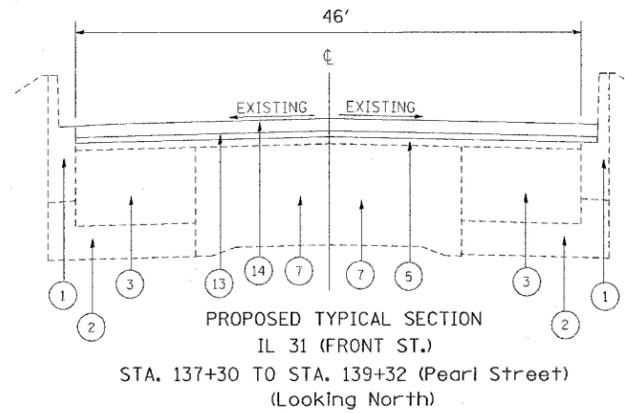
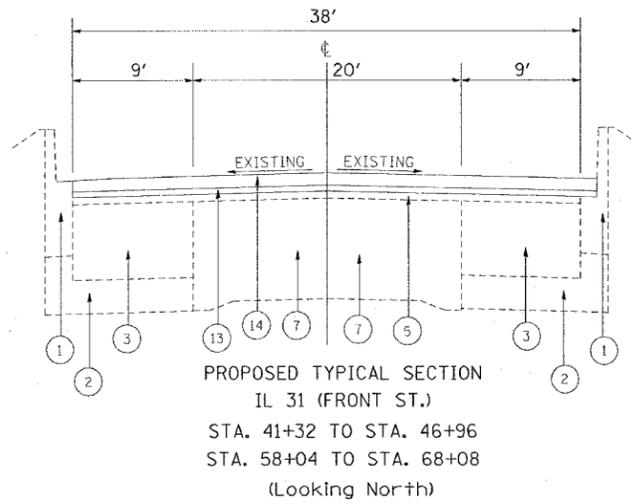
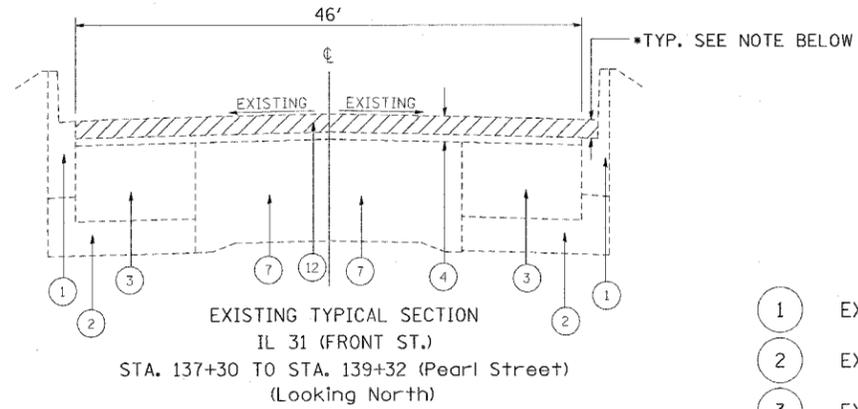
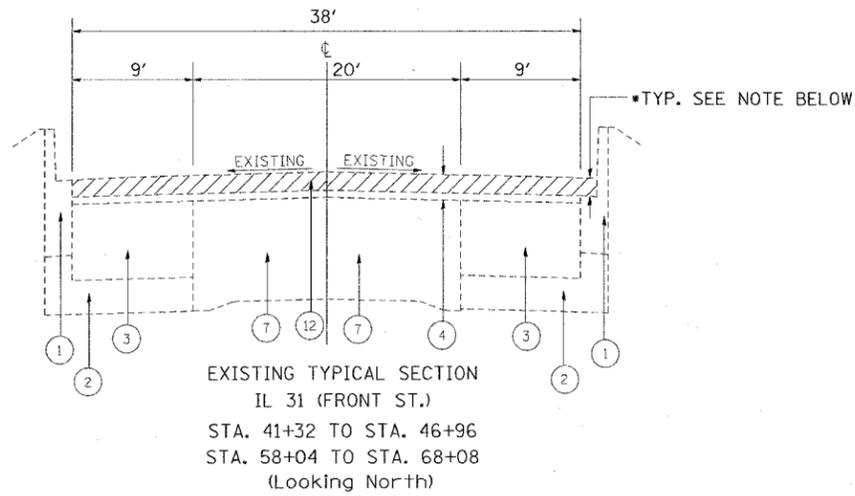
\* GUTTER FLAG HAS BEEN OVERLAYED AT SOME LOCATIONS THROUGHOUT JOB LIMITS. REMOVAL OF THE HOT-MIX ASPHALT OVERLAY ON THE GUTTER SHALL BE PAID FOR UNDER "HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4". LOCATIONS TO BE DETERMINED BY ENGINEER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
IL 31 (FRONT ST.)  
EXISTING AND PROPOSED  
TYPICAL CROSS SECTIONS

SCALE: N.T.S. DRAWN BY  
DATE CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	5
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



**LEGEND**

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

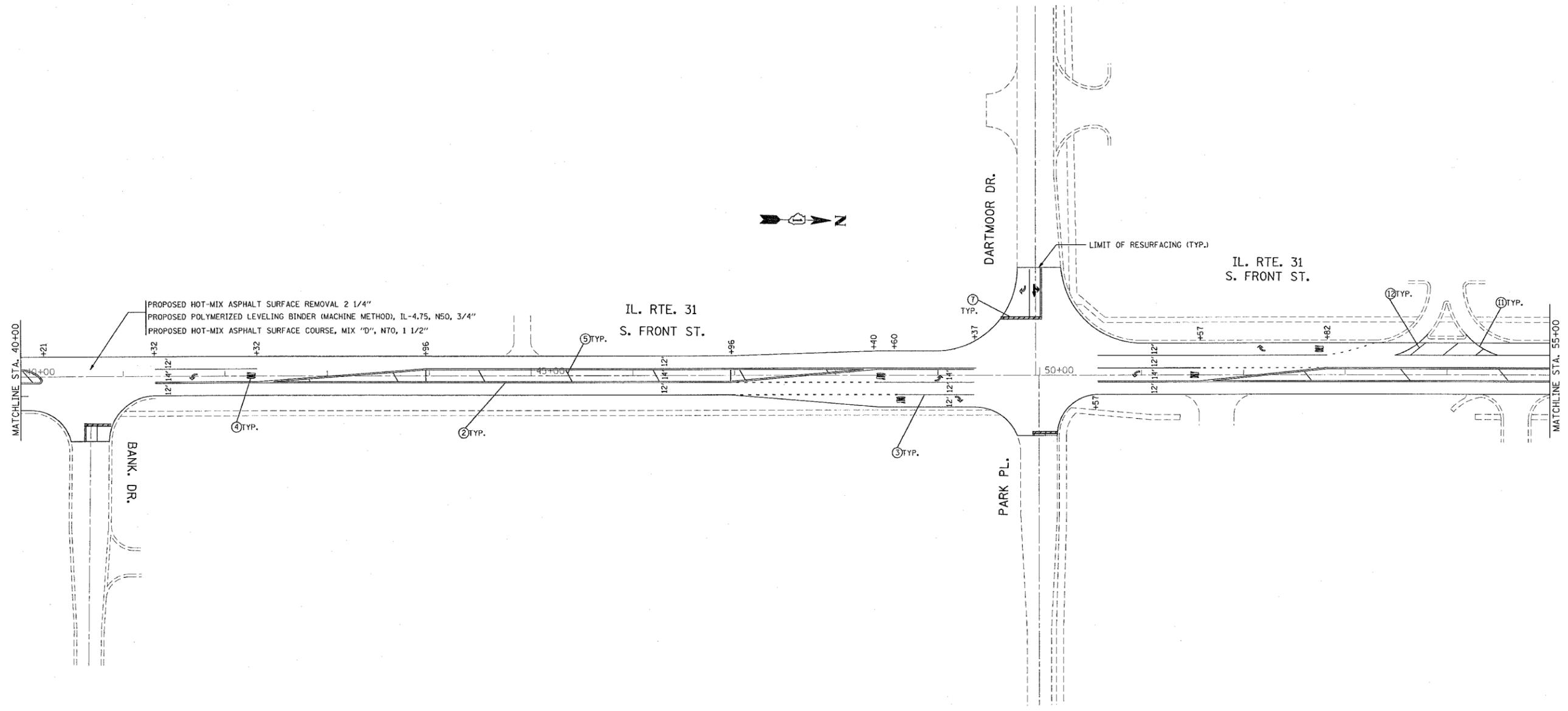
IL 31 (FRONT ST.)  
EXISTING AND PROPOSED  
TYPICAL CROSS SECTIONS

SCALE: N.T.S. DRAWN BY  
DATE CHECKED BY





F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	8
STA. 15+00		TO STA. 30+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



- PAVEMENT MARKING LEGEND**
- ① 4" LANE LINE- WHITE SKIP DASH
  - ② 4" DOUBLE YELLOW SOLID LINE
  - ③ 5" TURN LANE-WHITE
  - ④ LETTERS & SYMBOLS-WHITE
  - ⑤ 12" DIAGONALS-YELLOW @ 45°
  - ⑥ 6" CROSS WALK-WHITE
  - ⑦ 24" STOP BAR-WHITE
  - ⑧ 6" TURN LANE DOTTED EXTENSIONS
  - ⑨ 12" SCHOOL CROSSING
  - ⑩ 4" WHITE LINE
  - ⑪ 8" WHITE ISLAND
  - ⑫ 12" DIAGONALS-WHITE @ 45°

**NOTES**

LIMIT OF RESURFACING ON SIDE-STREETS THROUGH OUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.

ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13).

ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL." (TC-11).

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ILLINOIS DEPARTMENT OF TRANSPORTATION

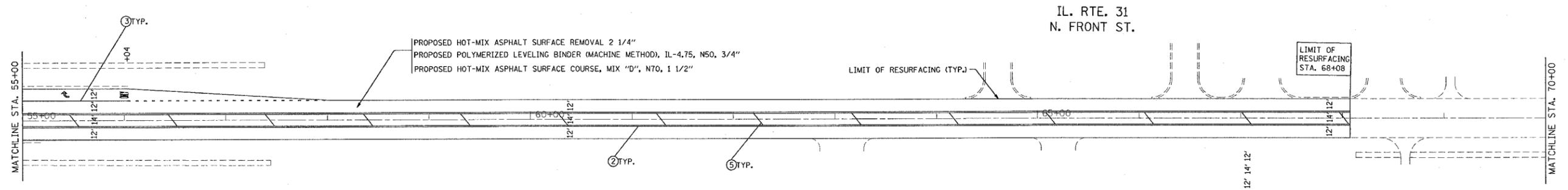
IL RTE. 31 (FRONT ST.)  
MCCULLOM LAKE RD. TO BULL VALLEY RD.  
ROADWAY & PAVEMENT MARKING PLAN

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE \_\_\_\_\_

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	9
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



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ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE. 31 (FRONT ST.)  
McCULLOM LAKE RD. TO BULL VALLEY RD.  
ROADWAY & PAVEMENT MARKING PLAN

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_

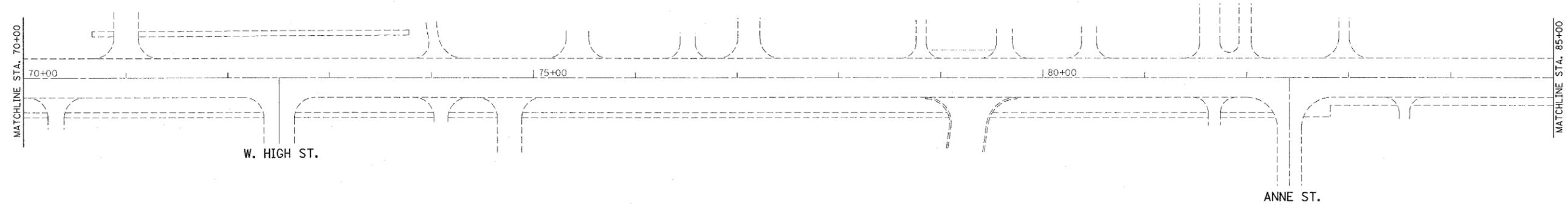
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336	112 RS-5	MCHENRY	34	10
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



IL. RTE. 31  
N. FRONT ST.



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ILLINOIS DEPARTMENT OF TRANSPORTATION

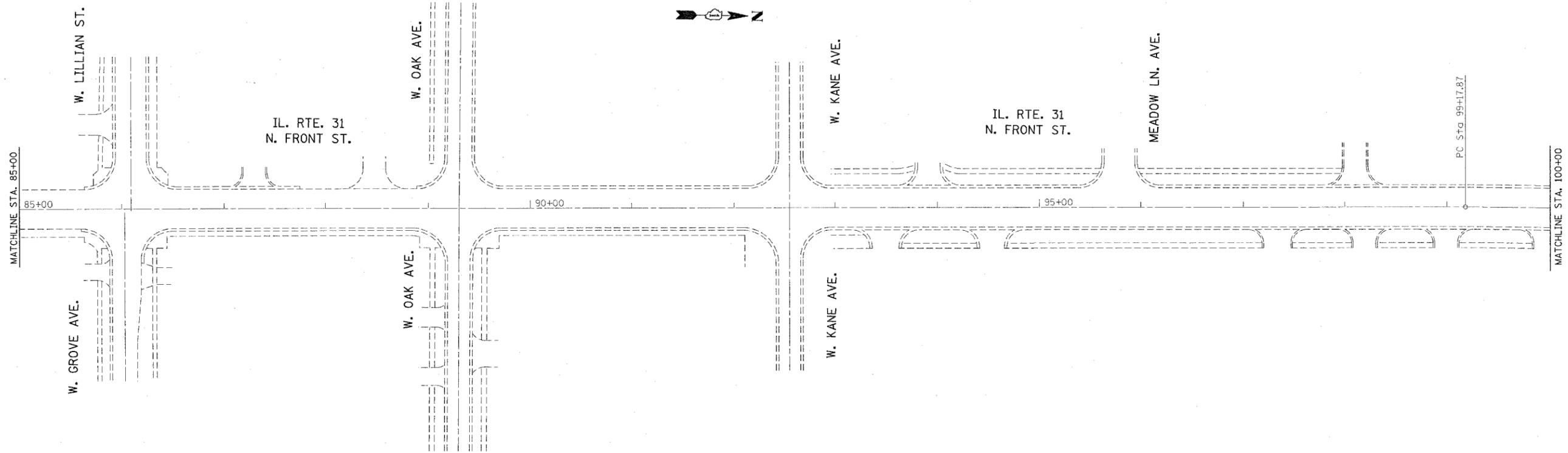
IL RTE. 31 (FRONT ST.)  
MCCULLOM LAKE RD. TO BULL VALLEY RD.  
ROADWAY & PAVEMENT MARKING PLAN

SCALE: VERT.  
DATE:           HORIZ.

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	11
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



- PAVEMENT MARKING LEGEND**
- ① 4" LANE LINE- WHITE SKIP DASH
  - ② 4" DOUBLE YELLOW SOLID LINE
  - ③ 6" TURN LANE-WHITE
  - ④ LETTERS & SYMBOLS-WHITE
  - ⑤ 12" DIAGONALS-YELLOW @ 45°
  - ⑥ 6" CROSS WALK-WHITE
  - ⑦ 24" STOP BAR-WHITE
  - ⑧ 6" TURN LANE DOTTED EXTENSIONS
  - ⑨ 12" SCHOOL CROSSING
  - ⑩ 4" WHITE LINE
  - ⑪ 8" WHITE ISLAND
  - ⑫ 12" DIAGONALS-WHITE @ 45°

**NOTES**  
 LIMIT OF RESURFACING ON SIDE-STREETS THROUGH OUT THE IMPROVEMENT SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.

ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13).

ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL." (TC-11).

REVISIONS	
NAME	DATE

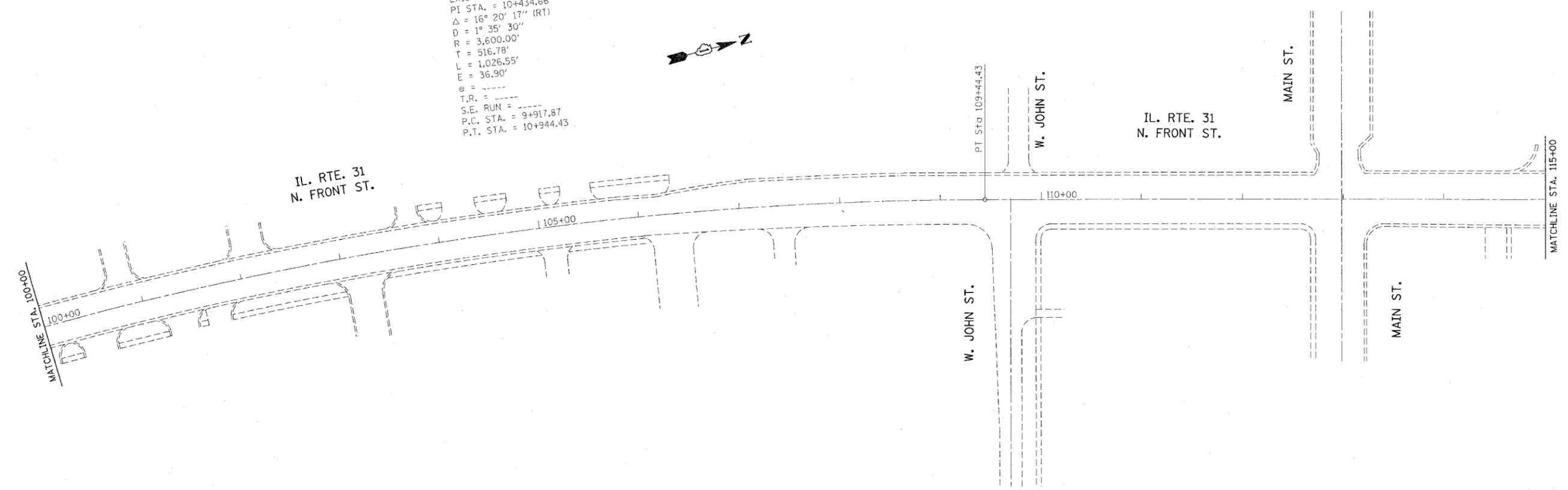
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 IL RTE. 31 (FRONT ST.)  
 McCULLOM LAKE RD. TO BULL VALLEY RD.  
 ROADWAY & PAVEMENT MARKING PLAN

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 3/6/2007  
 FILE NAME = c:\p\projects\142806\plan\chp.eadgn  
 PLOT SCALE = 80.0000' / 1" IN.  
 USER NAME = ABREUHI

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	12
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

EXIST. CURVE E..IL31-4  
 PI STA. = 10+434.66  
 $\Delta = 16^\circ 20' 17''$  (RT)  
 $D = 1^\circ 35' 30''$   
 $R = 3,600.00'$   
 $T = 516.78'$   
 $L = 1,026.55'$   
 $E = 36.90'$   
 $e = \dots$   
 $T.R. = \dots$   
 $S.E. RUN = \dots$   
 $P.C. STA. = 9+917.87$   
 $P.T. STA. = 10+944.43$



- PAVEMENT MARKING LEGEND**
- ① 4" LANE LINE- WHITE SKIP DASH
  - ② 4" DOUBLE YELLOW SOLID LINE
  - ③ 6" TURN LANE-WHITE
  - ④ LETTERS & SYMBOLS-WHITE
  - ⑤ 12" DIAGONALS-YELLOW @ 45°
  - ⑥ 6" CROSS WALK-WHITE
  - ⑦ 24" STOP BAR-WHITE
  - ⑧ 6" TURN LANE DOTTED EXTENSIONS
  - ⑨ 12" SCHOOL CROSSING
  - ⑩ 4" WHITE LINE
  - ⑪ 8" WHITE ISLAND
  - ⑫ 12" DIAGONALS-WHITE @ 45°

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

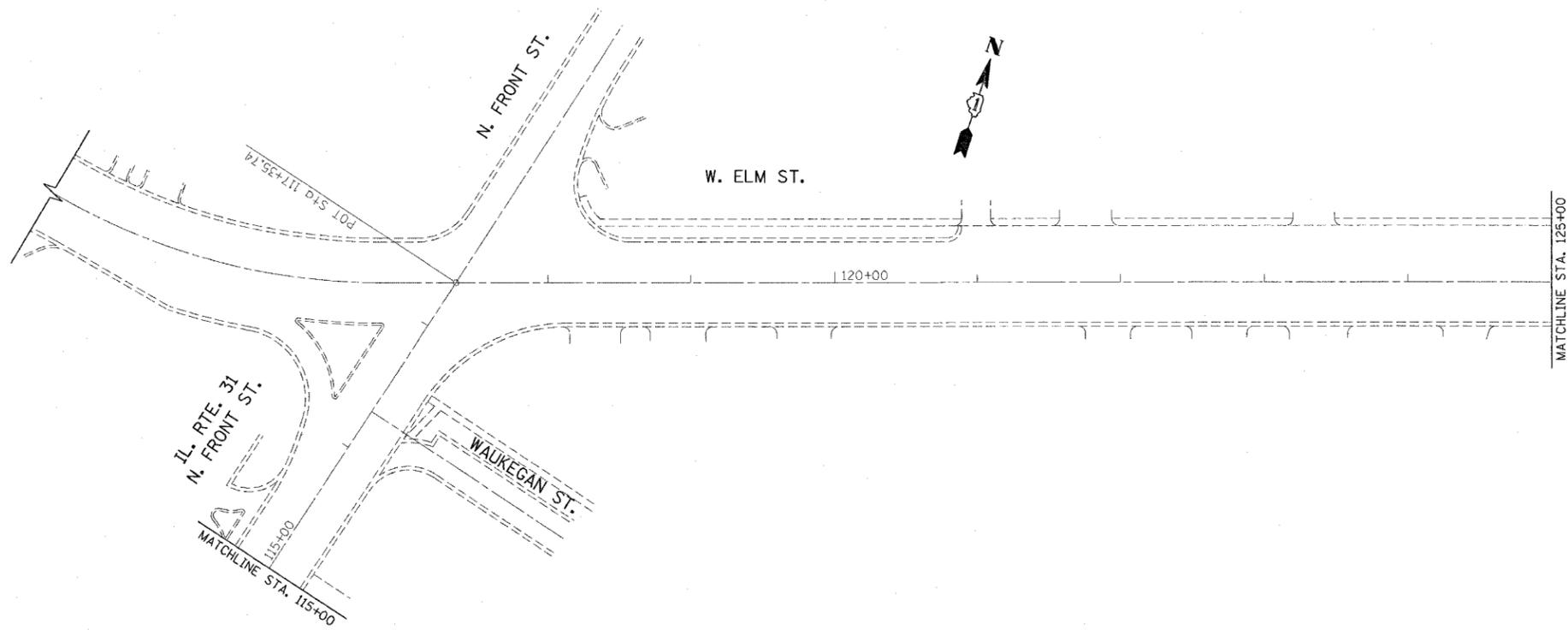
IL RTE. 31 (FRONT ST.)  
 McCULLOM LAKE RD. TO BULL VALLEY RD.  
 ROADWAY & PAVEMENT MARKING PLAN

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 3/6/2007  
 FILE NAME = c:\pms\mch\112\60b77\112\60b77.dwg  
 USER NAME = ABREUHA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	13
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**PAVEMENT MARKING LEGEND**

- ① 4" LANE LINE-WHITE SKIP DASH
- ② 4" DOUBLE YELLOW SOLID LINE
- ③ 6" TURN LANE-WHITE
- ④ LETTERS & SYMBOLS-WHITE
- ⑤ 12" DIAGONALS-YELLOW @ 45°
- ⑥ 6" CROSS WALK-WHITE
- ⑦ 24" STOP BAR-WHITE
- ⑧ 6" TURN LANE DOTTED EXTENSIONS
- ⑨ 12" SCHOOL CROSSING
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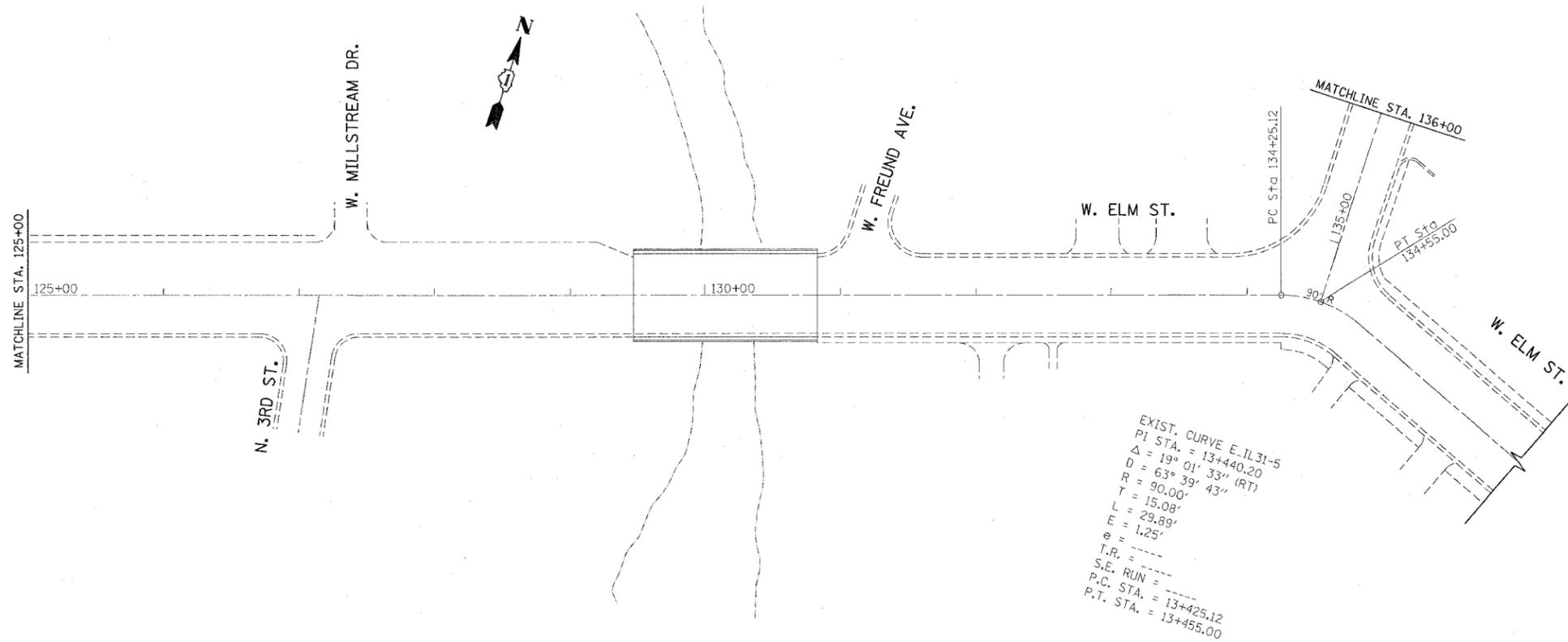
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 IL RTE. 31 (FRONT ST.)  
 McCULLOM LAKE RD. TO BULL VALLEY RD.  
 ROADWAY & PAVEMENT MARKING PLAN

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 3/6/2007  
 FILE NAME = c:\p\projects\112RS5\112RS5.dgn  
 PLOT SCALE = 600000 / 1" IN.  
 USER NAME = HIREURH

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	14
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



- PAVEMENT MARKING LEGEND**
- ① 4" LANE LINE- WHITE SKIP DASH
  - ② 4" DOUBLE YELLOW SOLID LINE
  - ③ 6" TURN LANE-WHITE
  - ④ LETTERS & SYMBOLS-WHITE
  - ⑤ 12" DIAGONALS-YELLOW @ 45°
  - ⑥ 6" CROSS WALK-WHITE
  - ⑦ 24" STOP BAR-WHITE
  - ⑧ 6" TURN LANE DOTTED EXTENSIONS
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

IL RTE. 31 (FRONT ST.)  
 McCULLOM LAKE RD. TO BULL VALLEY RD.  
 ROADWAY & PAVEMENT MARKING PLAN

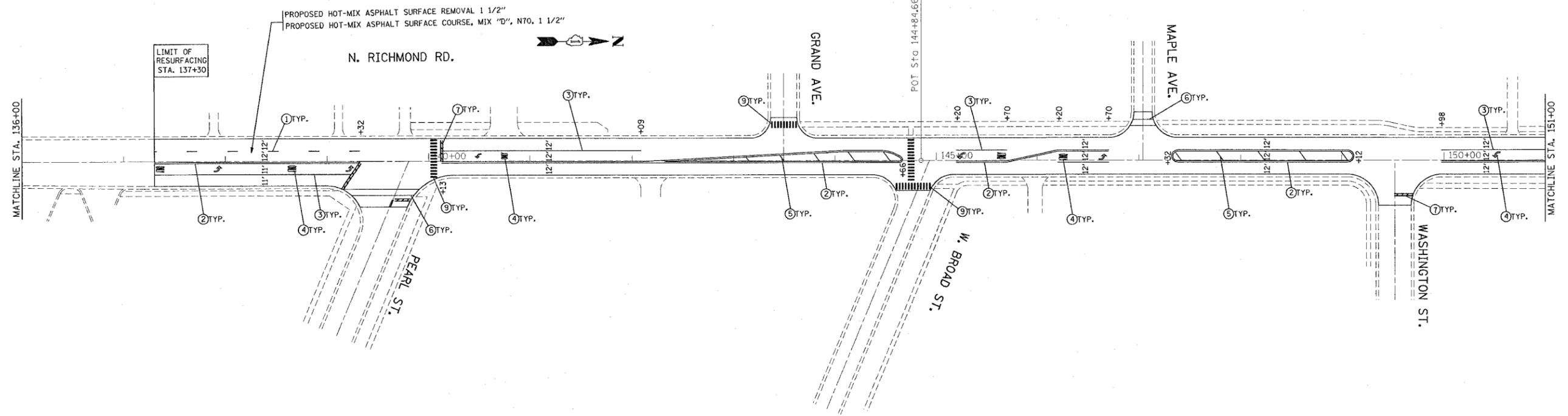
SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 3/16/2007  
 FILE NAME = c:\pav\mch\112rs5\112rs5.dwg  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = ABRELIAN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	15
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



PAVEMENT MARKING LEGEND

- ① 4" LANE LINE- WHITE SKIP DASH
- ② 4" DOUBLE YELLOW SOLID LINE
- ③ 6" TURN LANE-WHITE
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- ⑦ 24" STOP BAR-WHITE
- ⑧ 6" TURN LANE DOTTED EXTENSIONS
- ⑨ 12" SCHOOL CROSSING
- ⑩ 4" WHITE LINE
- ⑪ 8" WHITE ISLAND
- ⑫ 12" DIAGONALS-WHITE @ 45°

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REVISIONS	
NAME	DATE

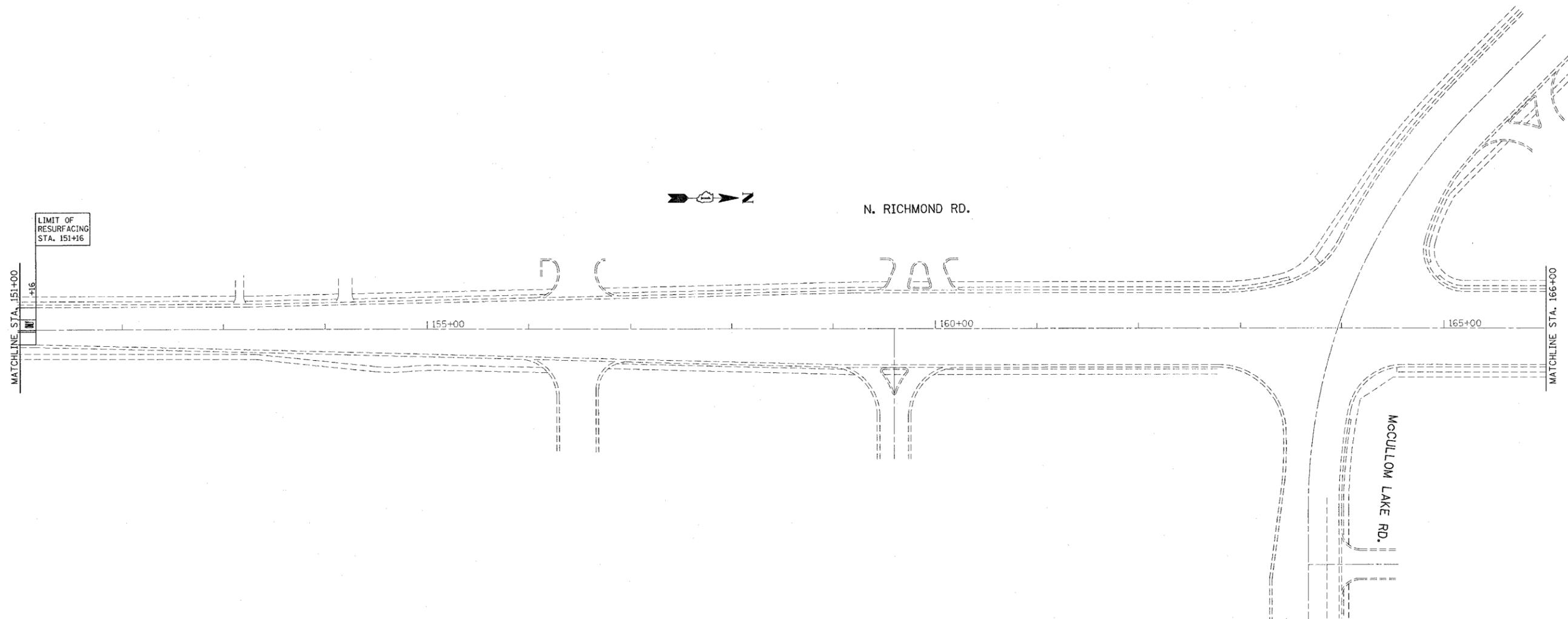
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 IL RTE. 31 (FRONT ST.)  
 MCCULLOM LAKE RD. TO BULL VALLEY RD.  
 ROADWAY & PAVEMENT MARKING PLAN

SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY

PLOT DATE = 3/6/2007  
 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = ABRELIAN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	16
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**PAVEMENT MARKING LEGEND**

- ① 4" LANE LINE- WHITE SKIP DASH
- ② 4" DOUBLE YELLOW SOLID LINE
- ③ 6" TURN LANE-WHITE
- ④ LETTERS & SYMBOLS-WHITE
- ⑤ 12" DIAGONALS-YELLOW @ 45°
- ⑥ 6" CROSS WALK-WHITE
- ⑦ 24" STOP BAR-WHITE
- ⑧ 6" TURN LANE DOTTED EXTENSIONS
- ⑨ 12" SCHOOL CROSSING
- ⑩ 4" WHITE LINE
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- ⑫ 12" DIAGONALS-WHITE @ 45°

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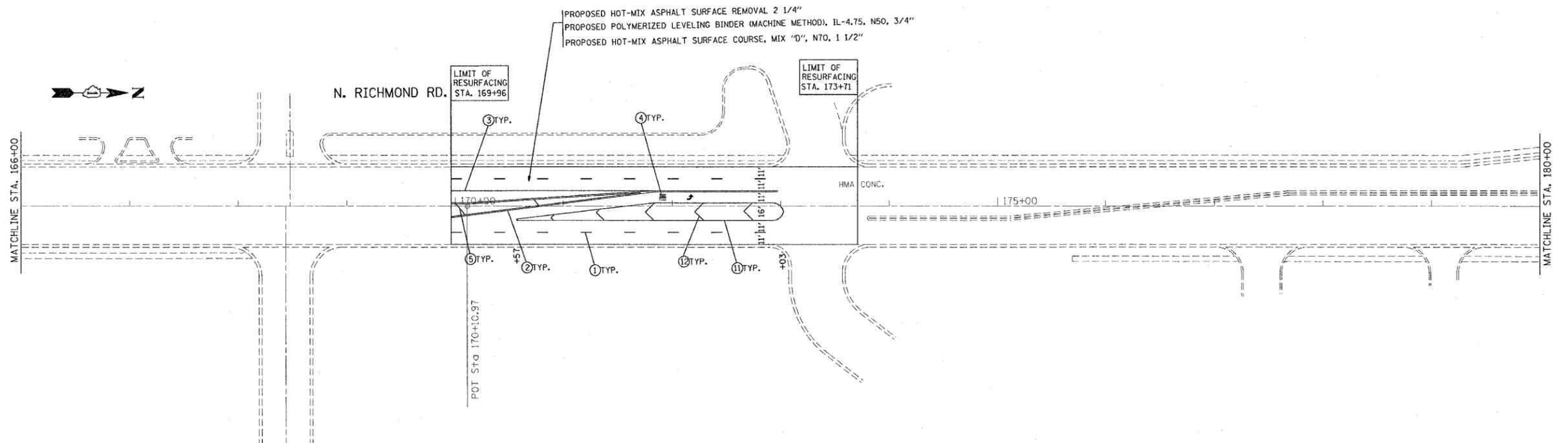
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 IL RTE. 31 (FRONT ST.)  
 McCULLOM LAKE RD. TO BULL VALLEY RD.  
 ROADWAY & PAVEMENT MARKING PLAN

SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	17
STA. 30+00		TO STA. 45+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



- PAVEMENT MARKING LEGEND**
- ① 4" LANE LINE- WHITE SKIP DASH
  - ② 4" DOUBLE YELLOW SOLID LINE
  - ③ 6" TURN LANE-WHITE
  - ④ LETTERS & SYMBOLS-WHITE
  - ⑤ 12" DIAGONALS-YELLOW @ 45°
  - ⑥ 6" CROSS WALK-WHITE
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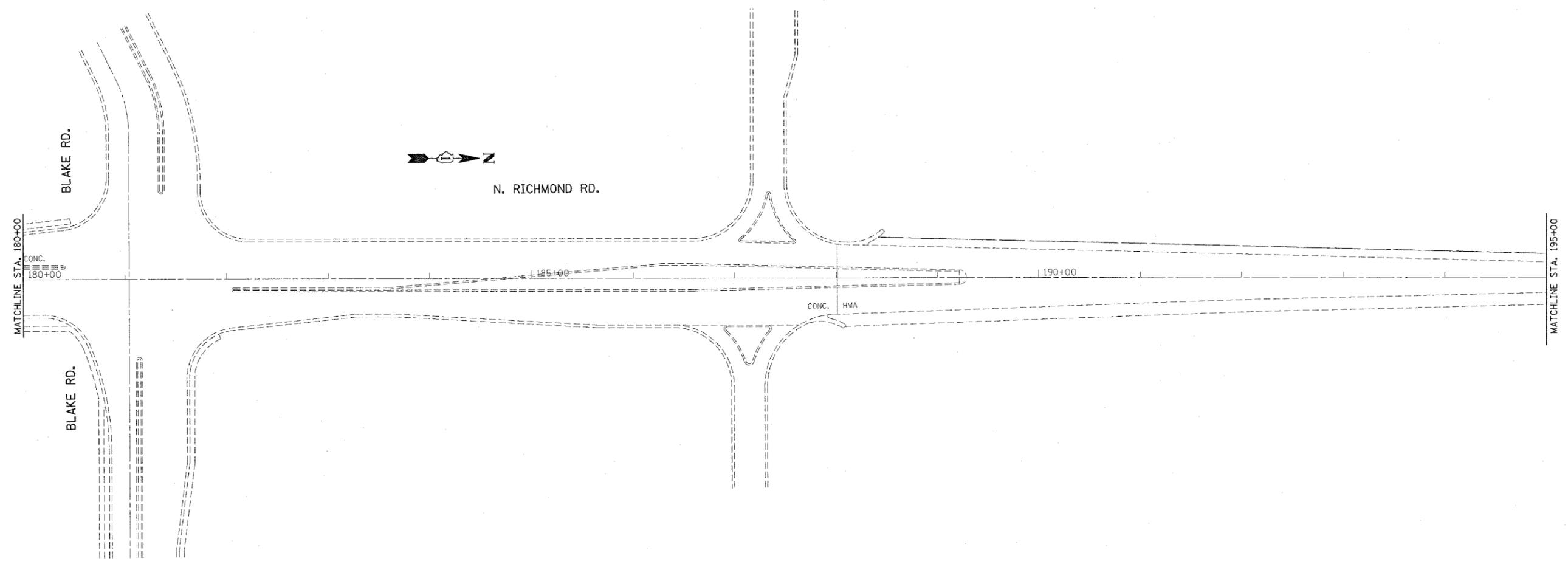
ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL," (TC-11).

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 IL RTE. 31 (FRONT ST.)  
 McCULLOM LAKE RD. TO BULL VALLEY RD.  
 ROADWAY & PAVEMENT MARKING PLAN  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 3/6/2007  
 FILE NAME = c:\p\proj\60b77\173\173.dwg  
 PLOT SCALE = 50.0000 / 1" = 100'-0"  
 USER NAME = HRELIH

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHENRY	34	18
STA. 30+00		TO STA.45+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



- PAVEMENT MARKING LEGEND**
- ① 4" LANE LINE- WHITE SKIP DASH
  - ② 4" DOUBLE YELLOW SOLID LINE
  - ③ 6" TURN LANE-WHITE
  - ④ LETTERS & SYMBOLS-WHITE
  - ⑤ 12" DIAGONALS-YELLOW @ 45°
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  - ⑦ 24" STOP BAR-WHITE
  - ⑧ 6" TURN LANE DOTTED EXTENSIONS
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  - ⑩ 4" WHITE LINE
  - ⑪ 8" WHITE ISLAND
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 IL RTE. 31 (FRONT ST.)  
 MCCULLOM LAKE RD. TO BULL VALLEY RD.  
 ROADWAY & PAVEMENT MARKING PLAN

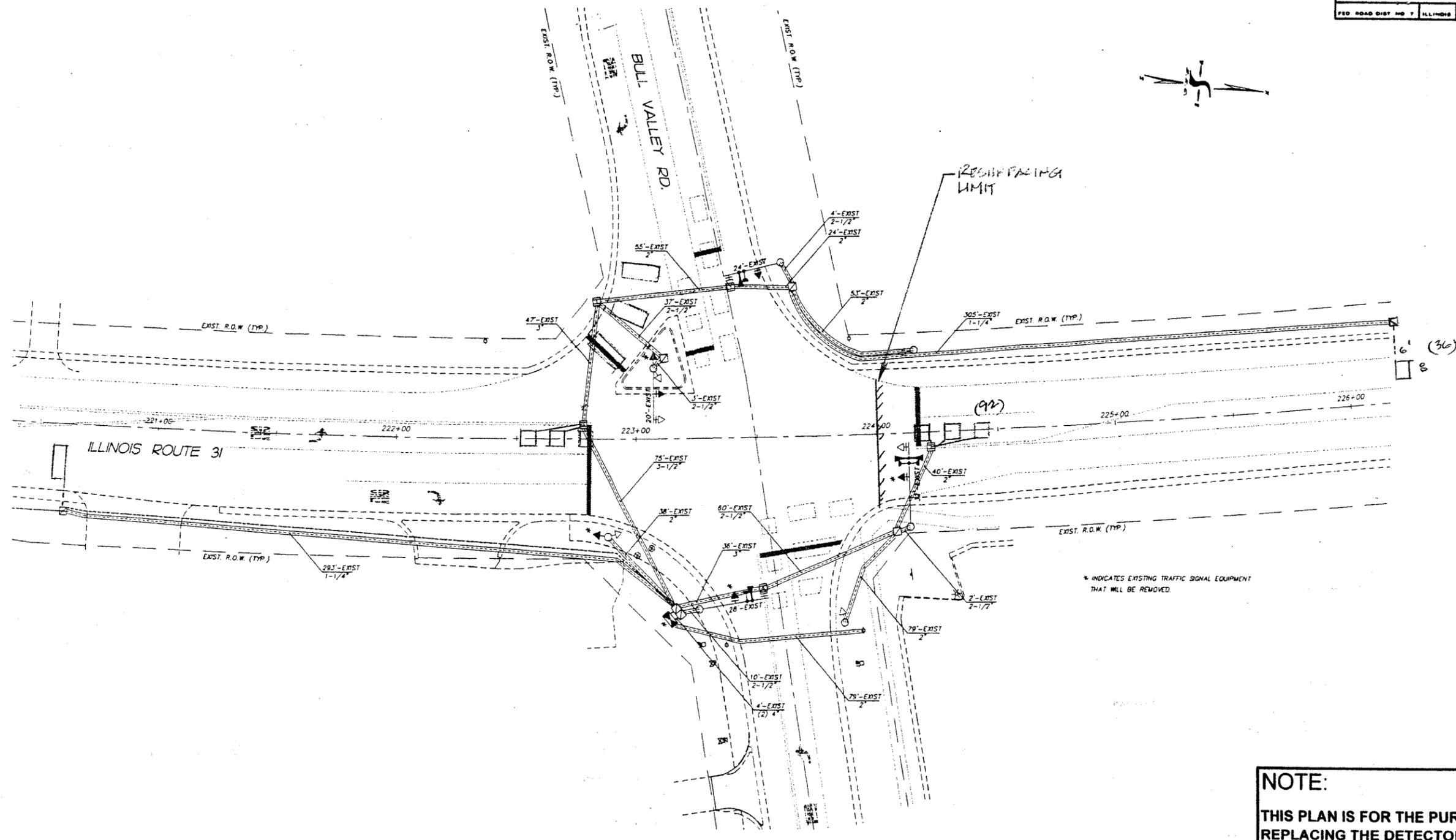
SCALE: VERT.  
 DATE:            HORIZ.

DRAWN BY  
 CHECKED BY

PLOT DATE = 3/6/2007  
 PLOT SCALE = 5000000 / 1" = 1 MI.  
 USER NAME = ABRELIAN

60377

FAP DIST	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112RS-5	McHENRY	34	19
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT



**NOTE:**  
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

**REPLACE ALL DETECTOR LOOPS AS SHOWN**  
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	128	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETECTOR LOOP REPLACEMENT**  
 ILLINOIS RTE 31 @ BULL VALLEY RD.

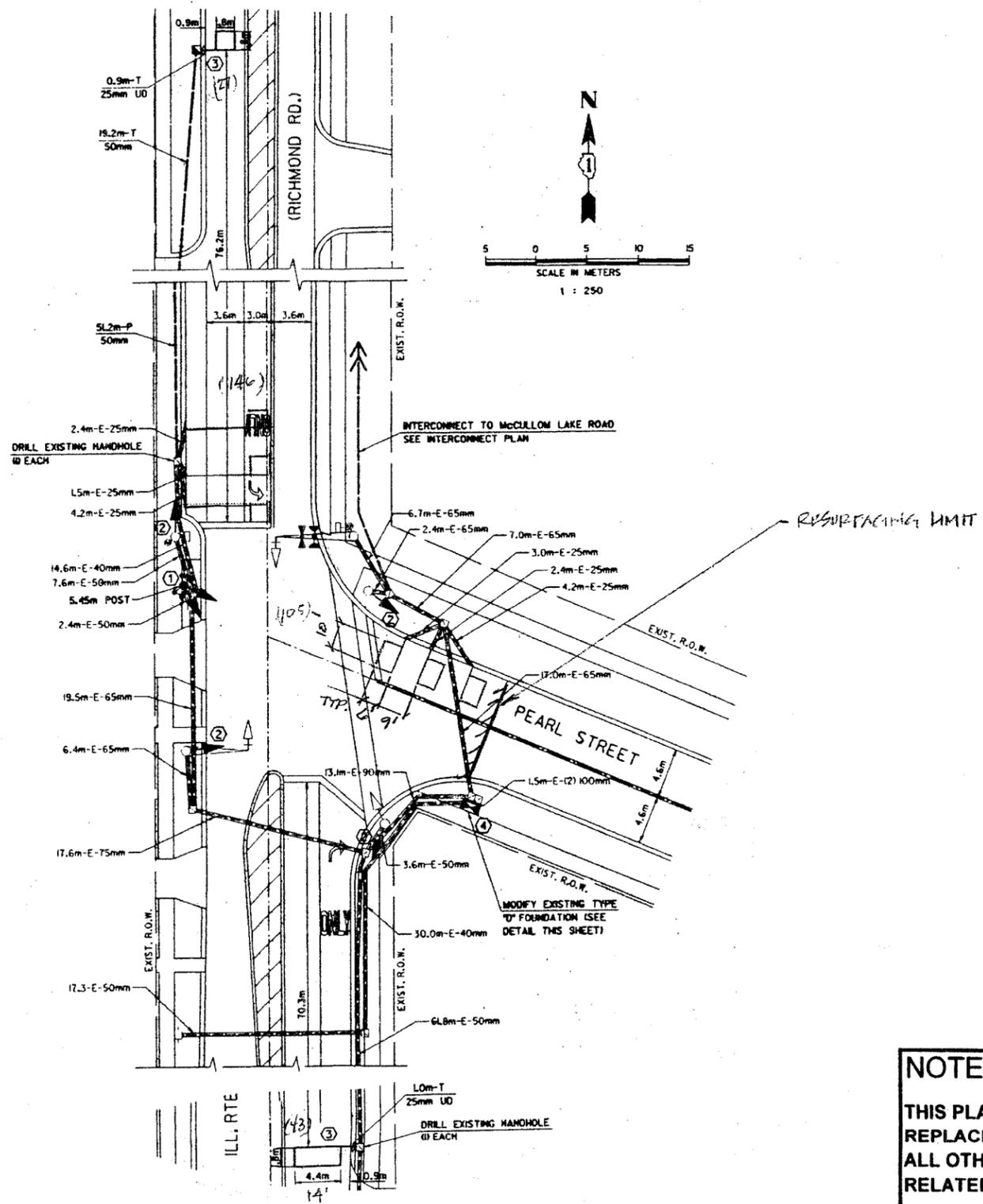
SCALE NONE  
 DATE DECEMBER 2006

DRAWN BY: JHE  
 DESIGNED BY: JHE  
 CHECKED BY: D.A.D.

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	McHenry	34	20
STA.	TO STA.			
FED. ROAD DIST NO. 7	ILLINOIS	FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE, STEEL		
MAST ARM ASSEMBLY AND POLE, ALUMINUM		
COMMON TRENCH		
UNIT DUCT		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN TRENCH OR PUSHED		
PEDESTRIAN PUSHBUTTON DETECTOR		
DETECTOR LOOP		
CAST IRON JUNCTION BOX		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
SIGNAL HEAD OPTICALLY PROGRAMMED		
CONDUIT SPLICE		
WOOD POLE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RAILROAD CONTROL CABINET		



**NOTE:**  
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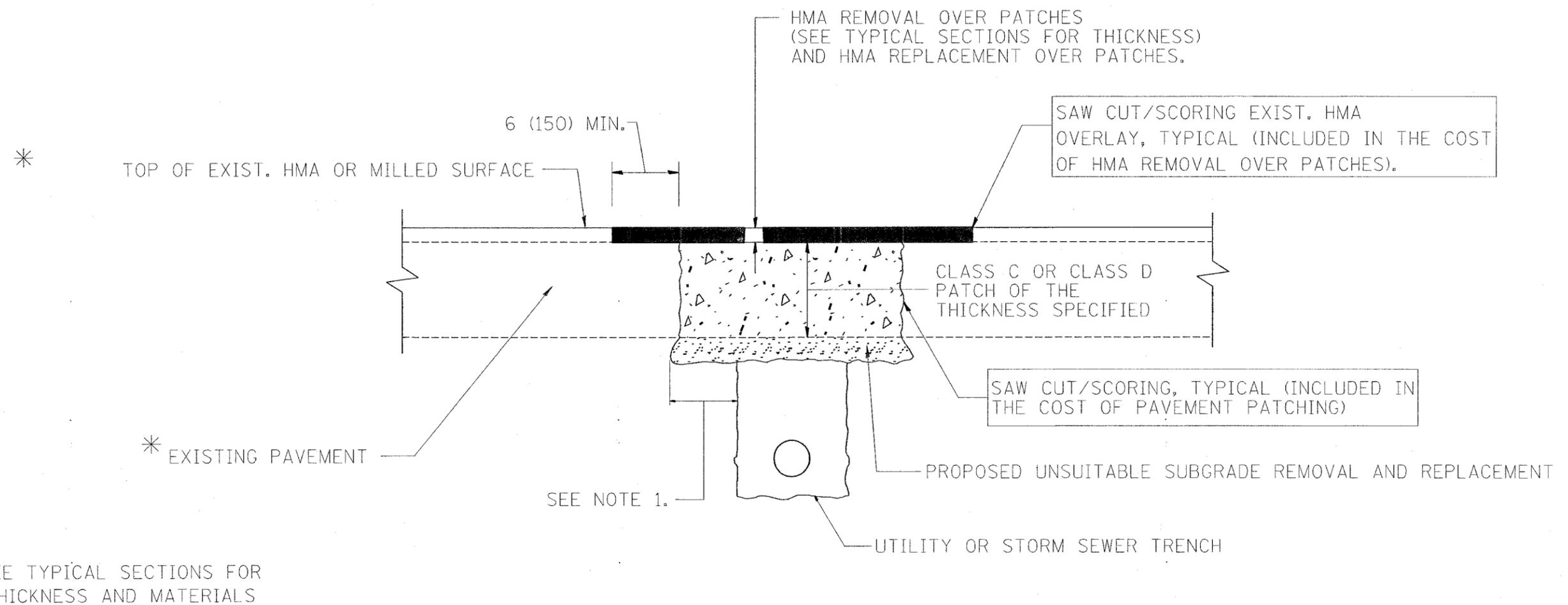
**REPLACE ALL DETECTOR LOOPS AS SHOWN**  
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	321	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETECTOR LOOP REPLACEMENT**  
 ILLINOIS ROUTE 31 @ PEARL STREET  
 SCALE: NONE  
 DATE: \_\_\_\_\_  
 DRAWN BY: JHE.  
 DESIGNED BY: JHE.  
 CHECKED BY: DAD.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MACOMB	34	21
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	10/11/06

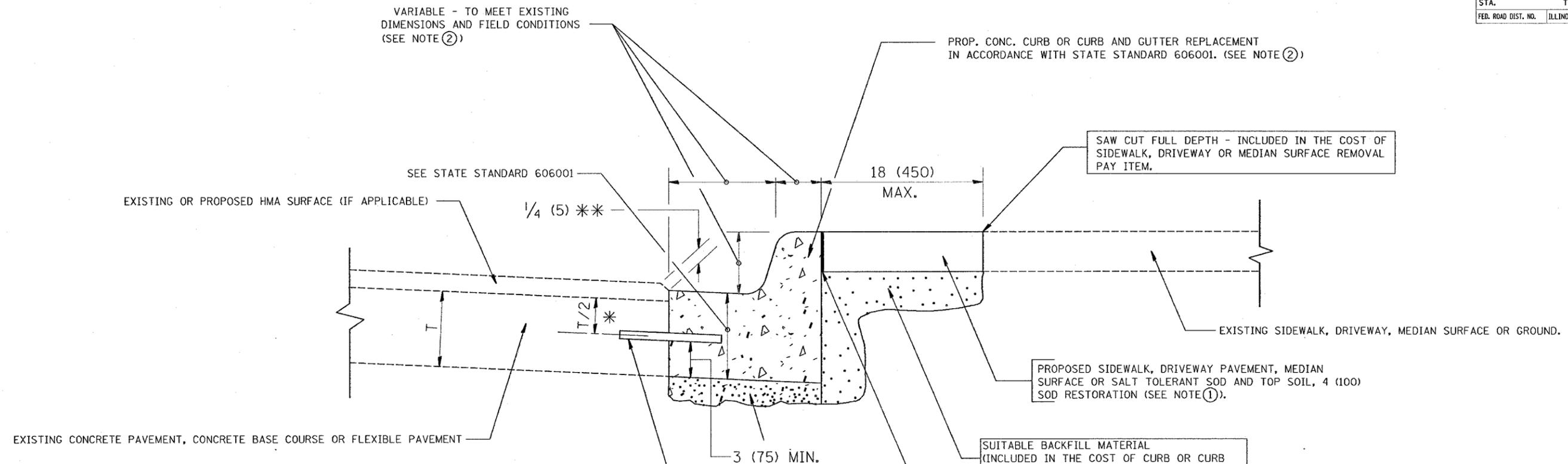
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT**

SCALE: VERT. DATE: 10/30/2006  
 HORIZ.

DRAWN BY  
 CHECKED BY  
 BD400-04 (BD-22)  
 REVISION DATE: 10/11/06

PLOT DATE = 10/30/2006  
 FILE NAME = W:\data\bd22.dgn  
 PLOT SCALE = 80.0000 / IN.  
 USER NAME = boro

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHEARNY	34	22
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



\* 3 (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.  
 \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
 SALT TOLERANT SOD AND TOP SOIL, 4 (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	10/11/06

ILLINOIS DEPARTMENT OF TRANSPORTATION

**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**

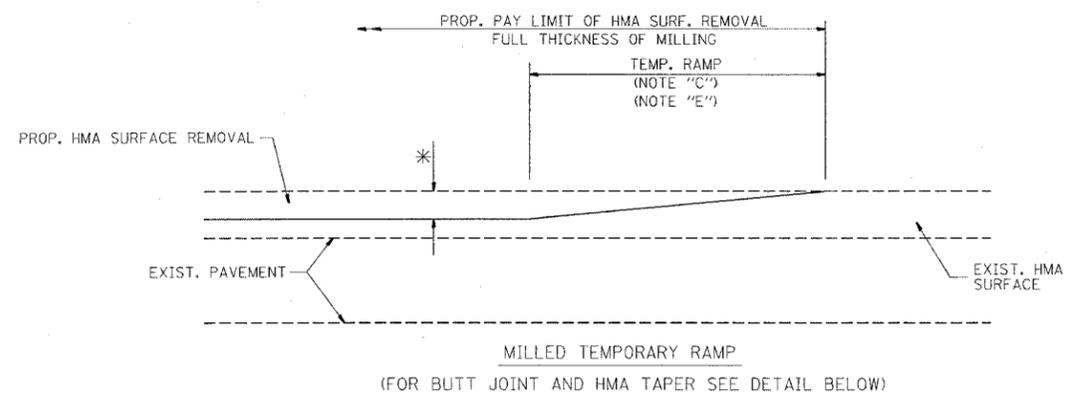
SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 10/30/2006

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

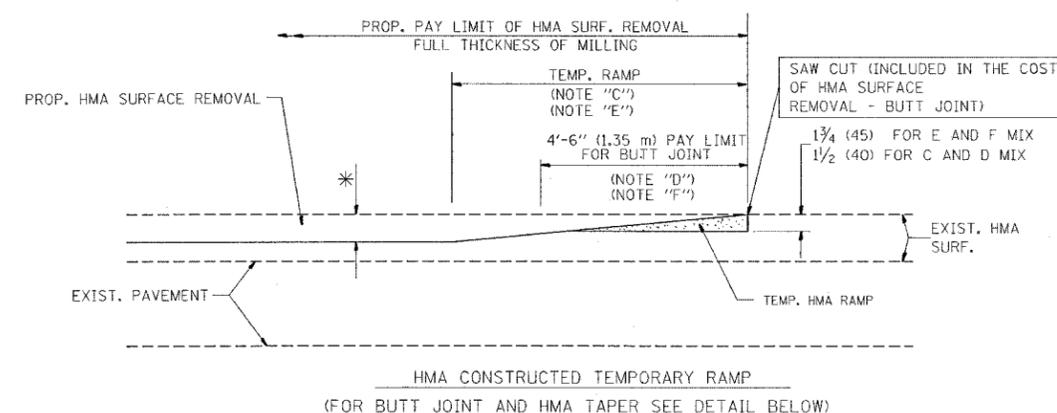
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

PLT DATE = 10/30/2006  
 FILE NAME = 10/30/2006\_10/30/2006.dgn  
 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = abraham

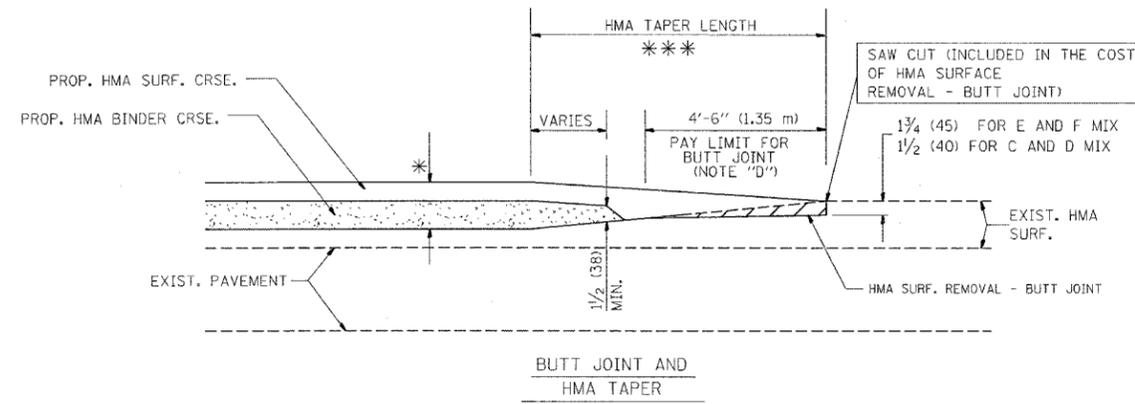
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
536	112 RS-5	MCHEMERY	39	23
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



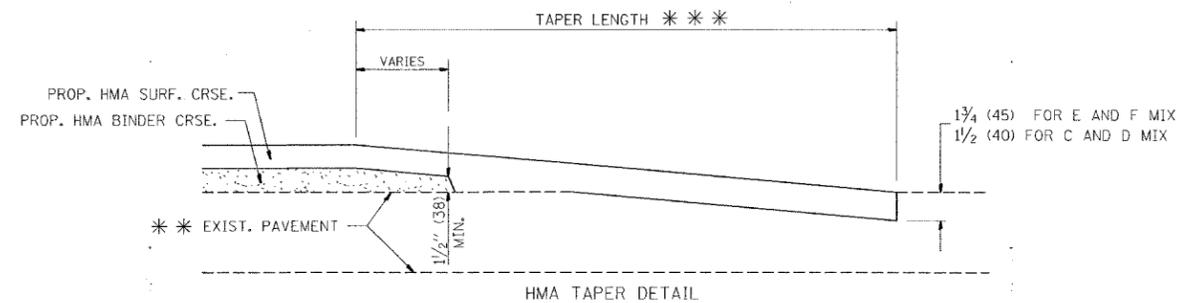
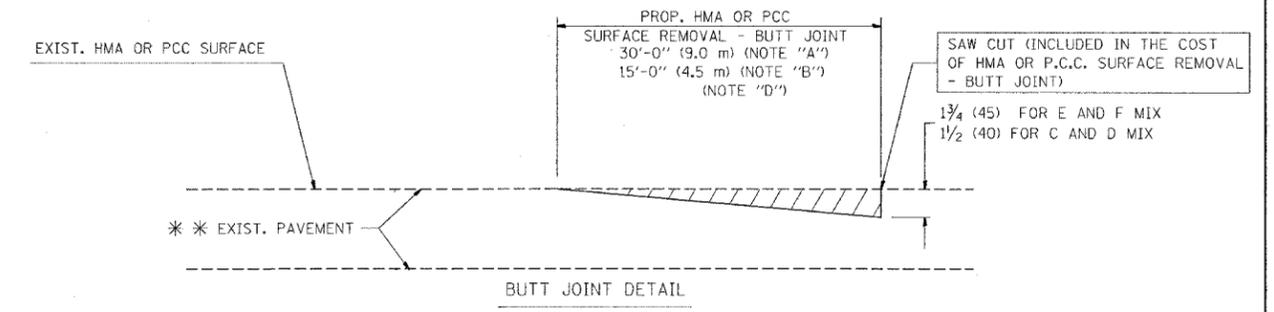
OPTION 1



OPTION 2  
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

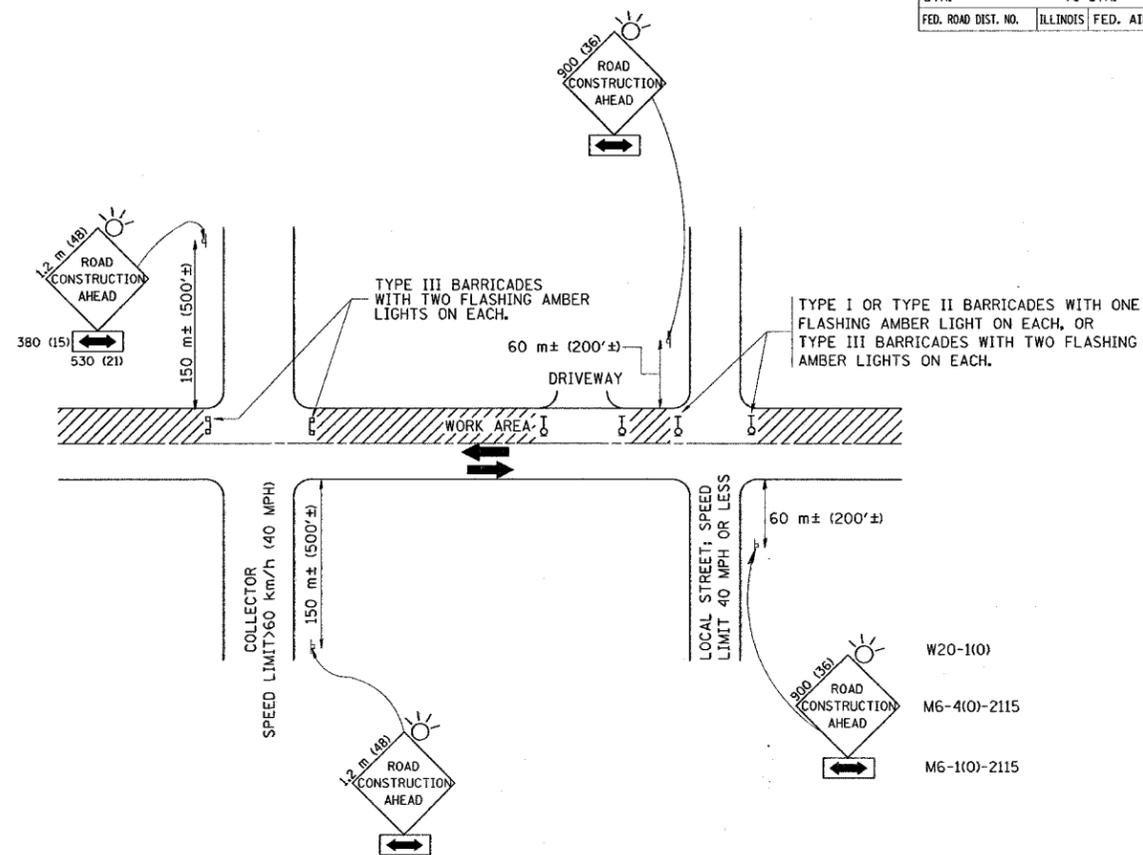
BASIS OF PAYMENT:  
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	10/11/06

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**BUTT JOINT AND HMA TAPER DETAILS**  
SCALE: VERT. \_\_\_\_\_  
          HORIZ. \_\_\_\_\_  
DATE: 10/30/2006  
DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_

PLOT DATE = 10/30/2006  
PLOT SCALE = 483989 / 1 IN.  
USER NAME = abraham

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHEERY	34	29
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

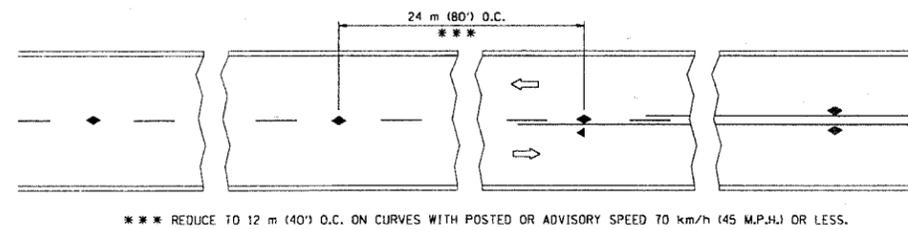
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL AND PROTECTION  
 FOR  
 SIDE ROADS, INTERSECTIONS, AND  
 DRIVEWAYS

SCALE:  
 DATE: 11/6/2006

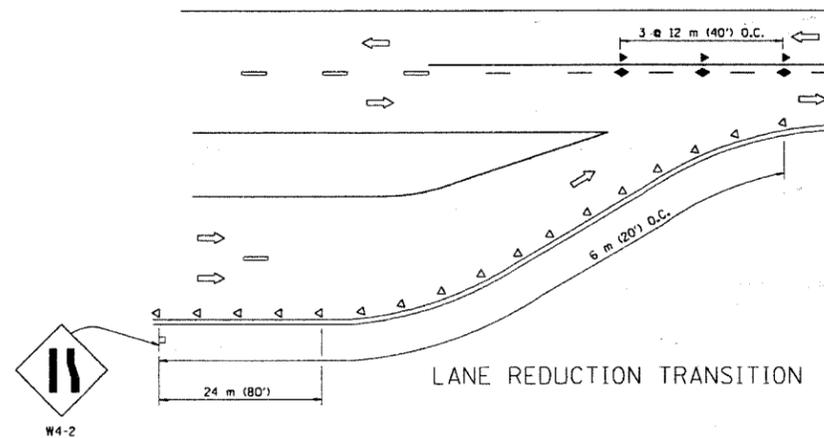
DRAWN BY  
 CHECKED BY

TC-10  
 REVISION DATE: 01/06/00

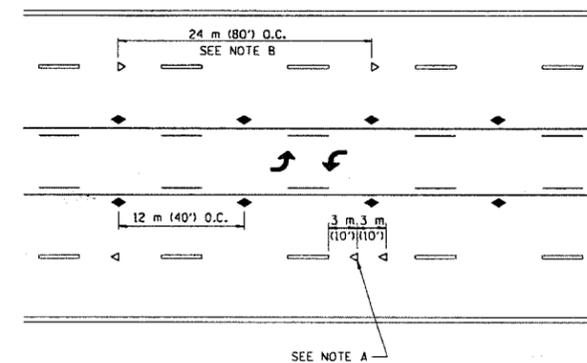
F.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHEMERY	39	25
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



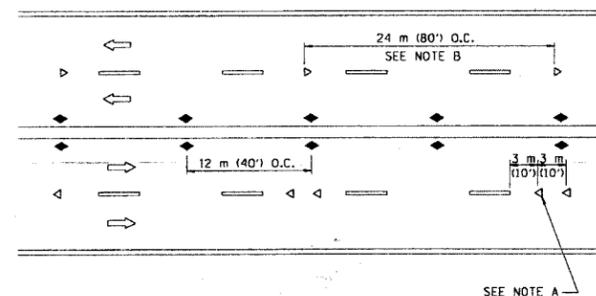
TWO-LANE/TWO-WAY



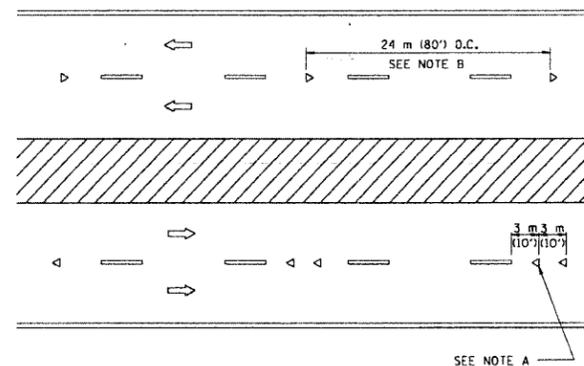
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

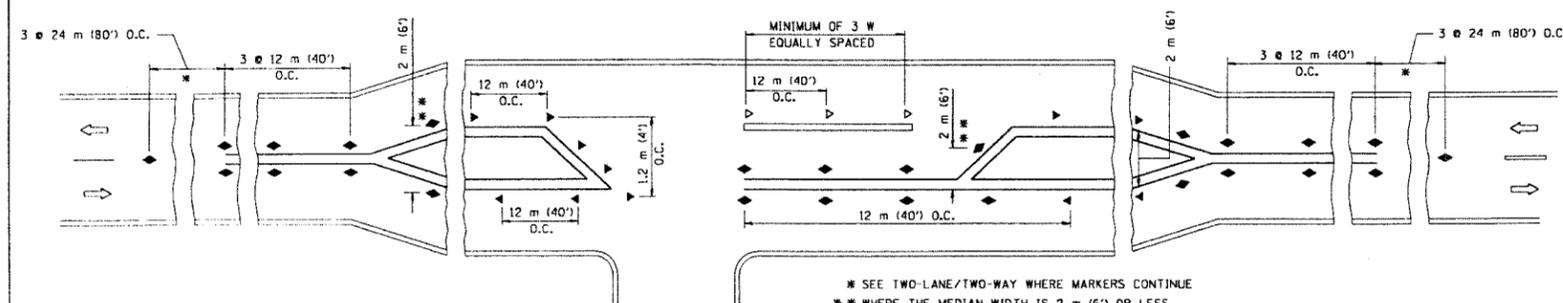
TYPICAL APPLICATIONS  
RAISED REFLECTIVE PAVEMENT MARKERS  
(SNOW-PLOW RESISTANT)

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

SCALE: NONE  
DATE: 10/18/2002

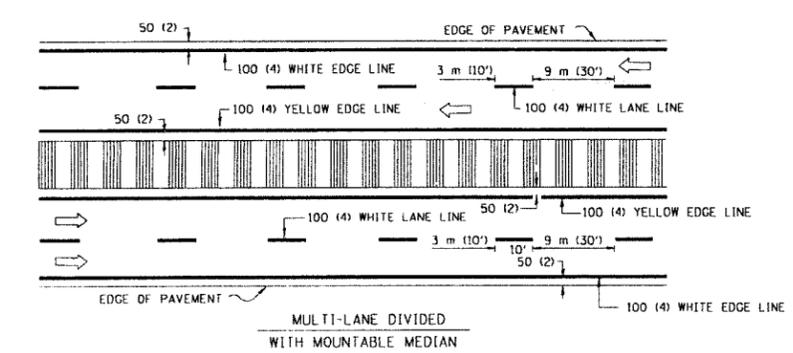
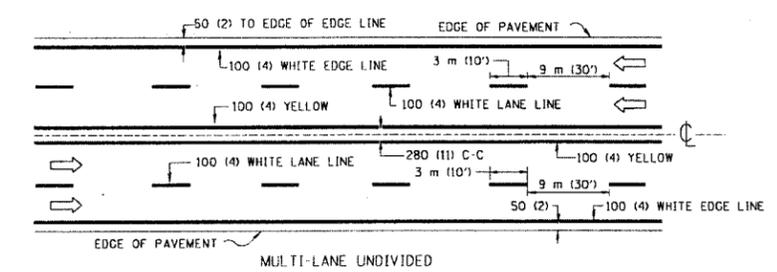
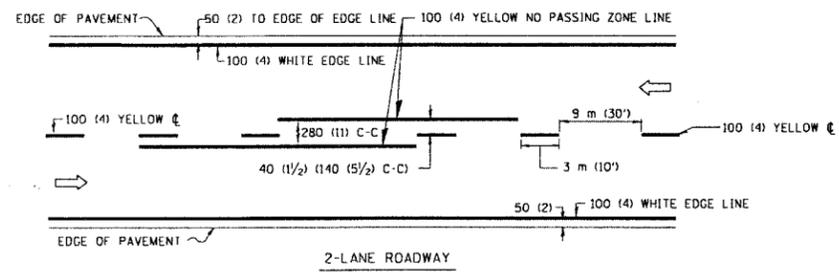
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CHECKED BY IC II

REVISION DATE: 01/06/00



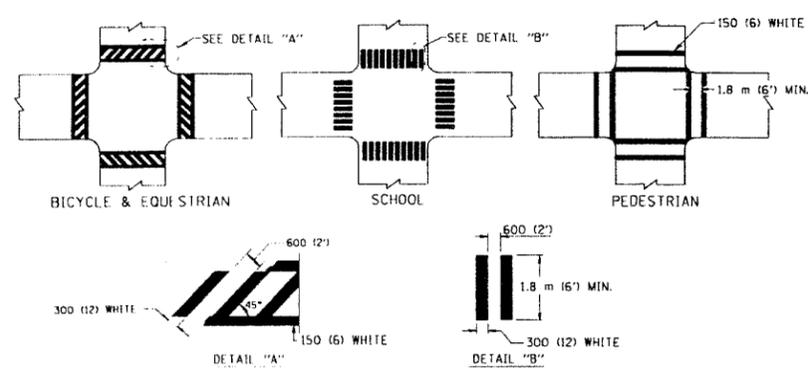
LEFT TURN

F. A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112 RS-5	MCHEMERY	34	26
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

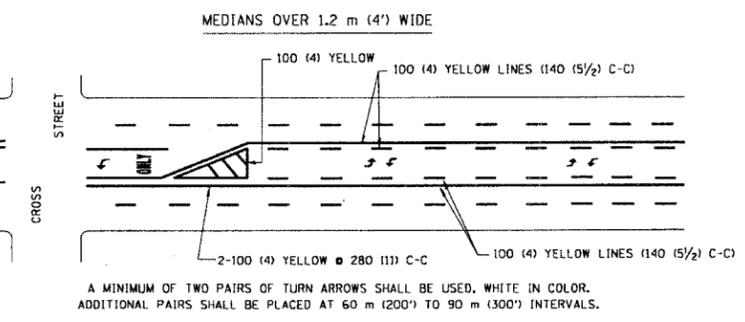
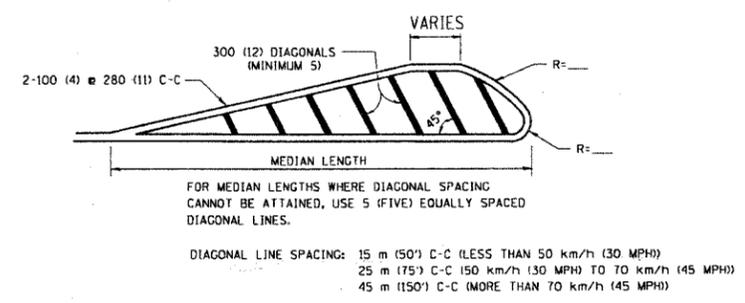
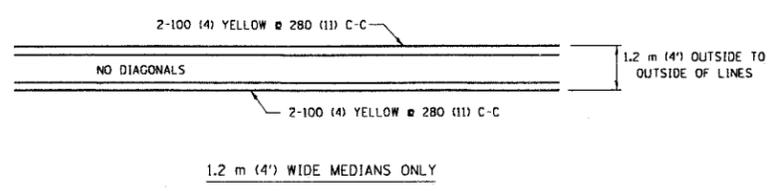


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

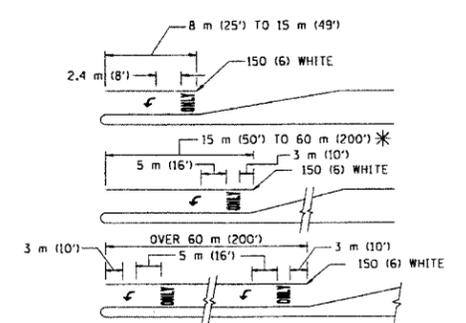
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



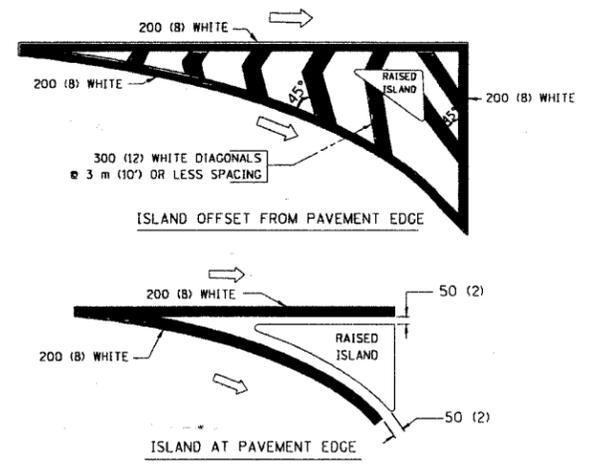
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.  
 \* AREA = 1.5 m<sup>2</sup> (15.6 SQ. FT.) ONLY AREA = 1.9 m<sup>2</sup> (20.8 SQ. FT.)  
 \* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 0.33m <sup>2</sup> (3.6 SQ. FT.) EACH "X": 5.0 m <sup>2</sup> (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

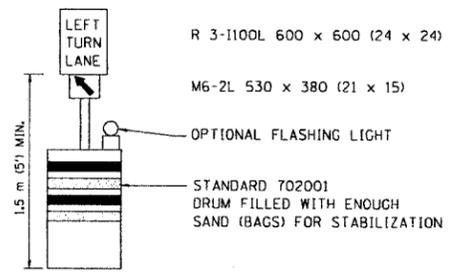
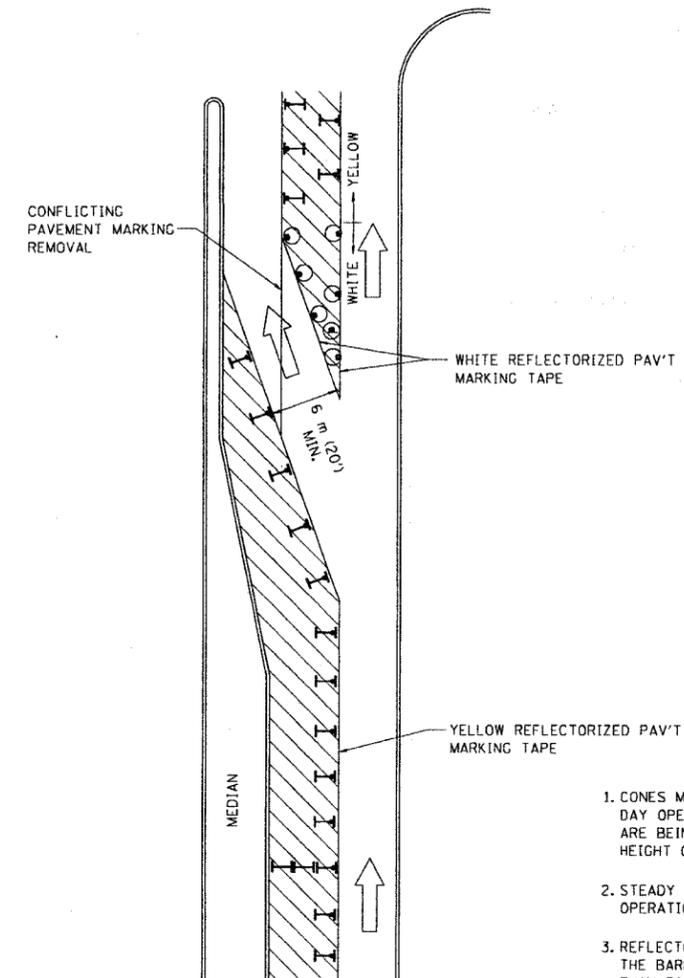
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DISTRICT ONE  
 TYPICAL PAVEMENT MARKINGS

REVISIONS		
NAME	DATE	
EVERS	03-19-90	
T. RAMMACHER	10-27-94	
ALEX HOUSEH	10-09-96	
ALEX HOUSEH	10-17-96	
T. RAMMACHER	01-06-00	

SCALE: NONE  
 DATE 10/18/2002  
 DRAWN BY CAOD  
 CHECKED BY  
 TC 13

REVISION DATE: 01/06/00

F. AP. SEC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	112 RS-5	MICHENRY	39	27
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL AND PROTECTION  
 AT TURN BAYS  
 (TO REMAIN OPEN TO TRAFFIC)

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

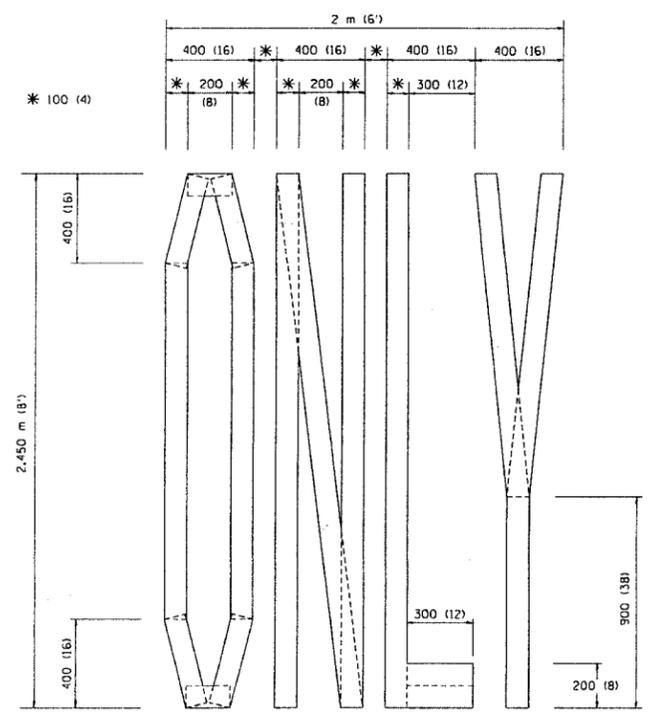
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 DATE: 10/18/2002

DRAWN BY  
 CHECKED BY: IHA  
 IC 14

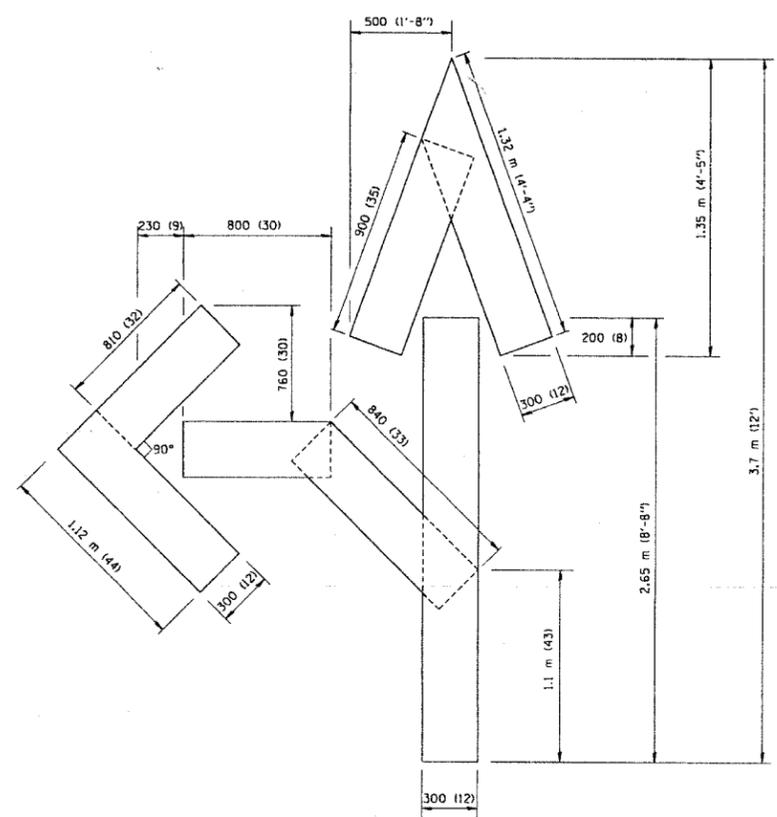
REVISION DATE: 01/06/00

DATE TIME  
 EXN SPEC  
 VIT04

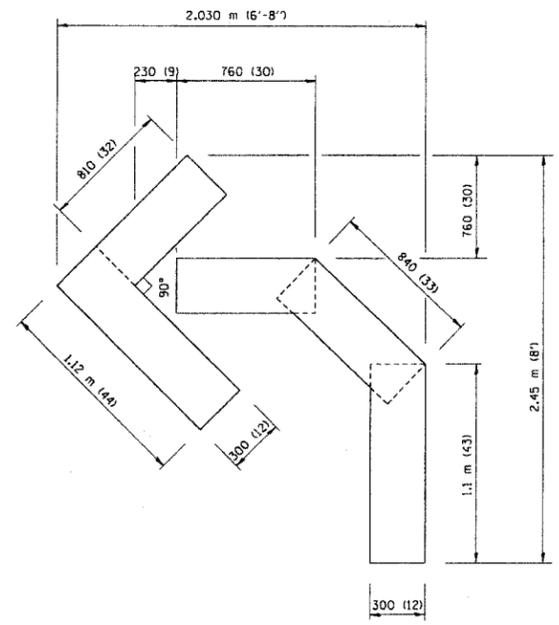
F.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	112 RS-5	MOHAWAY	34	28
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



QUANTITY  
 100 (4) LINE = 19.7 m (64.1 ft.)  
 1.97 sq. m (21.1 sq. ft.)



QUANTITY  
 100 (4) LINE = 25.3 m (82.5 ft.)  
 2.53 sq. m (27.5 sq. ft.)



QUANTITY  
 100 (4) LINE = 13.9 m (45.5 ft.)  
 1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING  
 LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

SCALE: NONE  
 DATE 10/18/2002

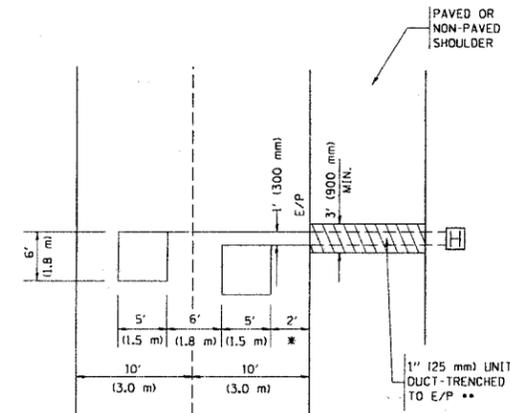
DRAWN BY CADD  
 CHECKED BY IC-16

REVISION DATE: 08/28/00

SECTION	COUNTY	15th SHEETS	SHEET NO.
112.RS-5	MCHEMERY	34	29
STA. TO STA.		FED. ROAD DIST. NO. 7	
		ILLINOIS	
		FED. AID PROJECT	

### LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

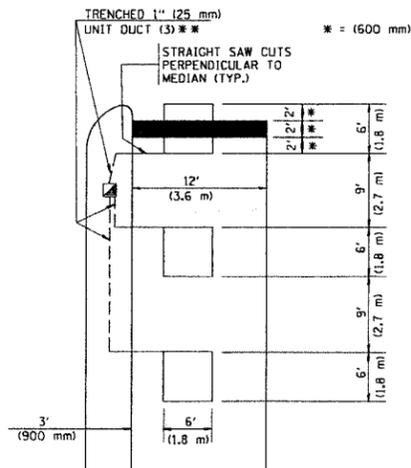


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

### LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

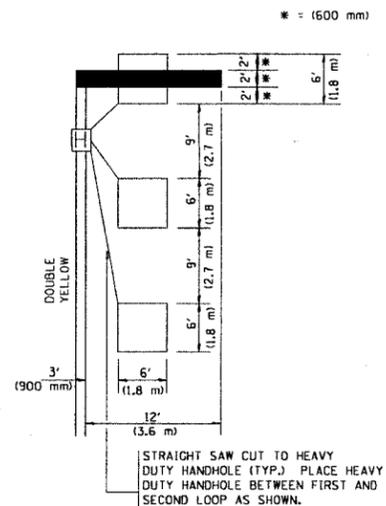


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

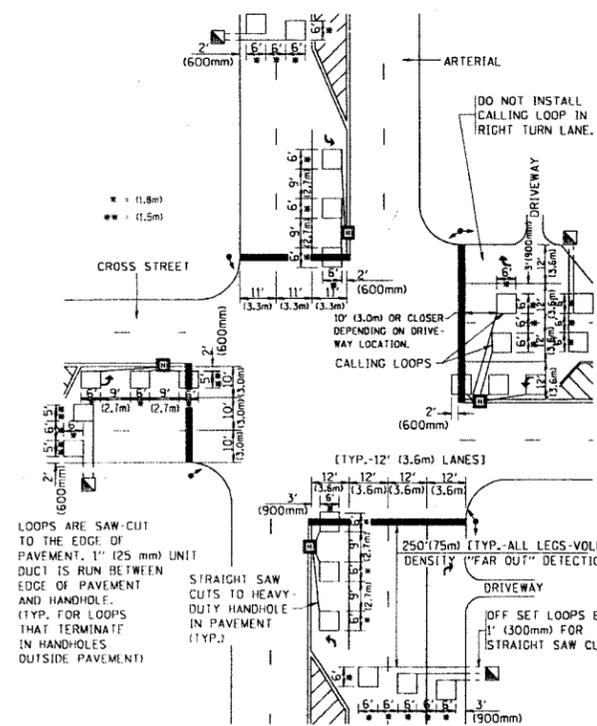
### LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)



\* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

### ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

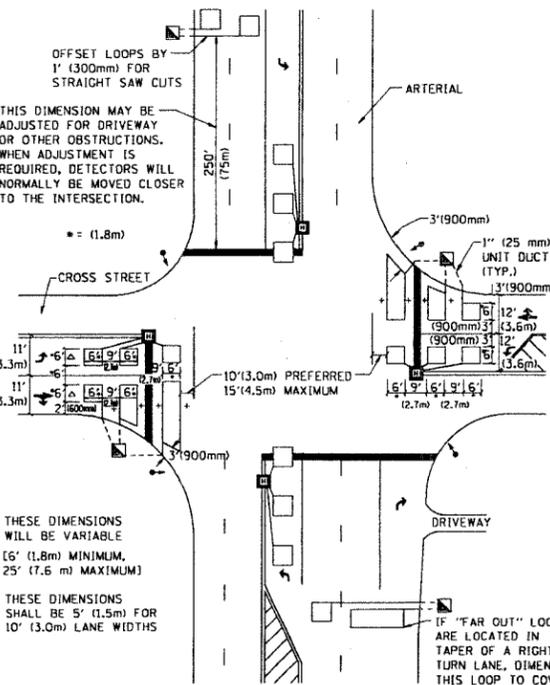


LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1  
N.T.S.

### ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2  
N.T.S.

#### NOTES:

##### VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

#### PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

#### NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ILLINOIS DEPARTMENT OF TRANSPORTATION

### DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

REVISIONS	
NAME	DATE

SCALE: NONE  
DATE: 10/18/2002

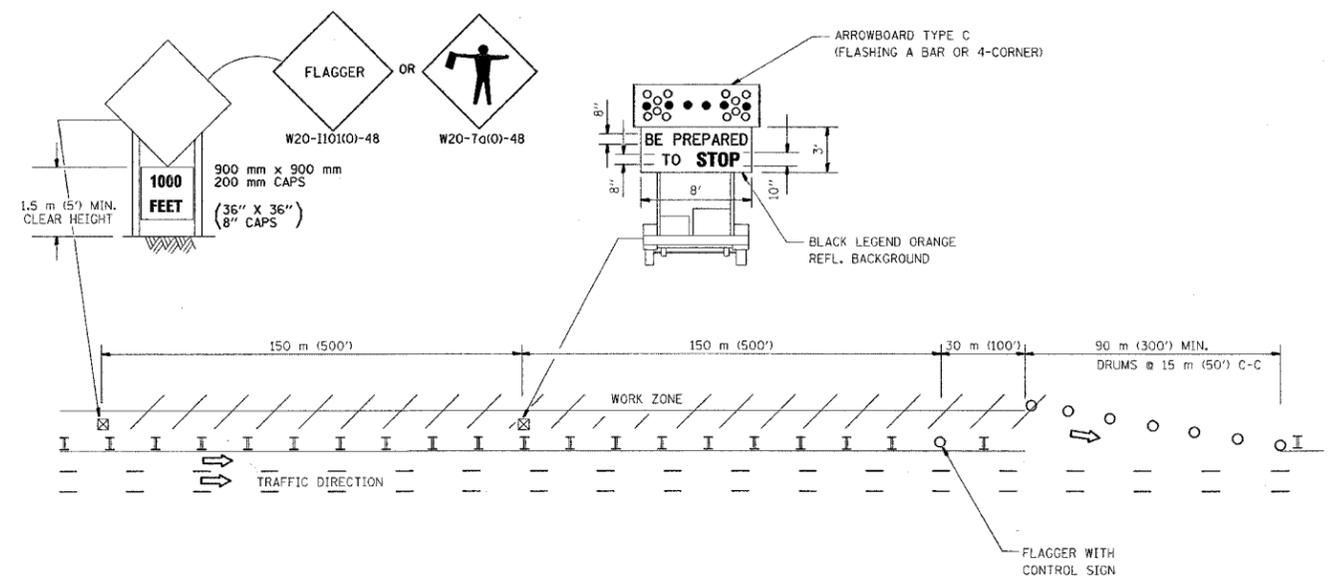
DRAWN BY CADD  
DESIGNED BY  
CHECKED BY R.K.F.  
ISOT

REVISION DATE:

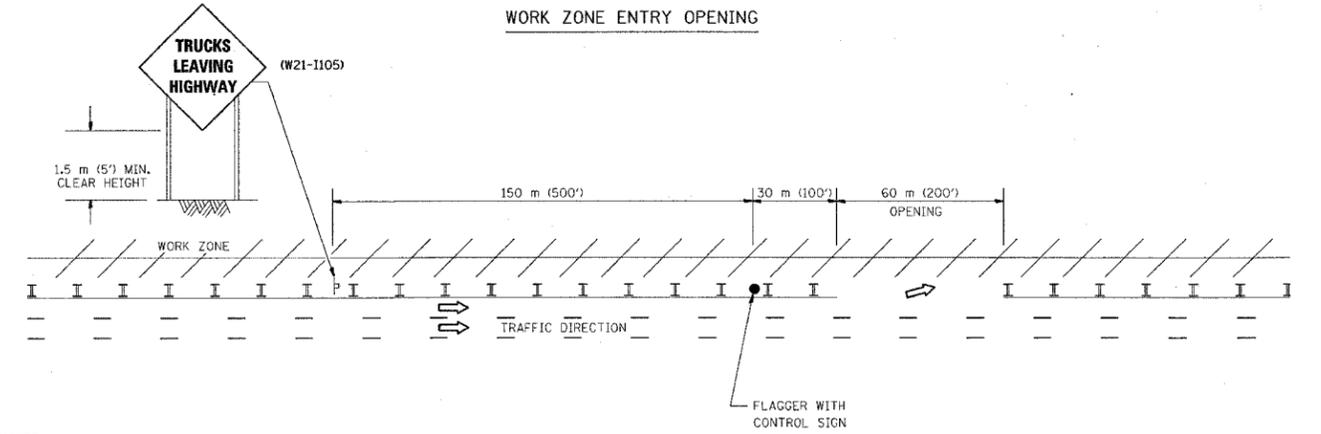
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	R2 RS-5	MCHEMERY	34	30
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
DWS	8/98
JAF	4/03
JAF	2/06

ILLINOIS DEPARTMENT OF TRANSPORTATION  
SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

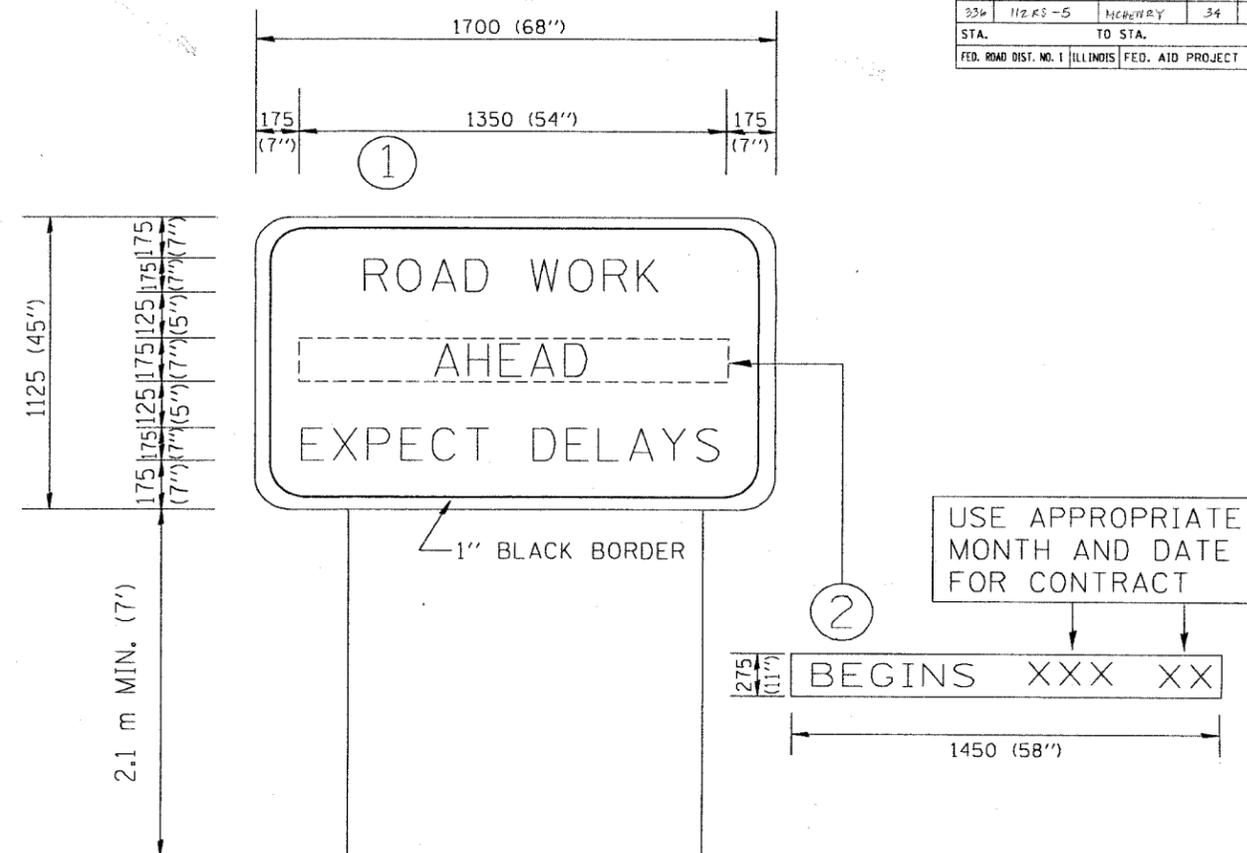
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DATE: 11/6/2006

DRAWN BY CADD  
CHECKED BY TC-18

REVISION DATE: 02/28/06

PLOT DATE = 11/6/2006  
FILE NAME = m:\gis\auto\1108.dgn  
USER NAME = abruash

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336	112.RS-5	MCHEERY	34	31
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

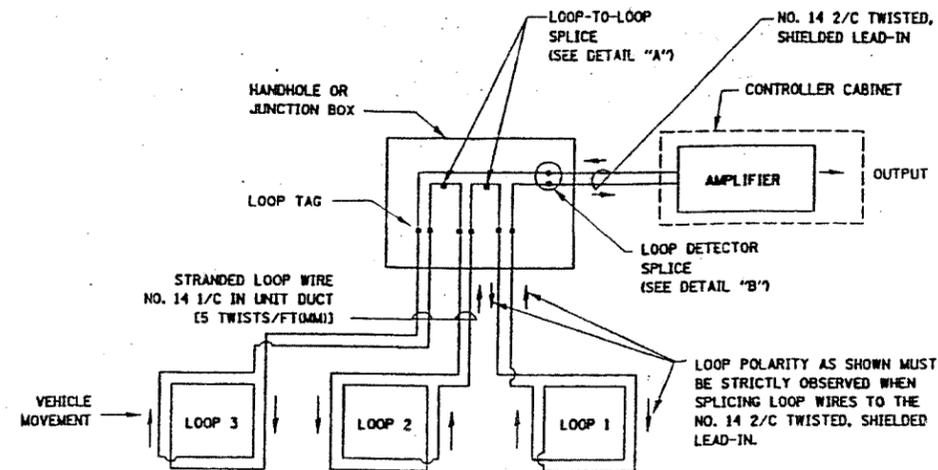
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION TEMPORARY INFORMATION SIGNING
NAME	DATE	
R. MIRS	9-15-97	SCALE: DATE 10/18/2002 DRAWN BY: BUR. OF DESIGN CHECKED BY:
R. MIRS	2-11-97	
T. RAMMACHER	2-2-99	

### LOOP DETECTOR NOTES

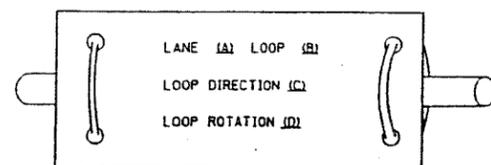
1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



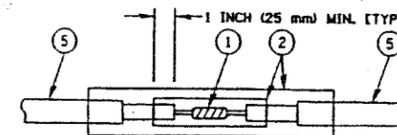
#### DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

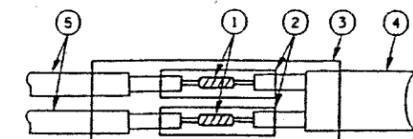
#### LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

#### LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL**  
**DESIGN DETAILS**

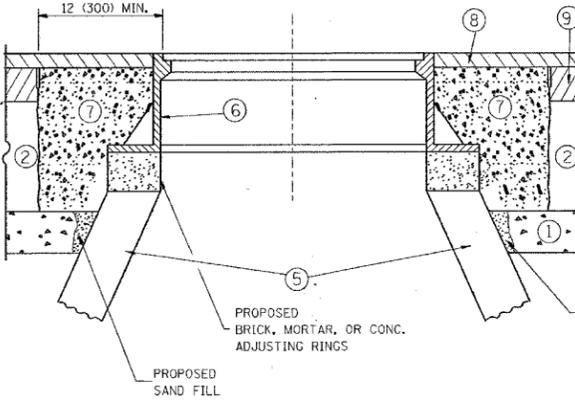
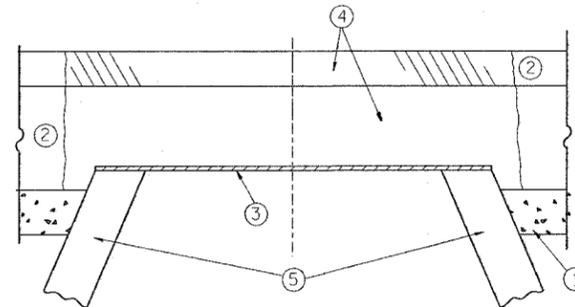
SCALE: VERT. NONE  
 HORIZ. NONE  
 DATE 1-01-02

DRAWN BY: RWP  
 DESIGNED BY: DAD  
 CHECKED BY: TAZ  
 SHEET OF

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	112 R3-5	MCHEWERY	34	34
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	10/11/06

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DETAILS FOR  
 FRAMES AND LIDS ADJUSTMENT  
 WITH MILLING

SCALE: VERT.  
 HORIZ.  
 DATE: 10/30/2006

DRAWN BY  
 CHECKED BY

BD600-03 (BD-8)  
 REVISION DATE: 10/11/06

PLOT DATE = 10/30/2006  
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