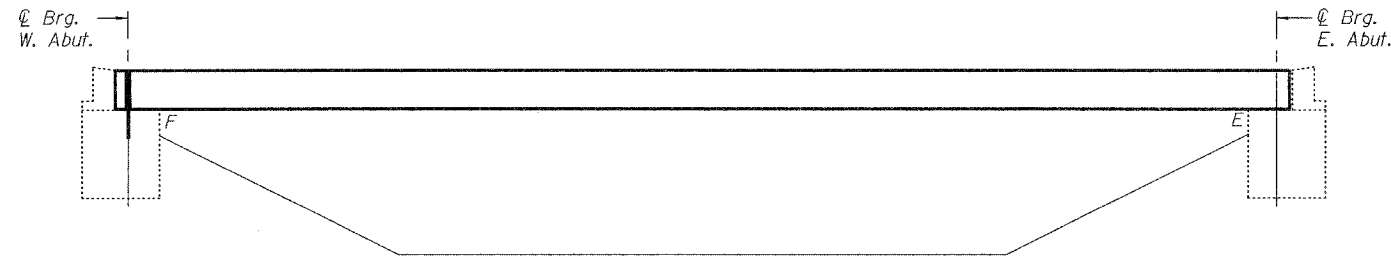


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

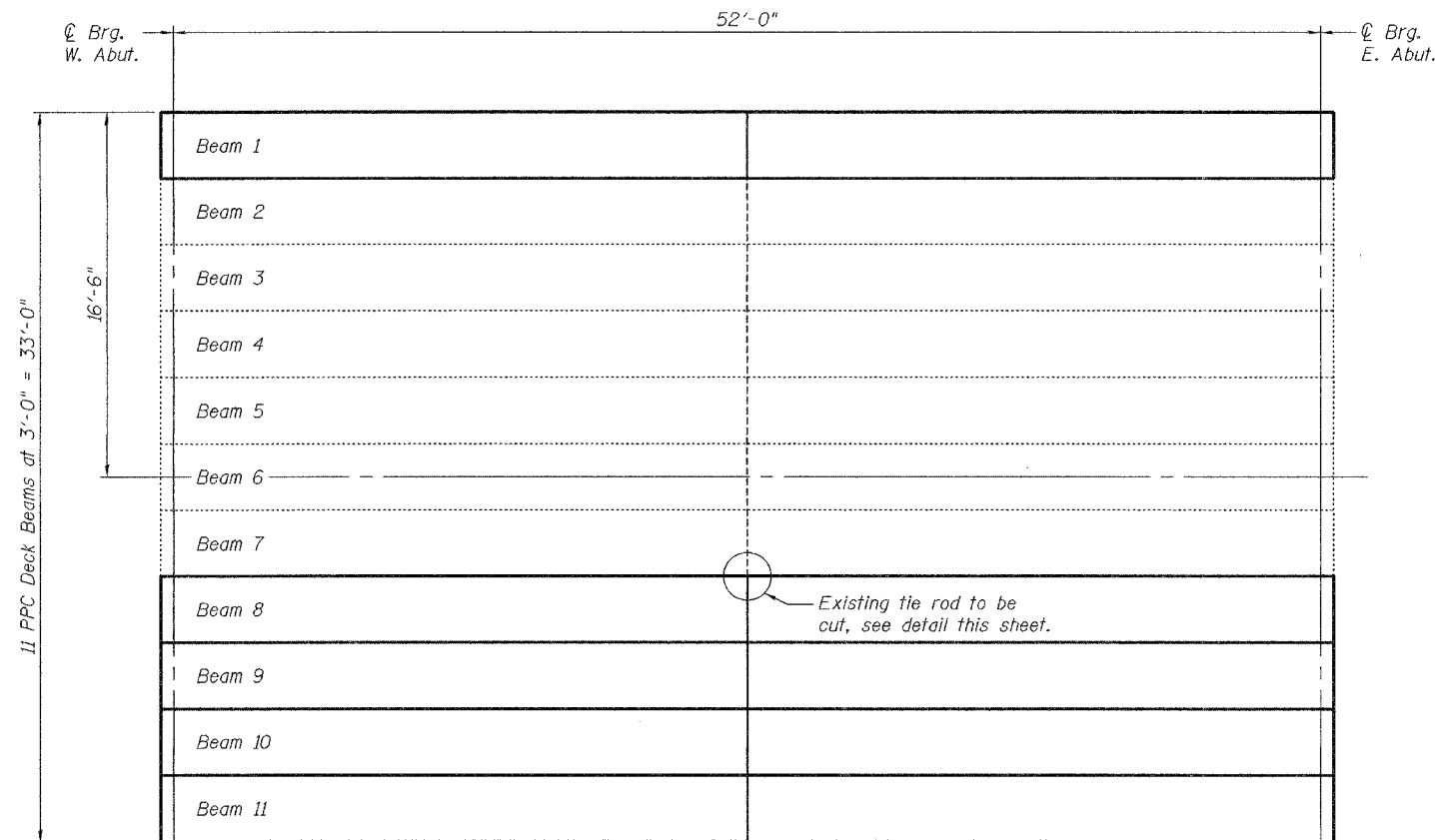
ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
		Ogle	15	13
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

Contract Number: 64C02

SHEET NO. 1  
3 SHEETS

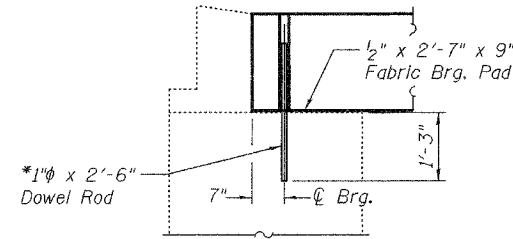


ELEVATION

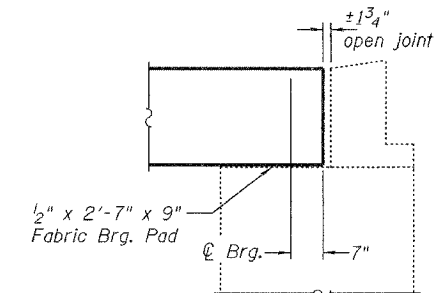


PLAN

\*Existing dowel rods are to be burned off, ground flush, and sealed with epoxy prior to placement of new beams. Cost included in Removal of Existing PPC Deck Beams. After beams have been erected holes shall be drilled into cap and dowel rods placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure a minimum of 24 hours prior to grouting the shear keys.



TYPICAL SECTION WEST ABUTMENT



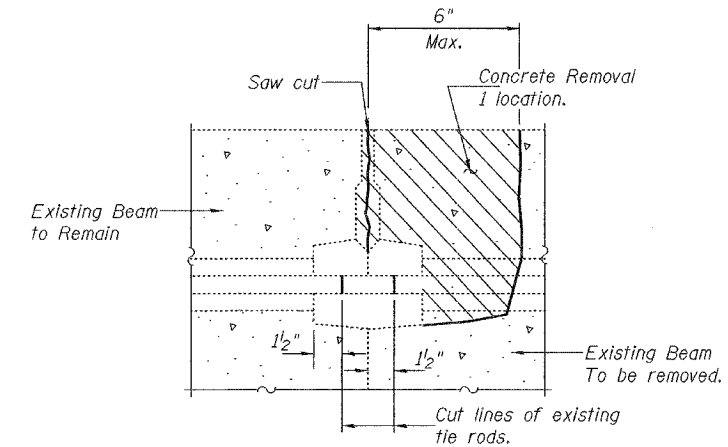
TYPICAL SECTION EAST ABUTMENT

NOTES

The cut strands at each beam end shall be given two coats of zinc dust spray or paint meeting the requirements of ASTM A780. The zinc dust spray or paint shall be applied before corrosion appears and allowed to dry according to manufacturer's specifications prior to another coat of zinc. A concrete sealer meeting the requirements of Section 587 of the Standard Specifications shall be applied to the exterior face and 9" in on the underside of each fascia beam. The sealer shall be applied after visible crack growth has subsided. This work shall be performed by the producer and included with the cost of the beam.

If the contractor's procedure for existing beam removal or placement of new beams involves placement of cranes or other heavy equipment on the bridge, a detailed procedure shall be submitted to the Engineer for approval. The procedure shall include calculations, prepared and sealed by an Illinois Licensed Structural Engineer, verifying that the equipment and procedure used will not overstress the new beams. To distribute load to multiple beams and protect the concrete, in all cases a double layer mat of heavy timbers shall be used at all times under crane tracks or wheels and any outriggers in the down position. If necessary, shims shall be used under the crane mat to ensure uniform contact with the underlying beams. Prior to placement of the timber mats the following shall be done: placement and tightening of transverse tie assemblies, grouting and curing the dowel rods 24 hours minimum and grouting and curing the shear keys. A temporary means of lateral restraint will be required for fascia beams at expansion ends of beams to prevent movement of the beams.

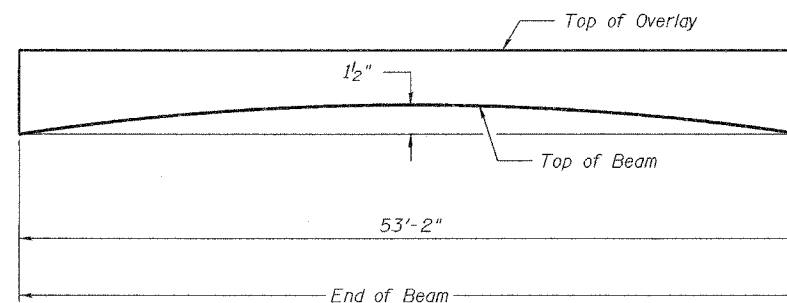
Any damage done to the bridge during beam removal shall be repaired by the Contractor. Cost to be included in the cost of Removal of Existing PPC Deck Beams.



BEAM REMOVAL DETAIL  
AT TRANSVERSE TIES

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Precast Prestressed Concrete Deck Beams (21" Depth)	Sq. Ft.	798
Removal of Existing PPC Deck Beams	Sq. Ft.	798
Asbestos Bearing Pad Removal	Each	5
Steel Railing, Type SM	Foot	107



ANTICIPATED INITIAL CAMBER DIAGRAM

DESIGN STRESSES  
PRECAST UNITS

$f'_c = 6,000$  psi  
 $f'_{ci} = 5,000$  psi  
 $f'_s = 270,000$  psi ( $1/2$ "  $\phi$  low lax strands)  
 $f'_{si} = 201,960$  psi ( $1/2$ "  $\phi$  low lax strands)

PLAN AND ELEVATION

F.A. RT. 549  
OGLE COUNTY  
SN 071-0068

DESIGNED	Vicor H. Voliz
CHECKED	John [Signature]
DRAWN	[Signature]
CHECKED	VHV AJB

EXAMINED	March 2, 2007
REPAIR PLANT UNIT CHIEF	[Signature]
PASSED	[Signature]
ENGINEER OF BRIDGES AND STRUCTURES	



Expires: November 30, 2008