

04-27-12 LETTING ITEM 154

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

McCARTHY ROAD-(FROM DERBY ROAD TO ARCHER AVENUE)

FAU 1587 (McCARTHY ROAD)
FAP 577 (IL ROUTE 171) (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
SECTION 02-00055-00-WR
PROJECT HPP-M-8003(504)
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION
VILLAGE OF LEMONT
COOK COUNTY
C-91-190-05

PROJECT LOCATION MAP

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	1
F.H.W.A. REG.	ILLINOIS	PROJECT HPP-M-8003(504)		

CONTRACT NO. 63557



LOCATION OF SECTION INDICATED THIS: ■

TRAFFIC DATA

ADT:	EXISTING:	DESIGN:
McCARTHY ROAD	6,500 (2009)	8,500 (2027)
ARCHER AVENUE	12,400 (2009)	14,500 (2027)
DERBY ROAD	5,200 (2009)	6,500 (2027)

DESIGN DESIGNATION:

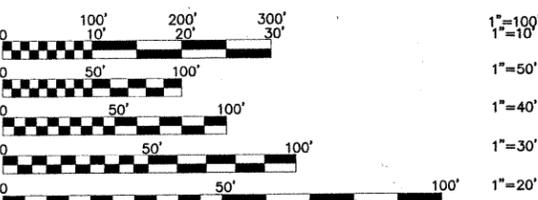
MINOR ARTERIAL
PRINCIPAL ARTERIAL
COLLECTOR

POSTED SPEED

40 MPH-McCARTHY ROAD (EXISTING)
45 MPH-ARCHER AVENUE (EXISTING)
40 MPH-DERBY ROAD (EXISTING)

DESIGN SPEED

45 MPH-McCARTHY ROAD (PROPOSED)
45 MPH-ARCHER AVENUE (PROPOSED)
45 MPH-DERBY ROAD (PROPOSED)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT
UTILITY
LOCATION
INFORMATION FOR
EXCAVATION
CALL 811



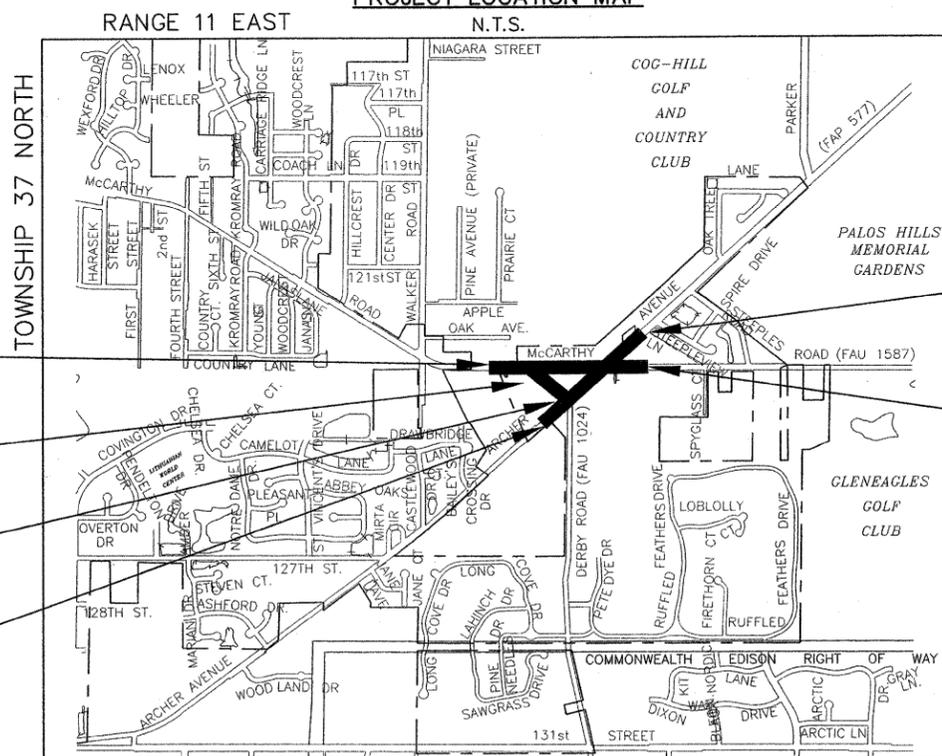
Know what's below.
Call before you dig.

Frank Novotny & Associates, Inc.
Civil Engineers/
Municipal Consultants
825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000928

FNA PROJECT NO. 05027 DRAWN/DESIGNED JFP/JLC CHECKED/APPROVED THK/JLC

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	THK	2-25-11	PER I.D.O.T. REVIEW
2	THK	3-17-11	PER I.D.O.T. REVIEW
3	THK	9-21-11	PER I.D.O.T. REVIEW
4	THK	10-31-11	PER I.D.O.T. REVIEW

CONTRACT NO. 63557



McCARTHY ROAD
PROJECT BEGINS
AT STA. 296+59

DERBY ROAD
PROJECT ENDS
AT STA. 55+53

DERBY ROAD
PROJECT BEGINS
AT STA. 49+24

ARCHER AVENUE
PROJECT BEGINS
AT STA. 149+41

ARCHER AVENUE
PROJECT ENDS
AT STA. 164+00

McCARTHY ROAD
PROJECT ENDS
AT STA. 323+23

PROJECT LOCATED IN THE VILLAGE OF LEMONT

— DENOTES LOCATION OF IMPROVEMENT IN LEMONT TOWNSHIP

LENGTH OF PROJECT	
McCARTHY ROAD-	2,664 FEET (0.50 MILES)
ARCHER AVENUE-	1,459 FEET (0.28 MILES)
DERBY ROAD-	629 FEET (0.12 MILES)
GROSS LENGTH OF PROJECT	4,752 FEET (0.90 MILES)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED October 31 2011

VILLAGE OF LEMONT Brian K. Reaves
BRIAN K. REAVES, VILLAGE MAYOR

PASSED NOVEMBER 2, 2011 2011

RELEASING FOR BID
BASED ON LIMITED
REVIEW NOVEMBER 3, 2011

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

CIVIL ENGINEER: SHEETS 1-45 AND 70-84

James L. Cainkar
JAMES L. CAINKAR, P.E.
IL. P.E. NO. 62-36190
EXPIRES 11-30-2011
DATE 10-31-11

TRAFFIC SIGNAL ENGINEER: SHEETS 46-69

Dana M. Schnabel
DANA M. SCHNABEL, P.E.
IL. P.E. NO. 062-054043
EXPIRES 11-30-2011
DATE 10-31-11

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLER, P.E. (847)705-4406 SCHAUMBURG, IL.

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FILE NAME = FAU 1587 (McCARTHY ROAD)
FAP 577 (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
CHANNELIZATION AND
TRAFFIC SIGNAL INSTALLATION

USER NAME =
PLOT SCALE =
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DESIGNED - JLC
DRAWN - JFP
CHECKED - THK
DATE - 11/10

REVISED - THK 2-25-11
REVISED - THK 9-21-11
REVISED - THK 10-31-11
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX TO SHEETS,
INDEX OF HIGHWAY STANDARDS**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

 **Frank Novotny & Associates, Inc.**
Civil Engineer
Municipal Consultants
825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000028

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	2
CONTRACT NO. 63557				
<small>FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)</small>				

GENERAL CONSTRUCTION NOTES

SPECIFICATIONS

THE LATEST EDITIONS OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", PREPARED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" SHALL GOVERN ALL WORK ASSOCIATED WITH THIS PROJECT. THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY GOVERN OTHER WORK ON THIS PROJECT AS INDICATED BY REFERENCE.

CARE IN EXCAVATION

CARE SHALL BE EXERCISED BY THE CONTRACTOR IN CARRYING OUT EARTH AND/OR TRENCHING OPERATIONS SO THAT LOCAL UTILITY SERVICES, WATER VALVES, MANHOLES, CATCH BASINS, INLETS, BUFFALO BOXES, AND OTHER STRUCTURES ARE NOT DAMAGED OR REMOVED. ANY DAMAGE DONE BY THE CONTRACTOR, WHETHER THE STRUCTURE OR SERVICE IS VISIBLE AT THE GROUND SURFACE OR NOT, SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

NOTIFICATION OF PUBLIC UTILITIES

PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE OFFICIALS OF THE PUBLIC WORKS DEPARTMENT OF THE LOCAL MUNICIPALITY, J.U.L.I.E. AT 1-800-892-0123 OR 811, AND OTHER PUBLIC AND PRIVATE UTILITIES SO THAT ARRANGEMENTS CAN BE MADE TO LOCATE THEIR VARIOUS FACILITIES WITHIN THE LIMITS OF CONSTRUCTION UNDER THIS CONTRACT, AS WELL AS TO PROVIDE ADEQUATE PROTECTION AND INSPECTION THERETO. IT SHALL BE THIS CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES IN THE FIELD.

TRAFFIC CONTROL DEVICES

BARRICADES AND WARNING SIGNS SHALL BE PROVIDED IN ACCORDANCE WITH ARTICLE 107.14 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

PROTECTION OF SIGNS AND PROPERTY

ALL TRAFFIC SIGNS, STREET SIGNS, ETC., THAT INTERFERE WITH THE CONSTRUCTION OPERATIONS SHALL BE REMOVED AND PLACED AT NEW LOCATIONS AS DESIGNATED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. IN ADDITION, ALL MAIL BOXES THAT INTERFERE WITH CONSTRUCTION SHALL BE SIMILARLY RELOCATED AT NO ADDITIONAL COST IN ACCORDANCE WITH ARTICLES 107.20 AND 107.21 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

SUPERINTENDENCE

SPECIAL ATTENTION IS DRAWN TO ARTICLE 105.06 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" WHICH REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT AND SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVING CONTROL OF ALL THE WORK AS THE AGENT OF THE GENERAL CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 108.07.

SAWING EXISTING IMPROVEMENTS

ALL PERMANENT TYPE PAVEMENTS OR OTHER PERMANENT IMPROVEMENTS WHICH ABOUT THE PROPOSED IMPROVEMENT AND MUST BE REMOVED, SHALL BE SAWED AS DIRECTED PRIOR TO REMOVAL. ALL ITEMS SO REMOVED SHALL BE REPLACED WITH SIMILAR CONSTRUCTION MATERIALS TO THEIR ORIGINAL CONDITION OR BETTER. PAYMENT FOR SAWING SHALL BE INCLUDED IN THE COST FOR THE REMOVAL OF EACH ITEM, AND REPLACEMENT WILL BE PAID FOR UNDER THE RESPECTIVE ITEMS IN THE CONTRACT UNLESS OTHERWISE INDICATED. SAW CUTTING FOR PATCHES WILL BE INCLUDED IN THE COST OF THE PATCHING ITEM. EXISTING DRIVEWAY PAVEMENT AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAWCUT TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND THE EXISTING, AND SUCH COST SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

CONSTRUCTION LAYOUT STAKES

THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH WOODEN STAKES OR OTHER LAYOUT MATERIALS FOR LAYOUT OF THE LINES AND GRADES OF THE PROJECT. FAILURE TO PROVIDE STAKES IN A TIMELY MANNER WILL RESULT IN A DELAY IN STAKEOUT WHICH WILL BE APPLICABLE AGAINST THE TIME LIMIT FOR COMPLETION SHOWN IN THE PROJECT SPECIFICATIONS. LINE AND GRADE WILL BE ESTABLISHED BY THE ENGINEER AT REGULAR INTERVALS ON PERMANENTLY PAVED SURFACES, SIDEWALKS OR STAKES AT THE ENGINEER'S OPTION, ALL WITHIN THE PUBLIC RIGHT-OF-WAY AND SHALL BE TRANSFERRED BY THE CONTRACTOR TO THE ACTUAL LINE OF CONSTRUCTION.

PROJECT SAFETY

BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE CONTRACTOR SHALL COMPLY WITH AND OBSERVE THE RULES AND REGULATIONS OF O.S.H.A. AND APPROPRIATE AUTHORITIES REGARDING SAFETY PROVISIONS. THE CONTRACTOR, ENGINEER, AND OWNER SHALL EACH BE RESPONSIBLE FOR THEIR OWN RESPECTIVE AGENTS AND EMPLOYEES.

THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS, OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF HIS WORK IN ACCORDANCE WITH THE DOCUMENTS AND SPECIFICATIONS.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

STORM SEWERS

ALL STORM SEWERS CONSTRUCTED ON THIS CONTRACT ARE TO BE REINFORCED CONCRETE SEWER PIPE MATERIALS AND SHALL MEET THE PIPE REQUIREMENTS OF ASTM C-76 WITH JOINTS CONFORMING TO ASTM C-443 (O-RING).

MISCELLANEOUS

SIDEWALKS SHALL BE INCREASED TO 7" THICKNESS AT ALL DRIVEWAYS.

PROTECTIVE COAT SHALL BE USED ON ALL PORTLAND CEMENT CONCRETE SURFACE IN ACCORDANCE WITH ARTICLES 420.21, 420.22, AND 420.23 OF THE STANDARD SPECIFICATIONS.

THE REMOVAL OF SHRUBS SHALL BE INCLUDED IN THE COST OF "EARTH EXCAVATION" OR "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL".

ALL PAVEMENT PATCHING WILL BE MARKED OUT AND CONSTRUCTED AFTER MILLING.

ALL 4" PERFORATED PVC INSTALLED IN SEWER TRENCHES SHALL HAVE A FABRIC SOCK INSTALLED AND JOINTS OF THE SOCK MATERIAL TAPED CLOSED.

RESIDENT ENGINEER SHALL CONTACT MS. PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN, AT -708-597-9800 A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ENVIRONMENTAL

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

BENCHMARKS

SOURCE BENCHMARK:

COOK COUNTY HIGHWAY DEPT. BENCHMARK 212705

MCCARTHY ROAD ±550' EAST OF ARCHER AVE.

A PK NAIL IN TOP OF AN 8" X 13.5' CONCRETE HEADWALL OF A 3' CONCRETE CULVERT UNDER MCCARTHY ROAD, 26' NORTH OF THE CENTERLINE OF MCCARTHY, 5.3' EAST OF THE WEST END OF THE HEADWALL, ±2' ABOVE THE GRADE OF MCCARTHY ROAD
ELEV. = 721.85 (NGVD29)

CROSS CUT IN CONCRETE WALK AT THE SOUTHWEST CORNER OF MCCARTHY AND DERBY
STA. 304+61.5, 44.9'RT
ELEV. = 736.49

MAG NAIL AT WEST EDGE OF ENTRANCE, SOUTH SIDE OF MCCARTHY
STA. 300+54.6, 43.4'RT
ELEV. = 737.29

MAG NAIL AT WEST EDGE OF ENTRANCE, SOUTH SIDE OF MCCARTHY
STA. 300+54.6, 43.4'RT
ELEV. = 737.29

CROSS CUT IN CURB AT ENTRANCE, SOUTHEAST SIDE OF ARCHER AVE.
STA. 148+33.3, 34.1'RT
ELEV. = 737.68

IRON PIPE SET BEHIND CURB INLET, NORTHWEST SIDE OF ARCHER AVE.
STA. 154+21.2, 28.6'LT
ELEV. = 732.33

FILE NAME = FAU 1587 (MCCARTHY ROAD)
FAP 577 (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
CHANNELIZATION AND
TRAFFIC SIGNAL INSTALLATION

USER NAME =
PLOT SCALE =
PLOT DATE =

DESIGNED -- JLC
DRAWN -- JFP
CHECKED -- THK
DATE -- 11/10

REVISED -- THK 2-25-11
REVISED -- THK 9-21-11
REVISED --
REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL CONSTRUCTION NOTES,
BENCHMARKS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
255 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 897-8640 • Fax: (630) 897-0132
Civil Engineers
Municipal Consultants
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 194-000228

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	3
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

Specialty Item	Special Provision	Item No	Description	Unit	Total Quantity	HPP FUNDING				STU FUNDING			
						Construction Code 003	Construction Code 004	Construction Code 005	Construction Code 0021	Construction Code 003	Construction Code 004	Construction Code 005	Construction Code 0021
						McCARTHY ROAD	DERBY ROAD	ARCHER AVENUE	TRAFFIC SIGNALS	McCARTHY ROAD	DERBY ROAD	ARCHER AVENUE	TRAFFIC SIGNALS
		20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	26	4.1				21.9			
		20200100	EARTH EXCAVATION	CU YD	5806	653.3	260.0			3499.7	1393.0		
		20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	950	58.8	90.6			315.2	485.4		
		20800150	TRENCH BACKFILL	CU YD	2788	360.4	78.2			1930.5	418.9		
		21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	4685	270.6	466.4			1449.4	2498.6		
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	8334	1181.5	129.5			6329.5	693.5		
		25000400	NITROGEN FERTILIZER NUTRIENT	POUND	155	22.0	2.4			118.0	12.6		
		25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	155	22.0	2.4			118.0	12.6		
		25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	155	22.0	2.4			118.0	12.6		
		25100630	EROSION CONTROL BLANKET	SQ YD	452	71.1				380.9			
		25200110	SODDING, SALT TOLERANT	SQ YD	7882	1110.4	129.5			5948.6	693.5		
		25200200	SUPPLEMENTAL WATERING	UNIT	41	5.8	0.6			31.2	3.4		
		28000400	PERIMETER EROSION BARRIER	FOOT	6181	772.7	199.6			4139.3	1069.4		
		28000500	INLET AND PIPE PROTECTION	EACH	1	0.2				0.8			
		28000510	INLET FILTERS	EACH	6	0.8	0.2			4.2	0.8		
		28100105	STONE RIPRAP, CLASS A3	SQ YD	96	15.1				80.9			
	SP	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	10480	1145.3	503.2			6135.7	2695.8		
		31101180	SUBBASE GRANULAR MATERIAL, TYPE B 2"	SQ YD	963	121.3	30.2			649.7	161.8		
		31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	674	62.8	43.3			336.2	231.7		
		35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	674	62.8	43.3			336.2	231.7		
		40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	200	18.9	12.6			101.1	67.4		
		40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	6.4	0.8	0.2			4.5	0.9		
		40600300	AGGREGATE (PRIME COAT)	TON	16	2.1	0.4			11.2	2.3		
		40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	352	55.4				296.6			
		40600895	CONSTRUCTING TEST STRIP	EACH	1	0.1	0.1			0.4	0.4		
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	142	22.3				119.7			
		40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	2065	324.8				1740.2			
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	76	7.1	4.9			37.9	26.1		
		40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1418	223.1				1194.9			
		40701856	HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 8 3/4"	SQ YD	2680		421.6				2258.4		
		42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	110	17.3				92.7			
		42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	10080	1201.1	313.7	70.8		6434.9	1680.3	379.2	
		42400400	PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH	SQ FT	70	11.0				59.0			
		42400800	DETECTABLE WARNINGS	SQ FT	240	25.2	12.6			134.8	67.4		
		44000100	PAVEMENT REMOVAL	SQ YD	2072		325.9				1746.1		
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	978	88.6	65.3			474.4	349.7		
		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1522	159.5	53.0	26.9		854.5	284.0	144.1	
		44000600	SIDEWALK REMOVAL	SQ FT	9112	951.2	411.3	70.8		5095.8	2203.7	379.2	
		44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	50	7.9				42.1			
		44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	425	66.9				358.1			
		44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	75	11.8				63.2			
		44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	100	15.7				84.3			
		48101202	AGGREGATE SHOULDER, TYPE B	CU YD	14	2.2				11.8			
		50105220	PIPE CULVERT REMOVAL	FOOT	277	20.0	23.6			107.0	126.4		
		50300300	PROTECTIVE COAT	SQ YD	3410	416.2	112.9	7.2		2229.8	605.1	38.8	
		54213465	END SECTIONS, 30"	EACH	1	0.2				0.8			
		54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	5	0.8				4.2			
		54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	2	0.3				1.7			
		54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	2	0.3				1.7			
		54215430	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 30"	EACH	1	0.2				0.8			
		54247100	GRATING FOR CONCRETE FLARED END SECTION 15"	EACH	4	0.6				3.4			

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 825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0138
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000628

FILE NAME = FAU 1587 (McCARTHY ROAD)
 FAP 577 (ARCHER AVENUE)
 FAU 1024 (DERBY ROAD)
 CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION
 05027

USER NAME =
 DESIGNED -- JLC
 DRAWN -- JFP
 CHECKED -- THK
 DATE -- 11/10
 REVISED -- THK 2-25-11
 REVISED -- THK 9-21-11
 REVISED -- THK 2-23-13
 REVISED --

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	4
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

Specialty Item	Special Provision	Item No	Description	Unit	Total Quantity	HPP FUNDING				STU FUNDING			
						Construction Code 003	Construction Code 0004	Construction Code 0005	Construction Code 0021	Construction Code 003	Construction Code 0004	Construction Code 0005	Construction Code 0021
						McCARTHY ROAD	DERBY ROAD	ARCHER AVENUE	TRAFFIC SIGNALS	McCARTHY ROAD	DERBY ROAD	ARCHER AVENUE	TRAFFIC SIGNALS
		54247110	GRATING FOR CONCRETE FLARED END SECTION 18"	EACH	1	0.2				0.8			
		54247130	GRATING FOR CONCRETE FLARED END SECTION 24"	EACH	1	0.2				0.8			
		550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	910	124.3	18.9			665.7	101.1		
		550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	58	9.1				48.9			
		550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	157	24.7				132.3			
		550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	104	16.4				87.6			
		550A0140	STORM SEWERS, CLASS A, TYPE 1 30"	FOOT	39	6.1				32.9			
		550A0330	STORM SEWERS, CLASS A, TYPE 2 10"	FOOT	4	0.6				3.4			
		550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	212	22.7	10.7			121.3	57.3		
		550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	499	60.7	17.8			325.3	95.2		
		550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	705	59.8	51.1			320.2	273.9		
		550A0400	STORM SEWERS, CLASS A, TYPE 2 21"	FOOT	508	79.9				428.1			
		550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	287	45.1				241.9			
		55100400	STORM SEWER REMOVAL 10"	FOOT	28	4.4				23.6			
		55100500	STORM SEWER REMOVAL 12"	FOOT	345	54.3				290.7			
		55100700	STORM SEWER REMOVAL 15"	FOOT	414		65.1				348.9		
		55100900	STORM SEWER REMOVAL 18"	FOOT	259	40.7				218.3			
		55101100	STORM SEWER REMOVAL 21"	FOOT	408	64.2				343.8			
		55101200	STORM SEWER REMOVAL 24"	FOOT	267	42.0				225.0			
	SP	56106700	ADJUSTING WATER MAIN 16"	FOOT	30	4.7				25.3			
		56300300	ADJUSTING WATER SERVICE LINES	FOOT	140	22.0				118.0			
*		56400100	FIRE HYDRANTS TO BE MOVED	EACH	5	0.8				4.2			
		60107600	PIPE UNDERDRAINS 4"	FOOT	360	31.5	25.2			168.5	134.8		
		60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	5	0.8				4.2			
		60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	36	4.4	1.3			23.6	6.7		
		60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	7	0.9	0.2			5.1	0.8		
		60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	14	1.6	0.6			8.4	3.4		
		60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	0.2				0.8			
		60250200	CATCH BASINS TO BE ADJUSTED	EACH	6	0.6	0.3			3.4	1.7		
		60255500	MANHOLES TO BE ADJUSTED	EACH	15	1.4	0.9			7.6	5.1		
		60265700	VALVE VAULTS TO BE ADJUSTED	EACH	6	0.8	0.2			4.2	0.8		
		60500040	REMOVING MANHOLES	EACH	1	0.2				0.8			
		60500050	REMOVING CATCH BASINS	EACH	11	1.6	0.2			8.4	0.8		
		60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	781	87.3	35.5			467.7	190.5		
		60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	6094	725.9	211.7	20.9		3889.1	1134.3	112.1	
*		63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	150	23.6				126.4			
*		63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	4	0.6				3.4			
*	SP	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	2710	347.6	47.2	31.5		1862.4	252.8	168.5	
*	SP	66900400	SPECIAL WASTE GROUNDWATER DISPOSAL	GALLON	8500.1	1101.1		236.0		5898.9		1264.1	
*	SP	66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	0.2				0.8			
*	SP	66900530	SOIL DISPOSAL ANALYSIS	EACH	6	0.6	0.2	0.2		3.4	0.8	0.8	
*	SP	66901000	BACKFILL PLUGS	CU YD	65	10.2				54.8			
		67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	0.9	0.9			5.1	5.1		
		67100100	MOBILIZATION	L SUM	1	0.1	0.1			0.4	0.4		
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	0.1	0.1			0.4	0.4		
		70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.1	0.1			0.4	0.4		
		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	0.1	0.1			0.4	0.4		
		70106800	CHANGEABLE MESSAGE SIGN	CAL MO	40	2.5	2.5	1.3		13.5	13.5	6.7	
		70300100	SHORT TERM PAVEMENT MARKING	FOOT	604	76.8	18.2			411.2	97.8		

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 835 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-9660 • Fax: (630) 887-0132
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 154-000288

FILE NAME = FAU 1587 (McCARTHY ROAD)
 FAP 577 (ARCHER AVENUE)
 FAU 1024 (DERBY ROAD)
 CHANNELIZATION AND
 TRAFFIC SIGNAL INSTALLATION

USER NAME =
 PLOT SCALE =
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
 SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	5
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

Specialty Item	Special Provision	Item No	Description	Unit	Total Quantity	HPP FUNDING				STU FUNDING				
						Construction Code 003	Construction Code 0004	Construction Code 0005	Construction Code 0021	Construction Code 003	Construction Code 0004	Construction Code 0005	Construction Code 0021	
						McCARTHY ROAD	DERBY ROAD	ARCHER AVENUE	TRAFFIC SIGNALS	McCARTHY ROAD	DERBY ROAD	ARCHER AVENUE	TRAFFIC SIGNALS	
		70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	80	12.6				67.4				
		70300280	TEMPORARY PAVEMENT MARKING LINE 24"	FOOT	150	23.6				126.4				
		70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	579	85.1	6.0			455.9	32.0			
*		72000100	SIGN PANEL - TYPE 1	SQ FT	224	14.6	5.4	5.5	9.8	77.9	29.1	29.5	52.2	
*		72000200	SIGN PANEL - TYPE 2	SQ FT	50				7.9				42.1	
*		72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	3	0.2	0.3			0.8	1.7			
*		72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	10	1.3	0.2	0.2		6.7	0.8	0.8		
*		72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	2	0.3				1.7				
*		72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	15	2.0	0.3			11.0	1.7			
*		72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	17	2.4	0.2	0.2		12.6	0.8	0.8		
*		72900200	METAL POST - TYPE B	FOOT	220	25.2	7.9	1.6		134.8	42.1	8.4		
*		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	537	45.9	15.6	23.0		246.1	83.4	123.0		
*		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11534	1402.8	263.5	148.0		7515.2	1411.5	793.0		
*		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4190	501.9	55.5	101.6		2689.1	297.5	544.4		
*		78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	448	43.3	27.2			231.7	145.8			
*		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	646	65.3	15.7	20.6		349.7	84.3	110.4		
*		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	298	28.9	9.8	8.2		155.1	52.2	43.8		
*		78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	303	37.4	5.5	4.7		200.6	29.5	25.3		
*		78200410	GUARDRAIL MARKERS, TYPE A	EACH	8	1.3				6.7				
*		78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	0.6				3.4				
		78300100	PAVEMENT MARKING REMOVAL	SQ FT	664			104.4				559.6		
		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	12			1.9				10.1		
*		80500010	SERVICE INSTALLATION - GROUND MOUNTED	EACH	1				0.2				0.8	
*		80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	2				0.3				1.7	
*		81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	3250				511.2				2738.8	
*		81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	297				46.7				250.3	
*		81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	317				49.9				267.1	
*		81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	1180				185.6				994.4	
*		81400100	HANDHOLE	EACH	17				2.7				14.3	
*		81400200	HEAVY-DUTY HANDHOLE	EACH	7				1.1				5.9	
*		81400300	DOUBLE HANDHOLE	EACH	5				0.8				4.2	
*		85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	2				0.3				1.7	
*		85700300	FULL-ACTUATED CONTROLLER AND TYPE V CABINET	EACH	1				0.2				0.8	
*	SP	86000100	MASTER CONTROLLER	EACH	1				0.2				0.8	
*		86400100	TRANSCEIVER - FIBER OPTIC	EACH	3				0.5				2.5	
*		87100020	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	FOOT	1534				241.3				1292.7	
*		87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	1364				214.6				1149.4	
*		87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1166				183.4				982.6	
*		87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2923				459.8				2463.2	
*		87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	5123				805.8				4317.2	
*		87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	2909				457.6				2451.4	
*		87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	5398				849.1				4548.9	
*		87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	202				31.3				170.2	
*		87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1907				300.0				1607.0	
*		87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	2				0.3				1.7	
*		87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	7				1.1				5.9	
*		87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4				0.6				3.4	

FILE NAME = FAU 1587 (McCARTHY ROAD)
 FAP 577 (ARCHER AVENUE)
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USER NAME =
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
 635 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0123
 Civil Engineers
 Municipal Consultants
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00028

F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
RTE. -	02-00055-00-WR	COOK	84	6
VAR				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPP-M-8003(504)		

Specialty Item	Special Provision	Item No	Description	Unit	Total Quantity	HPP FUNDING				STU FUNDING			
						Construction Code 003	Construction Code 0004	Construction Code 0005	Construction Code 0021	Construction Code 003	Construction Code 0004	Construction Code 0005	Construction Code 0021
						McCARTHY ROAD	DERBY ROAD	ARCHER AVENUE	TRAFFIC SIGNALS	McCARTHY ROAD	DERBY ROAD	ARCHER AVENUE	TRAFFIC SIGNALS
*		87601200	PEDESTRIAN PUSH-BUTTON POST, GALVANIZED STEEL, TYPE II	EACH	1				0.2				0.8
*		87700130	STEEL MAST ARM ASSEMBLY AND POLE, 18 FT.	EACH	1				0.2				0.8
*		87700150	STEEL MAST ARM ASSEMBLY AND POLE, 22 FT.	EACH	2				0.3				1.7
*		87700180	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	2				0.3				1.7
*		87700190	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	1				0.2				0.8
*		87700210	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	2				0.3				1.7
*		87700260	STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.	EACH	1				0.2				0.8
*		87702350	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 26 FT. AND 32 FT.	EACH	1				0.2				0.8
*		87800100	CONCRETE FOUNDATION, TYPE A	FOOT	60				9.4				50.6
*		87800150	CONCRETE FOUNDATION, TYPE C	FOOT	12				1.9				10.1
*		87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	91				14.3				76.7
*	SP	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	25				3.9				21.1
*		88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	9				1.4				7.6
*		88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4				0.6				3.4
*		88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2				0.3				1.7
*		88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	5				0.8				4.2
*		88030210	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	1				0.2				0.8
*		88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	4				0.6				3.4
*	SP	88055150	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3				0.5				2.5
*	SP	88055160	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	5				0.8				4.2
*	SP	88055200	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	3				0.5				2.5
*	SP	88060180	COMBINATION SIGNAL HEAD, LED, 2-FACE, 1-5 SECTION OPTICALLY PROGRAMMED, 1-3 SECTION, BRACKET MOUNTED	EACH	2				0.3				1.7
*		88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	6				0.9				5.1
*		88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	3				0.5				2.5
*		88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	22				3.5				18.5
*		88500100	INDUCTIVE LOOP DETECTOR	EACH	23				3.6				19.4
*		88600100	DETECTOR LOOP, TYPE I	FOOT	1876				295.1				1580.9
*		88700200	LIGHT DETECTOR	EACH	7				1.1				5.9
*		88700300	LIGHT DETECTOR AMPLIFIER	EACH	3				0.5				2.5
*		88800100	PEDESTRIAN PUSH-BUTTON	EACH	10				1.6				8.4
*		89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	3				0.5				2.5
*	SP	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	3				0.5				2.5
*	SP	89502380	REMOVE EXISTING HANDHOLE	EACH	19				3.0				16.0
	SP	K1005421	SEEDING (SPECIAL)	ACRE	0.10	0.0				0.1			
	SP	X0322118	REMOVE CONCRETE FLARED END SECTIONS	EACH	16	1.7	0.8			9.3	4.2		
	SP	X0323389	STORM SEWER CONNECTION	EACH	4	0.5	0.2			2.5	0.8		
	SP	X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	7821	1230.2				6590.8			
*	SP	X8730250	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED	FOOT	1534				241.3				1292.7
*	SP	Z0016702	DETOUR SIGNING	L SUM	1	0.1	0.1			0.4	0.4		
	SP	Z0023202	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	12	1.6	0.3			8.4	1.7		
	SP	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	60	60							
*	SP	Z0033056	OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1				0.2				0.8
	SP	Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	979	64.0	90.0			343.0	482.0		
*	SP	Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	3				0.5				2.5
	SP	Z0076600	TRAINEES	HOURL	1500								
*	SP	X8620200	UNINTERRUPTIBLE POWER SUPPLY SPECIAL	EACH	3				0.5				2.5

TRAINEES CONSTRUCTION CODE 0042 (HPP=235.5 HOURS) (STU=1,264.5 HOURS)

FILE NAME = FAU 1587 (McCARTHY ROAD)
FAP 577 (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
CHANNELIZATION AND
TRAFFIC SIGNAL INSTALLATION

USER NAME =
PLOT SCALE =
PLOT DATE =

DESIGNED -- JLC
DRAWN -- JFP
CHECKED -- THK
DATE -- 11/10
REVISED -- THK 2-25-11
REVISED -- THK 9-21-11
REVISED -- THK 10-31-11
REVISED -- THK 2-23-13

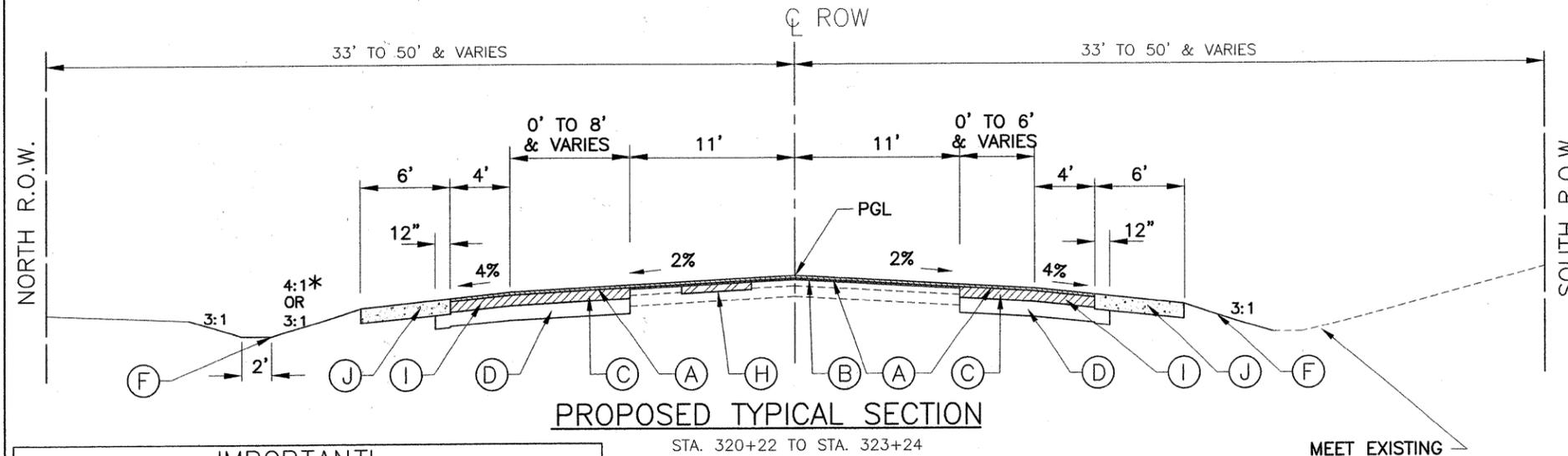
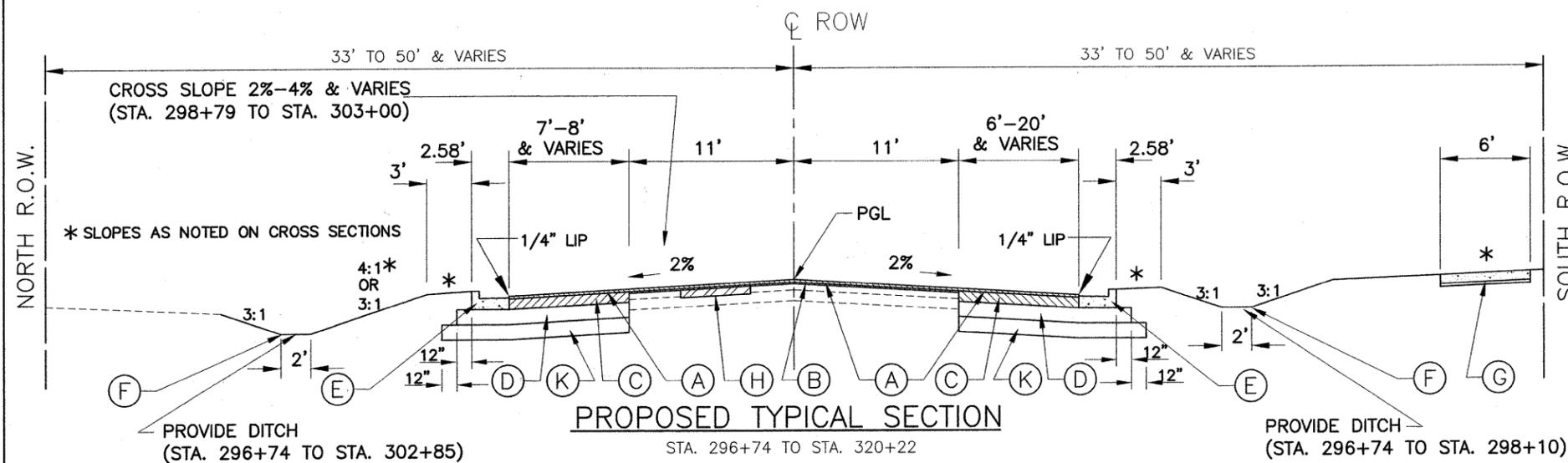
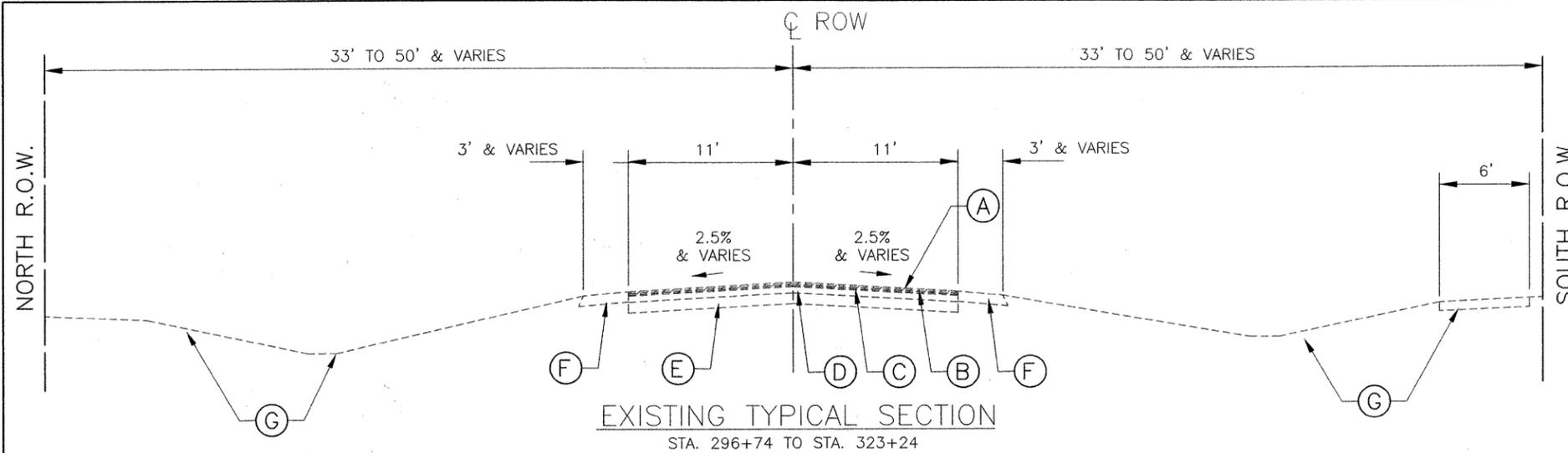
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
Civil Engineers
Municipal Consultants
825 Midway Drive • Willowbrook, IL • 60377 • Telephone: (630) 887-9640 • Fax: (630) 887-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000828

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	7
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



EXISTING LEGEND

- (A) PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-3/4" & (VARIABLE DEPTH)
 - (B) EXISTING HOT-MIX ASPHALT SURFACE COURSE, 1-1/2"
 - (C) EXISTING HOT-MIX ASPHALT BINDER COURSE, 2"
 - (D) EXISTING HOT-MIX ASPHALT BASE COURSE, 5" AVG.
 - (E) EXISTING PORTLAND CEMENT CONCRETE BASE COURSE, 8-1/2" AVG.
 - (F) EXISTING AGGREGATE SHOULDER
 - (G) EXISTING PORTLAND CEMENT CONCRETE DRIVEWAY, HOT-MIX ASPHALT SURFACE DRIVEWAY, PORTLAND CEMENT CONCRETE SIDEWALK (STA. 316+45 TO STA. 319+35, NORTH SIDE AND STA. 301+00 TO STA. 316+55, SOUTH SIDE), AND EXISTING GRASS PARKWAY
- /// INDICATES REMOVAL ITEM

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS (McCARTHY ROAD)

MIXTURE TYPE	AIR VOIDS @ NDES
PAVEMENT WIDENING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL-9.5mm), 2"	4% @ 70 GYR
LEVELING BINDER (MACHINE METHOD), N70, (IL-9.5mm), 3/4"	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, (IL-19 mm), 7-1/2" (IN 3 LIFTS)	4% @ 70 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL-9.5mm), 2"	4% @ 50 GYR
HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19 mm), 8" (IN 3 LIFTS)	
PATCHING	
CLASS D PATCH, (HMA BINDER IL-19 mm), 6"	4% @ 70 GYR

THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

"THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS"

"FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS"

PROPOSED LEGEND

- (A) PROPOSED "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH
- (B) PROPOSED "LEVELING BINDER (MACHINE METHOD), N70", 3/4 INCH MIN.
- (C) PROPOSED "HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70", 7-1/2 INCH
- (D) PROPOSED "AGGREGATE SUBGRADE IMPROVEMENT, 12" -WITH 4" PERFORATED PVC IN SEWER TRENCH
- (E) PROPOSED "COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24" AS APPLICABLE
- (F) PROPOSED "DRIVEWAY PAVEMENT REMOVAL" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50", 2 INCH ON "HOT MIX ASPHALT BASE COURSE, 8 INCH" ON "SUBBASE GRANULAR MATERIAL, TYPE B, 4 INCH" OR PROPOSED "TOPSOIL FURNISH & PLACE, 4 INCH" AND "SODDING"
- (G) PROPOSED "PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH". (AS DIRECTED) ON "SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
- (H) PROPOSED "CLASS D PATCHES, TYPE I - IV, 6 INCH"-AS DIRECTED
- (I) PROPOSED HOT-MIX ASPHALT SHOULDER (SAME AS (A) & (C) ABOVE)
- (J) PROPOSED "AGGREGATE SHOULDER, TYPE B, 12 INCH"
- (K) PROPOSED "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL" AND "POROUS GRANULAR EMBANKMENT, SUBGRADE" WITH "GEOTECHNICAL FABRIC FOR GROUND STABILIZATION", SUBSEQUENT TO PROOF ROLL VERIFICATION (AS DIRECTED). STA. 308+50 TO STA. 311+50, 9 INCH STA. 314+50 TO STA. 317+50, 6 INCH

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

McCARTHY ROAD

FILE NAME = FAU 1587 (McCARTHY ROAD)	USER NAME =	DESIGNED -- JLC	REVISED -- THK 3-17-11
FAP 577 (ARCHER AVENUE)		DRAWN -- JFP	REVISED -- THK 9-21-11
FAU 1024 (DERBY ROAD)		CHECKED -- THK	REVISED -- THK 10-31-11
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION		DATE -- 11/10	REVISED -- THK 2-23-12

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS (McCARTHY ROAD) HOT-MIX ASPHALT MIXTURE REQUIREMENTS

SCALE: 1"=5' SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
 885 Midway Drive • Willowbrook, IL • 60207 • Telephone (630) 897-8640 • Fax (630) 897-0132
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-080028

F.A.U. F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	9
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

EXISTING LEGEND

- (A) PROPOSED PAVEMENT REMOVAL
- (B) EXISTING HOT-MIX ASPHALT SURFACE COURSE, 1"
- (C) EXISTING HOT-MIX ASPHALT BINDER COURSE, 3.2"
- (D) EXISTING HOT-MIX ASPHALT BASE COURSE, 8.75" AVG.
- (E) EXISTING POZZOLANIC AGGREGATE MATERIAL, 8.75 AVG.
- (F) EXISTING AGGREGATE SHOULDER
- (G) EXISTING PORTLAND CEMENT CONCRETE DRIVEWAY, HOT-MIX ASPHALT SURFACE DRIVEWAY, PORTLAND CEMENT CONCRETE SIDEWALK (STA. 316+45 TO STA. 319+35, NORTH SIDE AND STA. 301+00 TO STA. 316+55, SOUTH SIDE), AND EXISTING PARKWAY

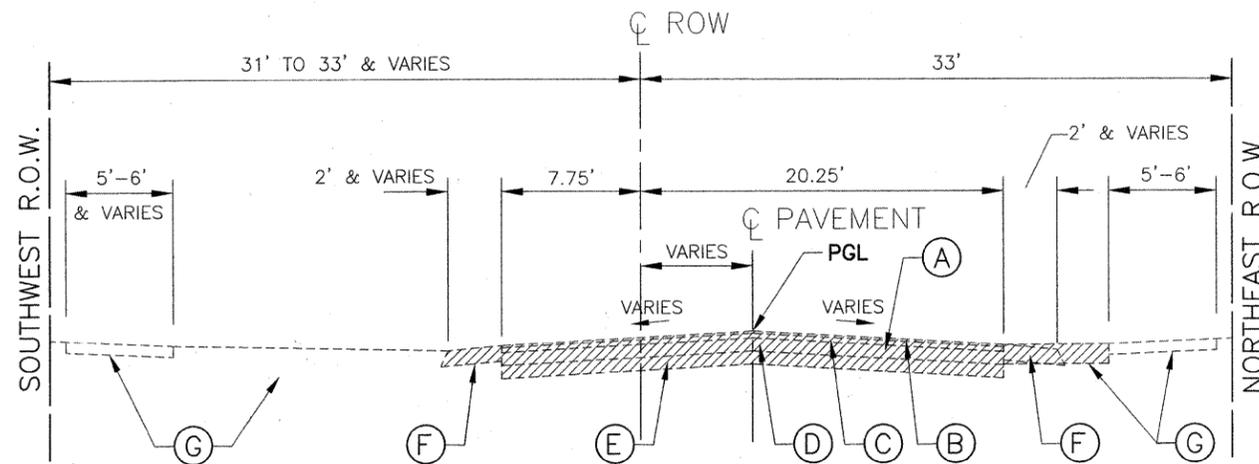
HOT-MIX ASPHALT MIXTURE REQUIREMENTS (DERBY ROAD)

MIXTURE TYPE	AIR VOIDS @ NDES
FULL DEPTH PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL-9.5mm), 2"	4% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, (IL-19 mm), 6-3/4" (IN 3 LIFTS)	4% @ 70 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL-9.5mm), 2"	4% @ 50 GYR
HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19 mm), 8" (IN 3 LIFTS)	

THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 "THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS"
 "FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS"

PROPOSED LEGEND

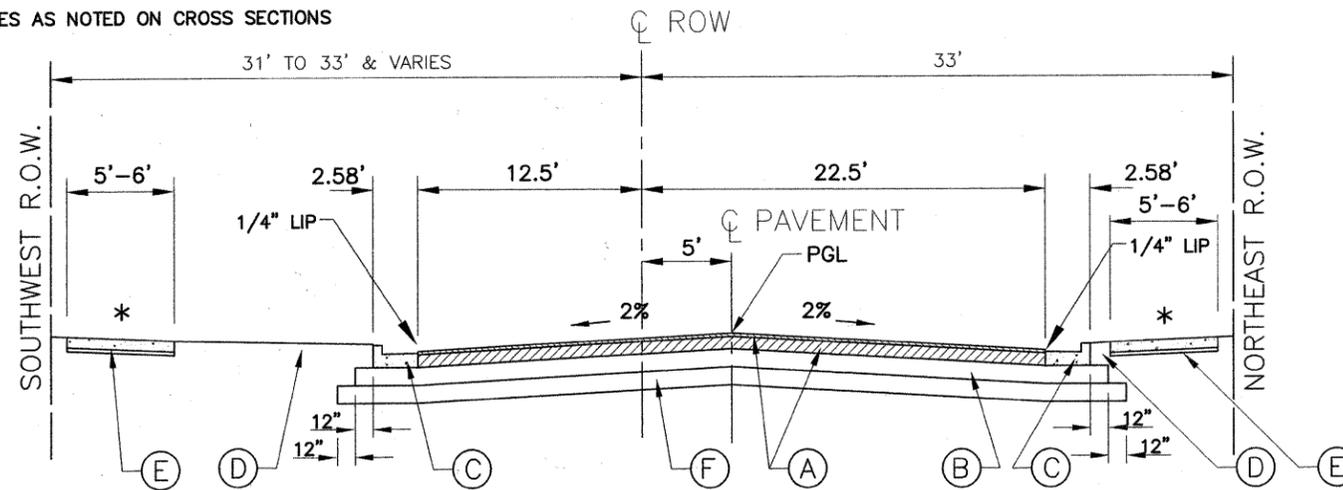
- (A) PROPOSED "HOT-MIX ASPHALT PAVEMENT, (FULL DEPTH)", 8-3/4 INCH
- (B) PROPOSED "AGGREGATE SUBGRADE IMPROVEMENT, 12" -WITH 4" PERFORATED PVC IN SEWER TRENCH
- (C) PROPOSED "COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24" AS APPLICABLE
- (D) PROPOSED "DRIVEWAY PAVEMENT REMOVAL" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50", 2 INCH ON "HOT MIX ASPHALT BASE COURSE, 8 INCH" ON "SUBBASE GRANULAR MATERIAL, TYPE B, 4 INCH" OR PROPOSED "TOPSOIL FURNISH & PLACE, 4 INCH" AND "SODDING"
- (E) PROPOSED "SIDEWALK REMOVAL" AND PROPOSED "PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH". (AS DIRECTED) ON "SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
- (F) PROPOSED "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL" AND "POROUS GRANULAR EMBANKMENT, SUBGRADE" WITH "GEOTECHNICAL FABRIC FOR GROUND STABILIZATION", SUBSEQUENT TO PROOF ROLL VERIFICATION (AS DIRECTED).
 STA. 50+25 TO STA. 52+00, 6 INCH
 STA. 52+00 TO STA. 53+75, 12 INCH
 STA. 53+75 TO STA. 55+53, 6 INCH



EXISTING TYPICAL SECTION

ARCHER AVENUE TO McARTHUR ROAD
 STA. 49+24 TO STA. 55+53

* SLOPES AS NOTED ON CROSS SECTIONS



PROPOSED TYPICAL SECTION

ARCHER AVENUE TO McARTHUR ROAD
 STA. 49+24 TO STA. 55+53

DERBY ROAD

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FILE NAME = FAU 1587 (McARTHUR ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED -- JLC	REVISED -- THK 2-25-11
		DRAWN -- JFP	REVISED -- THK 9-21-11
		CHECKED -- THK	REVISED -- THK 2-23-12
		DATE -- 11/10	REVISED --

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS (DERBY ROAD)
 HOT-MIX ASPHALT MIXTURE REQUIREMENTS

SCALE: 1"=5' SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
 825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0132
 ILLINOIS PROFESSIONAL DESIGN PERM NO. 184-000928

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAP	02-00055-00-WR	COOK	84	10
VAR				
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



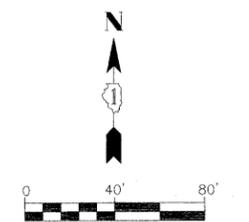
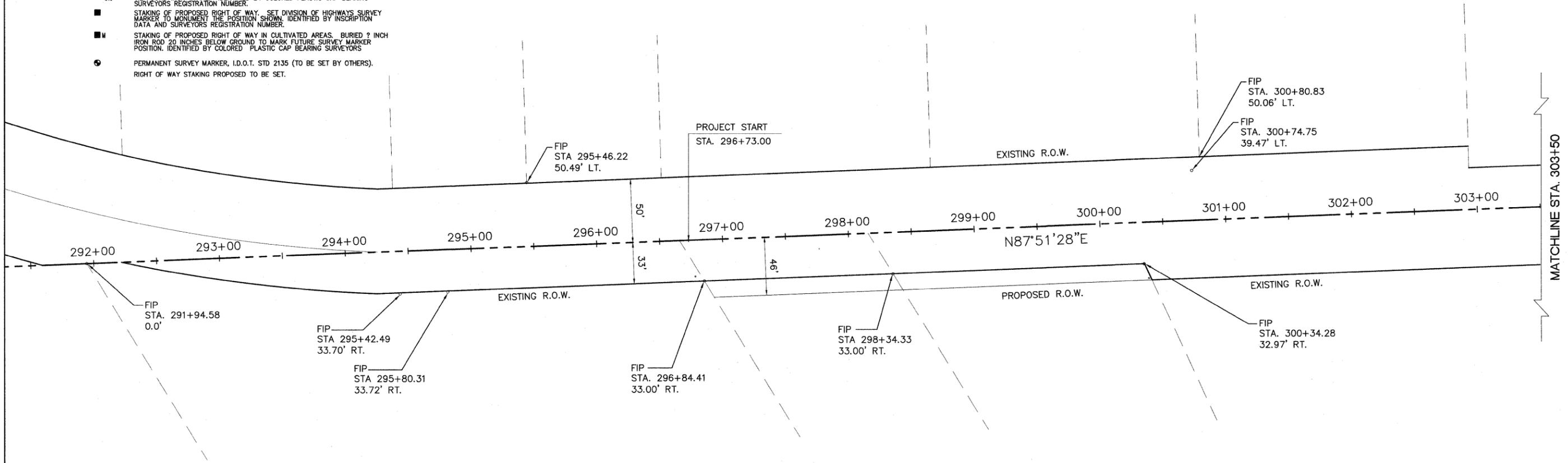
SECTION
QUARTER



QUARTER
SECTION
QUARTER

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 129.32' MEASURED DIMENSION
- 129.32'(COMP.) COMPUTED DIMENSION
- () RECORD DATA
- EXISTING BUILDING

- IRON PIPE OR ROD FOUND ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET ● 1" REBAR SET
- 11 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 1/2 INCH
12 IRON ROAD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY
13 COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- 81 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONU-
82 MENTATION. BURIED 1/2 INCH IRON ROD 20 INCHES BELOW GROUND TO
83 TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING
SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY
MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION
DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 1/2 INCH
IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER
POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS
- PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS).
RIGHT OF WAY STAKING PROPOSED TO BE SET.



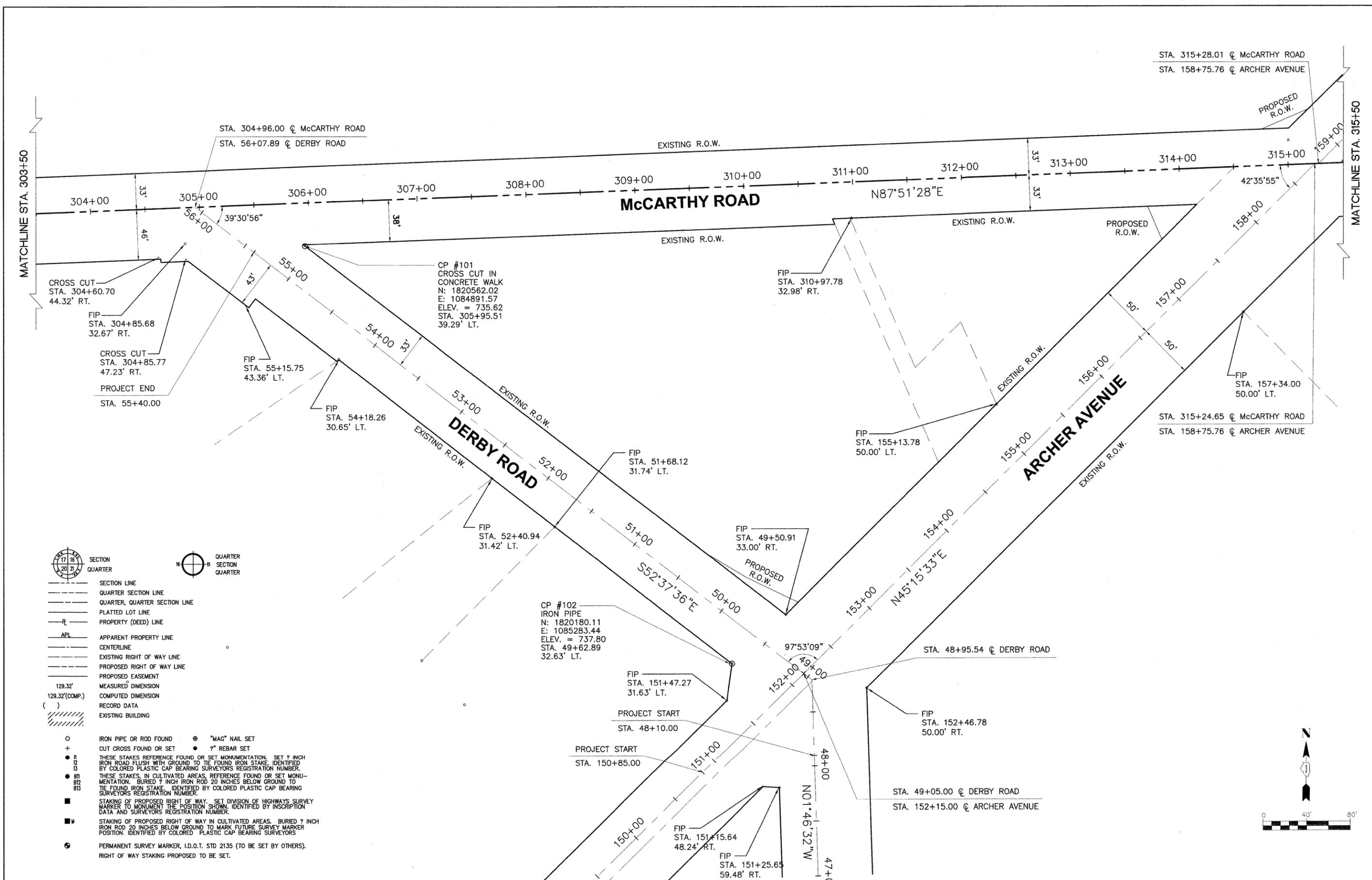
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
550 N. COOKS DR., STE 116 AURORA, IL 60504
License No. 184-000613

USER NAME =CMH	DESIGNED - CEW	REVISED -
PLOT SCALE =	DRAWN - CMH	REVISED -
PLOT DATE =03/11/10	CHECKED - CEW	REVISED -
	DATE - 03/11/10	REVISED -

**ILLINOIS DEPARTMENT OF TRANSPORTATION
ALIGNMENT AND TIES**

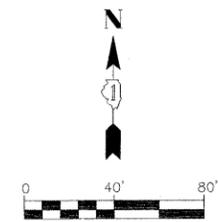
ROUTE: VILLAGE OF LEMONT/ McCARTHY ROAD	SECTION: 27	COUNTY: COOK	JOB#
SEC: 02-00055-00-WR	T 37 N, R 11 E OF 3RD P.M.	PROJECT#	
SCALE: 1"=50'	SHEET NO. 10 OF 79 SHEETS	STA	TO STA
		CONTRACT NO. 63557	

TOTAL SHEETS	SHEET NO.
84	11



- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- 129.32'(COMP.) COMPUTED DIMENSION
- () RECORD DATA
- EXISTING BUILDING

- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- ?" REBAR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET ? INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED ? INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED ? INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS). RIGHT OF WAY STAKING PROPOSED TO BE SET.



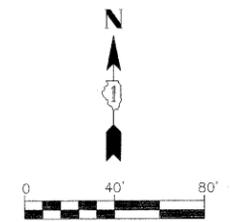
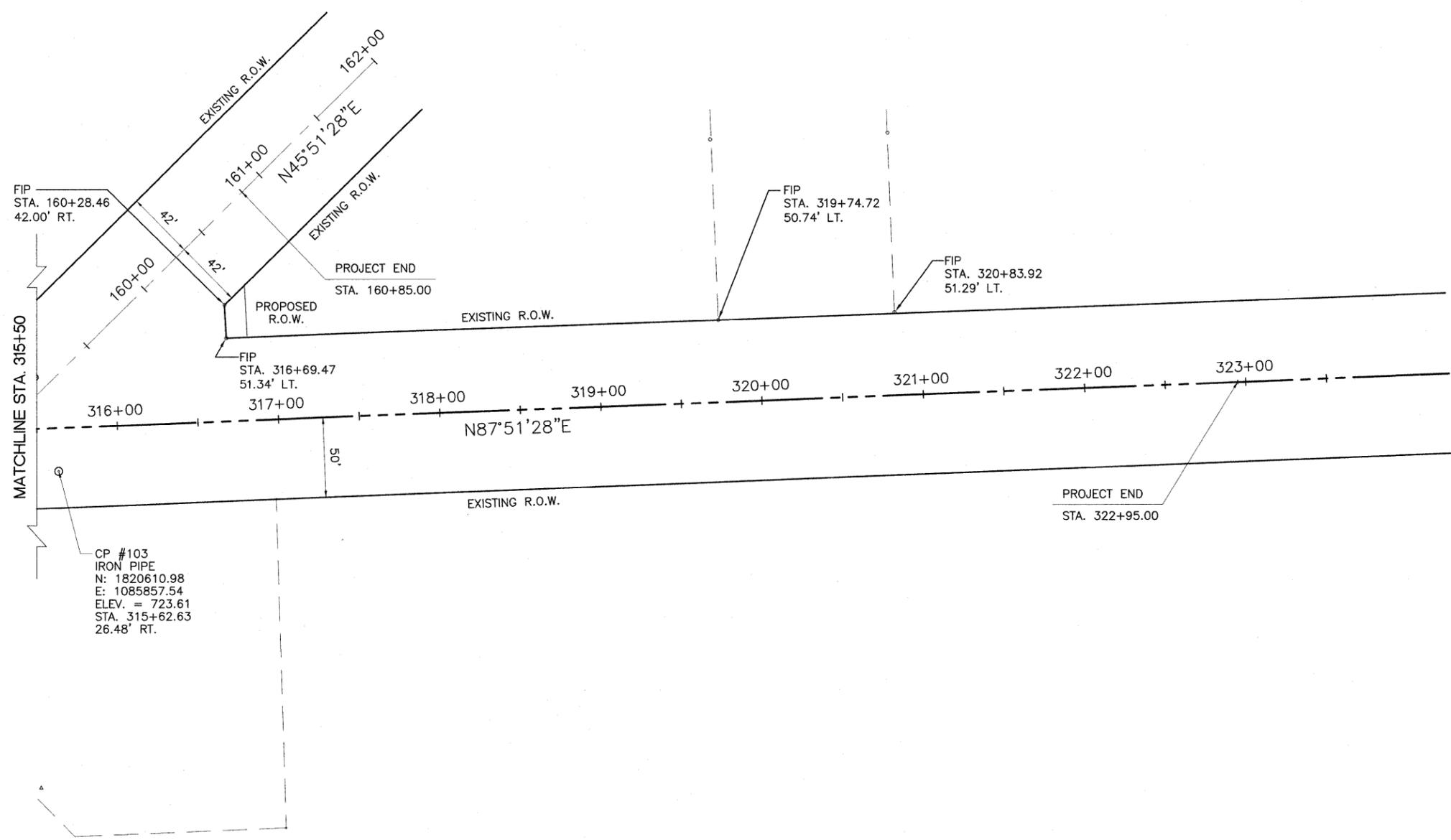
 CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS 550 N. COMMONS DR., STE 116 AURORA, IL 60504 License No. 184-000611	USER NAME =CMH	DESIGNED - CEW	REVISED -	ILLINOIS DEPARTMENT OF TRANSPORTATION ALIGNMENT AND TIES	ROUTE: VILLAGE OF LEMONT/ McCARTHY ROAD	SECTION: 27	COUNTY: COOK	JOB#
	PLOT SCALE =	DRAWN - CMH	REVISED -		SEC: 02-00055-00-WR	T 37 N , R 11 E OF 3RD P.M.	PROJECT#	
	PLOT DATE =03/11/10	CHECKED - CEW	REVISED -		SCALE: 1"=50'	SHEET NO. 10 OF 79 SHEETS	STA TO STA	CONTRACT NO. 63557

TOTAL SHEETS	SHEET NO.
84	12



- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- 129.32'(COMP.) COMPUTED DIMENSION
- () RECORD DATA
- EXISTING BUILDING

- IRON PIPE OR ROD FOUND ⊗ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET ● ?" REBAR SET
- II THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET ? INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- 12
- 13 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED ? INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- 811
- 812
- 813 STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED ? INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS
- PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS). RIGHT OF WAY STAKING PROPOSED TO BE SET.



CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 550 N. COMMONS DR., STE 116 AURORA, IL 60504
 (License No. 154-000613)

USER NAME =CMH	DESIGNED - CEW	REVISED -
PLOT SCALE =	DRAWN - CMH	REVISED -
PLOT DATE =03/11/10	CHECKED - CEW	REVISED -
	DATE - 03/11/10	REVISED -

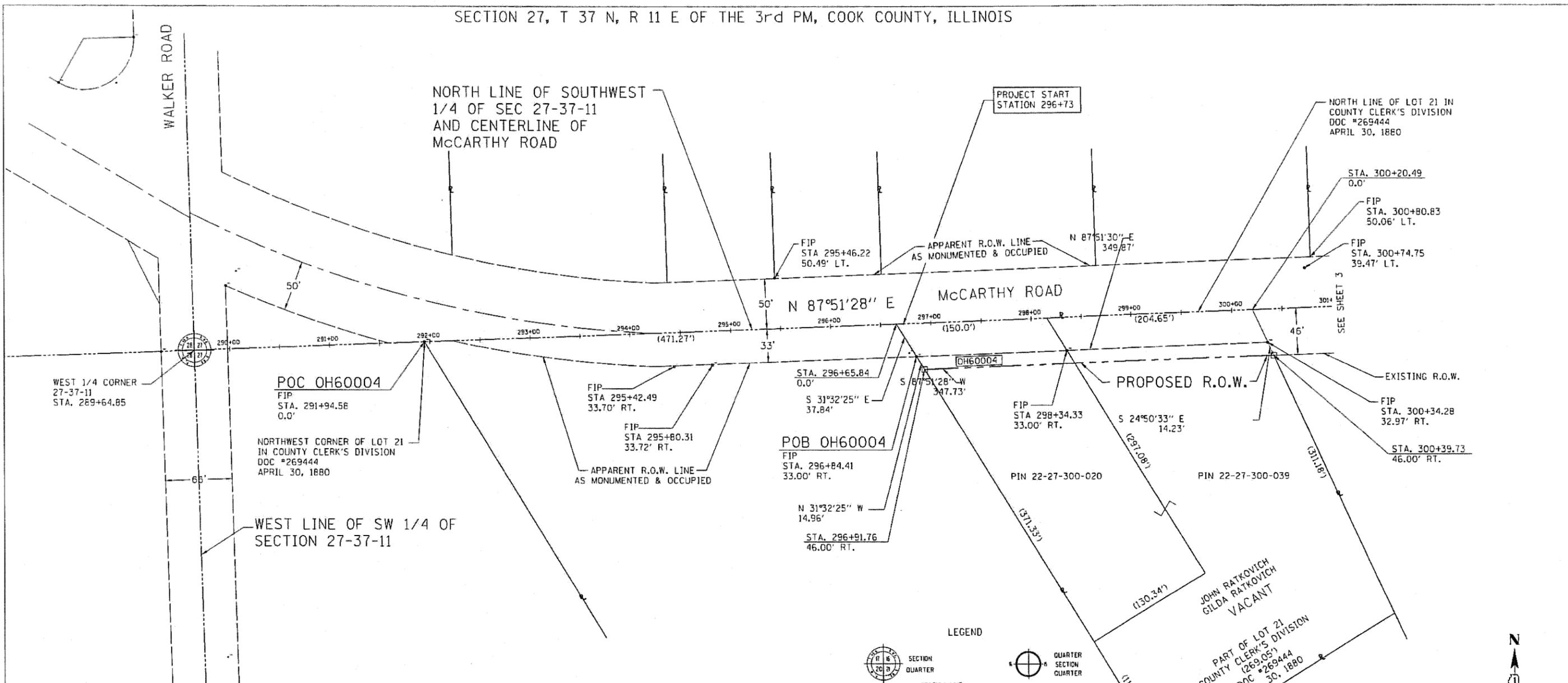
**ILLINOIS DEPARTMENT OF TRANSPORTATION
 ALIGNMENT AND TIES**

ROUTE: VILLAGE OF LEMONT/ MCGARTHY ROAD	SECTION: 27
SEC: 02-00055-00-WR	T 37 N, R 11 E OF 3RD P.M.
SCALE: 1"=50'	SHEET NO. 11 OF 79 SHEETS

COUNTY: COOK	JOB#
PROJECT#	
STA	TO STA
CONTRACT NO. 63557	

TOTAL SHEETS	SHEET NO.
84	13

SECTION 27, T 37 N, R 11 E OF THE 3rd PM, COOK COUNTY, ILLINOIS



STATE OF ILLINOIS)
) SS
 COUNTY OF DUPAGE)

THIS IS TO CERTIFY THAT I, COREY WORTHTEL, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 27, TOWNSHIP 37 NORTH, RANGE 11 EAST, OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED Sept 7, 2011

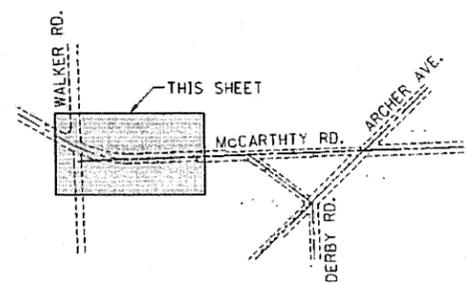
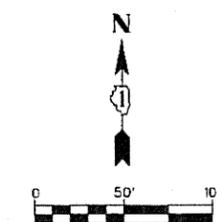
COREY E. WORTHTEL ILLINOIS PLS #3440
 MY LICENSE EXPIRES 11/30/2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



- LEGEND**
- SECTION QUARTER
 - QUARTER SECTION QUARTER
 - SECTION LINE
 - QUARTER SECTION LINE
 - QUARTER, QUARTER SECTION LINE
 - PLATTED LOT LINE
 - PROPERTY (DEED) LINE
 - APPARENT PROPERTY LINE
 - CENTERLINE
 - EXISTING RIGHT OF WAY LINE
 - PROPOSED RIGHT OF WAY LINE
 - PROPOSED EASEMENT
 - MEASURED DIMENSION
 - COMPUTED DIMENSION
 - RECORD DATA
 - EXISTING BUILDING

- IRON PIPE OR ROD FOUND
- CUT CROSS FOUND OR SET
- * MAG NAIL SET
- 7' REBAR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 7 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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- PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS).
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

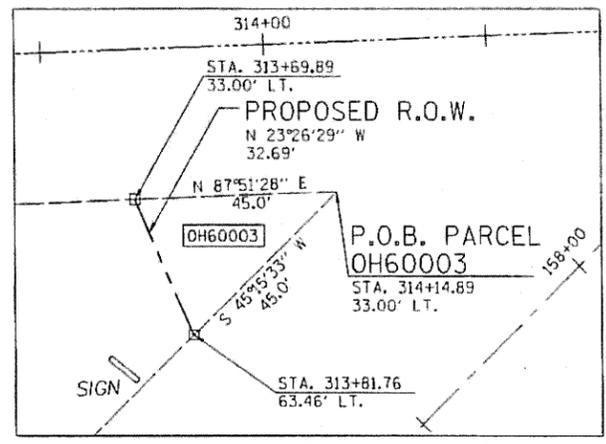
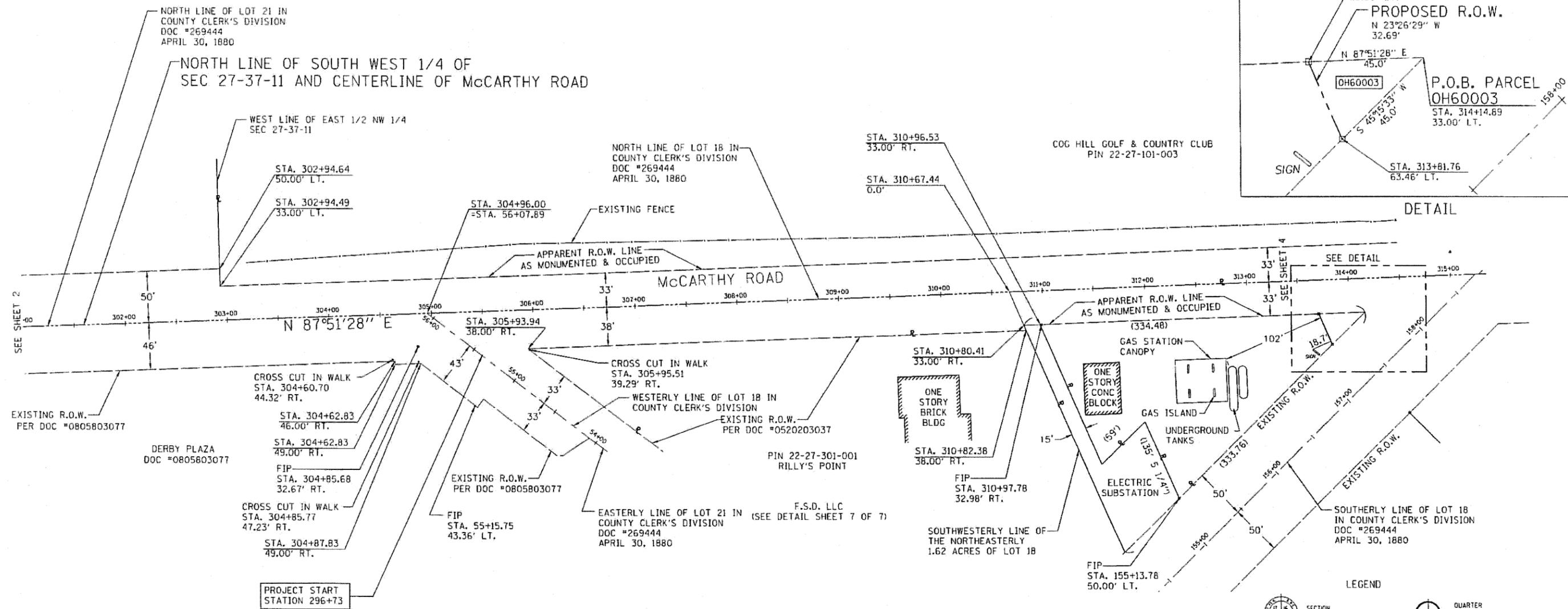


BASIS OF BEARING
 NORTH LINE OF
 SOUTH HALF OF
 SECTION 27-37-11

PARCEL NO.	OWNER	TOTAL HOLDING	R.O.W. REQUIRED	PREVIOUSLY DEDICATED	REMAINDER	PERMANANT TAX NUMBER	PROPERTY ACQUIRED BY
OH60004	JOHN RATKOVICH GILDA RATKOVICH	2.55 AC	0.10 AC	N/A	2.44 AC	22-27-300-020 22-27-300-039	

CMT CRAWFORD, MURPHY & TOLLY, INC. CONSULTING ENGINEERS 550 N. COMMONS DR., STE 100 AURORA, IL 60504 License No. 042-000033	USER NAME = CMH	DESIGNED - CEW	REVISED - 05/14/10	ILLINOIS DEPARTMENT OF TRANSPORTATION PLAT OF HIGHWAYS	ROUTE: VILLAGE OF LEMONT/ McCARTHY ROAD	SECTION: 27	COUNTY: COOK	JOB#
	PLDT SCALE =	DRAWN - CMH	REVISED - 05/24/10		SEC: 02-00055-00-WR	T 37 N, R 11 E OF 3RD P.M.	PROJECT# R-90-015-10	
PLDT DATE = 03/11/10	CHECKED - CEW	REVISED - 06/30/11	SCALE: 1"=50'		SHEET NO. 2 OF 7 SHEETS	STA 289+64.85 TO STA 301+00	CONTRACT NO. 63557	
	DATE - 03/11/10	REVISED - 07/14/11						

SECTION 27, T 37 N, R 11 E OF THE 3rd PM, COOK COUNTY, ILLINOIS

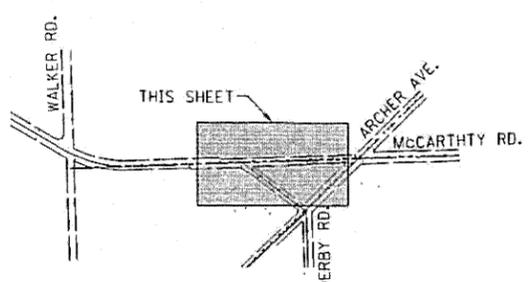


EXISTING R.O.W. PER DOC #0805803077

DERBY PLAZA DOC #0805803077

PROJECT START STATION 296+73

BASIS OF BEARING
NORTH LINE OF
SOUTH HALF OF
SECTION 27-37-11



KEYMAP



STATE OF ILLINOIS)
) SS
COUNTY OF DUPAGE)

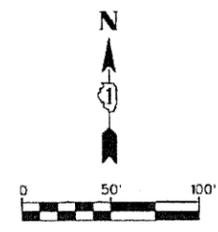
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DATED: *Sept 7, 2011*

COREY E. WORTHEL ILLINOIS PLS #3440
MY LICENSE EXPIRES 11/30/2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

- LEGEND
- SECTION QUARTER
 - QUARTER SECTION LINE
 - QUARTER, QUARTER SECTION LINE
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 - PROPERTY (DEED) LINE
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 - PROPOSED RIGHT OF WAY LINE
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 - MEASURED DIMENSION
 - COMPUTED DIMENSION
 - RECORD DATA
 - EXISTING BUILDING
 - IRON PIPE OR ROD FOUND
 - "MAG" NAIL SET
 - CUT CROSS FOUND OR SET
 - 2" REBAR SET
 - THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 2 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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 - PERMANENT SURVEY MARKER, I.D.D.T. STD 2135 (TO BE SET BY OTHERS).
 - RIGHT OF WAY STAKING PROPOSED TO BE SET.



PARCEL NO.	OWNER	TOTAL HOLDING	R.O.W. REQUIRED	PREVIOUSLY DEDICATED	PERMANENT EASEMENT AREA	EASEMENT PURPOSE	REMAINDER	PERMANENT TAX NUMBER	PROPERTY ACQUIRED BY
OH60003	TCF MANAGEMENT CORPORATION	0.68 AC	0.016 AC	N/A			0.66 AC	22-27-301-004	

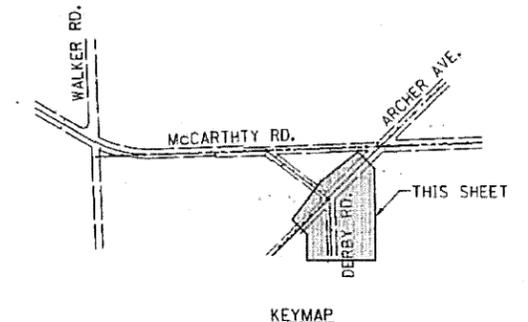
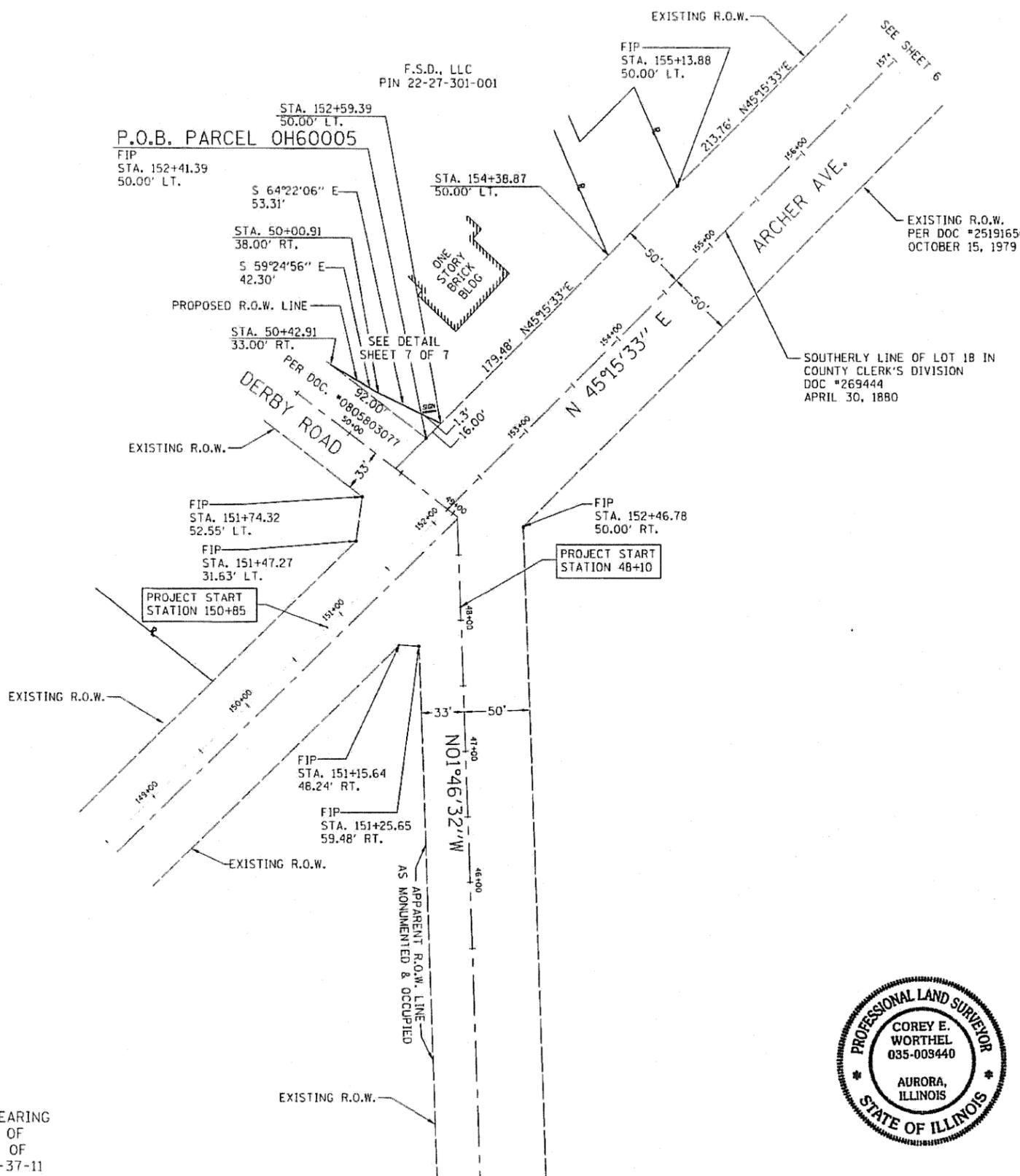
CMT
CRAWFORD, MURPHY & TOLLY, INC.
CONSULTING ENGINEERS
540 N. EDWARDS DR., STE. 86 AURORA, IL 60504
(708) 938-1111

USER NAME = CMH	DESIGNED - CEW	REVISED - 05/24/10
PLOT SCALE =	DRAWN - CMH	REVISED - 06/30/11
PLOT DATE = 03/11/10	CHECKED - CEW	REVISED - 07/14/11
	DATE - 03/11/10	REVISED - 08/07/11

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS

ROUTE: VILLAGE OF LEMONT / McCARTHY ROAD	SECTION: 27	COUNTY: COOK	JOB#
SEC: 02-00055-00-WR	T 37 N, R 11 E OF 3RD P.M.	PROJECT# R-90-015-10	
SCALE: 1"=50'	SHEET NO. 3 OF 7 SHEETS	STA 301+00 TO STA 315+00	CONTRACT NO. 63557

SECTION 27, T 37 N, R 11 E OF THE 3rd PM, COOK COUNTY, ILLINOIS



LEGEND

- SECTION QUARTER
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
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- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
) SS
 COUNTY OF DUPAGE)

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DATED: *Spt. 7, 2011*



COREY E. WORTHEL ILLINOIS PLS #3440
 MY LICENSE EXPIRES 11/30/2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

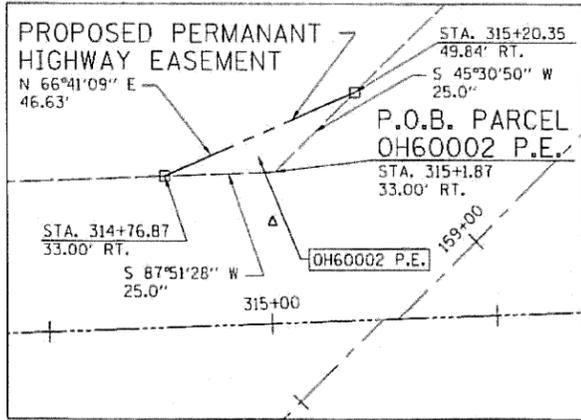
BASIS OF BEARING
 NORTH LINE OF
 SOUTH HALF OF
 SECTION 27-37-11

PARCEL NO.	OWNER	TOTAL HOLDING	R.O.W. REQUIRED	PREVIOUSLY DEDICATED	REMAINDER	PERMANANT TAX NUMBER	PROPERTY ACQUIRED BY
OH60005	CLARK STREET HOLDINGS LLC SERIES I	2.49 AC	0.015 AC	N/A	2.48 AC	22-27-301-001	

 CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS 550 N. COMINGS DR., STE 40 AURORA, IL 60504 LICENSE NO. 084-000612	USER NAME = CMH	DESIGNED - CEW	REVISED - 05/24/10	ILLINOIS DEPARTMENT OF TRANSPORTATION PLAT OF HIGHWAYS
	PLOT SCALE =	DRAWN - CMH	REVISED - 06/30/11	
	PLOT DATE = 03/11/10	CHECKED - CEW	REVISED - 07/14/11	
		DATE - 03/11/10	REVISED - 08/07/11	

ROUTE: VILLAGE OF LEMONT/ McCARTHY ROAD	SECTION: 27	COUNTY: COOK	JOB#
SEC: 02-00055-00-WR	T 37 N , R 11 E OF 3RD P.M.	PROJECT# R-90-015-10	
SCALE: 1"=50'	SHEET NO. 5 OF 7 SHEETS	STA 148+00 TO STA 157+00	CONTRACT NO. 63557

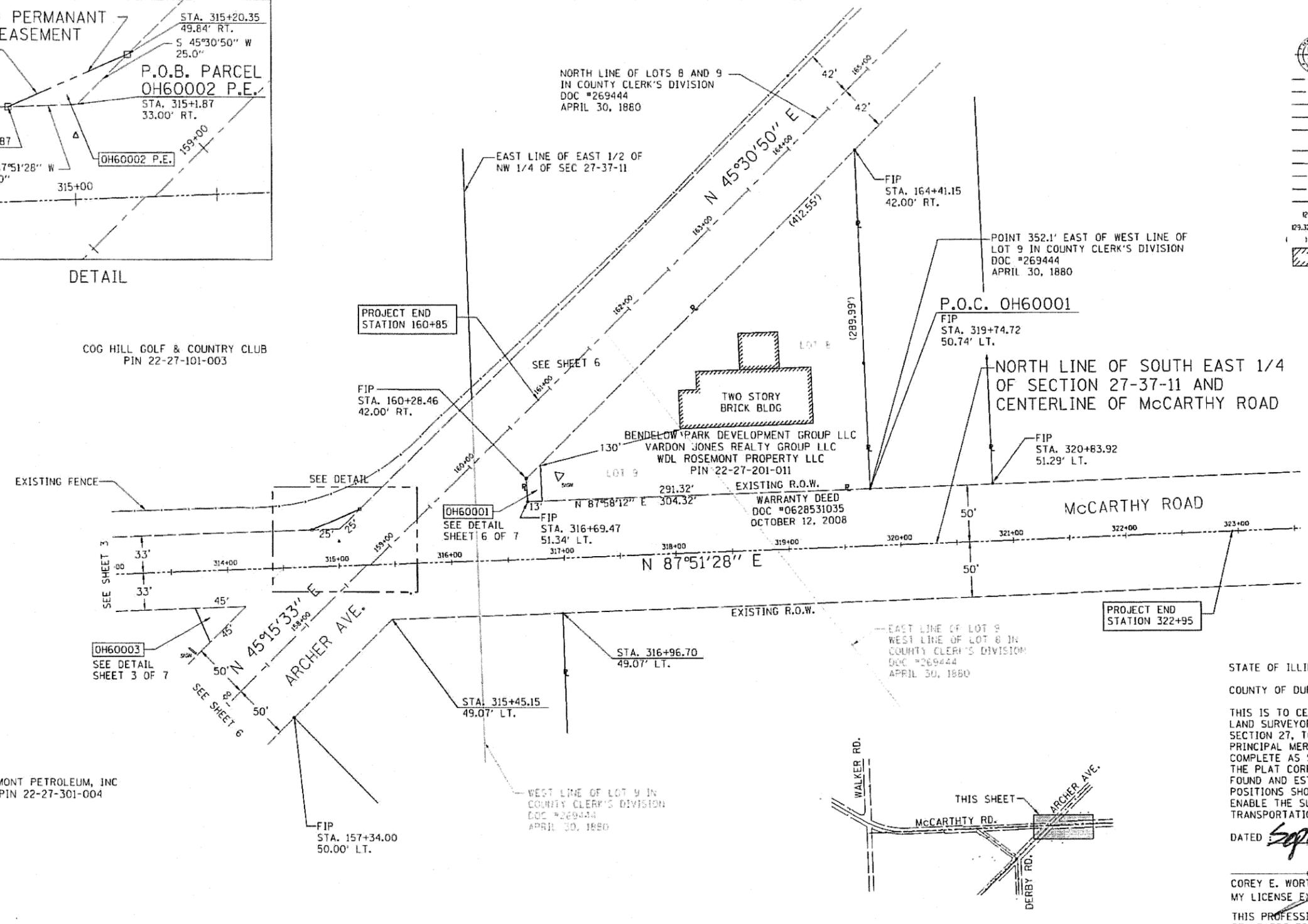
SECTION 27, T 37 N, R 11 E OF THE 3rd PM, COOK COUNTY, ILLINOIS



DETAIL

LEGEND

- SECTION QUARTER
- QUARTER SECTION QUARTER
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- RIGHT OF WAY STAKING PROPOSED TO BE SET.



STATE OF ILLINOIS)
) SS
 COUNTY OF DUPAGE)

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DATED Sept. 7, 2011

COREY E. WORTHEL ILLINOIS PLS #3440
 MY LICENSE EXPIRES 11/30/2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

KEYMAP

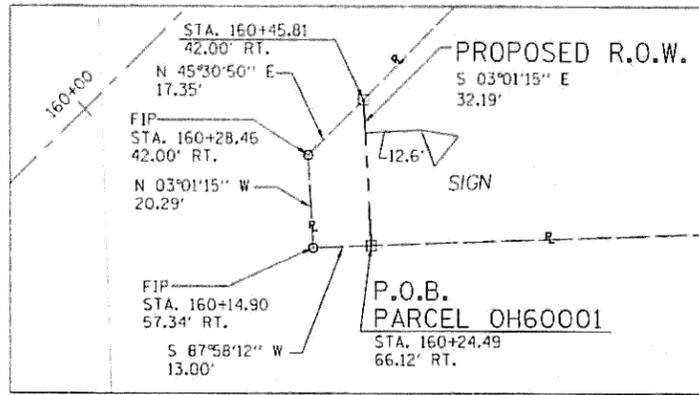
PARCEL NO.	OWNER	TOTAL HOLDING	R.O.W. REQUIRED	PREVIOUSLY DEDICATED	PERMANENT EASEMENT AREA	EASEMENT PURPOSE	REMAINDER	PERMANENT TAX NUMBER	PROPERTY ACQUIRED BY
OH60001	FIRST PERSONAL BANK, AN ILLINOIS STATE CHARTERED BANK	1.12 AC	0.008 AC	N/A			1.11 AC	22-27-201-011	
OH60002 P.E.	COOK GOLF PROPERTIES, INC.	79.73 AC	N/A	N/A	0.005 AC	HIGHWAY	79.73 AC	22-27-101-003 22-27-101-002	
OH60003	TCF MANAGEMENT CORPORATION	0.68 AC	0.016 AC	N/A			0.66 AC	22-27-301-004	

BASIS OF BEARING
 NORTH LINE OF
 SOUTH HALF OF
 SECTION 27-37-11



<p>CMT CRAWFORD, MURPHY & TILLY, INC. CIVIL ENGINEERS 550 N. COMMONS DR., STE 16 AURORA, IL 60504 (License No. 084-00054)</p>	USER NAME = CMH	DESIGNED - CEW	REVISED - 05/24/10	ILLINOIS DEPARTMENT OF TRANSPORTATION PLAT OF HIGHWAYS	ROUTE: VILLAGE OF LEMONT/ McCARTHY ROAD	SECTION: 27	COUNTY: COOK	JOB#	
	PLOT SCALE =	DRAWN - CMH	REVISED - 06/30/11		SEC: 02-00055-00-WR	T 37 N, R 11 E OF 3RD P.M.	PROJECT# R-90-015-10		
	PLOT DATE = 03/11/10	CHECKED - CEW	REVISED - 07/14/11		SCALE: 1"=50'	SHEET NO. 4 OF 7 SHEETS	STA 313+00 TO STA 321+50	CONTRACT NO. 63557	
	DATE = 03/11/10	REVISOR - 08/07/11						TOTAL SHEETS 84 SHEET NO. 17	

SECTION 27, T 37 N, R 11 E OF THE 3rd PM, COOK COUNTY, ILLINOIS

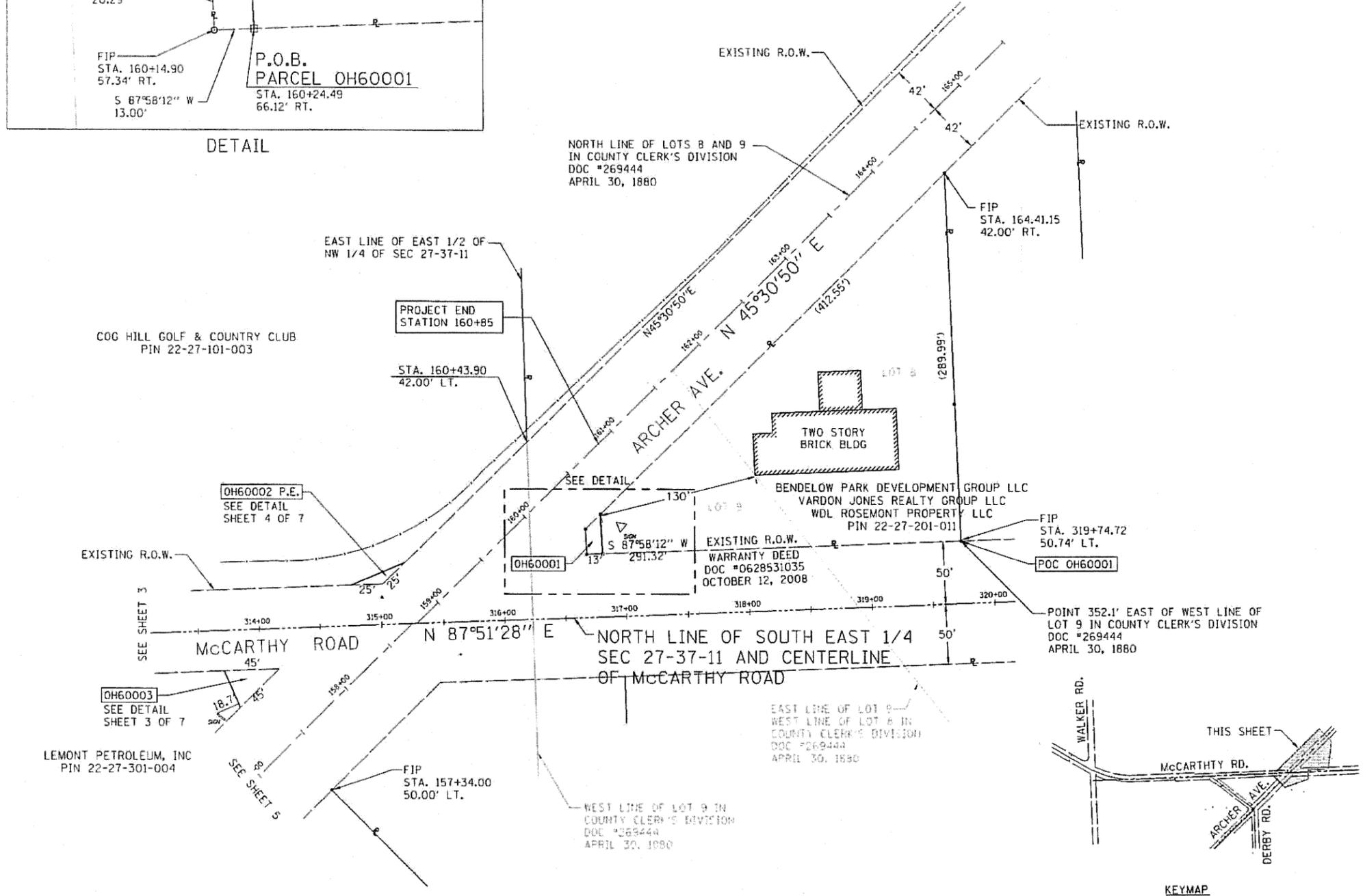


DETAIL

LEGEND

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PERMANENT SURVEY MARKER, I.D.O.I. STD 2035 (TO BE SET BY OTHERS).
RIGHT OF WAY STAKING PROPOSED TO BE SET.



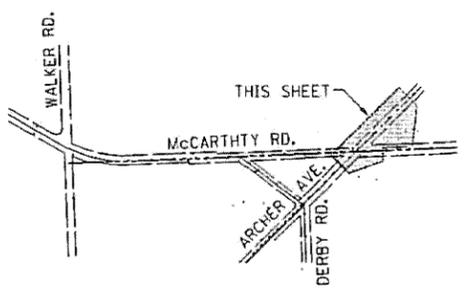
STATE OF ILLINOIS)
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COUNTY OF DUPAGE)

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DATED: Sept. 7, 2011

COREY E. WORTHTEL ILLINOIS PLS #3440
MY LICENSE EXPIRES 11/30/2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



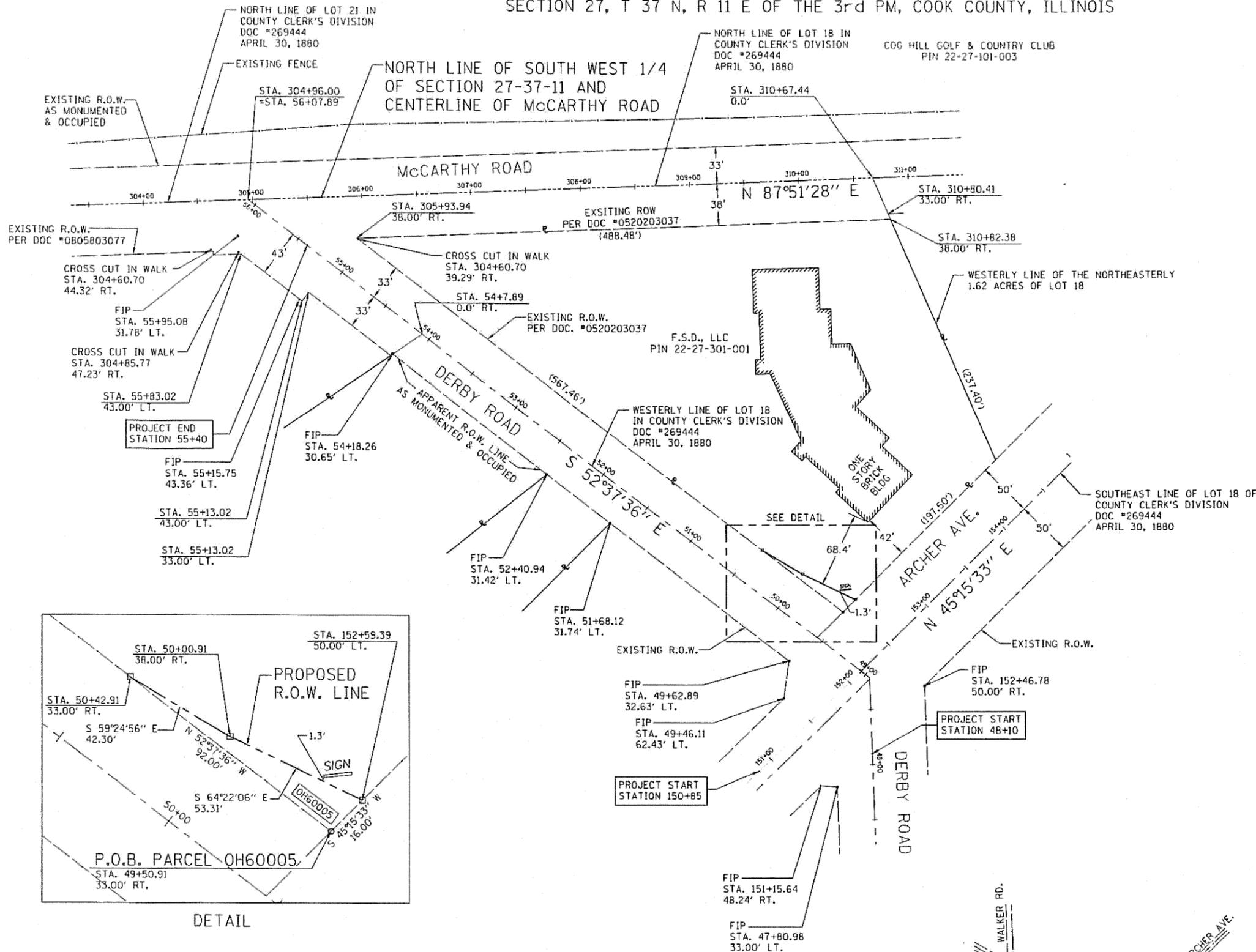
PARCEL NO.	OWNER	TOTAL HOLDING	R.O.W. REQUIRED	PREVIOUSLY DEDICATED	PERMANANT EASEMENT AREA	EASEMENT PURPOSE	REMAINDER	PERMANANT TAX NUMBER	PROPERTY ACQUIRED BY
OH60001	FIRST PERSONAL BANK, AN ILLINOIS STATE CHARTERED BANK	1.12 AC	0.008 AC	N/A			1.11 AC	22-27-201-011	
OH60002 P.E.	COOK GOLF PROPERTIES, INC.	79.73 AC	N/A	N/A	0.005 AC	HIGHWAY	79.73 AC	22-27-101-003	
OH60003	TCF MANAGEMENT CORPORATION	0.68 AC	0.016 AC	N/A			0.66 AC	22-27-301-004	

BASIS OF BEARING
NORTH LINE OF
SOUTH HALF OF
SECTION 27-37-11



<p>CMT CRANFORD, MURPHY & TOLLY, INC. CONSULTING ENGINEERS 150 N. COMMONS DR., STE 16 AURORA, IL 60504 (License No. 044-000001)</p>	USER NAME = CMH	DESIGNED - CEW	REVISED - 05/24/10	<p align="center">ILLINOIS DEPARTMENT OF TRANSPORTATION PLAT OF HIGHWAYS</p>	ROUTE: VILLAGE OF LEMONT/ McCARTHY ROAD	SECTION: 27	COUNTY: COOK	JOB#	
	PLLOT SCALE =	DRAWN - CMH	REVISED - 06/30/11		SEC: 02-00055-00-WR	T 37 N , R 11 E OF 3RD P.M.	PROJECT# R-90-015-10		
	PLLOT DATE = 03/11/10	CHECKED - CEW	REVISED - 07/14/11		SCALE: 1"=50'	SHEET NO. 6 OF 7 SHEETS	STA 157+00 TO STA 166+00	CONTRACT NO. 63557	
		DATE - 03/11/10	REVISED - 08/07/11					TOTAL SHEET NO. 84	

SECTION 27, T 37 N, R 11 E OF THE 3rd PM, COOK COUNTY, ILLINOIS



LEGEND

SECTION QUARTER: [Symbol]

QUARTER SECTION QUARTER: [Symbol]

SECTION LINE: [Symbol]

QUARTER SECTION LINE: [Symbol]

QUARTER, QUARTER SECTION LINE: [Symbol]

PLATTED LOT LINE: [Symbol]

PROPERTY (DEED) LINE: [Symbol]

APL: [Symbol]

APPARENT PROPERTY LINE: [Symbol]

CENTERLINE: [Symbol]

EXISTING RIGHT OF WAY LINE: [Symbol]

PROPOSED RIGHT OF WAY LINE: [Symbol]

PROPOSED EASEMENT: [Symbol]

MEASURED DIMENSION: [Symbol]

COMPUTED DIMENSION: [Symbol]

RECORD DATA: [Symbol]

EXISTING BUILDING: [Symbol]

IRON PIPE OR ROD FOUND: [Symbol]

CUT CROSS FOUND OR SET: [Symbol]

THESE STAKES REFERENCE FOUND OR SET MONUMENTATION, SET 2" INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION, BURIED 2" INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 2" INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS

PERMANENT SURVEY MARKER, I.D.D.T. STD 2035 (TO BE SET BY OTHERS).

RIGHT OF WAY STAKING PROPOSED TO BE SET: [Symbol]

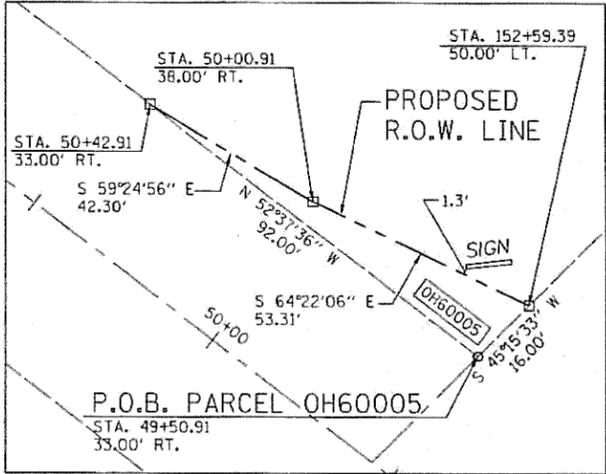
IRON PIPE OR ROD FOUND: [Symbol]

'MAG' NAIL SET: [Symbol]

2" REBAR SET: [Symbol]

Scale: 0 50' 100'

North Arrow



STATE OF ILLINOIS)
) SS
 COUNTY OF DUPAGE)

THIS IS TO CERTIFY THAT I, COREY WORTHEL, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 27, TOWNSHIP 37 NORTH, RANGE 11 EAST, OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED: Sept. 7, 2011

COREY E. WORTHEL ILLINOIS PLS #3440
 MY LICENSE EXPIRES 11/30/2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

BASIS OF BEARING
 NORTH LINE OF
 SOUTH HALF OF
 SECTION 27-37-11

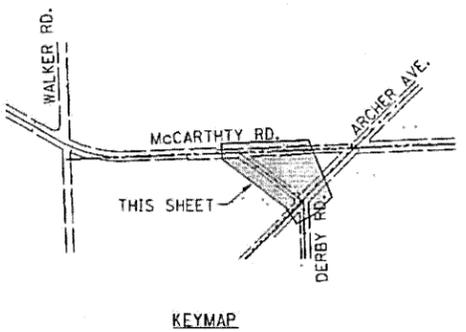
PARCEL NO.	OWNER	TOTAL HOLDING	R.O.W. REQUIRED	PREVIOUSLY DEDICATED	REMAINDER	PERMANENT TAX NUMBER	PROPERTY ACQUIRED BY
OH60005	CLARK STREET HOLDINGS LLC SERIES 1	2.49 AC	0.015 AC	N/A	2.48 AC	22-27-301-001	

CMT
 CHRYSTIE, MURPHY & TOLLY, INC.
 CONSULTING ENGINEERS
 350 N. COMMONS DR., STE 16 AURORA, IL 60004
 License No. 042-00063

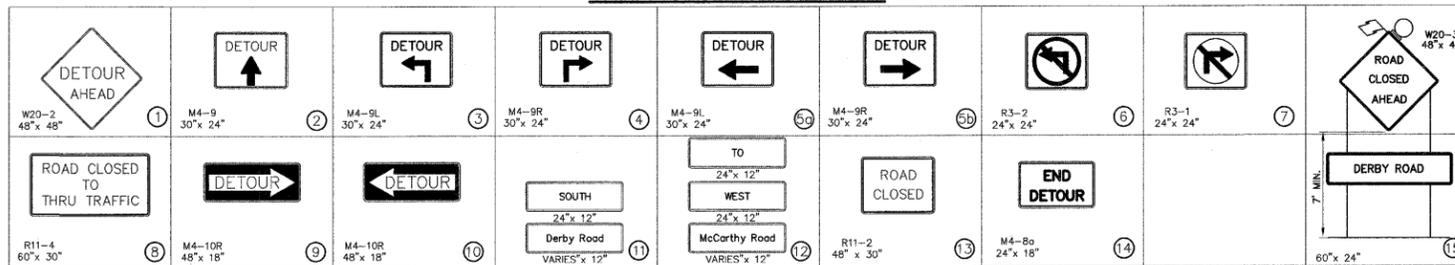
USER NAME = CMH	DESIGNED - CEW	REVISED - 05/24/10
PLOT SCALE =	DRAWN - CMH	REVISED - 06/30/11
PLOT DATE = 03/11/10	CHECKED - CEW	REVISED - 07/14/11
DATE = 03/11/10	DATE = 03/11/10	REVISED - 08/07/11

ILLINOIS DEPARTMENT OF TRANSPORTATION
 PLAT OF HIGHWAYS

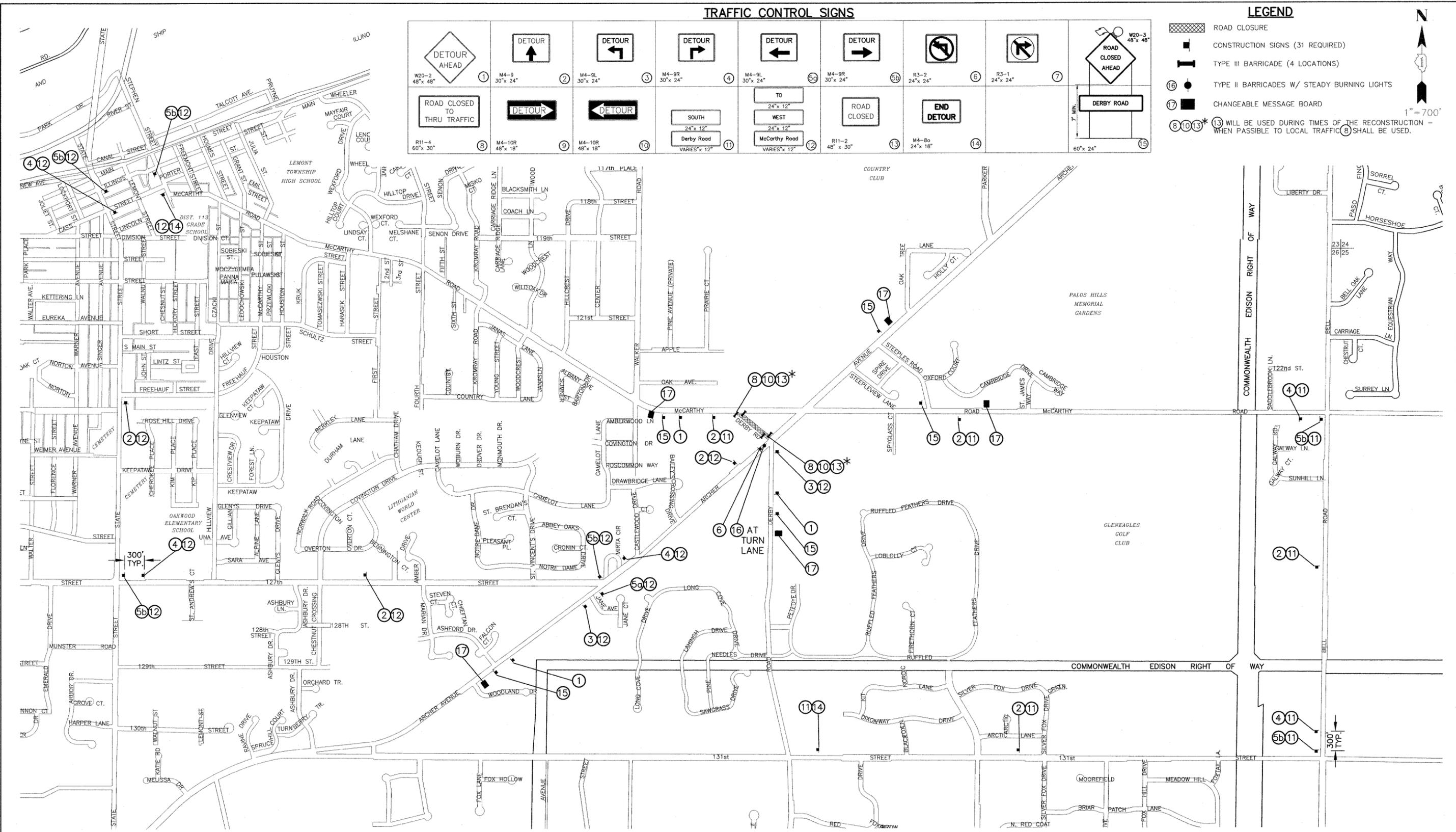
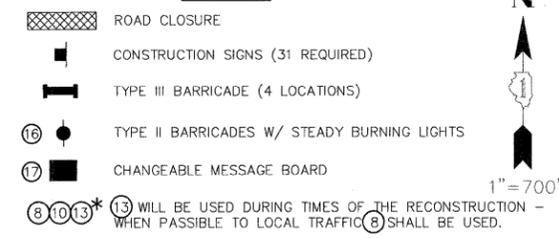
ROUTE: VILLAGE OF LEMONT/McCARTHY ROAD	SECTION: 27	COUNTY: COOK	JOB#
SEC: 02-00055-00-WR	T 37 N, R 11 E OF 3RD P.M.	PROJECT# R-90-015-10	
SCALE: 1"=50'	SHEET NO. 7 OF 7 SHEETS	STA 48+00 TO STA 57+00	CONTRACT NO. 63557



TRAFFIC CONTROL SIGNS



LEGEND



IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.



FILE NAME = FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED = THK	REVISED = THK 2-25-11
#05027	PLOT SCALE =	DRAWN = JFP-JEP	REVISED = THK 9-21-11
	PLOT DATE =	CHECKED = THK	REVISED =
		DATE = 11/10	REVISED =

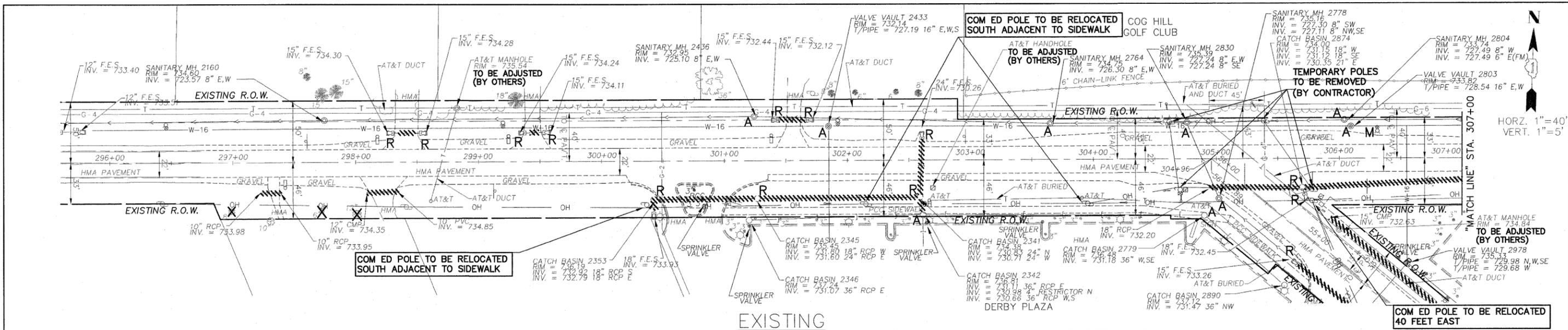
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLAN:
TRAFFIC CONTROL AND DETOUR PLAN
FOR CLOSURE OF DERBY ROAD**

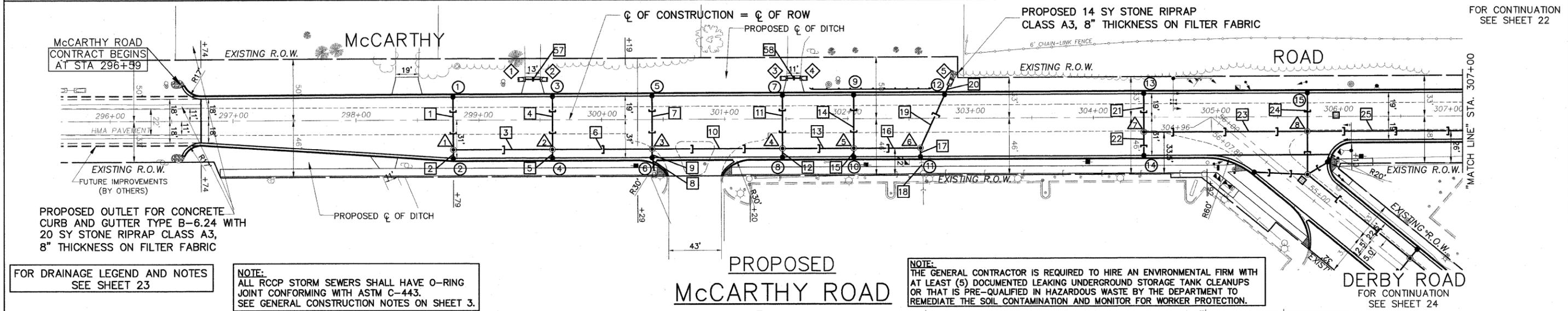
SCALE: AS NOTED SHEET NO. OF SHEETS STA. TO STA.

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Civil Engineers
835 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 987-9940 • Fax: (630) 987-0133
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00029

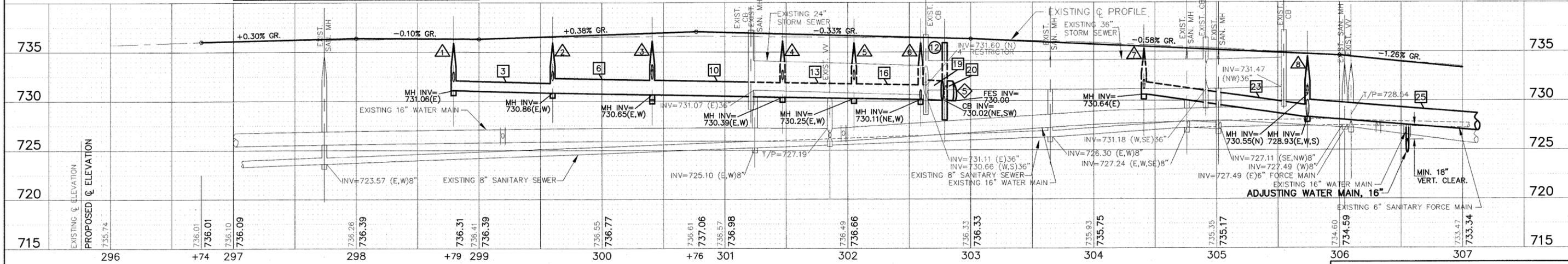
F.A. R.T.E. VAR	SECTION 02-00055-00-WR	COUNTY COOK	TOTAL SHEETS 84	SHEET NO. 20
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



EXISTING



PROPOSED
McCARTHY ROAD



IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9



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 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00028

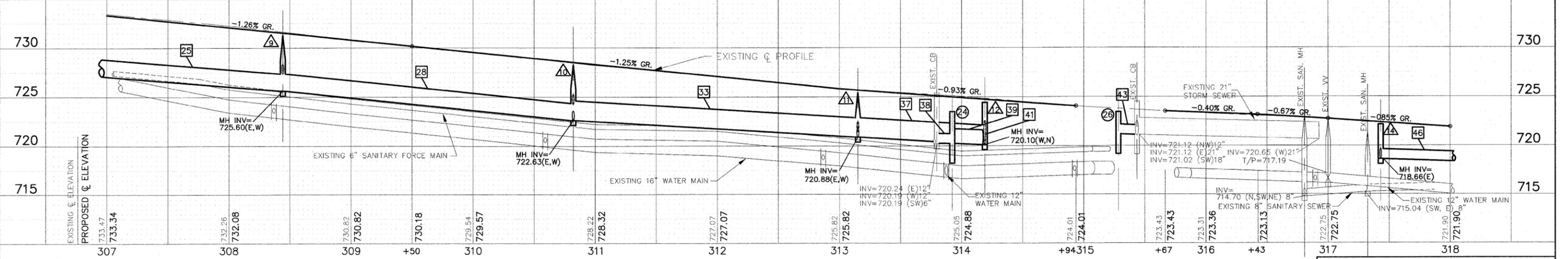
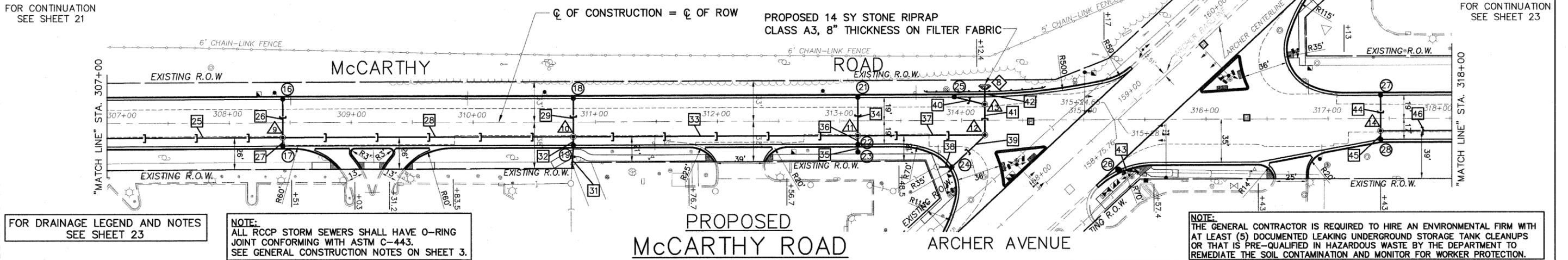
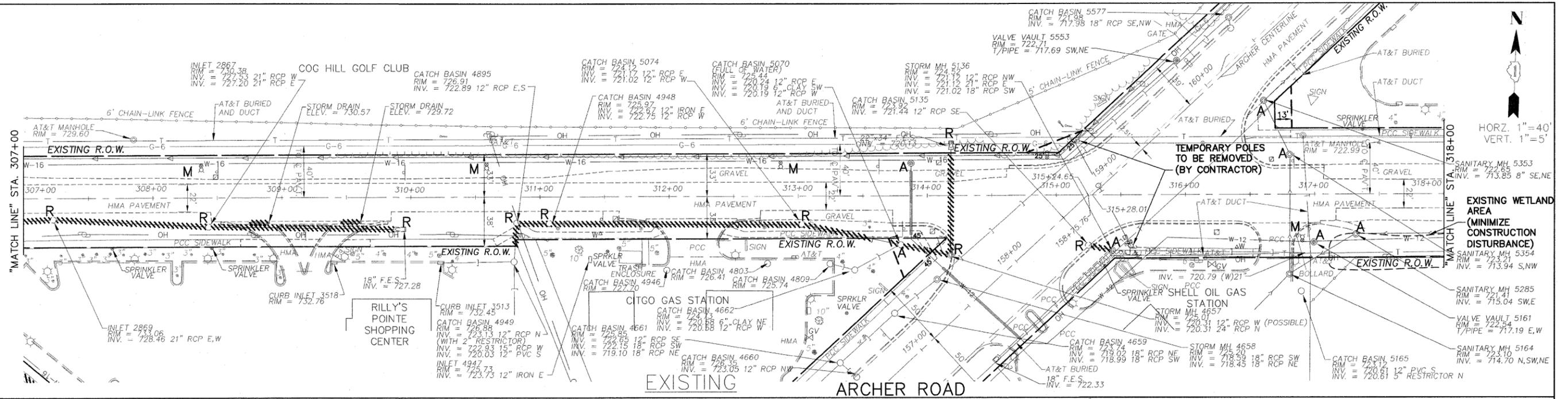
FILE NAME FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED - JLC	REVISED - THK 3-17-11
	PLOT SCALE =	DRAWN - JFP-JEP	REVISED - THK 9-21-11
	PLOT DATE =	CHECKED - THK	REVISED - THK 10-31-11
		DATE - 11/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
McCARTHY ROAD- (EXISTING AND PROPOSED ROADWAY DRAINAGE PLAN)

SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. 296+74 TO STA. 307+00

F.A. RTE. VAR	SECTION 02-00055-00-WR	COUNTY COOK	TOTAL SHEETS 84	SHEET NO. 21
CONTRACT NO. 63557			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)	



IMPORTANT!
 FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES.
 REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION
 OF NEW PAVEMENT WORK
 SEE SHEET 9

FILE NAME = FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED -- JLC	REVISED -- THK 3-17-11
PLOT SCALE =	CHECKED -- THK	DRAWN -- JFP-JEP	REVISED -- THK 9-21-11
PLOT DATE =	DATE -- 11/10		

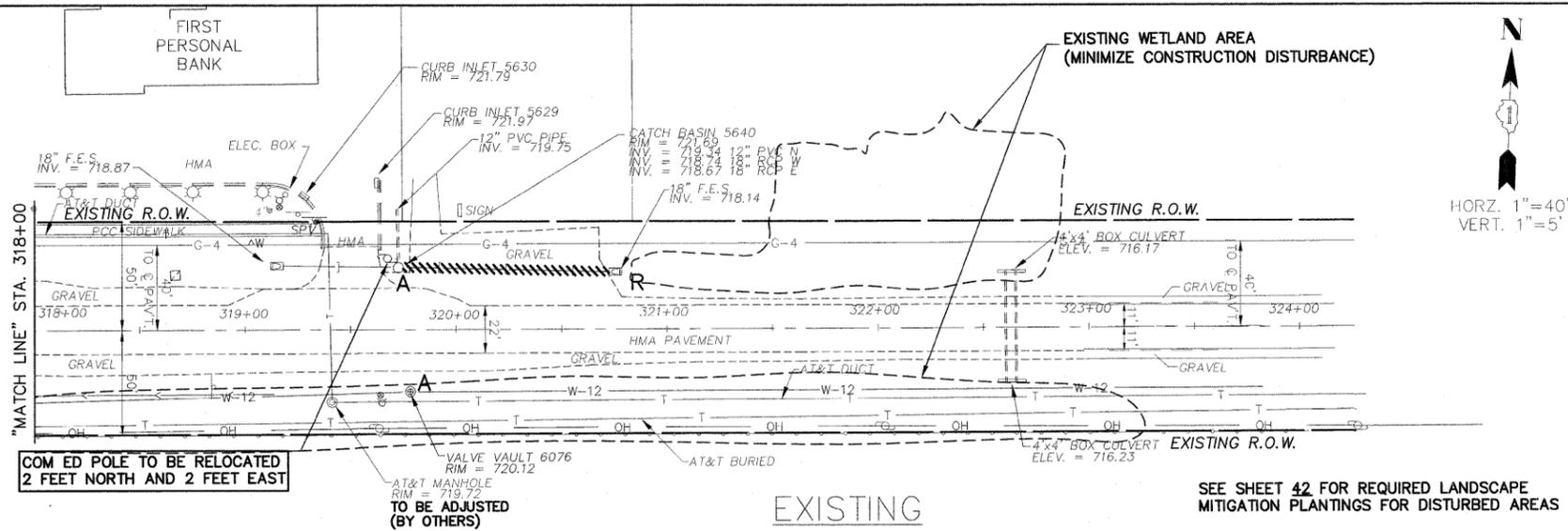
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
 McCARTHY ROAD- (EXISTING AND PROPOSED ROADWAY DRAINAGE PLAN)
 SCALE: AS NOTED SHEET NO. OF SHEETS STA. 307+00 TO STA. 318+00



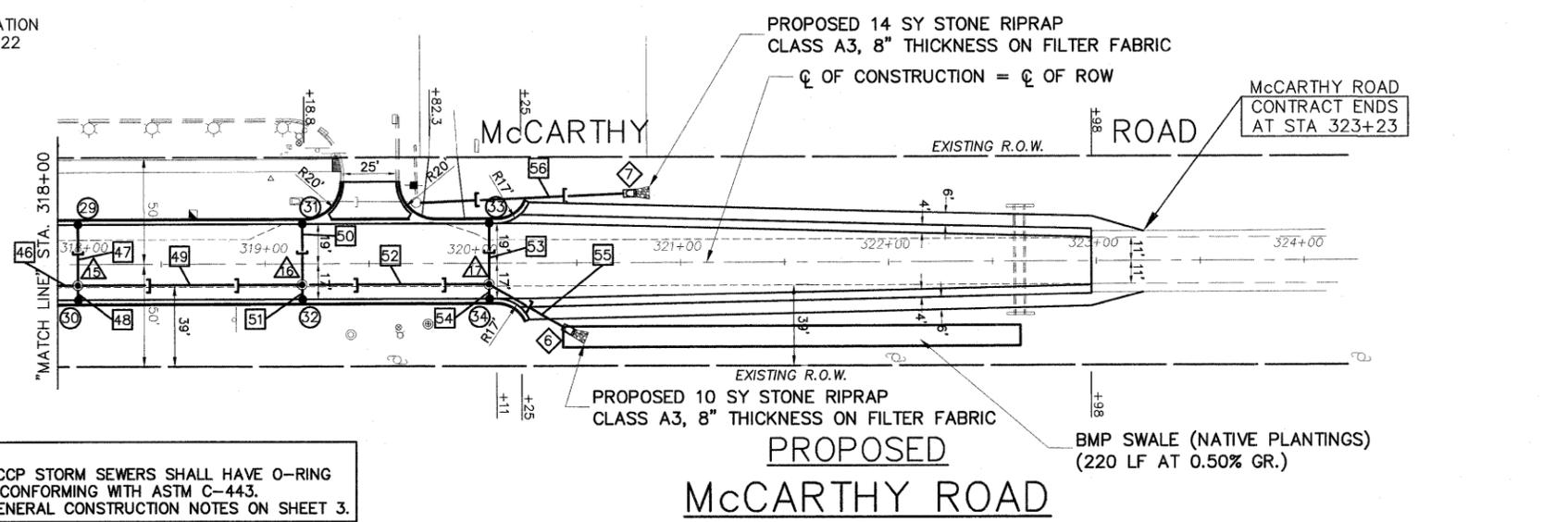
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 835 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-9640 • Fax: (630) 887-0132
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00089

F.A. R.T.E. VAR	SECTION 02-00055-00-WR	COUNTY COOK	TOTAL SHEETS 84	SHEET NO. 22
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



- ### DRAINAGE LEGEND
- EXISTING STORM SEWER OR PIPE CULVERT TO BE REMOVED
 - EXISTING CATCH BASIN TO BE REMOVED
 - EXISTING MANHOLE TO BE REMOVED
 - EXISTING FLARED END SECTION TO BE REMOVED
 - EXISTING FIRE HYDRANT TO BE MOVED, 5' B/C
 - EXISTING CATCH BASIN TO BE ADJUSTED
 - EXISTING MANHOLE TO BE ADJUSTED
 - EXISTING VALVE VAULT TO BE ADJUSTED
 - DOMESTIC SERVICE BOX (W) WATER (G) GAS TO BE ADJUSTED
 - PROPOSED STORM SEWER
 - PROPOSED CATCH BASIN
 - PROPOSED MANHOLE
 - PROPOSED PRECAST REINFORCED CONCRETE FLARED END SECTION WITH GRATING FOR CONCRETE FLARED END SECTION
 - RELOCATED FIRE HYDRANT

FOR CONTINUATION SEE SHEET 22

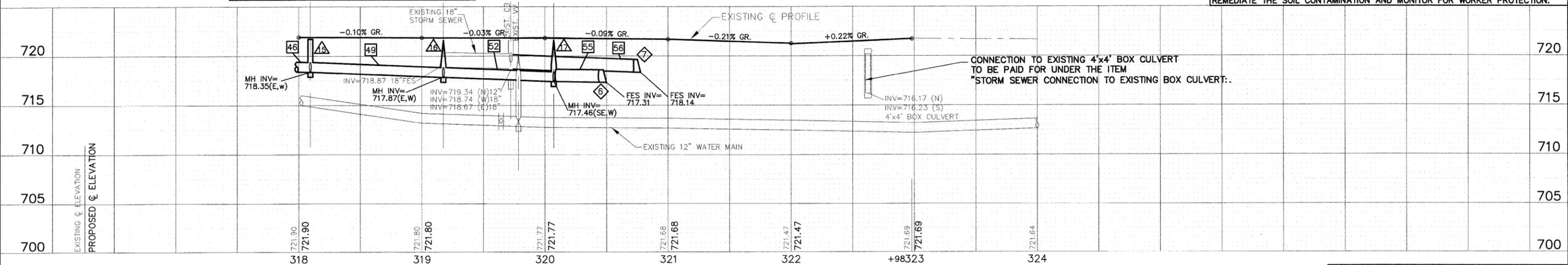


FOR DRAINAGE LEGEND AND NOTES SEE RIGHT

NOTE:
ALL RCCP STORM SEWERS SHALL HAVE O-RING JOINT CONFORMING WITH ASTM C-443. SEE GENERAL CONSTRUCTION NOTES ON SHEET 3.

NOTE:
THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

FOR TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLANS SEE SHEETS 52 THRU 69



IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9



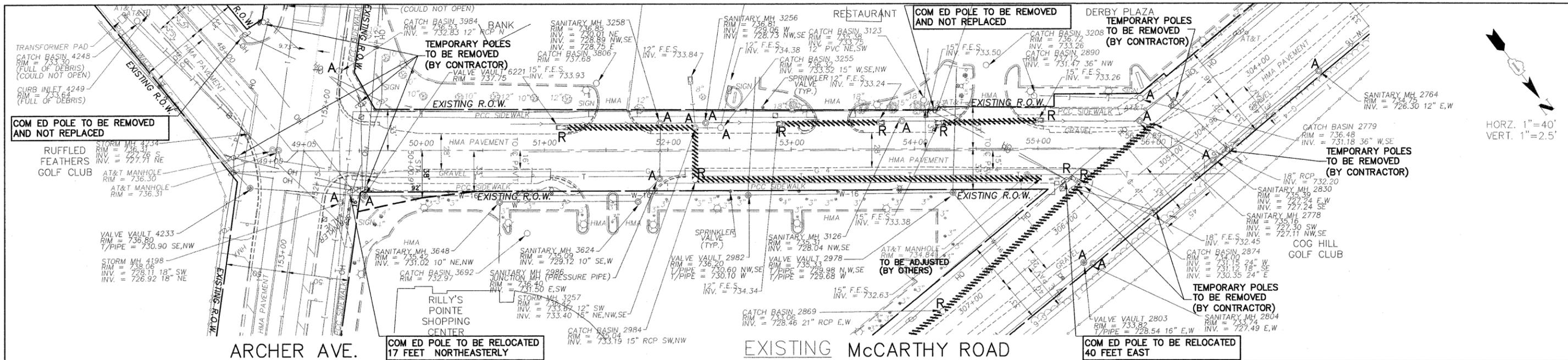
Frank Novotny & Associates, Inc.
Civil Engineers
Municipal Consultants
835 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 687-9640 • Fax: (630) 687-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00828

FILE NAME = FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD)	USER NAME =	DESIGNED - JLC	REVISED - THK 3-17-11
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	PLOT SCALE =	DRAWN - JFP-JEP	REVISED - THK 9-21-11
	PLOT DATE =	CHECKED - THK	REVISED -
		DATE - 11/10	REVISED -

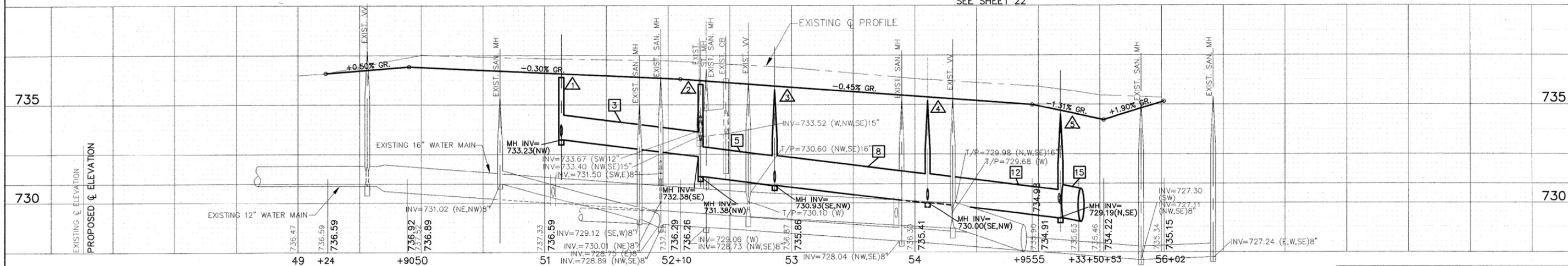
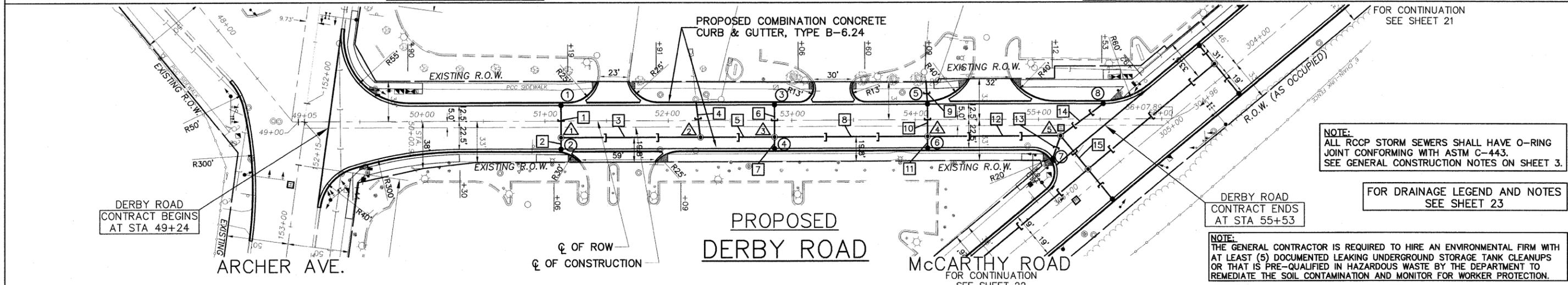
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
McCARTHY ROAD- (EXISTING AND PROPOSED ROADWAY DRAINAGE PLAN)
SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. 318+00 TO STA. 323+24

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	23
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

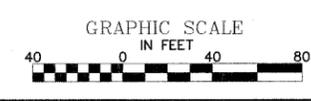


HORZ. 1"=40'
VERT. 1"=2.5'



IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 10



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Municipal Consultants
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ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000228

FILE NAME = FAU 1587 (McCarthy Road)
FAP 577 (Archer Avenue)
FAU 1024 (Derby Road)
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION

USER NAME =
DESIGNED = JLC
DRAWN = JFP-JEP
CHECKED = THK
PLOT SCALE =
PLOT DATE =

DESIGNED = JLC
DRAWN = JFP-JEP
CHECKED = THK
DATE = 11/10

REVISED = THK 2-25-11
REVISED = THK 9-21-11
REVISED =
REVISED =

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
DERBY ROAD- (EXISTING AND PROPOSED ROADWAY DRAINAGE PLAN)
SCALE: AS NOTED SHEET NO. OF SHEETS STA 49+24 TO STA 55+53

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	24
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

STRUCTURE ADJUSTMENTS

LOCATION	STATION	OFFSET (FT)	LT/RT	EXISTING STRUCTURE	EXISTING RIM ELEVATION	CATCH BASINS TO BE ADJUSTED 60250200 EACH	MANHOLES TO BE ADJUSTED 60255500 EACH	VALVE VAULTS TO BE ADJUSTED 60265700 EACH	HANDHOLE TO BE ADJUSTED 81400115 EACH
McCarthy Road	301+24	36	LT	San MH	732.95		1		
McCarthy Road	301+85	28	LT	VV	732.14			1	
McCarthy Road	302+61	45	RT	CB	736.81	1			
McCarthy Road	303+65	29.5	LT	San MH	734.75		1		
McCarthy Road	304+76	29.5	LT	San MH	735.39	1			
McCarthy Road	304+90	46	RT	CB	736.48				
McCarthy Road	305+02	32	RT	San MH	735.16		1		
McCarthy Road	306+04	31.5	LT	San MH	733.74		1		
McCarthy Road	309+09	26	LT	VV	733.82			1	
McCarthy Road	313.79	38	RT	CB	725.44	1			
McCarthy Road	313.87	25	LT	VV	723.2			1	
McCarthy Road	315+43	43	RT	ST MH	724.52		1		
McCarthy Road	316+61	72.5	LT	San MH	722.65		1		
McCarthy Road	316+81	31	LT	San MH	723.21		1		
McCarthy Road	317+00	37	RT	VV	722.54			1	
McCarthy Road	317+32	31	RT	San MH	721.41		1		
McCarthy Road	319+73	29	LT	CB	721.69	1			
McCarthy Road	319+79	29	RT	VV	720.12			1	
Derby Road	49+33	63	LT	CB	736.53	1			
Derby Road	49+35	42	RT	ST MH	738.08		1		
Derby Road	49+54	36.3	RT	VV	737.75			1	
Derby Road	51+94	23	RT	San MH	735.09		1		
Derby Road	51+94	21	LT	San MH	736.85		1		
Derby Road	52+22	17	LT	ST MH	736.42		1		
Derby Road	52+31	21	LT	San MH	736.81		1		
Derby Road	52+43	17	LT	CB	736.32	1			
Derby Road	53+90	22	LT	San MH	735.31		1		
TOTALS						6	15	6	0

SEE TRAFFIC SIGNAL REMOVAL SHEETS

NOTE:
ALL "PRECAST REINFORCED CONCRETE FLARED END SECTIONS"
SHALL ALSO BE CONSTRUCTED WITH
"GRATING FOR CONCRETE FLARED END SECTIONS"

INLET FILTER 28000500					
LOCATION	STATION	OFFSET (FT)	LT/RT	28000500	INLET FILTER
McCarthy Road	302+61	45	RT	1	
McCarthy Road	304+90	46	RT	1	
McCarthy Road	313+15	25.5	RT	1	
McCarthy Road	313+79	38	RT	1	
McCarthy Road	319+73	29	LT	1	
Derby Road	52+43	17	LT	1	
TOTALS				6	

PROPOSED CATCH BASINS

LOCATION	STRUCTURE NUMBER	STATION	OFFSET (FT)	LT/RT	RIM ELEVATION	INVERT ELEVATION	INVERT ELEVATION	CATCH BASINS, TYPE 1 FRAME 4'-DIAMETER OPEN LID 60200105 EACH	CATCH BASINS, TYPE 1 FRAME 2'-DIAMETER OPEN LID 60206905 EACH
McCarthy Road	1	298+79	19.57	LT	735.79	732.29			1
McCarthy Road	2	298+79	32	RT	735.54	732.04			1
McCarthy Road	3	299+60	19.83	LT	735.91	732.41			1
McCarthy Road	4	299+60	32	RT	735.85	732.17			1
McCarthy Road	5	300+41	20	LT	736.03	732.53			1
McCarthy Road	6	300+41	32	RT	736.16	732.29 N	733.85 S	1	
McCarthy Road	7	301+47	20	LT	736.00	732.50			1
McCarthy Road	8	301+47	32	RT	736.06	732.26			1
McCarthy Road	9	302+05	20	LT	735.92	732.42			1
McCarthy Road	10	302+05	32	RT	735.87	732.18			1
McCarthy Road	11	302+59	32	RT	735.69	730.15 N	730.93 S	1	
McCarthy Road	12	302+79	20	LT	735.83	730.02 SW,NE		1	
McCarthy Road	13	304+41	20	LT	734.98	731.48			1
McCarthy Road	14	304+41	32	RT	734.74	731.24			1
McCarthy Road	15	305+74	20	LT	734.21	730.71			1
McCarthy Road	16	308+44	20	LT	731.00	727.50			1
McCarthy Road	17	308+44	20	RT	731.00	727.50			1
McCarthy Road	18	310+82	20	LT	728.01	724.52			1
McCarthy Road	19	310+82	20	RT	728.01	722.67 N,S,SW		1	
McCarthy Road	21	313+15	20	LT	725.15	721.65			1
McCarthy Road	22	313+15	20	RT	725.15	720.94 S,N			1
McCarthy Road	23	313+15	25.5	RT	725.20	720.97			1
McCarthy Road	24	313+92.5	38	RT	723.50	720.20 W,NE		1	
McCarthy Road	25	313+94	20	LT	724.42	720.92			1
McCarthy Road	26	315+28.7	43.2	RT	723.45	721.19			1
McCarthy Road	27	317+43	20	LT	721.85	718.82			1
McCarthy Road	28	317+43	18	RT	721.89	718.71			1
McCarthy Road	29	318+40	20	LT	721.36	718.51			1
McCarthy Road	30	318+10	18	RT	721.40	718.40			1
McCarthy Road	31	319+18	20	LT	721.26	718.03			1
McCarthy Road	32	319+18	18	RT	721.50	717.92			1
McCarthy Road	33	320+08	20	LT	721.23	717.83			1
McCarthy Road	34	320+08	18	RT	721.27	717.87			1
Derby Road	1	51+13	18.5	LT	736.05	733.55			1
Derby Road	2	51+13	18.5	RT	736.05	733.55			1
Derby Road	3	52+86	18.5	LT	735.42	731.92			1
Derby Road	4	52+86	18.5	RT	735.42	731.92			1
Derby Road	5	54+10	18.5	LT	734.86	731.36 NE	733.19 SW		1
Derby Road	6	54+10	18.5	RT	734.86	731.36			1
Derby Road	7	55+11.1	27.17	LT	733.75	730.25			1
Derby Road	8	55+11.1	27.17	RT	733.75	730.25			1
TOTALS								5	36

PROPOSED MANHOLES

LOCATION	STRUCTURE NUMBER	STATION	OFFSET (FT)	LT/RT	RIM ELEVATION	INVERT ELEVATION (Main Line)	MANHOLES TYPE A, 4'-DIA. TYPE 1 FRAME CLOSED LID 60218400 EACH	MANHOLES TYPE A, 5'-DIA. TYPE 1 FRAME CLOSED LID 60221100 EACH	MANHOLES TYPE A, 6'-DIA. TYPE 1 FRAME CLOSED LID 60223800 EACH	INVERT ELEVATION (Lateral)	INVERT ELEVATION (Lateral)	COMMENTS
McCarthy Road	1	298+79	24	RT	735.83	731.06 E	1			732.07 N	732.00S	
McCarthy Road	2	299+60	24	RT	736.10	730.86 E,W		1		732.19 N	732.13 S	
McCarthy Road	3	300+41	24	RT	736.41	730.65 E,W		1		732.31 N	732.25 S	
McCarthy Road	4	301+47	24	RT	736.31	730.39 E,W		1		732.28 N	732.22 S	
McCarthy Road	5	302+05	24	RT	736.12	730.25 E,W		1		732.20 N	732.14 S	
McCarthy Road	6	302+59	24	RT	735.94	730.11 N,W,E		1		730.11 S		
McCarthy Road	7	304+41	12	RT	735.27	730.64 E	1			731.32 N	731.14 S	
McCarthy Road	8	305+74	12	RT	734.50	728.93 E,W,S		1		730.55 N		
McCarthy Road	9	308+44	12	RT	731.29	725.80 E,W		1		727.34 N	727.40 S	
McCarthy Road	10	310+82	12	RT	728.30	722.83 E,W		1		724.36 N	722.83 S	
McCarthy Road	11	313+15	12	RT	725.44	720.88 E,W		1		721.48 N	720.88 S	
McCarthy Road	12	314+10	12	RT	724.48	720.10 W,W			1	720.10 SW		Flat Top
McCarthy Road	13	314+19	13	LT	724.44	720.04 N,S		1		720.80 W		
McCarthy Road	14	317+43	10	RT	722.18	718.66 E		1		718.67 N	718.67 S	Flat Top
McCarthy Road	15	318+10	10	RT	721.69	718.35 E,W		1		718.36 N	718.36 S	Flat Top
McCarthy Road	16	319+18	10	RT	721.59	717.87 E,W		1		717.88 N	717.88 S	
McCarthy Road	17	320+08	10	RT	721.56	717.46 SE,W		1		717.68 N	717.83 S	
Derby Road	1	51+13	8.4	RT	736.38	733.23 N,W	1			733.50 NE	733.42 SW	Flat Top
Derby Road	2	52+26	8.4	RT	736.02	732.38 SW,731.38 NW		1		733.23 SW		Flat Top
Derby Road	3	52+80	8.4	RT	735.75	730.93 SE,NW		1		731.87 NE	731.79 SW	
Derby Road	4	54+10	8.4	RT	735.19	730.00 SE,NW		1		731.31 NE	731.23 SW	
Derby Road	5	53+18	8.4	RT	734.50	729.19 N,SE		1		729.98 W	730.15 E	
TOTALS							7	14	1			

PROPOSED FLARED END SECTIONS

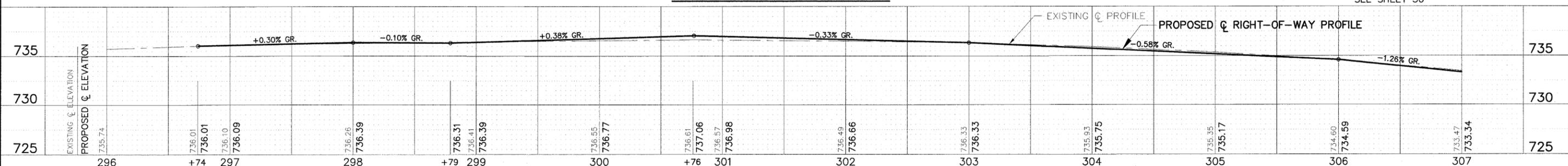
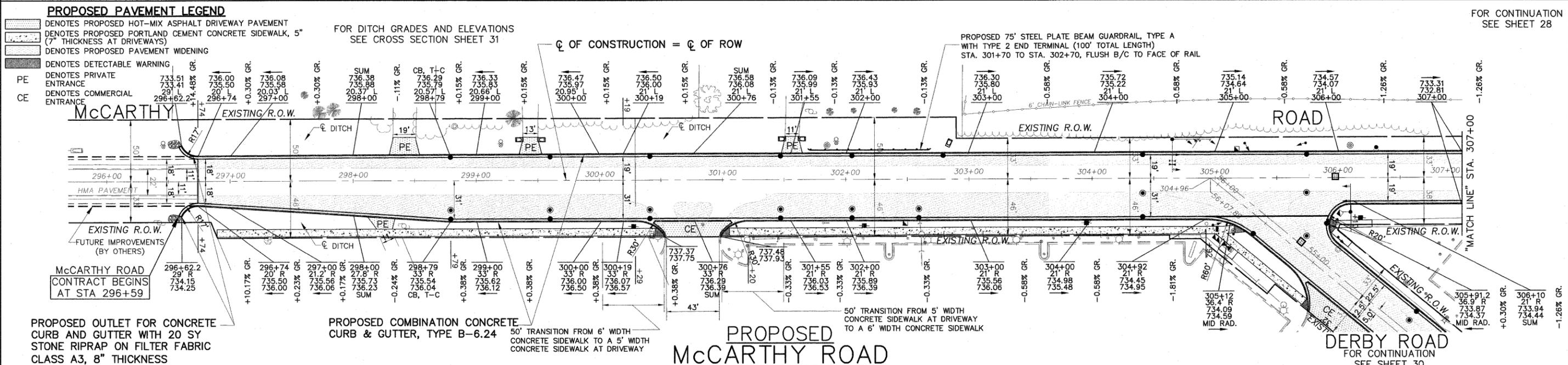
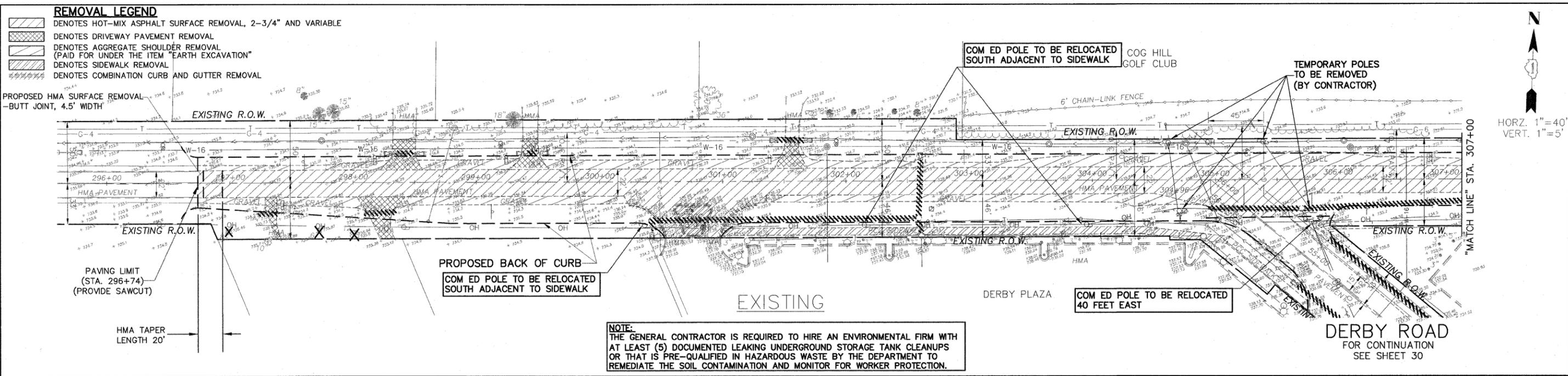
LOCATION	STRUCTURE NUMBER	STATION	OFFSET (FT)	LT/RT	INVERT ELEVATION	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 15" 54213663 EACH	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 18" 54213663 EACH	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 24" 54213669 EACH	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS, 30" 54215430 EACH
McCarthy Road	1	299+30	33	LT	733.35	1			
McCarthy Road	2	299+60	33	LT	735.23	1			
McCarthy Road	3	301+40	37	LT	732.30	1			
McCarthy Road	4	301+70	37	LT	732.15	1			
McCarthy Road	5	302+83.66	37	LT	730.00			1	
McCarthy Road	6	320+46	31	RT	717.31	1			
McCarthy Road	7	320+75.37	33	LT	718.14		1		
McCarthy Road	8	314+19	26	LT	720.00				1
TOTALS						5	1	1	1

DRAINAGE STRUCTURE REMOVAL

LOCATION	STATION	OFFSET (FT)	LT/RT	EXISTING STRUCTURE	REMOVING CATCH BASINS 60500050 EACH	REMOVING MANHOLES 60500040 EACH	REMOVE CONCRETE FLARED END SECTIONS X0322118 EACH	INLET AND PIPE PROTECTION 28000510 EACH
McCarthy Road	298+30	23.5	LT	FES			1	
McCarthy Road	298+54	23.5	LT	FES			1	
McCarthy Road	299+32	23.5	LT	FES			1	
McCarthy Road	299+54	23.5	LT	FES			1	
McCarthy Road	300+45	31.5	RT	CB	1			
McCarthy Road	301+27	31	RT	CB	1			
McCarthy Road	301+42	33	LT	FES			1	
McCarthy Road	301+70	33	LT	FES			1	
McCarthy Road	302+59	30	RT	CB	1			
McCarthy Road	302+60.5	21.5	LT	FES			1	
McCarthy Road	305+70	23.5	RT	CB	1			
McCarthy Road	305+75	32.5	RT	FES			1	
McCarthy Road	307+23	16.5	RT	CB	1			
McCarthy Road	308+43	22.5	RT	CB	1			
McCarthy Road	309+91	25	RT	FES			1	
McCarthy Road	310+82	20	RT	CB	1			1
McCarthy Road	311+07	20.5	RT	CB	1			
McCarthy Road	313+03	22	RT	CB	1			
McCarthy Road	314+18	46	RT	MH		1		
McCarthy Road	314+18	40.5	LT	FES			1	
McCarthy Road	315+25	39	RT	CB	1			
McCarthy Road	320+77	27	LT	FES			1	
Derby Road	51+15	18	LT	FES			1	
Derby Road	52+22	24	RT	CB	1			
Derby Road	52+90	20	RT	FES			1	
Derby Road	53+75	21	RT	FES			1	
Derby Road	54+20	21	RT	FES			1	
Derby Road	55+05	23	RT	FES			1	
TOTALS					11	1	16	1

FILE NAME = FAU 1587 (MCCARTHY ROAD)
FAP 577 (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION

USER NAME =
PLOT SCALE =
PLOT DATE =



IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD INDICATING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES ENGINEERED IN TITLE BLOCK.

GRAPHIC SCALE
0 40 80 IN FEET

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ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000228

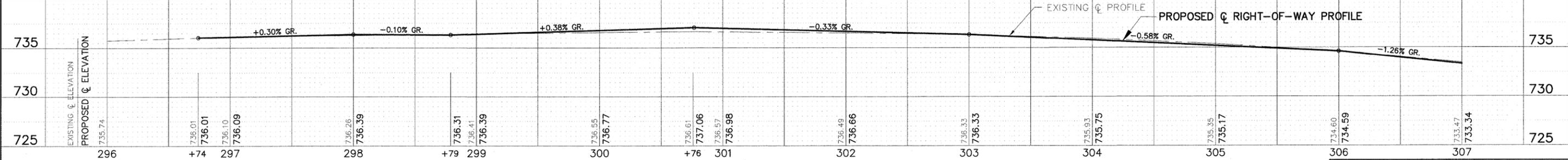
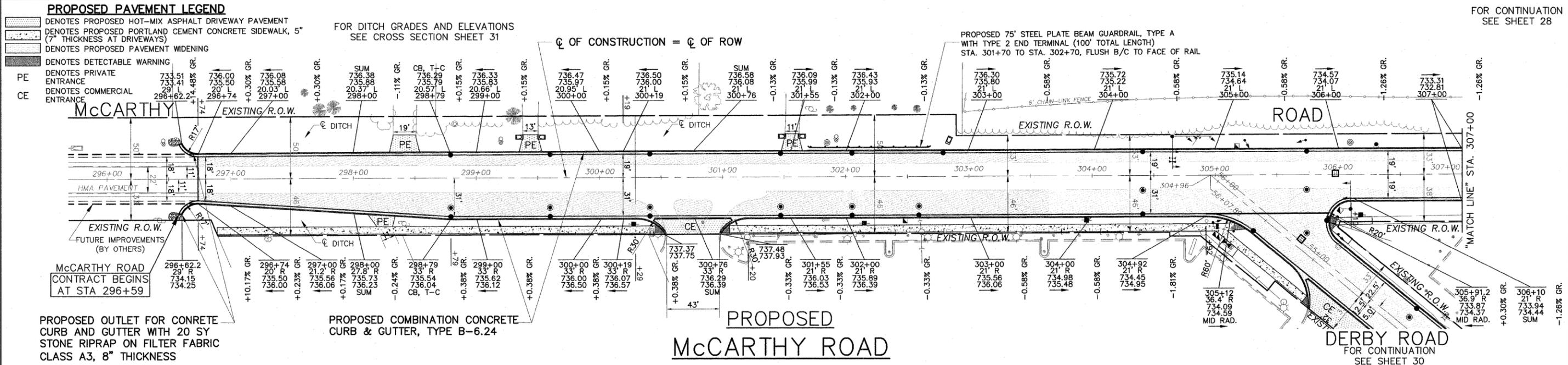
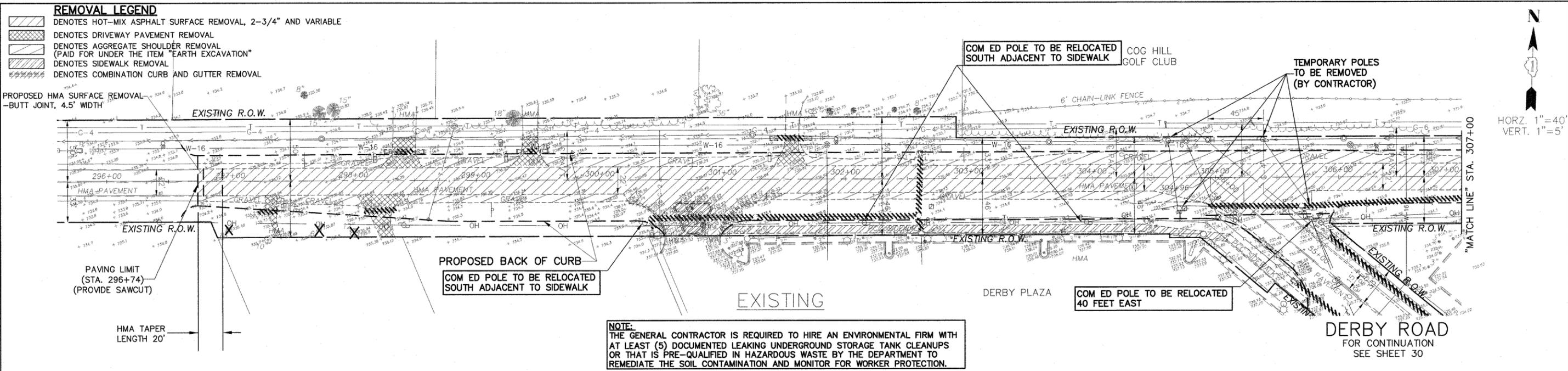
FILE NAME FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED - JLC	REVISED - THK 2-25-11
	PLOT SCALE =	DRAWN - JFP-JEP	REVISED - THK 9-21-11
	PLOT DATE =	CHECKED - THK	REVISED - THK 10-31-11
		DATE - 11/10	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLAN & PROFILE:
McCARTHY ROAD- (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)**

SCALE: AS NOTED SHEET NO. OF SHEETS STA. 296+74 TO STA. 307+00

F.A. R.T.E. VAR	SECTION 02-00055-00-WR	COUNTY COOK	TOTAL SHEETS 84	SHEET NO. 27
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZE PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE: MCCARTHY ROAD - (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)

SCALE: AS NOTED SHEET NO. OF SHEETS STA. 296+74 TO STA. 307+00

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FILE NAME FAU 1587 (MCCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME = DESIGNED - JLC DRAWN - JFP-JEP CHECKED - THK DATE - 11/10	REVISED - THK 2-25-11 REVISED - THK 9-21-11 REVISED - THK 10-31-11 REVISED -	SCALE: AS NOTED SHEET NO. OF SHEETS STA. 296+74 TO STA. 307+00	F.A. R.T.E. VAR 02-00055-00-WR COUNTY COOK TOTAL SHEETS 84 SHEET NO. 27 CONTRACT NO. 63557 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)
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- REMOVAL LEGEND**
- Denotes HOT-MIX ASPHALT SURFACE REMOVAL, 2-3/4" AND VARIABLE
 - Denotes DRIVEWAY PAVEMENT REMOVAL
 - Denotes AGGREGATE SHOULDER REMOVAL (PAID FOR UNDER THE ITEM "EARTH EXCAVATION")
 - Denotes SIDEWALK REMOVAL
 - Denotes COMBINATION CURB AND GUTTER REMOVAL

COG HILL GOLF CLUB

EXISTING PIPE TO BE REMOVED TO THE RIGHT-OF-WAY LINE. SEAL END WITH BRICK AND MORTAR. COST TO BE CONSIDERED INCLUDED IN THE COST OF STORM SEWER REMOVAL.

TEMPORARY POLES TO BE REMOVED (BY CONTRACTOR)

PROPOSED HMA SURFACE REMOVAL - BUTT JOINT, 4.5' WIDTH PAVING LIMIT (STA. 315+67) (PROVIDE SAWCUT)

HMA TAPER LENGTH 20'

PAVING LIMIT (STA. 314+94) (PROVIDE SAWCUT)

PROPOSED HMA SURFACE REMOVAL - BUTT JOINT, 4.5' WIDTH

TEMPORARY POLES TO BE REMOVED (BY CONTRACTOR)

HORIZ. 1" = 40'
VERT. 1" = 5'

EXISTING WETLAND AREA (MINIMIZE CONSTRUCTION DISTURBANCE)

FOR CONTINUATION SEE SHEET 29

PROPOSED BACK OF CURB

6" CHAIN-LINK FENCE

EXISTING R.O.W.

EXISTING R.O.W.

EXISTING R.O.W.

EXISTING R.O.W.

RILLY'S POINTE SHOPPING CENTER

CITGO GAS STATION

SHELL OIL GAS STATION

NOTE:
THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

FOR CONTINUATION SEE SHEET 27

EXISTING

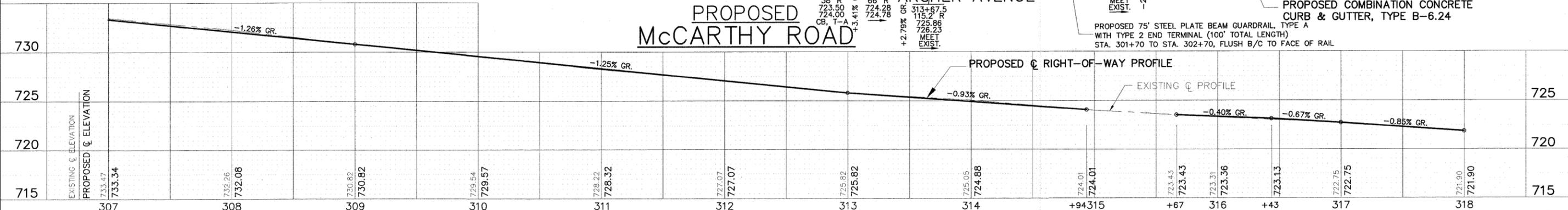
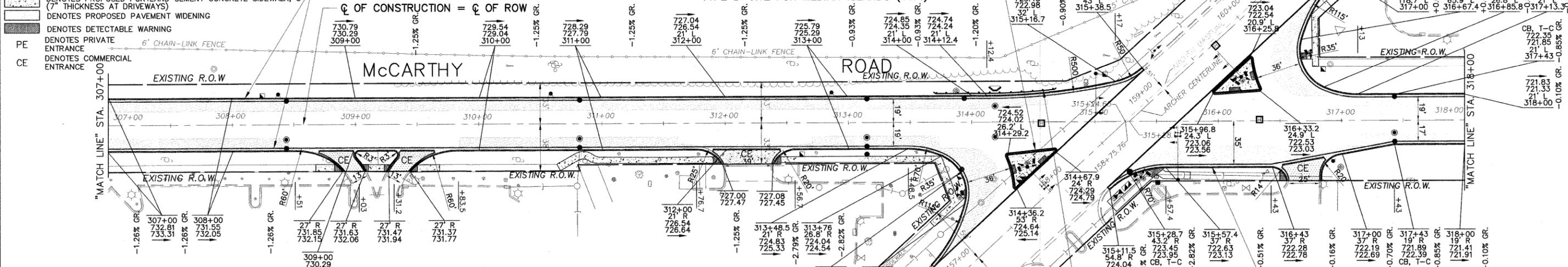
ARCHER ROAD

PROPOSED PAVEMENT LEGEND

- Denotes PROPOSED HOT-MIX ASPHALT DRIVEWAY PAVEMENT
- Denotes PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" THICKNESS AT DRIVEWAYS)
- Denotes PROPOSED PAVEMENT WIDENING
- Denotes DETECTABLE WARNING
- Denotes PRIVATE ENTRANCE
- Denotes COMMERCIAL ENTRANCE

PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24

PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 FOR MEDIAN ISLANDS (TYP.)



IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9



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ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00028

FILE NAME = FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED = JLC	REVISED = THK 2-25-11
		DRAWN = JFP-JEP	REVISED = THK 9-21-11
	PLOT SCALE =	CHECKED = THK	REVISED =
	PLOT DATE =	DATE = 11/10	REVISED =

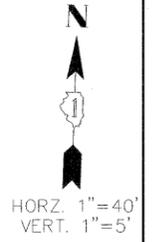
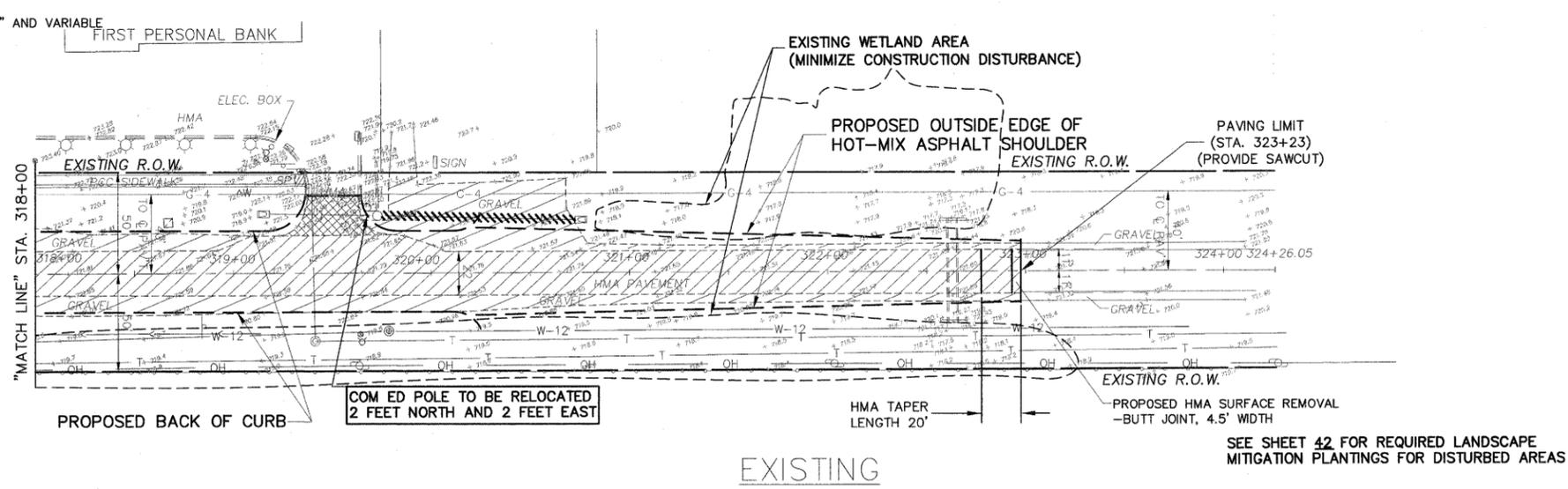
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
McCARTHY ROAD- (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)

SCALE: AS NOTED SHEET NO. OF SHEETS STA. 307+00 TO STA. 318+00

F.A. RTE. VAR	SECTION 02-00055-00-WR	COUNTY COOK	TOTAL SHEETS 84	SHEET NO. 28
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

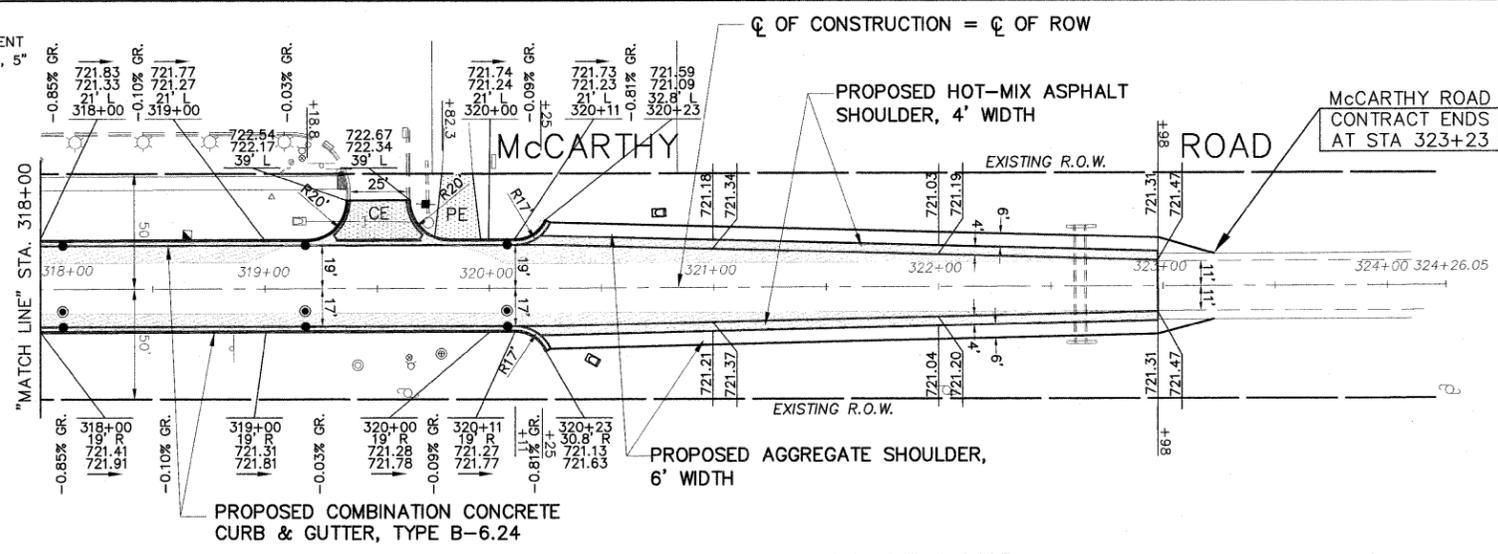
- REMOVAL LEGEND**
- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, 2-3/4" AND VARIABLE
 - DENOTES DRIVEWAY PAVEMENT REMOVAL
 - DENOTES AGGREGATE SHOULDER REMOVAL (PAID FOR UNDER THE ITEM "EARTH EXCAVATION")
 - DENOTES SIDEWALK REMOVAL
 - DENOTES COMBINATION CURB AND GUTTER REMOVAL



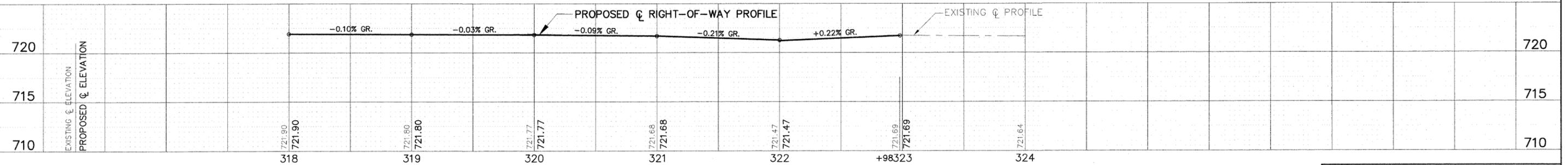
FOR CONTINUATION
SEE SHEET 28

NOTE:
THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

- PROPOSED PAVEMENT LEGEND**
- DENOTES PROPOSED HOT-MIX ASPHALT DRIVEWAY PAVEMENT
 - DENOTES PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" THICKNESS AT DRIVEWAYS)
 - DENOTES PROPOSED PAVEMENT WIDENING
 - DENOTES DETECTABLE WARNING
 - PE DENOTES PRIVATE ENTRANCE
 - CE DENOTES COMMERCIAL ENTRANCE



**PROPOSED
McCARTHY ROAD**



IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK
SEE SHEET 9



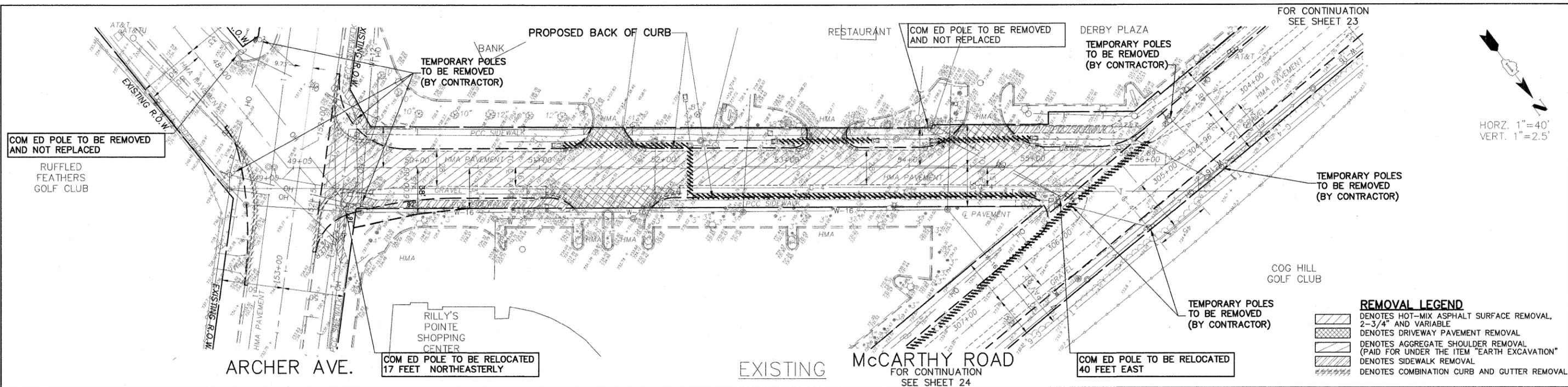
Frank Novotny & Associates, Inc.
Civil Engineers & Municipal Consultants
825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-9640 • Fax: (630) 887-0133
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000208

FILE NAME = FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED - JLC	REVISED - THK 2-25-11
	PLOT SCALE =	DRAWN - JFP-JEP	REVISED - THK 9-21-11
	PLOT DATE =	CHECKED - THK	REVISED -
		DATE - 11/10	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

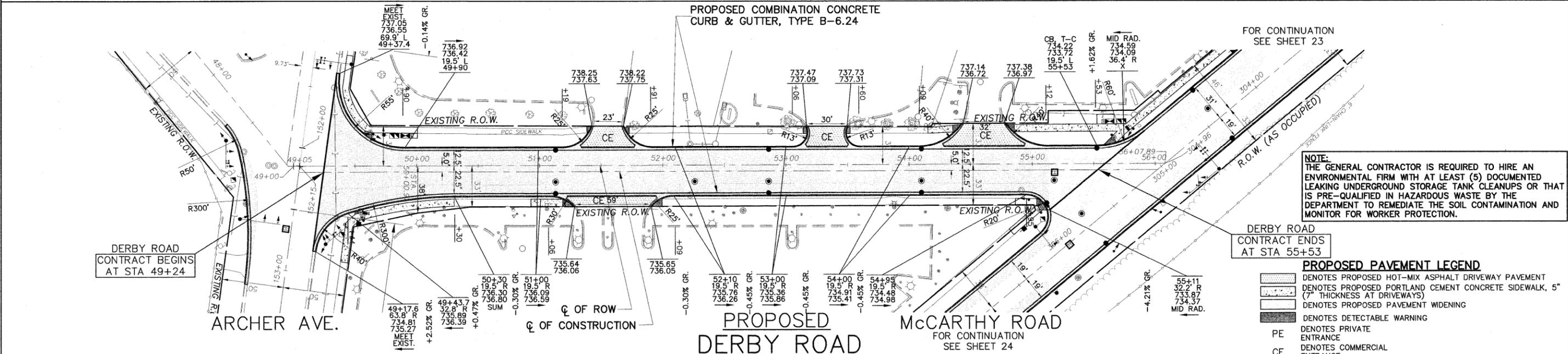
PLAN & PROFILE:
McCARTHY ROAD- (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)
SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. 318+00 TO STA. 323+24

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	29
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



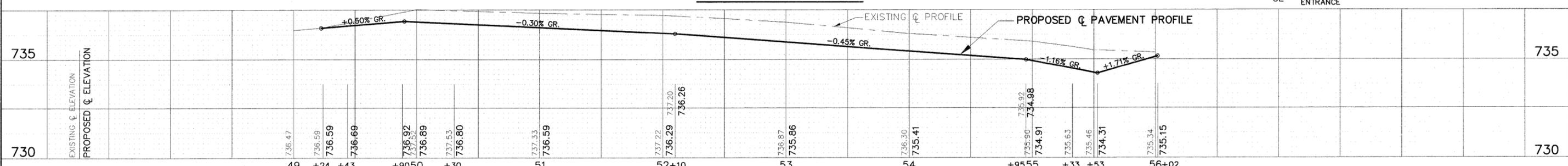
HORZ. 1"=40'
VERT. 1"=2.5'

- REMOVAL LEGEND**
- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, 2-3/4" AND VARIABLE
 - DENOTES DRIVEWAY PAVEMENT REMOVAL
 - DENOTES AGGREGATE SHOULDER REMOVAL (PAID FOR UNDER THE ITEM "EARTH EXCAVATION")
 - DENOTES SIDEWALK REMOVAL
 - DENOTES COMBINATION CURB AND GUTTER REMOVAL



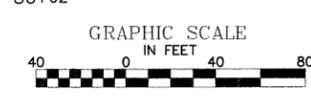
NOTE:
THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

- PROPOSED PAVEMENT LEGEND**
- DENOTES PROPOSED HOT-MIX ASPHALT DRIVEWAY PAVEMENT
 - DENOTES PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" THICKNESS AT DRIVEWAYS)
 - DENOTES PROPOSED PAVEMENT WIDENING
 - DENOTES DETECTABLE WARNING
 - PE DENOTES PRIVATE ENTRANCE
 - CE DENOTES COMMERCIAL ENTRANCE



IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 10



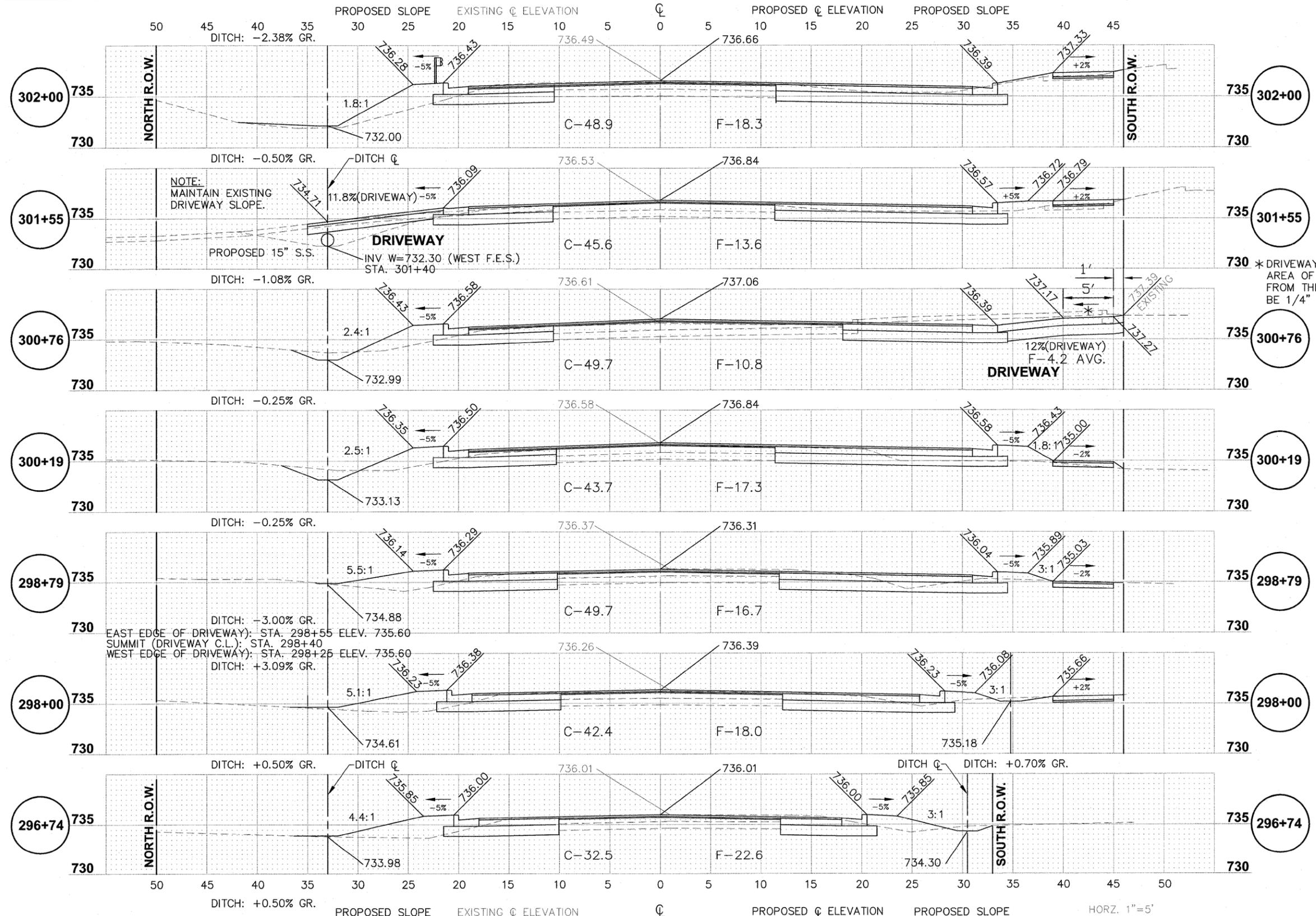
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ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00028

FILE NAME = FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED = JLC	REVISED = THK 2-25-11
	PLOT SCALE =	DRAWN = JFP-JEP	REVISED = THK 9-21-11
	PLOT DATE =	CHECKED = THK	REVISED =
		DATE = 11/10	REVISED =

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE:
DERBY ROAD- (EXISTING AND PROPOSED ROADWAY PAVING AND REMOVAL PLAN)
SCALE: AS NOTED SHEET NO. OF SHEETS STA. 49+24 TO STA. 55+53

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	30
CONTRACT NO. 63557				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT HPP-M-8003(504)			



* DRIVEWAY CROSS SLOPE IN THE AREA OF THE PROJECTED SIDEWALK FROM THE EAST AND WEST SHALL BE 1/4" PER FOOT.

McCARTHY ROAD



IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9

FILE NAME FAU 1587 (McCARTHY ROAD)
 FAP 577 (ARCHER AVENUE)
 FAU 1024 (DERBY ROAD)
 CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION

USER NAME =
 PLOT SCALE =
 PLOT DATE =

DESIGNED - JLC
 DRAWN - JFP-JEP
 CHECKED - THK
 DATE - 11/10

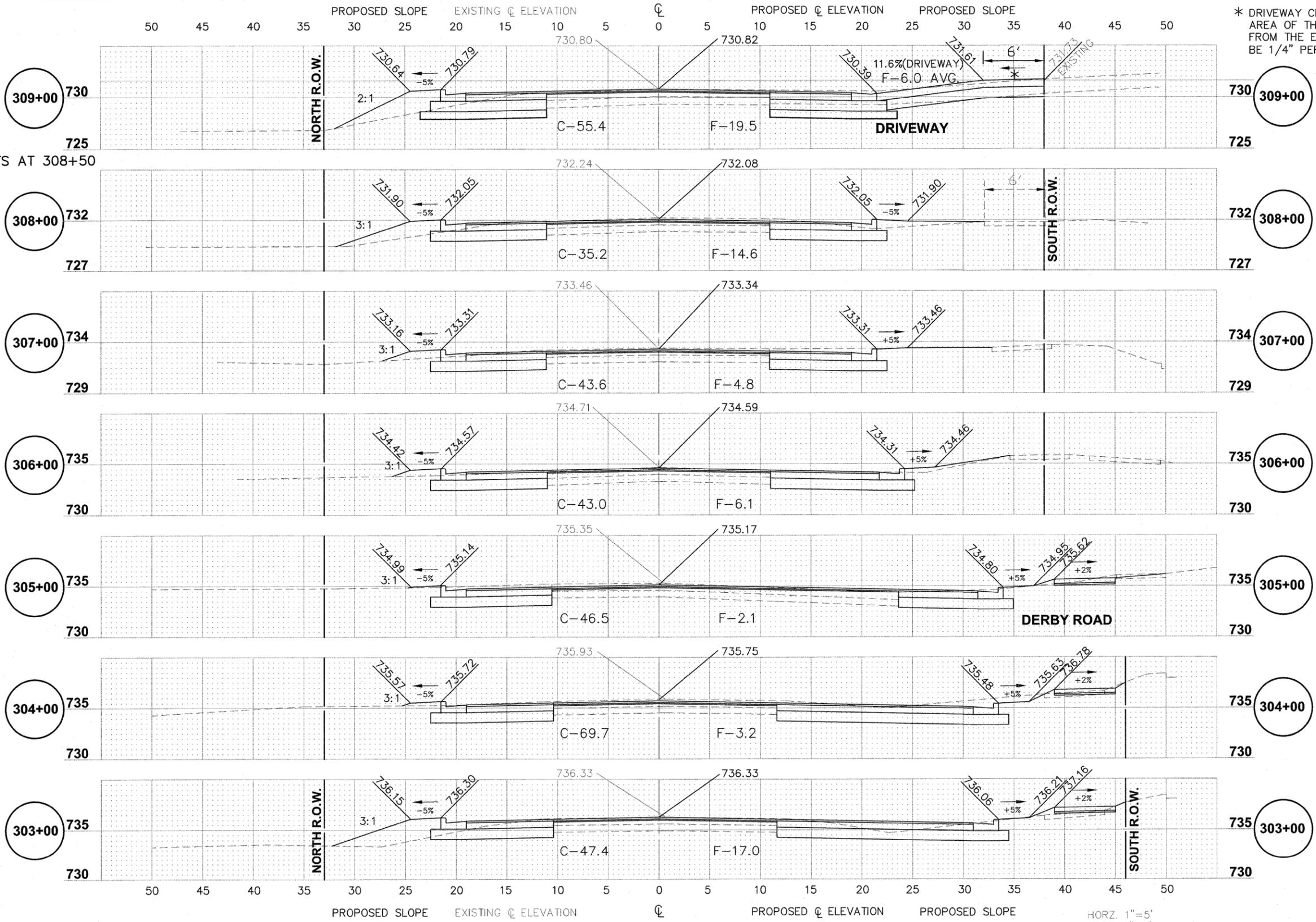
REVISED - THK 2-25-11
 REVISED - THK 9-21-11
 REVISED - THK 10-31-11

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS:
 McCARTHY ROAD- STA. 296+74 TO STA. 302+00
 SCALE: AS NOTED SHEET NO. OF SHEETS STA. TO STA.

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 Municipal Consultants
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	31
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



* DRIVEWAY CROSS SLOPE IN THE AREA OF THE PROJECTED SIDEWALK FROM THE EAST AND WEST SHALL BE 1/4" PER FOOT.

PGES 9" STARTS AT 308+50

McCARTHY ROAD



IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9

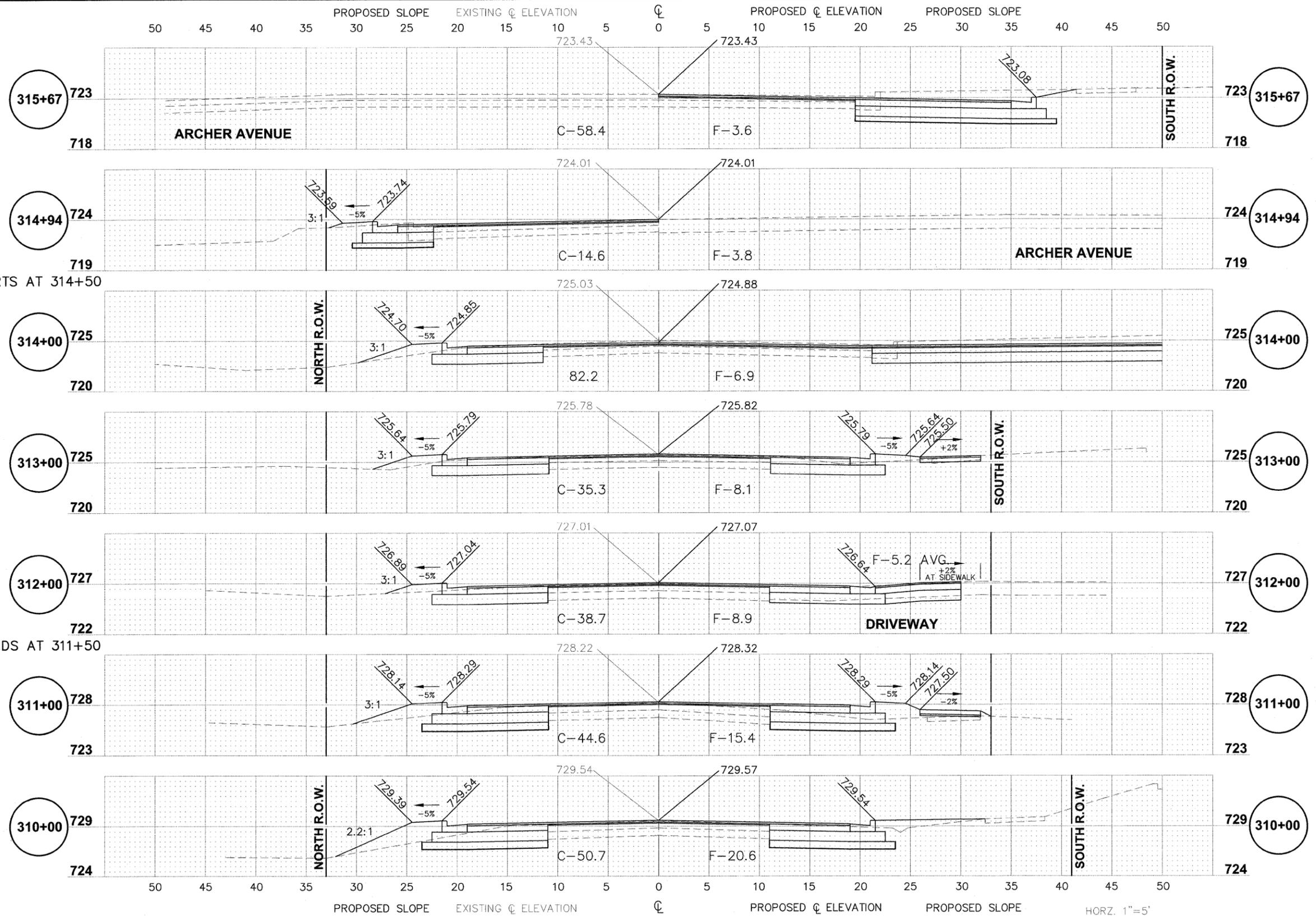
FILE NAME FAU 1587 (McCARTHY ROAD)	USER NAME =	DESIGNED - JLC	REVISED - THK 2-25-11
FAP 577 (ARCHER AVENUE)		DRAWN - JFP-JEP	REVISED - THK 9-21-11
FAU 1024 (DERBY ROAD)		CHECKED - THK	REVISED - THK 10-31-11
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION		DATE - 11/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS:
McCARTHY ROAD- STA. 303+00 TO STA. 309+00
SCALE: AS NOTED SHEET NO. OF SHEETS STA. TO STA.

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Civil Engineers Municipal Consultants ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000828

F.A. RT.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	32
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



PGES 6" STARTS AT 314+50

PGES 9" ENDS AT 311+50

McCARTHY ROAD



IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9

FILE NAME FAU 1587 (McCARTHY ROAD)
FAP 577 (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION

USER NAME =
PLOT SCALE =
PLOT DATE =

DESIGNED - JLC
DRAWN - JFP-JEP
CHECKED - THK
DATE - 11/10

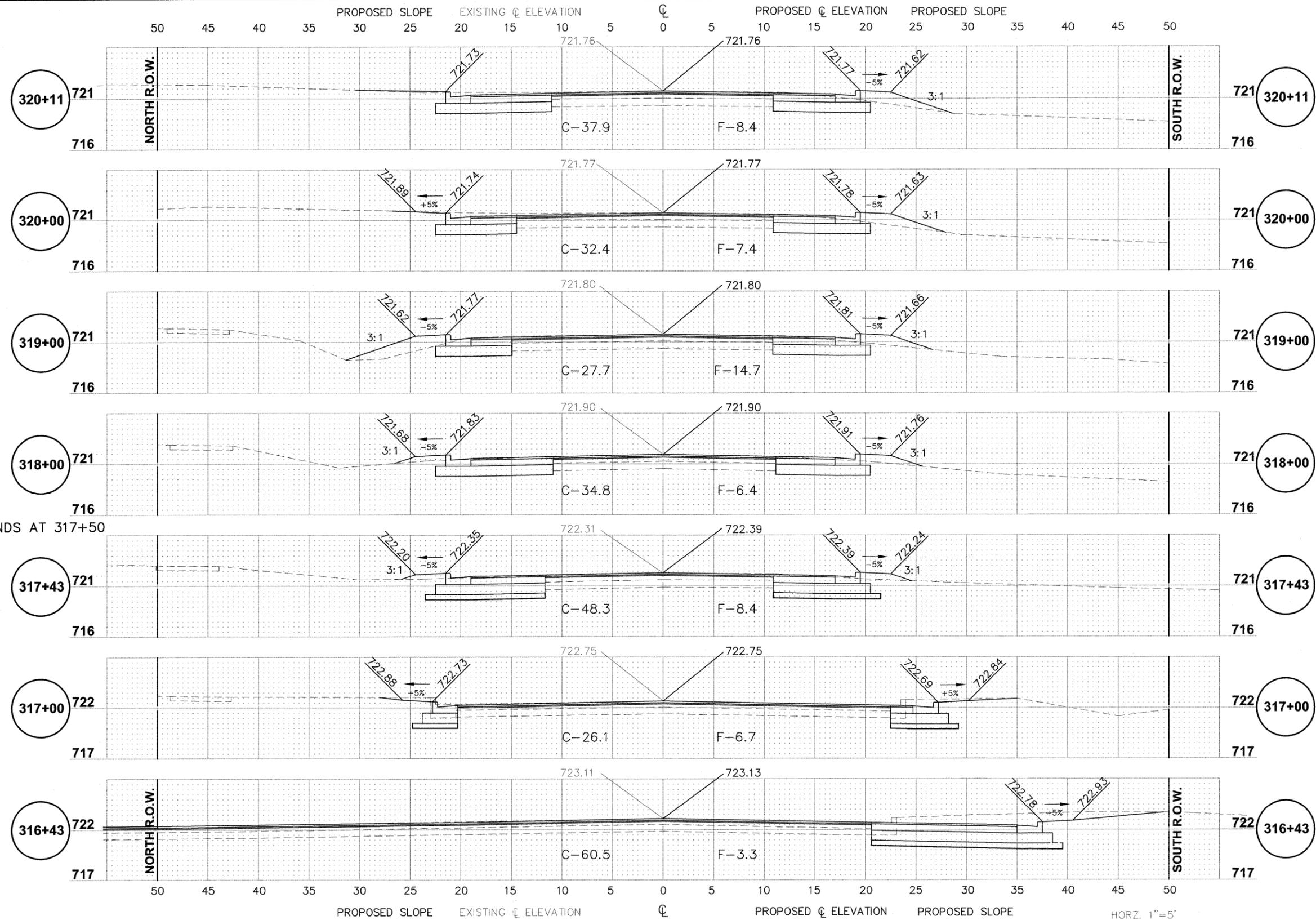
REVISED - THK 2-25-11
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS:
McCARTHY ROAD- STA. 310+00 TO STA. 315+67
SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. TO STA.

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F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	33
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



PGES 6" ENDS AT 317+50

McCARTHY ROAD



IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9

FILE NAME FAU 1587 (McCARTHY ROAD)	USER NAME =	DESIGNED - JLC	REVISED - THK 2-25-11
FAP 577 (ARCHER AVENUE)		DRAWN - JFP-JEP	REVISED -
FAU 1024 (DERBY ROAD)		CHECKED - THK	REVISED -
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION		DATE - 11/10	REVISED -

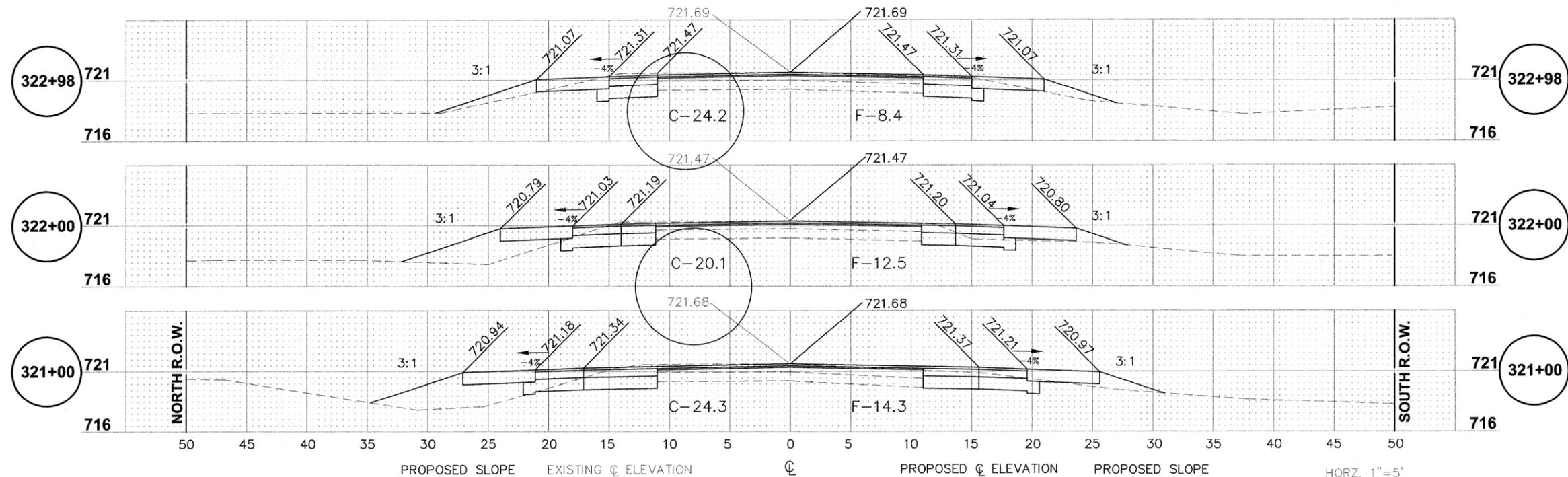
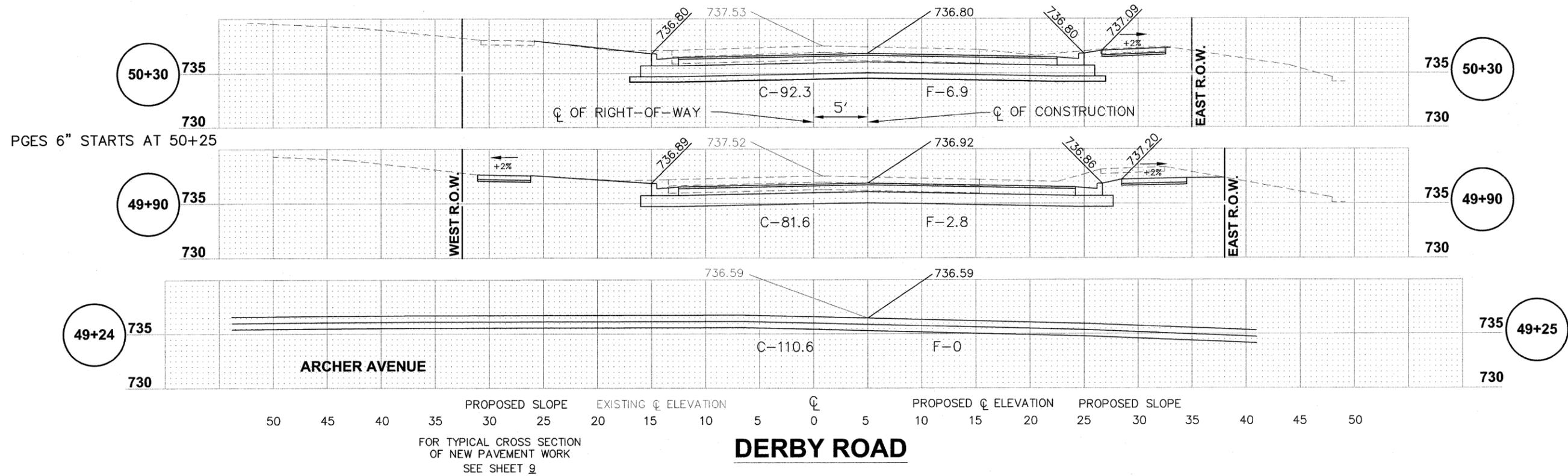
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS:
McCARTHY ROAD- STA. 316+43 TO STA. 322+11

SCALE: AS NOTED SHEET NO. OF SHEETS STA. TO STA.

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F.A. RT.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	34
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 9 & 10

FILE NAME FAU 1587 (McCARTHY ROAD) FAU 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) #05027	USER NAME =	DESIGNED - JLC	REVISED - THK 2-25-11
	PLOT SCALE =	DRAWN - JFP-JEP	REVISED -
	PLOT DATE =	CHECKED - THK	REVISED -
		DATE - 11/10	REVISED -

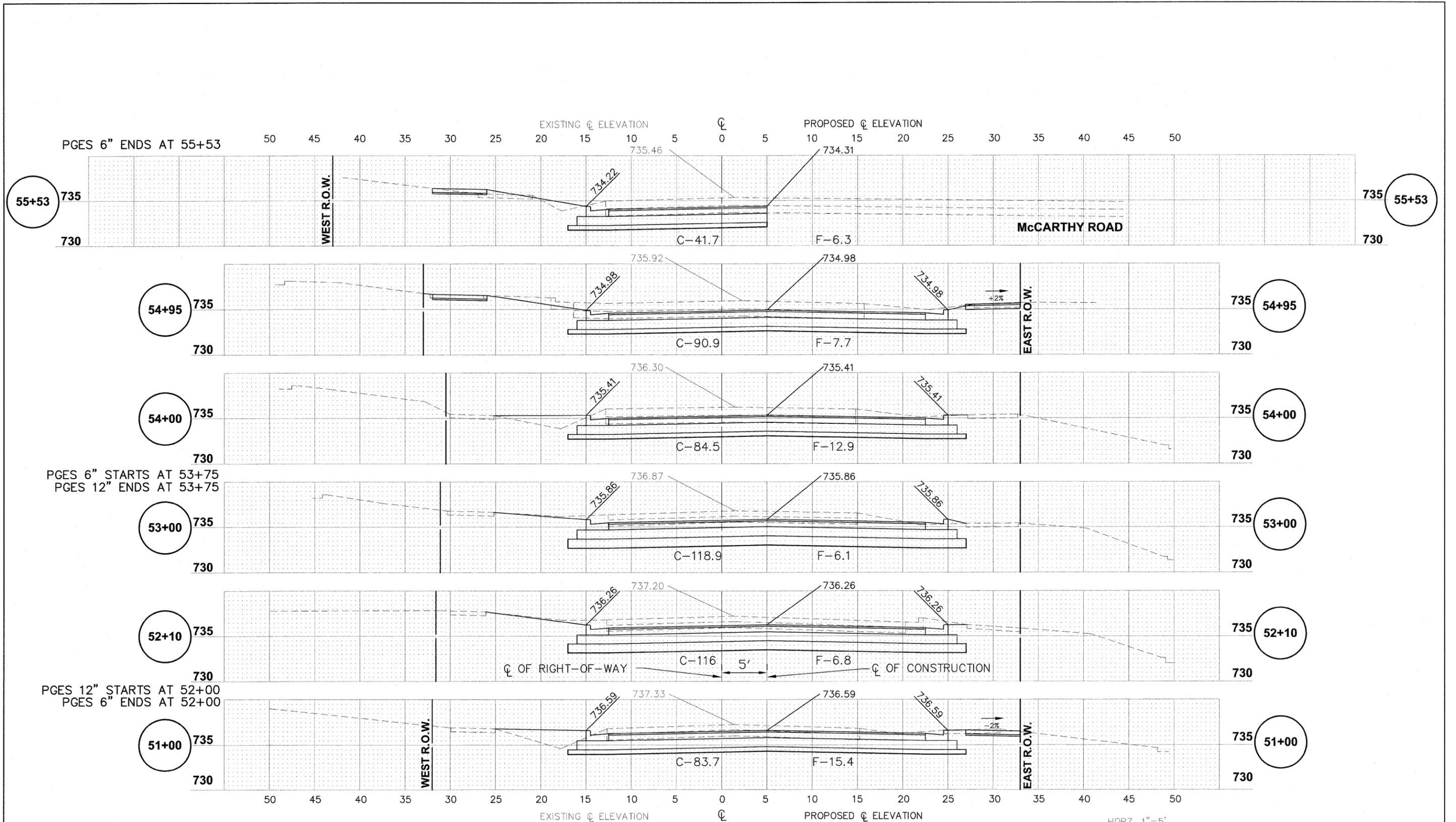
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS:
McCARTHY ROAD- STA. 321+00 TO STA. 322+98
DERBY ROAD- STA. 49+25 TO STA. 55+39

SCALE: AS NOTED | SHEET NO. OF SHEETS STA. TO STA.

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ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00028

F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	35
CONTRACT NO. 63557				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	HPP-M-8003(504)		



DERBY ROAD



IMPORTANT!
 FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES.
 REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FOR TYPICAL CROSS SECTION
 OF NEW PAVEMENT WORK
 SEE SHEET 10

FILE NAME FAU 1587 (McCARTHY ROAD)	USER NAME =	DESIGNED — JLC	REVISED — THK 2-25-11
FAP 577 (ARCHER AVENUE)		DRAWN — JFP-JEP	REVISED —
FAU 1024 (DERBY ROAD)	PLOT SCALE =	CHECKED — THK	REVISED —
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	PLOT DATE =	DATE — 11/10	REVISED —

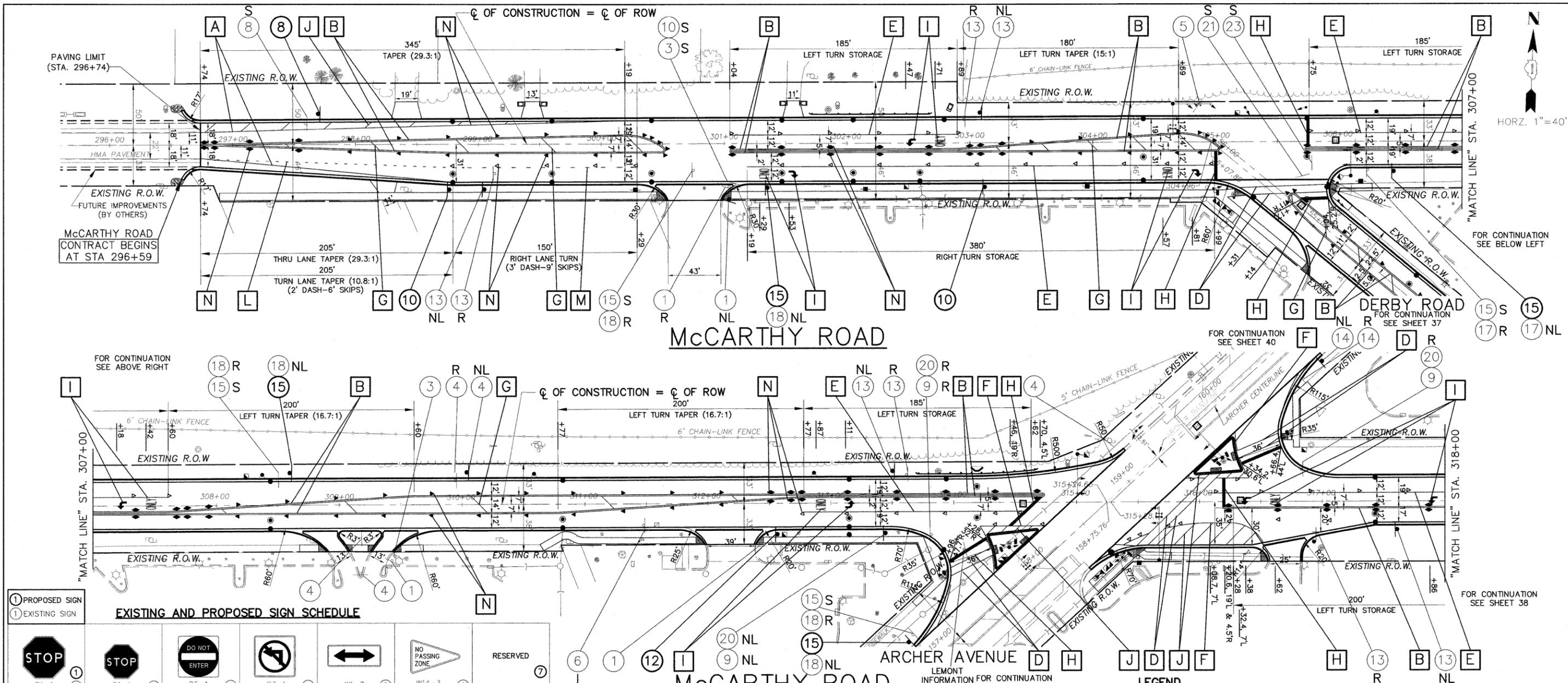
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS:
DERBY ROAD- STA. 49+25 TO STA. 55+39

SCALE: AS NOTED	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	36
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



EXISTING AND PROPOSED SIGN SCHEDULE

						RESERVED
						RESERVED

IMPORTANT!
 FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES.
 REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FILE NAME FAU 1587 (McCARTHY ROAD)
 FAP 577 (ARCHER AVENUE)
 FAU 1024 (DERBY ROAD)
 CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION

USER NAME =
 DESIGNED -- JLC
 DRAWN -- JFP-JEP
 CHECKED -- THK
 DATE -- 11/10

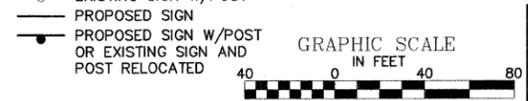
REVISED -- THK 2-25-11
 REVISED -- THK 9-21-11
 REVISED --
 REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

- THERMOPLASTIC STRIPING CODE**
- A PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" (LANE AND EDGE LINE)
 - B PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTERLINE 4", 11" O/C
 - C PROPOSED SKIP-DASH YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" - 10' DASH, 30' SKIP (LANE LINE)
 - D PROPOSED SKIP-DASH WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK MARKING)
 - E PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE)
 - F PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 8" (LANE AND EDGE LINE)
 - G PROPOSED SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 12", 45' DIAGONALS, (MEDIAN LINE)
 - H PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR)
 - I PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS
 - J PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 12", 45' DIAGONALS
 - K PROPOSED SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" (LANE LINE)
 - L PROPOSED SKIP-DASH WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" - 2' DASH, 6' SKIP (LANE LINE)
 - M PROPOSED SKIP-DASH WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" - 2' DASH, 6' SKIP (LANE LINE)
- REFLECTIVE PAVEMENT MARKERS**
- N PROPOSED RAISED REFLECTIVE PAVEMENT MARKERS - 80' O/C OR AS SHOWN
 - ◆ TWO-WAY AMBER MARKER
 - ▶ ONE-WAY AMBER MARKER
 - ▷ ONE-WAY CRYSTAL MARKER

DETECTOR LOOPS
 FOR LOOP DETECTOR PLACEMENT AT McCARTHY ROAD AND DERBY ROAD, SEE SHEET 54
 FOR LOOP DETECTOR PLACEMENT AT McCARTHY ROAD AND ARCHER AVENUE, SEE SHEETS 63 & 64

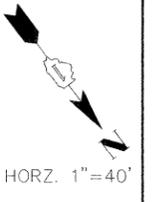
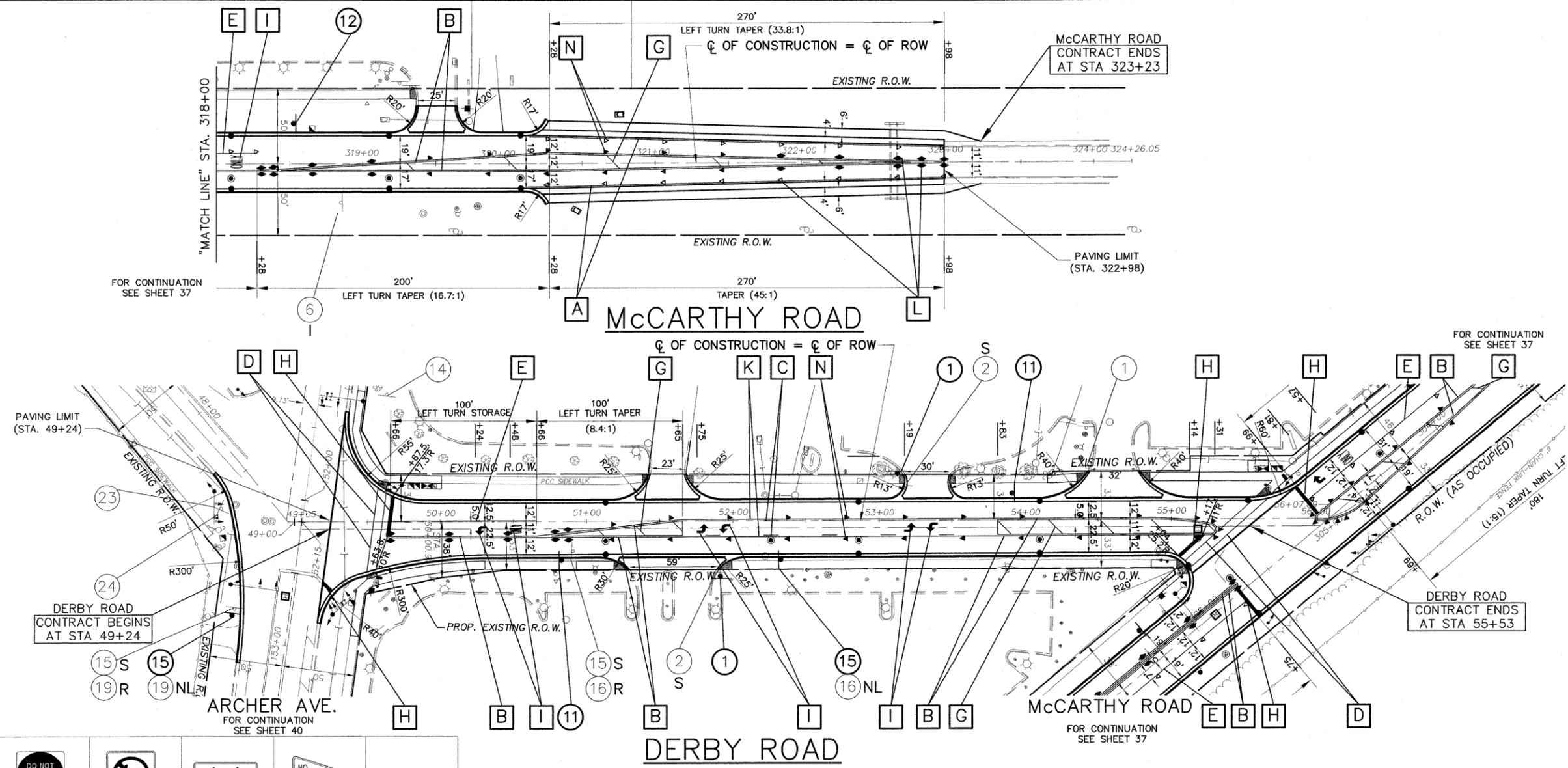
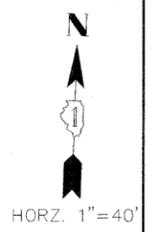
- LEGEND**
- 19 DENOTES EXISTING SIGN
 - 19 DENOTES PROPOSED SIGN
 - A DENOTES PROPOSED STRIPING
 - R DENOTES EXISTING SIGN OR SIGN W/POST WITHIN THE PROJECT LIMITS TO BE REMOVED BY CONTRACTOR AND RESET BY THE CONTRACTOR UPON COMPLETION OF CURB AND GUTTER CONSTRUCTION.
 - NL DENOTES NEW SIGN LOCATION
 - S DENOTES EXISTING SIGN OR SIGN W/POST WITHIN THE PROJECT LIMITS TO BE REMOVED BY CONTRACTOR AND SALVAGED TO THE VILLAGE OF LEMONT PUBLIC WORKS DEPARTMENT.
 - I DENOTES EXISTING SIGN OR SIGN W/POST WITHIN THE PROJECT LIMITS TO BE REMOVED BY CONTRACTOR AND SALVAGED TO I.D.O.T.
 - EXISTING SIGN
 - EXISTING SIGN W/POST
 - PROPOSED SIGN
 - PROPOSED SIGN W/POST OR EXISTING SIGN AND POST RELOCATED
- NOTE:** ALL NEW AND RELOCATED SIGNS TO RECEIVE NEW METAL POST-TYPE B
- NOTE:** REMOVAL OF METAL POST - TYPE B AFTER SIGN PANEL HAS BEEN RELOCATED TO NEW POST SHALL BE CONSIDERED INCLUDED IN THE COST OF "RELOCATE SIGN PANEL - TYPE 1"
- NOTE:** SHORT-TERM PAVEMENT MARKING IS PROPOSED ON THE MILLED PAVEMENT, NEW BINDER AND ON THE NEW SURFACE. SEE ARTICLE 703.04 OF THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".
- NOTE:** ALL "ARROWS" AND "ONLAYS" SHALL BE 8' IN HEIGHT.
- NOTE:** PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4", 11" O/C (NOTE: A) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.
- NOTE:** SEE SHEET 69 FOR MAST ARM MOUNTED STREET NAME SIGNS.
- FOR RAISED REFLECTIVE PAVEMENT MARKING DETAILS SEE SHEET 78 FOR DISTRICT DETAIL TC-11**
- FOR PAVEMENT MARKING DETAILS SEE SHEET 79 FOR DISTRICT DETAIL TC-13**



PLAN :
McCARTHY ROAD- (PAVEMENT MARKING AND SIGNING)
 SCALE: AS NOTED SHEET NO. OF SHEETS STA. 296+74 TO STA. 318+00

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 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00028

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	37
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



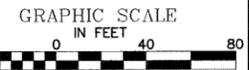
<p>① PROPOSED SIGN</p> <p>① EXISTING SIGN</p>						
						RESERVED
						RESERVED

IMPORTANT!
 FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES.
 REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

- THERMOPLASTIC STRIPING CODE**
- A PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" (LANE AND EDGE LINE)
 - B PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTERLINE 4" ,11" O/C
 - C PROPOSED SKIP-DASH YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" - 10' DASH, 30' SKIP (LANE LINE)
 - D PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK MARKING)
 - E PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE)
 - F PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 8" (LANE AND EDGE LINE)
 - G PROPOSED SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 12", 45' DIAGONALS, (MEDIAN LINE)
 - H PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR)
 - I PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS
 - J PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 12", 45' DIAGONALS
 - K PROPOSED SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" (LANE LINE)
 - L PROPOSED SKIP-DASH WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" - 2' DASH, 6' SKIP (LANE LINE)
 - M PROPOSED SKIP-DASH WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" - 2' DASH, 6' SKIP (LANE LINE)
- REFLECTIVE PAVEMENT MARKERS**
- N PROPOSED RAISED REFLECTIVE PAVEMENT MARKERS - 80' O/C OR AS SHOWN
 - ◆ TWO-WAY AMBER MARKER
 - ▶ ONE-WAY AMBER MARKER
 - ▷ ONE-WAY CRYSTAL MARKER

DETECTOR LOOPS
 FOR LOOP DETECTOR PLACEMENT AT McCARTHY ROAD AND DERBY ROAD, SEE SHEET 54
 FOR LOOP DETECTOR PLACEMENT AT McCARTHY ROAD AND ARCHER AVENUE, SEE SHEETS 63 & 64

- LEGEND**
- ① DENOTES EXISTING SIGN
 - ① DENOTES PROPOSED SIGN
 - A DENOTES PROPOSED STRIPING
 - R DENOTES EXISTING SIGN OR SIGN W/POST WITHIN THE PROJECT LIMITS TO BE REMOVED BY CONTRACTOR AND RESET BY THE CONTRACTOR UPON COMPLETION OF CURB AND GUTTER CONSTRUCTION.
 - NL DENOTES NEW SIGN LOCATION
 - S DENOTES EXISTING SIGN OR SIGN W/POST WITHIN THE PROJECT LIMITS TO BE REMOVED BY CONTRACTOR AND SALVAGED TO THE VILLAGE OF LEMONT PUBLIC WORKS DEPARTMENT.
 - I DENOTES EXISTING SIGN OR SIGN W/POST WITHIN THE PROJECT LIMITS TO BE REMOVED BY CONTRACTOR AND SALVAGED TO I.D.O.T.
 - EXISTING SIGN
 - EXISTING SIGN W/POST
 - PROPOSED SIGN
 - PROPOSED SIGN W/POST OR EXISTING SIGN AND POST RELOCATED
- NOTE:** ALL NEW AND RELOCATED SIGNS TO RECEIVE NEW METAL POST-TYPE B
- NOTE:** REMOVAL OF METAL POST - TYPE B AFTER SIGN PANEL HAS BEEN RELOCATED TO NEW POST SHALL BE CONSIDERED INCLUDED IN THE COST OF "RELOCATE SIGN PANEL - TYPE 1"
- NOTE:** SHORT-TERM PAVEMENT MARKING IS PROPOSED ON THE MILLED PAVEMENT, NEW BINDER AND ON THE NEW SURFACE. SEE ARTICLE 703.04 OF THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".
- NOTE:** ALL "ARROWS" AND "ONLYS" SHALL BE 8' IN HEIGHT.
- NOTE:** PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4", 11" O/C (NOTE: A)) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.
- NOTE:** SEE SHEET 69 FOR MAST ARM MOUNTED STREET NAME SIGNS.
- FOR RAISED REFLECTIVE PAVEMENT MARKING DETAILS SEE SHEET 78 FOR DISTRICT DETAIL TC-11
 FOR PAVEMENT MARKING DETAILS SEE SHEET 79 FOR DISTRICT DETAIL TC-13



FILE NAME = FAU 1587 (McCARTHY ROAD)
 FAP 577 (ARCHER AVENUE)
 FAU 1024 (DERBY ROAD)
 CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION

USER NAME =
 DESIGNED - JLC
 DRAWN - JFP-JEP
 CHECKED - THK
 DATE - 11/10

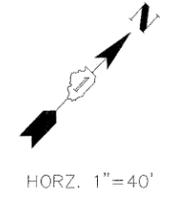
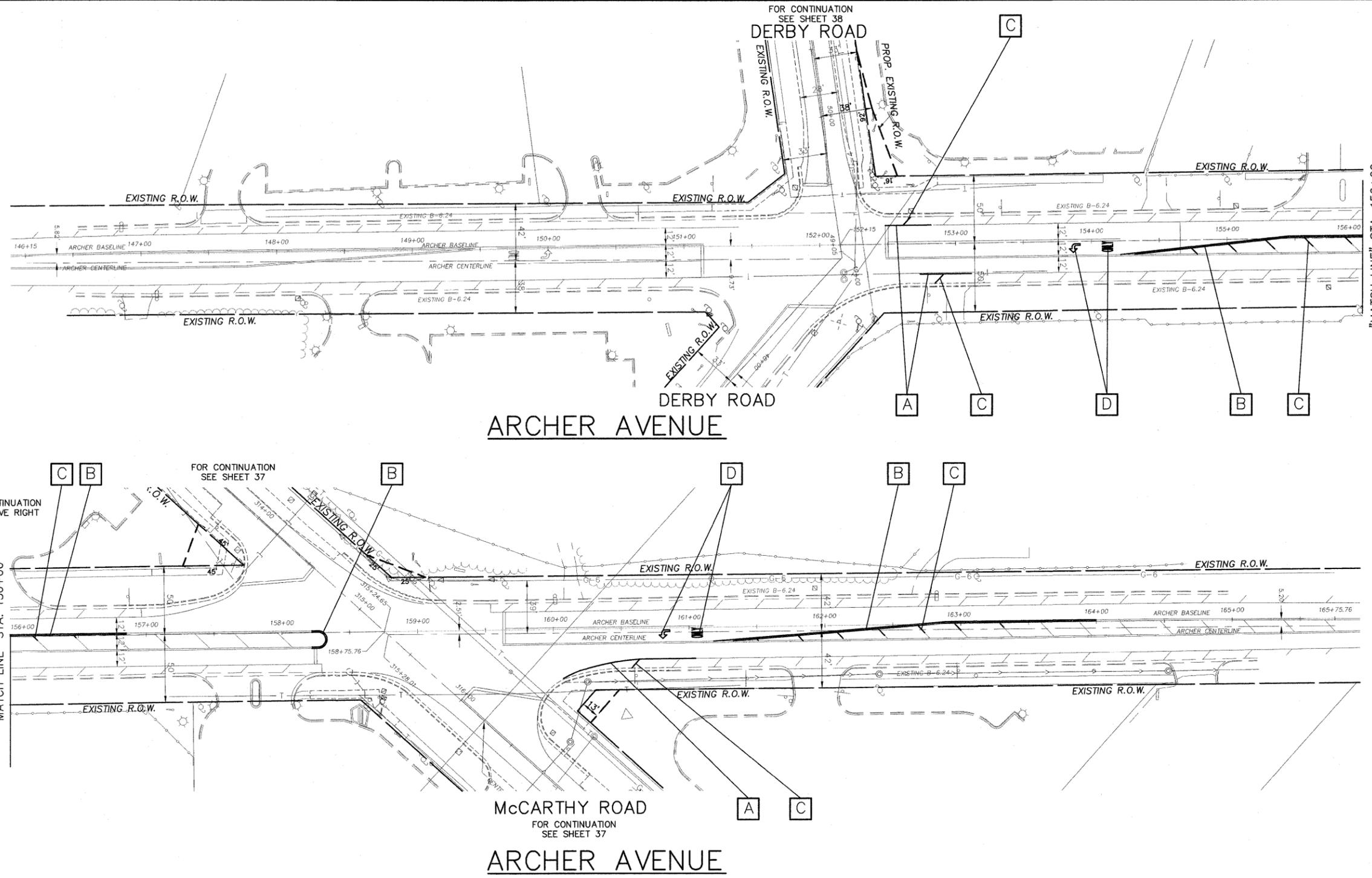
REVISED - THK 2-25-11
 REVISED - THK 9-21-11
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN:
McCARTHY ROAD- (PAVEMENT MARKING AND SIGNING)
DERBY ROAD- (PAVEMENT MARKING AND SIGNING)
 SCALE: AS NOTED SHEET NO. OF SHEETS STA. 318+00 TO STA. 323+23
 49+24 55+53

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 Civil Engineers
 Municipal Consultants
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000628

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	38
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



THERMOPLASTIC STRIPING CODE REMOVAL

- A** EXISTING SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" (EDGE LINE)
- B** EXISTING DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTERLINE 4" ,11"O/C
- C** EXISTING SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 12", 45' DIAGONALS, (MEDIAN LINE)
- D** EXISTING SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS

REFLECTIVE PAVEMENT MARKER REMOVAL

ON ARCHER AVENUE, EXISTING RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REMOVED AND REPLACED AS NECESSARY TO ACCOMMODATE NEW PAVEMENT MARKING CONFIGURATION. SEE DISTRICT DETAIL TC-11.

FOR RAISED REFLECTIVE PAVEMENT MARKING DETAILS
SEE SHEET 78 FOR DISTRICT DETAIL TC-11
FOR PAVEMENT MARKING DETAILS
SEE SHEET 79 FOR DISTRICT DETAIL TC-13

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.



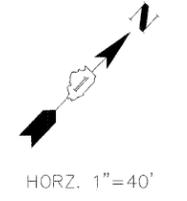
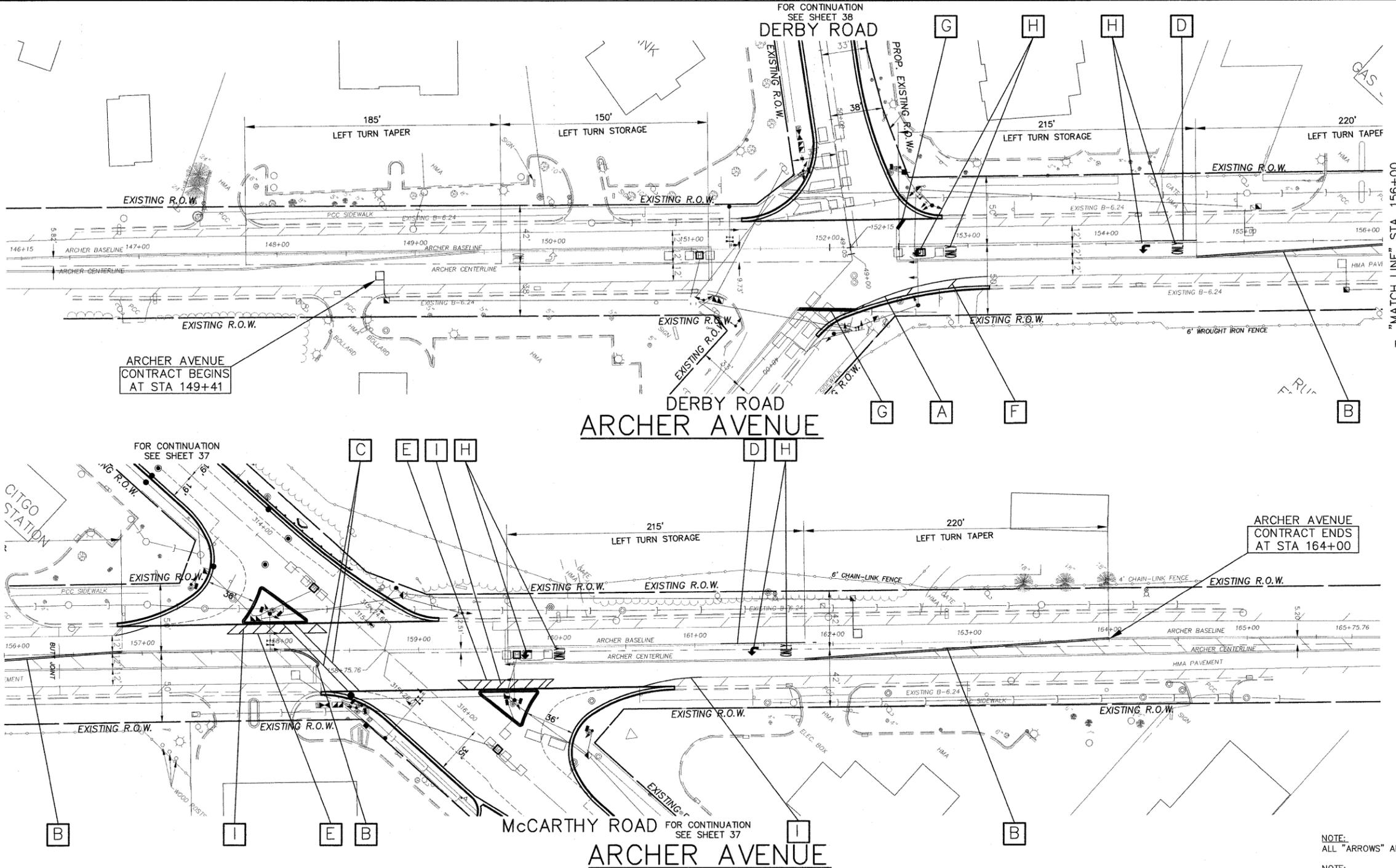
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 835 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 987-9640 • Fax: (630) 987-0132
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000828

FILE NAME = FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD)	USER NAME =	DESIGNED - JLC	REVISED - THK 2-25-11
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	PLOT SCALE =	DRAWN - JFP-JEP	REVISED -
	PLOT DATE =	CHECKED - THK	REVISED -
		DATE - 11/10	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLAN :
ARCHER AVENUE- (PAVEMENT MARKING REMOVAL)**
 SCALE: AS NOTED SHEET NO. OF SHEETS STA. 151+63 TO STA. 164+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	39
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



ARCHER AVENUE
CONTRACT BEGINS
AT STA 149+41

ARCHER AVENUE
CONTRACT ENDS
AT STA 164+00

- THERMOPLASTIC STRIPING CODE**
- A PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" (EDGE LINE)
 - B PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTERLINE 4" .11" O/C
 - C PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK MARKING)
 - D PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE)
 - E PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 8" (LANE AND EDGE LINE)
 - F PROPOSED SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 12", 45° DIAGONALS, (MEDIAN LINE)
 - G PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR)
 - H PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS
 - I PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 12", 45° DIAGONALS, (MEDIAN LINE)
- REFLECTIVE PAVEMENT MARKERS**
- ON ARCHER AVENUE, EXISTING RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REMOVED AND REPLACED AS NECESSARY TO ACCOMMODATE NEW PAVEMENT MARKING CONFIGURATION. SEE DISTRICT DETAIL TC-11.

NOTE:
ALL "ARROWS" AND "ONLYS" SHALL BE 8" IN HEIGHT.

NOTE:
PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - CENTER LINE 4" .11" O/C (NOTE: A) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.

FOR RAISED REFLECTIVE PAVEMENT MARKING DETAILS
SEE SHEET 78 FOR DISTRICT DETAIL TC-11

FOR PAVEMENT MARKING DETAILS
SEE SHEET 79 FOR DISTRICT DETAIL TC-13

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES.
REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.



FILE NAME = FAU 1587 (MC CARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED - JLC	REVISED - THK 2-25-11
	PLOT SCALE =	DRAWN - JFP-JEP	REVISED -
	PLOT DATE =	CHECKED - THK	REVISED -
		DATE - 11/10	REVISED -

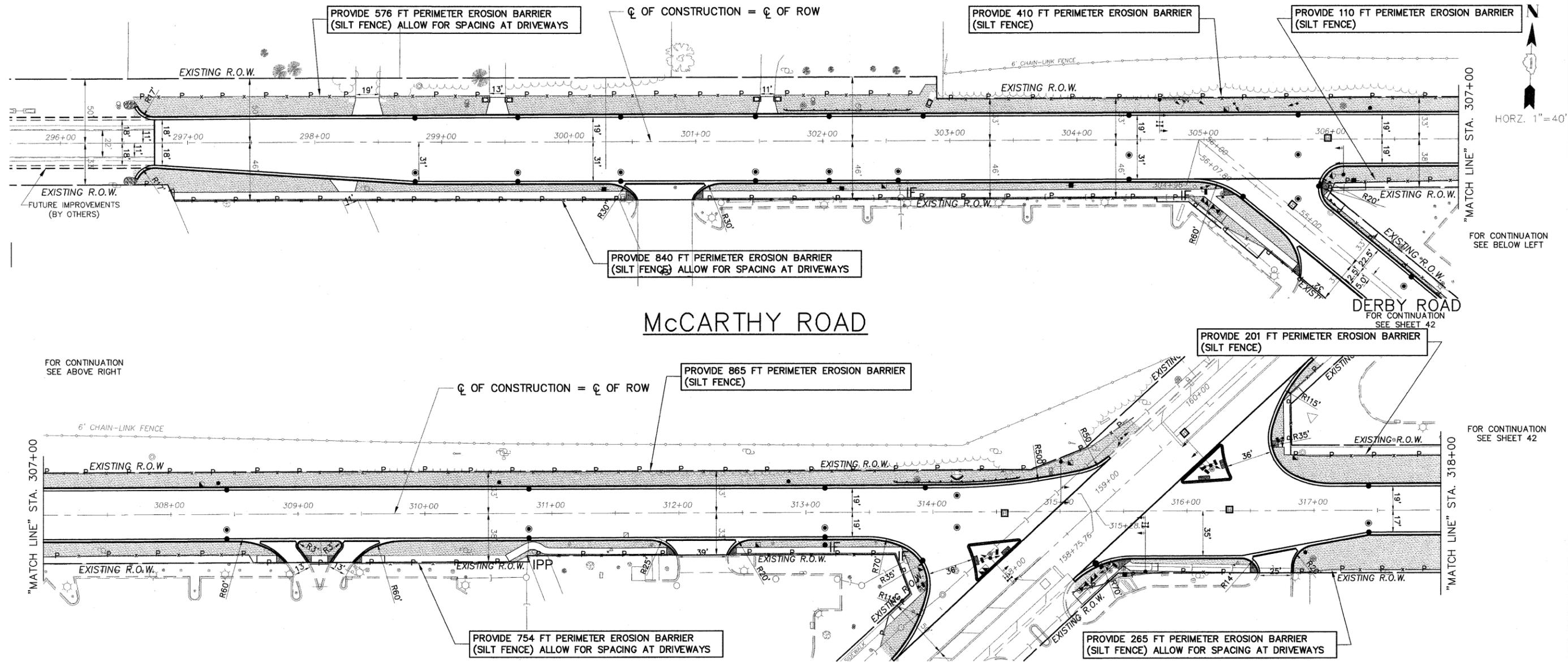
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN :
ARCHER AVENUE- (PAVEMENT MARKING)

SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. 151+63 TO STA. 164+00

Frank Novotny & Associates, Inc.
Civil Engineers
185 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0133
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000285

F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
RTE -	02-00055-00-WR	COOK	84	40
VAR			CONTRACT NO. 63557	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPP-M-8003(504)		



HORZ. 1" = 40'

FOR CONTINUATION SEE BELOW LEFT

FOR CONTINUATION SEE SHEET 42

FOR CONTINUATION SEE ABOVE RIGHT

SEDIMENT & EROSION NOTES

- A. ALL EROSION CONTROL MEASURES ARE TO BE IN PLACE BEFORE ANY WORK BEGINS ON SITE. ALL EROSION CONTROL ITEMS SHALL BE MAINTAINED THROUGHOUT THE PROJECT DURATION AND UNTIL ALL AREAS ARE PERMANENTLY STABILIZED. THE SILT FENCE SHALL BE INSPECTED IMMEDIATELY AND SEDIMENTS SHOULD BE REMOVED AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. THE SILT FENCE FABRIC SHOULD BE REPLACED IF IT BECOMES INEFFECTIVE BEFORE GROUND HAS BECOME PERMANENTLY STABILIZED.
- B. ANY SOIL, MUD OR DEBRIS THAT IS WASHED, TRACKED OR DEPOSITED ONTO THE STREET SHALL BE REMOVED BEFORE THE END OF EACH WORKDAY.
- C. THE SURFACE OF STRIPPED AREAS SHALL BE PERMANENTLY OR TEMPORARILY PROTECTED FROM SOIL EROSION WITHIN 14 DAYS AFTER FINAL GRADE IS REACHED. STRIPPED AREAS THAT WILL REMAIN UNDISTURBED FOR MORE THAN 14 DAYS AFTER INITIAL DISTURBANCE SHALL BE PROTECTED FROM EROSION. TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE MAINTAINED CONTINUOUSLY UNTIL PERMANENT COVER IS ESTABLISHED.
- D. NO STOCKPILES SHALL BE STORED ON SITE. EXCESS EARTH SHALL BE REMOVED UPON EXCAVATION.
- E. STORM SEWER INLETS SHALL BE PROTECTED WITH SEDIMENT TRAPPING OR FILTER CONTROL DEVICES DURING CONSTRUCTION.
- F. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED.
- G. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED WITH PERMANENT SOIL STABILIZATION MEASURES.

ENDANGERED SPECIES NOTES

- ACCIDENTAL TRAPPING OF WILDLIFE IN CONSTRUCTION TRENCHES AND IN EROSION CONTROL BLANKET MUST BE AVOIDED.
- 1) NO CONSTRUCTION PITS OR TRENCHES SHALL BE LEFT OPEN OVERNIGHT.
 - 2) THE CONSTRUCTION WORK AREAS SHALL BE INSPECTED EACH MORNING PRIOR TO START OF WORK TO ENSURE THAT THERE IS NO WILDLIFE ENTRAPMENT.

LEGEND

- P - P - DENOTES PERIMETER EROSION BARRIER (SILT FENCE)
- [Hatched Box] DENOTES "TOPSOIL FURNISH & PLACE, 4 INCH" AND "SODDING"
- IF DENOTES DRAINAGE STRUCTURE INLET FILTER
- IPP DENOTES INLET PIPE AND PROTECTION

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.



FOR EROSION CONTROL NOTES AND DETAILS SEE SHEET 43 AND 44

Frank Novotny & Associates, Inc.
 835 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 987-8640 • Fax: (630) 987-0132
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000228

FILE NAME FAJ 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED -- JLC	REVISED -- THK 2-25-11
	PLOT SCALE =	DRAWN -- JFP-JEP	REVISED -- THK 9-21-11
	PLOT DATE =	CHECKED -- THK	REVISED --
		DATE -- 11/10	REVISED --

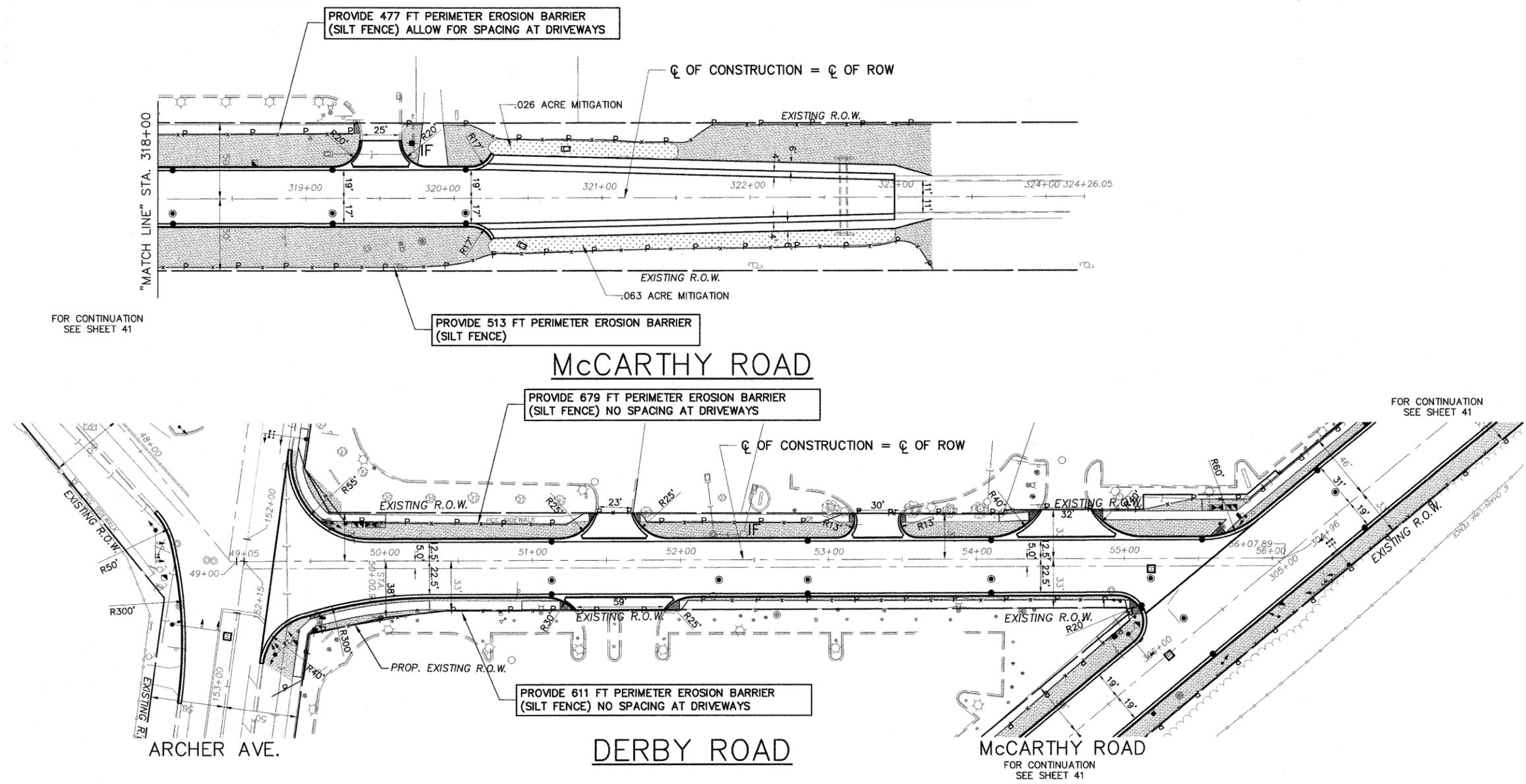
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN :
McCARTHY ROAD- (LANDSCAPE PLAN AND SEDIMENT AND EROSION CONTROL)

SCALE: AS NOTED | SHEET NO. OF SHEETS | STA. 296+74 TO STA. 318+00

F.A. - RTE. - VAR	SECTION 02-00055-00-WR	COUNTY COOK	TOTAL SHEETS 84	SHEET NO. 41
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

N
 1"=40'
 1"=5'



FOR CONTINUATION
 SEE SHEET 41

FOR CONTINUATION
 SEE SHEET 41

FOR CONTINUATION
 SEE SHEET 41

SEDIMENT & EROSION NOTES

- A. ALL EROSION CONTROL MEASURES ARE TO BE IN PLACE BEFORE ANY WORK BEGINS ON SITE. ALL EROSION CONTROL ITEMS SHALL BE MAINTAINED THROUGHOUT THE PROJECT DURATION AND UNTIL ALL AREAS ARE PERMANENTLY STABILIZED. THE SILT FENCE SHALL BE INSPECTED IMMEDIATELY AND SEDIMENTS SHOULD BE REMOVED AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. THE SILT FENCE FABRIC SHOULD BE REPLACED IF IT BECOMES INEFFECTIVE BEFORE GROUND HAS BECOME PERMANENTLY STABILIZED.
- B. ANY SOIL, MUD OR DEBRIS THAT IS WASHED, TRACKED OR DEPOSITED ONTO THE STREET SHALL BE REMOVED BEFORE THE END OF EACH WORKDAY.
- C. THE SURFACE OF STRIPPED AREAS SHALL BE PERMANENTLY OR TEMPORARILY PROTECTED FROM SOIL EROSION WITHIN 14 DAYS AFTER FINAL GRADE IS REACHED. STRIPPED AREAS THAT WILL REMAIN UNDISTURBED FOR MORE THAN 14 DAYS AFTER INITIAL DISTURBANCE SHALL BE PROTECTED FROM EROSION. TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE MAINTAINED CONTINUOUSLY UNTIL PERMANENT COVER IS ESTABLISHED.
- D. NO STOCKPILES SHALL BE STORED ON SITE. EXCESS EARTH SHALL BE REMOVED UPON EXCAVATION.
- E. STORM SEWER INLETS SHALL BE PROTECTED WITH SEDIMENT TRAPPING OR FILTER CONTROL DEVICES DURING CONSTRUCTION.
- F. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED.
- G. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED WITH PERMANENT SOIL STABILIZATION MEASURES.

ENDANGERED SPECIES NOTES

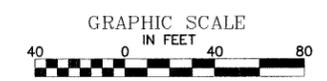
- ACCIDENTAL TRAPPING OF WILDLIFE IN CONSTRUCTION TRENCHES AND IN EROSION CONTROL BLANKET MUST BE AVOIDED.
- 1) NO CONSTRUCTION PITS OR TRENCHES SHALL BE LEFT OPEN OVERNIGHT.
 - 2) THE CONSTRUCTION WORK AREAS SHALL BE INSPECTED EACH MORNING PRIOR TO START OF WORK TO ENSURE THAT THERE IS NO WILDLIFE ENTRAPMENT.

LEGEND

- DENOTES PERIMETER EROSION BARRIER (SILT FENCE)
- DENOTES "TOPSOIL FURNISH & PLACE, 4 INCH" AND "SODDING"
- DENOTES MITIGATION FOR WETLAND IMPACT. MITIGATION FOR WETLAND IMPACTED AREAS (STA. 320+20 TO STA. 323+00) WHICH SHALL CONSIST OF EROSION CONTROL BLANKET WITH TOPSOIL PLACEMENT AND RE-SEEDING WITH AGROSTIS ALBA (RED TOP) AT 5 LBS./ACRE (PAID FOR AS SEEDING, SPECIAL). A MITIGATION RATIO OF 1.0:1.0 WILL BE APPLIED, REQUIRING 0.089 ACRE MITIGATION
- DENOTES DRAINAGE STRUCTURE INLET FILTER

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.



FOR EROSION CONTROL NOTES AND DETAILS
 SEE SHEET 43 AND 44

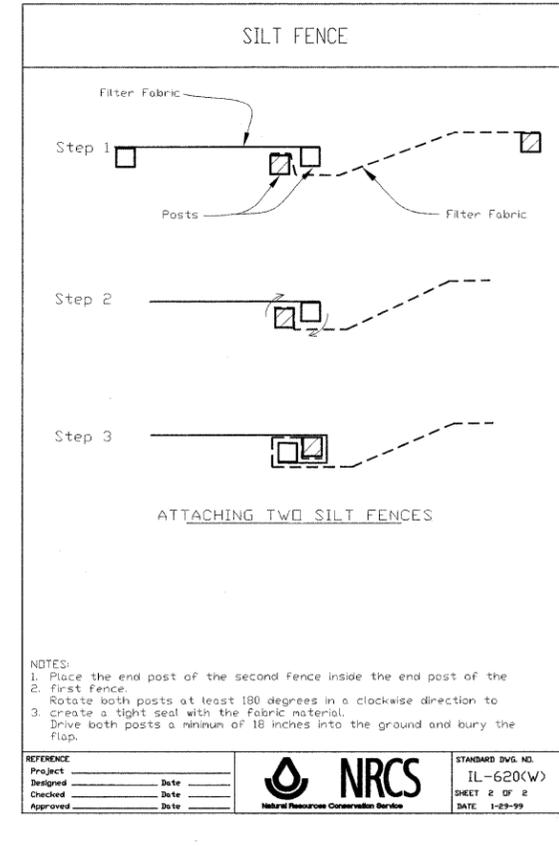
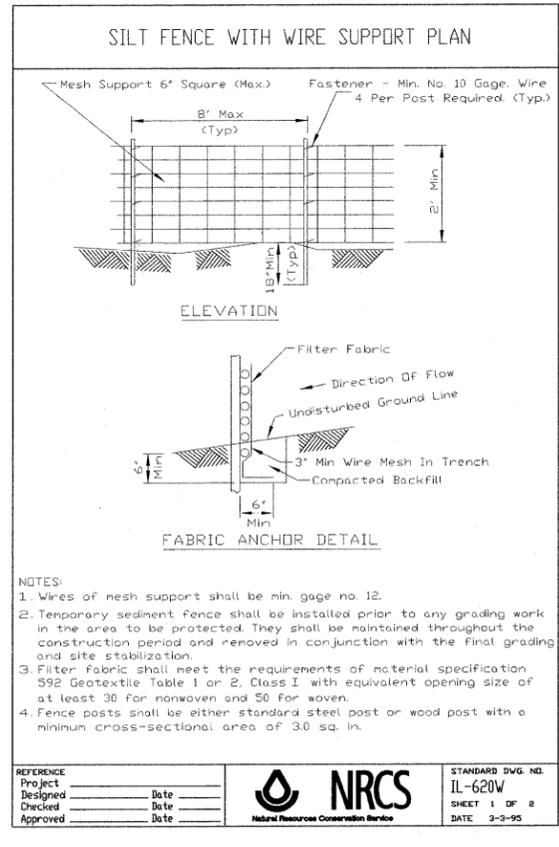
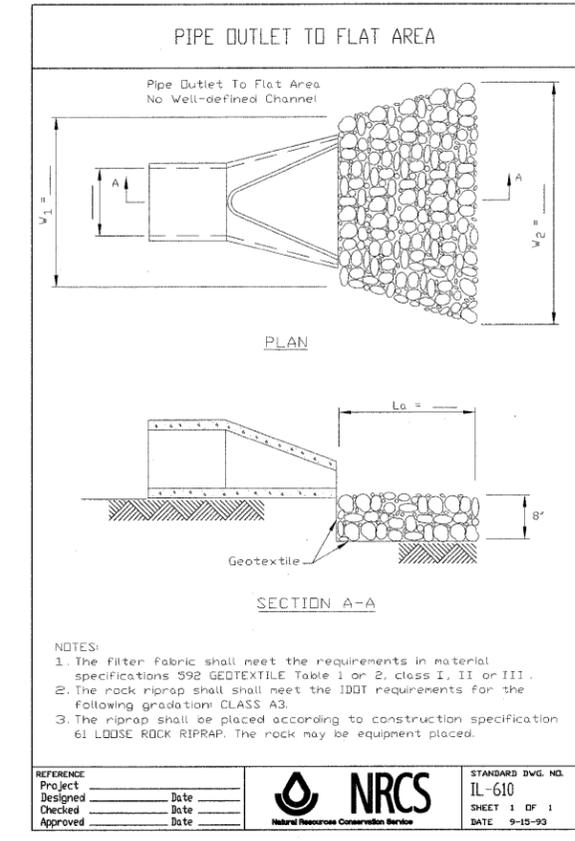
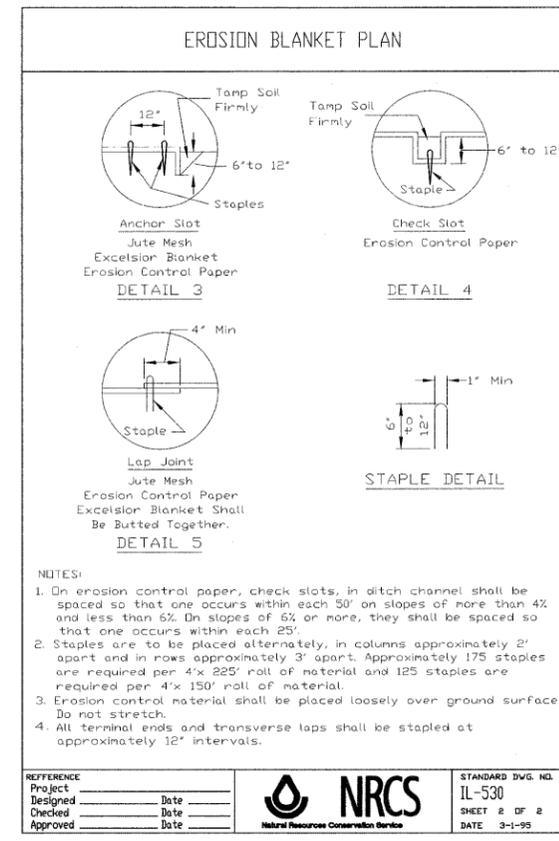
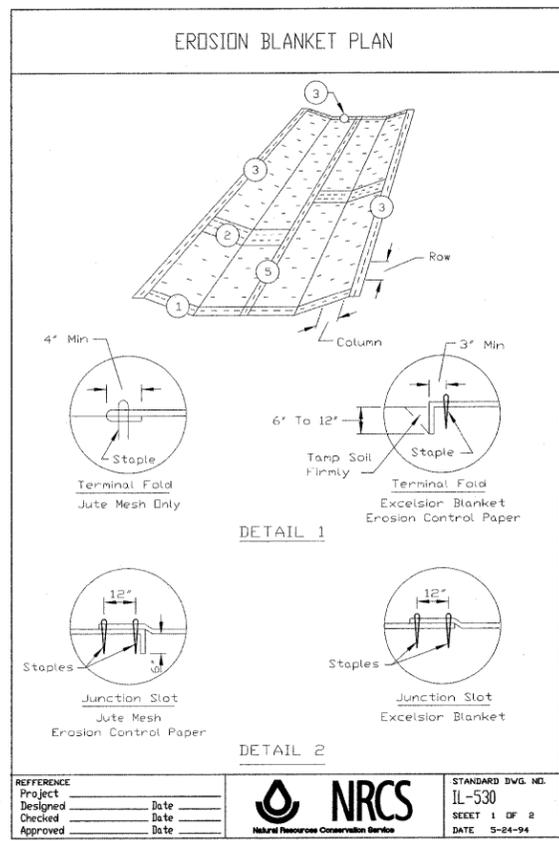
Frank Novotny & Associates, Inc.
 825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-9640 • Fax: (630) 887-0132
 Civil Engineers
 Municipal Consultants
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000228

FILE NAME = FAU 1587 (McCARTHY ROAD) FAP 577 (ARCHER AVENUE) FAU 1024 (DERBY ROAD) CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION	USER NAME =	DESIGNED - JLC DRAWN - JFP-JEP CHECKED - THK DATE - 11/10	REVISED - THK 2-25-11 REVISED - THK 9-21-11 REVISED - REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN: McCARTHY ROAD- (LANDSCAPE PLAN AND SEDIMENT AND EROSION CONTROL) DERBY ROAD- (LANDSCAPE PLAN AND SEDIMENT AND EROSION CONTROL)	SCALE: AS NOTED	SHEET NO. OF SHEETS	STA. 318+00 TO STA. 323+23
		49+24	55+53

F.A. R.T.E. VAR	SECTION 02-00055-00-WR	COUNTY COOK	TOTAL SHEETS 84	SHEET NO. 42
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



GENERAL NOTES

THE CONTRACTOR WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATIONS TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION AREA. THE CONTRACTOR'S ATTENTION IS CALLED TO THE STANDARD SPECIFICATIONS FOR TEMPORARY EROSION CONTROL.

PERMANENT DRAINAGE FEATURES SHALL BE CONSTRUCTED AS SOON AS POSSIBLE.

SOIL EROSION CONTROL NOTES

THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN EROSION CONTROL MEASURES PRIOR TO EXISTING VEGETATION.

THE CONSTRUCTION LIMITS SHALL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.

ALL DRAINAGE STRUCTURES WITH OPEN GRATES IN THE PARKWAY SHALL HAVE INLET AND PIPE PROTECTION INSTALLED AROUND THE FRAME (REFERENCE STANDARD IL-563). INSTALLATIONS SHALL BE INSPECTED AFTER EVERY RUNOFF PRODUCING RAIN AND SEDIMENT SHALL BE REMOVED ONCE IT HAS ACCUMULATED TO ONE-HALF THE HEIGHT OF THE BARRIER (ILLINOIS URBAN MANUAL STANDARD 563).

PERIMETER EROSION BARRIER SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER (REFERENCE STANDARD IL-620). SEDIMENT SHALL BE REMOVED AFTER EACH RAINFALL AND ONCE IT HAS ACCUMULATED TO ONE-HALF THE HEIGHT OF THE BARRIER (ILLINOIS URBAN MANUAL STANDARD 920).

ALL SALT TOLERANT SOD SHALL BE INSTALLED AS DIRECTED BY ENGINEER. SOD SHALL BE WATERED EVERY WEEK TO TWO WEEKS AND FERTILIZER APPLIED AS DIRECTED BY THE ENGINEER (ILLINOIS URBAN MANUAL STANDARD 925).

SEEDING SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER. SEEDING SHALL BE CLASS 4A, A PERENNIAL RYEGRASS, AND PERFORMED BETWEEN APRIL 1 AND JUNE 1, IF PERFORMED IN THE SPRING, OR AUGUST 1 AND SEPTEMBER 1, IF PERFORMED IN THE FALL. SEEDING SHALL BE PERFORMED AT A RATE OF 43 LBS/ACRE (ILLINOIS URBAN MANUAL STANDARD 880).

THE SOIL AND WATER CONSERVATION DISTRICT IS RESPONSIBLE FOR CONDUCTING SITE VISITS AND VERIFYING THAT THE PRACTICES ARE WORKING PROPERLY AND DETERMINE IF ADDITIONAL PRACTICES ARE NEEDED FOR BETTER SOIL EROSION AND SEDIMENT CONTROL. IF ADDITIONAL PRACTICES ARE DEEMED NECESSARY BY THE SWCD, THE CONTRACTOR WILL IMPLEMENT THE PRACTICES OF THE ILLINOIS URBAN MANUAL IN A TIMELY MANNER.

SOIL EROSION AND SEDIMENTATION CONTROL - NOTES

- GENERAL**
 - THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE PROVISIONS OF THE ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL, AND IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENTATION CONTROL.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER INSTALLATION AND MAINTENANCE OF ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES.
 - SEDIMENT AND EROSION CONTROL DEVICES SHALL BE FUNCTIONAL BEFORE LAND IS OTHERWISE DISTURBED ON THE SITE.
 - SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLOTATION BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEANUP FUEL OR CHEMICAL SPILLS AND LEAKS.
 - DUST ON THE SITE SHALL BE CONTROLLED. THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATION IS PROHIBITED.
 - SLOPES SHALL BE LEFT IN A ROUGHENED CONDITION DURING THE GRADING PHASE TO REDUCE RUNOFF VELOCITIES AND EROSION.
 - REPAIR ANY SILTATION OR EROSION DAMAGE TO ADJOINING SURFACES AND DRAINAGE WAYS RESULTING FROM LAND DEVELOPING OR DISTURBING ACTIVITIES.
- IMPLEMENTATION**
 - BEFORE STARTING SITE GRADING WORK, PERIMETER EROSION CONTROL (SILT FENCES) SHALL BE INSTALLED AS SHOWN ON THE PLANS. IF DIRECTED BY THE RESIDENT ENGINEER, THE CONTRACTOR SHALL INSTALL ADDITIONAL SILT FENCES WHERE NEEDED.
 - GRAVELED ROADS, ACCESS DRIVES, PARKING AREAS OF SUFFICIENT WIDTH AND LENGTH, AND VEHICLE WASH DOWN FACILITIES, IF NECESSARY, SHALL BE PROVIDED TO PREVENT SOIL FROM BEING TRACKED ONTO PUBLIC OR PRIVATE ROADWAYS. ANY SOIL REACHING A PUBLIC OR PRIVATE ROADWAY SHALL BE REMOVED BEFORE THE END OF EACH WORKDAY.
 - ANY PUBLIC AND/OR PRIVATE ROADS THAT ARE ADJACENT TO THE SITE AND USED FOR INGRESS AND EGRESS, SHALL BE MONITORED AND SWEEPED WHEN DIRTY AT THE DIRECTION OF THE VILLAGE OF LEMONT ENGINEER.
 - STAKED STRAW BALES SHALL BE INSTALLED AND MAINTAINED AROUND INTAKE STRUCTURES (I.E. INLETS, CATCH BASINS) AS SHOWN ON THE PLAN. THE CONTRACTOR, AT HIS OPTION, MAY USE SILT FENCES INSTEAD OF STRAW BALES.
 - IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN 10 DAYS, SEDIMENT AND EROSION CONTROL SHALL BE PROVIDED AROUND SUCH STOCKPILE. IF MORE THAN 2 MONTHS, THEN IT IS REQUIRED THAT THE STOCKPILE BE SEED SO AS TO MINIMIZE SOIL EROSION BY BOTH WIND AND WATER.
- INSPECTION AND MAINTENANCE**
 - THE TEMPORARY EROSION CONTROL MEASURES SHALL BE IN PLACE AND WORK EFFECTIVELY UNTIL ALL THE PERMANENT EROSION CONTROL ITEMS ARE FULLY FUNCTIONAL.
 - THE CONTRACTOR SHALL INSPECT EROSION CONTROL MEASURES WEEKLY AND AFTER ANY STORM EVENT IN EXCESS OF 1/2". ANY DEFICIENCIES SHALL BE CORRECTED IMMEDIATELY.
 - AT THE COMPLETION OF THE PROJECT, ALL STORM SEWER PIPES AND STRUCTURES SHALL BE CLEANED AND FREE OF DIRT AND DEBRIS. THE SEDIMENTATION SHALL BE REMOVED FROM THE STORM SEWER SYSTEM AND SHALL NOT BE WASHED OUT IN THE STORM SEWER SYSTEM.

FILE NAME = FAU 1587 (McCARTHY ROAD)
FAP 577 (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
CHANNELIZATION AND
TRAFFIC SIGNAL INSTALLATION

USER NAME =
DESIGNED -- JLC
DRAWN -- JFP
CHECKED -- THK
PLOT DATE =
DATE -- 11/10

REVISED -- THK 2-25-11
REVISED --
REVISED --
REVISED --

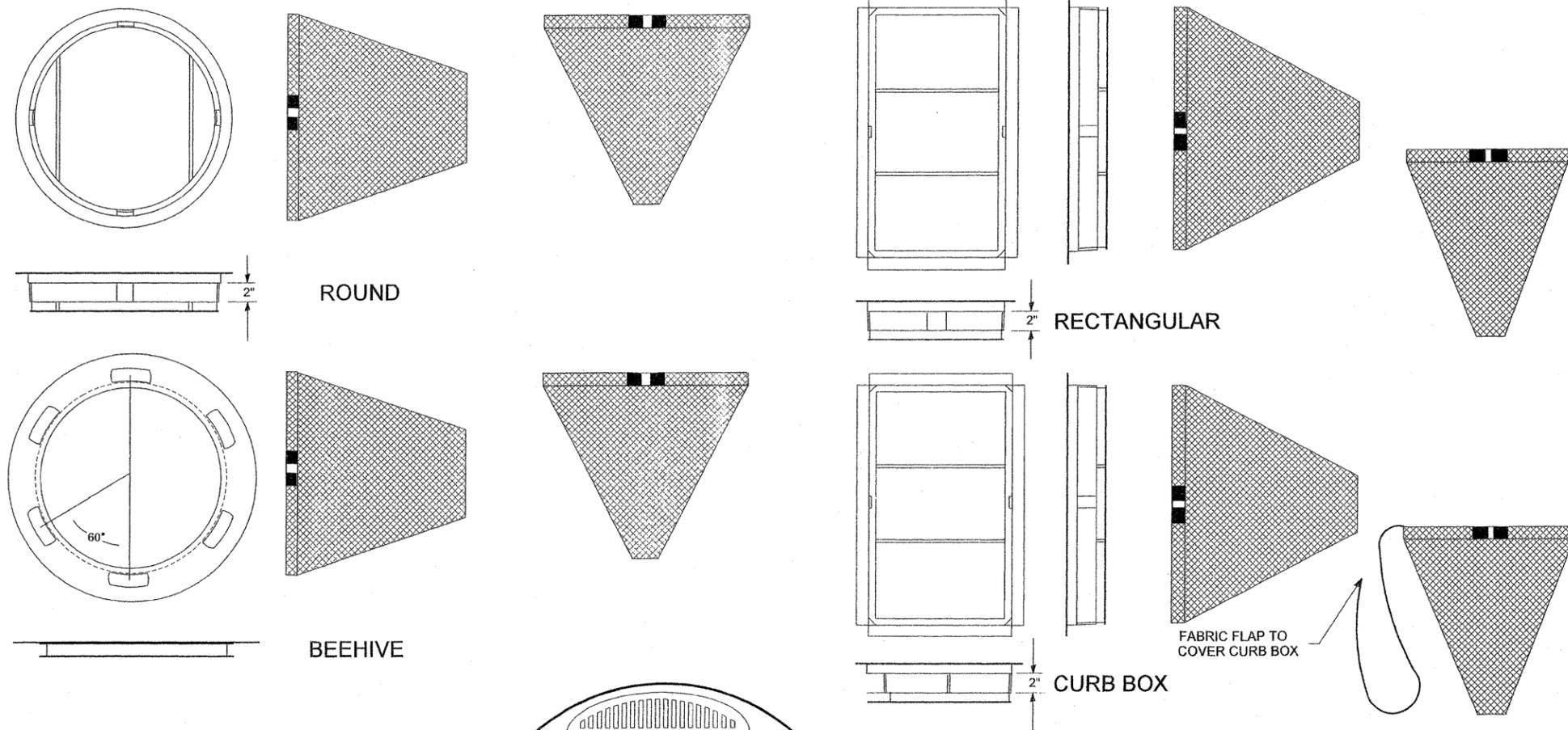
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL NOTES AND DETAILS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
205 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 897-8640 • Fax: (630) 897-0123
Illinois Professional Design Firm No. 194-000263

F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	43
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



INLET FILTER

DESCRIPTION: This work shall consist of furnishing, installation, and removal of a drainage structure inlet filter assembly, consisting of a frame and filter bag, to collect sediment in surface storm water runoff at locations shown on the plans or as directed by the Engineer.

The Contractor shall inspect the worksite and review the plans to determine the number and dimensions of the various types of drainage structure frames (circular and rectangular) into which the inlet filters will be installed prior to ordering materials.

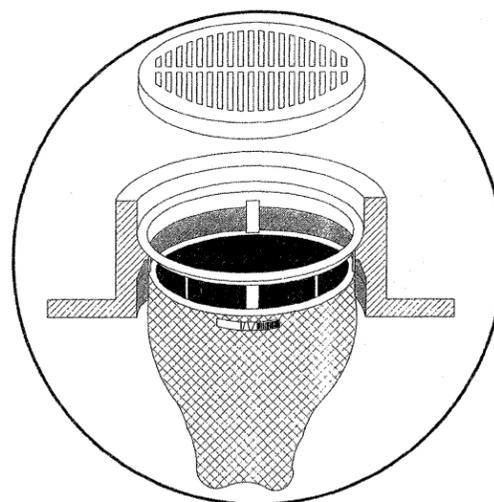
The drainage structure inlet filter assembly shall be installed under the grate on the lip of the drainage structure frame with the fabric bag hanging down into the drainage structure.

The drainage structure inlet filter assembly shall remain in place until final removal of the assembly is directed by the Engineer. The drainage structure inlet filter assembly shall remain the property of the Contractor.

Final removal of the assembly shall include the disposal of debris or silt that has accumulated in the filter bag at the time of final removal. Periodic cleaning of the filter is paid for separately.

The drainage structure inlet filter assembly shall consist of a steel frame with a replaceable geotextile fabric bag attached with a steel band with locking cap that is suspended from the frame. A clean used bag and used steel frame in good condition, meeting the approval of the Engineer, may be substituted for new materials.

The drainage structure inlet filter assembly frame shall be rigid steel meeting the requirements of ASTM-A36. The frame shall include an overflow feature that is welded to the frame's ring. The overflow feature shall be designed to allow full flow of water into the structure if the filter bag is filled with sediment. The dimensions of the assembly frame shall allow the drainage structure grate to fit into the inlet filter assembly frame opening. The assembly frame shall rest on the inside lip of the drainage structure frame for the full variety of existing and proposed drainage structure frames that are present on this contract.



DRAINAGE STRUCTURE INLET FILTER

The drainage structure inlet filter assembly bag shall be constructed of polypropylene geotextile fabric with a minimum weight of 4 ounces per square yard, minimum flow rate of 145 gallons per minute per square foot, and designed for minimum silt and debris capacity of 2 cubic feet. The filter bag shall be reinforced with an outer layer of polyester mesh fabric with a minimum weight of 4 ounces per square yard. The filter bag shall be suspended from the steel frame with a stainless steel band and locking cap. The inlet filter assembly frame shall not cause the drainage structure grate to extend higher than 1/8-inch above the drainage structure frame.

BASIS OF PAYMENT: The work will be paid for at the contract unit price per EACH for INLET FILTER, which price shall include all cost of labor, materials, equipment, and incidental items necessary to perform the work.

TEMPORARY EROSION CONTROL			
COMPUTED	MAL	APPROVED	20 05
DRAWN	MAL		
CHECKED	SDR		
DRAINAGE AND UTILITIES DIVISION HEAD			

FILE NAME = FAU 1587 (McCARTHY ROAD)
 FAP 577 (ARCHER AVENUE)
 FAU 1024 (DERBY ROAD)
 CHANNELIZATION AND
 TRAFFIC SIGNAL INSTALLATION

USER NAME =	DESIGNED -- JLC	REVISED -- THK 2-25-11
	DRAWN -- JFP	REVISED --
PLOT SCALE =	CHECKED -- THK	REVISED --
PLOT DATE =	DATE -- 11/10	REVISED --

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL NOTES AND DETAILS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
 Civil Engineers
 Municipal Consultants
 255 Midway Drive • Wheelbrook, IL • 60097 • Telephone: (630) 887-9640 • Fax: (630) 887-0132
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000208

F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	44
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM SEWER WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE UNDER NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE CONSTRUCTION SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIME FRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN IN STANDARD 280001 OF THE PLANS. SECTION 280, TEMPORARY EROSION CONTROL, OF THE STANDARD SPECIFICATIONS ADDITIONALLY SUPPLEMENTS THIS PLAN.

SITE DESCRIPTION DESCRIPTION OF CONSTRUCTION ACTIVITY

- THE PROJECT IS LOCATED IN THE VILLAGE OF LEMONT, ILLINOIS, ALONG MCCARTHY ROAD FROM DERBY ROAD TO ARCHER AVENUE, ALSO ON FAU 1024 (DERBY ROAD) FROM MCCARTHY ROAD TO ARCHER AVENUE AND ON FAP 577 (IL ROUTE 17) (ARCHER AVENUE) FROM DERBY ROAD TO MCCARTHY ROAD.
- CONSTRUCTION INCLUDES TRENCH EXCAVATION, STORM SEWER INSTALLATION, PAVEMENT WIDENING, FULL DEPTH PAVEMENT, PATCHING, DRIVEWAY REPLACEMENT, LANDSCAPING, AND OTHER MISCELLANEOUS ITEMS OF CONSTRUCTION.

DESCRIPTION OF INTENDED SEQUENCE FOR MAJOR CONSTRUCTION ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTION OF THE CONSTRUCTION SITE

- INSTALL EROSION CONTROL MEASURES.
- EXCAVATION WILL BE COMPLETED ALONG MCCARTHY ROAD, ALONG ARCHER AVENUE, AND ON DERBY ROAD FOR PLACEMENT OF PERMANENT TYPE PAVEMENTS, INCLUDING DRIVING AND TURNING LANES.
- INSTALL STORM SEWER PIPE AND APPURTANCES.
- WATER JET TRENCHES FOR SETTLEMENT, AND COMPLETE PROJECT LANDSCAPING AND PAVEMENT RESTORATION.
- PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER EROSION CONTROL BARRIER, TEMPORARY DITCH CHECKS, INLET AND PIPE PROTECTION.
- FINAL GRADING, AND OTHER MISCELLANEOUS ITEMS.
- PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS TOPSOIL AND SEEDING.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 3.70 ACRES BY WHICH 3.70 ACRES WILL BE DISTURBED BY TRENCHING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

- INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILITIES FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
- PROJECT PLAN DOCUMENTS, SPECIFICATIONS AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

- ILLINOIS & MICHIGAN CANAL

CONTROLS, EROSION CONTROLS AND SEDIMENT CONTROL:

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

- AREAS OF EXISTING VEGETATION, WOOD AND GRASSLANDS, OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.
- DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER, ALONG WITH REQUIRED TREE REMOVAL.
- AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET AND PIPE PROTECTION, AND PERIMETER EROSION BARRIER SHALL BE INSTALLED AS CALLED OUT IN THIS PLAN AND DIRECTED BY THE ENGINEER.
- BARE AND SPARSELY VEGETATED GROUND IN HIGH ERODABLE AREAS AS DETERMINED BY THE ENGINEER SHALL BE TEMPORARILY SEEDED AT THE BEGINNING OF CONSTRUCTION WHERE NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN (7) DAYS.
- IMMEDIATELY AFTER TREE REMOVAL IS COMPLETED, AREAS WHICH ARE HIGHLY ERODABLE AS DETERMINED BY THE ENGINEER, SHALL BE TEMPORARILY SEEDED WHEN NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN (7) DAYS.
- AT LOCATIONS WHERE A SIGNIFICANT AMOUNT OF WATER DRAINS INTO THE CONSTRUCTION ZONE FROM OUTSIDE AREAS ON ADJACENT LANDOWNERS, TEMPORARY DITCH CHECKS WILL BE UTILIZED TO LOCALLY DIVERT WATER, REDUCE FLOW RATES, AND COLLECT OUTSIDE SILTATION INSIDE THE RIGHT-OF-WAY LINE.

2. ESTABLISHMENT OF THESE TEMPORARY EROSION CONTROL MEASURES WILL HAVE ADDITIONAL BENEFITS TO THE PROJECT. DESIRABLE GRASS SEED WILL BECOME ESTABLISHED IN THESE AREAS AND WILL SPREAD SEEDS ONTO THE CONSTRUCTION SITE UNTIL PERMANENT SEEDING/MOWING AND OVERSEEDING CAN BE COMPLETED.

3. THE SOIL AND WATER CONSERVATION DISTRICT IS RESPONSIBLE FOR CONDUCTING SITE VISITS AND VERIFYING THAT THE PRACTICES ARE WORKING PROPERLY AND DETERMINE IF ADDITIONAL PRACTICES ARE NEEDED FOR BETTER SOIL EROSION AND SEDIMENT CONTROL. IF ADDITIONAL PRACTICES ARE DEEMED NECESSARY BY THE SWCD THE CONTRACTOR WILL IMPLEMENT THE PRACTICES IN A TIMELY MANNER.

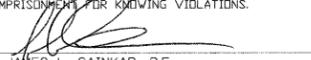
THE WILL/SOUTH COOK SOIL AND WATER CONSERVATION DISTRICT MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITIES, AND ONE WEEK PRIOR TO FINAL INSPECTION.

THE SOIL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSPECTED WEEKLY AND AFTER 1/2 INCH OF RAIN OR MORE BY THE INDIVIDUAL IN CHARGE OF SOIL EROSION AND SEDIMENT CONTROL DURING CONSTRUCTION OF THE PROJECT.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERRED FROM THE ILLINOIS URBAN MANUAL.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

ENGINEER  JAMES L. CAINKAR, P.E.

SEPTEMBER 21, 2011
DATE

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

- DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING, EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER, PARKING OF VEHICLES OF CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS OR OTHER CONSTRUCTION RELATED ACTIVITIES.
 - WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
 - AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER.
 - PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - TEMPORARILY SEED ERODABLE BARE EARTH ON A WEEKLY BASIS TO MINIMIZE THE AMOUNT OF ERODABLE SURFACE AREA WITHIN THE CONTRACT LIMITS.
 - PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
 - EXCAVATED AREAS AND EMBANKMENT SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN (7) DAYS.
 - CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR OTHER POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
 - THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2-INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING THE WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE CONSTRUCTION FIELD ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT EROSION CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.
 - SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON THE SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR EARTH EXCAVATION FOR EROSION CONTROL.
 - THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

- TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS SODDED AND ESTABLISHED.
- ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER ACCEPTANCE BY THE MUNICIPALITY. MAINTENANCE UP TO THIS DATE WILL BE BY THE CONTRACTOR.

MISCELLANEOUS:

- TEMPORARY DITCH CHECKS SHALL BE LOCATED AT EVERY 1.5-FOOT FALL/RISE IN DITCH GRADE.
- TEMPORARY EROSION CONTROL SEEDING SHALL BE APPLIED AT A RATE OF 100 LBS/ACRES, IF DIRECTED.
- STRAW BALES, HAY BALES, PERIMETER EROSION BARRIER AND SILT FENCES WILL NOT BE PERMITTED FOR TEMPORARY OR PERMANENT DITCH CHECKS. DITCH CHECKS SHALL BE COMPOSED OF AGGREGATE, SILT PANELS, ROLLED EXCELSTOR, URETHANE FORM/GEOTEXTILE SILT WEDGES, AND/OR ANY OTHER MATERIAL APPROVED BY THE EROSION AND SEDIMENT CONTROL COORDINATOR.
- SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON THE SITE ON A REGULAR BASIS, AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR EARTH EXCAVATION FOR EROSION CONTROL.
- ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THE EROSION CONTROL PLAN. PRIOR TO THE APPROVAL AND USE OF THE PROJECT, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A NOTARIZED CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER INSTALLATION PROCEDURES TO FACILITATE THE ENGINEER IN CONSTRUCTION INSPECTION.

LEGEND

-  TEMPORARY DITCH CHECK
-  EROSION CONTROL BLANKET
-  PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  INLET AND PIPE PROTECTION
-  SEDIMENT BASIN

NOTE: ALL ITEMS SHALL BE CONSTRUCTED AS SHOWN IN THE ILLINOIS URBAN MANUAL AND AS DIRECTED BY THE ENGINEER. MAINTENANCE AND CLEANING OF THE EROSION CONTROL ITEMS SHALL BE INCLUDED IN THE RESPECTIVE EROSION CONTROL PAY ITEM.

FILE NAME = FAU 1587 (MCCARTHY ROAD)
FAP 577 (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
CHANNELIZATION AND
TRAFFIC SIGNAL INSTALLATION

USER NAME =	DESIGNED -- JLC	REVISED -- THK 2-25-11
	DRAWN -- JFP	REVISED --
PLOT SCALE =	CHECKED -- THK	REVISED --
PLOT DATE =	DATE -- 11/10	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL NOTES AND DETAILS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

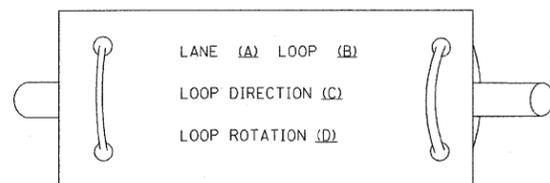
Frank Novotny & Associates, Inc.
625 Midway Drive • Willowbrook, IL • 60377 • Telephone: (630) 887-8640 • Fax: (630) 887-0122
Civil Engineers
Municipal Consultants
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000828

F.A. - R.T.E. - VAR	SECTION 02-00055-00-WR	COUNTY COOK	TOTAL SHEETS 84	SHEET NO. 45
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

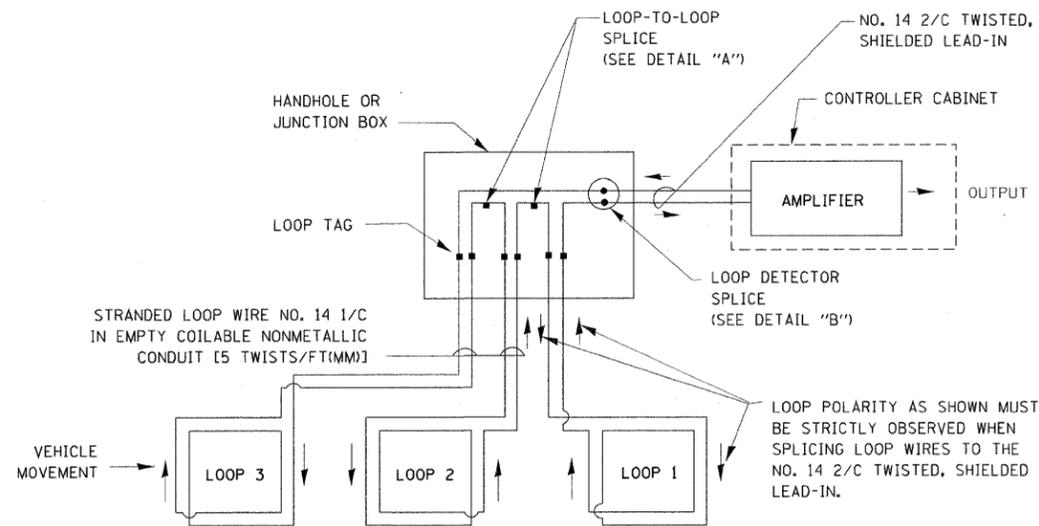
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

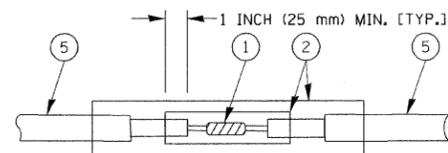


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

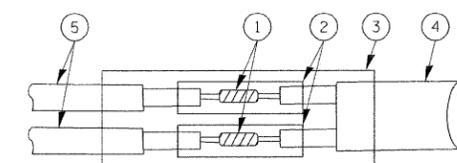


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

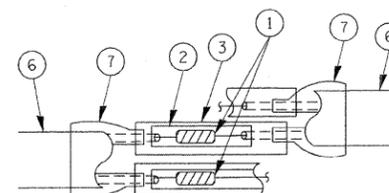


DETAIL "A"
LOOP-TO-LOOP SPLICE

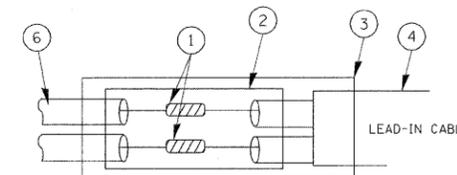


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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	PLOT SCALE = 20.0000 "/ IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 10/6/2009	DATE - 10/28/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

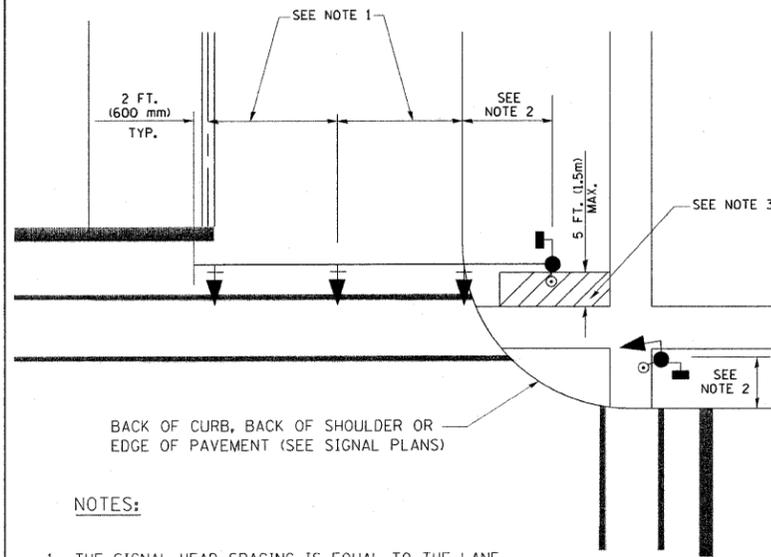
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	46
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT HPP-M-8003(504)	

SCALE: SHEET NO. 1 OF 6 SHEETS STA. TO STA.

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

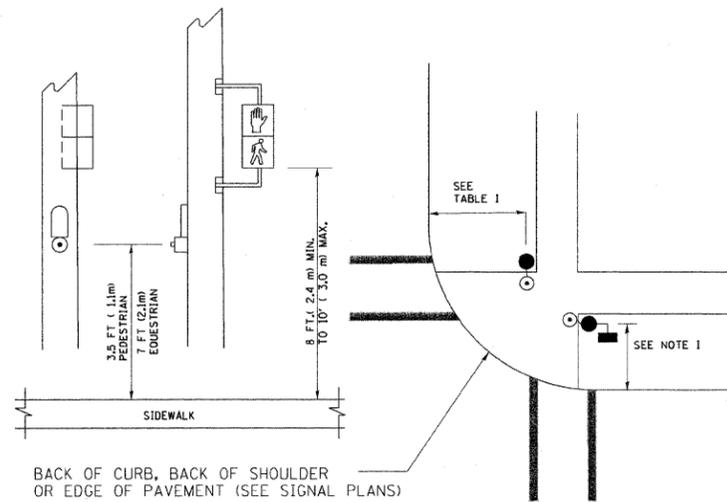
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

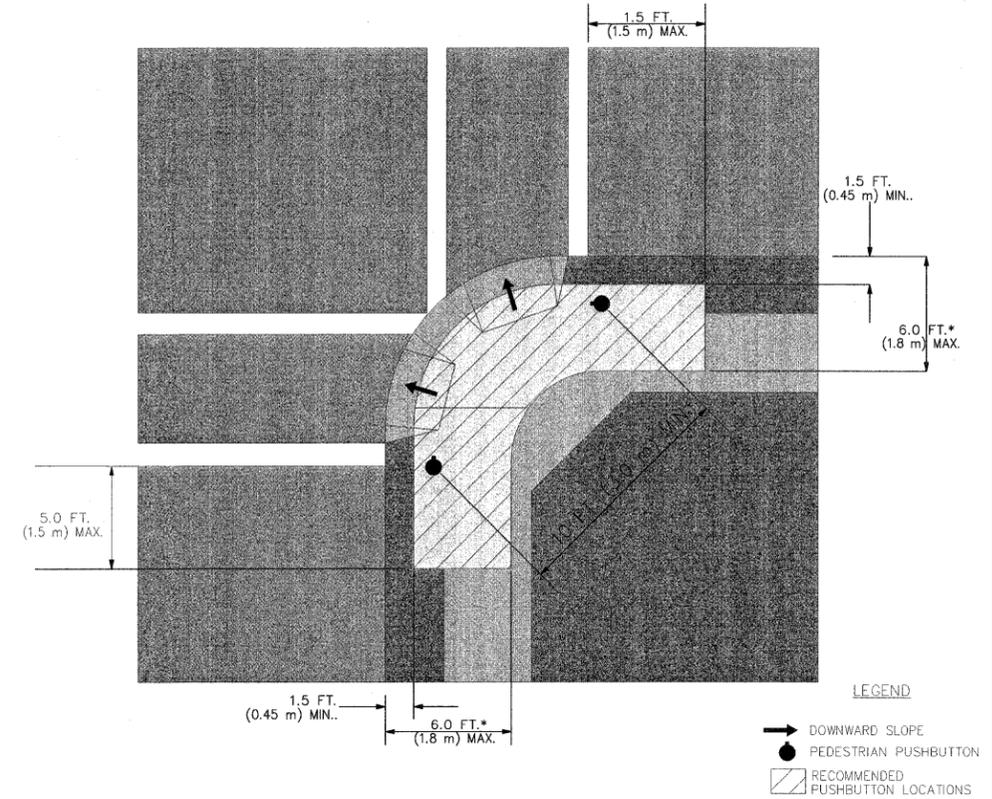
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

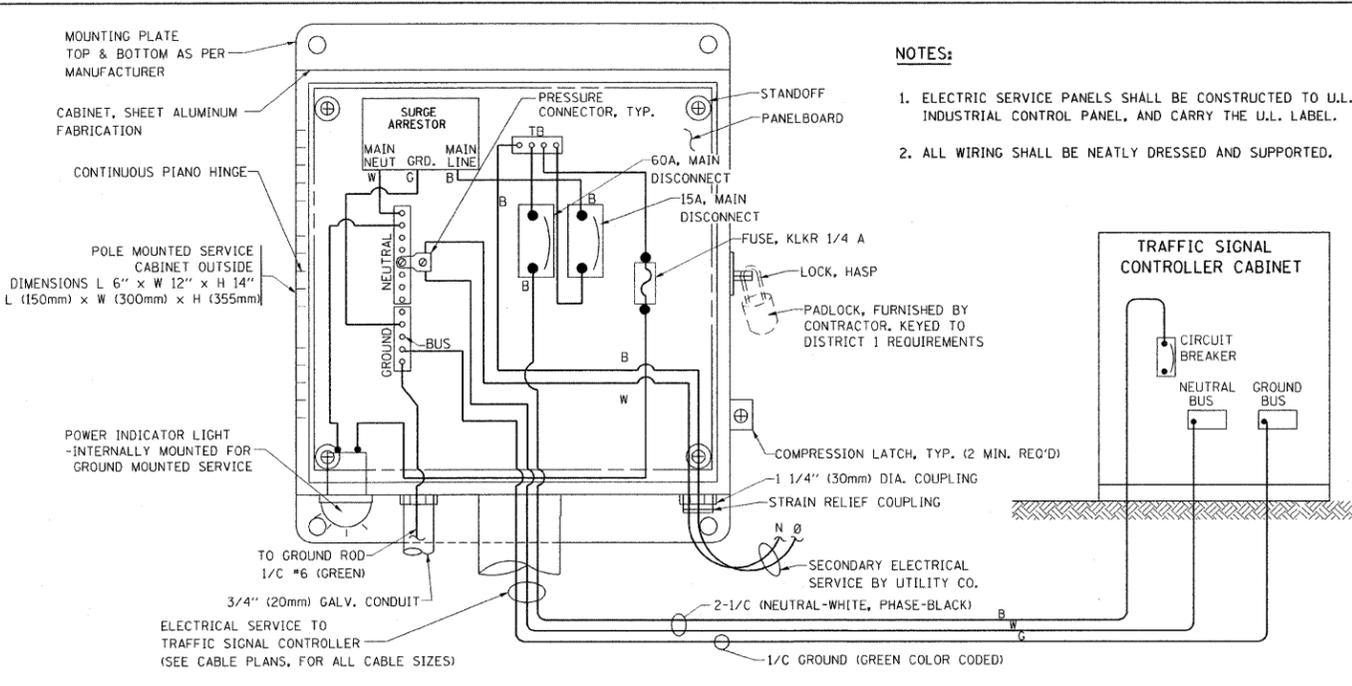
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

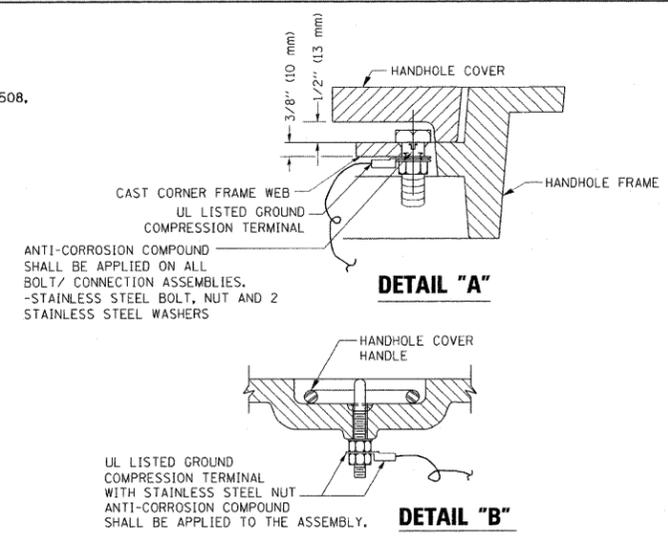
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

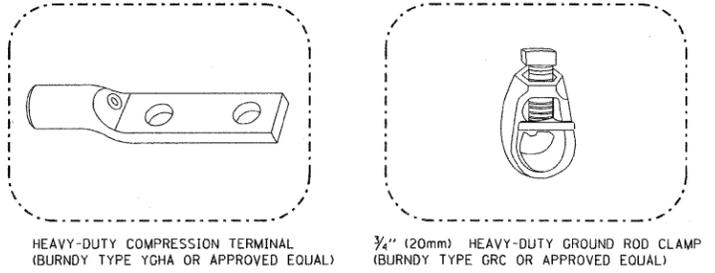
1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.



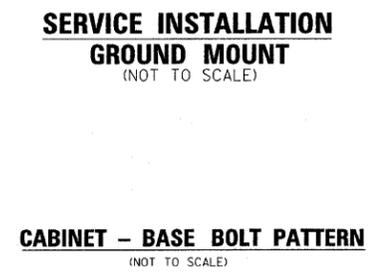
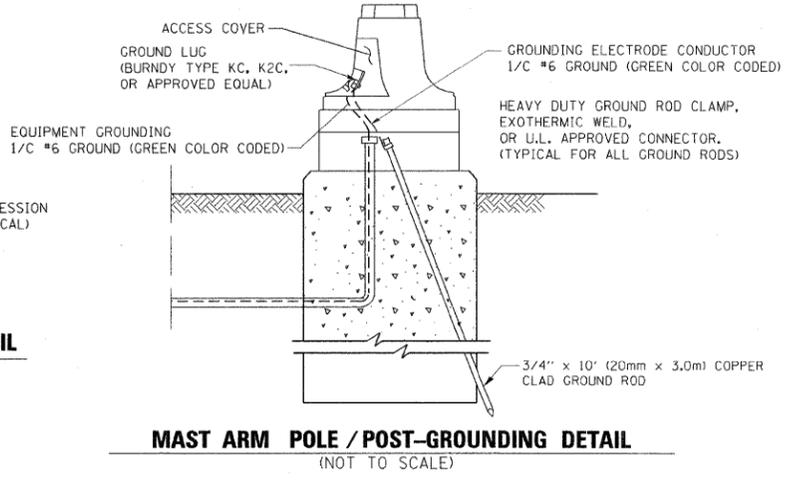
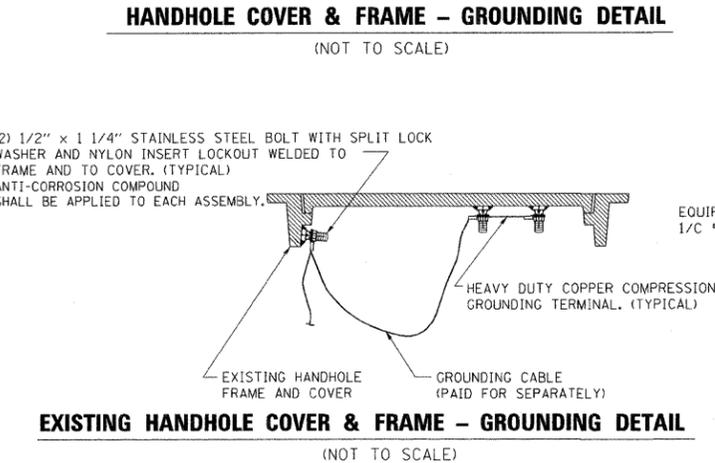
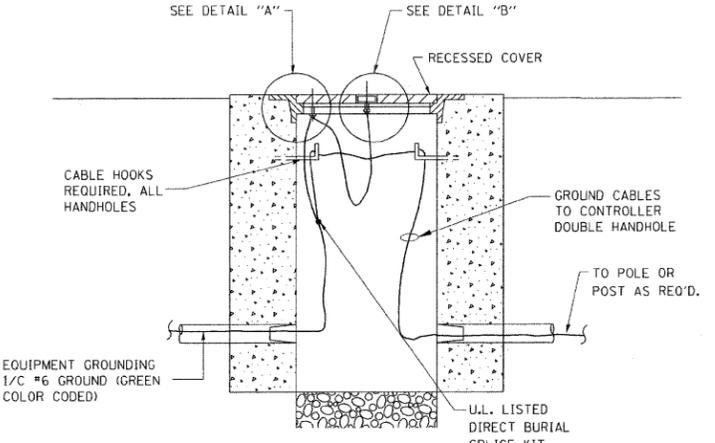
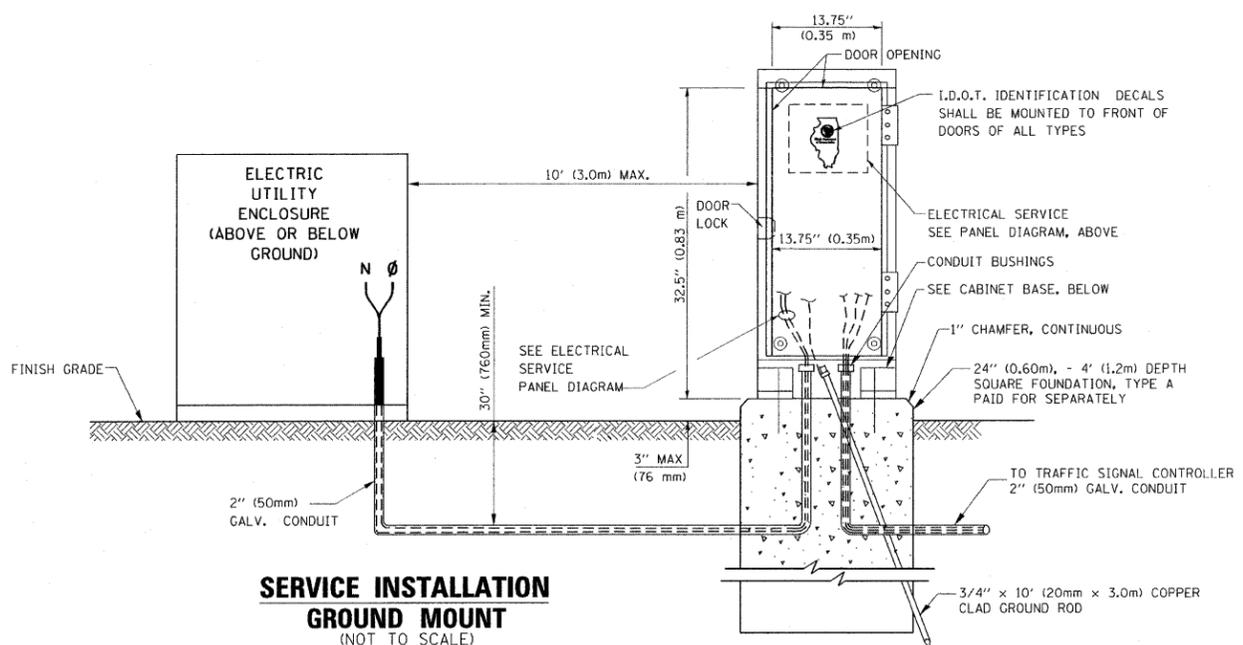
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



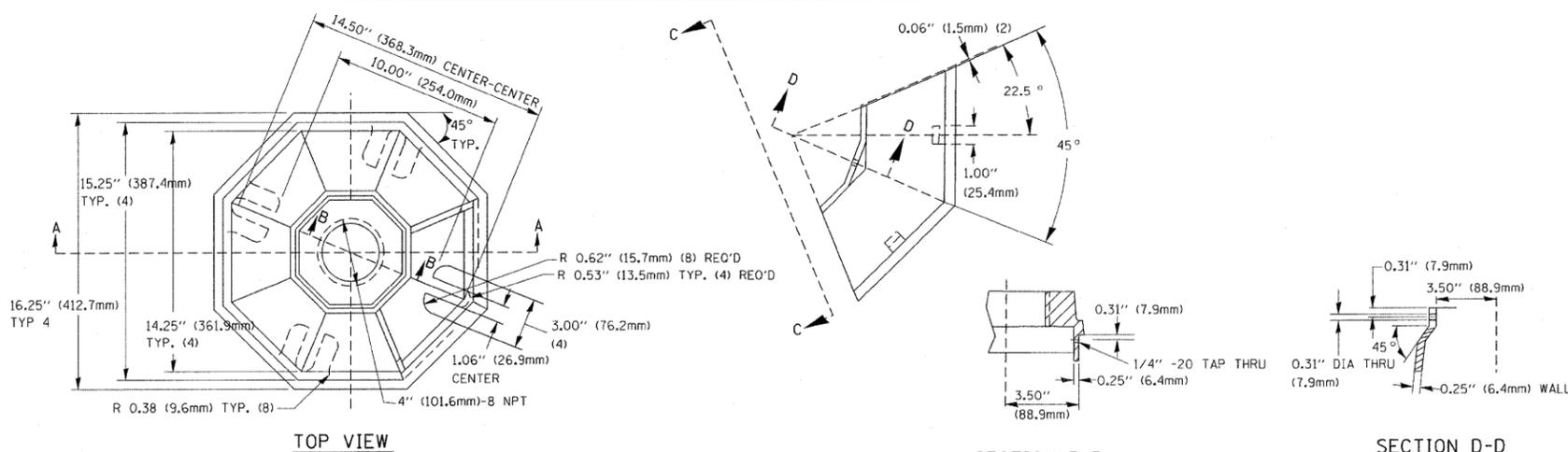
- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
 2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
 3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
 4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



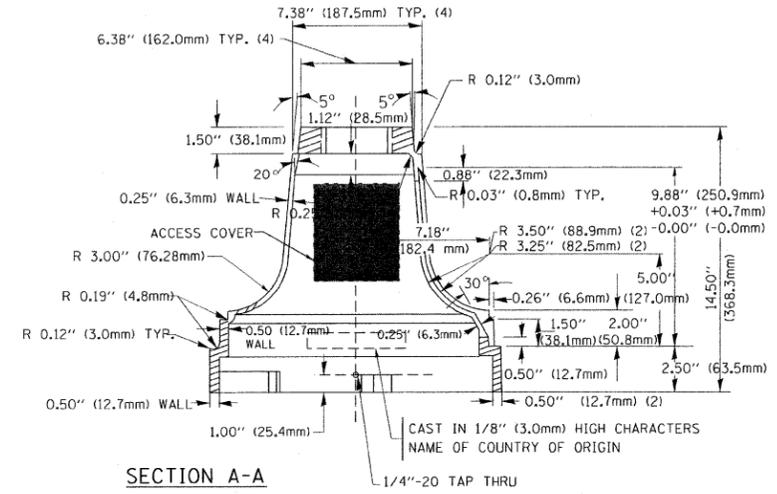
FILE NAME =	USER NAME = kanthaphixaybc	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\pwwid01\KANTHAPHIXAYBC\011264\traffic.legend.v7.dgn	PLOT SCALE = 20.0000' / IN.	DRAWN - BCK	REVISED -			VAR	02-00055-00-WR	COOK	84	48	
	PLOT DATE = 10/6/2009	CHECKED - DAD	REVISED -			CONTRACT NO. 63557					
		DATE - 10/28/09	REVISED -			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPP-M-8003(504)					



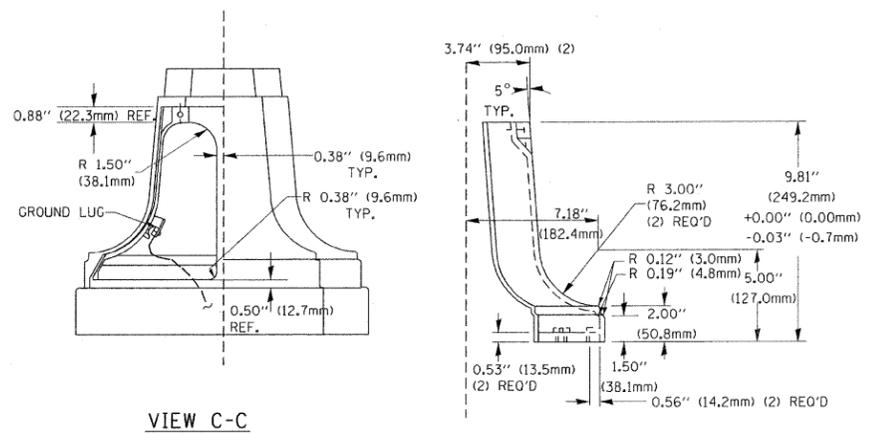
TOP VIEW

SECTION B-B

SECTION D-D

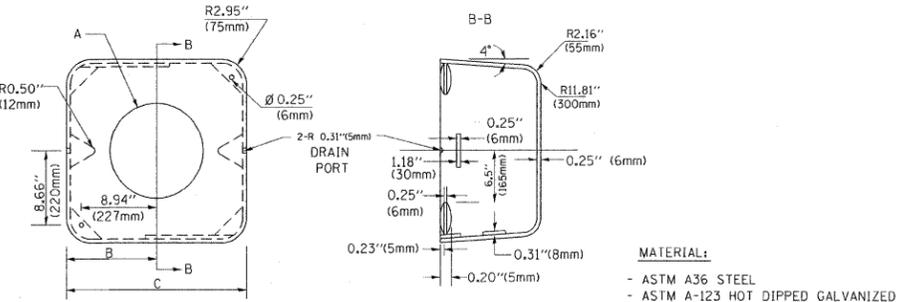


SECTION A-A



VIEW C-C

TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



SHROUD

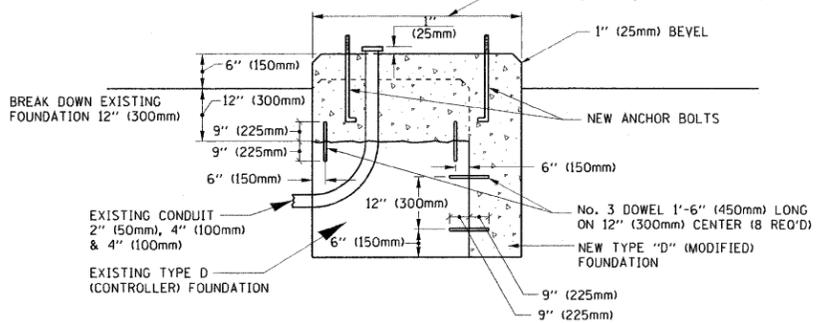
	A	B	C	HEIGHT	WEIGHT
	VARIES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
	VARIES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
	VARIES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
	VARIES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

NOTES:

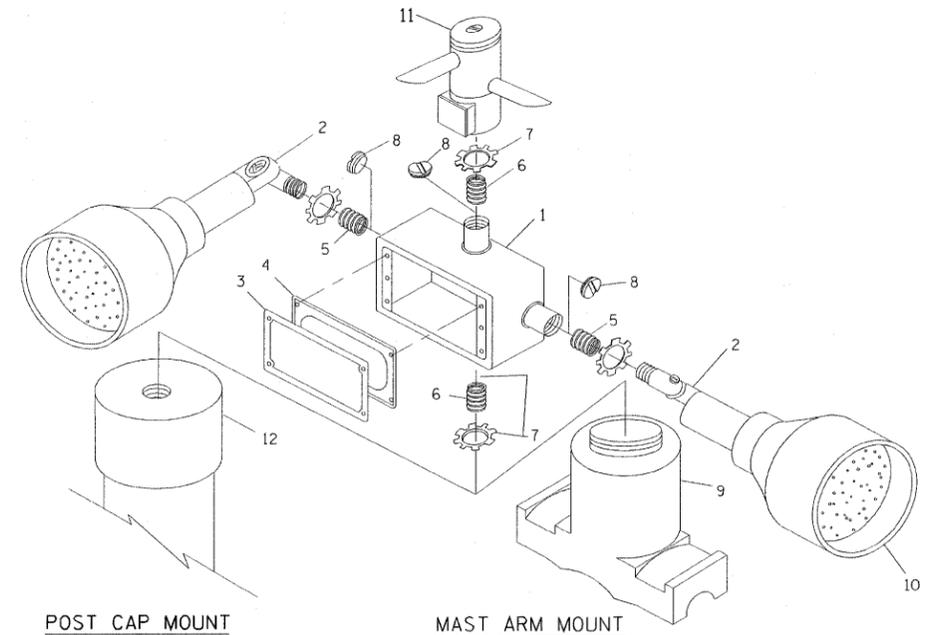
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



POST CAP MOUNT

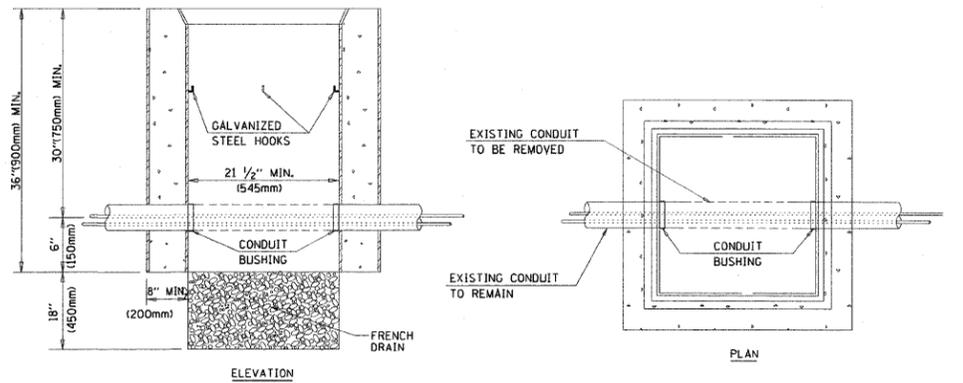
MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

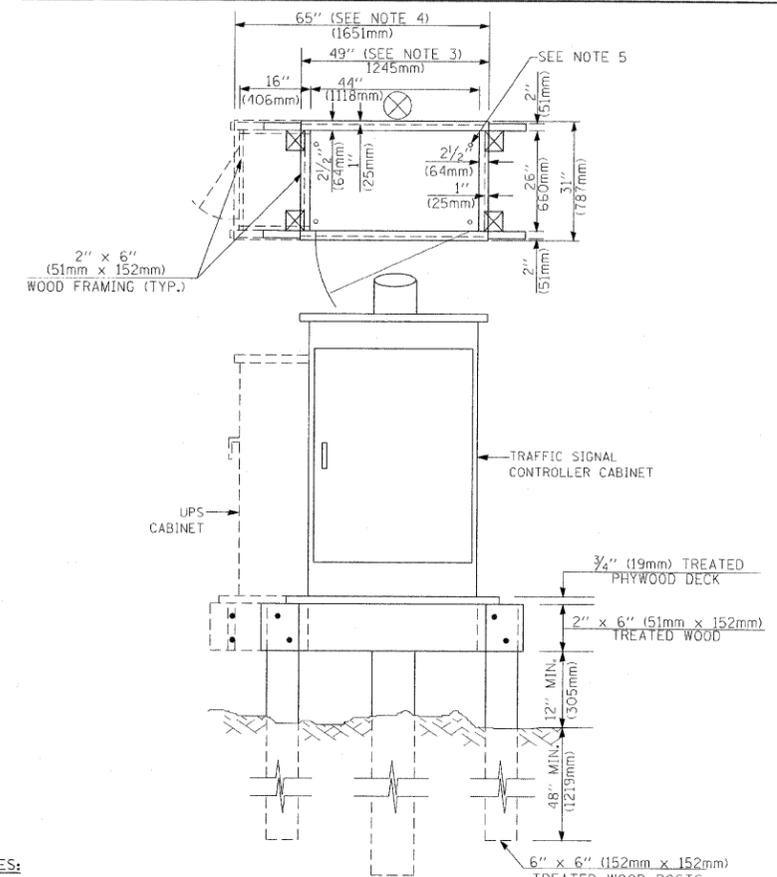
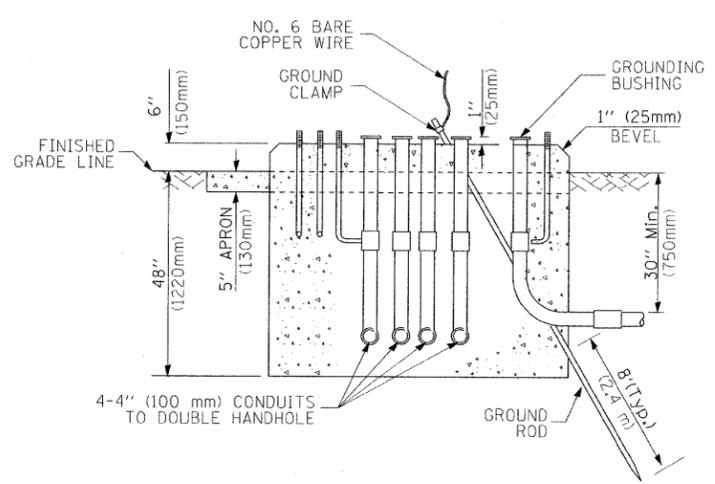
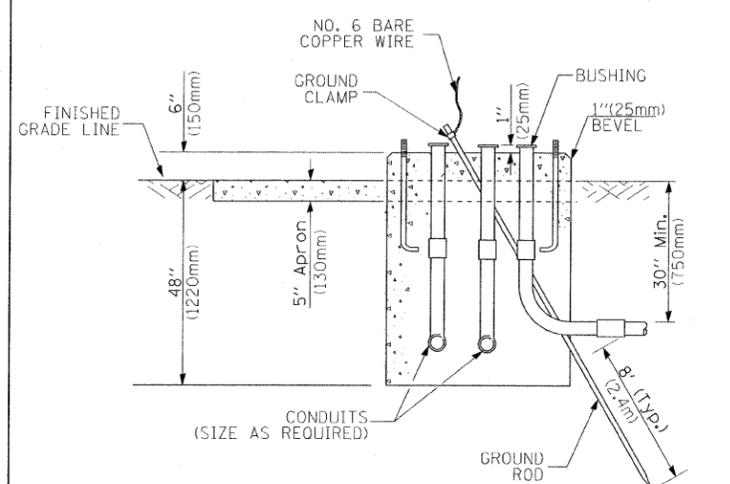
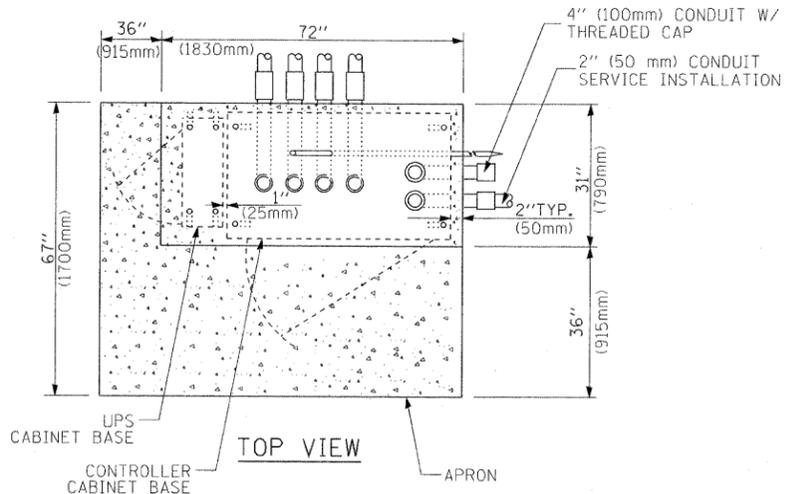
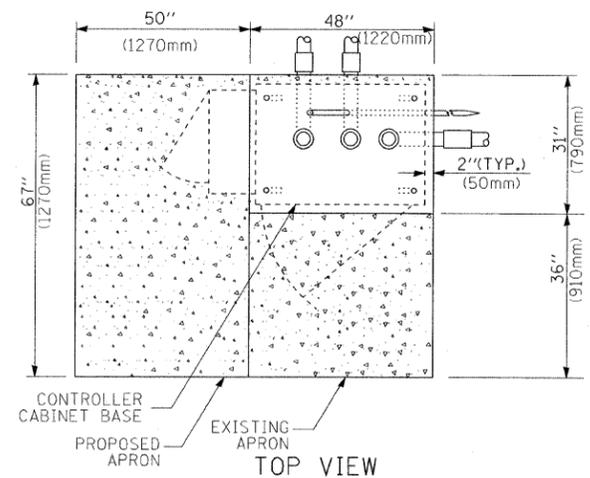
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/CEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



HANDHOLE TO INTERCEPT EXISTING CONDUIT

NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.



- NOTES:**
- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 - BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
 - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
 - DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
 - FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

**TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
 - Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
 - Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
 - For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED				
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE							
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE							
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA							
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED							
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MMI2F							
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MMI2F SM12F							
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MMI2F SM21F							
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)							
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE							
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED							
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED							
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED							
SIGNAL POST				REMOVE ITEM	R			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED							
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			SIGNAL POST AND FOUNDATION TO BE REMOVED							
GUY WIRE				ABANDON ITEM	A			INTERSECTION & SAMPLING (SYSTEM) DETECTOR							
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR							
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR							
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR							
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR							
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED SAMPLING (SYSTEM) DETECTOR							
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				RAILROAD SYMBOLS							
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				EXISTING		PROPOSED					
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				RAILROAD CONTROL CABINET							
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				RAILROAD CANTILEVER MAST ARM							
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				FLASHING SIGNAL							
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				CROSSING GATE							
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)				CROSSBUCK							
MICROWAVE VEHICLE SENSOR															
VIDEO DETECTION CAMERA															
VIDEO DETECTION ZONE															
PAN, TILT, ZOOM CAMERA															
WIRELESS DETECTOR SENSOR															
WIRELESS ACCESS POINT															

CONSTRUCTION NOTES

STAGE 1

LEFT TURN ARROWS FOR WB MCCARTHY RD WILL BE DISCONNECTED AT THE CONTROLLER AND THE ARROW SECTIONS REMAIN BAGGED THRU STAGE 1.

STAGE 2

UPON COMPLETION OF THE ROADWAY, THE AREA TRAFFIC ENGINEER WILL BE NOTIFIED PRIOR TO UN-BAGGING AND ACTIVATION OF THE LEFT-TURN ARROWS TO INTRODUCE THE LEFT-TURN PHASE.

DERBY ROAD CLOSURE

DURING THE TIME OF ROAD CLOSURE/DETOUR OF DERBY ROAD THE PHASING FOR NORTHBOUND TRAFFIC SHALL BE DISABLED AND THE SIGNAL HEADS REMAIN BAGGED UNTIL DERBY ROAD IS REOPENED TO TRAFFIC.

① - STAGE 1 VIDEO DETECTION ZONE

② - STAGE 2 VIDEO DETECTION ZONE

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: VILLAGE OF LEMONT FIRE DEPARTMENT

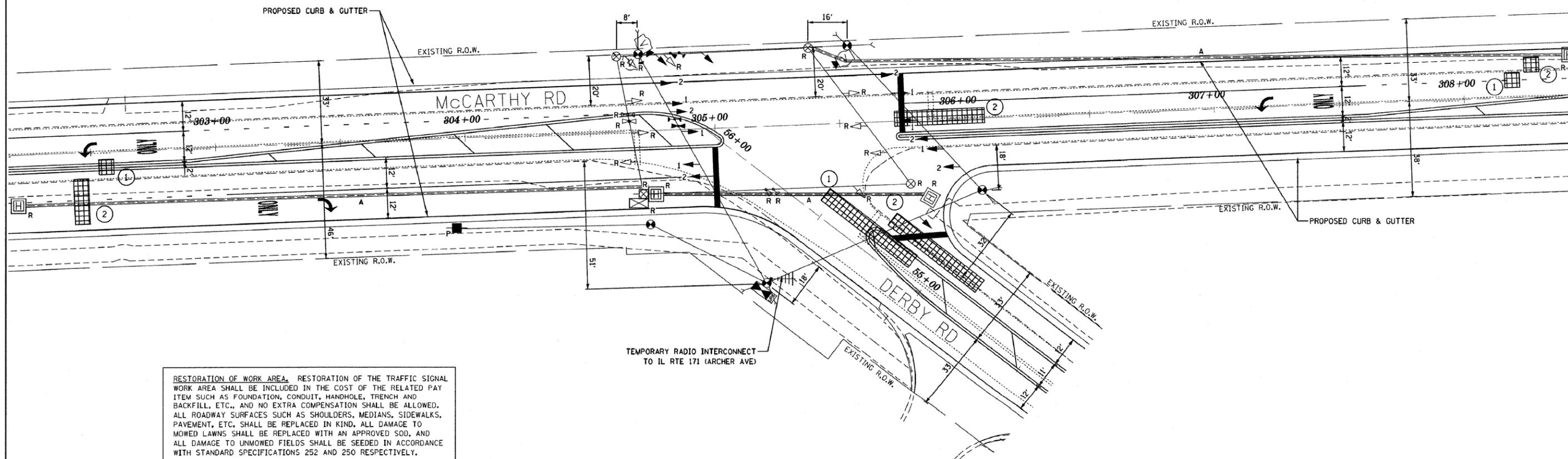
- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CONTROLLER AND CABINET, COMPLETE
- 6 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, SPAN WIRE MOUNTED
- 3 EACH SIGNAL HEAD, 1-FACE, 4-SECTION, SPAN WIRE MOUNTED
- 4 EACH WOOD POLE

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL. TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.



RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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	PLOT DATE = 3/1/2011	DATE - 6/23/2010	REVISED -

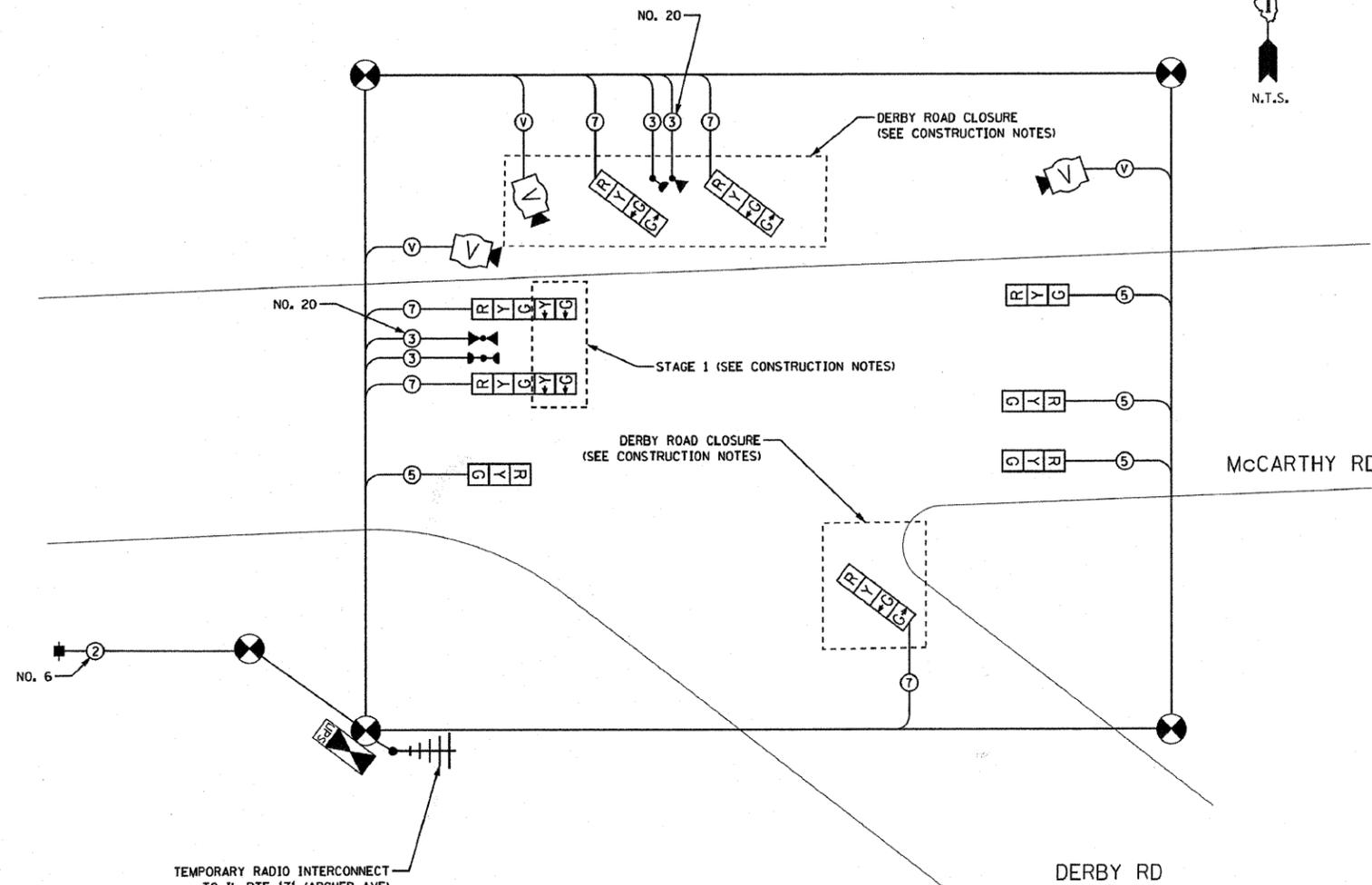
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN
MCCARTHY RD AT DERBY RD**

SCALE: 1" = 20' SHEET NO. OF SHEETS STA. TO STA.

KLOA Kenig, Lindgren, O'Hara, Aboona, Inc.		9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 618-9990 F: (847) 618-9987	
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
VAR.	02-00055-00-WR	COOK	84
		SHEET NO. 52	
CONTRACT NO. 63557			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT #PP-W-8003(504)	

TEMPORARY CABLE PLAN



CONSTRUCTION NOTES

STAGE 1

LEFT TURN ARROWS FOR WB McCARTHY RD WILL BE DISCONNECTED AT THE CONTROLLER AND THE ARROW SECTIONS REMAIN BAGGED THRU STAGE 1.

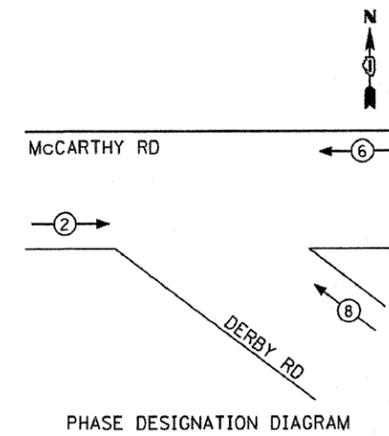
STAGE 2

UPON COMPLETION OF THE ROADWAY, THE AREA TRAFFIC ENGINEER WILL BE NOTIFIED PRIOR TO UN-BAGGING AND ACTIVATION OF THE LEFT-TURN ARROWS TO INTRODUCE THE LEFT-TURN PHASE.

DERBY ROAD CLOSURE

DURING THE TIME OF ROAD CLOSURE/DETOUR OF DERBY ROAD THE PHASING FOR NORTHBOUND TRAFFIC SHALL BE DISABLED AND THE SIGNAL HEADS REMAIN BAGGED UNTIL DERBY ROAD IS REOPENED TO TRAFFIC.

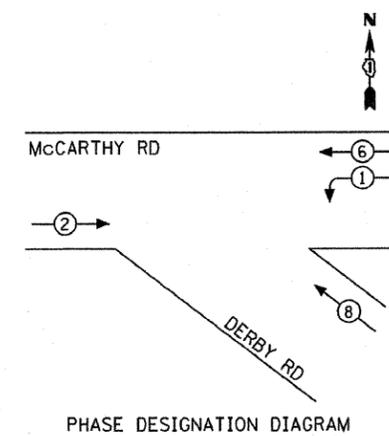
CONTROLLER SEQUENCE STAGE 1



LEGEND

- ◉ DUAL ENTRY PHASE
- ◻ SINGLE ENTRY PHASE
- ◊ OL OVERLAP
- ◉-◉ PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

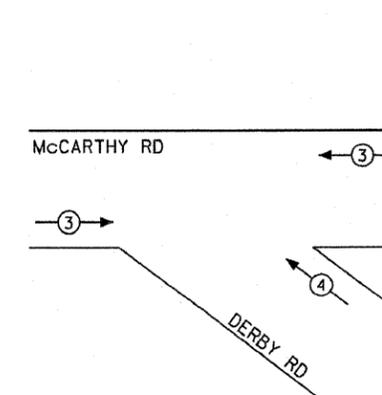
CONTROLLER SEQUENCE STAGE 2



LEGEND

- ◉ DUAL ENTRY PHASE
- ◻ SINGLE ENTRY PHASE
- ◊ OL OVERLAP
- ◉-◉ PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←	↙

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	LED	% OPERATION	
SIGNAL (RED)	9		17	0.50	76.5
(YELLOW)	9		25	0.25	56.25
(GREEN)	6		15	0.25	22.5
ARROW	10		12	0.10	12.0
PED. SIGNAL			25	1.00	
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN				0.05	
VIDEO SYSTEM	1	150		1.00	150.0
FLASHER				0.50	
ENERGY COSTS TO: VILLAGE OF LEMONT 418 MAIN STREET LEMONT, ILLINOIS 60439					TOTAL = 417.25
ENERGY SUPPLY CONTACT: MARK ANDERSON PHONE: (815) 724-5988 COMPANY: COM. ED.					

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	PLOT SCALE =	CHECKED - DMS	REVISED -
	PLOT DATE = 3/1/2011	DATE - 6/23/2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, PHASE
DESIGNATION DIAGRAM & EVP SEQUENCE
McCARTHY RD AT DERBY RD

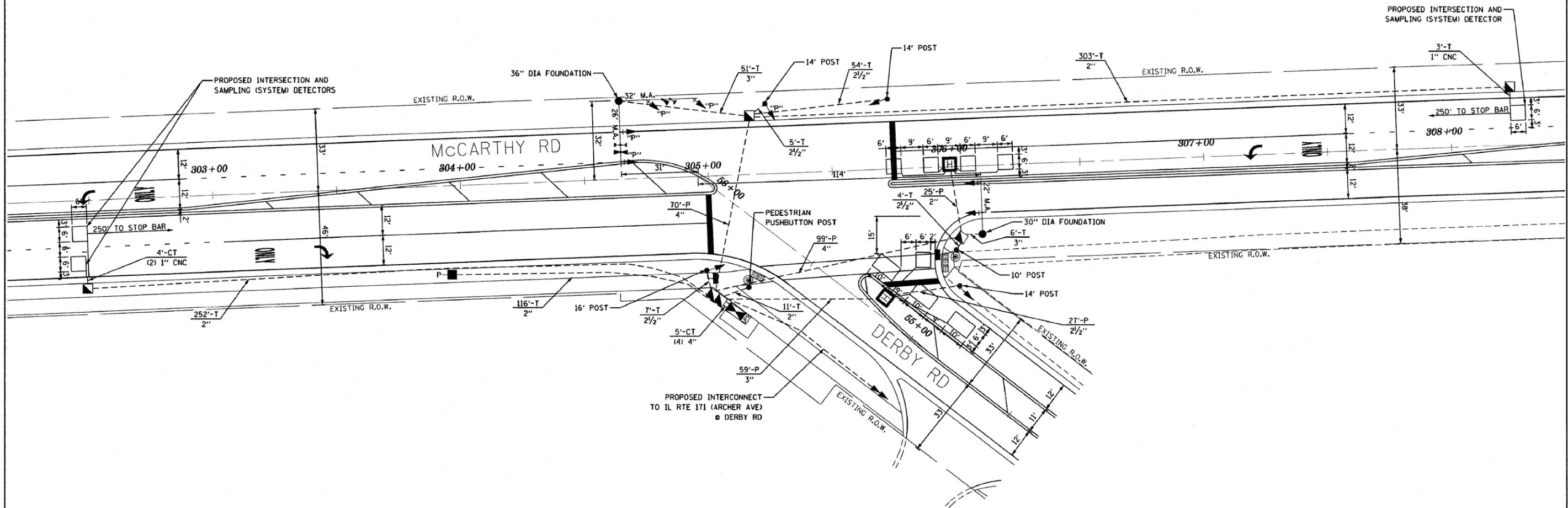
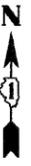
SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

KLOAN Kerig, Lindgren, O'Hara, Aboona, Inc.		9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 518-9990 F: (847) 518-0987	
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
VAR.	02-00055-00-WR	COOK	84
		CONTRACT NO. 63557	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-800315041			

PROJECT # 10-001

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

ALL CONDUITS SHALL BE PAID FOR AS LISTED IN THE PAY ITEM LIST REGARDLESS IF THEY ARE LABELED AS "TRENCHED" OR "PUSHED" ON THIS SHEET.



FILE NAME =
...:\signal\54-signal McCDerb.dgn

USER NAME =
PLOT SCALE =
PLOT DATE = 10/31/2011

DESIGNED - GJG
DRAWN - GJG
CHECKED - DMS
DATE - 6/23/2010

REVISED - 2/25/2011
REVISED - 10/31/2011
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

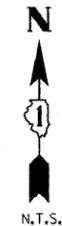
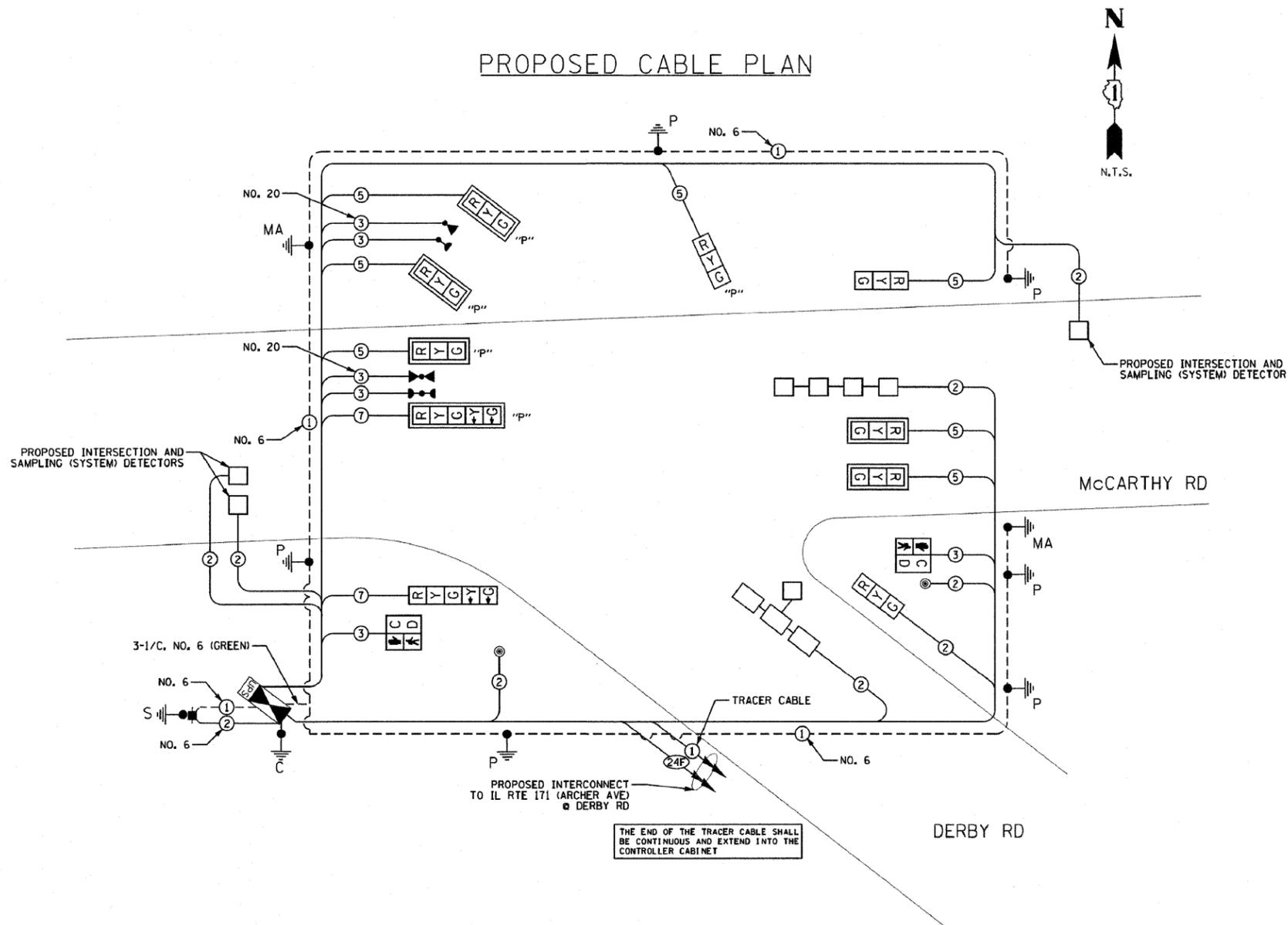
TRAFFIC SIGNAL INSTALLATION PLAN
McCARTHY RD AT DERBY RD

SCALE: 1" = 20' SHEET NO. OF SHEETS STA. TO STA.

KLOA
Kenig, Lindgren, O'Hara, Aboona, Inc.
9575 West Higgins Road, Suite 400
Rosemont, Illinois 60018
P: (847) 518-9980 F: (847) 518-9987
PROJECT # 10-001

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	02-00055-00-WR	COOK	84	54
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

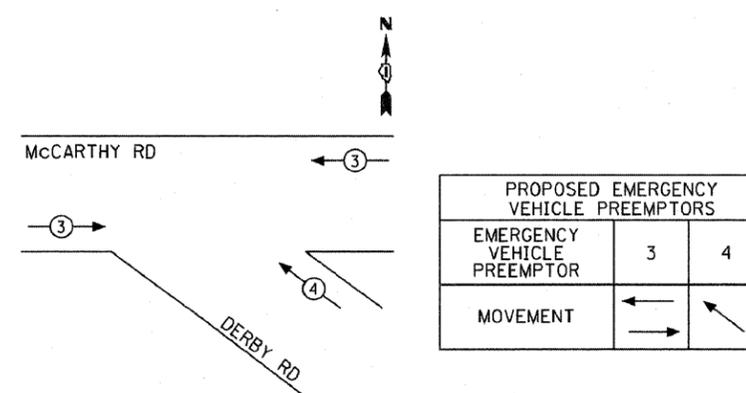
PROPOSED CABLE PLAN



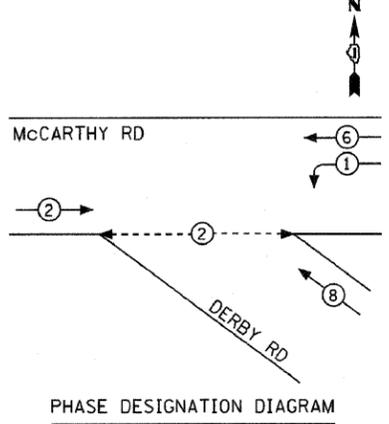
SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
22.5	SQ FT	SIGN PANEL - TYPE 1
1	EACH	SERVICE INSTALLATION, POLE MOUNTED
707	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA
97	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA
116	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA
194	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA
4	EACH	HANDHOLE
2	EACH	HEAVY-DUTY HANDHOLE
1	EACH	DOUBLE HANDHOLE
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET
1	EACH	TRANSCEIVER - FIBER OPTIC
401	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED
181	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
596	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1263	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
244	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1218	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
134	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
597	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.
3	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
1	EACH	PEDESTRIAN PUSH-BUTTON POST, GALVANIZED STEEL, TYPE II
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 22 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 26 FT. AND 32 FT.
24	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
10	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
12	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
2	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
1	EACH	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
3	EACH	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
1	EACH	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
6	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED,
5	EACH	INDUCTIVE LOOP DETECTOR
397	FOOT	DETECTOR LOOP, TYPE I
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
2	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
4	EACH	REMOVE EXISTING HANDHOLE
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	UNINTERRUPTIBLE POWER SUPPLY SPECIAL

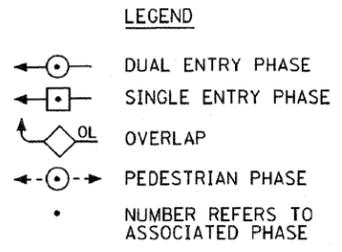
EMERGENCY VEHICLE PREEMPTION SEQUENCE



CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	LED % OPERATION	
SIGNAL (RED)	10	17	0.50	85.0
(YELLOW)	10	25	0.25	62.5
(GREEN)	10	15	0.25	37.5
ARROW	4	12	0.10	4.8
PED. SIGNAL	2	25	1.00	50.0
CONTROLLER	1	100	1.00	100.0
ILLUM. SIGN			0.05	
VIDEO SYSTEM			1.00	
FLASHER			0.50	
TOTAL =				339.8

ENERGY COSTS TO: VILLAGE OF LEMONT, 418 MAIN STREET, LEMONT, ILLINOIS 60439. ENERGY SUPPLY CONTACT: MARK ANDERSON, (815) 724-5988, COM. ED.

FILE NAME = ...signal\55-cable McDerb.dgn	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011
	PLOT SCALE =	DRAWN - GJG	REVISED - 10/31/2011
	PLOT DATE = 10/31/2011	CHECKED - DMS	REVISED -
		DATE - 6/23/2010	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SEQUENCE & SCHEDULE OF QUANTITIES
McCARTHY RD AT DERBY RD

SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.
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KLOA Kenig, Lindgren, O'Hara, Aboona, Inc. 9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 516-9900 F: (847) 518-9987 PROJECT # 10-001

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	02-00055-00-WR	COOK	84	55
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003504				

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL. TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: VILLAGE OF LEMONT FIRE DEPARTMENT

- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CONTROLLER AND CABINET, COMPLETE
- 10 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, SPAN WIRE MOUNTED
- 2 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, SPAN WIRE MOUNTED
- 4 EACH WOOD POLE

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

CONSTRUCTION NOTES

STAGE 1

LEFT TURN ARROWS FOR NB, EB, AND SB WILL BE DISCONNECTED AT THE CONTROLLER AND THE ARROW SECTIONS REMAIN BAGGED THRU STAGE 1.

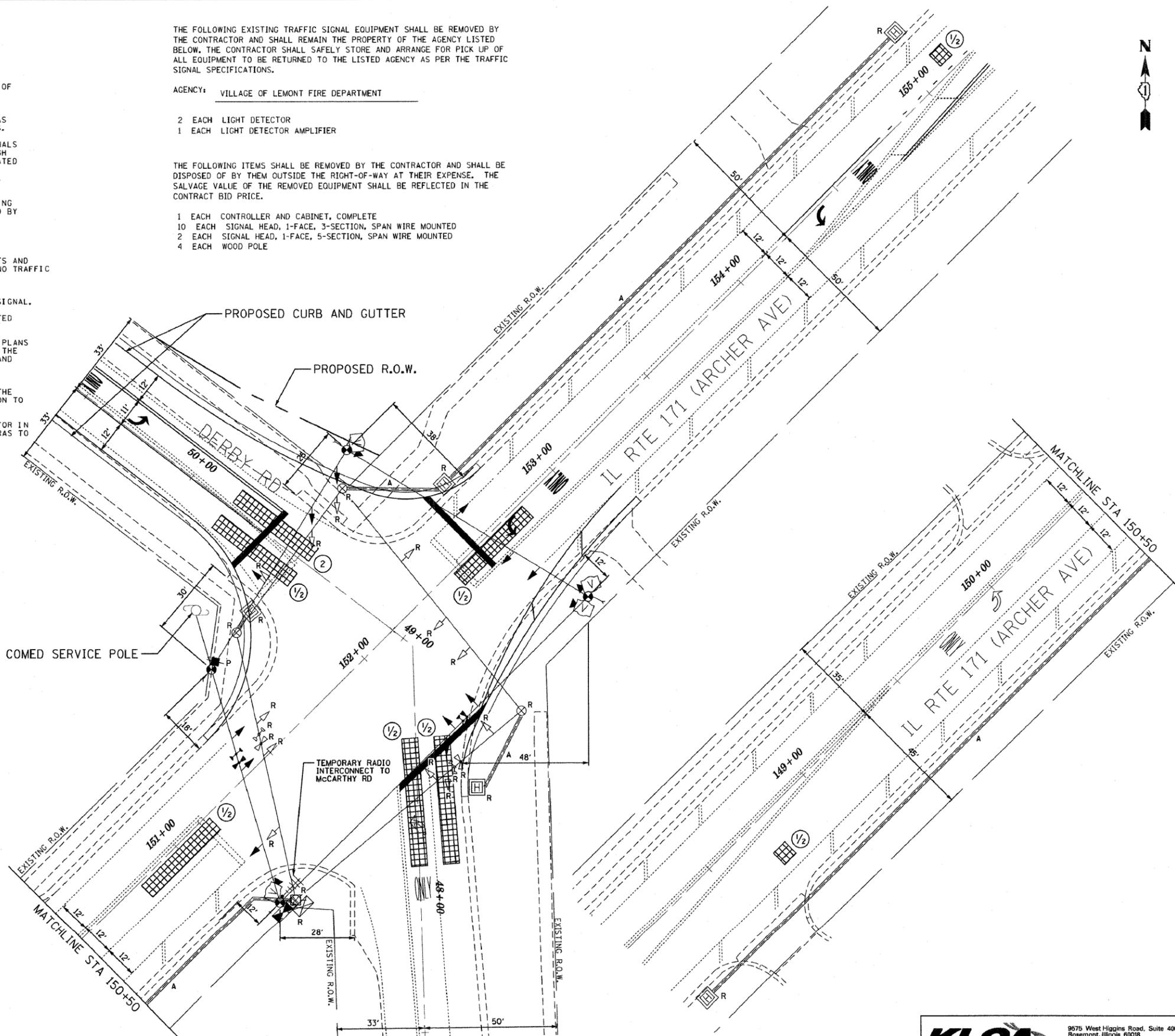
STAGE 2

UPON COMPLETION OF THE ROADWAY, THE AREA TRAFFIC ENGINEER WILL BE NOTIFIED PRIOR TO UN-BAGGING AND ACTIVATION OF THE LEFT-TURN ARROWS TO INTRODUCE THE LEFT-TURN PHASES.

DERBY ROAD CLOSURE

DURING THE TIME OF ROAD CLOSURE/DETOUR OF DERBY ROAD THE PHASING FOR SOUTHBOUND TRAFFIC SHALL BE DISABLED AND THE SIGNAL HEADS REMAIN BAGGED UNTIL DERBY ROAD IS REOPENED TO TRAFFIC.

- 1/2 - STAGE 1 & 2 VIDEO DETECTION ZONE
- 2 - STAGE 2 VIDEO DETECTION ZONE



FILE NAME =	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011
...signal\11-tmp\ts ArchDerby.dgn		DRAWN - GJG	REVISED -
	PLOT SCALE =	CHECKED - DMS	REVISED -
	PLOT DATE = 3/1/2011	DATE - 6/23/2010	REVISED -

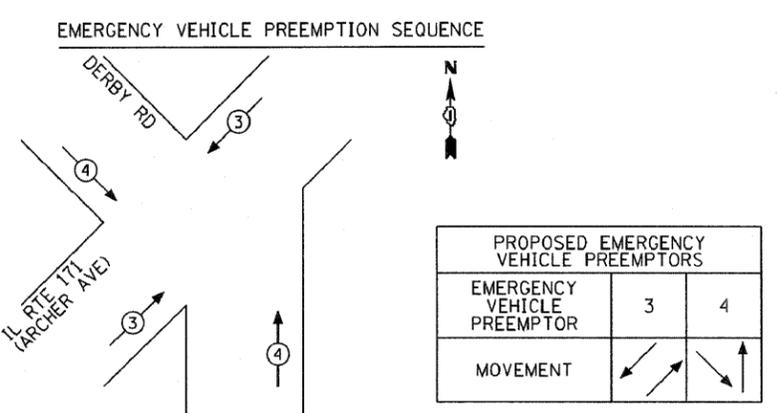
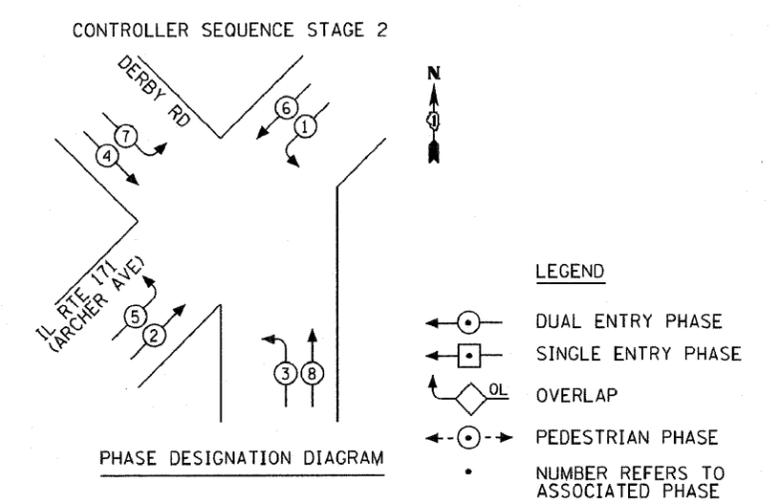
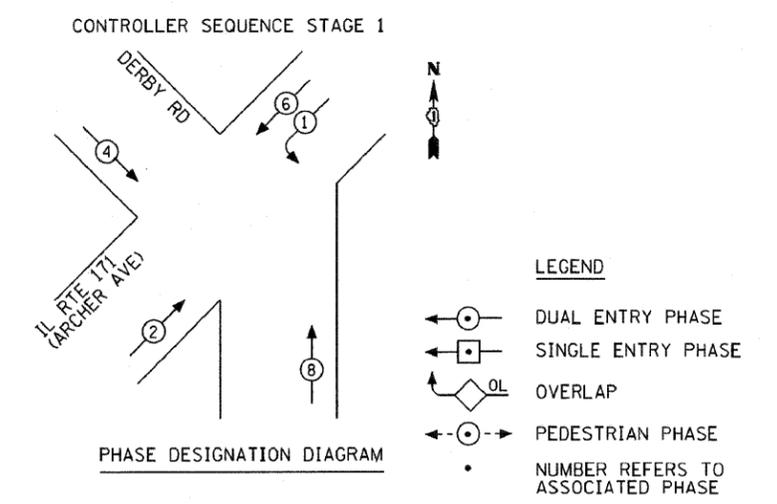
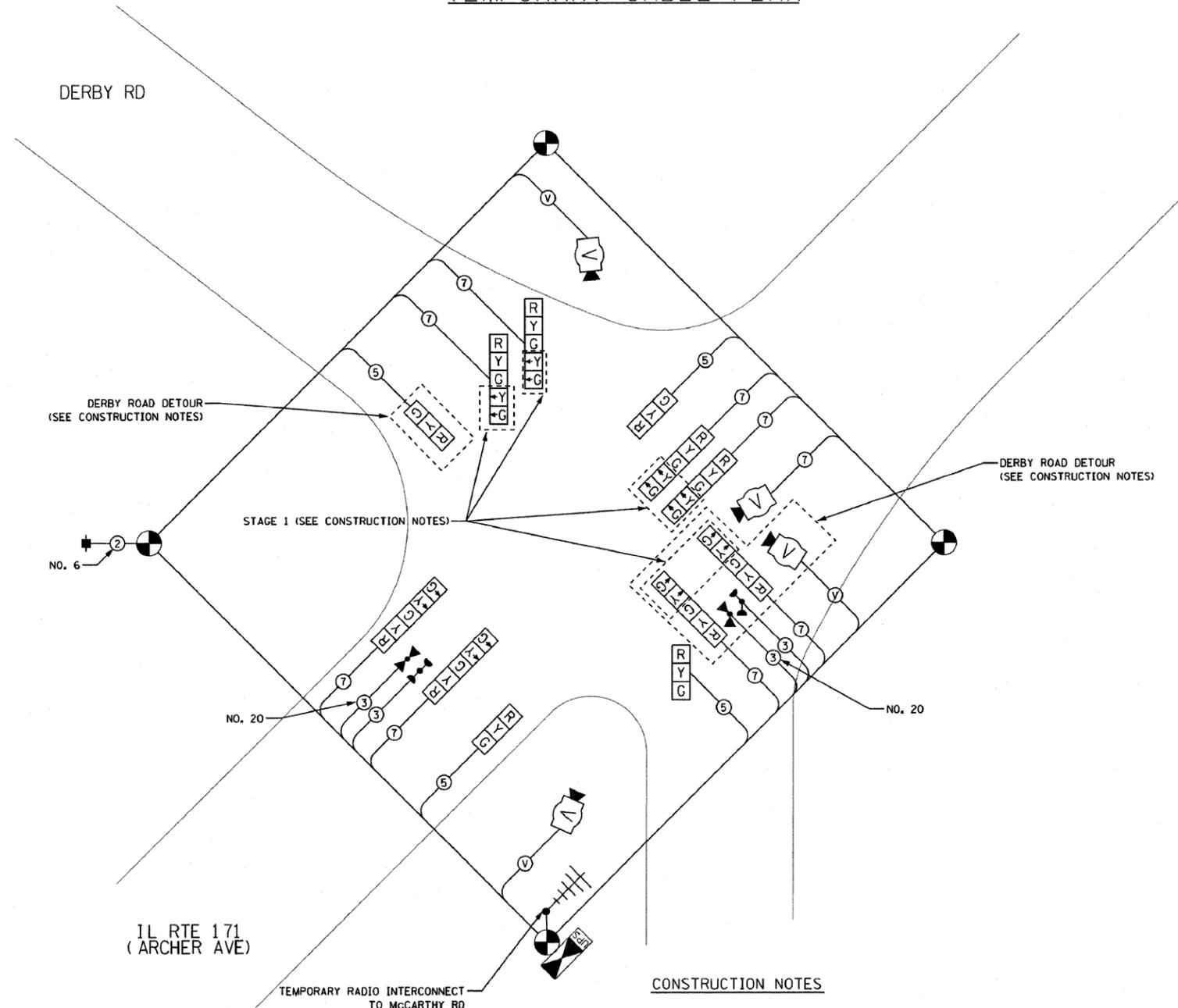
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN
IL RTE 171 (ARCHER AVE) AT DERBY RD**

SCALE: 1" = 20' SHEET NO. OF SHEETS STA. TO STA.

KLOA Kenig, Lindgren, O'Hara, Aboona, Inc.		9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 518-9900 F: (847) 518-9987	
PROJECT # 10-001			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
VAR.	02-00055-00-WR	COOK	84
		SHEET NO. 56	
CONTRACT NO. 63557			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPP-M-8003(50-4)	

TEMPORARY CABLE PLAN



CONSTRUCTION NOTES

STAGE 1
LEFT TURN ARROWS FOR NB, EB, AND SB WILL BE DISCONNECTED AT THE CONTROLLER AND THE ARROW SECTIONS REMAIN BAGGED THRU STAGE 1.

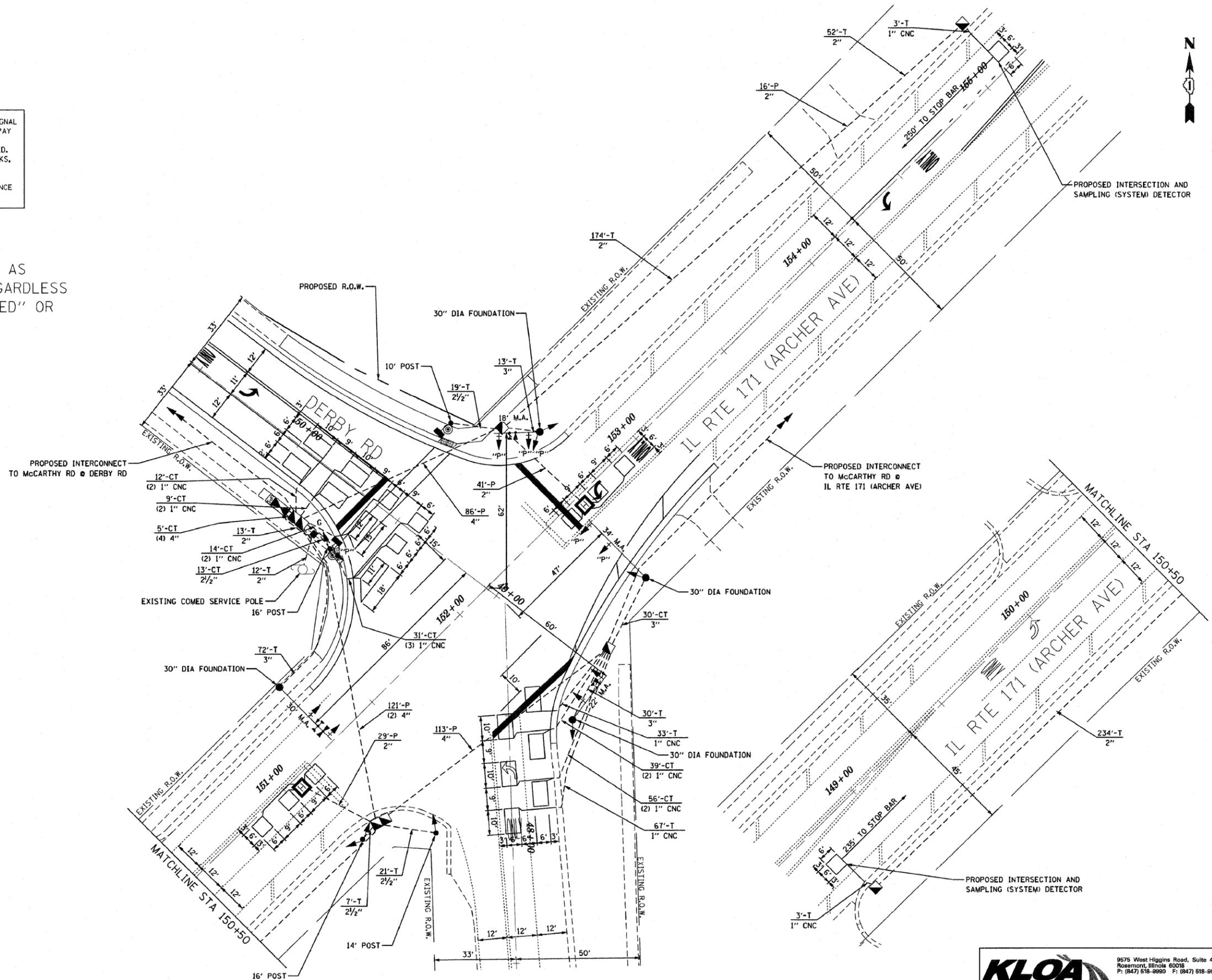
STAGE 2
UPON COMPLETION OF THE ROADWAY, THE AREA TRAFFIC ENGINEER WILL BE NOTIFIED PRIOR TO UN-BAGGING AND ACTIVATION OF THE LEFT-TURN ARROWS TO INTRODUCE THE LEFT-TURN PHASES.

DERBY ROAD CLOSURE
DURING THE TIME OF ROAD CLOSURE/DETOUR OF DERBY ROAD THE PHASING FOR SOUTHBOUND TRAFFIC SHALL BE DISABLED AND THE SIGNAL HEADS REMAIN BAGGED UNTIL DERBY ROAD IS REOPENED TO TRAFFIC.

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		% OPERATION	
		INCAND.	LED		
SIGNAL (RED)	12		17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	16		12	0.10	19.2
PED. SIGNAL			25	1.00	
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN				0.05	
VIDEO SYSTEM	1	150		1.00	150.0
FLASHER				0.50	
ENERGY COSTS TO: VILLAGE OF LEMONT 418 MAIN STREET LEMONT, ILLINOIS 60439					TOTAL = 491.2
ENERGY SUPPLY CONTACT: MARK ANDERSON PHONE: (815) 724-5988 COMPANY: COM. ED.					

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

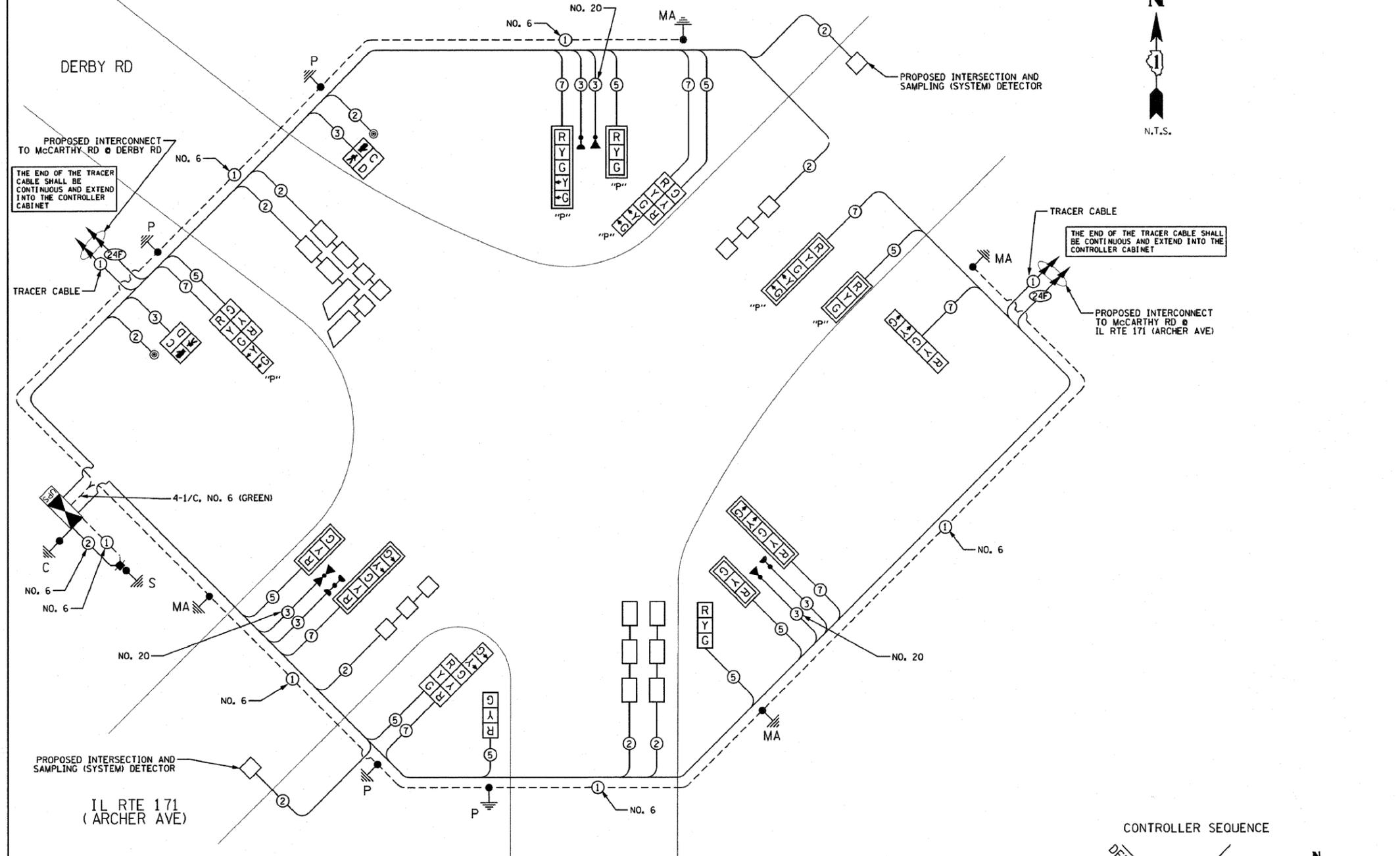
ALL CONDUITS SHALL BE PAID FOR AS LISTED IN THE PAY ITEM LIST REGARDLESS IF THEY ARE LABELED AS "TRENCHED" OR "PUSHED" ON THIS SHEET.



FILE NAME = ...signal\58-signal ArchDerb.dgn	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL INSTALLATION PLAN IL RTE 171 (ARCHER AVE) AT DERBY RD			KLOA Kenig, Lindgren, O'Hara, Abouma, Inc. 9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 518-9990 F: (847) 518-9987 PROJECT # 10-001		
		DRAWN - GJG	REVISED - 10/31/2011		F.A. RTE.	SECTION	COUNTY		TOTAL SHEETS	
		CHECKED - DMS	REVISED -		VAR.	02-00055-00-WR	COOK		84	
		DATE - 6/23/2010	REVISED -		SCALE: 1" = 20'	SHEET NO.	OF SHEETS		STA. TO STA.	
								FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	HPP-M-8003(504)

PROPOSED CABLE PLAN

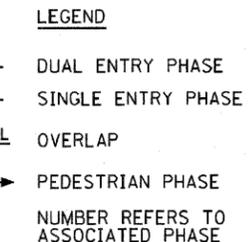
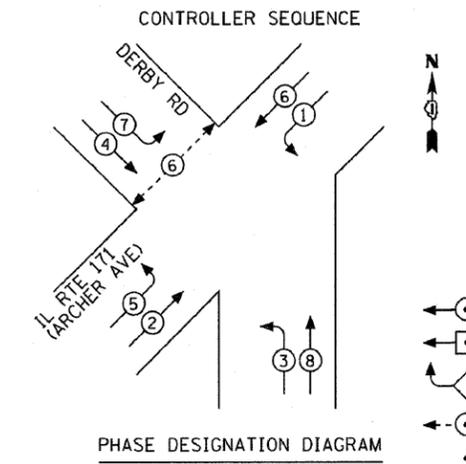
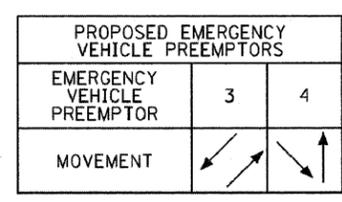
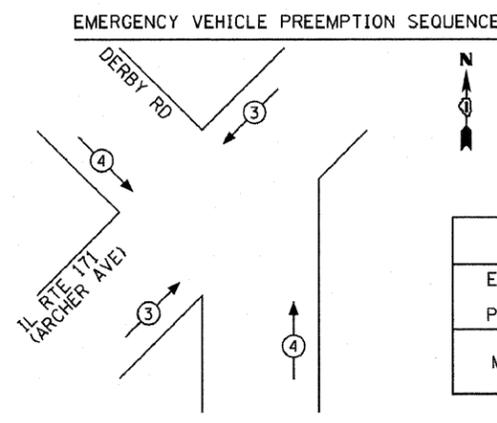
SCHEDULE OF QUANTITIES



QTY	UNIT	ITEM DESCRIPTION
13.5	SQ FT	SIGN PANEL - TYPE 1
25	SQ FT	SIGN PANEL - TYPE 2
1	EACH	SERVICE INSTALLATION, GROUND MOUNTED
571	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA
120	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA
145	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA
466	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA
4	EACH	HANDHOLE
2	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET
1	EACH	TRANSCIVER - FIBER OPTIC
683	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED
185	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
866	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1547	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1724	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1712	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
22	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
624	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.
2	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 18 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 22 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.
20	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
47	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
2	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
2	EACH	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
2	EACH	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
2	EACH	COMBINATION SIGNAL HEAD, LED, 2-FACE, 1-5 SECTION OPTICALLY PROGRAMMED, 1-3 SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED,
8	EACH	INDUCTIVE LOOP DETECTOR
908	FOOT	DETECTOR LOOP, TYPE I
3	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
2	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
5	EACH	REMOVE EXISTING HANDHOLE
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	UNINTERRUPTIBLE POWER SUPPLY SPECIAL

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	% OPERATION	
SIGNAL (RED)	17	17	0.50	144.5
(YELLOW)	17	25	0.25	106.25
(GREEN)	17	15	0.25	63.75
ARROW	16	12	0.10	19.2
PED. SIGNAL	2	25	1.00	50.0
CONTROLLER	1	100	1.00	100.0
ILLUM. SIGN VIDEO SYSTEM			0.05	
FLASHER			0.50	
TOTAL =				483.7

ENERGY COSTS TO:
VILLAGE OF LEMONT
 418 MAIN STREET
 LEMONT, ILLINOIS 60439
 ENERGY SUPPLY CONTACT: **MARK ANDERSON**
 PHONE: (815) 724-5988
 COMPANY: **COM. ED.**



FILE NAME =	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011
...signal\59-cable ArchDerb.dgn		DRAWN - GJG	REVISED - 10/31/2011
	PLOT SCALE =	CHECKED - DMS	REVISED -
	PLOT DATE = 10/31/2011	DATE - 6/23/2010	REVISED -

STATE OF ILLINOIS	DESIGNED - GJG	REVISED - 2/25/2011
DEPARTMENT OF TRANSPORTATION	DRAWN - GJG	REVISED - 10/31/2011
	CHECKED - DMS	REVISED -
	DATE - 6/23/2010	REVISED -

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SEQUENCE & SCHEDULE OF QUANTITIES
 IL RTE 171 (ARCHER AVE) AT DERBY RD

KLOA
 Kenig, Lindgren, O'Hara, Aboona, Inc.
 9575 West Higgins Road, Suite 400
 Rosemont, Illinois 60018
 P: (847) 518-8990 F: (847) 518-9887
 PROJECT # 10-001

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	02-00055-00-WR	COOK	84	59
CONTRACT NO. 63557				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPP-M-8003504				

NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL. TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

CONSTRUCTION NOTES

STAGE 1

LEFT TURN ARROWS FOR EB AND WB WILL BE DISCONNECTED AT THE CONTROLLER AND THE ARROW SECTIONS REMAIN BAGGED THRU STAGE 1.

STAGE 2

UPON COMPLETION OF THE ROADWAY, THE AREA TRAFFIC ENGINEER WILL BE NOTIFIED PRIOR TO UN-BAGGING AND ACTIVATION OF THE LEFT-TURN ARROWS TO INTRODUCE THE LEFT-TURN PHASES.

- ① - STAGE 1 VIDEO DETECTION ZONE
- ② - STAGE 2 VIDEO DETECTION ZONE
- 1/2 - STAGE 1 & 2 VIDEO DETECTION ZONE

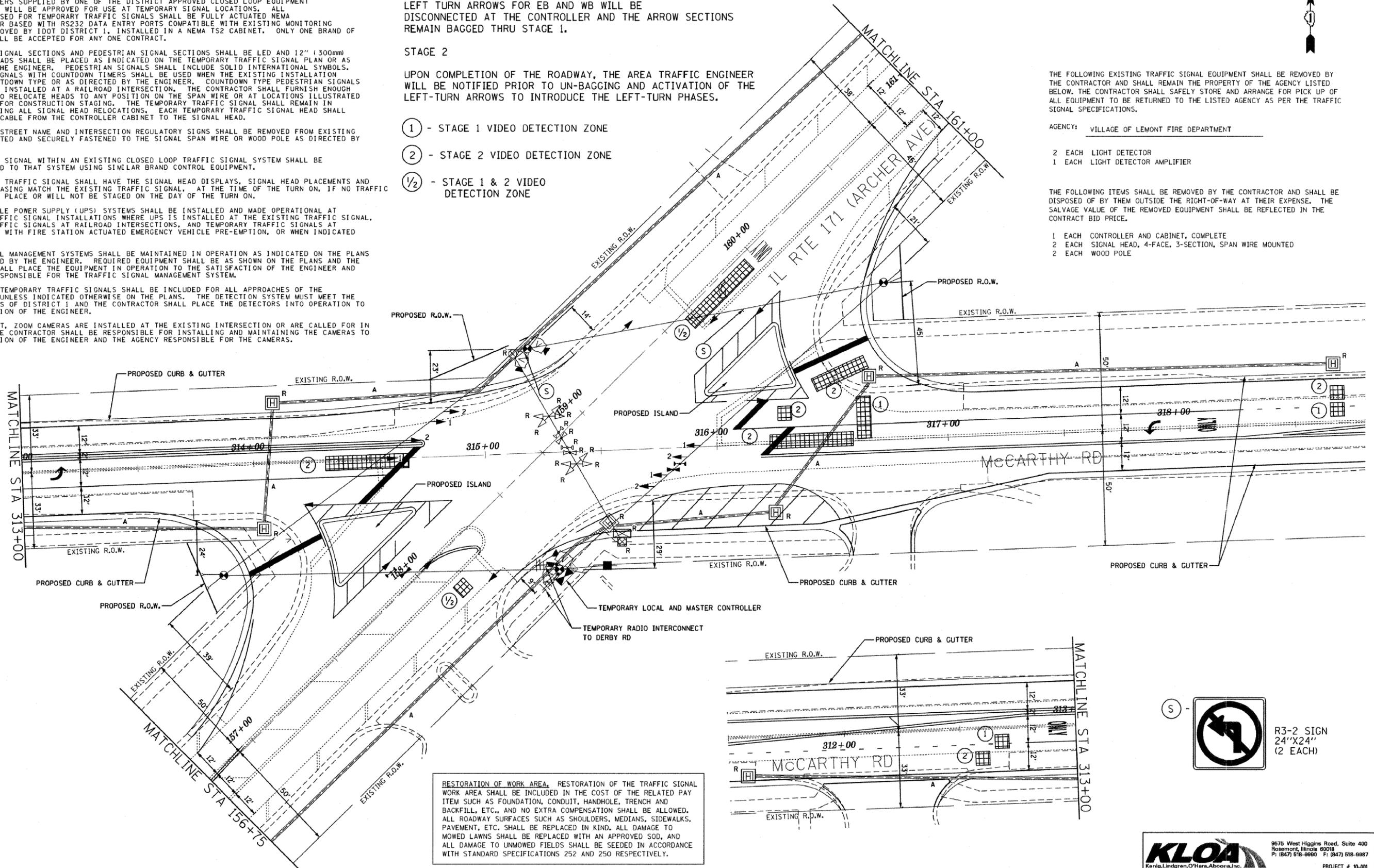
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: VILLAGE OF LEMONT FIRE DEPARTMENT

- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CONTROLLER AND CABINET, COMPLETE
- 2 EACH SIGNAL HEAD, 4-FACE, 3-SECTION, SPAN WIRE MOUNTED
- 2 EACH WOOD POLE



RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011
...signal\15-temp\ArcherMcC.dgn		DRAWN - GJG	REVISED -
	PLOT SCALE =	CHECKED - DMS	REVISED -
	PLOT DATE = 3/1/2011	DATE - 6/23/2010	REVISED -

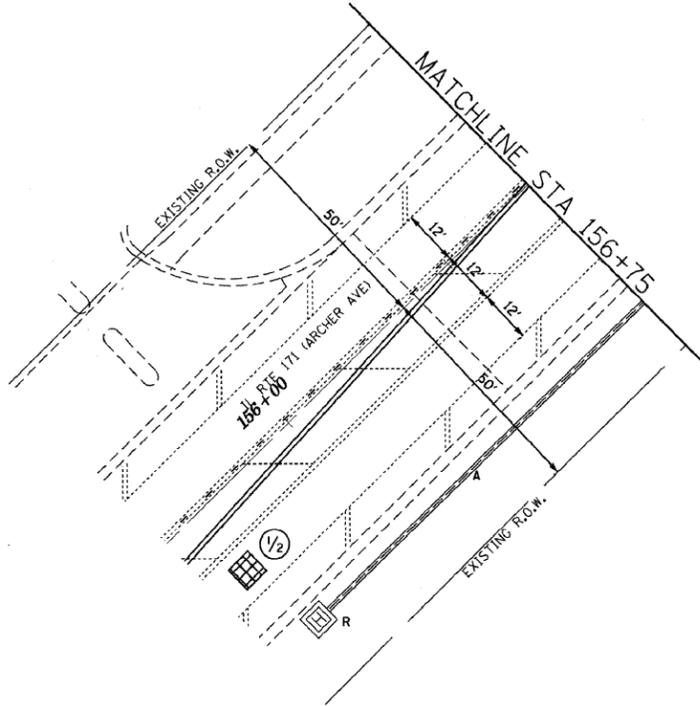
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN
IL RTE 171 (ARCHER AVE) AT MCCARTHY RD**

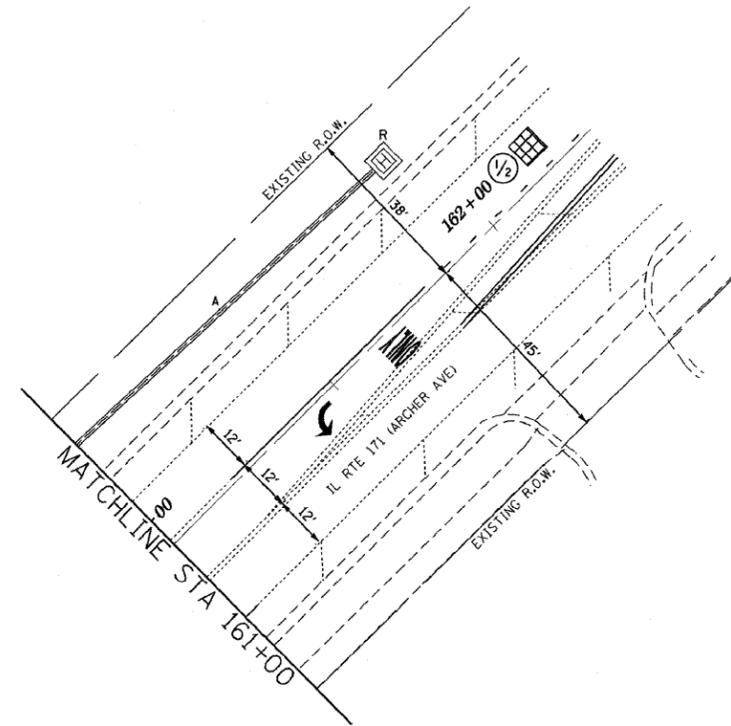
SCALE: 1" = 20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

KLOA Kenig, Lindgren, O'Hara, Aboona, Inc.		9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 518-9990 F: (847) 518-9987	
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS NO.
VAR.	02-00055-00-WR	COOK	84 60
			CONTRACT NO. 63557
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-80035041			

PROJECT # 10-001



⊙^{1/2} - STAGE 1 & 2 VIDEO DETECTION ZONE



RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN
IL RTE 171 (ARCHER AVE) AT MCCARTHY RD**

KLOA
Kenig, Lindgren, O'Hara, Aboona, Inc.
9575 West Higgins Road, Suite 400
Rosemont, Illinois 60018
P: (847) 518-9980 F: (847) 518-9987
PROJECT # 10-01

FILE NAME = ...signal\16-temp\ts ArchMcC.dgn	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011
		DRAWN - GJG	REVISED -
	PLOT SCALE =	CHECKED - DMS	REVISED -
	PLOT DATE = 3/1/2011	DATE - 6/23/2010	REVISED -

SCALE: 1" = 20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	02-00055-00-WR	COOK	84	61
CONTRACT NO. 63557				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPP-M-8003604		

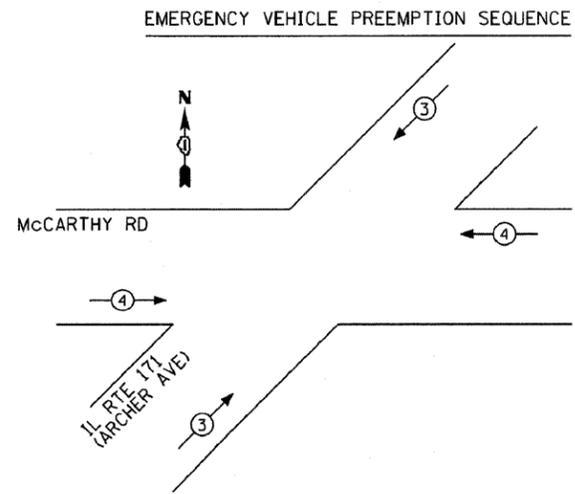
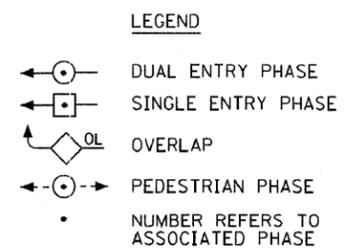
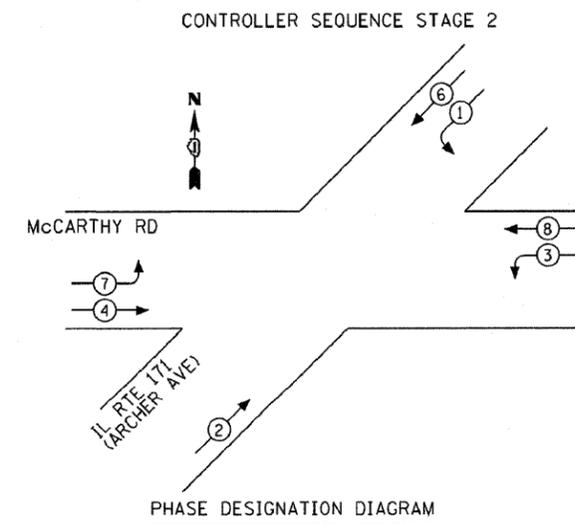
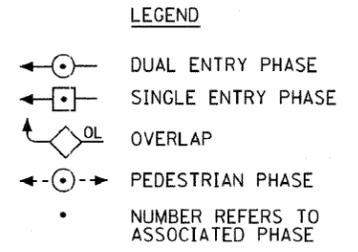
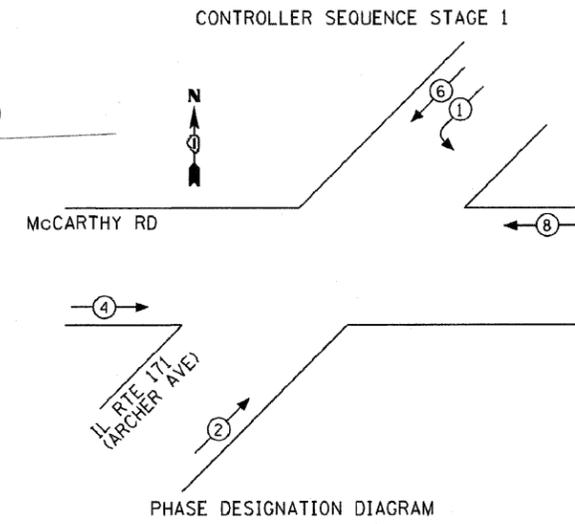
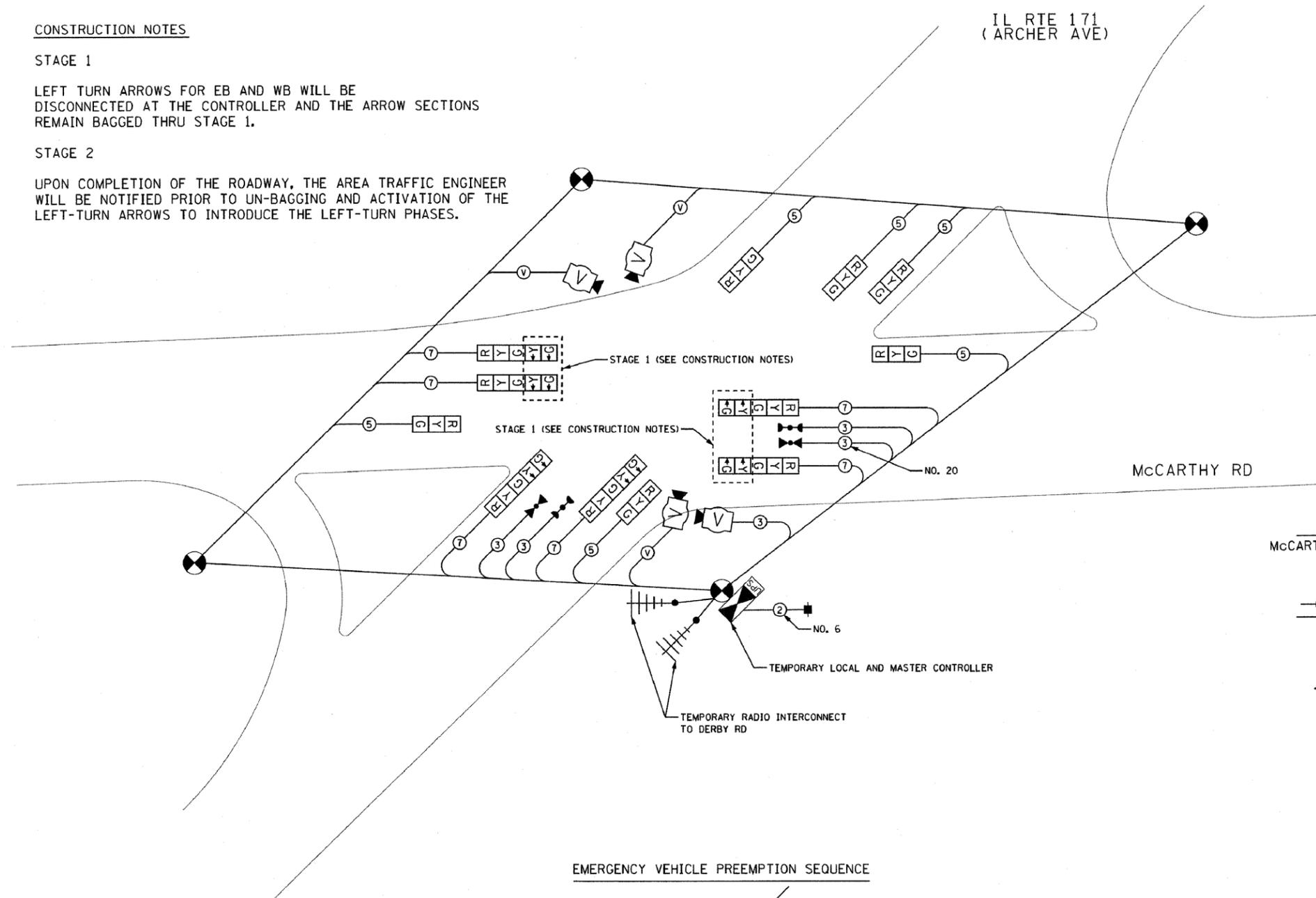
CONSTRUCTION NOTES

STAGE 1

LEFT TURN ARROWS FOR EB AND WB WILL BE DISCONNECTED AT THE CONTROLLER AND THE ARROW SECTIONS REMAIN BAGGED THRU STAGE 1.

STAGE 2

UPON COMPLETION OF THE ROADWAY, THE AREA TRAFFIC ENGINEER WILL BE NOTIFIED PRIOR TO UN-BAGGING AND ACTIVATION OF THE LEFT-TURN ARROWS TO INTRODUCE THE LEFT-TURN PHASES.



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT		

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	12		17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	12		12	0.10	14.4
PED. SIGNAL			25	1.00	
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN				0.05	
VIDEO SYSTEM	1	150		1.00	150.0
FLASHER				0.50	
TOTAL =					486.4

ENERGY COSTS TO: VILLAGE OF LEMONT, 418 MAIN STREET, LEMONT, ILLINOIS 60439

ENERGY SUPPLY CONTACT: MARK ANDERSON, PHONE: (815) 724-5988, COMPANY: COM. ED.

FILE NAME = ...signal171-tempcab ArchMcC.dgn	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011
		DRAWN - GJG	REVISED -
		CHECKED - DMS	REVISED -
		DATE - 6/23/2010	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, PHASE DESIGNATION DIAGRAM & EVP SEQUENCE				
IL RTE 171 (ARCHER AVE) AT MCCARTHY RD				
SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.		

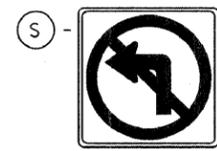
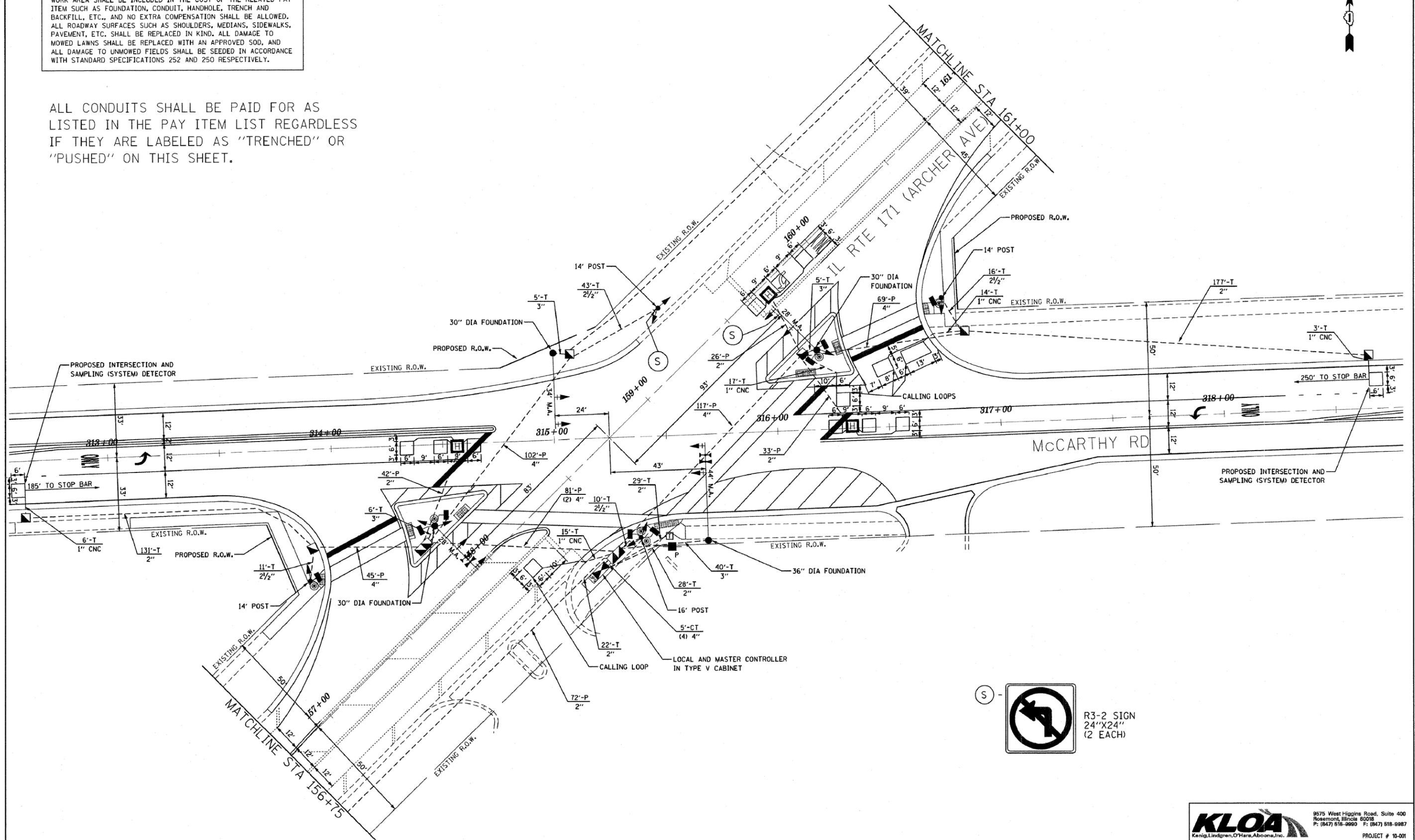
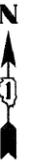
KLOA Kanig, Lindgren, O'Hara, Aboona, Inc.

9575 West Higgins Road, Suite 400, Rosemont, Illinois 60018
P: (847) 518-9980 F: (847) 518-9987 PROJECT # 10-001

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	02-00055-00-WR	COOK	84	62
CONTRACT NO. 63557			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-800315041	

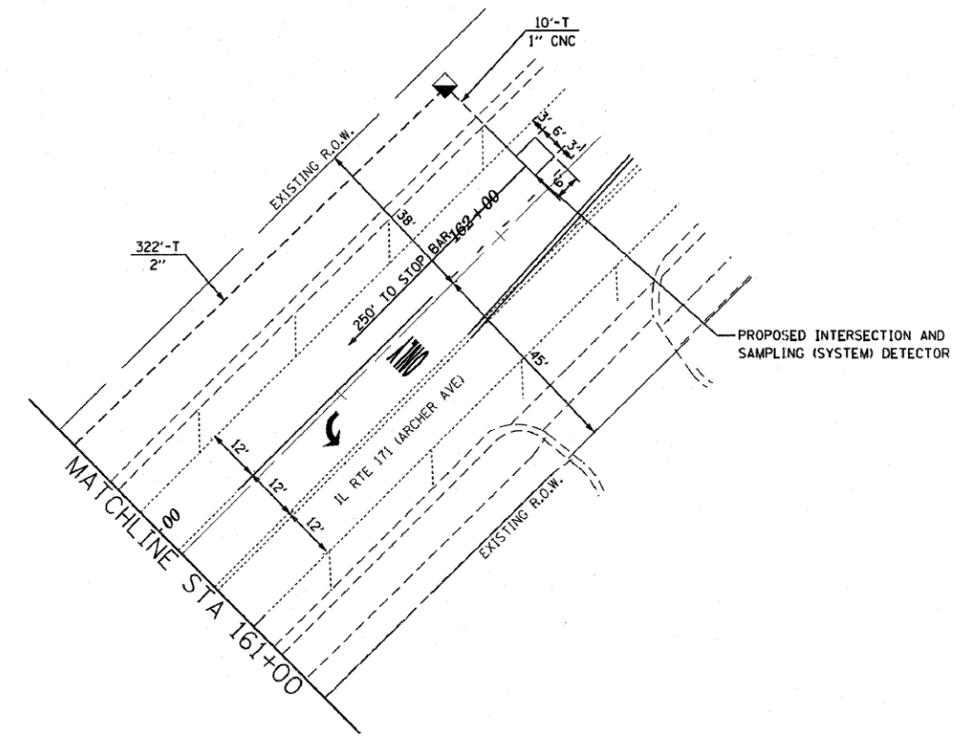
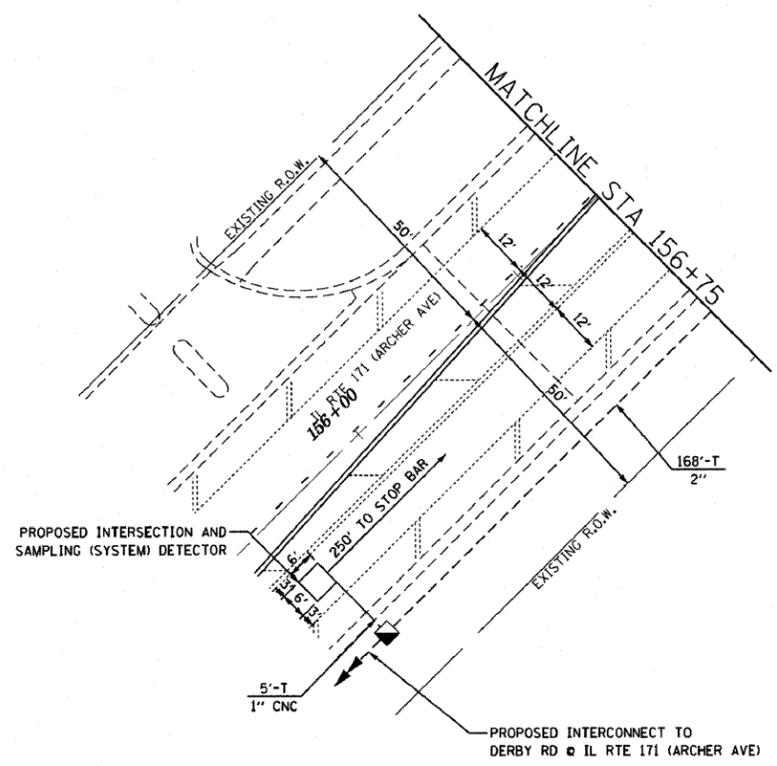
RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

ALL CONDUITS SHALL BE PAID FOR AS LISTED IN THE PAY ITEM LIST REGARDLESS IF THEY ARE LABELED AS "TRENCHED" OR "PUSHED" ON THIS SHEET.



R3-2 SIGN
24"X24"
(2 EACH)

FILE NAME = ...signal\63-signal ArchMcC.dgn	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL INSTALLATION PLAN IL RTE 171 (ARCHER AVE) AT MCCARTHY RD		KLOA Kenig, Lindgren, O'Hara, Aboona, Inc.		9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 518-9990 F: (847) 518-9987	
PLLOT SCALE =	PLLOT DATE = 10/31/2011	DRAWN - GJG	REVISED - 10/31/2011		SCALE: 1" = 20'		SHEET NO. 1 OF 2 SHEETS		PROJECT # 10-001	
		CHECKED - DMS	REVISED -		STA. TO STA.		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPP-M-8003(504)		TOTAL SHEETS 84	
		DATE - 6/23/2010	REVISED -		CONTRACT NO. 63557		COUNTY COOK		SHEET NO. 63	



ALL CONDUITS SHALL BE PAID FOR AS LISTED IN THE PAY ITEM LIST REGARDLESS IF THEY ARE LABELED AS "TRENCHED" OR "PUSHED" ON THIS SHEET.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME = ...signal\64-signal ArchMcC.dgn	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011
		DRAWN - GJG	REVISED - 10/31/2011
	PLOT SCALE =	CHECKED - DMS	REVISED -
	PLOT DATE = 10/31/2011	DATE - 6/23/2010	REVISED -

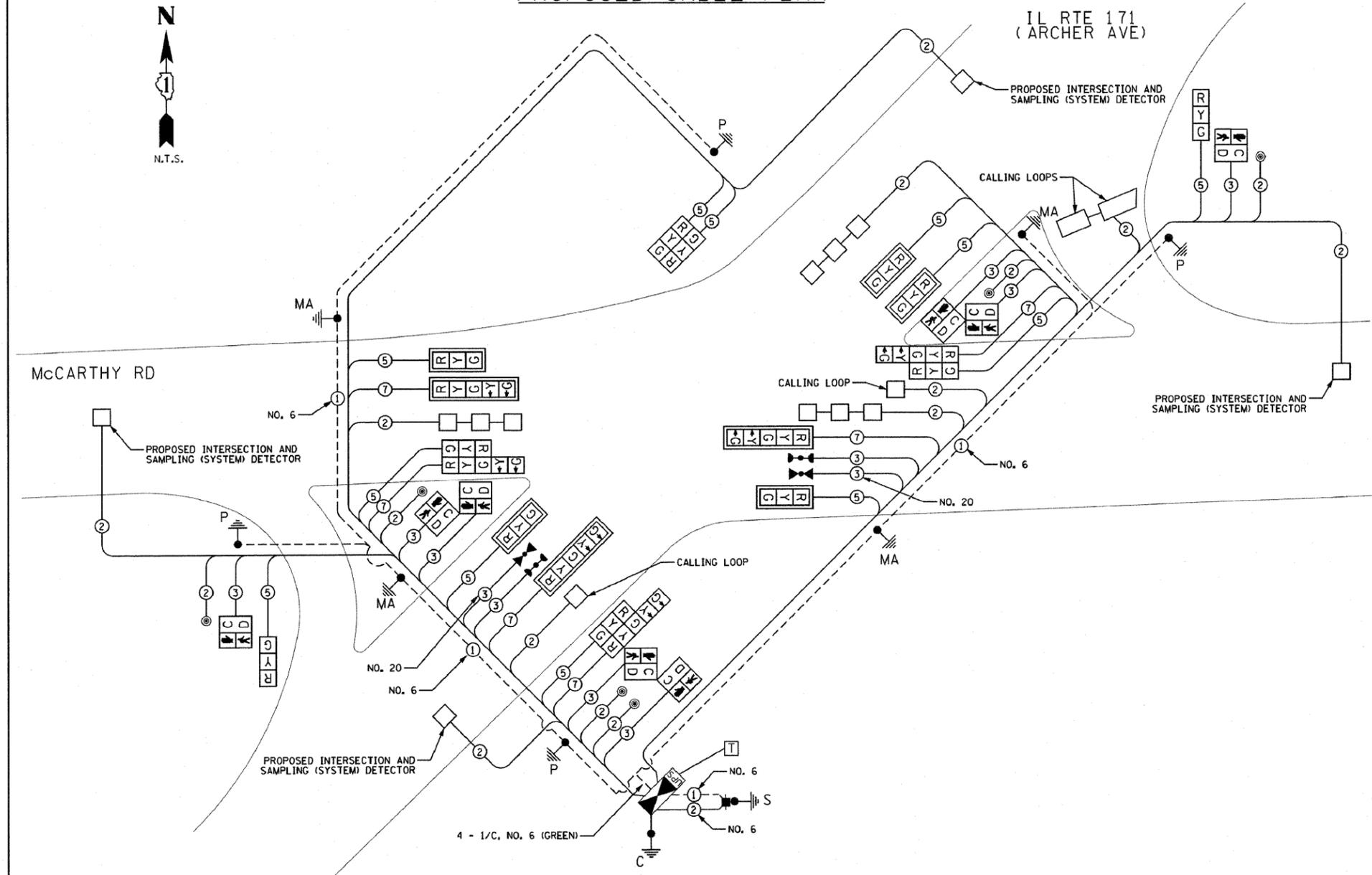
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC SIGNAL INSTALLATION PLAN IL RTE 171 (ARCHER AVE) AT MCCARTHY RD			
SCALE: 1" = 20'	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

KLOA Kenig, Lindgren, O'Hara, Aboona, Inc.		9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 518-9990 F: (847) 518-9987	
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
VAR.	02-00055-00-WR	COOK	84
			SHEET NO. 64
CONTRACT NO. 63557			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPP-M-8003(504)	

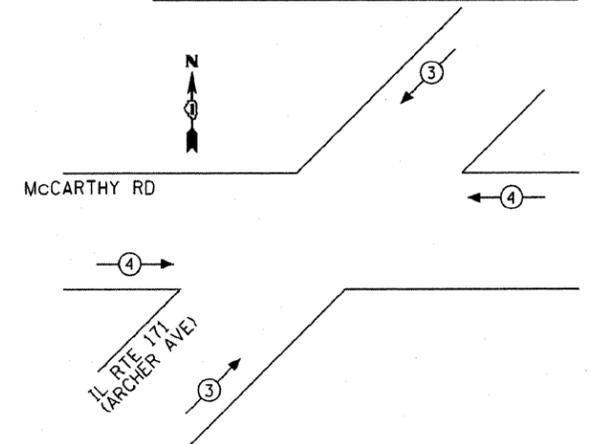
PROPOSED CABLE PLAN

SCHEDULE OF QUANTITIES



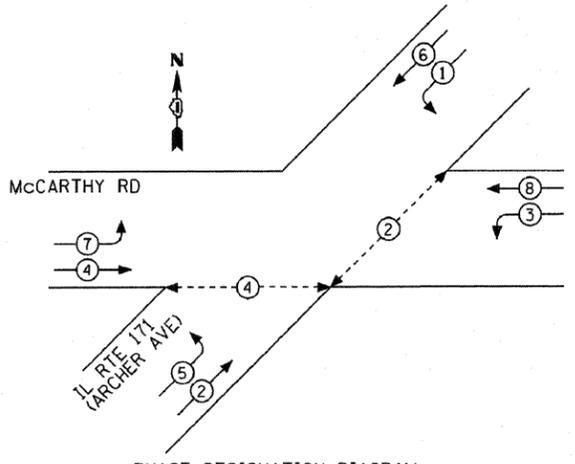
QTY	UNIT	ITEM DESCRIPTION
26	SQ FT	SIGN PANEL - TYPE 1
25	SQ FT	SIGN PANEL - TYPE 2
1	EACH	SERVICE INSTALLATION, POLE MOUNTED
1050	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA
80	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA
56	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA
520	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA
8	EACH	HANDHOLE
3	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE V CABINET
1	EACH	MASTER CONTROLLER
1	EACH	TRANSCIVER - FIBER OPTIC
300	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED
800	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1461	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
2313	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
941	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2468	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
46	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
688	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
3	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
2	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.
16	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
34	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
13	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
5	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED
3	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
3	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED
10	EACH	INDUCTIVE LOOP DETECTOR
570	FOOT	DETECTOR LOOP, TYPE I
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
6	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
9	EACH	REMOVE EXISTING HANDHOLE
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	UNINTERRUPTIBLE POWER SUPPLY SPECIAL

EMERGENCY VEHICLE PREEMPTION SEQUENCE

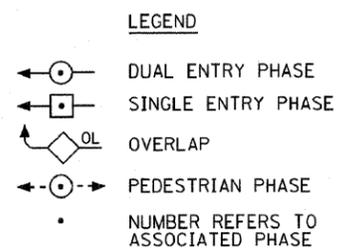


PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT		

CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	% OPERATION	
SIGNAL (RED)	18	17	0.50	153.0
(YELLOW)	18	25	0.25	112.5
(GREEN)	18	15	0.25	67.5
ARROW	12	12	0.10	14.4
PED. SIGNAL	8	25	1.00	200.0
CONTROLLER	1	100	1.00	100.0
ILLUM. SIGN			0.05	
VIDEO SYSTEM			1.00	
FLASHER			0.50	
TOTAL =				620.4

ENERGY COSTS TO:
 VILLAGE OF LEMONT
 418 MAIN STREET
 LEMONT, ILLINOIS 60439
 ENERGY SUPPLY CONTACT: MARK ANDERSON
 PHONE: (815) 724-5988
 COMPANY: COM. ED.

FILE NAME =	USER NAME =	DESIGNED -	GJG	REVISED -	2/25/2011
...signal\65-cable ArchMcC.dgn		DRAWN -	GJG	REVISED -	10/31/2011
	PLOT SCALE =	CHECKED -	DMS	REVISED -	
	PLOT DATE = 10/31/2011	DATE -	6/23/2010	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM,
 EVP SEQUENCE & SCHEDULE OF QUANTITIES
 IL RTE 171 (ARCHER AVE) AT MCCARTHY RD

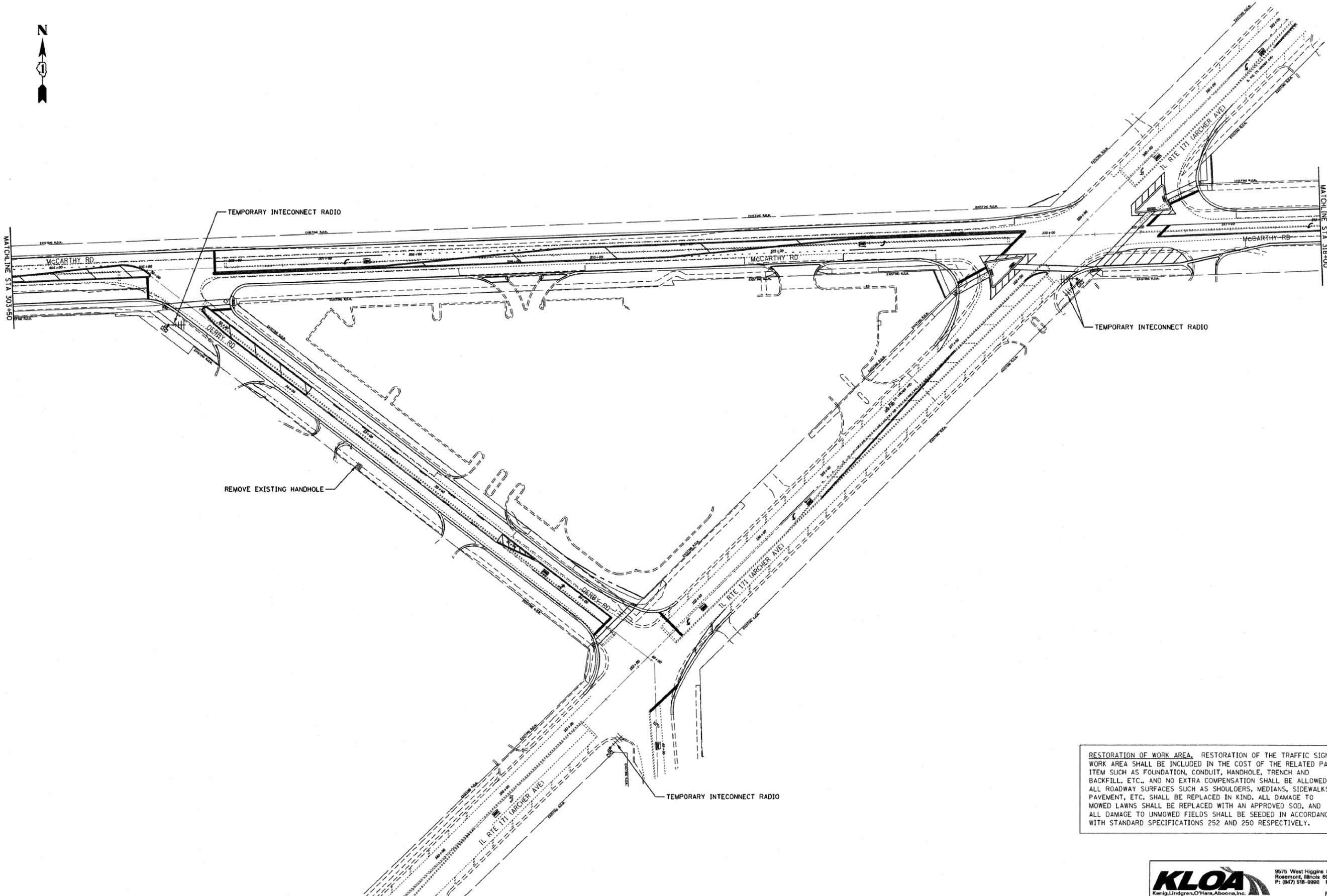
SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

KLOA
 Kenig, Lindgren, O'Hara, Aboum, Inc.

9575 West Higgins Road, Suite 400
 Rosemont, Illinois 60018
 P: (847) 518-9990 F: (847) 518-9987

PROJECT # 10-001

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	02-00055-00-WR	COOK	84	65
CONTRACT NO. 63557			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPP-M-800315041	



RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOO, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =
...signal\21-tempint.dgn

USER NAME =
PLOT SCALE =
PLOT DATE = 3/1/2011

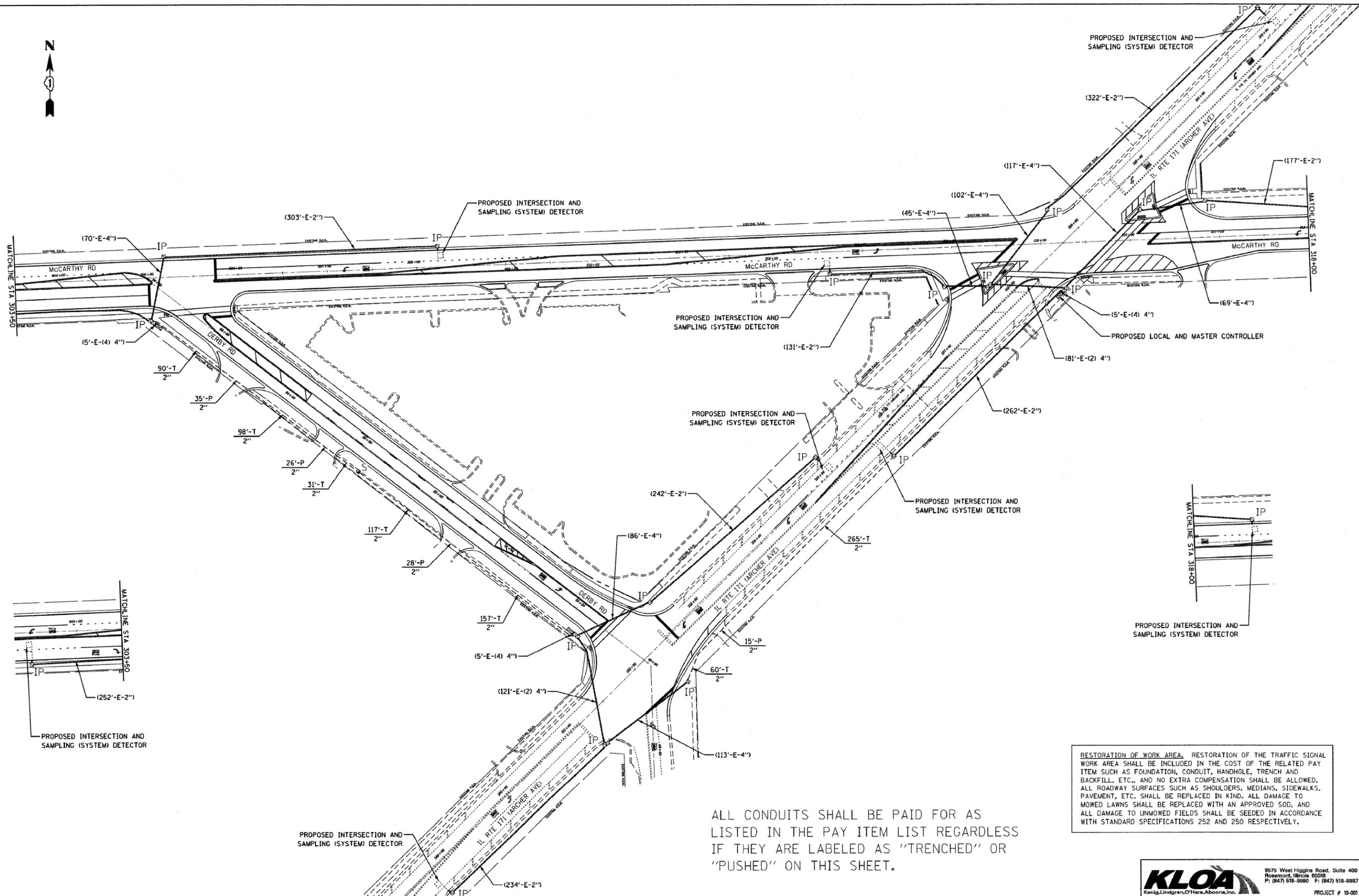
DESIGNED - GJG	REVISED - 2/25/2011
DRAWN - GJG	REVISED -
CHECKED - DMS	REVISED -
DATE - 6/23/2010	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN
McCARTHY RD AT DERBY RD; IL RTE 171 (ARCHER AVE) AT McCarthy RD;
IL RTE 171 (ARCHER AVE) AT DERBY RD**

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. TO STA.

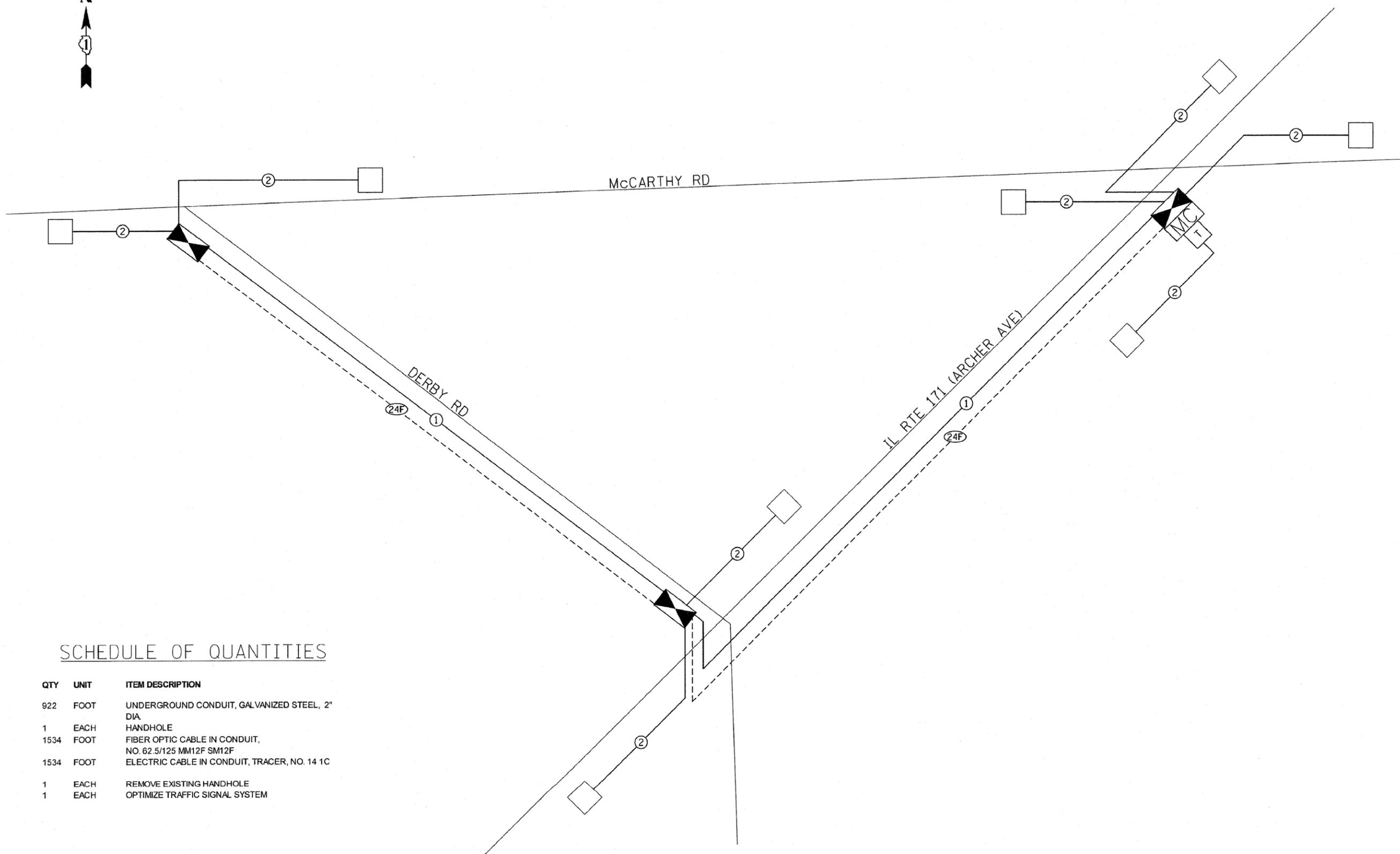
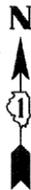
KLOA Kanig, Lindgren, O'Hara, Aboona, Inc.		9575 West Higgins Road, Suite 400 Rosemont, Illinois 60018 P: (847) 518-0980 F: (847) 518-0987		PROJECT # 10-001	
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
VAR.	02-00055-00-WR	COOK	84	66	
			CONTRACT NO. 63557		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPP-M-8003604			



ALL CONDUITS SHALL BE PAID FOR AS LISTED IN THE PAY ITEM LIST REGARDLESS IF THEY ARE LABELED AS "TRENCHED" OR "PUSHED" ON THIS SHEET.

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME = ...signal\67-1mt.dgn	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INTERCONNECT PLAN McCARTHY RD AT DERBY RD; IL RTE 171 (ARCHER AVE) AT McCARTHY RD; IL RTE 171 (ARCHER AVE) AT DERBY RD			KLOA Kenig, Lindgren, O'Hara, Aboona, Inc. PROJECT # 10-001		
		DRAWN - GJG	REVISED - 10/31/2011		F.A. RTE.	SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
		CHECKED - DMS	REVISED -		VAR.	02-00055-00-WR	COOK		84	67
		DATE - 6/23/2010	REVISED -		SCALE: 1" = 50'		SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 63557	
					FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPP-M-80031504					



SCHEDULE OF QUANTITIES

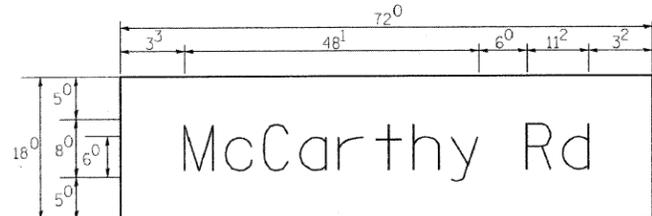
QTY	UNIT	ITEM DESCRIPTION
922	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
1	EACH	HANDHOLE
1534	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 MM12F SM12F
1534	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C
1	EACH	REMOVE EXISTING HANDHOLE
1	EACH	OPTIMIZE TRAFFIC SIGNAL SYSTEM



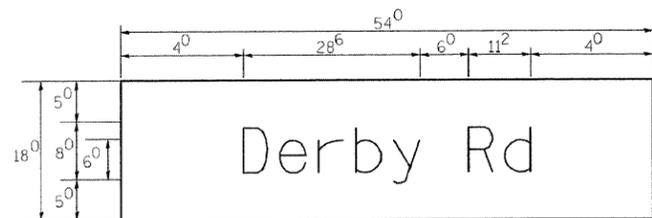
PROJECT # 10-001

FILE NAME = ...\\s1gnal\68-schematic.dgn	USER NAME =	DESIGNED - GJG	REVISED - 2/25/2011	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INTERCONNECT SCHEMATIC McCARTHY RD AT DERBY RD; IL RTE 171 (ARCHER AVE) AT MCCARTHY RD; IL RTE 171 (ARCHER AVE) AT DERBY RD			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	DRAWN - GJG	REVISED - 10/31/2011		SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.	COOK	84	68
	PLOT DATE = 10/31/2011	CHECKED - DMS	REVISED -					CONTRACT NO. 63557				
		DATE - 6/23/2010	REVISED -					FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT HPP-M-800315041		

PANEL SIGN DESIGN TYPE 1



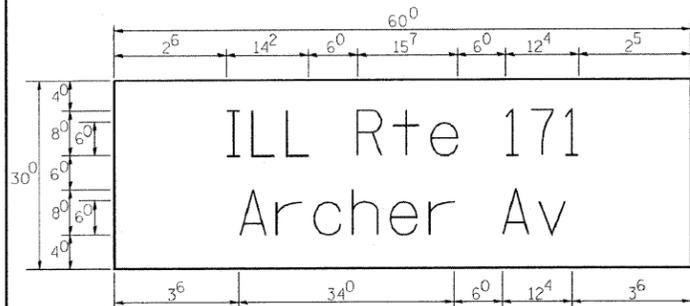
___ Sq. Ft. each
9.0 Sq. Ft. each
 ___ 3 Required
 Design Series D



___ Sq. Ft. each
6.75 Sq. Ft. each
 ___ 4 Required
 Design Series D

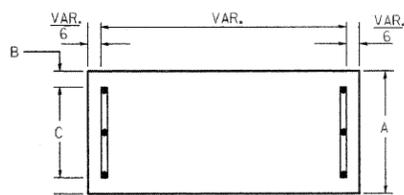
NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

PANEL SIGN DESIGN TYPE 2

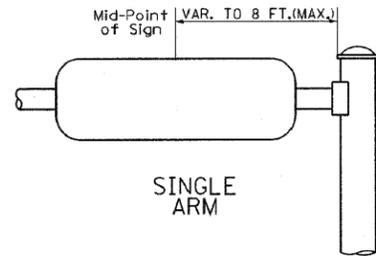


___ Sq. Ft. each
12.5 Sq. Ft. each
 ___ 4 Required
 Design Series D

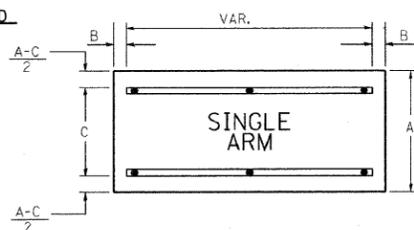
SUPPORTING CHANNELS



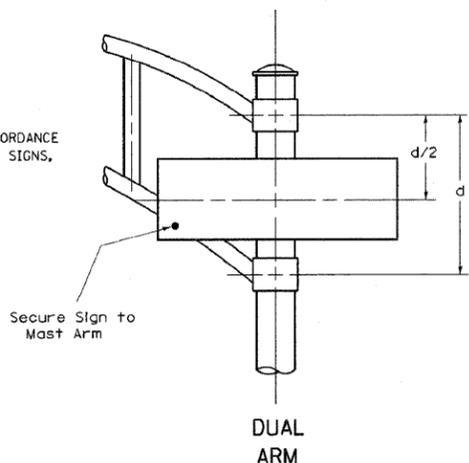
A	B	C
18"	2"	14"



SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM

Shall be used. See Note #5.

Upper Case To Lower Case
 Spacing Chart 8-6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER																	
	a c d e		g o q		b h i k l		m n p r u		f w		j		s t		v y		x z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14	14	
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17	17	
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15	15	
D O O R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15	15	
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12	12	
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21	21	
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21	21	
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14	14	
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14	14	
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14	14	
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14	14	
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14	14	
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12	12	
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21	21	

Lower Case To Lower Case
 Spacing Chart 6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER																	
	a c d e		g o q		b h i k l		m n p r u		f w		j		s t		v y		x z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
ad h g i j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17	17	
l m n q u																		
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14	14	
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14	14	
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10	10	
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14	14	
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12	12	
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14	14	
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14	14	

Number To Number
 Spacing Chart 8 Inch Series "C & D"

FIRST NUMBER	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21
2 3 4	14	15	14	15	14	15	12	14	14	14	15	14	15	11	12	16	17	14	15	
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15
6	16	17	14	15	14	15	12	14	14	14	15	14	15	11	12	14	15	14	15	15
7	12	14	12	14	14	15	12	15	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	15	12	14	14	15	16	17	12	14	16	17	14	15

EXAMPLE, 2³ DENOTES 3"

UPPER AND LOWER CASE
 LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 ⁶	5 ⁰	5 ⁰	6 ⁵	a	3 ⁵	4 ²
B	3 ²	4 ⁰	4 ³	5 ³	b	3 ⁵	4 ²
C	3 ²	4 ⁰	4 ³	5 ³	c	3 ⁵	4 ¹
D	3 ²	4 ⁰	4 ³	5 ³	d	3 ⁵	4 ²
E	3 ⁰	3 ⁵	4 ⁰	4 ⁷	e	3 ⁵	4 ²
F	3 ⁰	3 ⁵	4 ⁰	4 ⁷	f	2 ³	2 ⁶
G	3 ²	4 ⁰	4 ³	5 ³	g	3 ⁵	4 ²
H	3 ²	4 ⁰	4 ³	5 ³	h	3 ⁵	4 ²
I	0 ⁷	0 ⁷	1 ¹	1 ²	i	1 ¹	1 ¹
J	3 ⁰	3 ⁶	4 ⁰	5 ⁰	j	2 ⁰	2 ²
K	3 ²	4 ¹	4 ³	5 ⁴	k	3 ⁵	4 ²
L	3 ⁰	3 ⁵	4 ⁰	4 ⁷	l	1 ¹	1 ¹
M	3 ⁷	4 ⁵	5 ¹	6 ¹	m	6 ⁰	7 ⁰
N	3 ²	4 ⁰	4 ³	5 ³	n	3 ⁵	4 ²
O	3 ⁴	4 ²	4 ⁵	5 ⁵	o	3 ⁶	4 ³
P	3 ²	4 ⁰	4 ³	5 ³	p	3 ⁵	4 ²
Q	3 ⁴	4 ²	4 ⁵	5 ⁵	q	3 ⁵	4 ²
R	3 ²	4 ⁰	4 ³	5 ³	r	2 ⁶	3 ²
S	3 ²	4 ⁰	4 ³	5 ³	s	3 ⁶	4 ²
T	3 ⁰	3 ⁵	4 ⁰	4 ⁷	t	2 ⁷	3 ²
U	3 ²	4 ⁰	4 ³	5 ³	u	3 ⁵	4 ²
V	3 ⁵	4 ⁴	4 ⁷	6 ⁰	v	4 ²	4 ⁷
W	4 ⁴	5 ²	6 ⁰	7 ⁰	w	5 ⁵	6 ⁴
X	3 ⁴	4 ⁰	4 ⁵	5 ³	x	4 ⁴	5 ¹
Y	3 ⁶	5 ⁰	5 ⁰	6 ⁶	y	4 ⁶	5 ³
Z	3 ²	4 ⁰	4 ³	5 ³	z	3 ⁶	4 ³

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	1 ²	1 ⁴	1 ⁵	2 ⁰
2	3 ²	4 ⁰	4 ³	5 ³
3	3 ²	4 ⁰	4 ³	5 ³
4	3 ⁵	4 ³	4 ⁷	5 ⁷
5	3 ²	4 ⁰	4 ³	5 ³
6	3 ²	4 ⁰	4 ³	5 ³
7	3 ²	4 ⁰	4 ³	5 ³
8	3 ²	4 ⁰	4 ³	5 ³
9	3 ²	4 ⁰	4 ³	5 ³
0	3 ⁴	4 ²	4 ⁵	5 ⁵

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012. AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

* J.O. HERBERT CO. MIDLOTHIAN, VA. * WESTERN REMAC INC. WOODRIDGE, IL.

PARTS LISTING:
 SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
 SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
 SELF TAPPING WITH NEOPRENE WASHER
 BRACKETS PART #HPN034 (UNIVERSAL)
 CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

FILE NAME =	USER NAME =	DESIGNED - DAG/BCK	REVISED - DAG 10/28/09
...sign\124-stname.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAG/DAD	REVISED -
		DATE - 3/15/2009	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

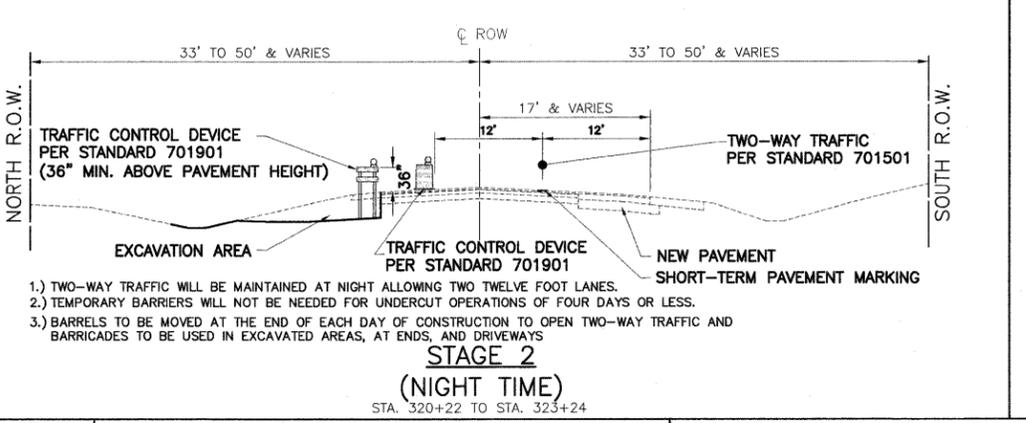
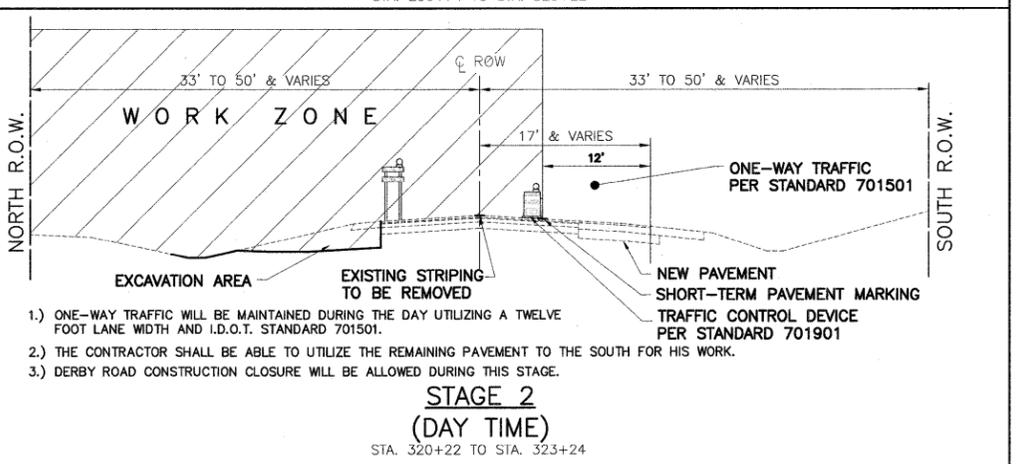
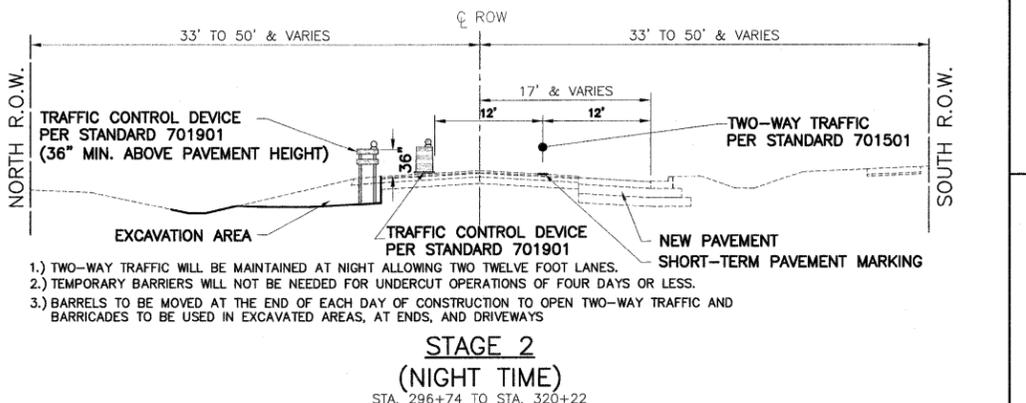
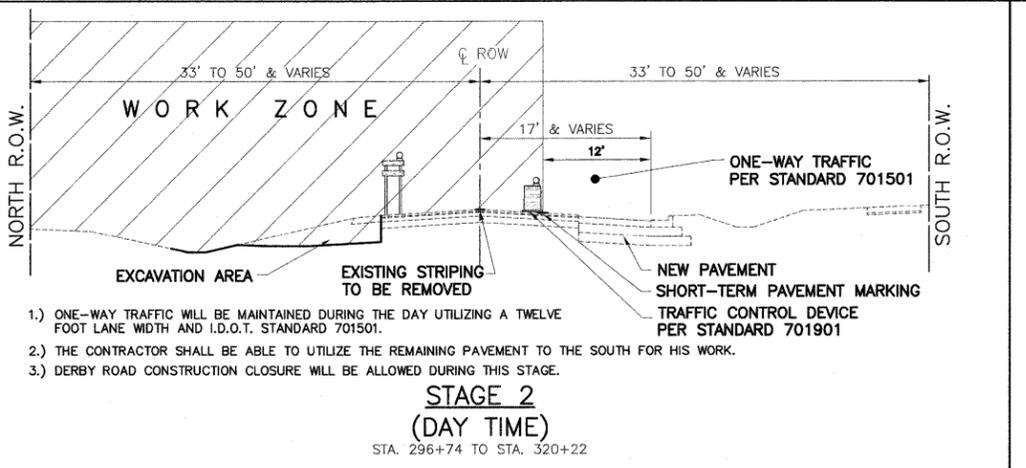
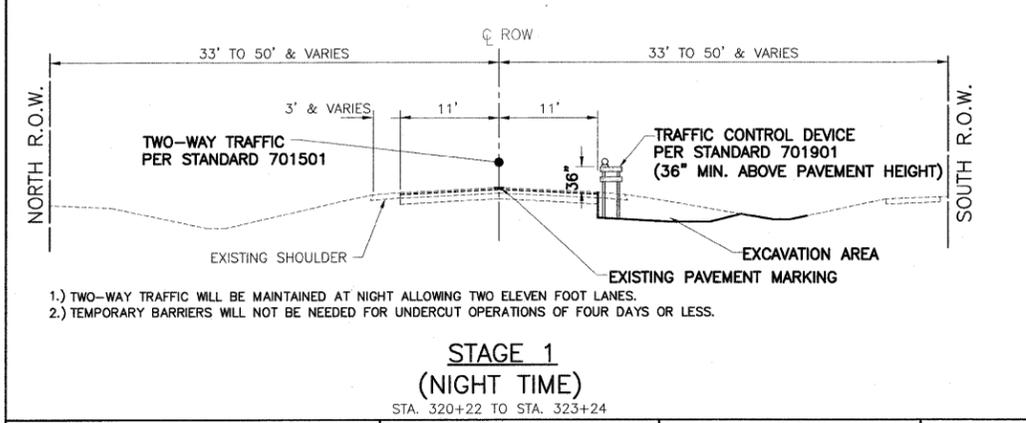
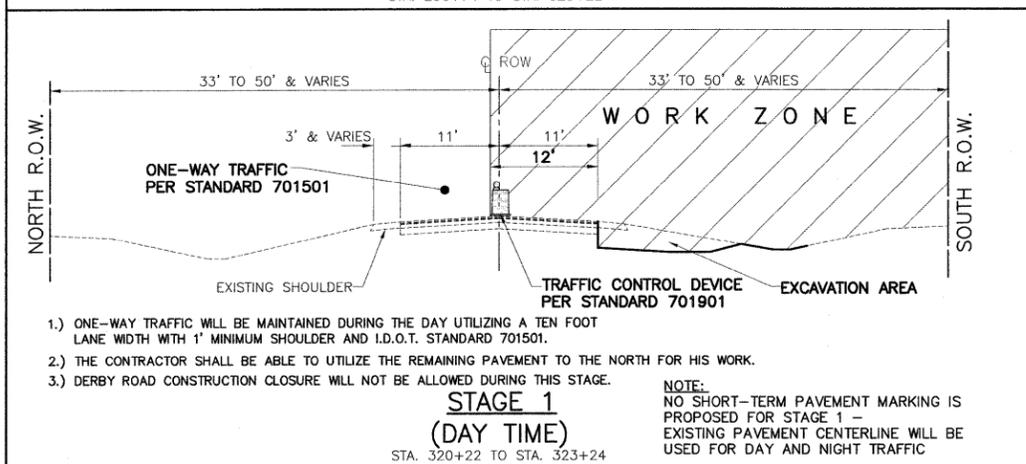
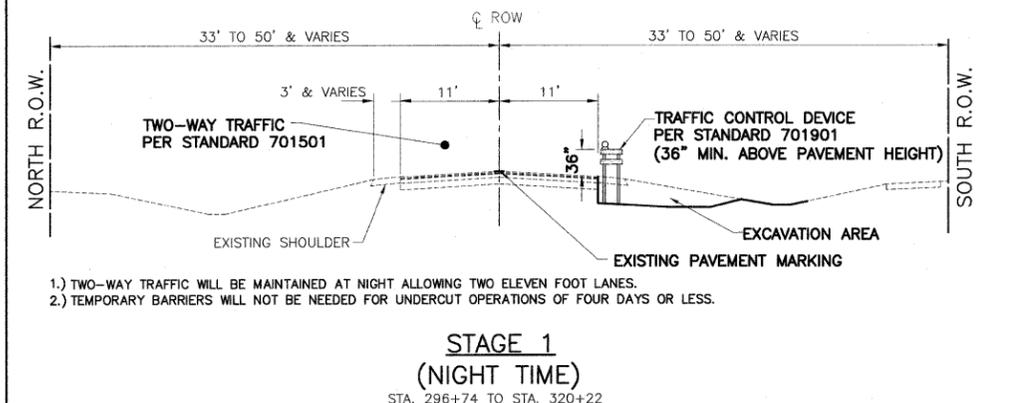
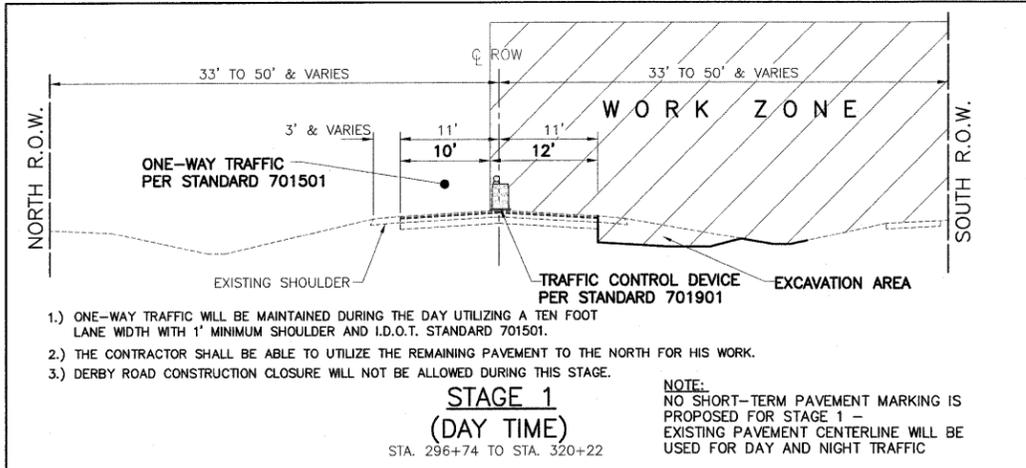
DISTRICT 1
 MAST ARM MOUNTED STREET NAME SIGNS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

KLOA Kanig, Lindgren, O'Hara, Aboona, Inc. PROJECT # 10-001

9575 West Higgins Road, Suite 400
 Rosemont, Illinois 60018
 P: (847) 516-9960 F: (847) 516-9987

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	02-00055-00-WR	COOK	84	69
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HFP-N-8003(504)				



MOT GENERAL NOTES

ALL OF THE TRAFFIC CONTROL DEVICES SHALL BE IN PLACE BEFORE CONSTRUCTION IS STARTED. TEMPORARY TRAFFIC SIGNALS SHALL BE IN PLACE AT THE INTERSECTIONS NOTED ON THE PLANS.

TEMPORARY INTERSECTION SIGNALIZATION SHALL BE ADJUSTED TO ACCOMMODATE THE VARIOUS STAGE OF CONSTRUCTION SHOWN. THE TRAFFIC CONTROL PLANS SHALL SERVE AS A GUIDE OF THE SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT.

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE KEPT OPEN TO TRAFFIC DURING TIMES OF NO CONSTRUCTION.

REMOVE ALL EXISTING PAVEMENT MARKINGS CONFLICTING WITH REVISED TRAFFIC PATTERS, AS APPLICABLE BY THE SPECIFICATIONS TEMPORARY PAVEMENT MARKING OR SHORT-TERM PAVEMENT MARKING SHALL BE USED AS SHOWN. CONFLICTING MARKING BETWEEN STAGES SHALL BE REMOVED BETWEEN STAGES WHEN NECESSARY.

PROVIDE TEMPORARY BITUMINOUS RAMP WEDGE TO TRANSITION THE DIFFERENTIAL ELEVATIONS CAUSED BY STAGED CONSTRUCTION BETWEEN THE NEW CONSTRUCTION AND THE EXISTING PAVEMENT. ANY PAVEMENT DROP-OFFS GREATER THAT 3 INCHES IMMEDIATELY ADJACENT TO TRAFFIC SHALL NOT BE LEFT OVERNIGHT. PROVIDE TEMPORARY BITUMINOUS RAMP WEDGE OR OTHER MITIGATING MEASURES APPROVE BY THE ENGINEER.

EXISTING TRAFFIC CONTROL SIGNS AND MESSAGES THAT ARE IN CONFLICT WITH THE PROPOSED MAINTENANCE OF TRAFFIC SHALL BE TEMPORARILY COVERED OR MODIFIED WITH TEMPORARY OVERLAY AS SHOWN IN THE PLANS AND DIRECTED BY THE ENGINEER.

ALL CHANGEABLE MESSAGE SIGNS SHALL BE SET TO WORN TRAFFIC OF SET UP OF STAGE 1 OR CHANGING TO STAGE 2 48 HOURS IN ADVANCE OF THE WORK.

Illinois WORK ZONE TRAFFIC CONTROL September 2010

Drop-off Location	Normal Posted Speed	Drop-off Height (x) and Type	Treatment Required
≤ 3 ft from edge of pavement ⁽¹⁾⁽²⁾	all	≤ 1 in	None
	< 45 mph	1 in < x ≤ 3 in	Low Shoulder signs (2 mile spacing)
		3 in < x ≤ 18 in	Place channelizing devices at 50-ft spacing
	≥ 45 mph	3 in < x ≤ 12 in	Place channelizing devices at 100-ft spacing
		18 in < x ≤ 24 in for < 0.5 mile or < 48 hours ⁽⁴⁾	Place channelizing devices at 50-ft spacing
	≥ 45 mph	12 in < x ≤ 18 in for < 0.5 mile or < 48 hours	Place channelizing devices at 100-ft spacing
12 in < x ≤ 24 in for > 0.5 mile or > 48 hours		Closure using temporary traffic barrier	
≥ 45 mph	18 in < x ≤ 24 in ⁽⁵⁾	Closure using temporary traffic barrier	
	all	> 24 in ⁽⁶⁾	Closure using temporary traffic barrier
3 ft < x ≤ 8 ft from edge of pavement ⁽³⁾	all	≤ 1 in	none
	< 45 mph	1 in < x ≤ 3 in	Low Shoulder signs (2-mile spacing)
		3 in < x ≤ 24 in ⁽⁴⁾	Place channelizing devices at 50-ft spacing
	≥ 45 mph	3 in < x ≤ 24 in	Place channelizing devices at 100-ft spacing
all	24 in ⁽⁵⁾	Closure using temporary traffic barrier	
	< 45 mph	12 in < x ≤ 24 in ⁽⁴⁾	Place channelizing devices at 50-ft spacing
≥ 45 mph		12 in < x ≤ 24 in	Place channelizing devices at 100-ft spacing
> 8 ft to clear zone ⁽³⁾	all	> 24 in ⁽⁶⁾	Closure using temporary traffic barrier

- Notes:**
- Place channelizing devices and temporary barrier at same level as traveling lane or shoulder profile.
 - Channelizing devices may be placed at the drop-off elevation to preserve lane width. Raise the reflective area and warning light (if required) to the elevation above traveling lane or shoulder profile; see Highway Standard 701901.
 - Place channelizing devices and temporary barrier at same level as side slope profile to be fully visible.
 - Length and duration may be exceeded for urban areas when engineering judgment indicates sight distance will be adversely affected by temporary barrier.
 - Temporary traffic barrier may be eliminated for stationary operations of less than 24 hours for multilane, and four days per stage for two lanes based on engineering judgment.

CONDITION 2 — DROP-OFF AT EDGE OF TRAVEL LANE (US Customary)

Figure 55-2.B

FILE NAME = FAU 1587 (McCARTHY ROAD)
FAP 577 (ARCHER AVENUE)
FAU 1024 (DERBY ROAD)
CHANNELIZATION AND TRAFFIC SIGNAL INSTALLATION

USER NAME =
DESIGNED — JLC
DRAWN — JFP
CHECKED — THK
DATE — 11/10

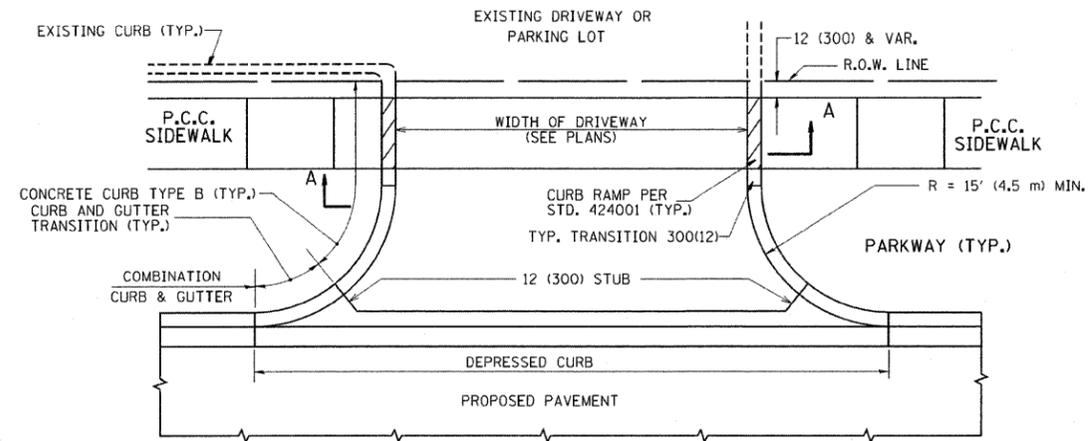
REVISOR — THK 3-17-11
REVISIONS —
REVISIONS —
REVISIONS —

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

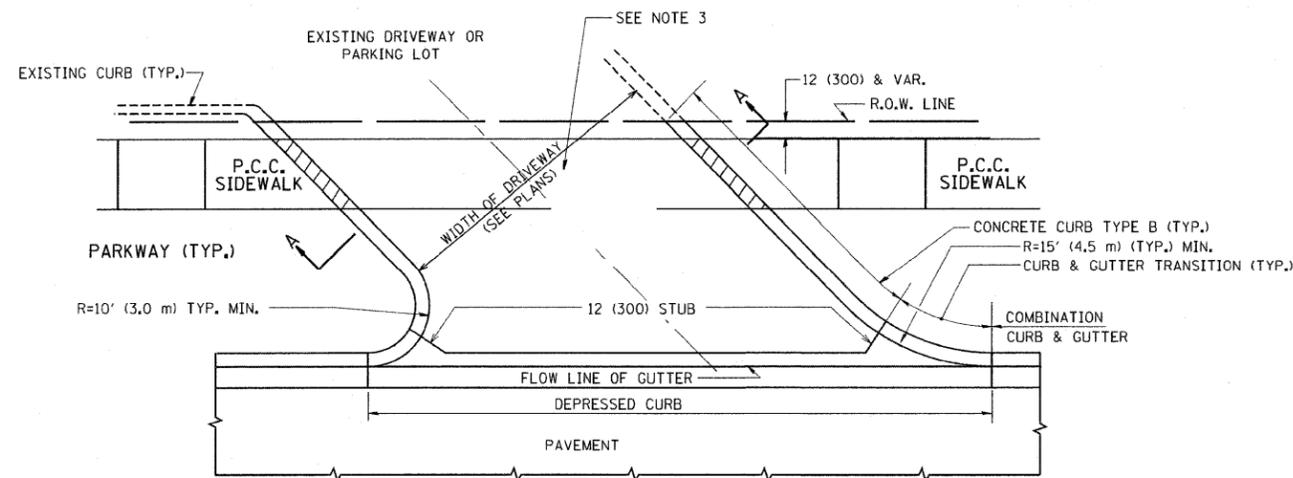
MAINTENANCE OF TRAFFIC:
McCARTHY ROAD- STA. 296+74 TO STA. 323+24
SCALE: 1"=10' SHEET NO. OF SHEETS STA. TO STA.

Frank Novotny & Associates, Inc.
225 Midway Drive • Willowbrook, IL • 60127 • Telephone: (630) 887-8640 • Fax: (630) 887-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-00826

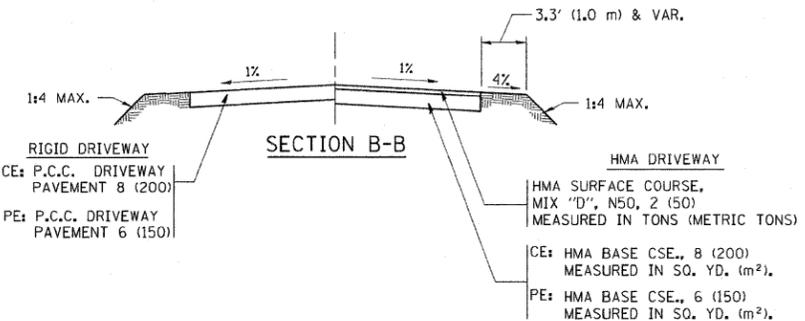
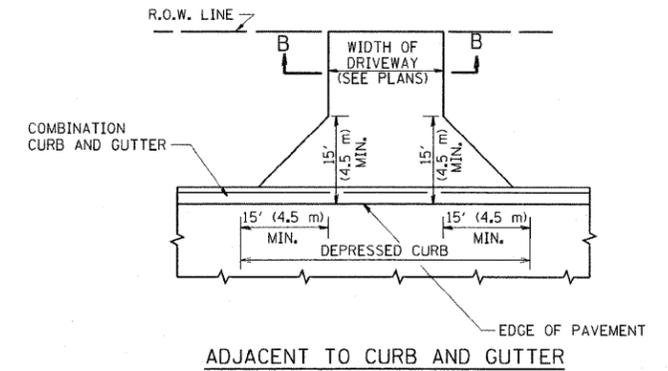
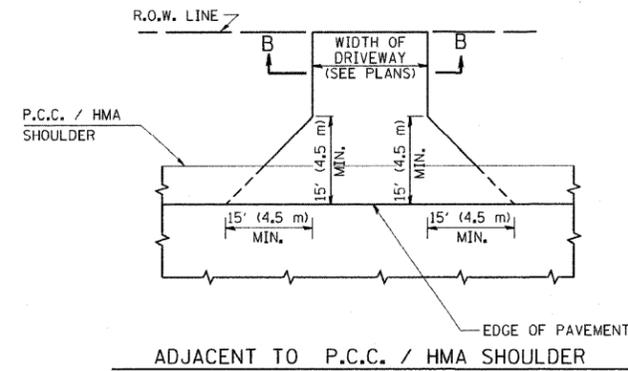
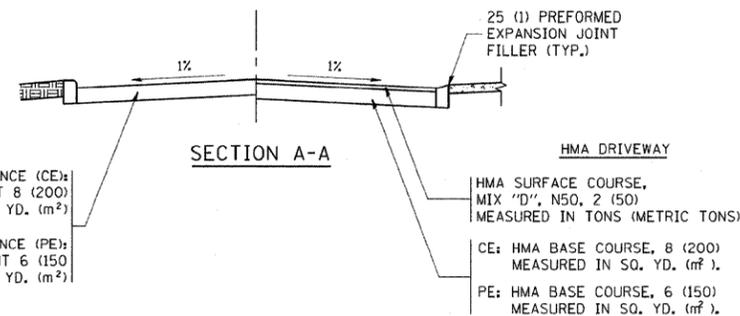
F.A.U. F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	70
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX 'D', N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²)

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

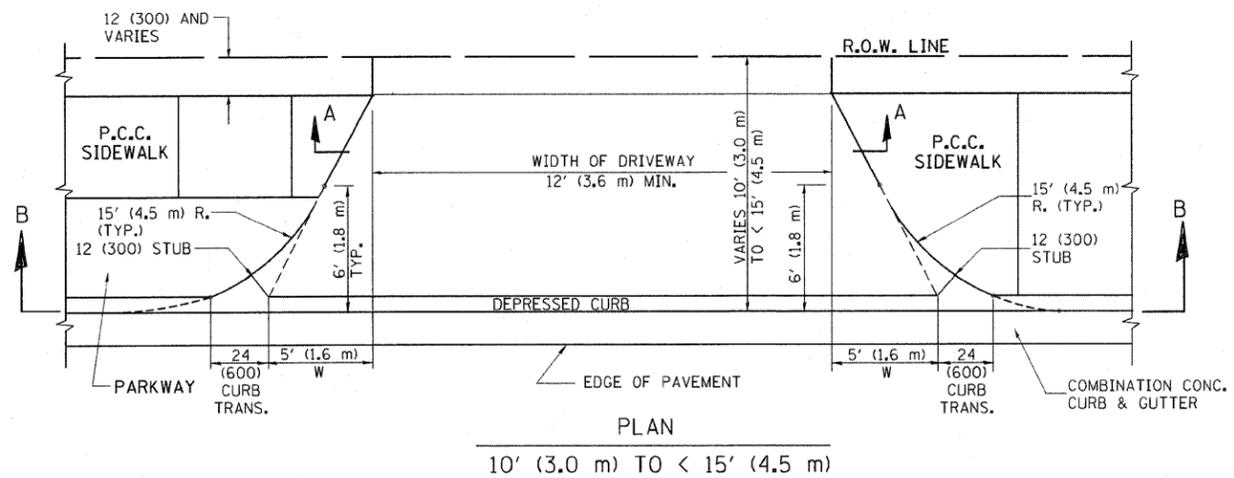
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		CHECKED -	REVISED - R. BORO 06-11-08
		DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

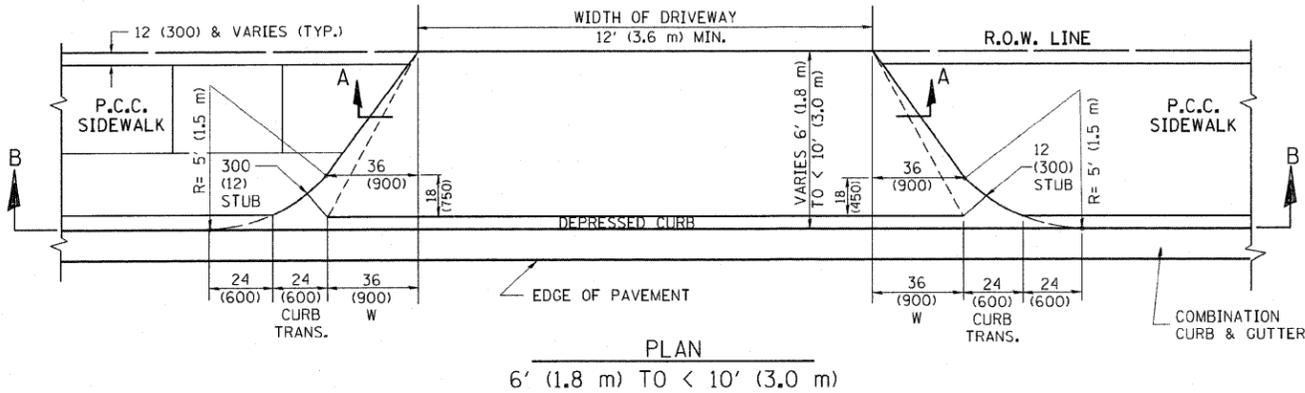
DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

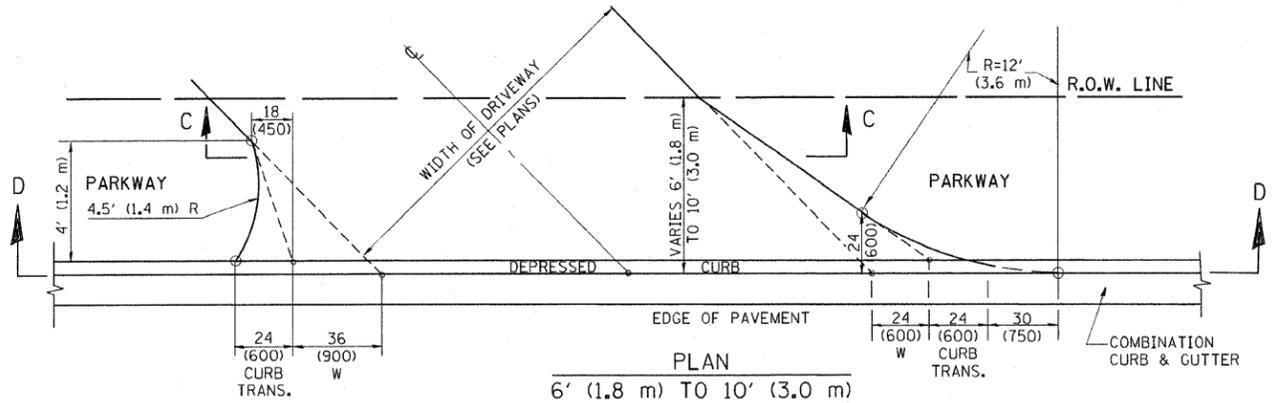
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	71
BD0156-07 (BD-01)		CONTRACT NO. 63557		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



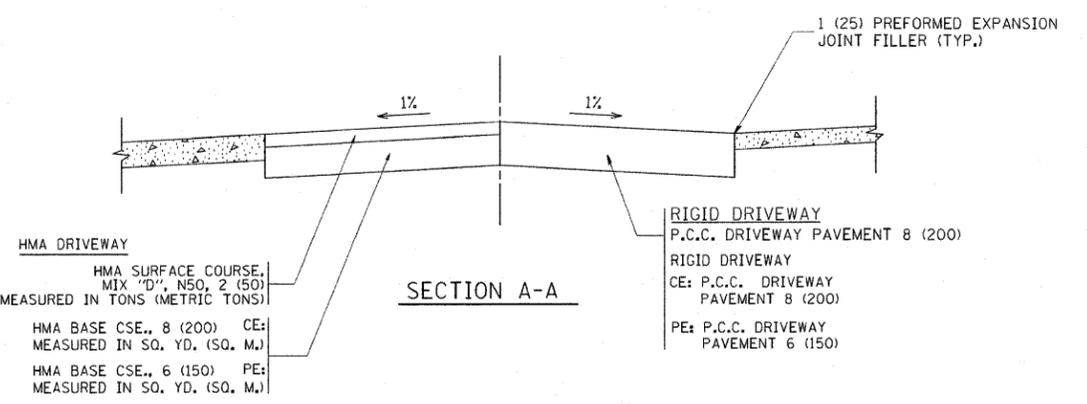
PLAN
10' (3.0 m) TO < 15' (4.5 m)



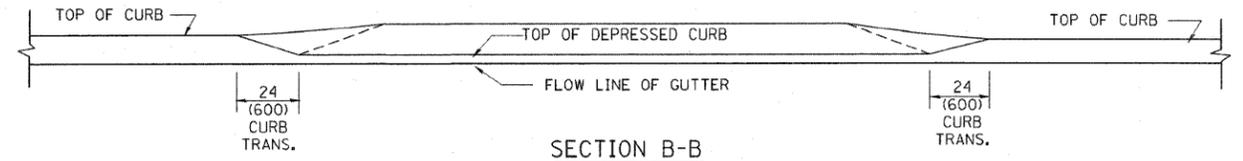
PLAN
6' (1.8 m) TO < 10' (3.0 m)



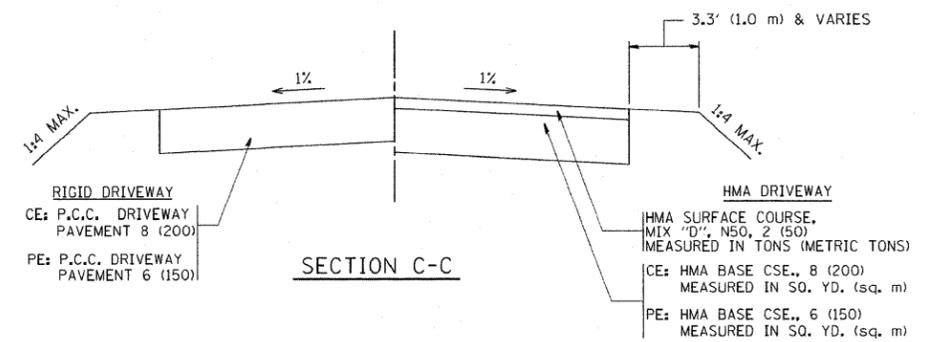
PLAN
6' (1.8 m) TO 10' (3.0 m)



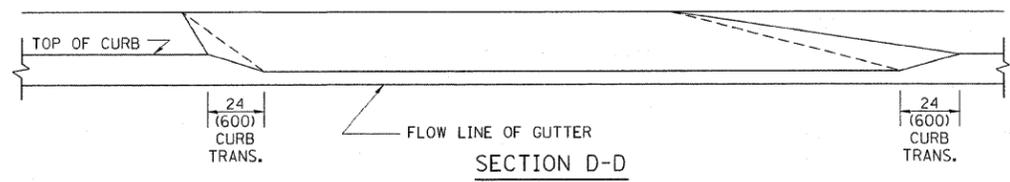
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

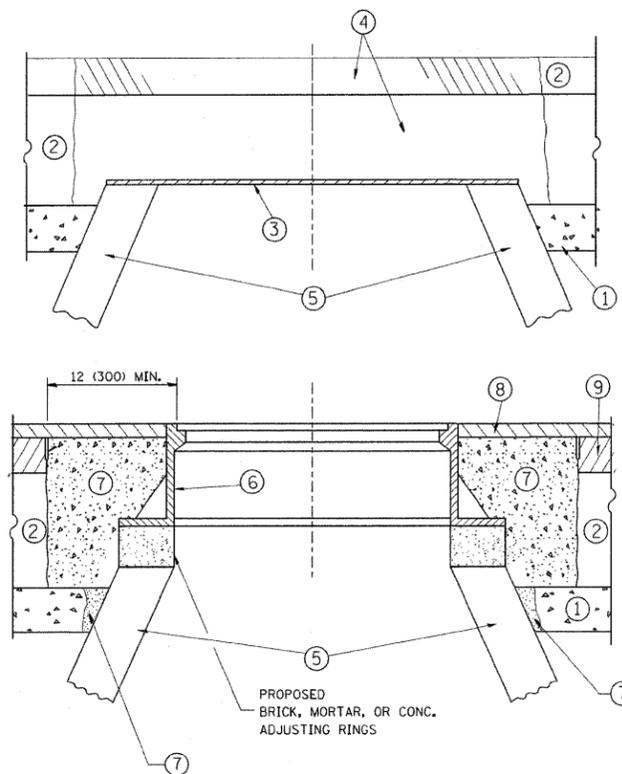
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = jeyse	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
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PLOT DATE = 10/28/2011		DATE - 11-06-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	72
BD400-02 (BD-02)		CONTRACT NO. 63557		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

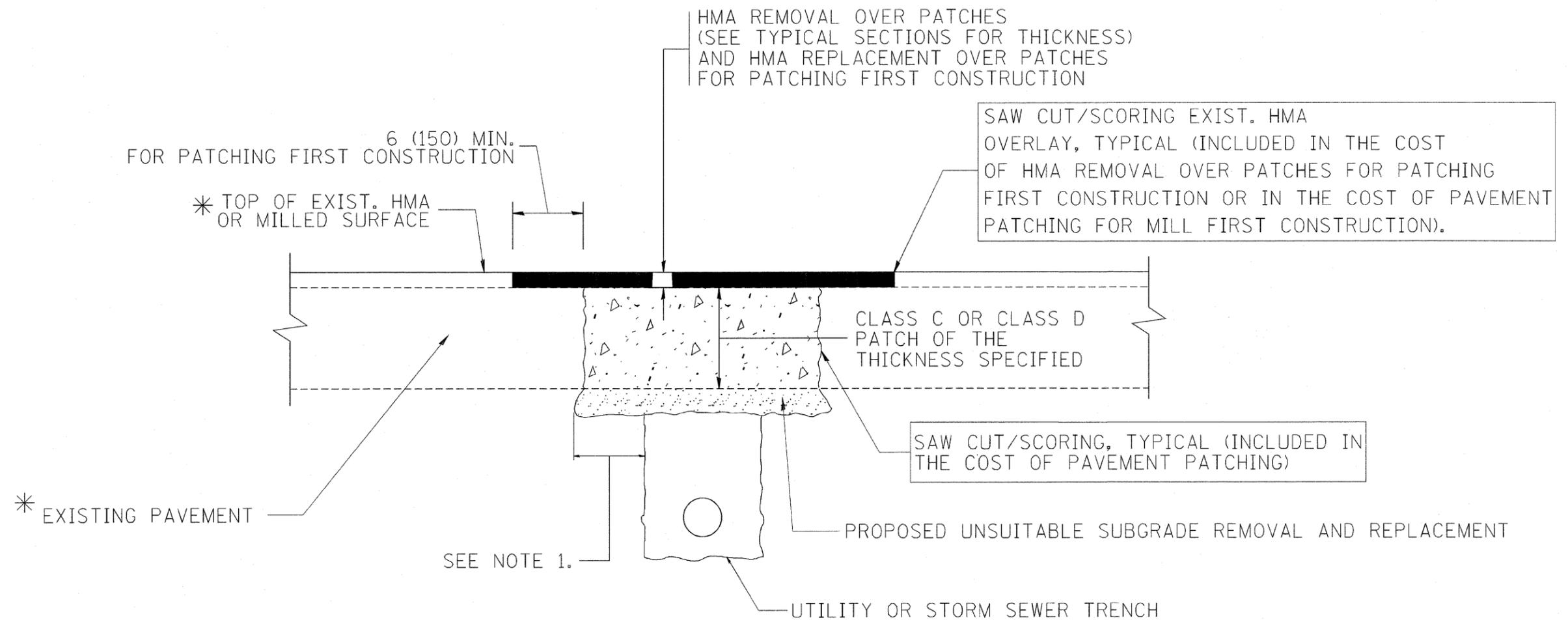
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	PLOT SCALE = 49.9999 ' / IN.	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 3/18/2011	DATE - 10-25-94	REVISED - R. BORO 03-09-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	73
BD600-03 (BD-8)			CONTRACT NO. 63557	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

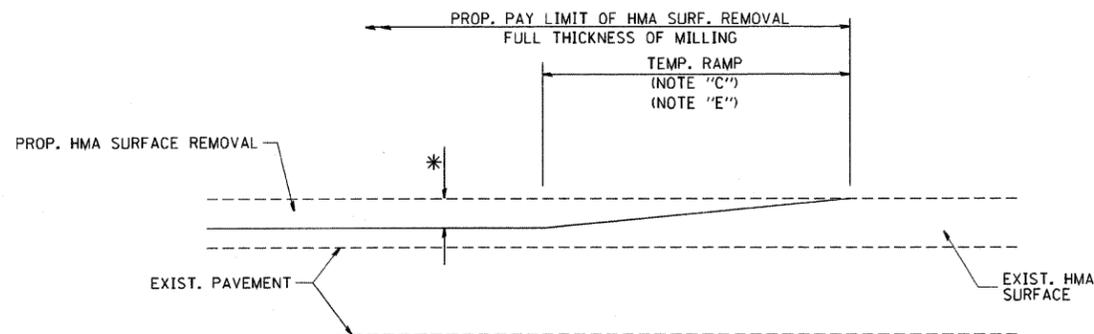
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

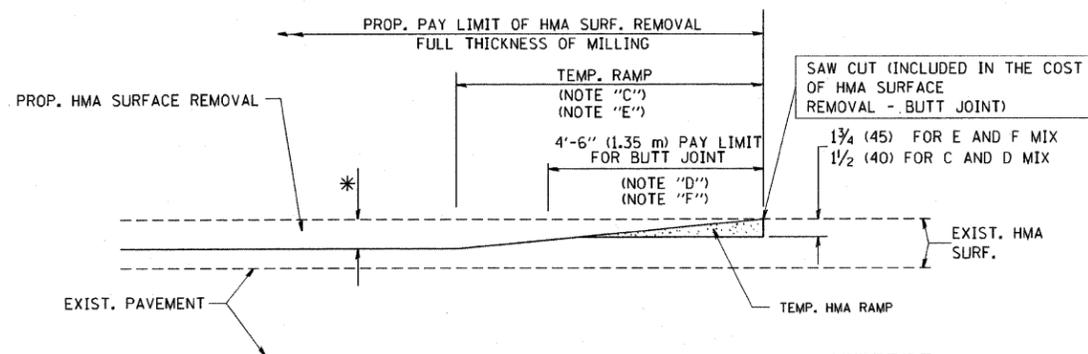
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT		F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	VAR 02-00055-00-WR	COOK	84	74
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07						BD400-04 (BD-22)		CONTRACT NO. 63557	
		DATE - 10-25-94	REVISED - K. ENG 10-27-08						<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)</small>			



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

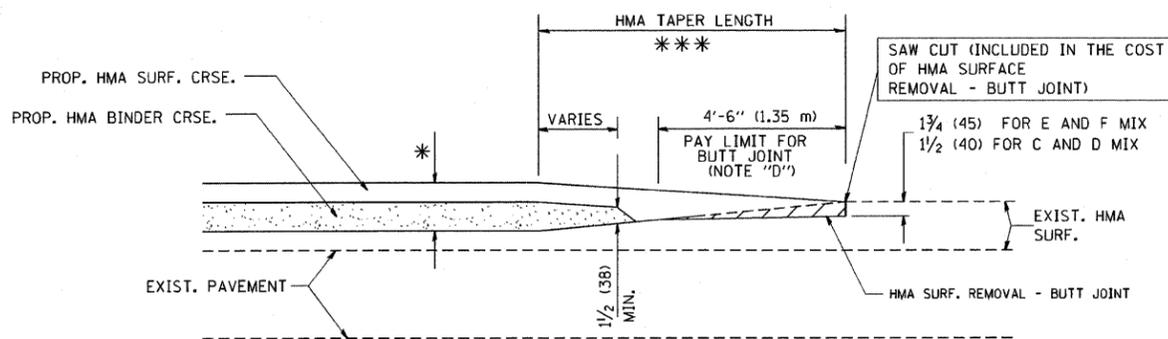
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

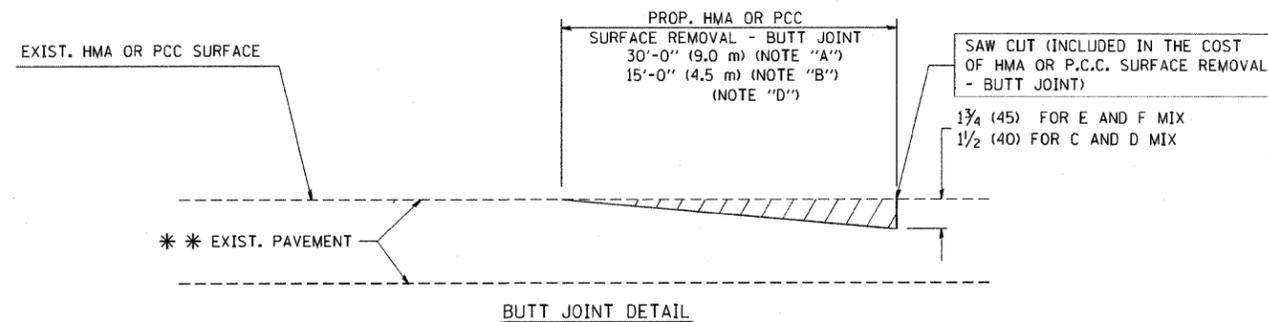
OPTION 2

TYPICAL TEMPORARY RAMP

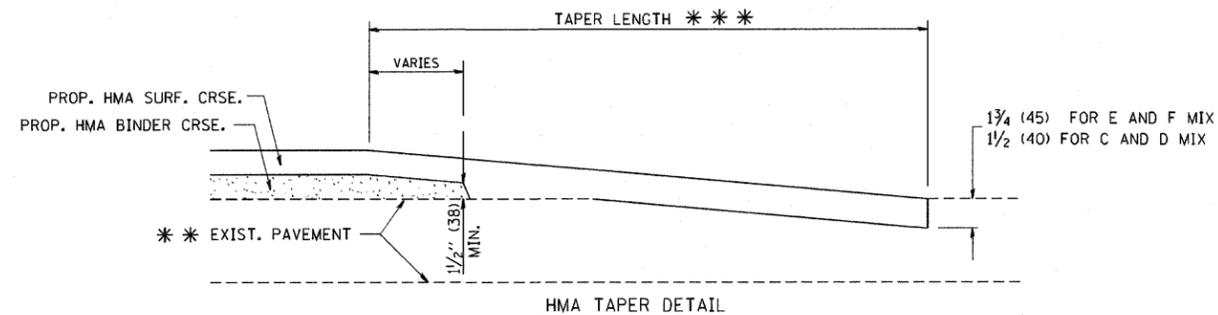


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

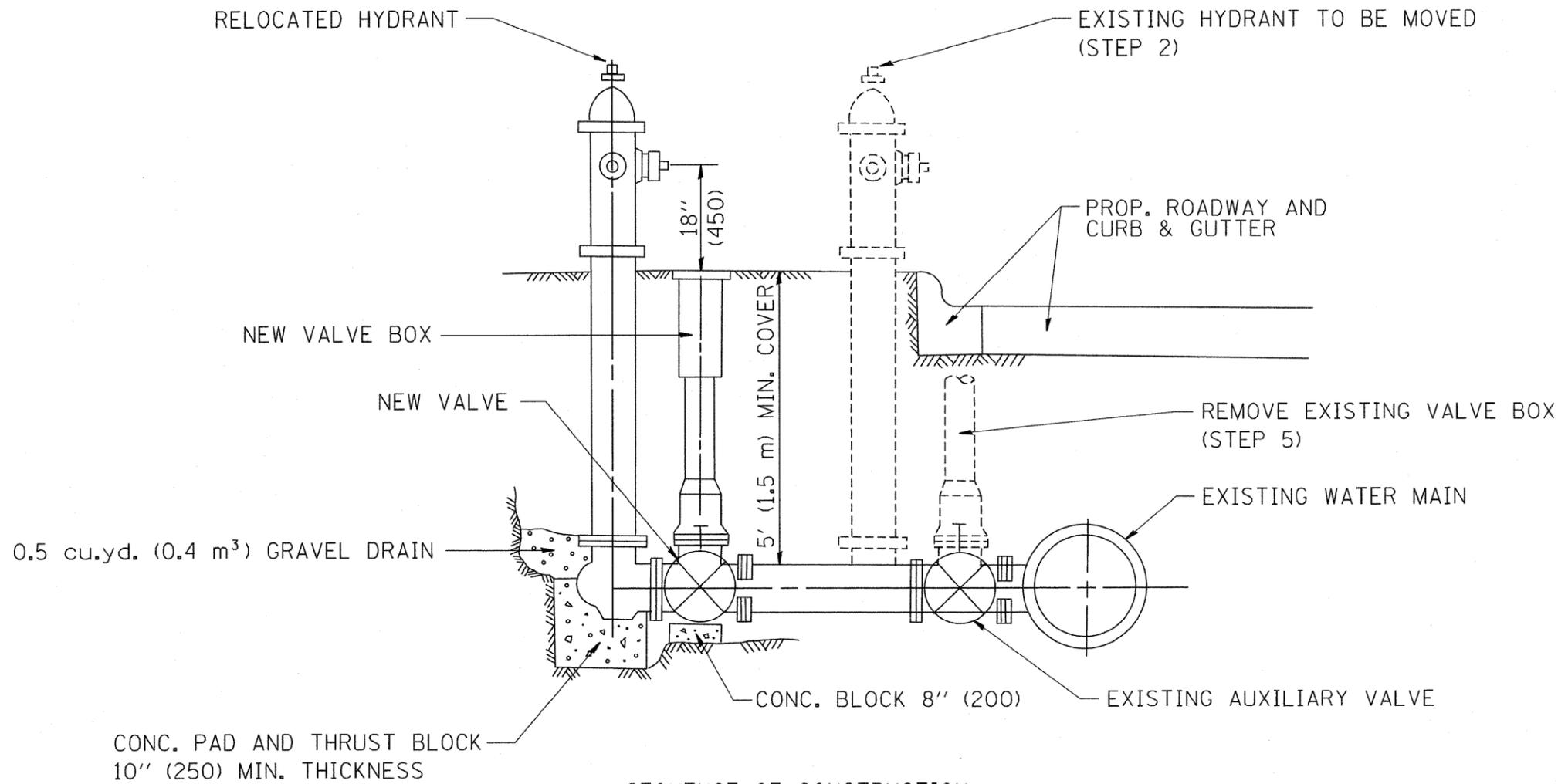
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	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	75
BD400-05 BD32			CONTRACT NO. 63557	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



SEQUENCE OF CONSTRUCTION:

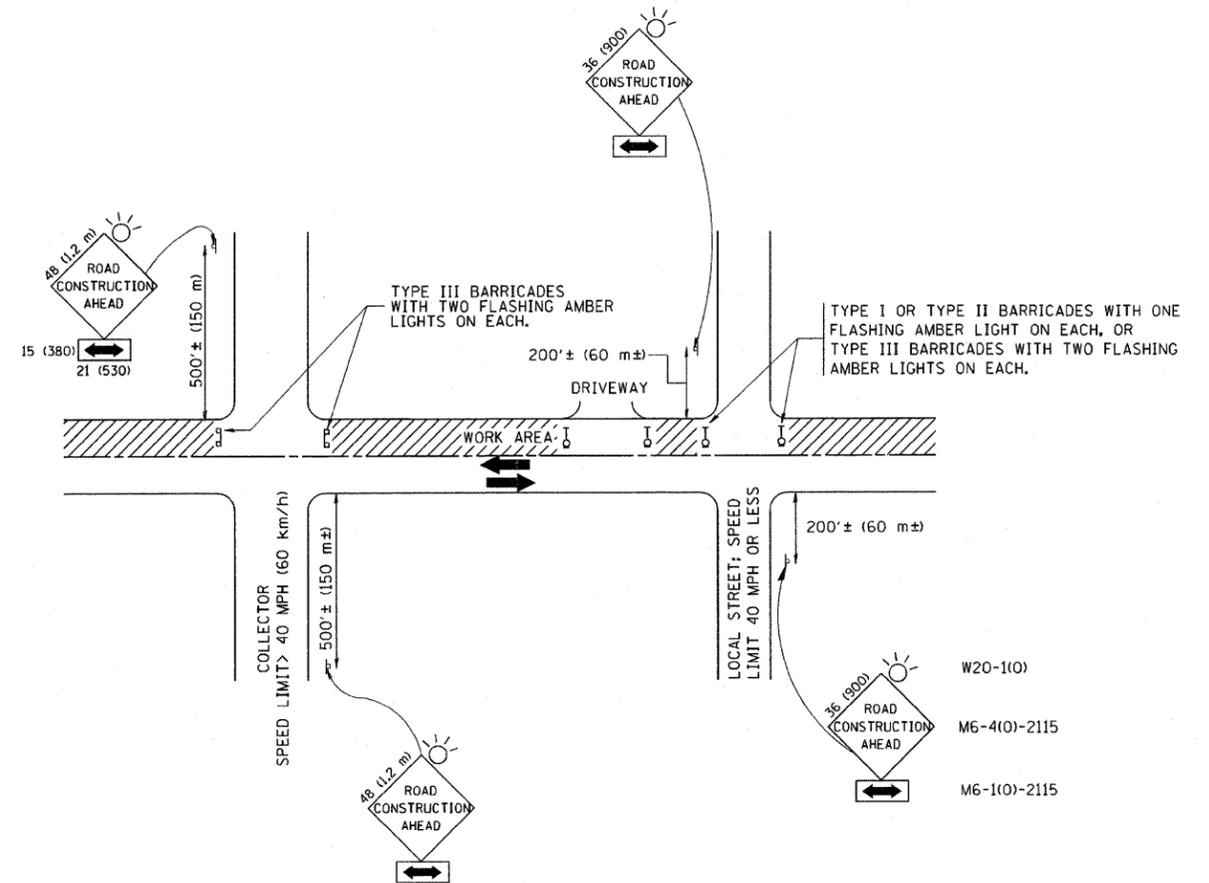
1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = w:\distatd\22x34\bd36.dgn	USER NAME = geglienobt	DESIGNED -	REVISED - R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRE HYDRANT TO BE MOVED			F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. SHAH 10-25-94					VAR.	02-00055-00-WR	COOK	84	76	
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	PLOT DATE = 1/4/2008	DATE -	REVISED -		BD-36 CONTRACT NO. 63557 FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT HPP-M-8003(504)								



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

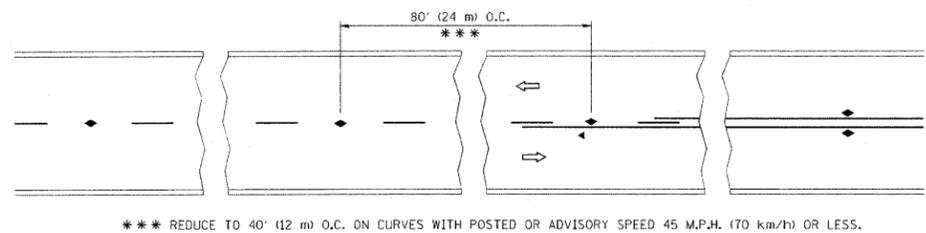
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

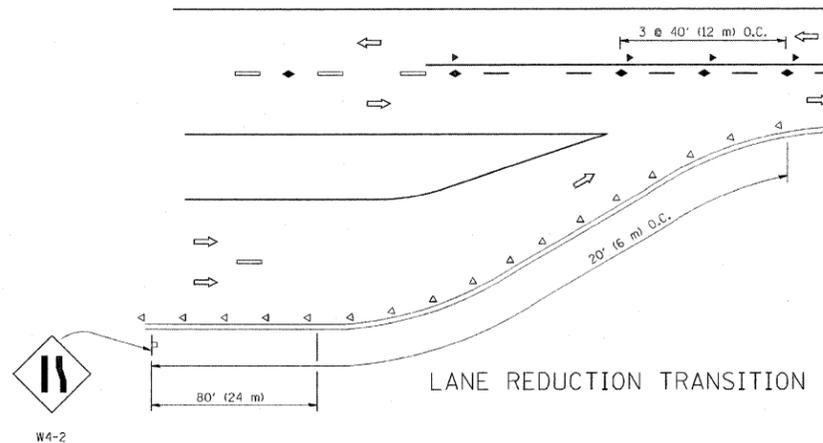
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

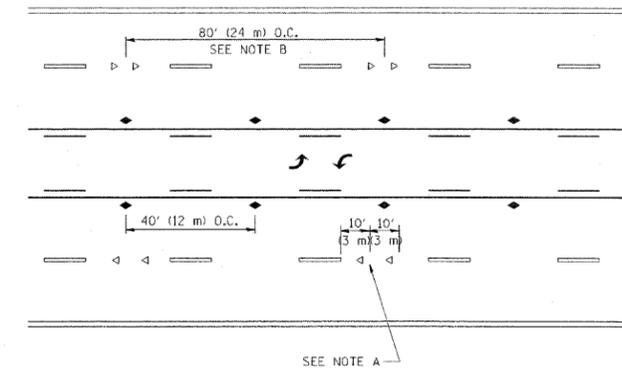
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TC-10			CONTRACT NO. 63557	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



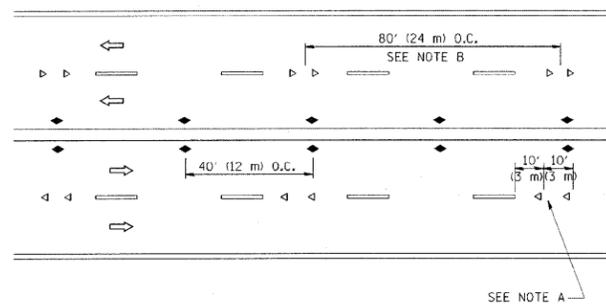
TWO-LANE/TWO-WAY



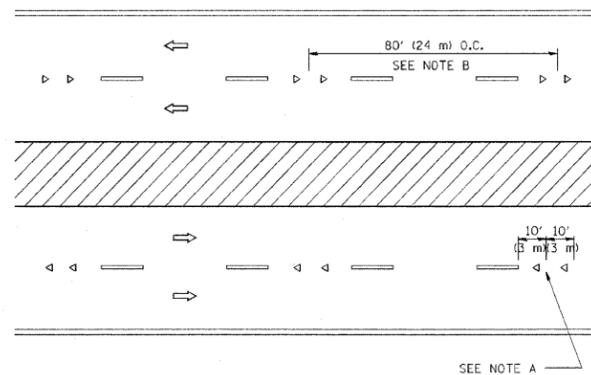
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

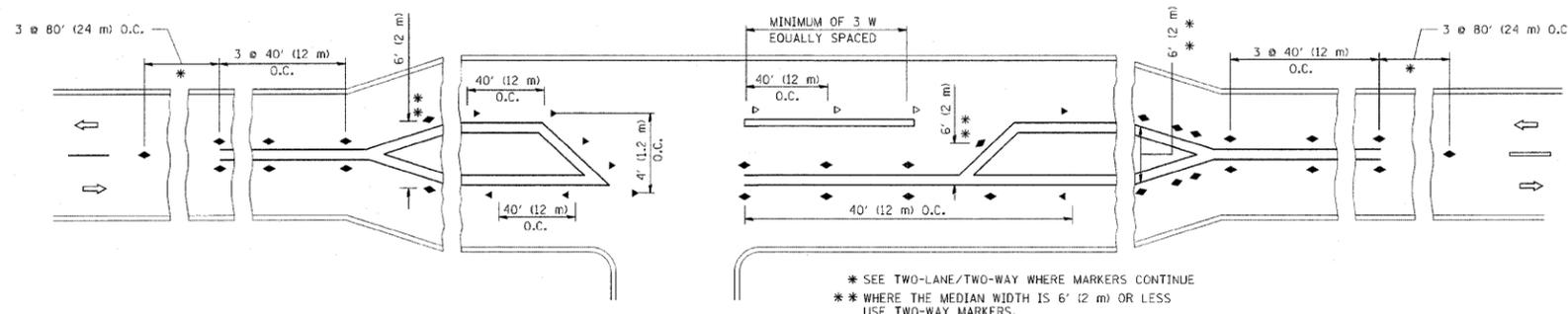
LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.



LEFT TURN

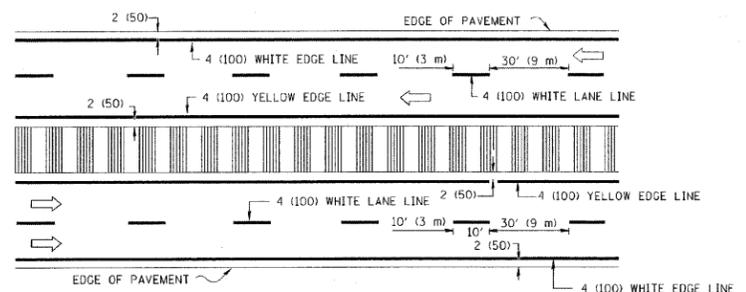
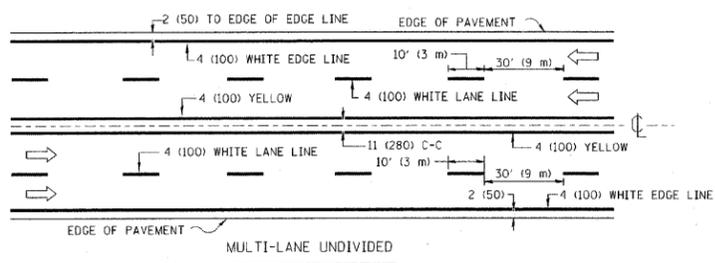
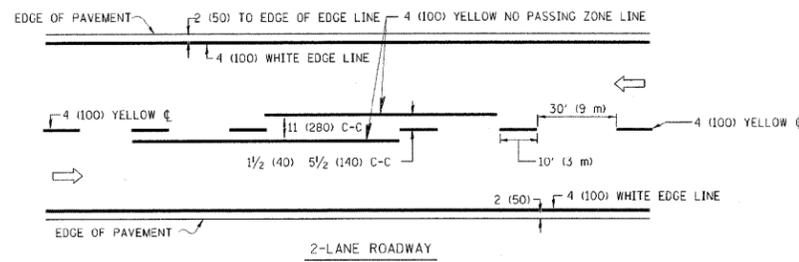
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

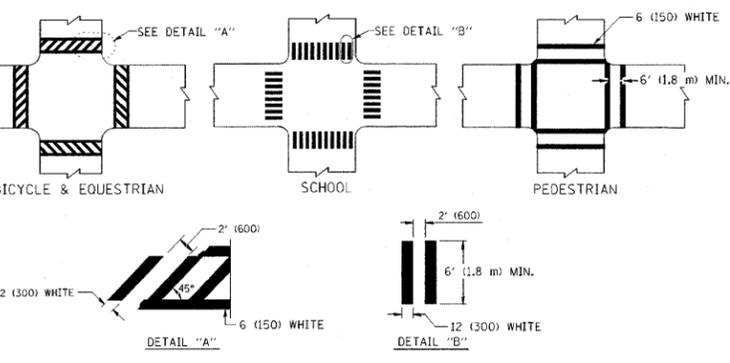
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

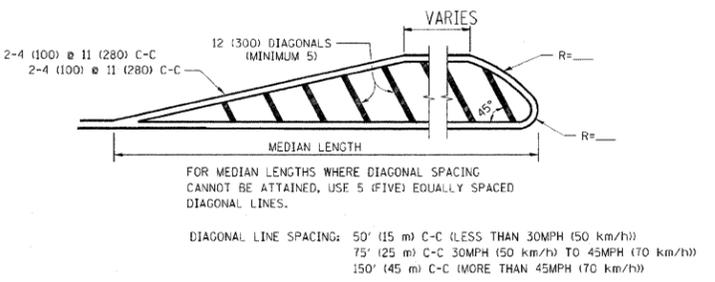
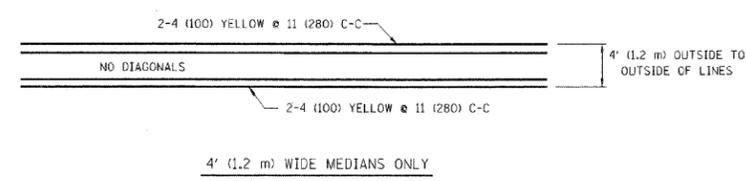
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	78
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



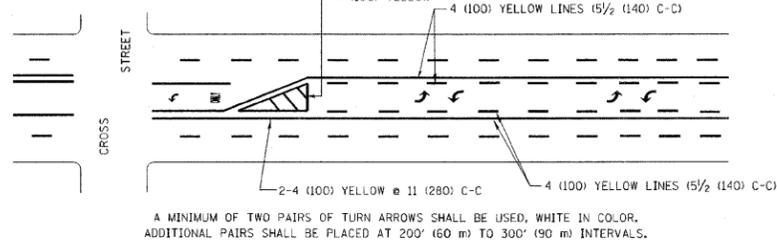
TYPICAL LANE AND EDGE LINE MARKING



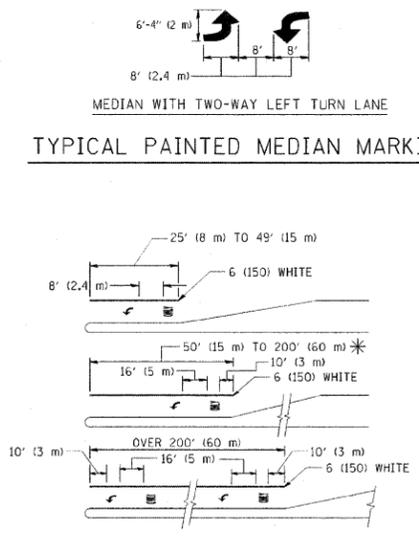
TYPICAL CROSSWALK MARKING



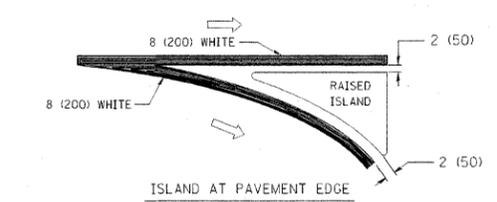
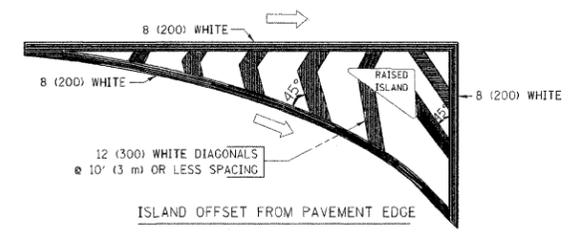
MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
ON FREEWAYS	5 (125)	SKIP-DASH	WHITE	
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4 m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS				
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

	LARGE SIZE	SMALL SIZE
THROUGH ARROW	1.07 (11.5)	0.60 (6.5)
LEFT OR RIGHT ARROW	1.47 (15.6)	0.60 (6.5)
COMBINATION LEFT (RIGHT) AND THROUGH ARROW	2.42 (26.0)	1.37 (14.7)
RAILROAD "R" 1.8m (6ft.)	0.33 (3.6)	—
RAILROAD "X" 6.1m (20ft.)	5.02(54.0)	—
HANDICAPPED SYMBOL	0.56 (6.0)	—

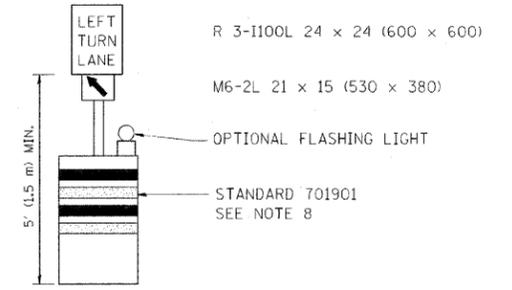
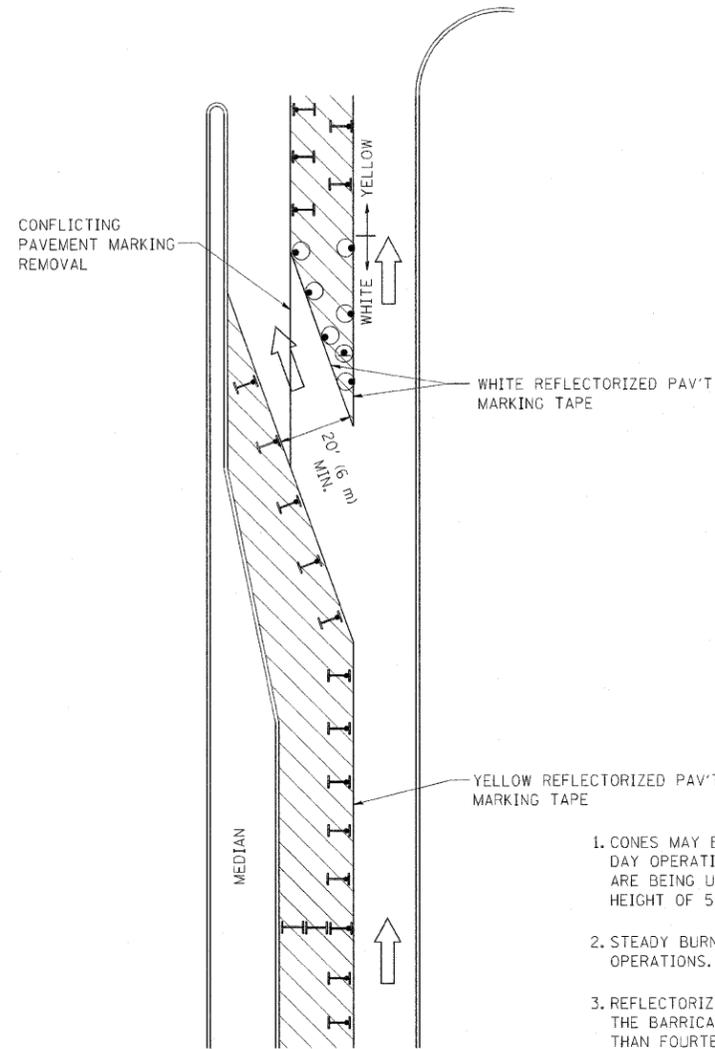
All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 50.000' / IN.	CHECKED -	REVISOR -	REVISOR -
PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISION -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	
TYPICAL PAVEMENT MARKINGS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	79
TC-13		CONTRACT NO. 63557		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

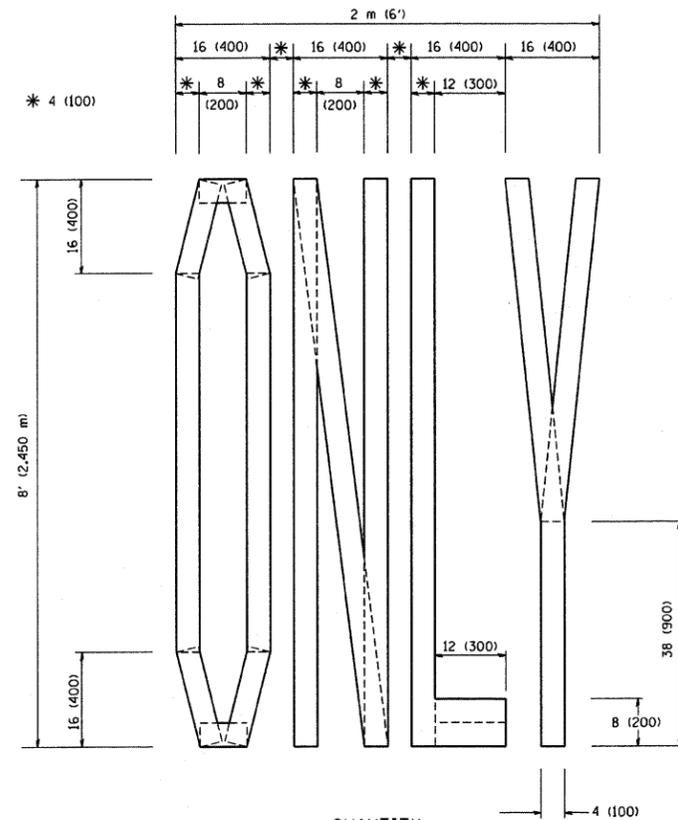
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		REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

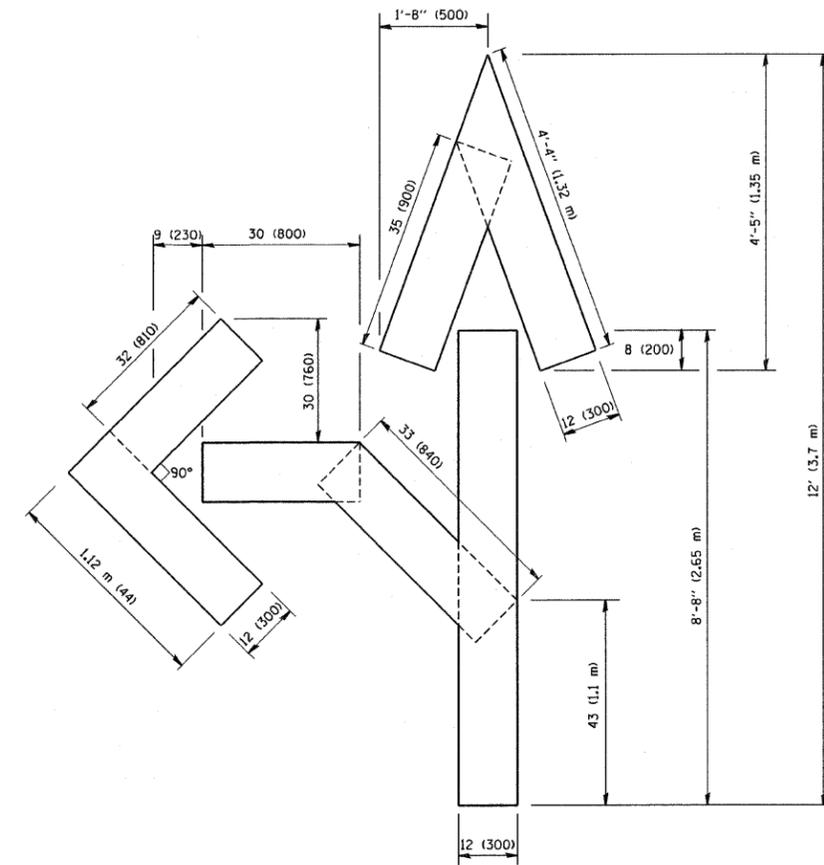
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

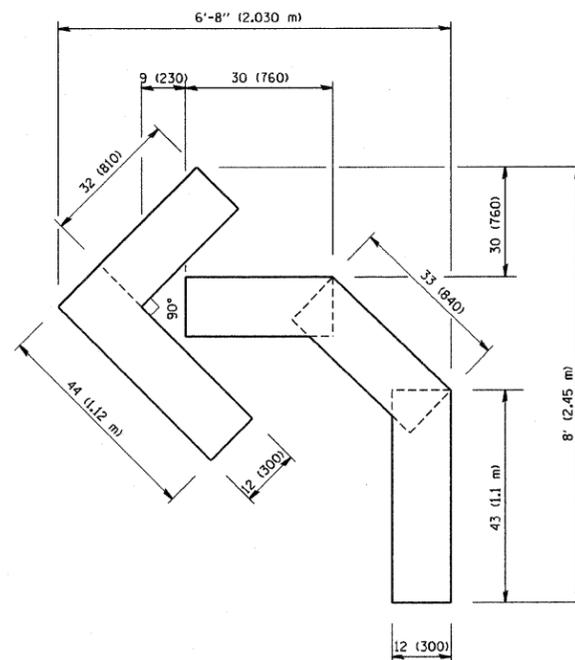
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	80
TC-14			CONTRACT NO. 63557	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tcl6.dgn	USER NAME = goglienobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

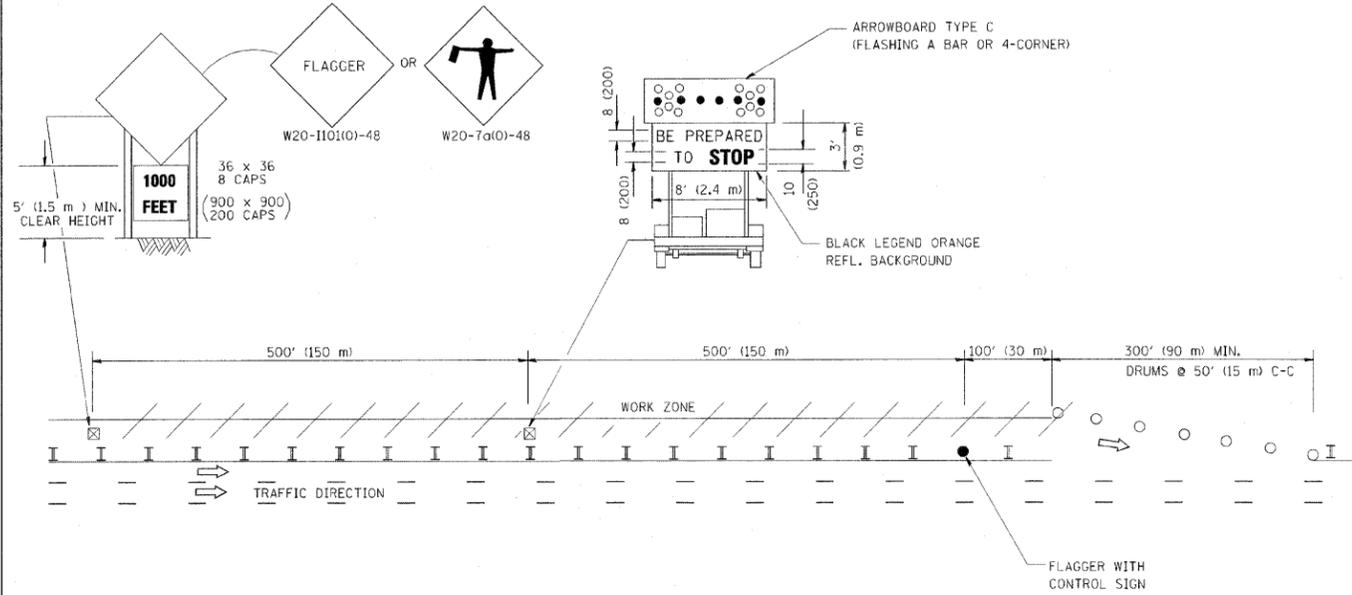
PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

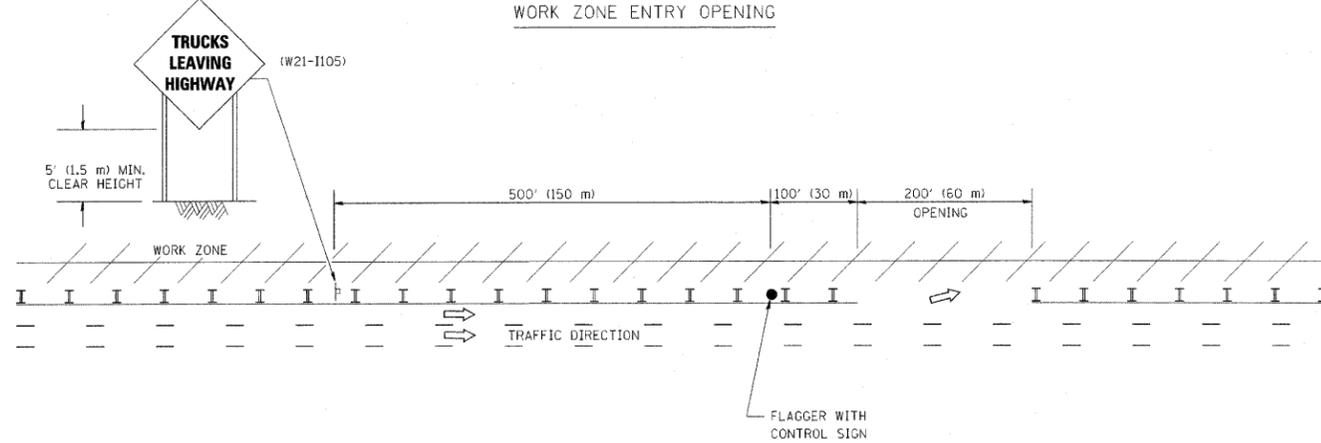
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	81
TC-16			CONTRACT NO. 63557	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\diststd\22x34\tc18.dgn	USER NAME = lqjso	DESIGNED -	REVISED - J.A.F. 04-03
		DRAWN -	REVISED - J.A.F. 02-06
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 1/26/2010	DATE -	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	82
TC-18			CONTRACT NO. 63557	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

RESERVED

FILE NAME = FAU 1587 (McCARTHY ROAD)
 FAP 577 (ARCHER AVENUE)
 FAU 1024 (DERBY ROAD)
 CHANNELIZATION AND
 TRAFFIC SIGNAL INSTALLATION

USER NAME =
 PLOT SCALE =
 PLOT DATE =

DESIGNED -- JLC
 DRAWN -- JFP
 CHECKED -- THK
 DATE -- 11/10

REVISED -- THK 9-21-11
 REVISED --
 REVISED --
 REVISED --

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

RESERVED

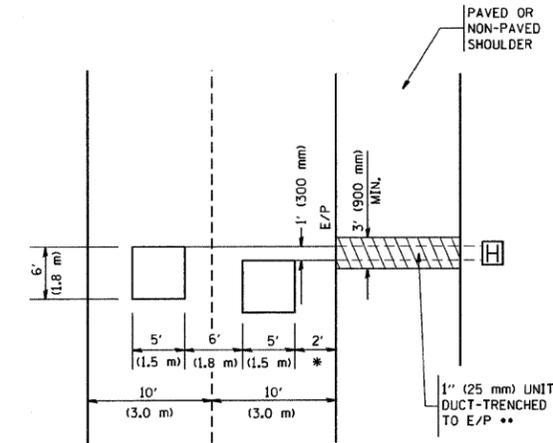
SCALE: 1"=10' SHEET NO. OF SHEETS STA. TO STA.

 **Frank Novotny & Associates, Inc.**
 825 Midway Drive • Willowbrook, IL • 60527 • Telephone: (630) 887-8640 • Fax: (630) 887-0182
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000928

F.A.U. F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	83
CONTRACT NO. 63557				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPP-M-8003(504)				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

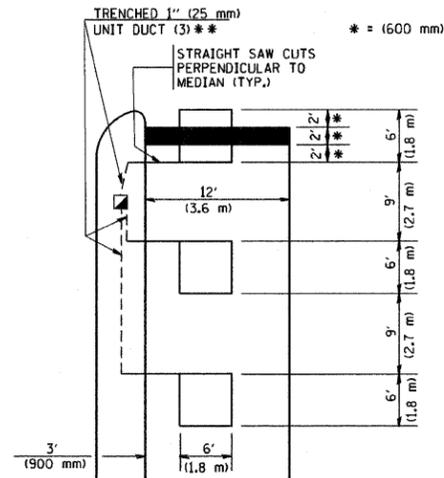


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

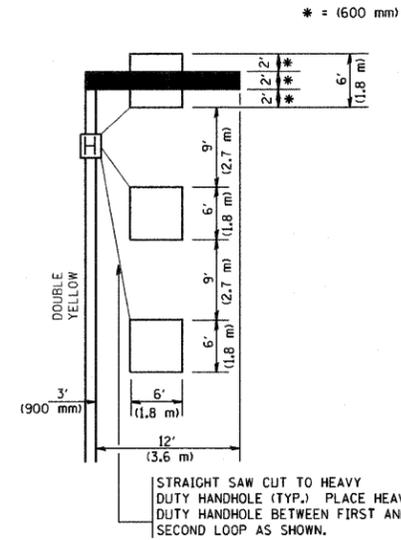
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

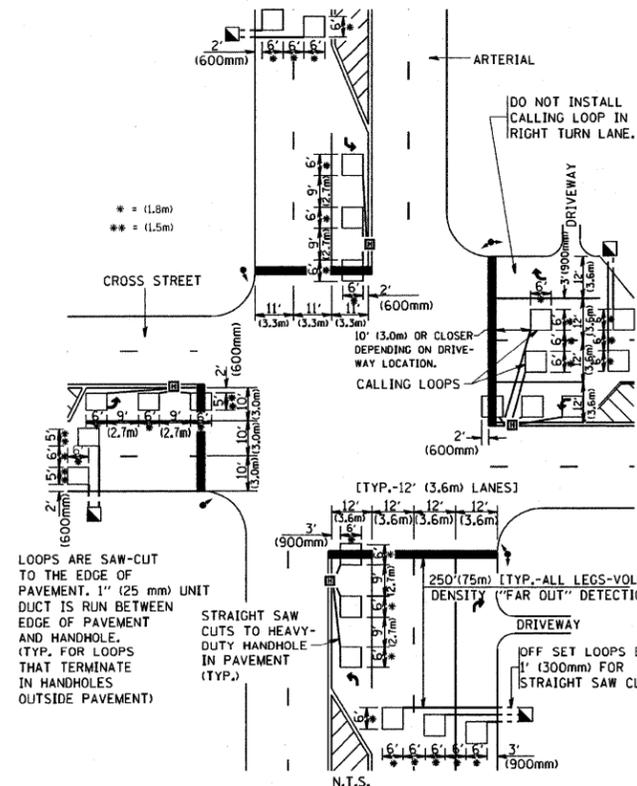
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



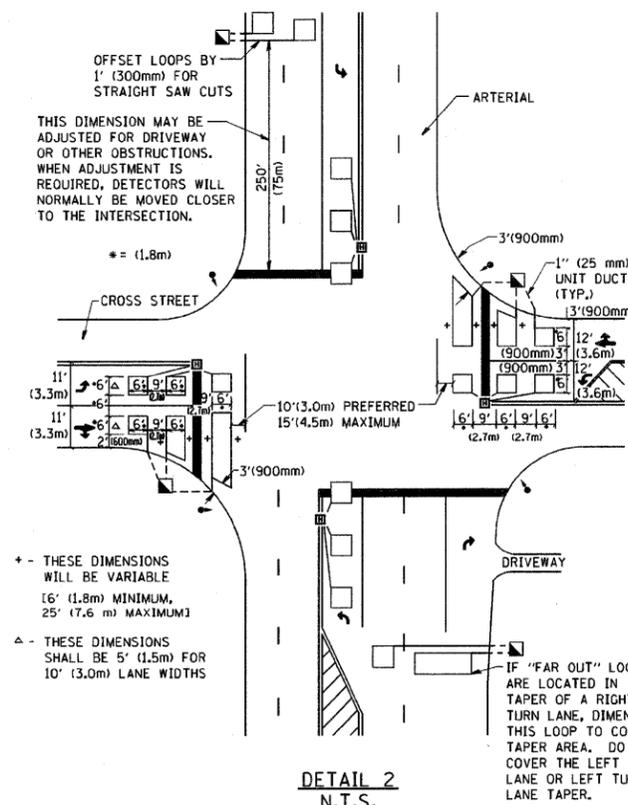
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\dststd\22x34\ts07.dgn

USER NAME = geglrenobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	02-00055-00-WR	COOK	84	84
TS-07			CONTRACT NO. 63557	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT HPP-M-8003(504)				