

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
FAU ROUTE 1411 (WASHINGTON BOULEVARD)
DES PLAINES RIVER TO LATHROP AVENUE
RESURFACING
SECTION NO. 11-00093-00-RS
PROJECT NO. M-9003(943)
VILLAGE OF RIVER FOREST
COOK COUNTY
JOB NO. C-91-214-12**

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 1411	11-00093-00-RS	COOK	14	1
ILLINOIS PROJECT M-9003(943)				
CONTRACT NO. 63686				



LOCATION OF SECTION INDICATED THUS:

TRAFFIC DATA

ADT (2011) = 5,800
POSTED SPEED LIMIT = 25 MPH
DESIGN SPEED LIMIT = 30 MPH

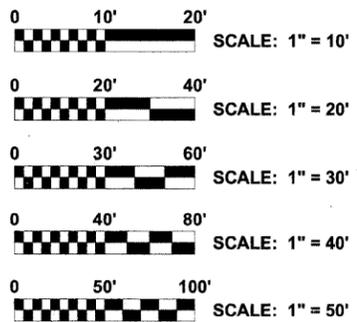
DESIGN DESIGNATION

COLLECTOR

PROJECT LOCATED IN THE VILLAGE OF RIVER FOREST

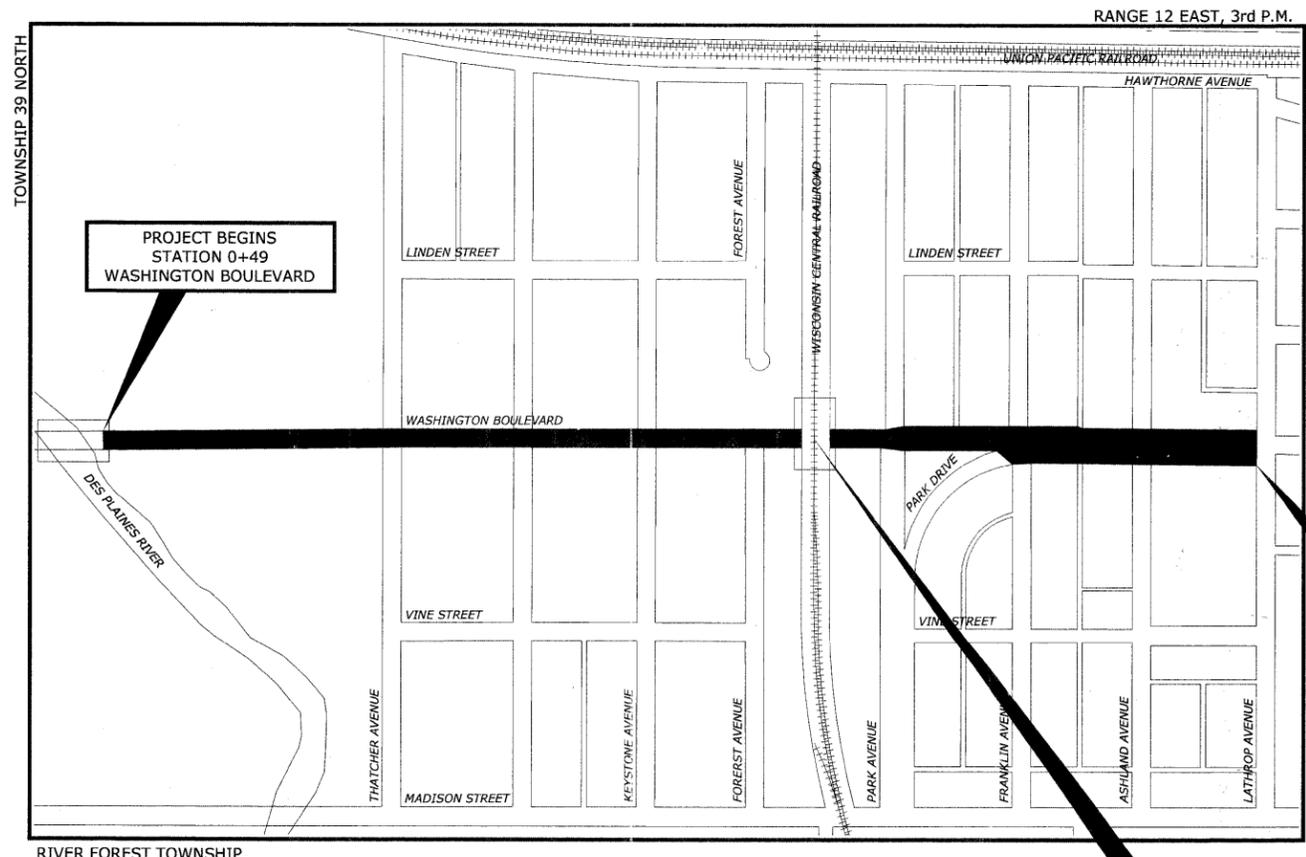
Call Before You Dig

JULIE
ILLINOIS
ONE-CALL SYSTEM
800-892-0123 OR 811



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CONTRACT NO. 63686



PROJECT ENDS STATION 42+10 WASHINGTON BOULEVARD

SN 016-3107 STATION 26+00

- AREA OF IMPROVEMENT
NOT TO SCALE
GROSS LENGTH OF IMPROVEMENT = 4,161 FT (0.79 MI.)
NET LENGTH OF IMPROVEMENT = 4,161 FT (0.79 MI.)

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

APPROVED January 25, 2012
John Rizzo
VILLAGE OF RIVER FOREST, PRESIDENT

PASSED FEBRUARY 10, 2012
Cheryl Chambers
DISTRICT ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW FEBRUARY 14, 2012
Debra M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS,
REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**



SIGNED: *My Sign*
DATE: 1-26-12 LICENSE EXPIRES: 11-30-13

EDWIN HANCOCK ENGINEERING COMPANY 9933
ROOSEVELT ROAD PHONE: (708) 865-0300
WESTCHESTER, ILLINOIS 60154

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E., (847) 705-4406, SCHAUMBURG, IL

Drawing file: W:\Projects\74011155 - Washington Blvd LAPP\Cover.dwg Jan 23, 2012 - 12:00pm

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET, LOCATION MAP
2	INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS, LEGEND OF SYMBOLS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4-5	EXISTING AND PROPOSED TYPICAL CROSS SECTIONS
6-8	WASHINGTON BOULEVARD PAVING PLAN
9	DETAILS
10	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD 08)
11	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD 22)
12	BUTT JOINT AND HMA TAPER DETAILS (BD 32)
13	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC 10)
14	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC 13)

I.D.O.T. STANDARD DRAWINGS

STANDARD NO.	TITLE OR DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-06	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016	MID BLOCK CURB RAMPS FOR SIDEWALKS
424021	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C&D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
701501-06	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701801-05	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
780001-03	TYPICAL PAVEMENT MARKINGS

LEGEND OF SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		MANHOLE
		INLET
		CATCH BASIN
		FIRE HYDRANT
		WATER MAIN VALVE VAULT
		POWER POLE
		STREET LIGHT
		RIM ELEVATION INVERT ELEVATION
	A	STRUCTURE TO BE ADJUSTED
	A*	STRUCTURE TO BE ADJUSTED (SPECIAL)
	1C	TYPE 1 FRAME AND CLOSED LID
	1P	TYPE 1 FRAME AND OPEN LID
	RC	STRUCTURE TO BE RECONSTRUCTED

STANDARDS

REFERENCES TO STANDARDS IN THE PLANS AND SPECIAL PROVISIONS SHALL BE THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE INDEX OF SHEETS. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2012, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2012, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2009 SIXTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS.

UNDERGROUND UTILITIES

BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED).

THE LOCATIONS OF THE UNDERGROUND UTILITIES, IF SHOWN ON THE PLANS, HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF RIVER FOREST, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT SUCH FACILITIES THAT MAY BE AFFECTED BY THE WORK. ALL DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE IN ACCORDANCE WITH ARTICLES 107.20 AND 107.31.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY OWNERS AND THE ENGINEER.

NOTIFICATION OF RESIDENTS

THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING WRITTEN NOTICE TO ALL AFFECTED RESIDENCES AND/OR PLACES OF BUSINESS AT LEAST ONE (1) WORKING DAY PRIOR TO PERFORMING ALL CONSTRUCTION ACTIVITY THAT WILL ELIMINATE ACCESS TO THEIR PROPERTY. THE WRITTEN NOTICE SHALL BE APPROVED BY THE ENGINEER AND A COPY PROVIDED TO THE VILLAGE PRIOR TO THE BEGINNING OF CONSTRUCTION.

MAINTENANCE OF TRAFFIC

WASHINGTON BOULEVARD WILL REMAIN OPEN TO THRU TRAFFIC DURING ALL OPERATIONS. FLAGGERS SHALL BE USED FOR THE INSTALLATION OF THE POLYMERIZED LEVELING BINDER, THE HOT-MIX ASPHALT SURFACE AND THERMOPLASTIC STRIPING.

FIELD OFFICE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.

BARRICADES

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED, ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.

OPEN EXCAVATIONS

THE CONTRACTOR SHALL NOT BE ALLOWED TO LEAVE EXCAVATION NECESSARY FOR PAVEMENT PATCHES OR STRUCTURE ADJUSTMENTS OPEN OVERNIGHT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETELY BACKFILLING OR INSTALLING A PLATE OVER ALL EXCAVATIONS AT THE END OF EACH DAY IN ACCORDANCE WITH ARTICLE 107.09.

CONCRETE BREAKERS

WHEN REMOVING PAVEMENT AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS SUCH AS DROP HAMMERS, WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES, WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE COST OF ITEM BEING REMOVED.

PAVEMENT PATCHING

LOCATIONS OF PATCHES WILL BE DETERMINED IN FIELD BY ENGINEER AFTER MILLING OPERATIONS.

CURING AND PROTECTION

AFTER THE CONCRETE HAS BEEN FINISHED AND THE WATER SHEEN HAS DISAPPEARED FROM THE SURFACE OF THE CONCRETE, THE SURFACE SHALL BE SEALED WITH MEMBRANE CURING COMPOUND OF A TYPE APPROVED BY THE ENGINEER. THE SEAL SHALL BE MAINTAINED FOR THE SPECIFIED CURING PERIOD. THE EDGES OF THE CONCRETE SHALL ALSO BE SEALED IMMEDIATELY AFTER THE FORMS ARE REMOVED. IN ADDITION, ALL CONCRETE PLACED DURING PERIODS OF COLD WEATHER SHALL BE PROTECTED IN ACCORDANCE WITH ARTICLE 1020.13 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

THE WORK SHALL BE UNDER THE CHARGE AND CARE OF THE CONTRACTOR UNTIL FINAL ACCEPTANCE BY THE DEPARTMENT. THE CONTRACTOR SHALL ASSUME ALL RESPONSIBILITY FOR INJURY OR DAMAGE TO THE WORK FROM ANY CAUSE WHATSOEVER AND HE SHALL REBUILD, REPAIR OR RESTORE THE DAMAGED WORK AT HIS OWN EXPENSE.

THIS ITEM WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS CONCRETE ITEMS IN THE CONTRACT.

RAILROAD FLAGMEN

IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE WISCONSIN CENTRAL RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE WISCONSIN CENTRAL RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.

GENERAL NOTES

MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

WHEN LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS AND DRAINAGE STRUCTURE OBSTRUCTING THE NATURAL FLOW OF WATER, THE LOOSE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS CAUSED BY CONSTRUCTION OPERATIONS.

MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS/HER OPERATIONS TO MAINTAIN FLOW AT ALL TIMES THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE/SHE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES THAT ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS/HER EXPENSE.

FRAMES AND LIDS

THE LOCATION AND TYPE OF FRAMES AND LIDS REQUIRED FOR ALL MANHOLES AND VALVE VAULTS LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.04 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE. ALL LIDS ON SANITARY MANHOLES, COMBINED SEWER MANHOLES, AND VALVE VAULTS SHALL BE OF THE SELF SEALING TYPE.

THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS THAT ARE TO BE ABANDONED ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF RIVER FOREST. THE CONTRACTOR IS TO DELIVER FRAMES AND LIDS TO THE VILLAGE OF RIVER FOREST PUBLIC WORKS YARD LOCATED AT 45 FOREST AVENUE. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF EACH BID ITEM.

FRAMES AND LIDS, TYPE 1

ALL COMBINED SEWER CLOSED LIDS SHALL BE SELF-SEALING WITH RECESSED PICK HOLES. ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR CONSTRUCTION, ADJUSTMENT, OR RECONSTRUCTION OF ANY MANHOLE OR VALVE VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS:

STRUCTURE TYPE	NEENAH FOUNDRY* FRAME AND LID	SELF SEALING	WORD CAST INTO LID
SANITARY MANHOLE	R-1713-B	YES	SANITARY
COMBINATION MANHOLE	R-1713-B	YES	SANITARY
STORM MANHOLE	R-1713-B	NO	STORM
VALVE VAULT	R-1713-B	YES	WATER
CATCH BASIN	R-2504-D	NO	-----
INLET	R-2504-D	NO	-----

* NOTE: OR ENGINEER APPROVED EQUAL.

PRECAST CONCRETE SECTIONS FOR STRUCTURES

ALL NEW STRUCTURES AND STRUCTURES TO BE RECONSTRUCTED SHALL BE BUILT WITH PRECAST REINFORCED CONCRETE SECTIONS FABRICATED IN ACCORDANCE WITH A.S.T.M. DESIGNATION C-478.

CONNECTING EXISTING PIPES TO STRUCTURES

ANY PIPE, UP TO FOUR FEET (4') IN LENGTH PER EACH PIPE AND THE NECESSARY TRENCH BACKFILL, USED TO CONNECT EXISTING PIPES TO THE STRUCTURE TO BE RECONSTRUCTED OR TO THE NEW STRUCTURE SHALL BE INCLUDED IN THE COST OF THE STRUCTURE TO BE RECONSTRUCTED OR THE NEW STRUCTURE.

CONNECTION OF DISSIMILAR PIPE MATERIALS

NON-SHEAR CONNECTORS SHALL BE USED WHERE NEW SEWER PIPES ARE TO BE CONNECTED TO EXISTING SEWER PIPES. THE NON-SHEAR CONNECTORS SHALL CONSIST OF A SYNTHETIC RUBBER SLEEVE, WHICH WILL RESIST DETERIORATION CAUSED BY WASTES FOUND IN SEWERS AND TWO STAINLESS STEEL STRAPS. THE CONNECTORS SHALL BE "MISSION" COUPLINGS, "FERNO" COUPLINGS, OR ENGINEER APPROVED EQUAL. ALL LABOR AND MATERIAL REQUIRED TO CONNECT NEW SEWER PIPE TO EXISTING SEWER PIPE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

FORMS FOR CONCRETE SIDEWALKS, DRIVEWAYS AND GUTTER FLAGS

THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 424.05, 423.05, AND 1103.05 EXCEPT THAT A 2 x 6 BOARD WILL BE USED AS A FORM FOR ALL SIDEWALKS TO BE INSTALLED FIVE INCHES (5") IN THICKNESS. A 2 x 8 BOARD WILL BE USED AS A FORM FOR ALL DRIVEWAYS TO BE INSTALLED SEVEN INCHES (7") IN THICKNESS. A 2 x 10 BOARD WILL BE USED AS A FORM FOR ALL DRIVEWAYS TO BE INSTALLED EIGHT INCHES (8") IN THICKNESS. A 2 x 12 BOARD WILL BE USED AS A FORM FOR ALL GUTTER FLAGS TO BE INSTALLED TEN INCHES (10") IN THICKNESS. ALL FORMS MUST BE OF A MINIMUM HEIGHT OF THE PROPOSED THICKNESS OF THE RESPECTIVE CONCRETE ITEMS TO BE INSTALLED.

AGGREGATE BEDDING FOR CONCRETE WORK

NEW SIDEWALK AND DRIVEWAY PAVEMENT SHALL BE PLACED ON A MINIMUM OF TWO INCHES (2") OF COMPACTED CA-6 STONE BEDDING. 8" DRIVEWAY PAVEMENT SHALL BE PLACED ON A MINIMUM OF FOUR INCHES (4") OF COMPACTED CA-6 STONE BEDDING. ADDITIONAL AGGREGATE REQUIRED TO ADJUST THE EXISTING ELEVATION OF THE SUBGRADE TO THE PROPOSED ELEVATION WILL BE INCLUDED AS PART OF THIS WORK.

THIS ITEM WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE FOR THE RESPECTIVE CONCRETE ITEMS IN THE CONTRACT.

BUTT JOINTS

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER" DETAIL SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

PROTECTIVE COAT

ALL NEW CONCRETE SURFACES SHALL BE PROTECTIVELY COATED WITH A BOILED LINSEED OIL MIXTURE REGARDLESS OF THE TIME OF YEAR THE CONCRETE WORK IS CONSTRUCTED. THE PREPARATION AND APPLICATION OF THIS MIXTURE SHALL BE IN ACCORDANCE WITH ARTICLE 420.18 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

<p>HANCOCK ENGINEERING Civil Engineers Municipal Consultants Established 1911</p>	USER NAME -	DESIGNED - ALA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS, LEGEND OF SYMBOLS, AND GENERAL NOTES	ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE -	DRAWN - MK, DMM	REVISED -			FAU 1411	11-00093-00-RS	COOK	14	2
	PLOT DATE -	CHECKED - ALA	REVISED -			FIELD BOOK NO. -JAERIALS/ILL	CONTRACT NO. 63686			
		DATE - 12-7-11	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS	FED. AID PROJECT M-9003(943)			

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

E.H.E. PROJECT NO. 740-11-15501

SUMMARY OF QUANTITIES

CODE	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE RESURFACING 0005 80% FEDERAL 20% LOCAL
20200100	EARTH EXCAVATION	CU YD	10	10
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	260	260
25200100	SODDING	SQ YD	260	260
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	4,100	4,100
40600300	AGGREGATE (PRIME COAT)	TON	85	85
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1/4 4.75, N50	TON	1,140	1,140
40600895	CONSTRUCTION TEST STRIP	EACH	2	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	310	310
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	2,300	2,300
42101300	PROTECTIVE COAT	SQ YD	430	430
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	40	40
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	1,300	1,300
42400800	DETECTABLE WARNINGS	SQ FT	258	258
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	20,400	20,400
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	40	40
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,100	1,100
44000600	SIDEWALK REMOVAL	SQ FT	1,300	1,300
44201785	CLASS D PATCHES, TYPE I, 12 INCH	SQ YD	205	205
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	410	410

SUMMARY OF QUANTITIES

CODE	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE RESURFACING 0005 80% FEDERAL 20% LOCAL
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	610	610
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	815	815
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	3	3
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	8	8
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	43	43
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	15	15
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	15	15
60603800	COMBINATION CURB AND GUTTER TYPE B-6.12	FOOT	1,100	1,100
67100100	MOBILIZATION	L SUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	5,550	5,550
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	420	420
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11,900	11,900
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	180	180
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	150	150
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,225	1,225
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	345	345
X6026624	VALVE BOXES TO BE ADJUSTED (SPECIAL)	EACH	1	1
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	24	24
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L. Sum	1	1

* DENOTES SPECIALTY ITEM

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USER NAME -	DESIGNED - ALA	REVISED -
PLOT SCALE -	DRAWN - MK, DMM	REVISED -
PLOT DATE -	CHECKED - ALA	REVISED -
	DATE - 12-7-11	REVISED -

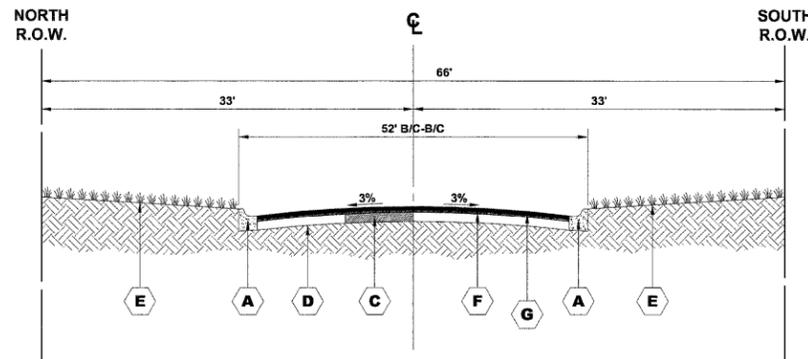
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

ROUTE NO. FAU 1411	SECTION 11-0093-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 3
FIELD BOOK NO. -AERIALS/LL		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-9003(943)		

E.H.E. PROJECT NO. 740-11-15501



SYMBOL	DESCRIPTION
A	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
C	PROPOSED CLASS D PATCH
D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE
E	EXISTING LANDSCAPED PARKWAY
F	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
G	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"

PROPOSED HOT-MIX ASPHALT SURFACE COURSE SHALL BE 1/4" ABOVE GUTTER FLAG.

PROPOSED TYPICAL SECTION
STA. 0+49 TO STA. 11+00

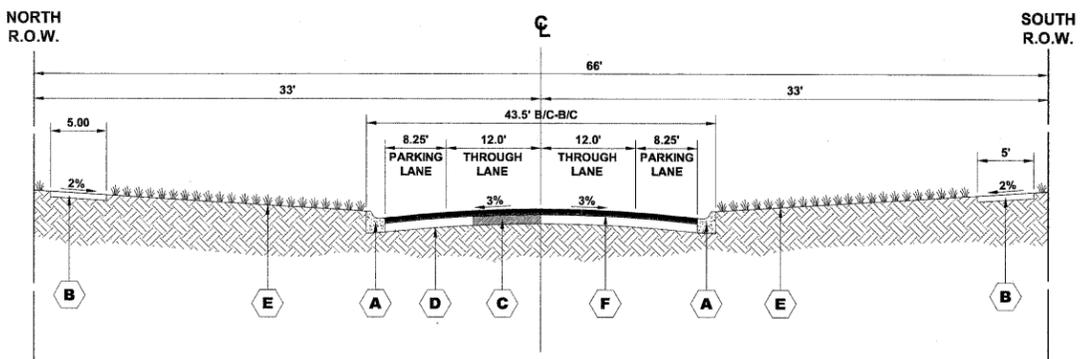
HOT-MIX ASPHALT (HMA) MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, IL 9.5 mm, MIX "D", N50, 2"	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50, 1"	3.5% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HOT-MIX ASPHALT BINDER COURSE, IL - 19.0 mm), 12" (IN 3 LIFTS)	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

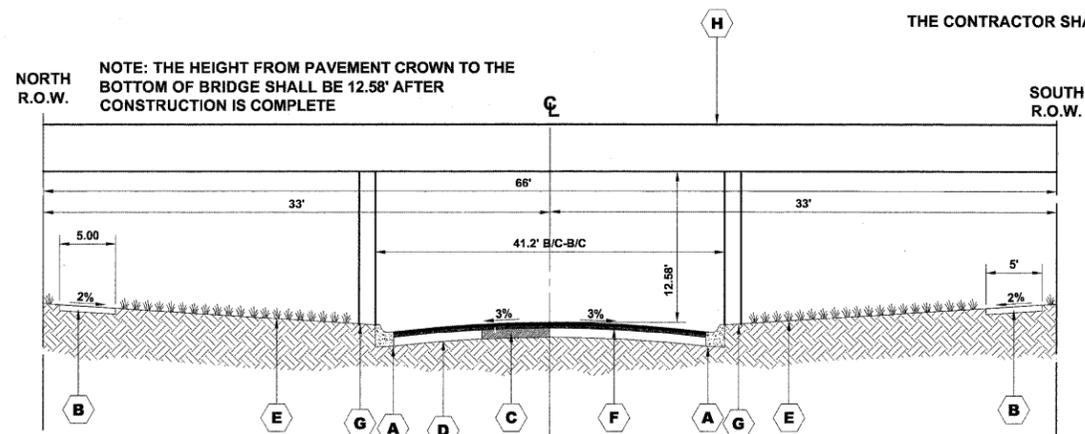
THE CONTRACTOR SHALL MILL BEFORE PATCHING.



SYMBOL	DESCRIPTION
A	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
C	PROPOSED CLASS D PATCH
D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE
E	EXISTING LANDSCAPED PARKWAY
F	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
G	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"

PROPOSED HOT-MIX ASPHALT SURFACE COURSE SHALL BE 1/4" ABOVE GUTTER FLAG.

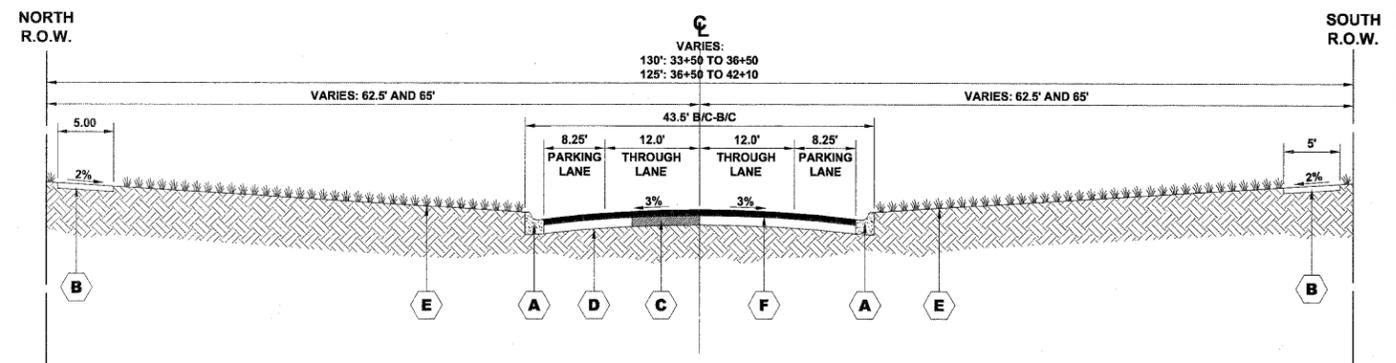
PROPOSED TYPICAL SECTION
STA. 11+00 TO STA. 26+06
STA. 26+68 TO STA. 33+50



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	E	EXISTING LANDSCAPED PARKWAY
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	F	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
C	PROPOSED CLASS D PATCH	G	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE	H	WISCONSIN CENTRAL RAILROAD BRIDGE

PROPOSED HOT-MIX ASPHALT SURFACE COURSE SHALL BE 1/4" ABOVE GUTTER FLAG.

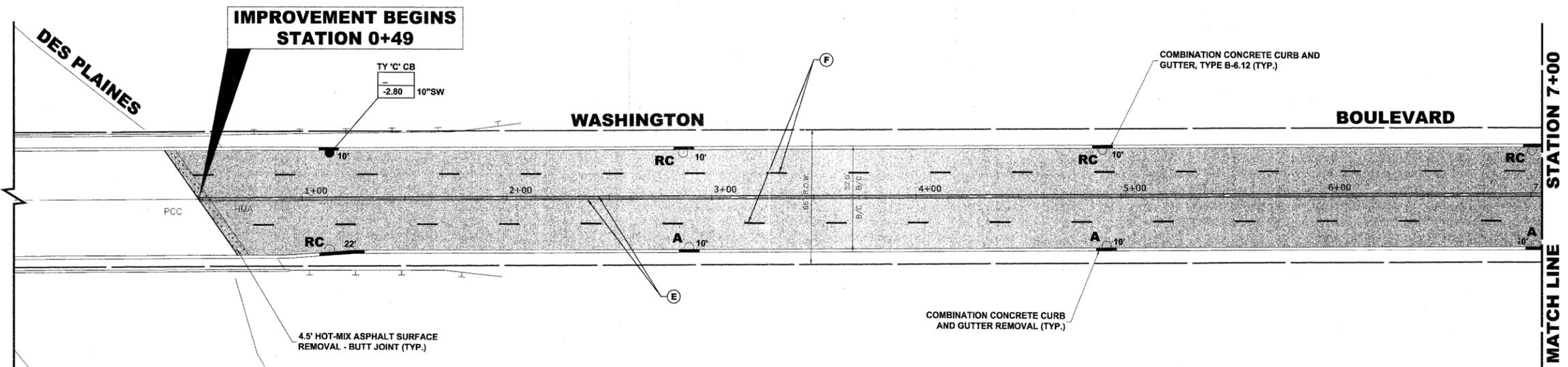
PROPOSED TYPICAL SECTION
(WISCONSIN CENTRAL RAILROAD BRIDGE)
STA. 26+06 TO STA. 26+68



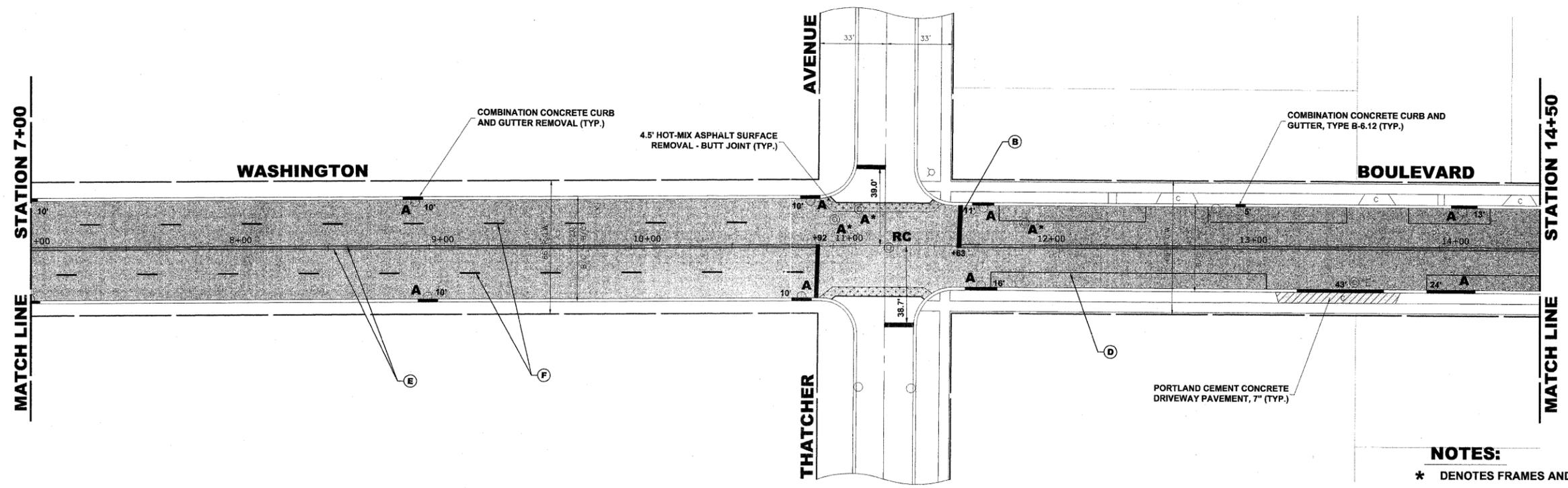
SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	E	EXISTING LANDSCAPED PARKWAY
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	F	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
C	PROPOSED CLASS D PATCH	G	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE		PROPOSED HOT-MIX ASPHALT SURFACE COURSE SHALL BE 1/4" ABOVE GUTTER FLAG.

PROPOSED TYPICAL SECTION
STA. 33+50 TO STA. 42+10

Drawing file: W:\Projects\74011155 - Washington Blvd LAPP\Washington Blvd LAPP.dwg Jan 17, 2012 10:08am



PAVEMENT MARKINGS LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSSWALK, WHITE	(A)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 12", SCHOOL CROSSWALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PARKING LANE LINE, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PAVEMENT CENTERLINE DOUBLE @ 11" C-C, YELLOW	(E)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", SKIP DASH, WHITE	(F)



NOTES:
 * DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

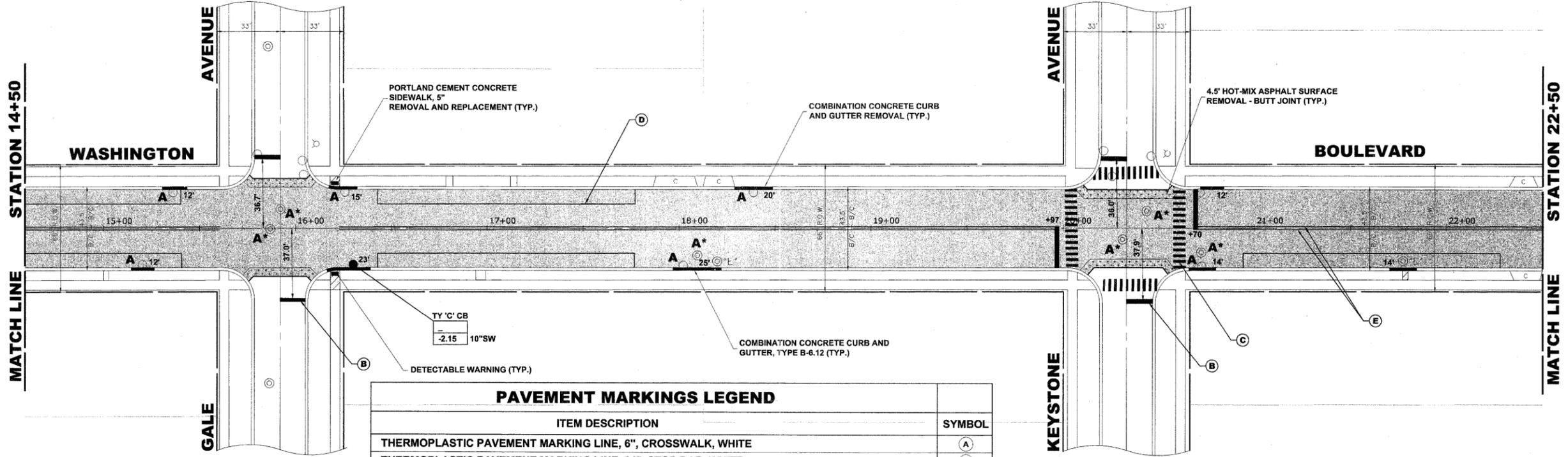
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PLOT SCALE -	DRAWN - MK, DMM	REVISED -
PLOT DATE -	CHECKED - ALA	REVISED -
	DATE - 12-7-11	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

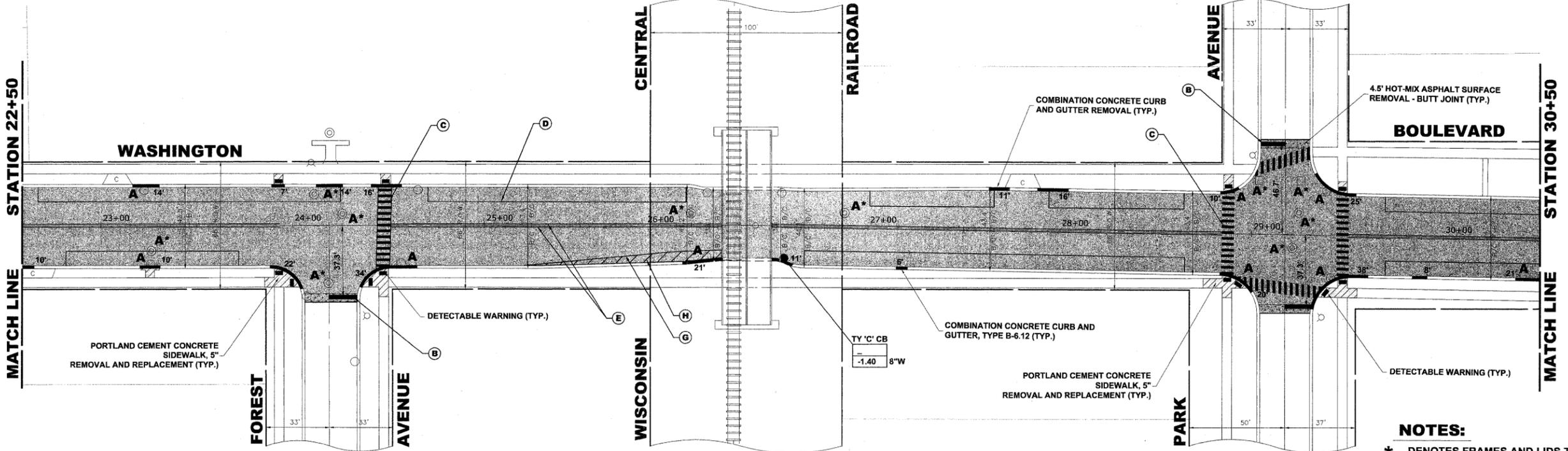
PAVING PLAN

SCALE: 1" = 30'
 SHEET NO. 1 OF 3 SHEETS
 STA. 0+49 TO STA. 14+50

ROUTE NO. FAU 1411	SECTION 11-00093-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 6
FIELD BOOK NO. -JAERIALS/L		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(943)				



PAVEMENT MARKINGS LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSSWALK, WHITE	(A)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 12", SCHOOL CROSSWALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PARKING LANE LINE, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PAVEMENT CENTERLINE DOUBLE @ 11" C-C, YELLOW	(E)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", SKIP DASH, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE, 8", GORE LINE, WHITE	(G)
THERMOPLASTIC PAVEMENT MARKING LINE, 12", DIAGONALS, WHITE	(H)



NOTES:
 * DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER

Drawing file: W:\Projects\74011155 - Washington Blvd LAPP\Washington Blvd LAPP.dwg Jan 17, 2012 - 10:08am

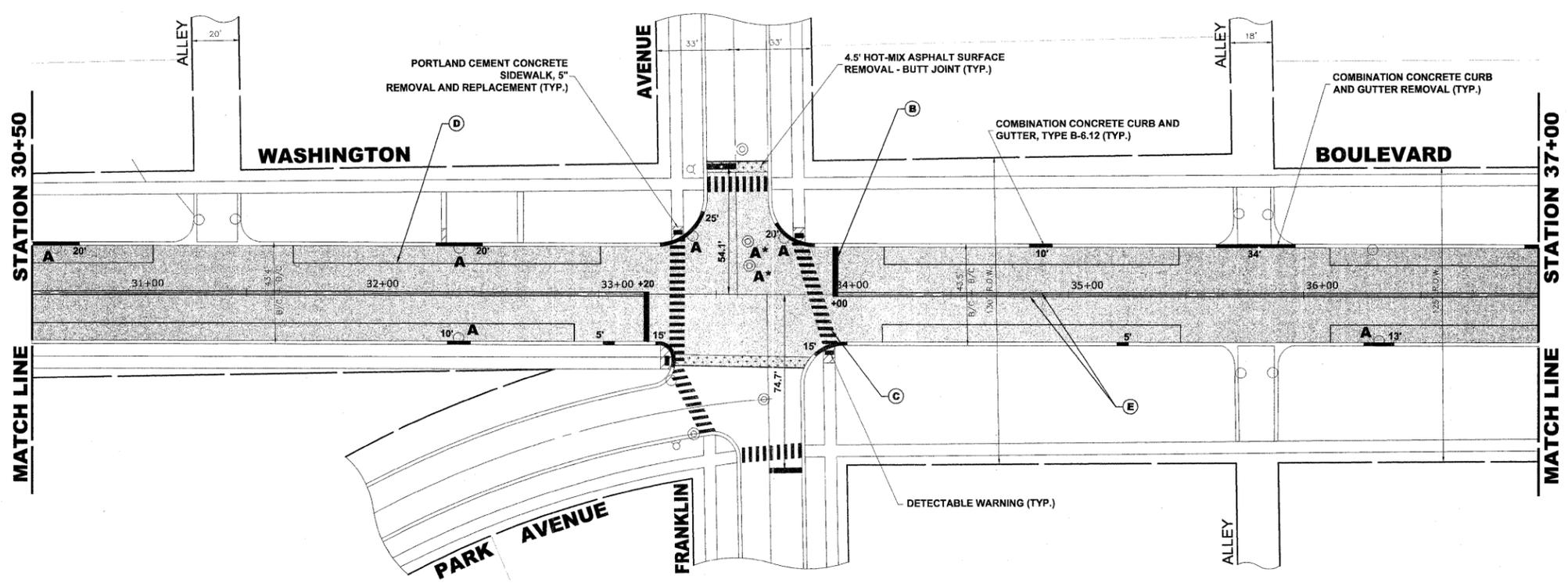
HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

USER NAME -	DESIGNED - ALA	REVISED -
PLOT SCALE -	DRAWN - MK, DMM	REVISED -
PLOT DATE -	CHECKED - ALA	REVISED -
	DATE - 12-7-11	REVISED -

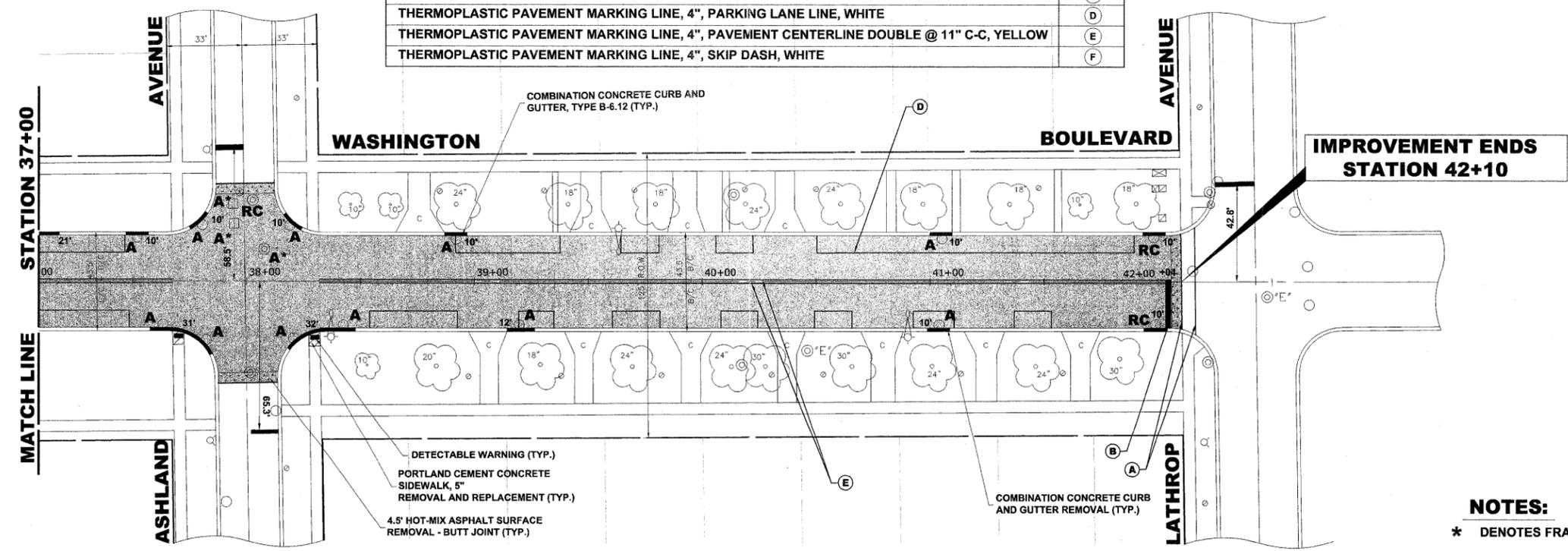
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVING PLAN
 SCALE: 1" = 30'
 SHEET NO. 2 OF 3 SHEETS
 STA. 14+50 TO STA. 30+50

ROUTE NO. FAU 1411	SECTION 11-00093-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 7
FIELD BOOK NO. -AERIAL/SLL		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(943)				



PAVEMENT MARKINGS LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSSWALK, WHITE	(A)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 12", SCHOOL CROSSWALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PARKING LANE LINE, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PAVEMENT CENTERLINE DOUBLE @ 11" C-C, YELLOW	(E)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", SKIP DASH, WHITE	(F)



NOTES:
 * DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER

Drawing file: W:\Projects\7401155 - Washington Blvd LAPP\Washington Blvd LAPP.dwg Jan 23, 2012 11:57am

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

USER NAME -	DESIGNED - ALA	REVISED -
PLOT SCALE -	DRAWN - MK, DMM	REVISED -
PLOT DATE -	CHECKED - ALA	REVISED -
	DATE - 12-7-11	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

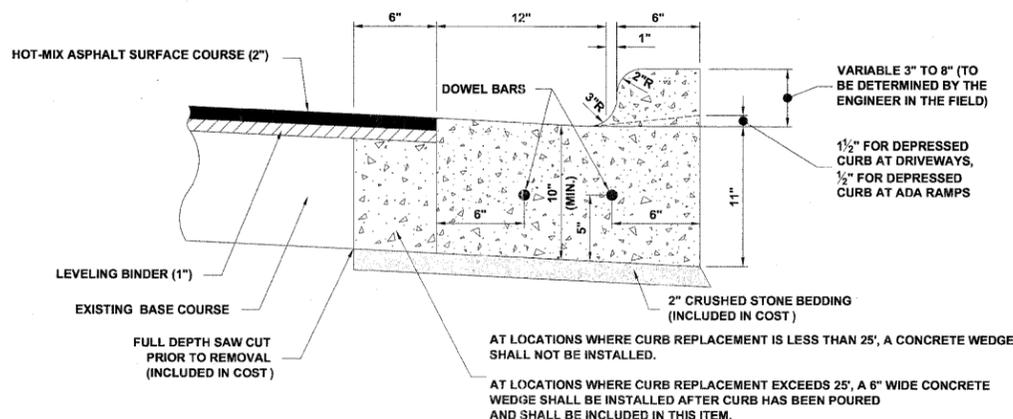
PAVING PLAN

SCALE: 1" = 30'
 SHEET NO. 3 OF 3 SHEETS STA. 30+50 TO STA. 42+10

ROUTE NO. FAU 1411	SECTION 11-00093-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 8
FIELD BOOK NO. -AERIALS/LL		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(943)				
E.H.E. PROJECT NO. 740-11-15501				

M.W.R.D.G.C. GENERAL NOTES

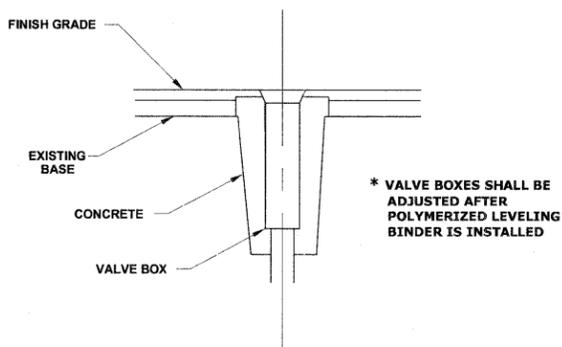
1. THE MWRD SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF WORK AT (708) 588-4055.
2. ELEVATION DATUM IS U.S.G.S.
3. ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER SYSTEM.
4. ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE INTO THE STORM SEWER SYSTEM.
5. ALL PVC STORM SEWER PIPE JOINTS SHALL CONFORM TO ASTM D-3139. ALL PVC SEWER PIPE 12" IN DIAMETER OR LESS SHALL CONFORM TO ASTM D-2241 (WATER QUALITY PIPE). ALL PVC SEWER PIPE SHALL BE SDR 26.
6. ALL D.I.P. STORM, COMBINED AND SANITARY SEWER PIPE JOINTS SHALL CONFORM TO ASTM A-21.11. ALL D.I.P. SEWER PIPE SHALL CONFORM TO ASTM A-21.51. ALL D.I.P. SEWER PIPE SHALL BE CLASS 52.
7. ALL SANITARY, COMBINED, AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS, REQUIRES STONE BEDDING 1/4" TO 1" IN SIZE, WITH A MINIMUM THICKNESS EQUAL TO 1/4 THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR INCHES (4") NOR MORE THAN EIGHT INCHES (8"). MATERIAL SHALL BE CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE.
8. "BAND SEAL" OR SIMILAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPE OF DISSIMILAR MATERIALS.
9. WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
 - a) CIRCULAR SAW-CUT OF SEWER MAIN BY MECHANICAL CORING MACHINE, AND PROPER INSTALLATION OF HUB-WYE SADDLE OR HUB-TEE SADDLE, IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
 - b) REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION. AFTER THE WYE OR TEE BRANCH IS INSERTED, CONCRETE SHALL BE PLACED OVER THE BROKEN AREA TO A MINIMUM THICKNESS OF 4" AND TO A DIMENSION OF 8" IN ALL DIRECTIONS.
 - c) USING PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING. USE "BAND SEAL" OR SIMILAR COUPLINGS TO HOLD FIRMLY IN PLACE. FOLLOW MANUFACTURER'S RECOMMENDATIONS FOR THE INSTALLATION.
10. WHEREVER A SEWER CROSSES UNDER A WATER MAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATER MAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATER MAINS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATER MAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18" VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATER MAIN, THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS.
11. ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE PRE-CAST REINFORCED CONCRETE.
12. ALL ABANDONED SEWERS SHALL BE PLUGGED AT BOTH ENDS WITH A MINIMUM OF TWO (2) FEET LONG, NON-SHRINK CONCRETE/MORTAR PLUG.
13. ALL INLET AND OUTLET PIPES OF SANITARY SEWER MANHOLES AND OTHER UNDERGROUND STRUCTURES (AND IN COMBINED SEWER AREAS, ALSO ALL COMBINED/STORM SEWER MANHOLES, CATCH BASINS, INLETS, AND UNDERGROUND DETENTION STORAGE STRUCTURES) SHALL BE JOINED WITH WATERTIGHT FLEXIBLE RUBBER CONNECTORS CONFORMING TO A.S.T.M. C-443 & C-923 WITH STAINLESS STEEL BANDS.
14. THE MAXIMUM ALLOWABLE INFILTRATION OR EXFILTRATION IS 100 GAL/DAY/MILE/INCH DIA OF THE SEWER PIPE.



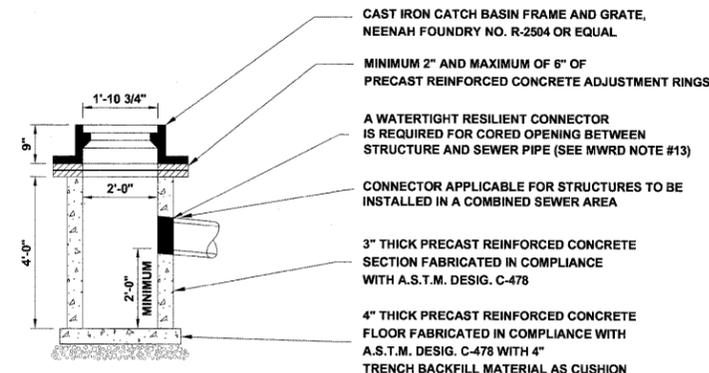
NOTES:

1. PROVIDE 2 - 3/4" DIAMETER, 18" LONG EPOXY COATED SMOOTH BARS WITH PLASTIC EXPANSION CAPS AT EACH EXPANSION JOINT.
2. CONTRACTION JOINT - 2" DEEP CONTRACTION JOINTS SHALL BE SAWED AT EQUAL SPACES (NOT EXCEEDING 15 FEET) BETWEEN NORMAL EXPANSION JOINTS, IN THE UPPER 1/3 OF CURB & GUTTERS WITHIN 24 HOURS OF PLACEMENT.
3. A LEAN CONCRETE WEDGE IS TO BE POURED IN THE SPACE BETWEEN THE NEW CURB & THE EXISTING PAVEMENT AFTER THE CURB HAS REACHED ITS INITIAL SET.

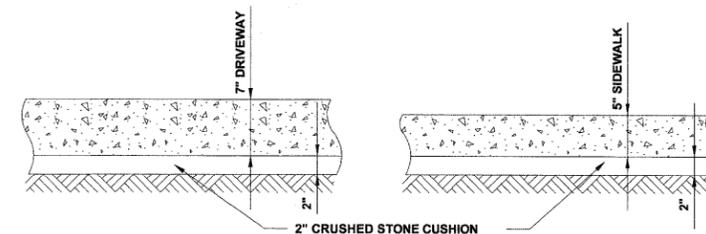
COMBINATION CONCRETE CURB AND GUTTER



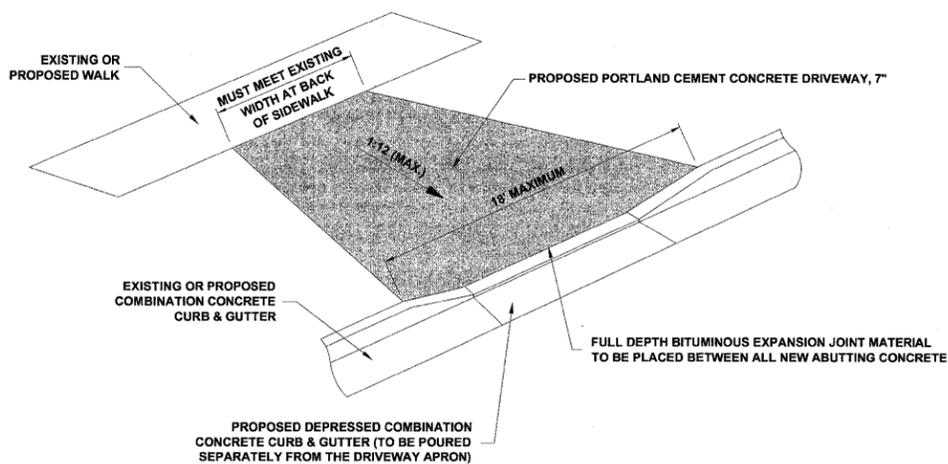
VALVE BOXES TO BE ADJUSTED (SPECIAL)



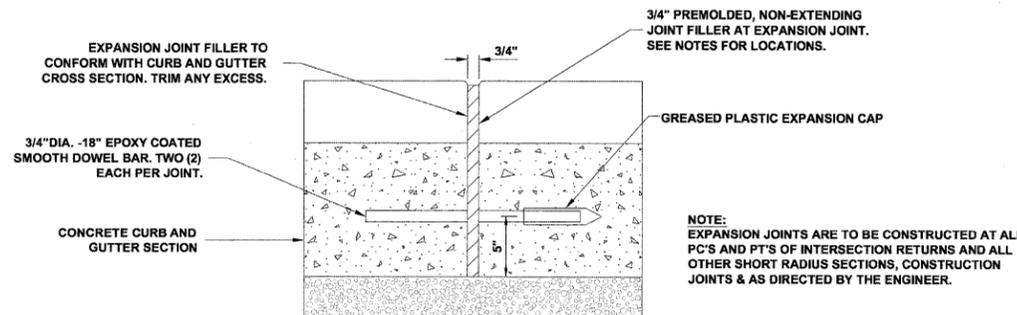
TYPE 'C' CATCH BASIN



TYPICAL P.C.C. SIDEWALK & DRIVEWAY DETAIL



DRIVEWAY

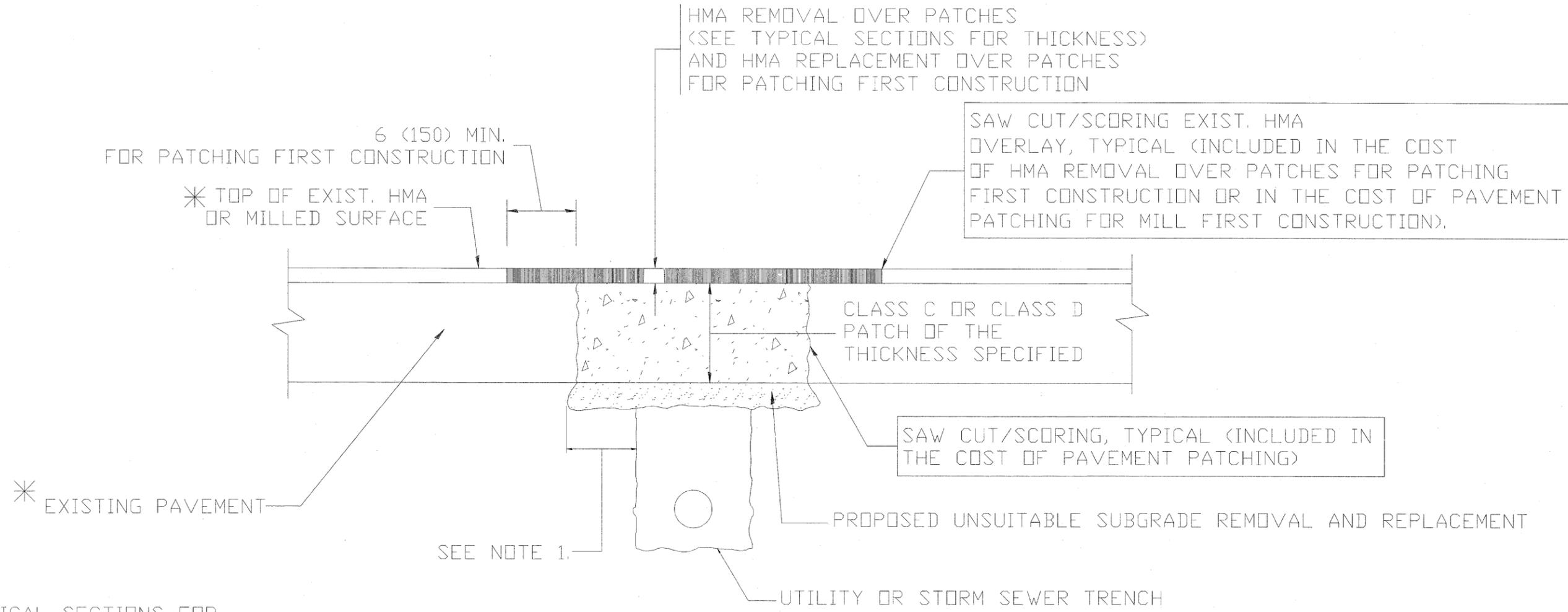


TYPICAL CURB AND GUTTER EXPANSION JOINT

USER NAME -	DESIGNED - ALA	REVISED -
PLOT SCALE -	DRAWN - MK, DMM	REVISED -
PLOT DATE -	CHECKED - ALA	REVISED -
	DATE - 12-7-11	REVISED -

DETAILS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 1411	11-00093-00-RS	COOK	14	9
FIELD BOOK NO. -AERIALS/ILL		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-9003(943)		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

Drawing file: M:\Projects\74011155 - Washington Blvd LAPP\Workings.dwg Jan 25, 2012 - 3:02pm

USER NAME =	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
PLOT SCALE =	DRAWN -	REVISED - R. BORO 01-01-07
PLOT DATE =	CHECKED -	REVISED - R. BORO 09-04-07
	DATE - 10-25-94	REVISED - K. ENG 10-27-08

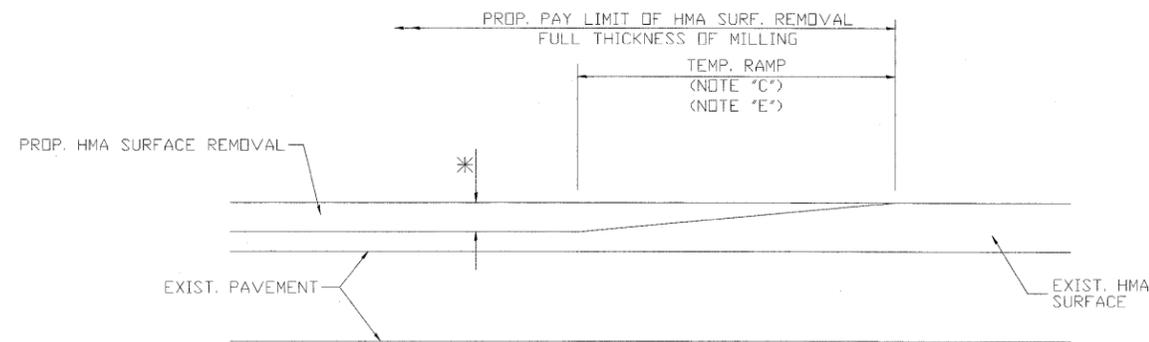
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1411	11-00093-00-RS	COOK	14	11
BD400-04 (BD-22)		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-9003(943)		

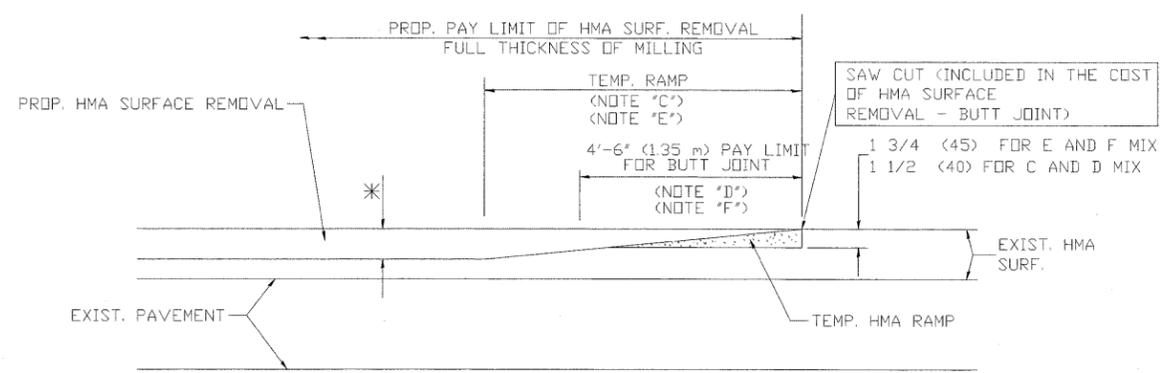
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

E.H.E. PROJECT NO. 740-11-15501



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

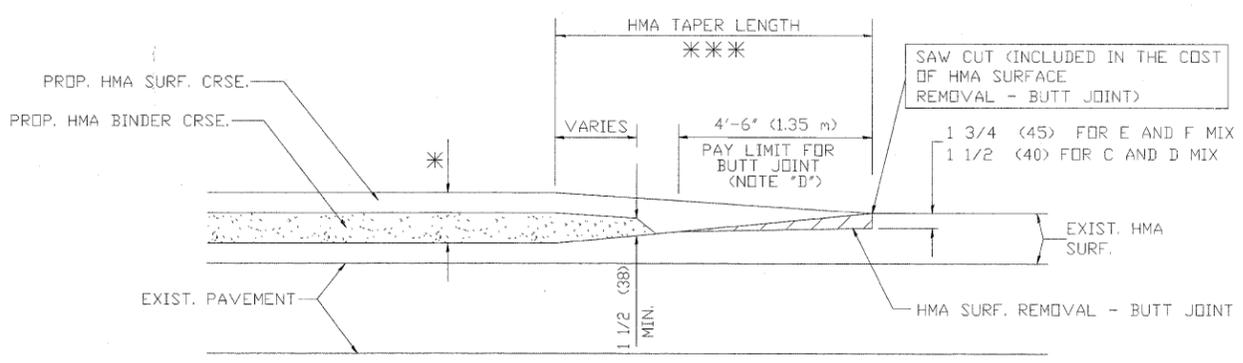
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

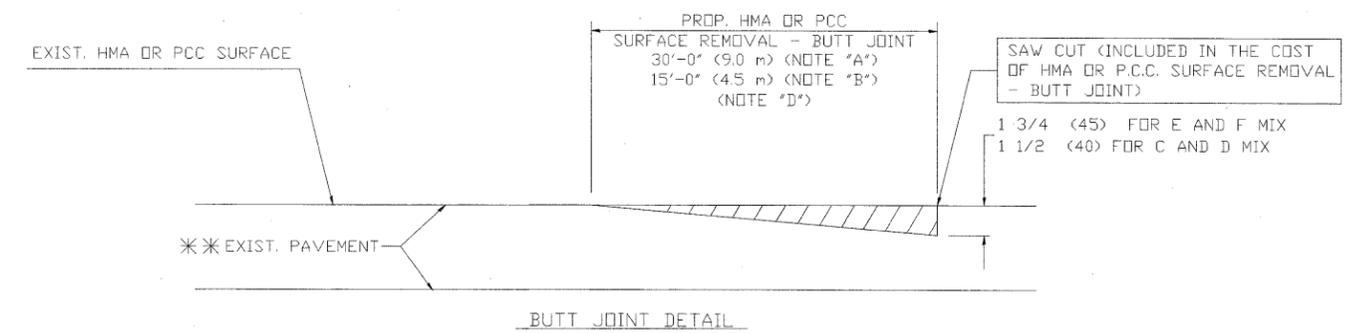
OPTION 2

TYPICAL TEMPORARY RAMP

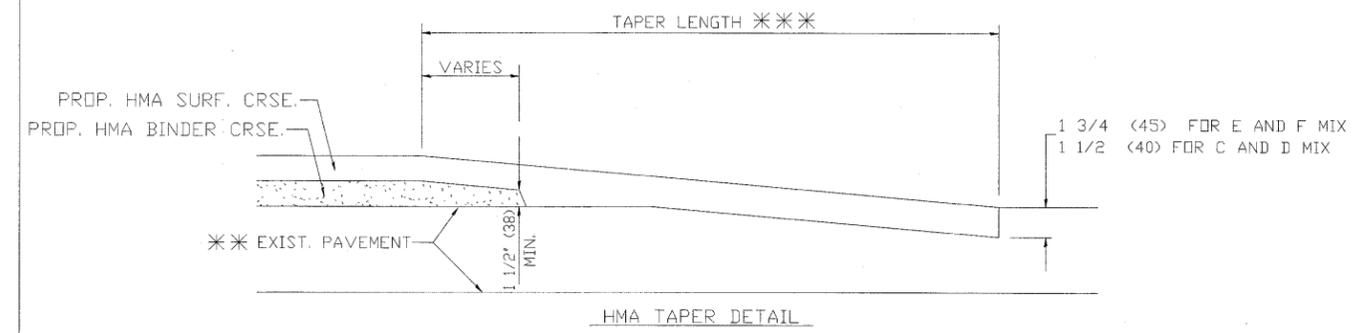


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR 'HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT'.
- ** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE 'A')
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE 'B')

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR 'HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT' OR FOR 'PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT'.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

Drawing file: W:\Projects\74011155 - Washington Blvd JAPP\Workings.dwg Jan 25, 2012 - 3:02pm

USER NAME =	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE =	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE =	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

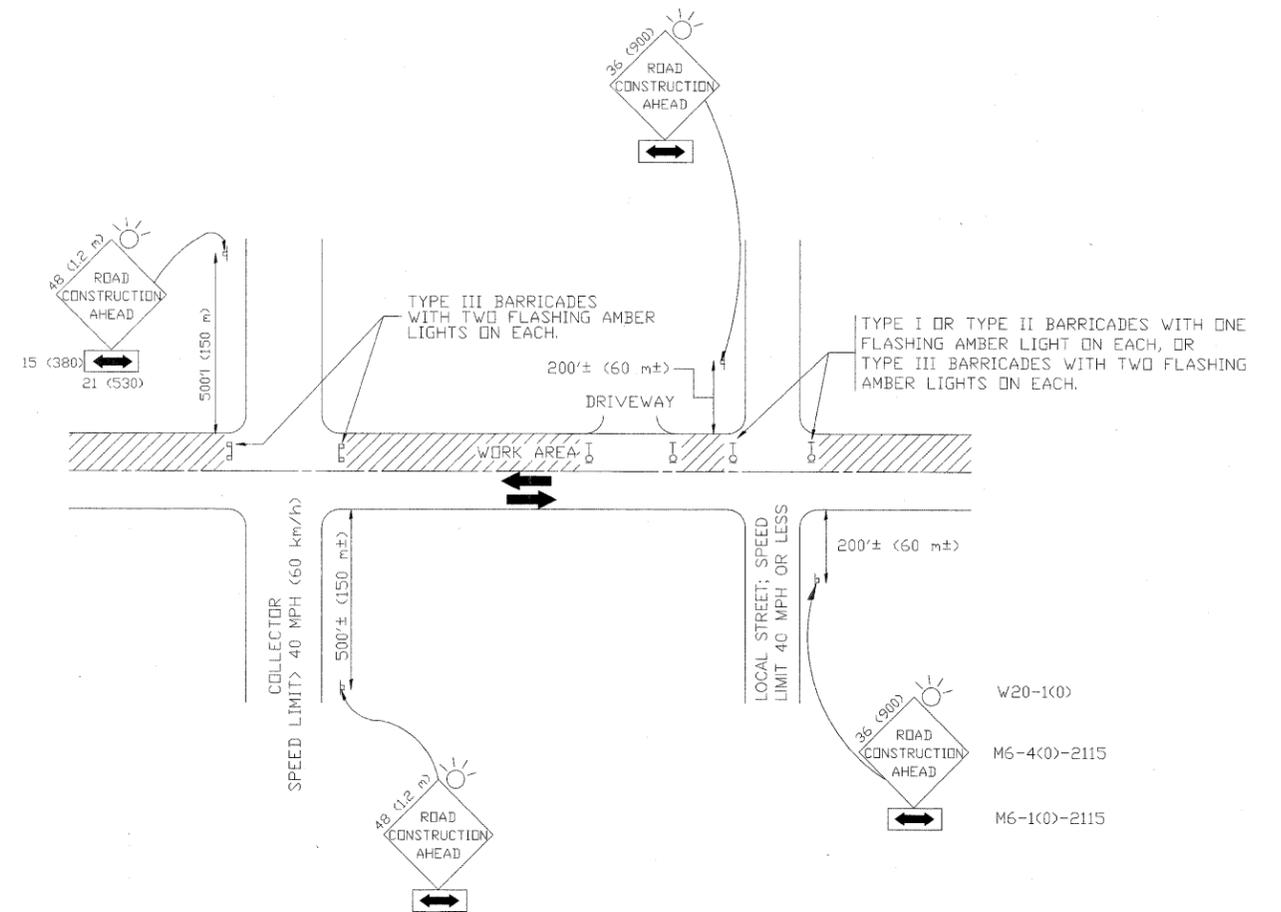
**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1411	11-00093-00-RS	COOK	14	12
BD400-05 (BD-32)		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(943)				

E.H.E. PROJECT NO. 740-11-15501

Drawing file: W:\Projects\7401155 - Washington Blvd JAPM\Workings.dwg Jan 25, 2012 - 3:02pm



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (12 m x 12 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

USER NAME =	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
	DRAWN -	REVISED - A. HOUSEH 03-06-96
PLOT SCALE =	CHECKED -	REVISED - A. HOUSEH 10-15-96
PLOT DATE =	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

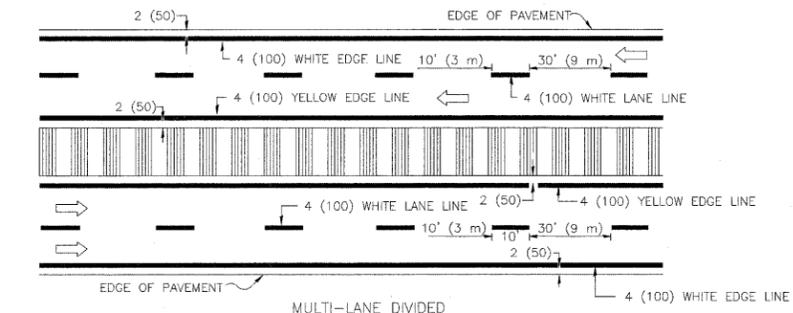
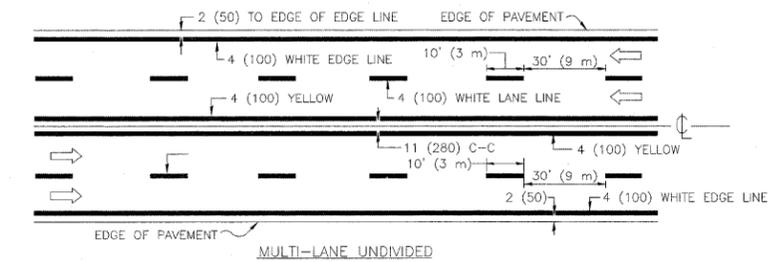
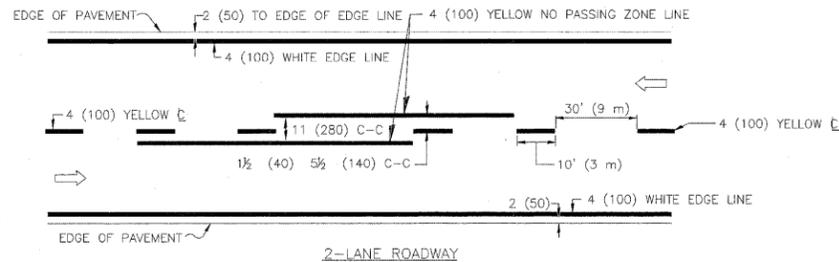
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1411	11-00093-00-RS	COOK	14	13
TC-10		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT M-9003(943)	

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

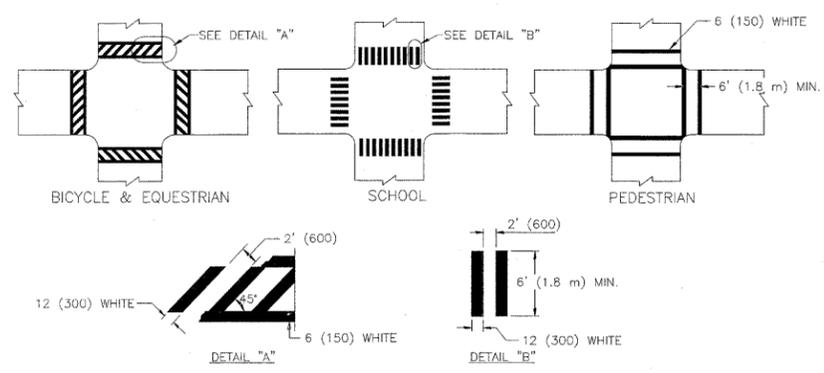
E.H.E. PROJECT NO. 740-11-15501

Drawing file: W:\Projects\4011185 - Washington Blvd LAPP\Workings.dwg Jan 25, 2012 - 3:02pm

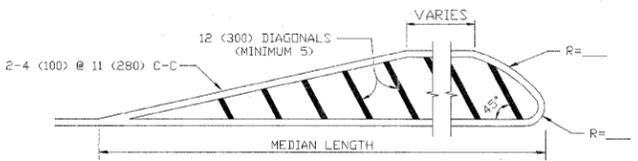
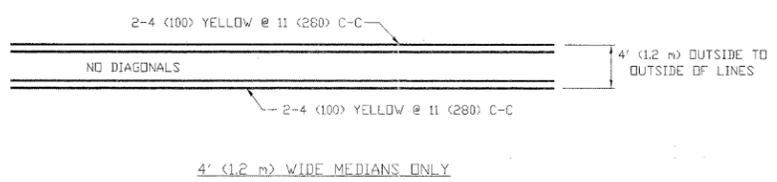


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

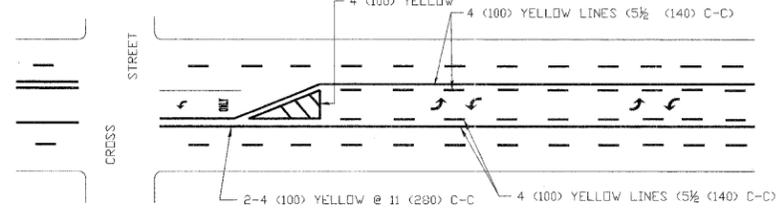


TYPICAL CROSSWALK MARKING

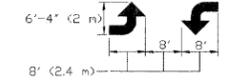


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

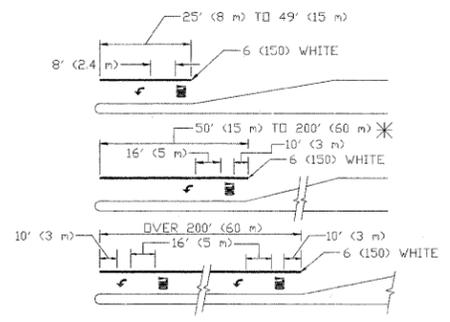


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

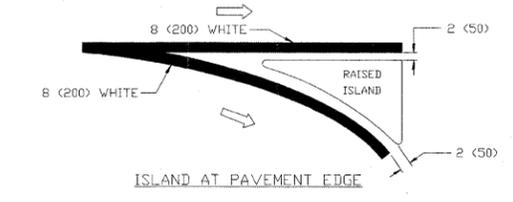
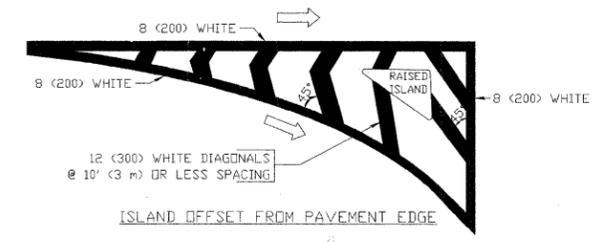
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - 'ONLY' INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - 'ONLY'.

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2' (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS @ 8' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2' (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; 'RR' IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR 'X'	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: 'R'=3.6 SQ. FT. (0.33 m ²) EACH 'X'=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T.RAMMACHER 10-27-94
PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1411	11-00093-00-RS	COOK	14	14
TC-13		CONTRACT NO. 63686		
FED. ROAD DIST. NO. 1 ILLINOIS / FED. AID PROJECT M-9003(943)				
E.H.E. PROJECT NO. 740-11-15501				