

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF PARK FOREST

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

04-27-12 LETTING ITEM 081

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
• 11-00095-00-RS		COOK	27	1
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED AID PROJECT		

• 1055/1056

CONTRACT NO. 63690

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 1056 (INDIANWOOD BOULEVARD)
FROM FAU 1632 (SAUK TRAIL) TO FAU 2845 (WESTERN AVENUE)
FAU 1055 (LAKEWOOD BOULEVARD)
FROM FAU 1632 (SAUK TRAIL) TO FAU 2836 (ORCHARD DRIVE)

RESURFACING

PROJECT No.: M-9003(969)
VILLAGE OF PARK FOREST
SECTION No.: 11-00095-00-RS
JOB No: C-91-261-12
COOK COUNTY



LOCATION OF SECTION INDICATED THIS: [Symbol]

TRAFFIC DATA

LAKEWOOD BOULEVARD
POSTED SPEED LIMIT = 20 MPH
DESIGN SPEED LIMIT = 25 MPH
2007 ADT = 3,700 VPD

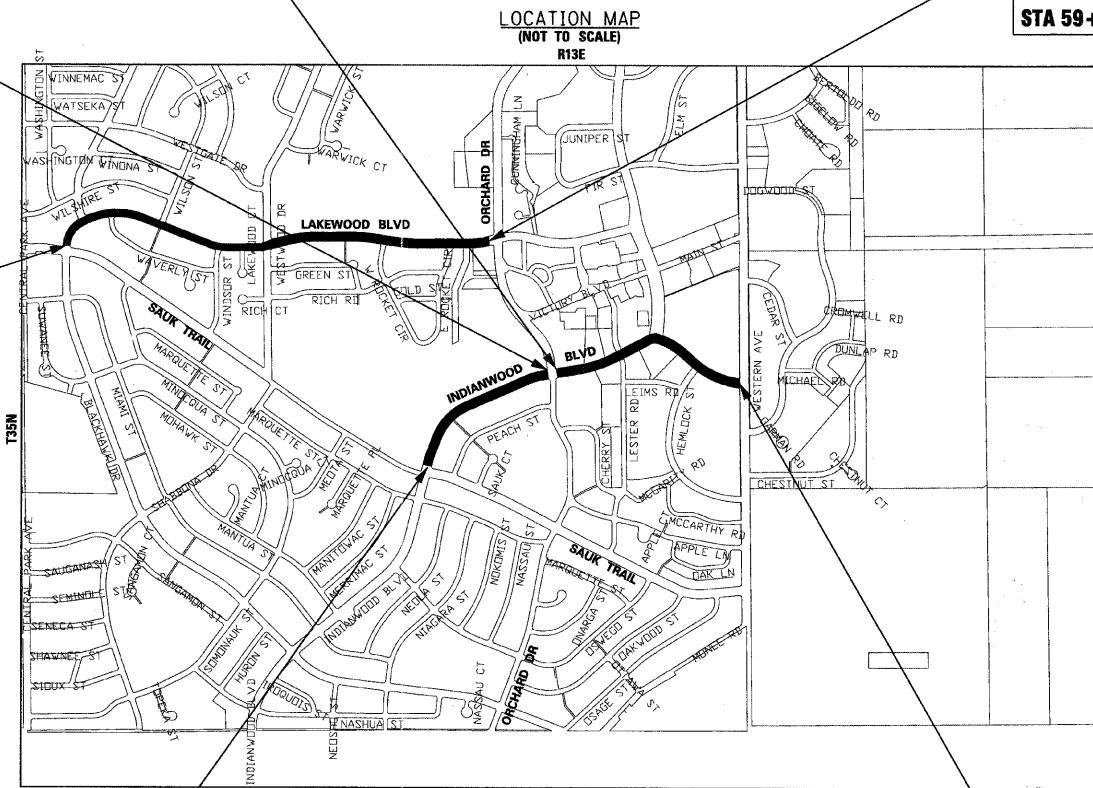
INDIANWOOD BOULEVARD
POSTED SPEED LIMIT = 20 MPH
DESIGN SPEED LIMIT = 25 MPH
2007 ADT = 2,800 VPD

DESIGN DESIGNATION
COLLECTOR

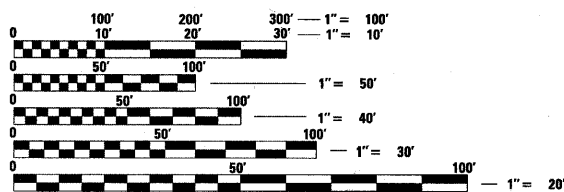
FAU 1056 INDIANWOOD BOULEVARD
OMISSION BEGINS
STA 118+45

FAU 1055 LAKEWOOD BOULEVARD
IMPROVEMENT BEGINS
STA 10+78

BAXTER & WOODMAN, INC.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. - 184-001121 - EXPIRES 4/30/2013



LOCATION MAP
(NOT TO SCALE)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. X3111486



CONTACT JULIE AT 811 OR 800-892-0123
WITH THE FOLLOWING:
COUNTY = COOK
CITY-TWNNSHP. = PARK FOREST - RICH
SEC. & 1/4 SEC. NO. T35N R13E
48 HOURS (2 working days) BEFORE YOU DIG

CONTRACT NO. 63690

FAU 1056 INDIANWOOD BOULEVARD
IMPROVEMENT BEGINS
STA 101+28 NORTHEAST BOUND
STA 102+16 SOUTHWEST BOUND

RICH TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 8,895 LF OR 1.685 MILES
NET LENGTH OF IMPROVEMENT = 8,771 LF OR 1.661 MILES

FAU 1056 INDIANWOOD BOULEVARD
IMPROVEMENT ENDS
STA 141+39



THOMAS M. SLATTERY
62-050844
LICENSED
PROFESSIONAL
ENGINEER
PROJECT MANAGER
"LICENSE EXPIRES 11-30-2013"

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	Feb 2, 2012 Kenneth A. Eyer VILLAGE OF PARK FOREST, DIRECTOR OF PUBLIC WORKS
PASSED	FEBRUARY 14, 2012 C. HOLT DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	FEBRUARY 14, 2012 Diana M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. - 184-001121 - EXPIRES 4/30/2013
PROGRAM AND OFFICE ENGINEER: CHARLES RIDDLE, P.E. (847) 705-4406 SCHAUMBURG, IL

GENERAL NOTES

1. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
2. ALL LOCATIONS OF PUBLIC AND PRIVATE UTILITIES SHOWN ON PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED IN THE STANDARD SPECIFICATIONS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER OR REPLACED. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
4. ALL SAW CUTTING SHALL BE INCLUDED IN THE UNIT COST OF THE PAY ITEM INDICATED FOR REMOVAL.
5. ALL CONSTRUCTION PERSONNEL WILL BE REQUIRED TO WEAR A FLUORESCENT VEST PER ARTICLE 701.12 AND 701.13 OF THE STANDARD SPECIFICATIONS AT ALL TIMES WHILE ON THE CONSTRUCTION SITE. COMPLIANCE WITH THIS REQUIREMENT SHALL BE INCLUDED IN THE CONTRACT.
6. NO STREET CLOSURES WILL BE ALLOWED.
7. THE CONTRACTOR SHALL REPLACE ALL STREET SIGNS AND MAIL BOXES REMOVED DURING CONSTRUCTION AS NEAR AS POSSIBLE TO THEIR ORIGINAL LOCATION OR AS DETERMINED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE CONTRACT.
8. ALL OPEN HOLE, BROKEN PAVEMENT AND TRENCHES RESULTING FROM STRUCTURE ADJUSTMENTS, OR CURB REPAIRS WORK SHALL BE BACKFILLED TO GRADE BY THE END OF THE DAY.
9. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED)
10. 10 FEET TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
11. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE VILLAGE OF PARK FOREST PUBLIC WORKS DEPARTMENT, AND BAXTER AND WOODMAN, INC.
12. BARRICADE: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAGS ACROSS EACH BOTTOM RAIL.
13. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETING EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
14. DRIVEWAYS SHALL REMAIN ACCESSIBLE DURING THE ENTIRE DURATION OF THE PROJECT.
15. THE CONTRACTOR SHALL NOTIFY VILLAGE PUBLIC WORKS AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
16. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE VILLAGE, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
17. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING" BAN IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE OF PARK FOREST WATER IF DEEMED NECESSARY.
18. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.

19. IN AREAS WHERE THE EXISTING DRIVEWAY OR SIDEWALK TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEM FOR PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT.
20. THE CONTRACTOR SHALL LIMIT THE WORK TO ONE SIDE OF THE STREET UNLESS AS DIRECTED OTHERWISE BY THE ENGINEER.
21. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
22. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY. THE CONTRACTOR SHALL PRIME IMMEDIATELY PRECEDING THE PAVING OPERATION.
23. THE LOCATIONS OF THE CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
24. THE DAYS PAVING OPERATION SHALL RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
25. ANY ANTI-STRIPPING ADDITIVE REQUIRED SHALL BE INCLUDED IN THE COST OF THE SURFACE COURSE.
26. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HOT-MIX ASPHALT OR PORTLAND CEMENT CONCRETE.
27. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
28. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
29. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25-INCH ABOVE THE GUTTER FLAG.
30. NEW OR REPLACEMENT CLOSED LIDS SHALL BE STAMPED TO INDICATE THE STRUCTURE TYPE. STORM LIDS SHALL BE STAMPED WITH "STORM", SANITARY LIDS SHALL BE STAMPED WITH "SANITARY" AND WATER VALVE VAULT LIDS SHALL BE STAMPED WITH "WATER". STAMPING SHALL BE INCLUDED IN THE COST OF THE NEW LID. ALL NEW TYPE 1 OPEN LIDS SHALL BE BICYCLE SAFE.
31. CONNECTION OF PROPOSED STORM SEWER INTO EXISTING STORM SEWER OR EXISTING STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF STORM SEWERS.
32. CONNECTION OF EXISTING STORM SEWER INTO PROPOSED STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF THE STORM SEWER STRUCTURE. ANY ADDITIONAL STORM SEWER PIPE REQUIRED TO MAKE THE CONNECTION SHALL BE OF THE SAME SIZE AND MATERIAL TYPE AS THE EXISTING STORM SEWER AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWER STRUCTURE.
33. ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS."
34. ANY DAMAGE TO PAVEMENT, SIDEWALK, CURB OR ANY OTHER PORTION OF THE ROADWAY NOT SPECIFICALLY TO BE REMOVED AND REPLACED SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL CHARGE.
35. CURB AND GUTTER AND DRIVEWAYS PROVIDING ACCESS SHALL BE REMOVED AND REPLACED WITHIN 3 DAYS.
36. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.
37. ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE EXISTING STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE IN ADDITION TO THE REQUIREMENTS FOR MANHOLES TO BE ADJUSTED AND SHALL BE PAID FOR ONCE AT THE CONTRACT UNIT PRICE FOR MANHOLES TO BE ADJUSTED.

38. PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE IN ADDITION TO THE REQUIREMENTS FOR MANHOLES TO BE ADJUSTED AND SHALL BE PAID FOR ONCE AT THE CONTRACT UNIT PRICE FOR MANHOLES TO BE ADJUSTED.
39. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET.
40. A 1/2-INCH EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT.
41. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE "ARMOR-TILE" 24"x48" NOMINAL PANEL WIDTH AS MANUFACTURED BY "ENGINEERED PLASTICS INC." (800) 682-2525 OF WILLIAMSVILLE, NY OR AN APPROVED EQUAL. THE PANEL SHALL BE A POLYMER COMPOSITE AND COMPLY WITH ADA REQUIREMENTS. THE PANEL COLOR SHALL BE SELECTED BY THE VILLAGE. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS.
42. THE CONTRACTOR SHALL INFORM THE CCHD DESIGN ENGINEER (312) 603-1730 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS NOTICE IS REQUIRED TO SCHEDULE MAINTENANCE TRANSFER OF THE TRAFFIC SIGNAL.
43. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	SCHEDULE OF MATERIALS
5 - 6	TYPICAL SECTIONS AND HMA MIXTURE REQUIREMENTS
7 - 15	PLAN SHEETS
16	DISTRICT 1 DETAIL - BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
17	DISTRICT 1 DETAIL - BD-24 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
18	DISTRICT 1 DETAIL - BD-32 BUTT JOINTS AND HMA TAPER DETAILS
19	DISTRICT 1 DETAIL - TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
20	DISTRICT 1 DETAIL - TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
21	DISTRICT 1 DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN
22	DISTRICT 1 DETAIL - TS-07 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
23	DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS
24 - 27	CCHD EXISTING SIGNAL PLANS (REFERENCE ONLY)

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-06	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006	DIAGONAL CURB RAMPS FOR SIDEWALKS
424016	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602301-03	INLET, TYPE A
602306-03	INLET, TYPE B
604001-03	FRAMES AND LIDS, TYPE 1
604006-04	FRAME AND GRATE, TYPE 3
604066-02	FRAME AND LID, TYPE 15
701501-06	URBAN LANE CLOSURE, 2L 2W UNDIVIDED
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATION
886006-01	TYPICAL LAYOUTS FOR DETECTOR LOOPS

*1055/1056

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CHECKED	- JUF	REVISED	-
DATE	- 10-26-11	FILE	- 091213-GenNotes.sht

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES

SCALE: NONE

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	11-00095-00-RS	COOK	27	2
CONTRACT NO. 63690				

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT N-900319691

SUMMARY OF QUANTITIES

ITEM NO.	PAY ITEM	UNIT	CONSTRUCTION TYPE CODE
			0005
			TOTAL QUANTITY
20200100	EARTH EXCAVATION	CU YD	47
20800150	TRENCH BACKFILL	CU YD	72
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	4,122
40600300	AGGREGATE (PRIME COAT)	TON	84
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,658
40600895	CONSTRUCTING TEST STRIP	EACH	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT	SQ YD	358
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	3,462
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	1
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	2,232
42400800	DETECTABLE WARNINGS	SQ FT	470
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	39,472
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	2,232
44000600	SIDEWALK REMOVAL	SQ FT	3,336
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	16
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	1,181
44300200	STRIP REFLECTIVE CRACK CONTROL	FOOT	15,629
550A2320	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12"	FOOT	240
55100500	STORM SEWER REMOVAL 12"	FOOT	240
60235700	INLETS, TYPE A, TYPE 3 FRAME AND GRATE	EACH	7
60237000	INLETS, TYPE A, TYPE 15 FRAME AND LID	EACH	14
60240320	INLETS, TYPE B, TYPE 15 FRAME AND LID	EACH	2
60260100	INLETS TO BE ADJUSTED	EACH	20
60404300	FRAMES AND GRATES, TYPE 3	EACH	1
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	22
60406400	FRAMES AND LIDS, TYPE 15	EACH	1
60500060	REMOVING INLETS	EACH	23
67100100	MOBLIZATION	L SUM	1
# 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
# 70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
# 70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2,396
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	400
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	360
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	8,617
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,178
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,479
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	105
* # 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
* # 88600100	DETECTOR LOOP, TYPE I	FOOT	75
# X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH. SPECIAL	SQ FT	3,968
# X4400196	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	1,737
# X8026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	5
# X8030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	17
# XX007278	PARKWAY RESTORATION	SQ YD	1,772
# Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	104
# Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	4,087

INDICATES ITEM COVERED BY SPECIAL PROVISION
 * INDICATES SPECIALTY ITEM

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DESIGNED - TMS	REVISED -
DRAWN - KAR	REVISED -
CHECKED - JJF	REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE STA. TO STA.

*1055/1056

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	11-00095-00-RS	COOK	27	3
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT M-9003(969)	
			CONTRACT NO. 63690	

SCHEDULE OF MATERIALS

Table: SIDEWALK AND DETECTABLE WARNING SCHEDULE. Columns include Station, O/S, Sidewalk Removal (SQ FT), PCC Sidewalk 5 Inch, Special (SQ FT), and Detectable Warnings (SQ FT). Rows list materials for Lakewood Blvd and Indianwood Blvd with subtotals of 1762.0, 1567.0, and 310.0.

Table: DRIVEWAYS SCHEDULE. Columns include Station, o/s, Driveway Pavement Removal (SQ YD), and PCC Driveway Pavement, 6" (SQ YD). Rows list materials for Lakewood Blvd and Indianwood Blvd with subtotals of 1730.0 and 502.0.

Table: CLASS D PATCHING. Columns include STATION, TO STATION, LENGTH, WIDTH, AREA (SQ YD), Type III (Sq Yd), and Type IV (Sq Yd). Rows list materials for Lakewood Blvd, Indianwood Blvd, and ST SEWER with subtotals of 16.0 and 931.0.

Table: INLETS TO BE ADJUSTED. Columns include Station, O/S, and Description of Work. Rows list materials for Lakewood Blvd and Indianwood Blvd with a subtotal of 13.

Table: SANITARY MANHOLES TO BE ADJUSTED. Columns include Station, O/S, and Description of Work. Rows list materials for Lakewood Blvd and Indianwood Blvd with subtotals of 3 and 2.

Table: INLETS TO BE REMOVED AND REPLACED. Columns include STATION, O/S, TYPE, and CASTING. Rows list materials for Lakewood Blvd and Indianwood Blvd with a subtotal of 6.

Table: FRAMES AND LIDS TO BE ADJUSTED (SPECIAL). Columns include Station, O/S, and Description of Work. Rows list materials for Lakewood Blvd and Indianwood Blvd with a subtotal of 11.

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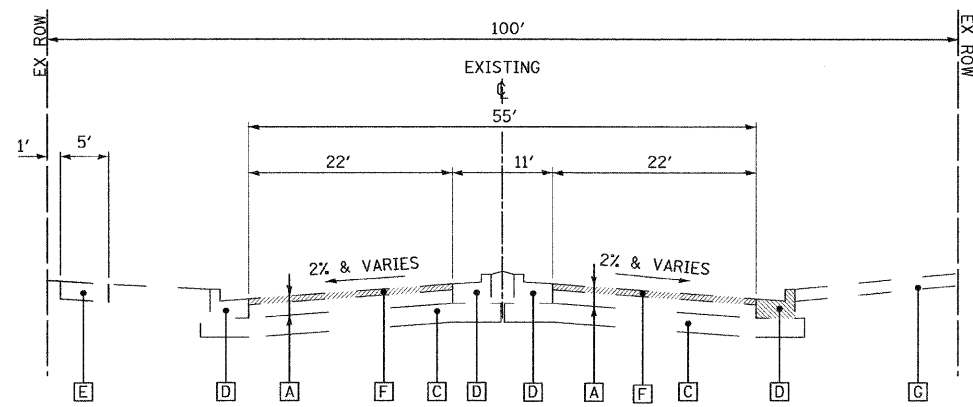
Table: Project metadata including Designer (TMS), Drawn (KAR), Checked (JJF), Date (10-26-11), Revised (), and File (091213-S00.shp).

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

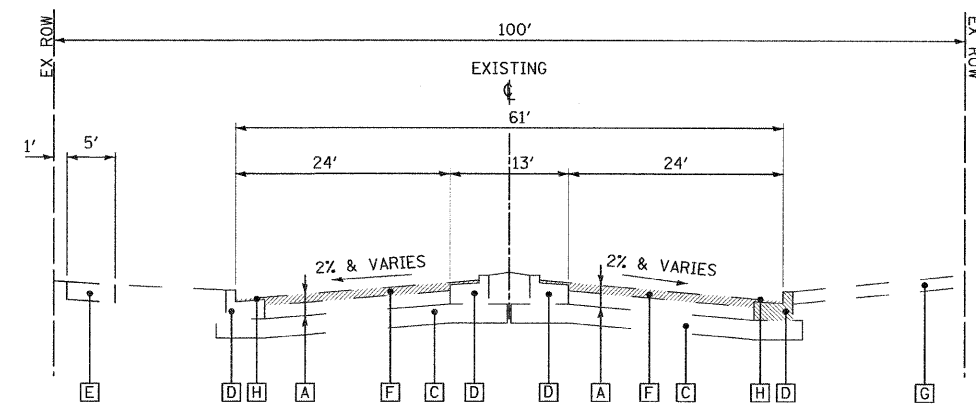
SCHEDULE OF MATERIALS

SCALE: NONE STA. TO STA.

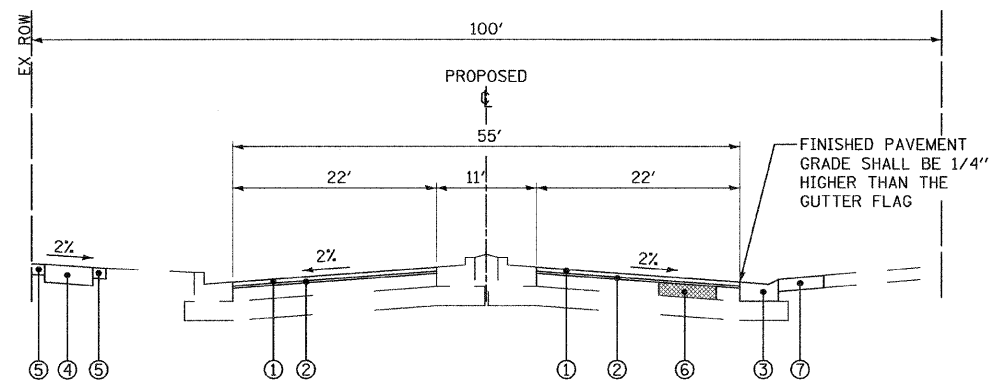
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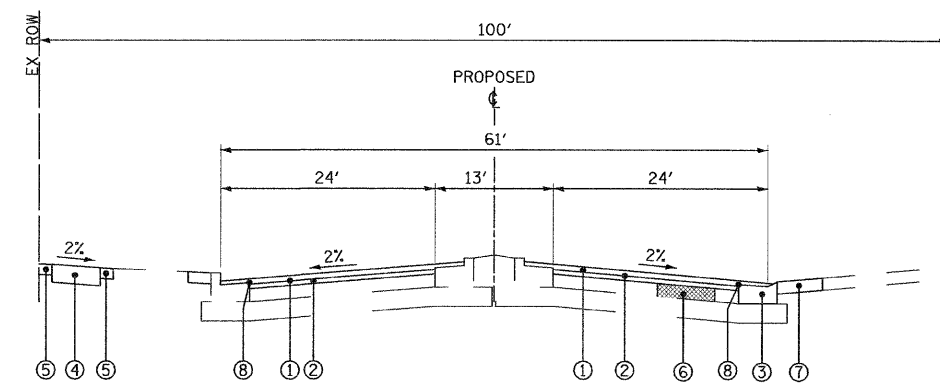
EXISTING TYPICAL SECTION
STA 101+28 TO STA 118+45, INDIANWOOD BOULEVARD



EXISTING TYPICAL SECTION
STA 119+45 TO STA 131+19, INDIANWOOD BOULEVARD



PROPOSED TYPICAL SECTION
STA 101+28 TO STA 118+45, INDIANWOOD BOULEVARD



PROPOSED TYPICAL SECTION
STA 119+45 TO STA 131+19, INDIANWOOD BOULEVARD

LEGEND

EXISTING

- A** HMA PAVEMENT
- B** PCC BASE COURSE
- C** AGGREGATE BASE COURSE
- D** COMBINATION CONCRETE CURB AND GUTTER
- E** SIDEWALK
- F** HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- G** DRIVEWAY
- H** HOT-MIX ASPHALT SURFACE REMOVAL (SPECIAL)

PROPOSED

- 1** HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- 2** POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- 3** COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY ENGINEER)
- 4** SIDEWALK REMOVAL
- 5** PARKWAY RESTORATION
- 6** CLASS D PATCHES (AS DETERMINED BY THE ENGINEER)
- 7** DRIVEWAY REMOVE AND REPLACE (AS DETERMINED BY THE ENGINEER)
- 8** STRIP REFLECTIVE CRACK CONTROL

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

ITEM	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm) - 1 1/2"	4% @ 50 Gyr
POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50 - 3/4"	3.5% @ 50 Gyr
PATCHING	
CLASS D PATCH TYPE III & IV (HMA BINDER IL-19 mm) - 6" (2 LIFTS)	4% @ 70 Gyr
DRIVEWAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm) - 3" (2 LIFTS)	4% @ 50 Gyr

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/50 YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

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DATE - 10-26-11	FILE - 091213-TypSec.shx

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

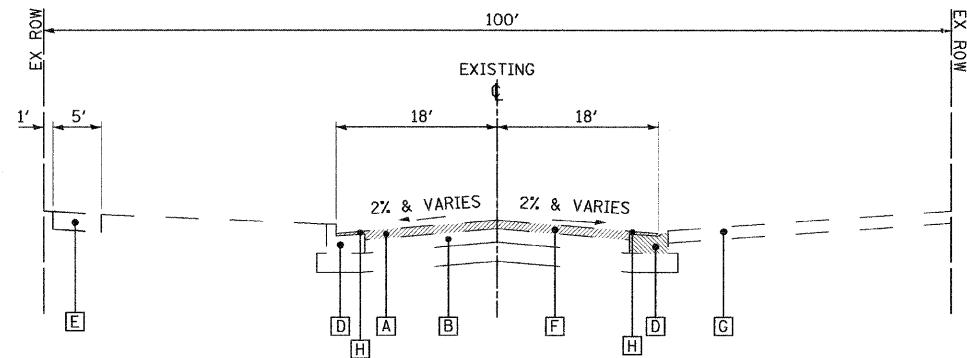
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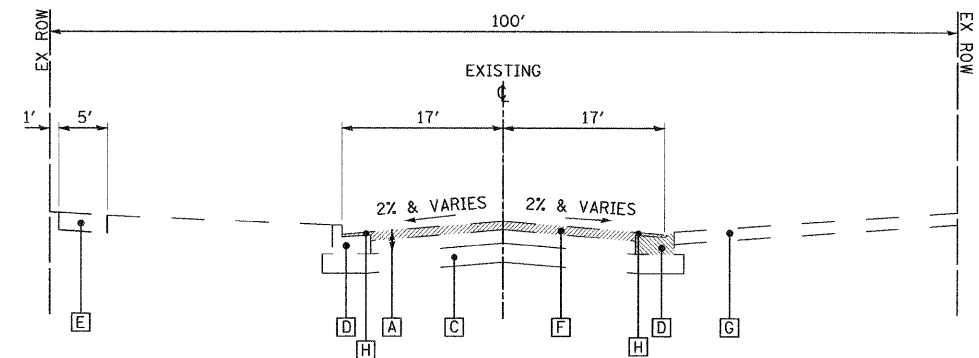
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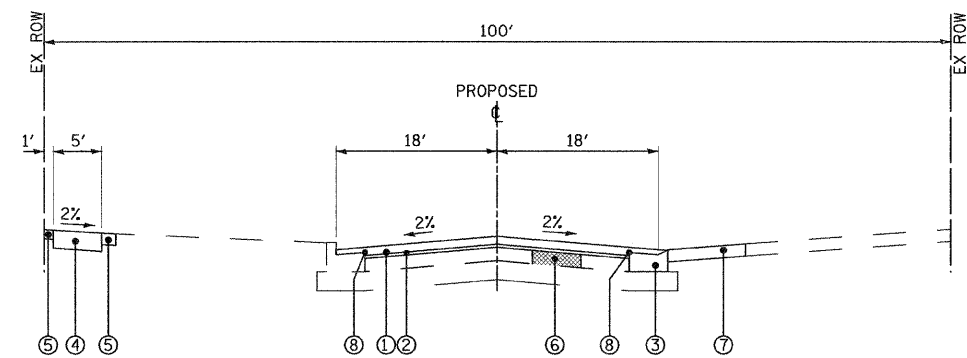
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FED. ROAD DIST. NO. ILLINOIS/FED. AID PROJECT M-9003969			CONTRACT NO. 63690	



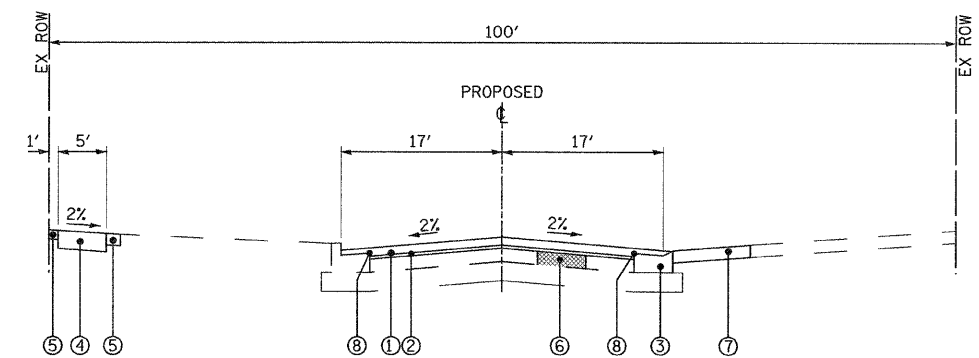
EXISTING TYPICAL SECTION
STA 131+19 TO STA 141+39, INDIANWOOD BOULEVARD



EXISTING TYPICAL SECTION
STA 10+78 TO STA 59+62, LAKEWOOD BOULEVARD



PROPOSED TYPICAL SECTION
STA 131+19 TO STA 141+39, INDIANWOOD DRIVE



PROPOSED TYPICAL SECTION
STA 10+78 TO STA 59+62, LAKEWOOD BOULEVARD

LEGEND

EXISTING

- A HMA PAVEMENT
- B PCC BASE COURSE
- C AGGREGATE BASE COURSE
- D COMBINATION CONCRETE CURB AND GUTTER
- E SIDEWALK
- F HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- G DRIVEWAY
- H HOT-MIX ASPHALT SURFACE REMOVAL (SPECIAL)

PROPOSED

- 1 HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- 2 POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- 3 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY ENGINEER)
- 4 SIDEWALK REMOVAL
- 5 PARKWAY RESTORATION
- 6 CLASS D PATCHES (AS DETERMINED BY THE ENGINEER)
- 7 DRIVEWAY REMOVE AND REPLACE (AS DETERMINED BY THE ENGINEER)
- 8 STRIP REFLECTIVE CRACK CONTROL

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 FILE - 091213-TypSec.sht



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DATE	10-26-11	FILE	091213-TypSec.sht

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DEPARTMENT OF TRANSPORTATION

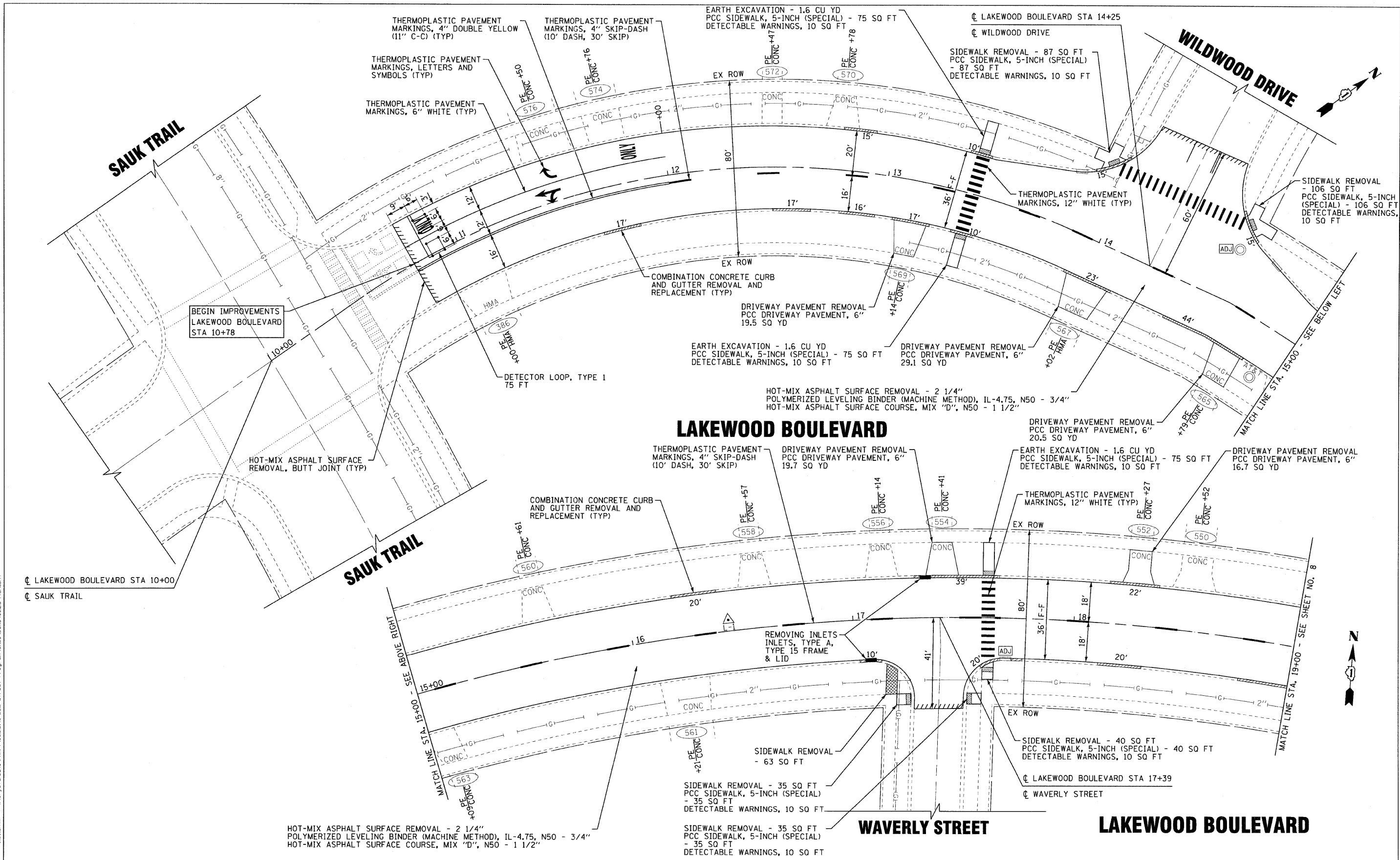
TYPICAL SECTIONS AND
HMA MIXTURE REQUIREMENTS

SCALE: NONE

STA. TO STA.

*1055/1056

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.	11-00095-00-RS	COOK	27	6
FED. ROAD DIST. NO. ILLINOIS/FED. AID PROJECT M-90031969			CONTRACT NO. 63690	



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DATE - 10-26-11	FILE - 091213-Plan1.shx

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DEPARTMENT OF TRANSPORTATION

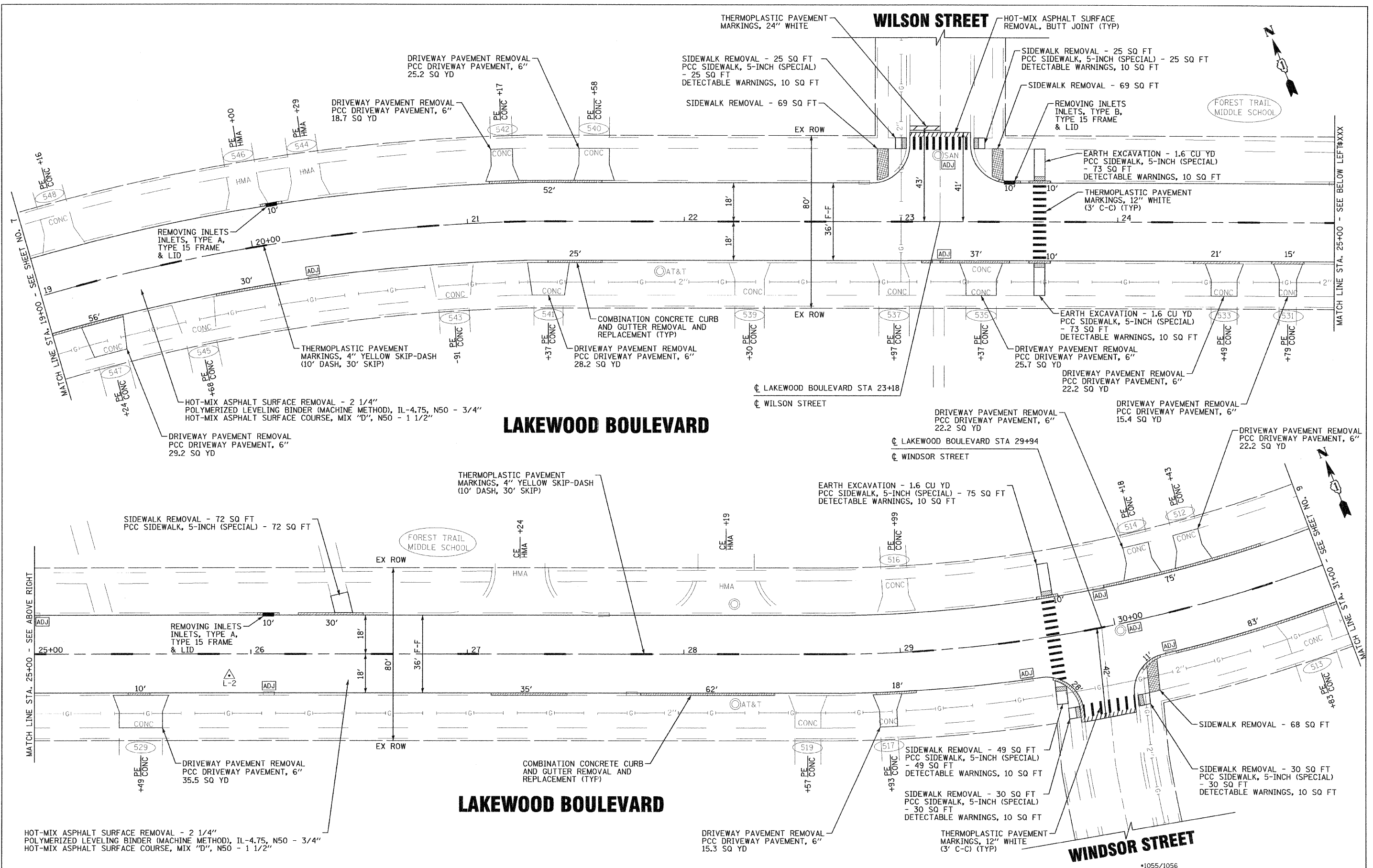
LAKWOOD BOULEVARD PLAN

SCALE: 1" = 20'

STA. 10+00 TO STA. 19+00

*1055/1056

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	7
CONTRACT NO. 63690				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT W-90039691			



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 CHECKED BY - JUF
 DATE - 10-26-11
 FILE - 091213-Plan2.sht

HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"

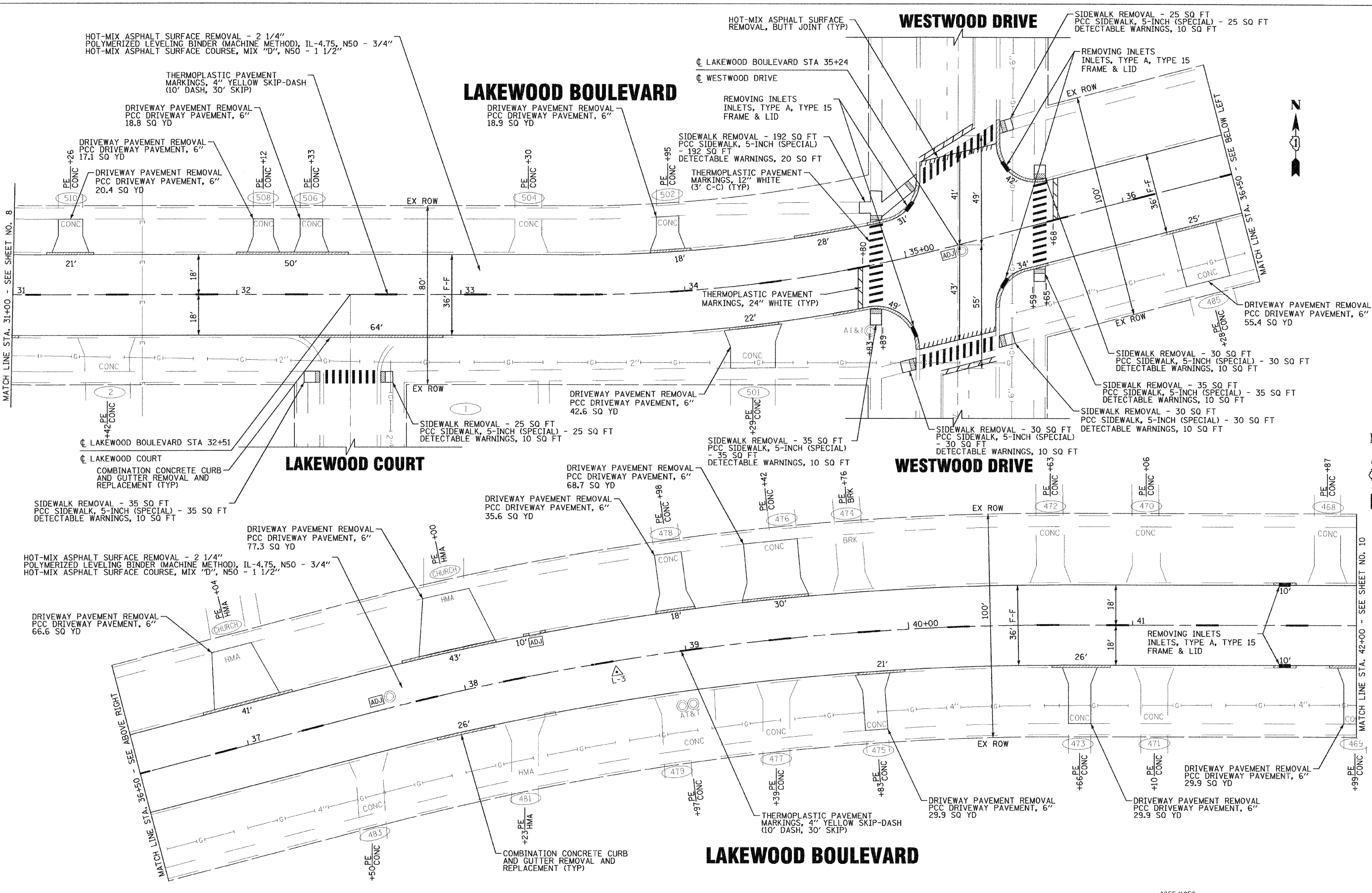
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CHECKED - JUF	REVISED -
DATE - 10-26-11	FILE - 091213-Plan2.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LAKWOOD BOULEVARD PLAN

SCALE: 1" = 20'
 STA. 19+00 TO STA. 31+00

*1055/1056	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		11-00095-00-RS	COOK	27	8
FED. ROAD DIST. NO.			ILLINOIS	CONTRACT NO. 63690	
			FED. AID PROJECT M-9003(959)		



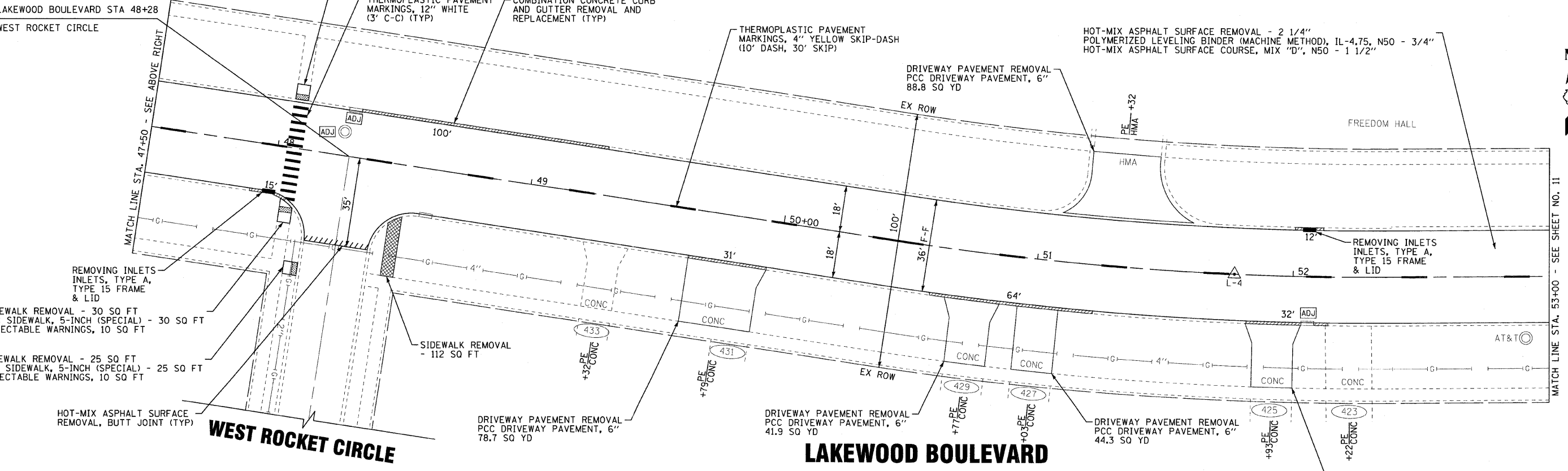
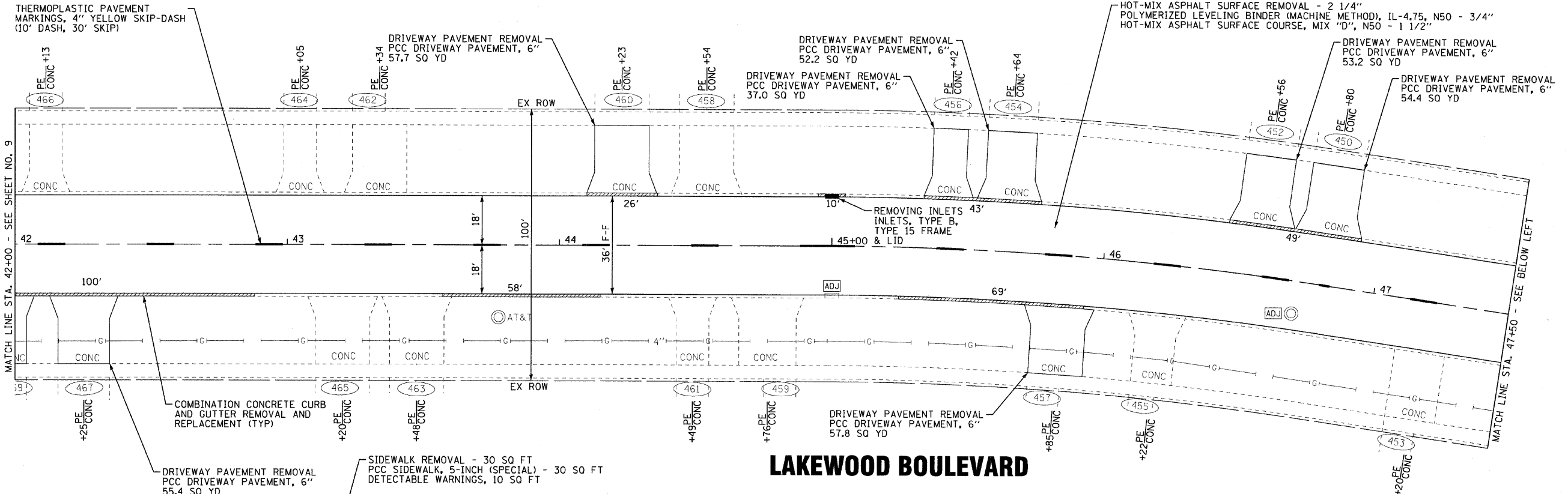
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DATE - 10-26-11	FILE - 091213-Plan3.shd

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LAKWOOD BOULEVARD PLAN	
SCALE: 1" = 20'	STA. 31+00 TO STA. 42+00

#1055/1056	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		11-00095-00-RS	COOK	27	9
FED. ROAD DIST. NO.			ILLINOIS/FED. AID PROJECT M-9003(969)		
CONTRACT NO. 63690					



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 DRAWN BY: KAR CHECKED BY: JJF DATE: 10-26-11
 FILE: 091213-Plan4.sh



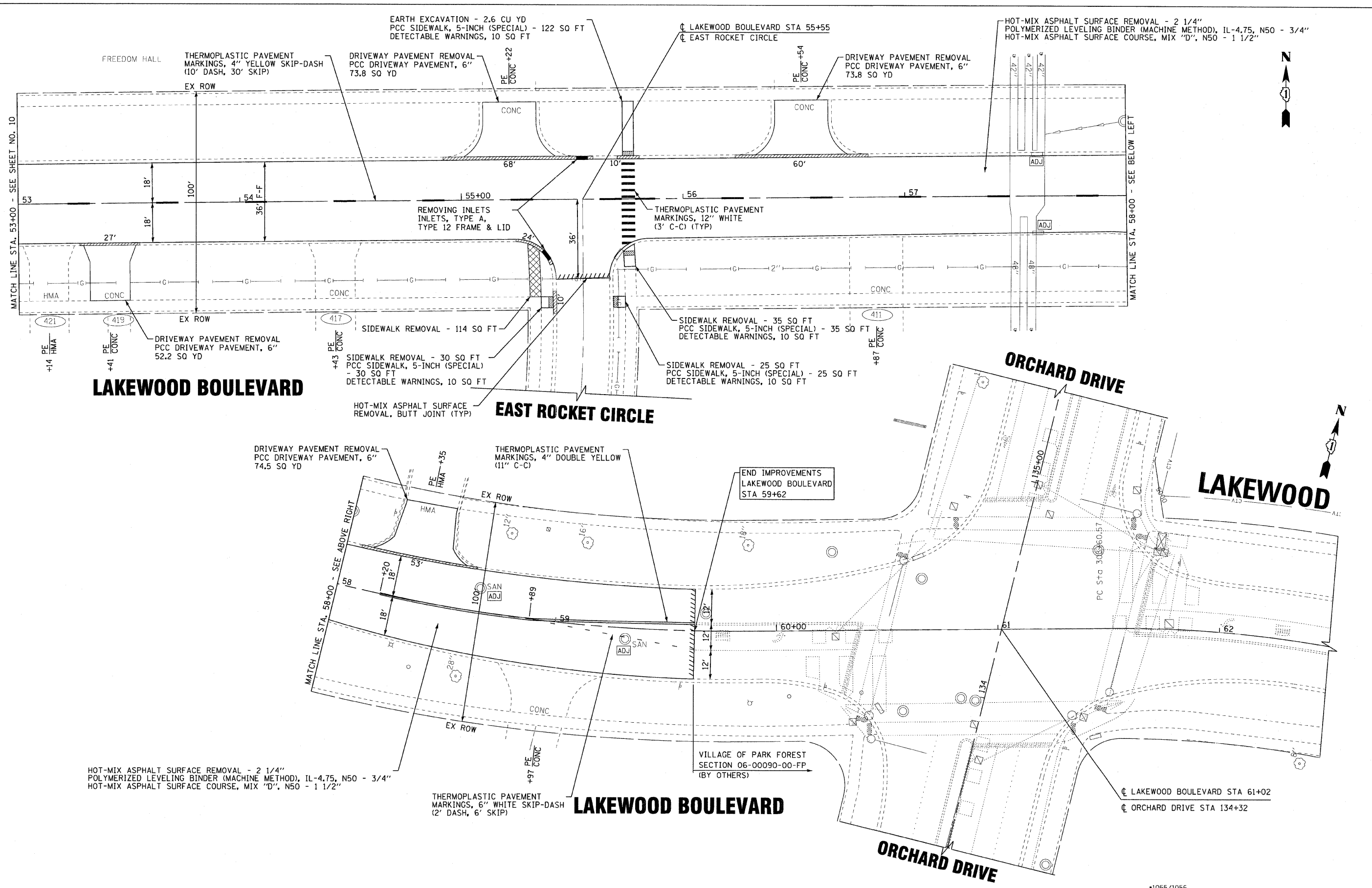
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DATE	- 10-26-11	FILE	- 091213-Plan4.sh

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LAKWOOD BOULEVARD PLAN

SCALE: 1" = 20' STA. 42+00 TO STA. 53+00

*1055/1056				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	10
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT M-900319691	
CONTRACT NO. 63690				



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 PROJECT: 11-00095-00-RS
 SHEET: 27 OF 11
 DATE: 10-26-11



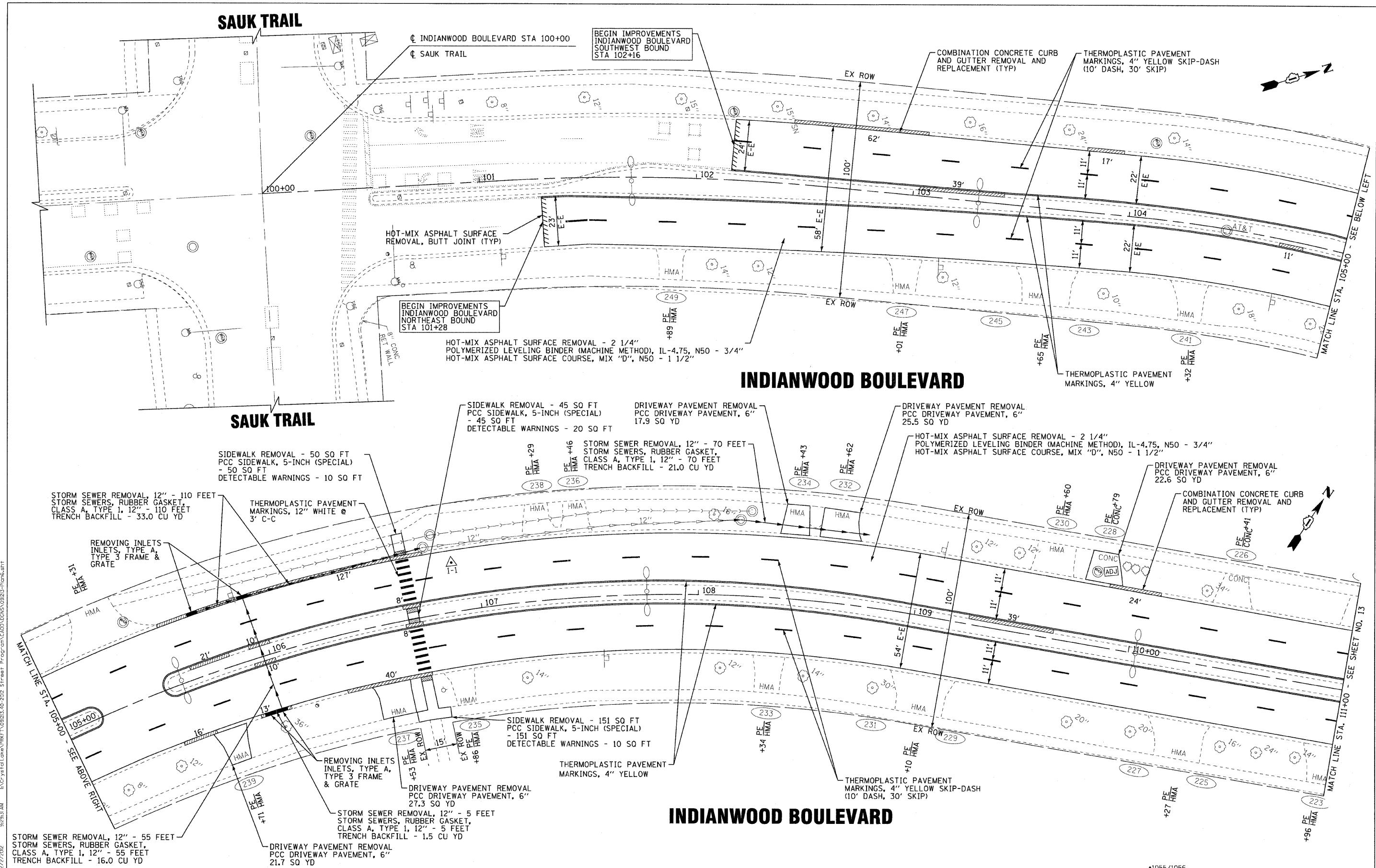
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CHECKED	- JJF	REVISED	-
DATE	- 10-26-11	FILE	- 091213-Plan5.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LAKWOOD BOULEVARD PLAN

SCALE: 1" = 20'
 STA. 53+00 TO STA. 62+50

*1055/1056		TOTAL SHEETS		SHEET NO.	
F.A.U. RTE.	SECTION	COUNTY	27	11	
	11-00095-00-RS	COOK	CONTRACT NO. 63690		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT M-90039691			



SAUK TRAIL

INDIANWOOD BOULEVARD

INDIANWOOD BOULEVARD

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STORM SEWER REMOVAL, 12" - 55 FEET
 STORM SEWERS, RUBBER GASKET,
 CLASS A, TYPE 1, 12" - 55 FEET
 TRENCH BACKFILL - 16.0 CU YD

SIDEWALK REMOVAL - 50 SQ FT
 PCC SIDEWALK, 5-INCH (SPECIAL)
 - 50 SQ FT
 DETECTABLE WARNINGS - 10 SQ FT

SIDEWALK REMOVAL - 45 SQ FT
 PCC SIDEWALK, 5-INCH (SPECIAL)
 - 45 SQ FT
 DETECTABLE WARNINGS - 20 SQ FT

SIDEWALK REMOVAL - 151 SQ FT
 PCC SIDEWALK, 5-INCH (SPECIAL)
 - 151 SQ FT
 DETECTABLE WARNINGS - 10 SQ FT

DRIVEWAY PAVEMENT REMOVAL
 PCC DRIVEWAY PAVEMENT, 6"
 17.9 SQ YD

DRIVEWAY PAVEMENT REMOVAL
 PCC DRIVEWAY PAVEMENT, 6"
 25.5 SQ YD

HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"

DRIVEWAY PAVEMENT REMOVAL
 PCC DRIVEWAY PAVEMENT, 6"
 22.6 SQ YD

STORM SEWER REMOVAL, 12" - 5 FEET
 STORM SEWERS, RUBBER GASKET,
 CLASS A, TYPE 1, 12" - 5 FEET
 TRENCH BACKFILL - 1.5 CU YD

DRIVEWAY PAVEMENT REMOVAL
 PCC DRIVEWAY PAVEMENT, 6"
 21.7 SQ YD



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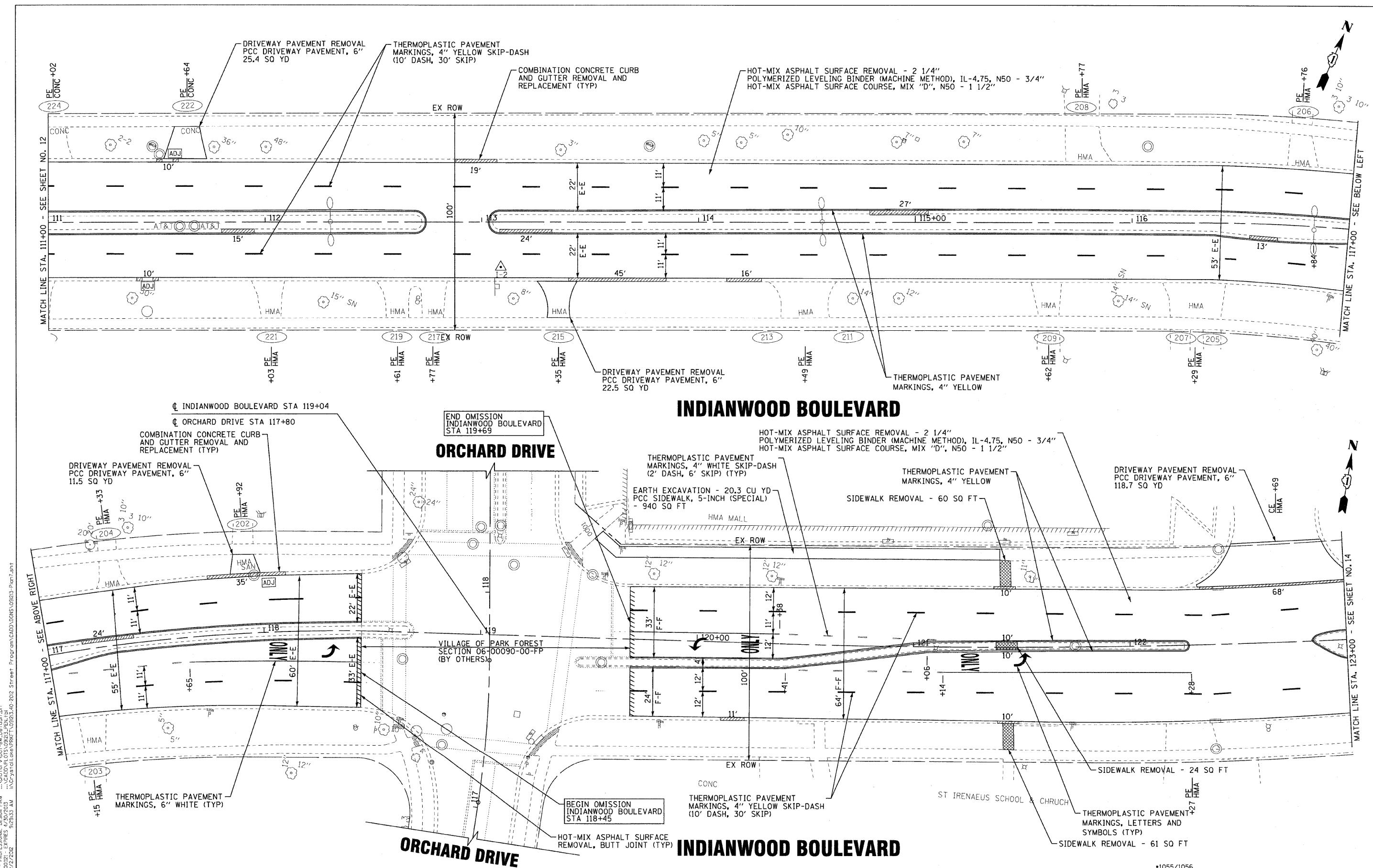
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

INDIANWOOD BOULEVARD PLAN

SCALE: 1" = 20' STA. 100+00 TO STA. 111+00

*1055/1056

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	12
FED. ROAD DIST. NO.				ILLINOIS FED. AID PROJECT M-9003(959)
CONTRACT NO. 63690				



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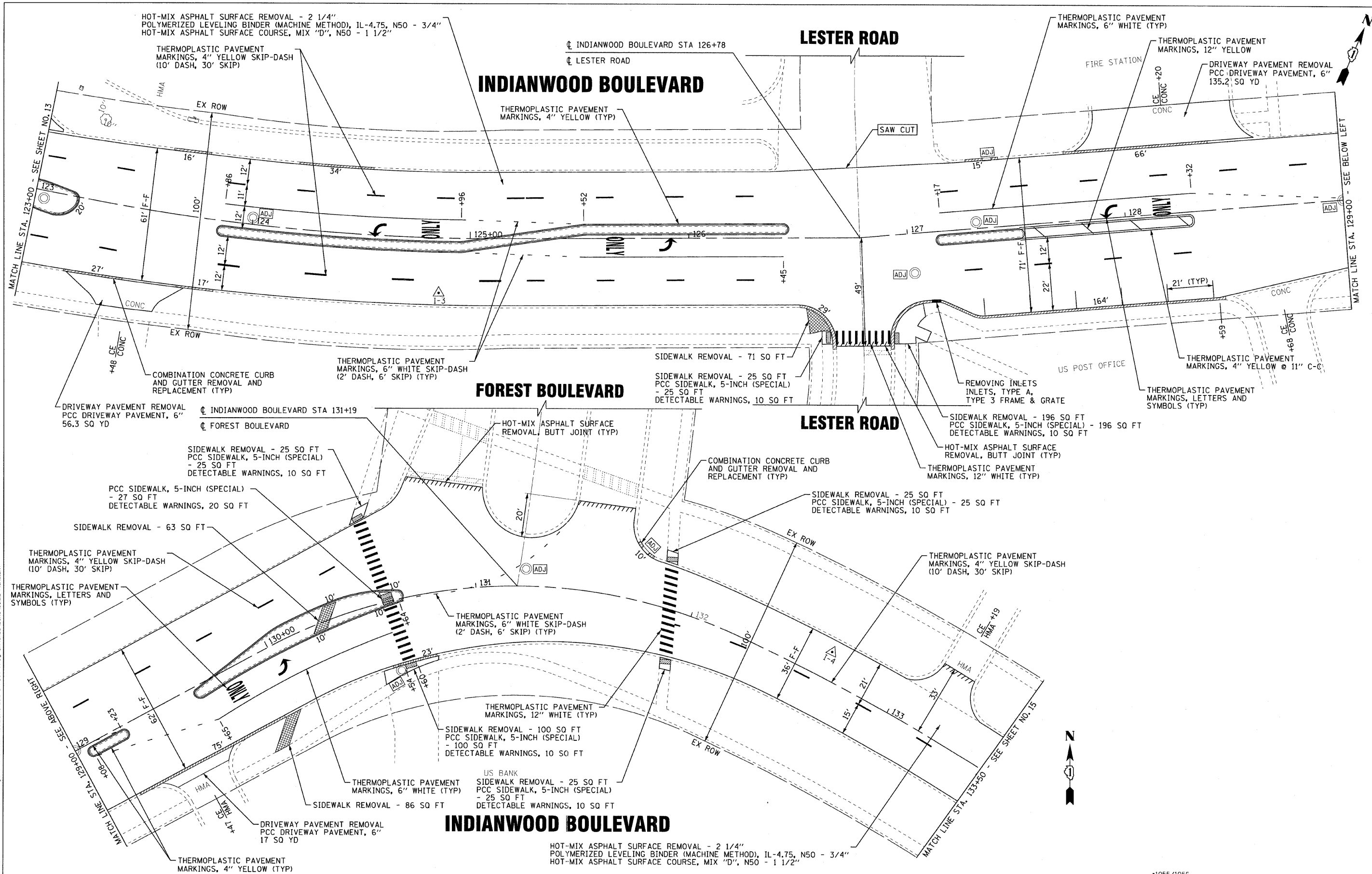
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDIANWOOD BOULEVARD PLAN

SCALE: 1" = 20' STA. 111+00 TO STA. 123+00

•1055/1056		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			11-00095-00-RS	COOK	27	13
		CONTRACT NO. 63690		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-900319691		



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

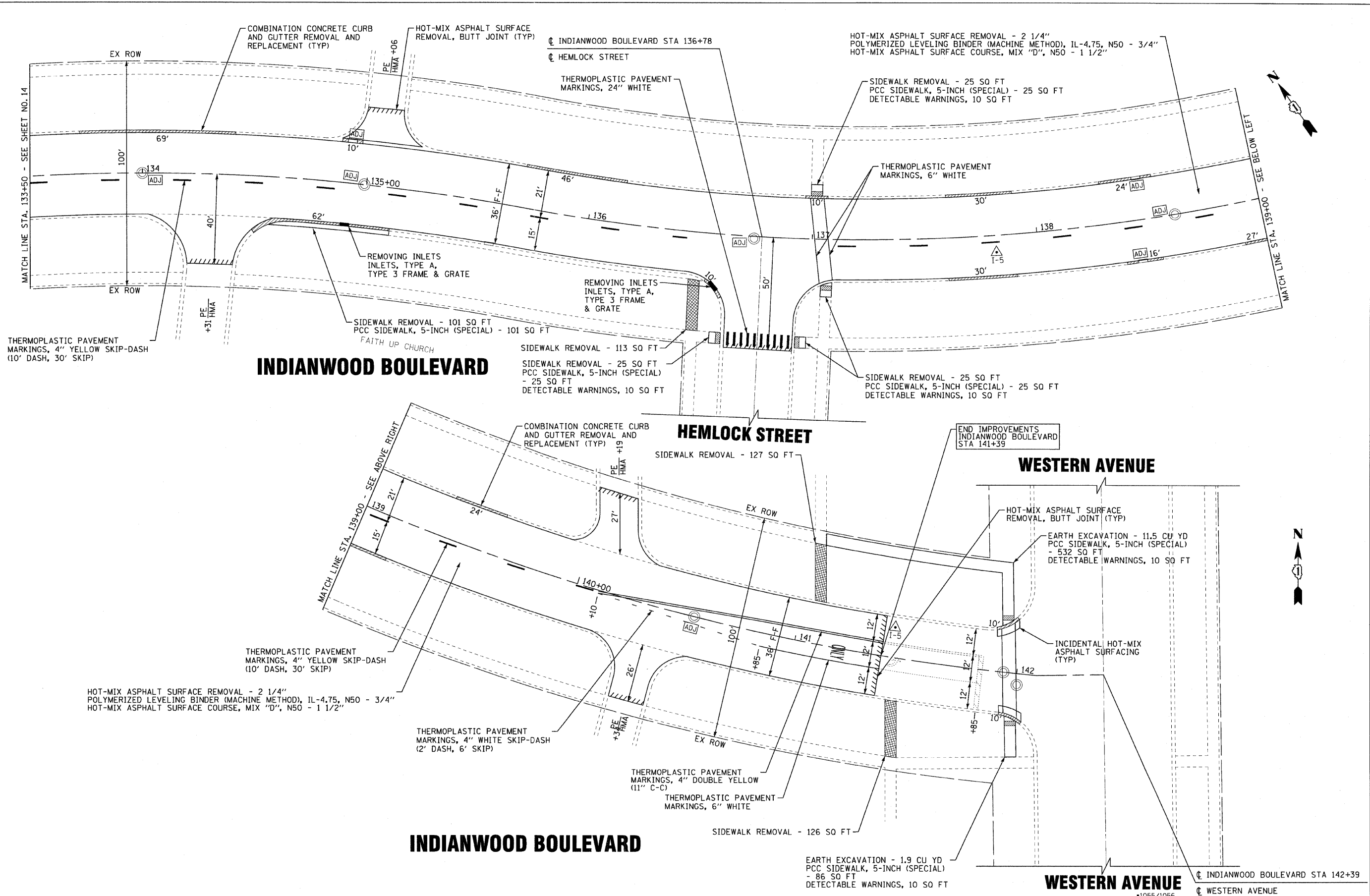
INDIANWOOD BOULEVARD PLAN

SCALE: 1" = 20'

STA. 123+00 TO STA. 133+50

*1055/1056

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	11-00095-00-RS	COOK	27	14
CONTRACT NO. 63690				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT M-9003(969)			



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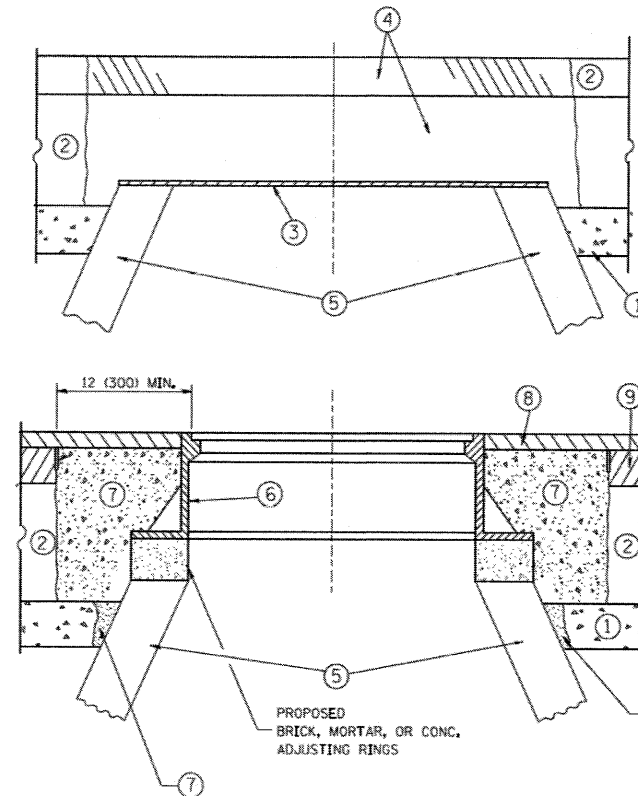
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

INDIANWOOD BOULEVARD PLAN

SCALE: 1" = 20'

STA. 133+50 TO STA. 142+39

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	15
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT M-9003(969)	
			CONTRACT NO. 63690	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

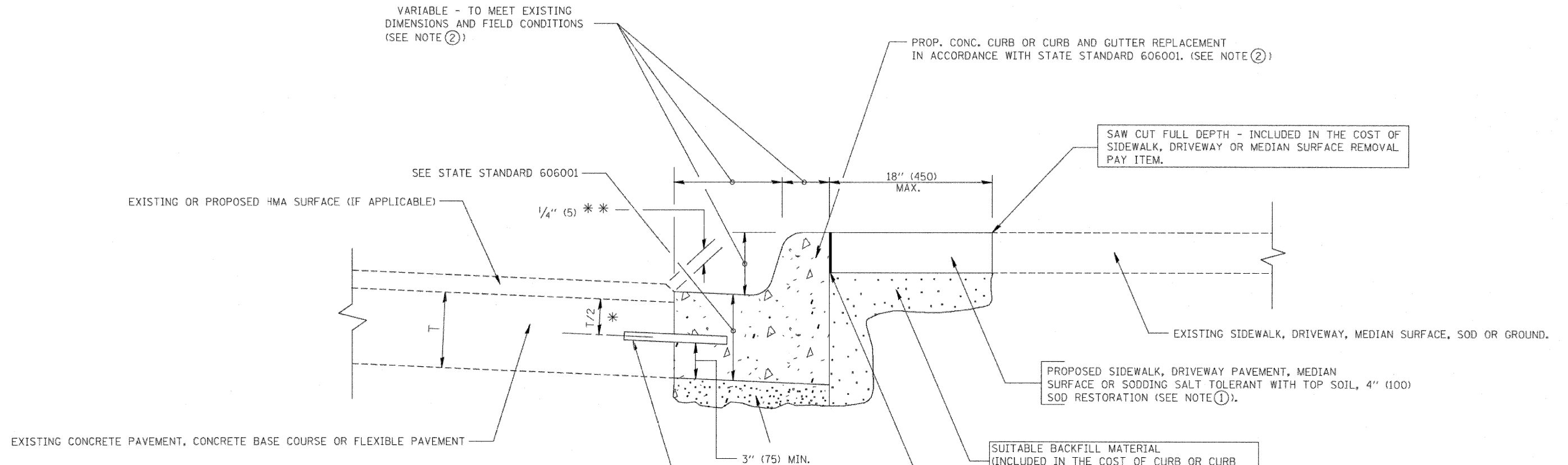
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN
*1055/1056

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	PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: NONE				SHEET NO. 1 OF 1 SHEETS				STA. TO STA.			
				DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING							

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	16
BD600-03 (BD-8)			CONTRACT NO. 63690	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(969)				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

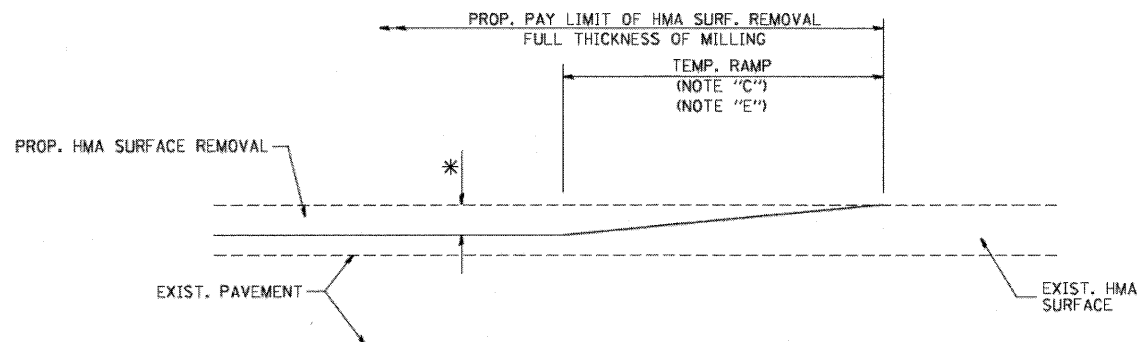
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

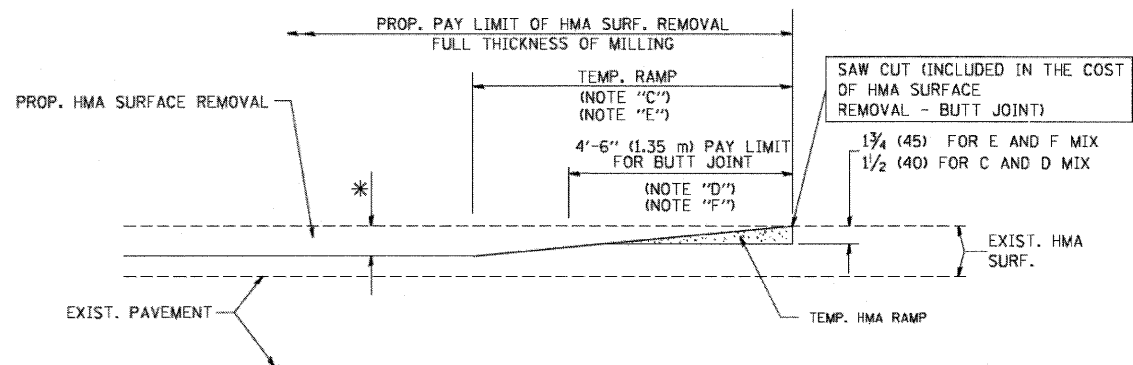
FILE NAME *	USER NAME * drvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\drvakosgn\0808315\bd24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			11-00095-00-RS	COOK	27	17	
PLOT SCALE - 50.000' / IN.	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)	CONTRACT NO. 63690			
PLOT DATE - 12/15/2009	DATE - 03-11-94					FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(969)				

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
•1055/1056



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

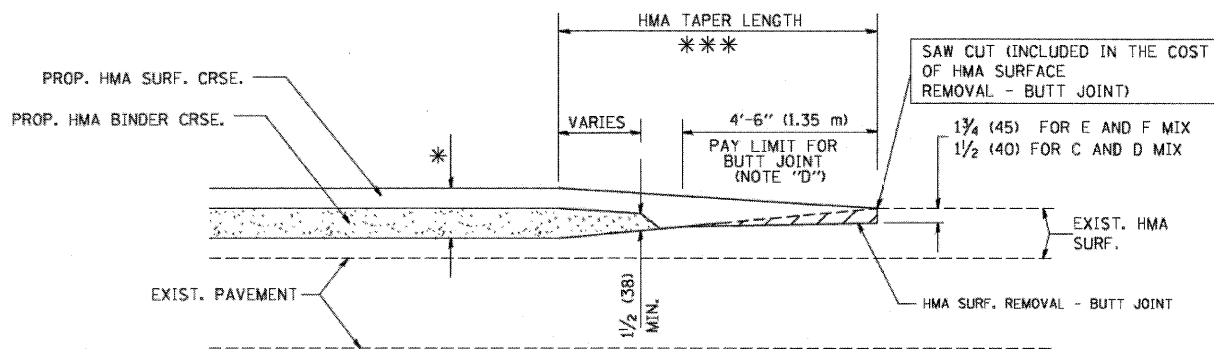
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

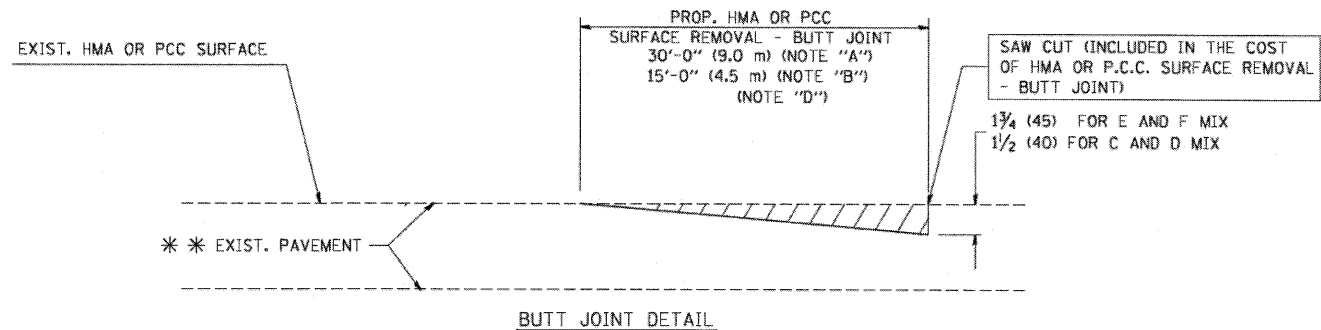
OPTION 2

TYPICAL TEMPORARY RAMP

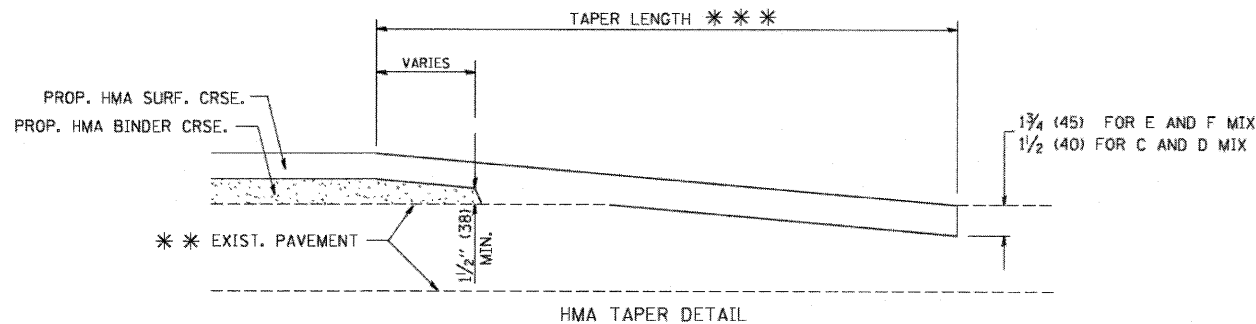


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

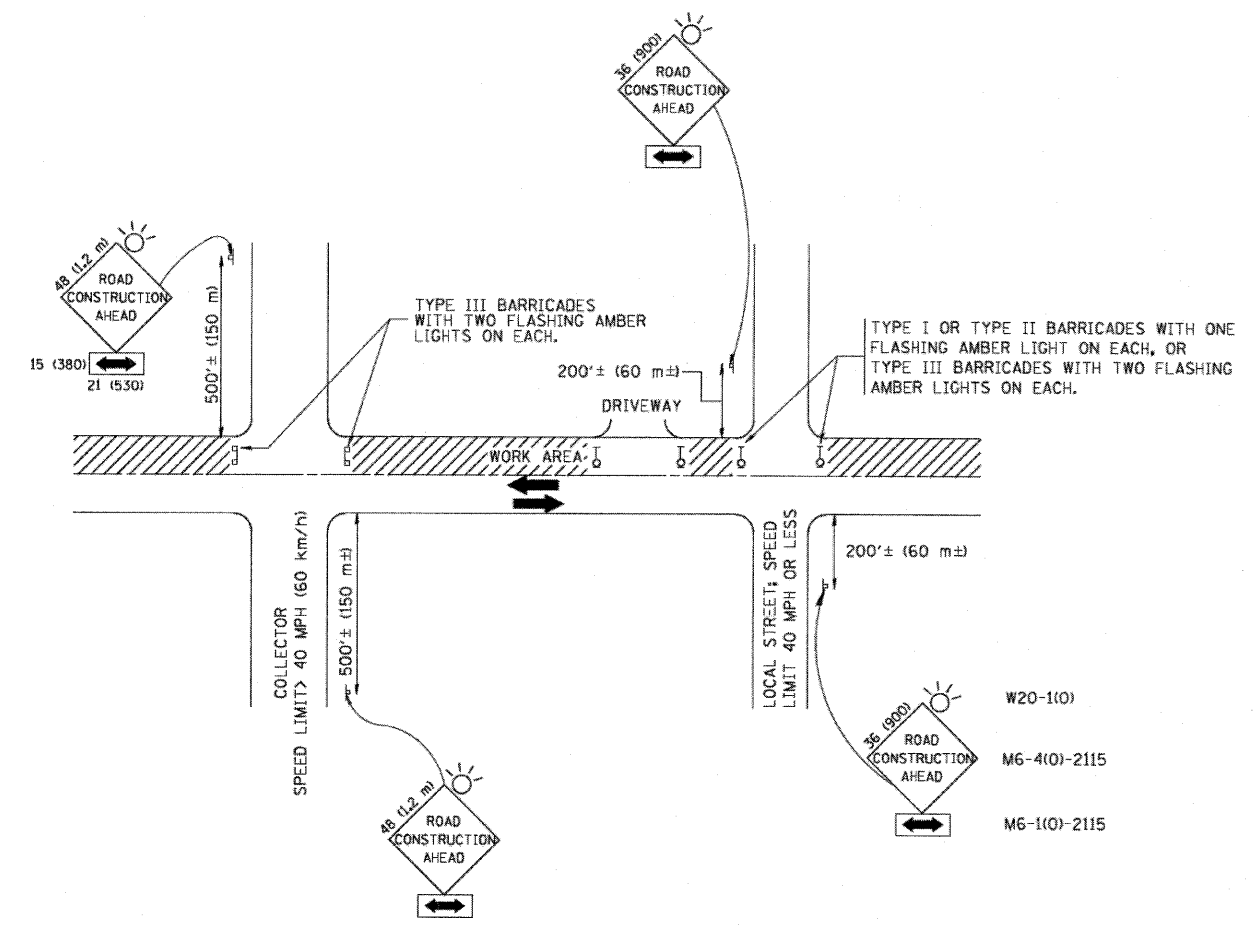
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / 1 IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	18
BD400-05 BD32		CONTRACT NO. 63690		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(969)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

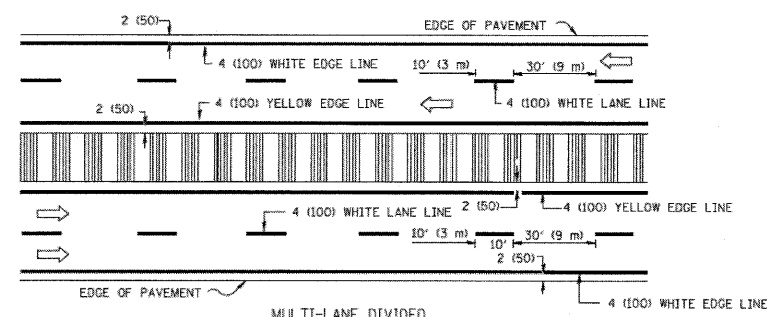
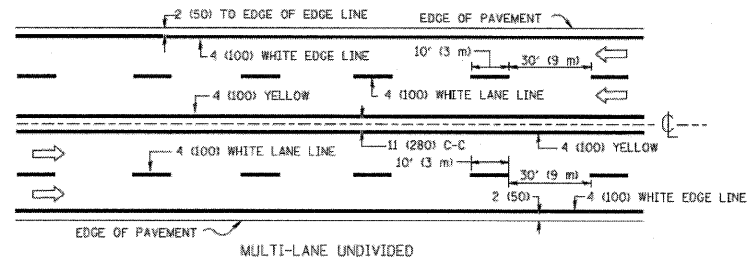
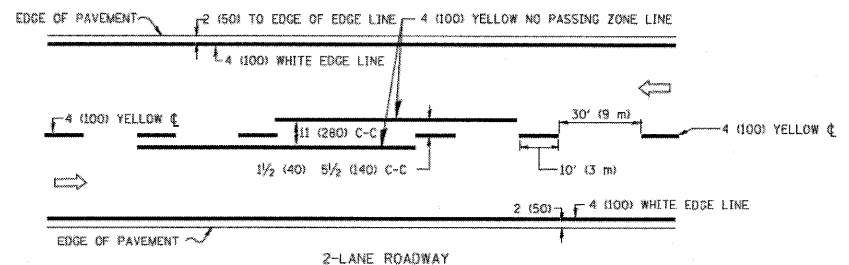
All dimensions are in millimeters (inches) unless otherwise shown.
 •1055/1056

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

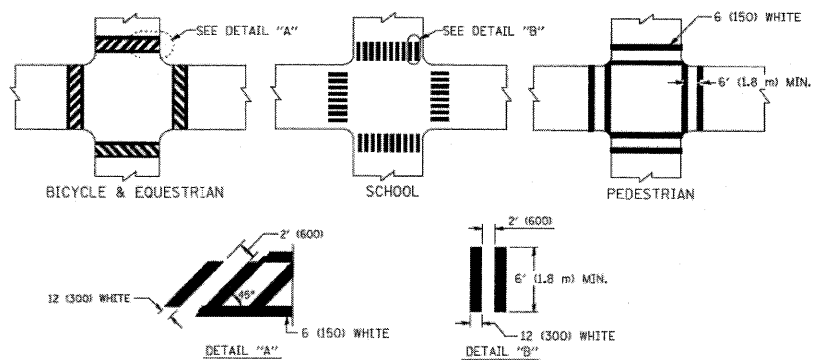
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	19
TC-10			CONTRACT NO. 63690	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(969)				

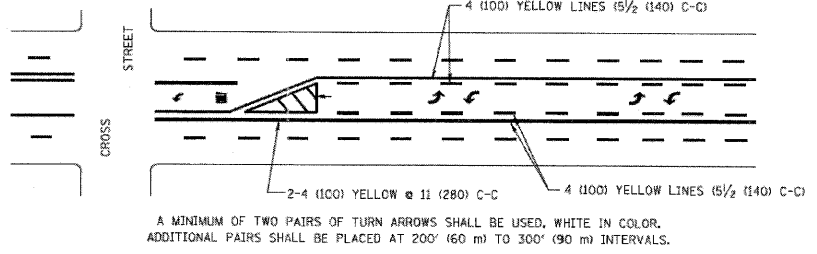
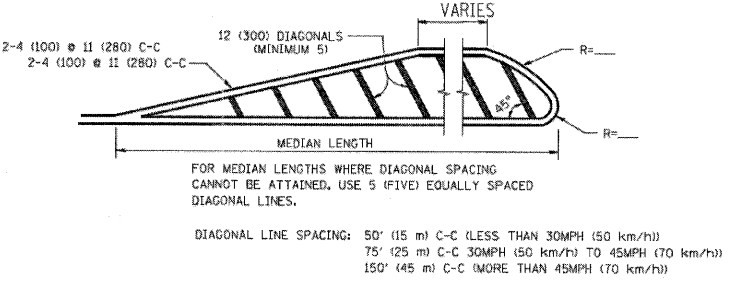
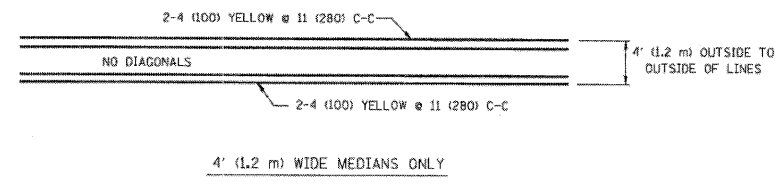


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

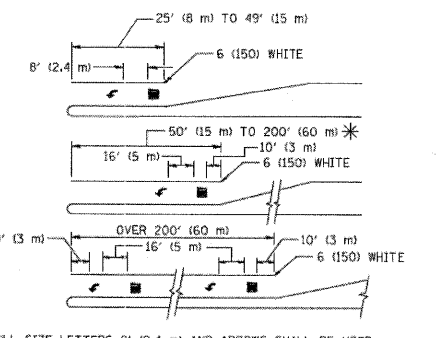
TYPICAL LANE AND EDGE LINE MARKING



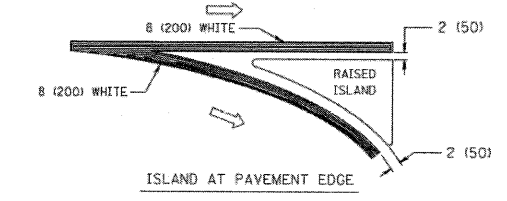
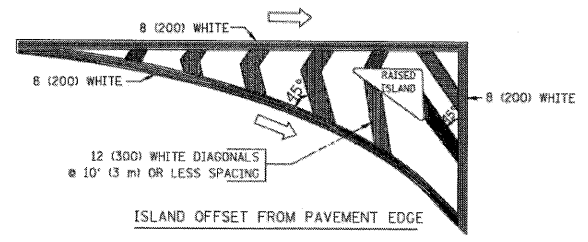
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.5 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

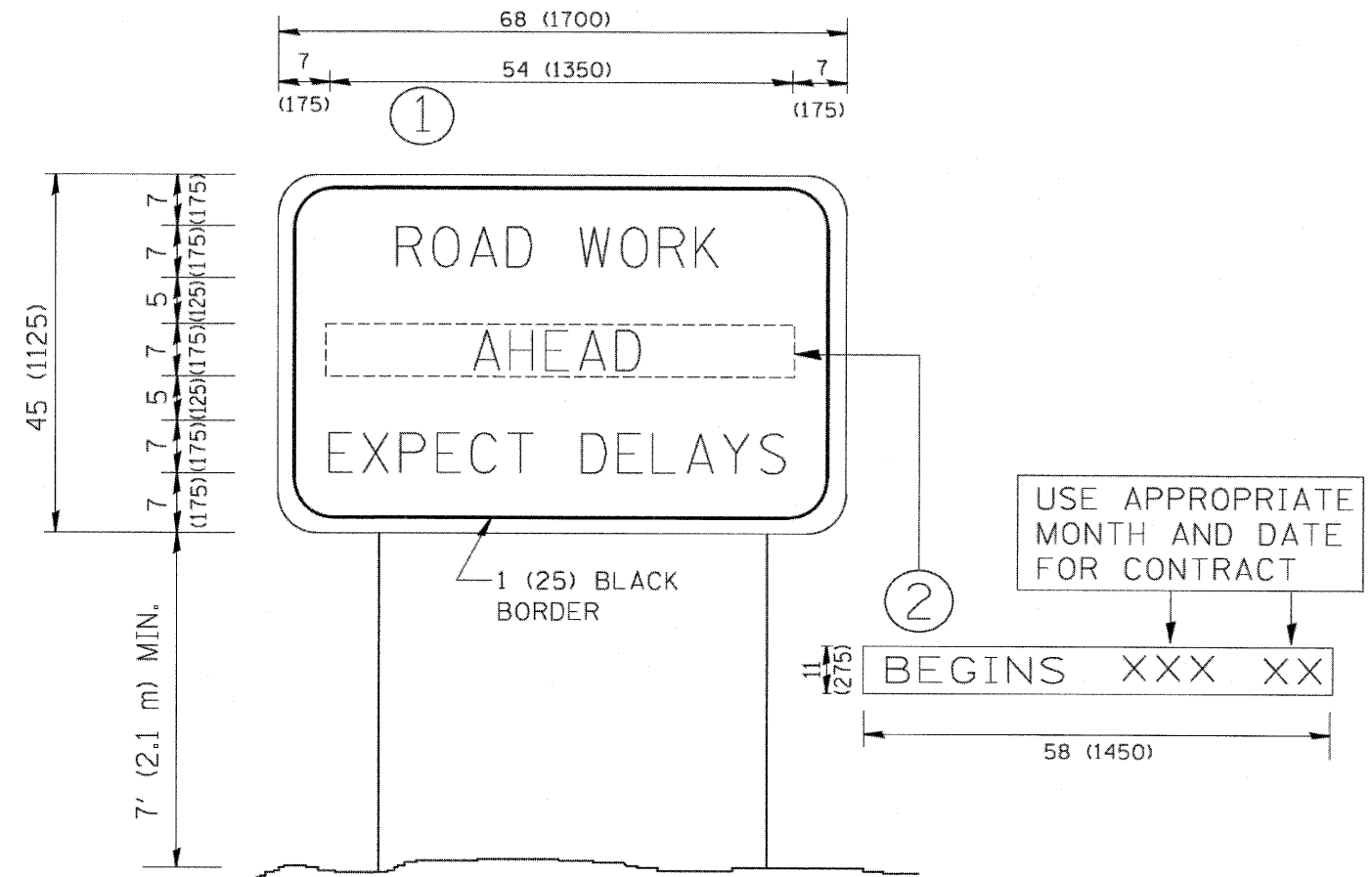
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drvakogrn	DESIGNED - EVERS	REVISED -T, RAMMACHER 10-27-94
		DRAWN -	REVISED -C, JUCIUS 09-09-09
		CHECKED -	REVISED -
PLOT SCALE = 50.0000' / IN.		DATE - 03-19-90	REVISED -
PLOT DATE = 9/9/2009			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		SECTION	
TYPICAL PAVEMENT MARKINGS		11-00095-00-RS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

*1055/1056			
F.A.U. RTE.	COUNTY	TOTAL SHEETS	SHEET NO.
	COOK	27	20
TC-13		CONTRACT NO. 63690	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(969)			



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME * w:\diststd\22x34\to22.dgn	USER NAME * goglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
	PLOT SCALE * 50.000 / IN.	DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT DATE * 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

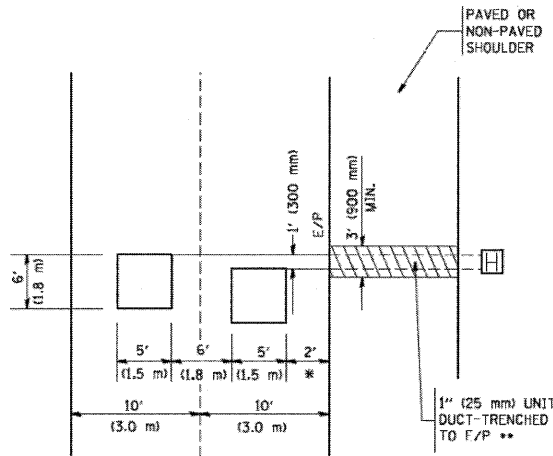
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

*1055/1056

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	21
TC-22			CONTRACT NO. 63690	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(969)				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

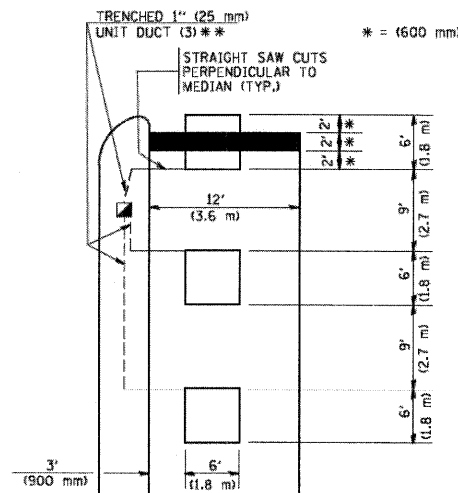


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

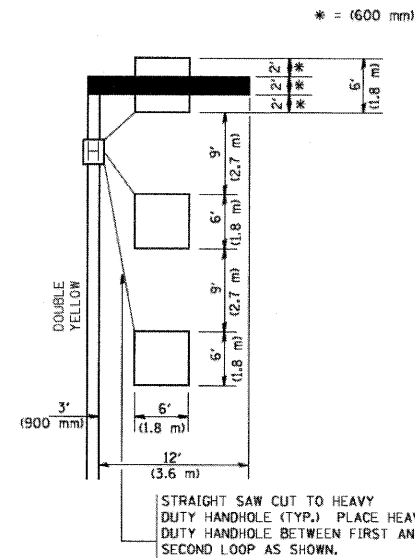
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

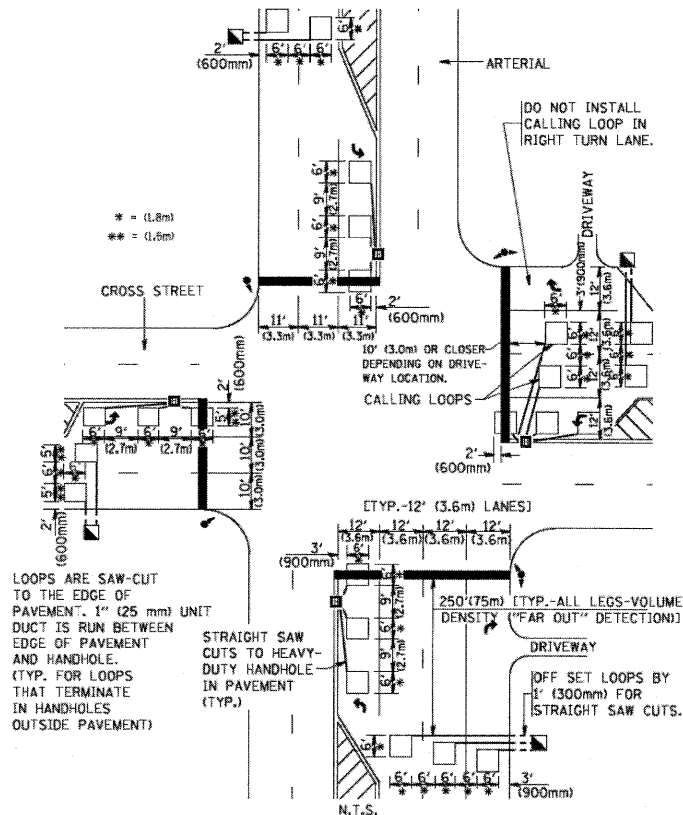
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



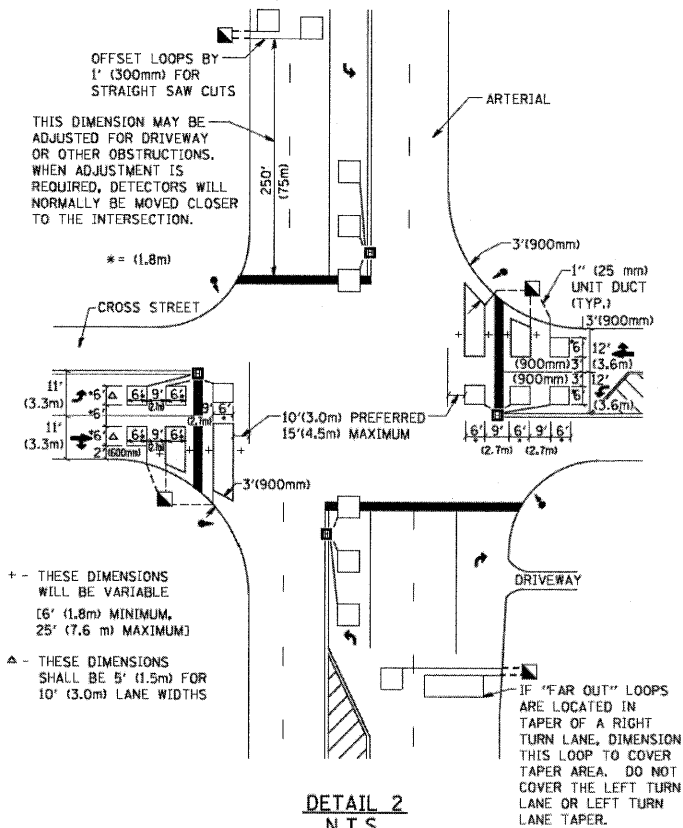
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = goglianob	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT 1 - DETECTOR LOOP INSTALLATION	
DETAILS FOR ROADWAY RESURFACING	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

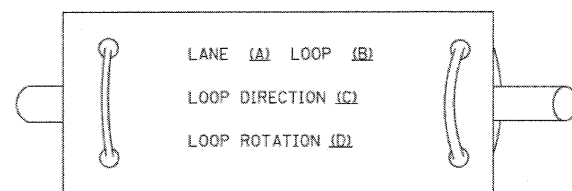
1055/1056

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	22
TS-07		CONTRACT NO. 63690		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(969)				

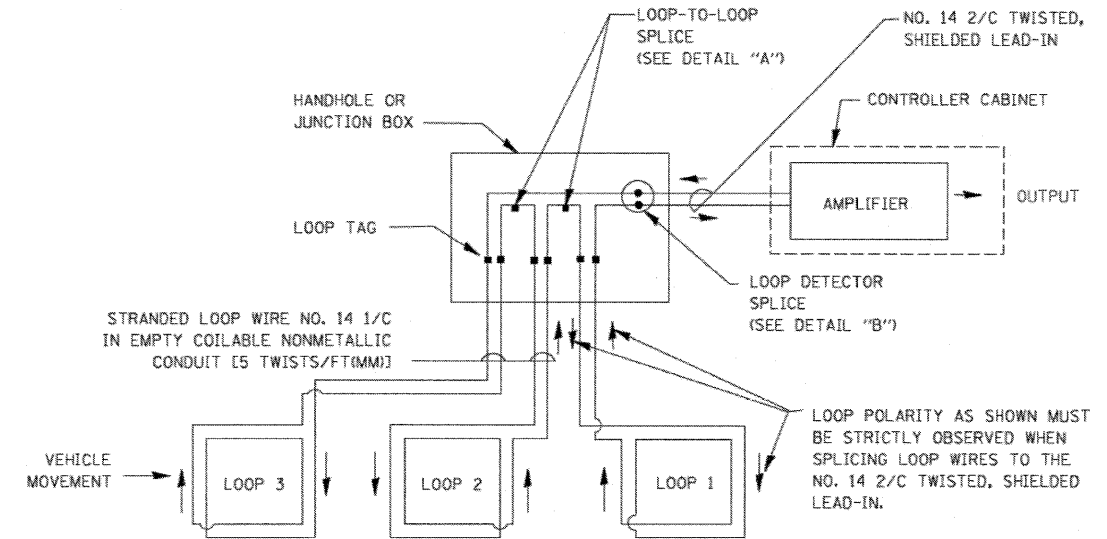
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SFAI ANT MANUFACTURFR RQUIREMFMENTS. THE DTFECTOR WIRF SHAI I BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

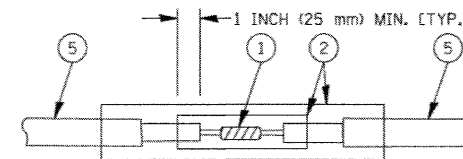


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

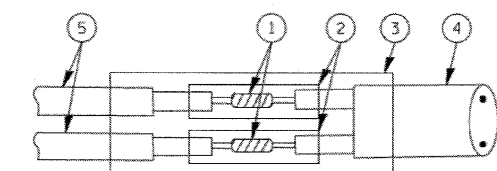


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

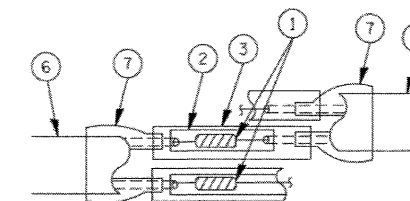


DETAIL "A"
LOOP-TO-LOOP SPLICE

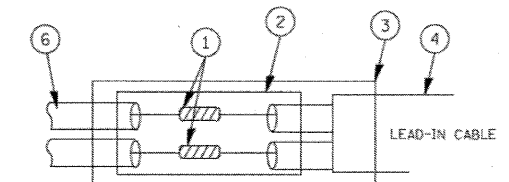


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



PRE-FORMED LOOP
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PRE-FORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

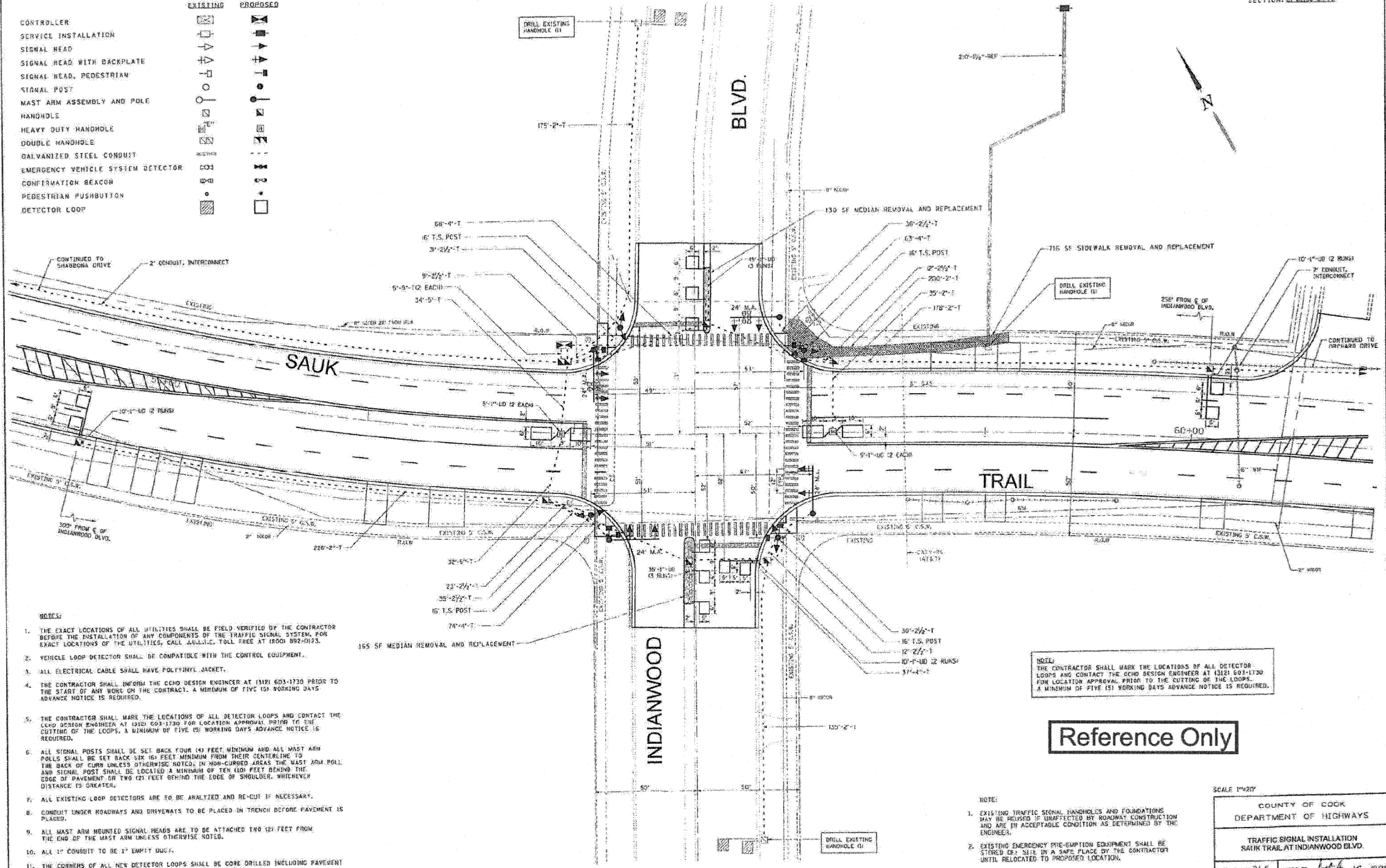
FILE NAME *	USER NAME * karthephuaybo	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\p\work\PWEDOT\KANTHAPHIXAYBC\00112614\tr\offic\legend.v7.dgn	DRAWN - BCK	REVISED -				11-00095-00-RS	COOK	27	23	
PLOT SCALE = 20.0000' / IN.	CHECKED - DAD	REVISED -				CONTRACT NO. 63690				
PLOT DATE = 10/6/2009	DATE - 10/28/09	REVISED -				FED. ROAD DIST. NO. ILLINOIS/FED. AID PROJECT M-9003(969)				

*1055/1056

SCALE: SHEET NO. 1 OF 6 SHEETS STA. TO STA.

LEGEND

	EXISTING	PROPOSED
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
GALVANIZED STEEL CONDUIT		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
PEDESTRIAN PUSHBUTTON		
DETECTOR LOOP		



- NOTES:**
1. THE EXACT LOCATIONS OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR EXACT LOCATIONS OF THE UTILITIES, CALL 312-625-7000 TOLL FREE AT 1-800-892-0123.
 2. VEHICLE LOOP DETECTOR SHALL BE COMPATIBLE WITH THE CONTROL EQUIPMENT.
 3. ALL ELECTRICAL CABLE SHALL HAVE POLYVINYL JACKET.
 4. THE CONTRACTOR SHALL INFORM THE CCHD DESIGN ENGINEER AT 312-603-1730 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
 5. THE CONTRACTOR SHALL MARK THE LOCATIONS OF ALL DETECTOR LOOPS AND CONTACT THE CCHD DESIGN ENGINEER AT 312-603-1730 FOR LOCATION APPROVAL PRIOR TO THE CUTTING OF THE LOOPS. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
 6. ALL SIGNAL POSTS SHALL BE SET BACK FOUR (4) FEET MINIMUM AND ALL MAST ARM POLLS SHALL BE SET BACK SIX (6) FEET MINIMUM FROM THEIR CENTERLINE TO THE BACK OF CURB UNLESS OTHERWISE NOTED. IN NON-CURBED AREAS THE MAST ARM POLL AND SIGNAL POST SHALL BE LOCATED A MINIMUM OF TEN (10) FEET BEHIND THE EDGE OF PAVEMENT OR TWO (2) FEET BEHIND THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER.
 7. ALL EXISTING LOOP DETECTORS ARE TO BE ANALYZED AND RE-CUT IF NECESSARY.
 8. CONDUIT UNDER ROADWAYS AND DRIVEWAYS TO BE PLACED IN TRENCH BEFORE PAVEMENT IS PLACED.
 9. ALL MAST ARM MOUNTED SIGNAL HEADS ARE TO BE ATTACHED TWO (2) FEET FROM THE END OF THE MAST ARM UNLESS OTHERWISE NOTED.
 10. ALL 1" CONDUIT TO BE 1" EMPTY DUCT.
 11. THE CORNERS OF ALL NEW DETECTOR LOOPS SHALL BE CORE DRILLED INCLUDING PAVEMENT JOINTS AND CRACKS.
 12. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE THE EXISTING TRAFFIC SIGNAL AND STREET LIGHTING CONDUIT AND CABLE. THIS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

NOTE:
THE CONTRACTOR SHALL MARK THE LOCATIONS OF ALL DETECTOR LOOPS AND CONTACT THE CCHD DESIGN ENGINEER AT 312-603-1730 FOR LOCATION APPROVAL PRIOR TO THE CUTTING OF THE LOOPS. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.

Reference Only

- NOTE:**
1. EXISTING TRAFFIC SIGNAL HANDHOLES AND FOUNDATIONS MAY BE REUSED IF UNAFFECTED BY ROADWAY CONSTRUCTION AND ARE IN ACCEPTABLE CONDITION AS DETERMINED BY THE ENGINEER.
 2. EXISTING EMERGENCY PRE-EMPTION EQUIPMENT SHALL BE STORED ON-SITE IN A SAFE PLACE BY THE CONTRACTOR UNTIL RELOCATED TO PROPOSED LOCATION.

SCALE 1"=20'

COUNTY OF COOK
DEPARTMENT OF HIGHWAYS

TRAFFIC SIGNAL INSTALLATION
SAUK TRAIL AT INDIANWOOD BLVD.

DESIGNED	R.L.S.	APPROVED	
DRAWN	F.A.C.		
CHECKED	P.L.B.		

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 STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM
 1100 W. WASHINGTON STREET, SUITE 200
 CHICAGO, ILLINOIS 60606-4202
 TEL: 312.467.1000 FAX: 312.467.1001
 WWW.BAXTERWOODMAN.COM



DESIGNED	- TMS	REVISED	-
DRAWN	- KAR	REVISED	-
CHECKED	- JJF	REVISED	-
DATE	- 10-26-11	FILE	- 091213-CCHD SIGNAL PLANS.sht

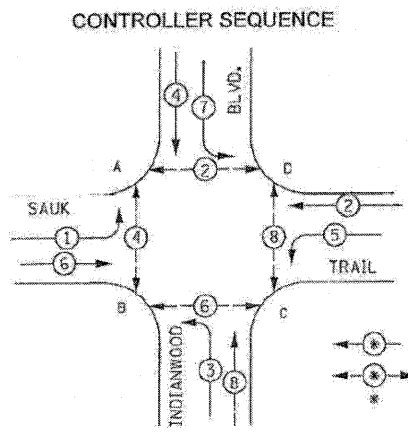
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

COOK COUNTY HIGHWAY DEPARTMENT
SIGNAL PLANS - REFERENCE ONLY

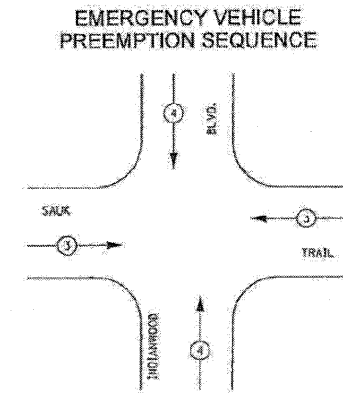
SCALE: NONE STA. TO STA.

*1055/1056

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	11-00095-00-RS	COOK	27	24
CONTRACT NO. 63690				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT M-9003(969)			

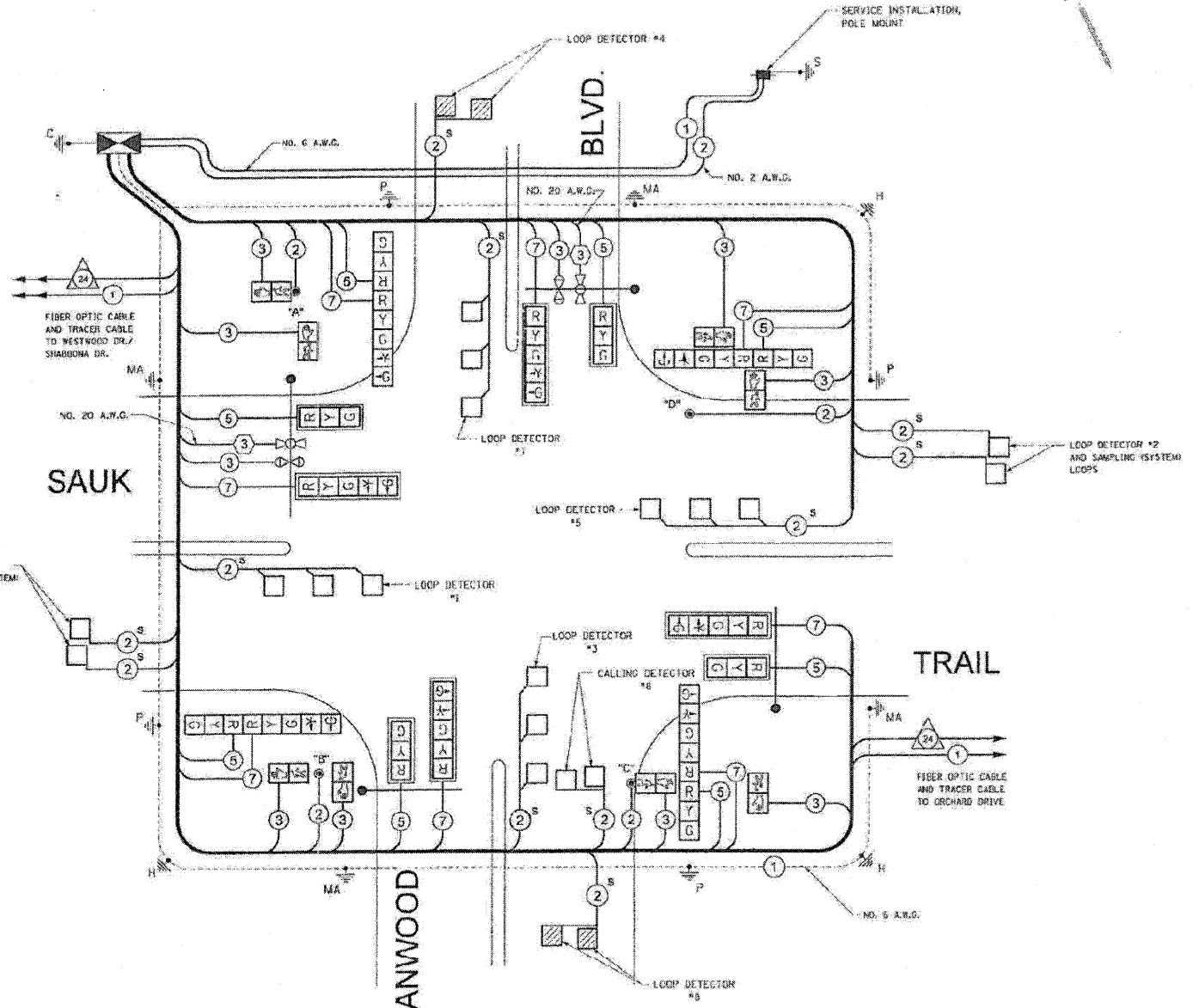
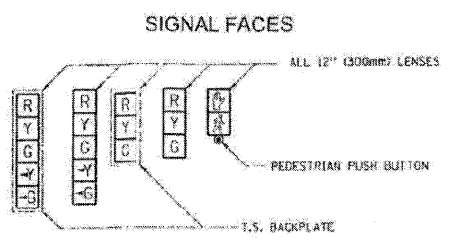


PHASE DESIGNATION DIAGRAM
DUAL ENTRY - ALL LEGS PROTECTED / PERMITTED LEFT TURN PHASING



PROPOSED EMERGENCY VEHICLE PREEMPTOR	
EMERGENCY VEHICLE PREEMPTOR	3
MOVEMENT	

- NOTE:**
- THIS IS A SYSTEM GROUND THAT SHALL INTERCONNECT ALL GROUND RODS WITH NO. 6 L/VF COPPER AWG.
 - THE NEUTRAL AND GROUND SHALL BE TIED AT THE SERVICE INSTALLATION BUT SEPARATE AT THE TRAFFIC CABINET.



CABLE PLAN
NOT TO SCALE

CABLE PLAN LEGEND

	PROPOSED	EXISTING
CONTROLLER CABINET	[Symbol]	[Symbol]
SERVICE INSTALLATION	[Symbol]	[Symbol]
12\"/>		

DENOTES NUMBER OF CONDUCTORS ALL CABLE NO. 18 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE TWISTED AND SHIELDED.
GROUND ROD AT HANDHOLE BR, DOUBLE HANDHOLE BR, OR CONTROL LER TO.
GROUND ROD AT POST EP, OR MAST ARM POLE IMAL.
GROUND ROD AT ELECTRIC SERVICE INSTALLATION.
FIBER OPTIC CABLE, NO. 52-5/125 MULTIMODE 12 FIBERS AND SINGLE MODE 12 FIBERS

NOTE:
THE CONTROLLER SHALL BE AN EGNOLITE CONTROLLER TO MATCH THE EXISTING SYSTEM.

Reference Only

COUNTY OF COOK DEPARTMENT OF HIGHWAYS	
SAUK TRAIL AT INDIANWOOD BLVD. CABLE DIAGRAM	
COMPUTER DRAWN CHECKED	P.L.N. P.L.N. P.L.N.

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 DATE: 10/26/11



DESIGNED - TMS	REVISED -
DRAWN - KAR	REVISED -
CHECKED - JJF	REVISED -
DATE - 10-26-11	FILE - 091213-CCHD SIGNAL PLANS.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

COOK COUNTY HIGHWAY DEPARTMENT
SIGNAL PLANS - REFERENCE ONLY

SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
11-00095-00-RS		COOK	27	25
FED. ROAD DIST. NO.		ILLINOIS/FED. AID PROJECT	M-90039691	

SCALE: NONE STA. TO STA.

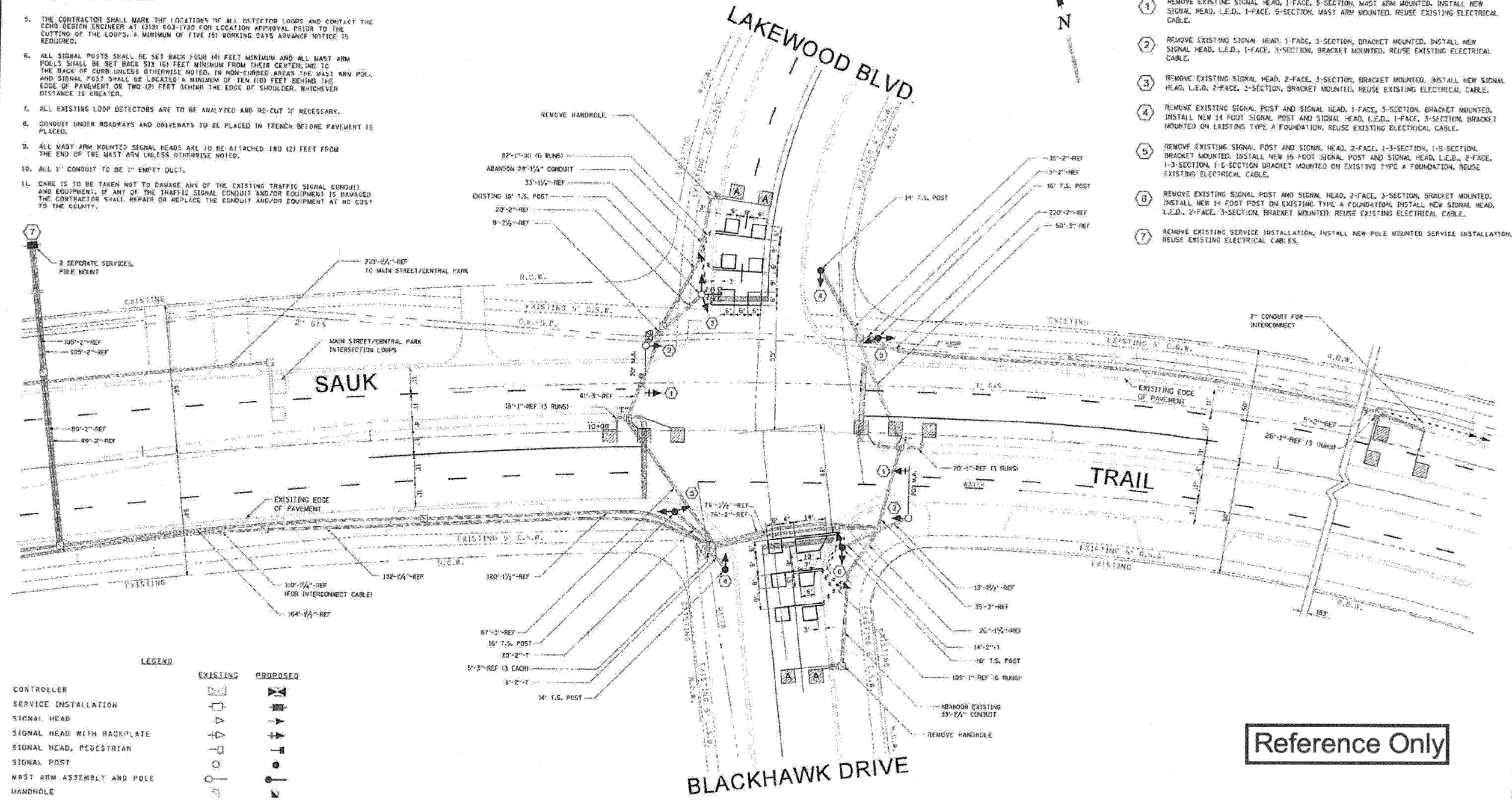
COUNTY HIGHWAY	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
011			

SECTION: 01-0095-00-RS

- NOTES:**
- THE EXACT LOCATIONS OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR EXACT LOCATIONS OF THE UTILITIES, CALL J.D.L.S.E. TOLL FREE AT 1800 892-0282.
 - VEHICLE LOOP DETECTOR SHALL BE COMPATIBLE WITH THE CONTROL EQUIPMENT.
 - ALL ELECTRICAL CABLE SHALL HAVE POLYVINYL JACKET.
 - THE CONTRACTOR SHALL INFORM THE CCHD DESIGN ENGINEER AT (312) 603-1130 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
 - THE CONTRACTOR SHALL MARK THE LOCATIONS OF ALL DETECTOR LOOPS AND CONTACT THE CCHD DESIGN ENGINEER AT (312) 603-1130 FOR LOCATION APPROVAL PRIOR TO THE CUTTING OF THE LOOPS. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
 - ALL SIGNAL POSTS SHALL BE SET BACK FOUR (4) FEET MINIMUM AND ALL MAST ARM POLLS SHALL BE SET BACK SIX (6) FEET MINIMUM FROM THEIR CENTERLINE TO THE BACK OF CURB UNLESS OTHERWISE NOTED. IN NON-CURBED AREAS THE MAST ARM POLL AND SIGNAL POST SHALL BE LOCATED A MINIMUM OF TEN (10) FEET BEHIND THE EDGE OF PAVEMENT OR TWO (2) FEET BEHIND THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER.
 - ALL EXISTING LOOP DETECTORS ARE TO BE ANALYZED AND RE-CUT IF NECESSARY.
 - CONDUIT UNDER ROADWAYS AND DRIVEWAYS TO BE PLACED IN TRENCH BEFORE PAVEMENT IS PLACED.
 - ALL MAST ARM MOUNTED SIGNAL HEADS ARE TO BE ATTACHED TWO (2) FEET FROM THE END OF THE MAST ARM UNLESS OTHERWISE NOTED.
 - ALL 1" CONDUIT TO BE 1" EMPTY DUCT.
 - CARE IS TO BE TAKEN NOT TO DAMAGE ANY OF THE EXISTING TRAFFIC SIGNAL CONDUIT AND EQUIPMENT. IF ANY OF THE TRAFFIC SIGNAL CONDUIT AND/OR EQUIPMENT IS DAMAGED THE CONTRACTOR SHALL REPAIR OR REPLACE THE CONDUIT AND/OR EQUIPMENT AT NO COST TO THE COUNTY.

- THE CORNERS OF ALL NEW DETECTOR LOOPS SHALL BE CORE-DRILLED INCLUDING PAVEMENT JOINTS AND CRACKS.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAKE SURE ALL NEW AND EXISTING TRAFFIC SIGNAL POSTS ON EXISTING OR NEW CONCRETE FOUNDATIONS ARE STRAIGHT UP. THIS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. SHIMS ARE NOT ACCEPTABLE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE THE EXISTING TRAFFIC SIGNAL AND STREET LIGHTING CONDUIT AND CABLE. THIS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

- NOTES:**
- REMOVE EXISTING SIGNAL HEAD, 1-FACE, 5-SECTION, MAST ARM MOUNTED. INSTALL NEW SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED. INSTALL NEW SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, BRACKET MOUNTED. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED. INSTALL NEW SIGNAL HEAD, L.E.D., 2-FACE, 3-SECTION, BRACKET MOUNTED. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL POST AND SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED. INSTALL NEW 14 FOOT SIGNAL POST AND SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, BRACKET MOUNTED ON EXISTING TYPE A FOUNDATION. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL POST AND SIGNAL HEAD, 2-FACE, 1-3-SECTION, 1-5-SECTION, BRACKET MOUNTED. INSTALL NEW 16 FOOT SIGNAL POST AND SIGNAL HEAD, L.E.D., 2-FACE, 1-3-SECTION, 1-5-SECTION BRACKET MOUNTED ON EXISTING TYPE A FOUNDATION. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL POST AND SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED. INSTALL NEW 14 FOOT POST ON EXISTING TYPE A FOUNDATION. INSTALL NEW SIGNAL HEAD, L.E.D., 2-FACE, 3-SECTION, BRACKET MOUNTED. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SERVICE INSTALLATION. INSTALL NEW POLE MOUNTED SERVICE INSTALLATION. REUSE EXISTING ELECTRICAL CABLES.



LEGEND

	EXISTING	PROPOSED
CONTROLLER	[Symbol]	[Symbol]
SERVICE INSTALLATION	[Symbol]	[Symbol]
SIGNAL HEAD	[Symbol]	[Symbol]
SIGNAL HEAD WITH BACKPLATE	[Symbol]	[Symbol]
SIGNAL HEAD, PEDESTRIAN	[Symbol]	[Symbol]
SIGNAL POST	[Symbol]	[Symbol]
MAST ARM ASSEMBLY AND POLE	[Symbol]	[Symbol]
HANDHOLE	[Symbol]	[Symbol]
HEAVY DUTY HANDHOLE	[Symbol]	[Symbol]
DOUBLE HANDHOLE	[Symbol]	[Symbol]
CALVANIZED STEEL CONDUIT	[Symbol]	[Symbol]
EMERGENCY VEHICLE SYSTEM DETECTOR	[Symbol]	[Symbol]
CONFIRMATION BEACON	[Symbol]	[Symbol]
PEDESTRIAN PUSHBUTTON	[Symbol]	[Symbol]
DETECTOR LOOP	[Symbol]	[Symbol]

Reference Only

SCALE 1"=20'

COUNTY OF COOK	
DEPARTMENT OF HIGHWAYS	
TRAFFIC SIGNAL INSTALLATION	
SAUK TRAIL AT	
LAKEWOOD BLVD. - BLACKHAWK DR.	
DESIGNED: RLS	DATE: June 17, 2003
DRAWN: B.A.C.	
CHECKED: P.L.W.	

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 PROJECT NO. 01-0095-00-RS
 DATE: 7/2/2003 9:38:24 AM



DESIGNED - TMS	REVISED -
DRAWN - KAR	REVISED -
CHECKED - JJF	REVISED -
DATE - 10-26-11	FILE - 091213-CCHD SIGNAL PLANS.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

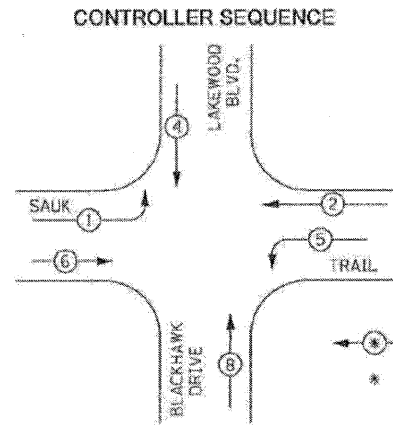
COOK COUNTY HIGHWAY DEPARTMENT
SIGNAL PLANS - REFERENCE ONLY

SCALE: NONE STA. TO STA.

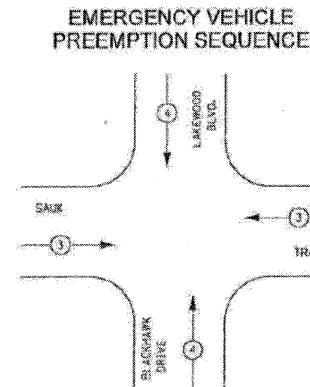
*1055/1056	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		11-00095-00-RS	COOK	27	26
	FED. ROAD DIST. NO.	ILLINOIS/FED. AID PROJECT M-90039691	CONTRACT NO. 63690		

COUNTY HIGHWAY	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
CH			

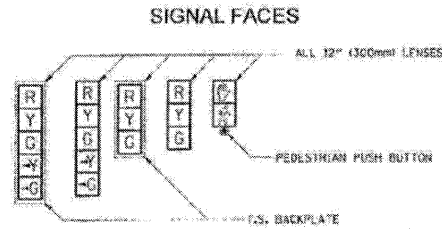
SECTION: 01-C1130-04-EP



PHASE DESIGNATION DIAGRAM
 DUAL ENTRY - ALL LEGS
 PROTECTED / PERMITTED LEFT TURN PHASING



PROPOSED EMERGENCY VEHICLE PREEMPTOR	
EMERGENCY VEHICLE PREEMPTOR	3 4
MOVEMENT	



- SIGNAL LENSES**
- R RED
 - Y YELLOW
 - C GREEN
 - Y YELLOW TURN INDICATOR
 - G GREEN TURN INDICATOR



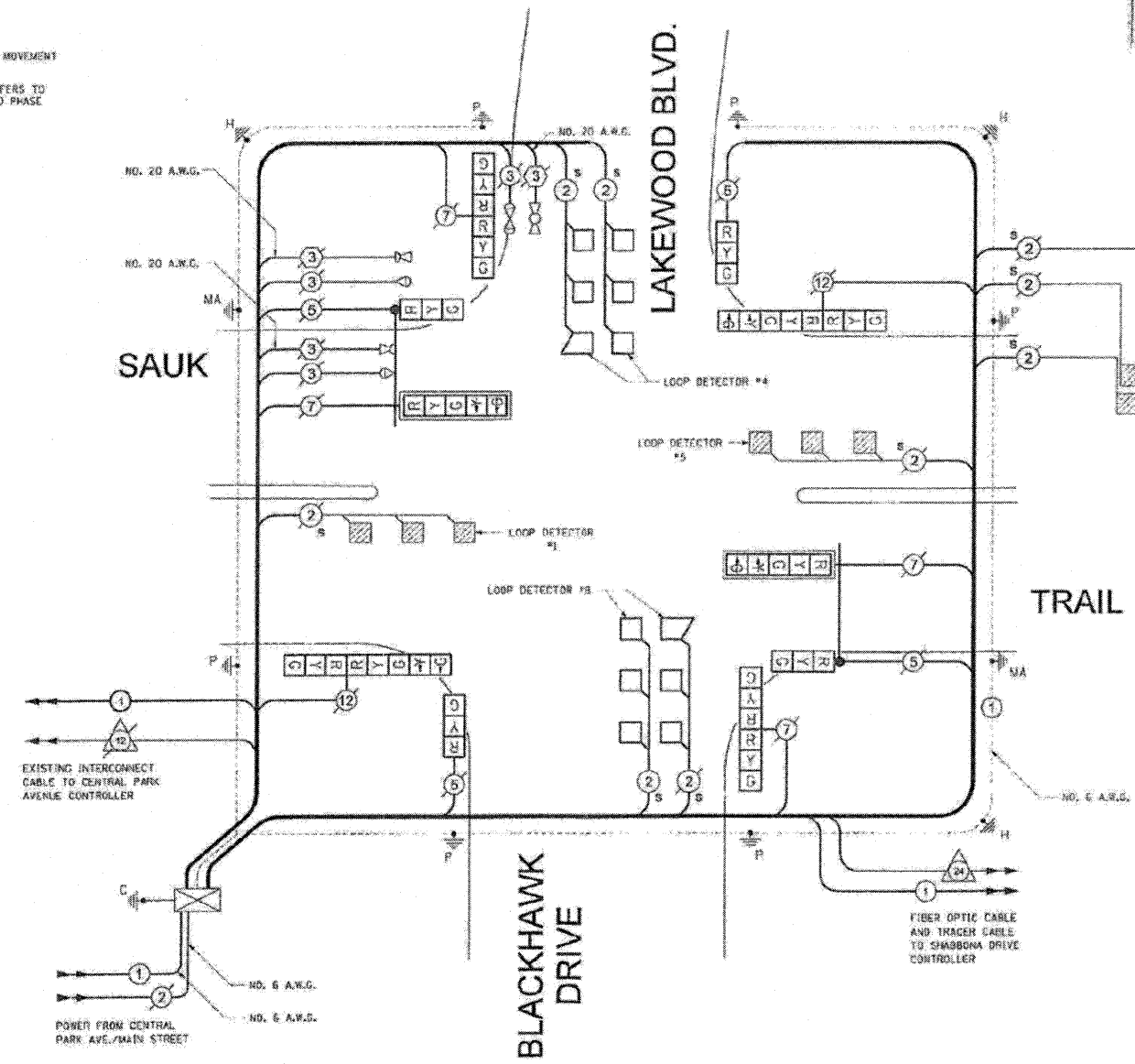
CABLE PLAN LEGEND

	PROPOSED	EXISTING
CONTROLLER CABINET	[Symbol]	[Symbol]
SERVICE INSTALLATION	[Symbol]	[Symbol]
12" TRAFFIC SIGNAL SECTION	[Symbol]	[Symbol]
2 DENOTES NUMBER OF CONDUCTORS ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE TWISTED AND SHIELDED.	[Symbol]	[Symbol]
VEHICLE DETECTOR, INDUCTION LOOP	[Symbol]	[Symbol]
SHIELDED & TWISTED	S	S
EMERGENCY VEHICLE LIGHT DETECTOR	[Symbol]	[Symbol]
CONFIRMATION BEACON	[Symbol]	[Symbol]
OPTICOM CABLE	[Symbol]	[Symbol]
300m PEDESTRIAN SIGNAL HEAD	[Symbol]	[Symbol]
PUSHBUTTON DETECTOR	[Symbol]	[Symbol]
GROUND ROD AT HANDHOLE W/ SCREW HANDHOLE OR CONTROLLER I.C.	[Symbol]	[Symbol]
GROUND ROD AT POST (P), OR MAST ARM POLE (MA)	[Symbol]	[Symbol]
GROUND ROD AT ELECTRIC SERVICE INSTALLATION	[Symbol]	[Symbol]
FIBER OPTIC INTERCONNECT CABLE NO. 62.5/125, 12/C	[Symbol]	[Symbol]
FIBER OPTIC CABLE, NO. 62.5/125 MULTIMODE 12 FIBERS AND SINGLE MODE 12 FIBERS	[Symbol]	[Symbol]

REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH 14" T.S. POSTS
- 3 EACH 16" T.S. POSTS
- 10 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 4 EACH SIGNAL HEAD, 1-FACE, 2-SECTION



- NOTES:**
- THIS IS A SYSTEM GROUND THAT SHALL INTERCONNECT ALL GROUND RODS WITH NO. 6 1/2 COPPER AWG.
 - THE NEUTRAL AND GROUND SHALL BE TIED AT THE SERVICE INSTALLATION BUT SEPARATE AT THE TRAFFIC CABINET.

CABLE PLAN
 NOT TO SCALE

Reference Only

COUNTY OF COOK DEPARTMENT OF HIGHWAYS	
SAUK TRAIL AT LAKEWOOD BLVD. - BLACKHAWK DR. CABLE DIAGRAM	
DESIGNED: R.L.S.	APPROVED: [Signature] 10/26/11
DRAWN: W.A.C.	CHECKED: P.L.W.

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 STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM
 No. 001-097272-3 FILED 9/26/10 AM
 1507 1st St. Lake Villa, IL 60149-3540
 2/22/2011



DESIGNED - TMS	REVISED -
DRAWN - KAR	REVISED -
CHECKED - JJF	REVISED -
DATE - 10-26-11	FILE - 091213-CCHD SIGNAL PLANS.shp

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

COOK COUNTY HIGHWAY DEPARTMENT
 SIGNAL PLANS - REFERENCE ONLY

SCALE: NONE STA. TO STA.

*1055/1056

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-00095-00-RS	COOK	27	27
CONTRACT NO. 63690				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT W-90039691				