

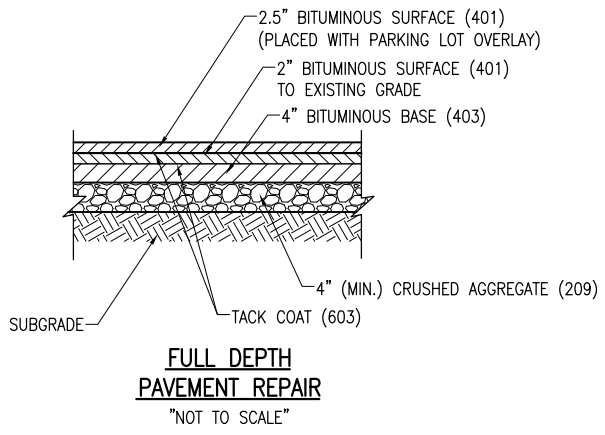
REVISION	DATE

**SAINT LOUIS DOWNTOWN AIRPORT**  
 A Division of Bi-State Development Agency  
 A.I.P. PROJ.: 3-17-0039-B28  
 IL PROJ.: CPS-4138

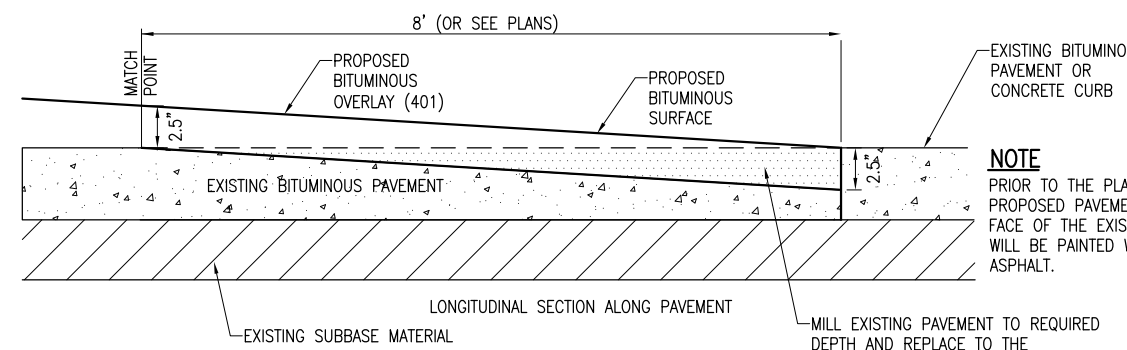
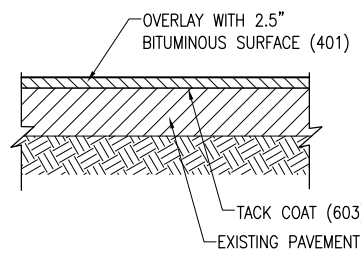
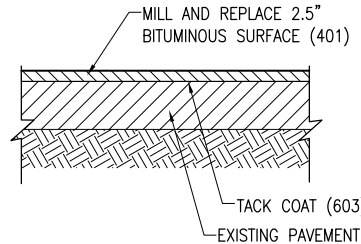
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Date 03/09/12	LAYOUT	

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REHABILITATE ENTRANCE ROAD AND PARKING LOT  
 PROPOSED TYPICAL SECTIONS AND DETAILS



THE AREA(S) DESIGNATED ON THE SITE PREPARATION AND CONSTRUCTION PLAN SHEETS WILL HAVE THE EXISTING BITUMINOUS PAVEMENT REMOVED (FULL DEPTH) AND REPLACED WITH BITUMINOUS MATERIAL. THE BASE WILL BE EXAMINED UPON REMOVAL BY THE RESIDENT ENGINEER AND IF IT APPEARS UNSTABLE IT WILL BE REPLACED BY NO LESS THAN 4 INCHES OF CRUSHED AGGREGATE BASE COURSE. THE COST OF PAVEMENT REMOVAL, THE CRUSHED AGGREGATE, BITUMINOUS BASE COURSE, BITUMINOUS SURFACE COURSE UP TO EXISTING GRADE ELEVATION, AND ALL LABOR AND INCIDENTALS TO COMPLETE THIS WORK SHALL BE PAID FOR UNDER ITEM 201910, REMOVE AND REPLACE BITUMINOUS PAVEMENT, PER SQUARE YARD.



**AR401655 BUTT JOINT CONSTRUCTION NOTES**

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 "BUTT JOINT CONSTRUCTION" AS STATED IN THE STANDARD SPECIFICATIONS, ADOPTED NOVEMBER 2, 2009.

THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED BITUMINOUS OVERLAY AT THE LOCATIONS AS SHOWN ON THE PLANS.

THE PAVEMENT SURFACE WILL THEN BE MILLED TO A DEPTH OF 2.5" AT THE BUTT END AND WILL TAPER TO 0 IN. AT THE OPPOSITE END. THE PLANER MUST BE CAPABLE OF MILLING THE SURFACE TO THE DESIRED ELEVATIONS AND GRADES.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE CONTRACTOR AND ACCEPTED BY THE RESIDENT ENGINEER.

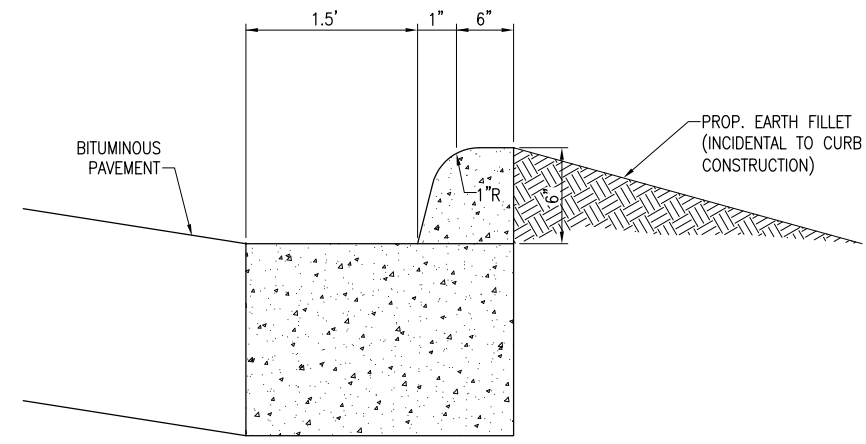
THE PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM: AR401655 "BUTT JOINT CONSTRUCTION", PER S.Y.

**ITEM 610**

ALL CONCRETE UTILIZED SHALL BE IN ACCORDANCE WITH ITEM 610 OF THE STANDARD SPECIFICATIONS.

**SUPERPAVE DESIGN CRITERIA**

BITUMINOUS SURFACE COURSE AND BITUMINOUS BASE COURSE MATERIAL SHALL CONFORM TO THE REQUIREMENTS OF ITEM 401/ITEM 403, TABLE 1, "AUTOMOBILE" CATEGORY WITH RESPECT TO THE DESIGN MIX.



NOTE: AT LOCATIONS WHERE STD CONC CURB AND GUTTER IS SHOWN ON THE PLANS, THE CURB AND GUTTER SHALL BE TYPE "B" BARRIER CURB IN CONFORMANCE WITH IDOT STANDARD DRAWING 606001 AND SECTION 606 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2012. CONSTRUCTION OF THE CURB AND GUTTER WILL BE PAID FOR UNDER ITEM AR754410, COMB CURB AND GUTTER, PER L.F.

