

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	1
		ILLINOIS	CONTRACT NO. 62D18	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE PROJECT IS LOCATED IN THE CITIES OF PARK RIDGE AND CHICAGO

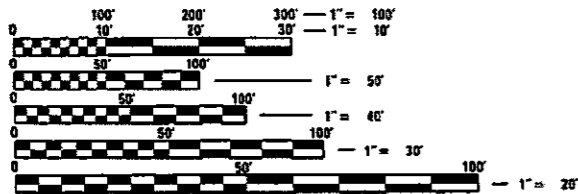
PROPOSED
HIGHWAY PLANS

F.A.U. ROUTE 1349: DEVON AVE.
EAST OF DES PLAINES RIVER RD. TO CANFIELD AVE.
SECTION: (3197A&B) RS-1
PROJECT: STP-9A65(879)
RESURFACING (3P), PEDESTRIAN RAMPS
COOK COUNTY

C-91-436-16

RESURFACING OMISSION:
STA. 96 + 86.8 TO STA. 99 + 07

TRAFFIC DATA:
2014 ADT = 19,300
POSTED SPEED LIMIT = 30-40 MPH



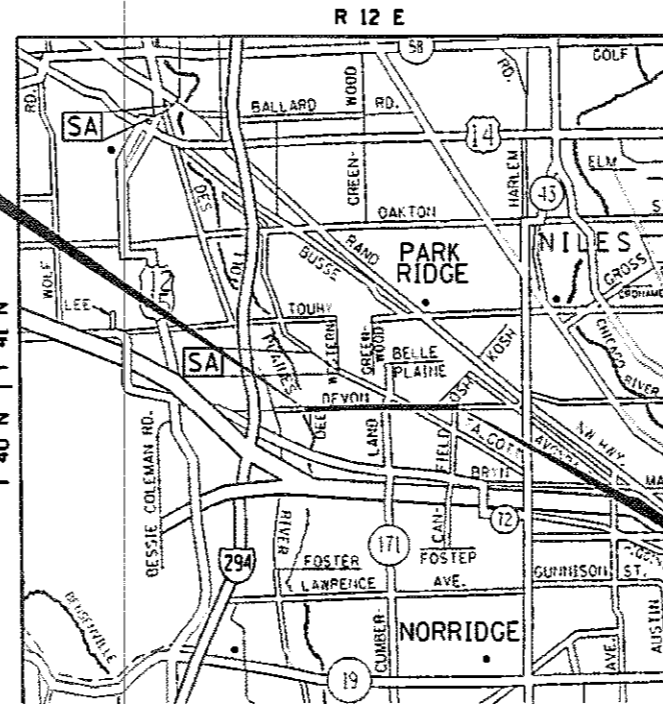
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-8123
OR 811

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
(312) 744-7000

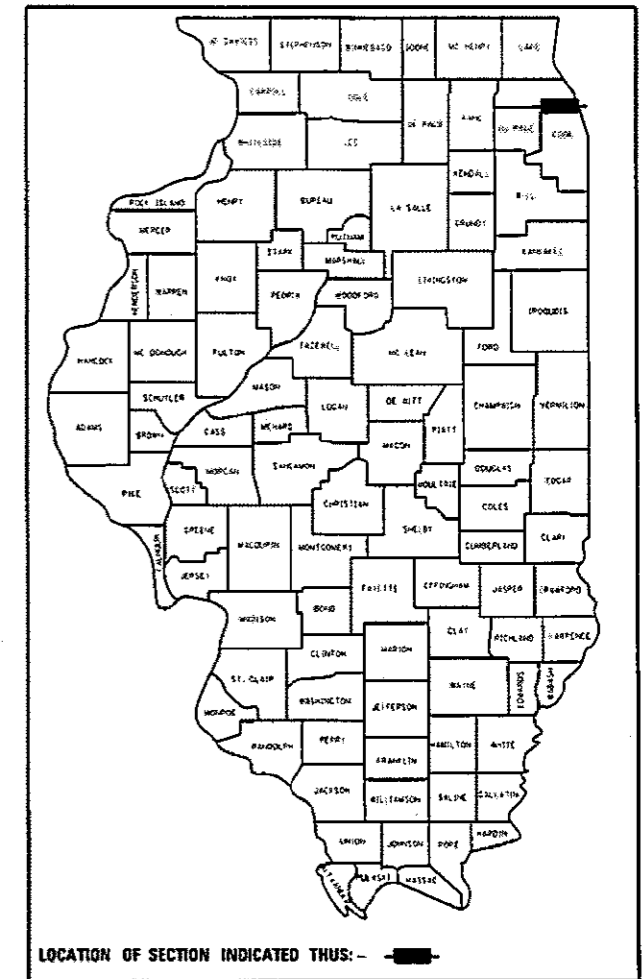
PROJECT ENGINEER KARI SMITH (847) 705-4437
PROJECT MANAGER FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62D18



LEYDEN, JEFFERSON, AND MAINE TOWNSHIP

GROSS LENGTH = 10,873.3 FT. = 2.06 MILES
NET LENGTH = 10,061.3 FT. = 1.91 MILES



LOCATION OF SECTION INDICATED THUS: [Symbol]

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
SUBMITTED January 24 2018
Anthony J. Quigley / CRS
REGIONAL ENGINEER
March 23 2018
ENGINEER OF DESIGN AND ENVIRONMENT
March 23 2018
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE				
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	15	15				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	30	30				
20200100	EARTH EXCAVATION	CU YD	145	145				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	988	988				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	12	12				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	12	12				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	12	12				
25100630	EROSION CONTROL BLANKET	SQ YD	745	745				
25200110	SODDING, SALT TOLERANT	SQ YD	1002	1002				
25200200	SUPPLEMENTAL WATERING	UNIT	3.5	3.5				
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	5	5				
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	12	12				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	41751	41751				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	188	188				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2551	2551				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	745	745				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	2	2				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	5244	5244				
42001300	PROTECTIVE COAT	SQ YD	2145	2145				
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	125	125				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	7	7				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	14907	14907				
42400800	DETECTABLE WARNINGS	SQ FT	1188	1188				
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	1702	1702				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	60718	60718				
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	135	135				

14

12

FILE NAME =	USER NAME = paragon	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.) SUMMARY OF QUANTITIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
OFFICE DISTRICT =	PROJECT =	CHECKED -	REVISED -			1349	(3197A&B) RS-1	COOK	70	3	
PLOT SCALE =	PLOT DATE =	DATE -	REVISED -			CONTRACT NO. 62D18					
						SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

REV

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE			
44000600	SIDEWALK REMOVAL	SO FT	14208	14208			
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SO YD	319	319			
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SO YD	90	90			
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SO YD	1049	1049			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	64	64			
60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1			
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2			
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1			
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2			
60266600	VALVE BOXES TO BE ADJUSTED	EACH	9	9			
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	2	2			
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	44	44			
60404940	FRAMES AND GRATES, TYPE 23	EACH	2	2			
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	15	15			

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE			
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	15	15			
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	145	145			
66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1			
66900530	SOIL DISPOSAL ANALYSIS	EACH	4	4			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6			
67100100	MOBILIZATION	LSUM	1	1			
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM	1	1			
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1			
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	30809	30809			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	10270	10270			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	219	219			

14

13

REV

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	16545	16545			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3405	3405			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	624	624			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	336	336			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	652	652			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	7703	7703			
* 72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1			
* 72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	2	2			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	219	219			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	16545	16545			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3405	3405			
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	624	624			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	336	336			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	652	652			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	701	701			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	565	565			
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	5	5			
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1612	1612			
* 89500400	RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	10	10			
* 89502376	REBUILD EXISTING HANDHOLE	EACH	6	6			
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1			
X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	32	32			
X2010350	TREE REMOVAL, ACRES (SPECIAL)	ACRE	0.15	0.15			
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	24	24			
X2501800	SEEDING, CLASS 4 (MODIFIED)	ACRE	0.15	0.15			
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	280	280			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	80	80			
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	11665	11665			

14

14

FILE NAME =	USER NAME = paraymat	DESIGNED -	REVISED -
pw\ANLDBE\BID\EG\Illinois.gov\FWIDOT\Documents\1000	Offices\Gatricia\Projects\043516\CADData\Design\043516	DRAWING -	REVISED -
	PLOT SCALE = 1000000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 2/1/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.)
SUMMARY OF QUANTITIES

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	5
CONTRACT NO. 62D18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. OF SHEETS STA. TO STA.

□ Revised

URBAN

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE						CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE					
X7240500	RELOCATE EXISTING SIGNS	EACH	5	5															
X8950205	REBUILD EXISTING HANDHOLE, SPECIAL	EACH	1	1															
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	5120	5120															
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	28	28															
Z0030850	TEMPORARY INFORMATION SIGNING	50 FT	102.8	102.8															
Z0076600	TRAINEES	HOUR	500	500															
Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	500	500															

5

FILE NAME =	USER NAME = paragraf	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.) SUMMARY OF QUANTITIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
par\140846\BID\INT\EG\Illinois\pwr\DOT\Documents\140846\Offices\Of\trid\N\Projects\04\3616\CAD\cadd\Design\04\3616\DWG\001.dwg	DESIGNED -	REVISED -	1349			(3197A&B) RS-1	COOK	70	6	
PLOT SCALE = 1000000' / in.	CHECKED -	REVISED -	CONTRACT NO. 62D18							
PLOT DATE = 2/11/2018	DATE -	REVISED -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

Revised 0042

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ±9"
- ② EXISTING P.C.C. SIDEWALK, 5" (TYP.)
- ③ EXISTING MEDIAN
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING COMB. CONCRETE CURB AND GUTTER
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 1/4"
- ⑥A PROPOSED HMA SURFACE REMOVAL, 1 1/2" (SEE PLAN FOR EXACT LOCATIONS)
- ⑦ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑧ PROPOSED POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED PAVEMENT MARKING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ N _{DES}	QMP
--------------	------------------------------	-----

ROADWAY RESURFACING:

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% AT 70 GYR.	QCP
POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP

HOT-MIX ASPHALT PATCHING:

CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC / QA
---------------------------------------	---------------	---------

HMA DRIVEWAY RESTORATION FOR CURB AND GUTTER REMOVE AN REPLACEMENT:

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% AT 50 GYR.	QC / QA
HOT-MIX ASPHALT BASE COURSE, 6" (HMA BINDER IL 19.0)	4% AT 50 GYR.	QC / QA
HOT-MIX ASPHALT BASE COURSE, 8" (HMA BINDER IL 19.0)	4% AT 50 GYR.	QC / QA

QMP Designation: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)

NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

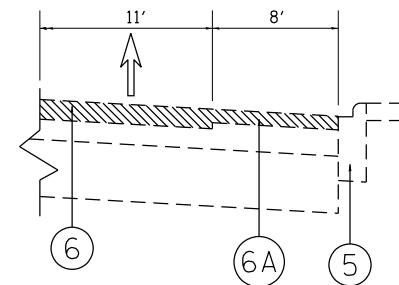
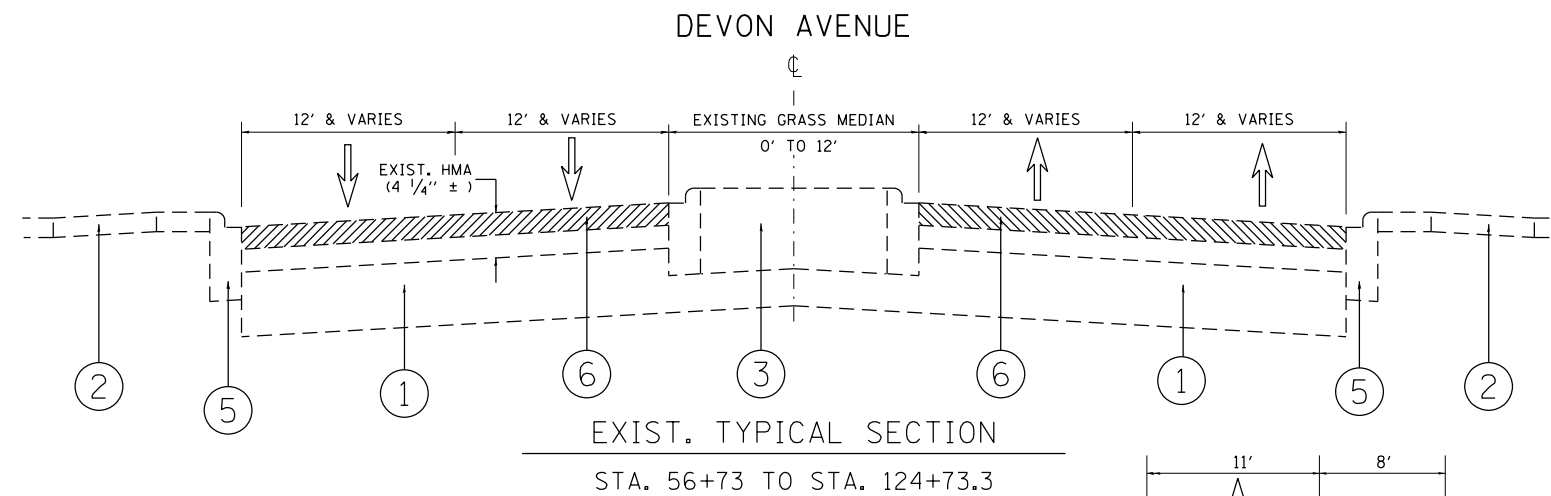
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

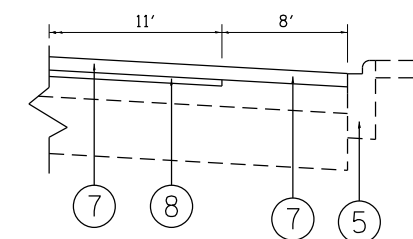
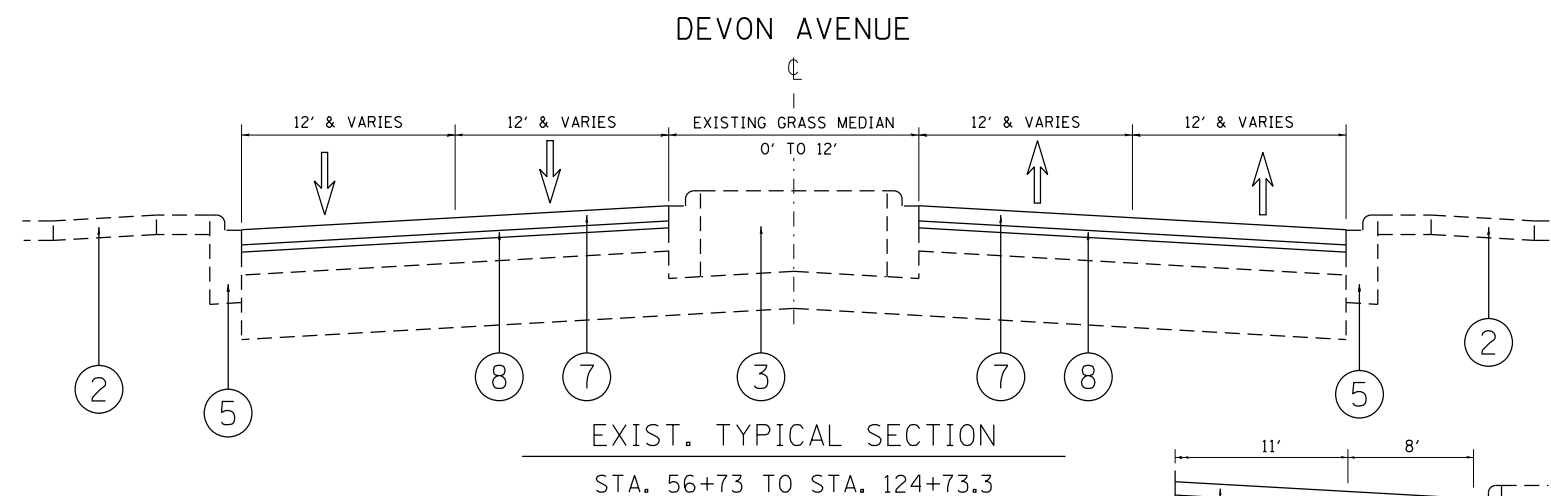
QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

NOTE:

THE CONTRACTOR SHALL MILL THE ROADWAY FIRST, THEN DO PAVEMENT PATCHING PER BD-22 DETAIL.



STREET PARKING
STA. 88+00 TO STA. 97+09
NOTE: MILLING FOR SURFACE COURSE ONLY.
BOTH SIDES OF THE ROAD. SEE PLAN
FOR EXACT LOCATIONS.

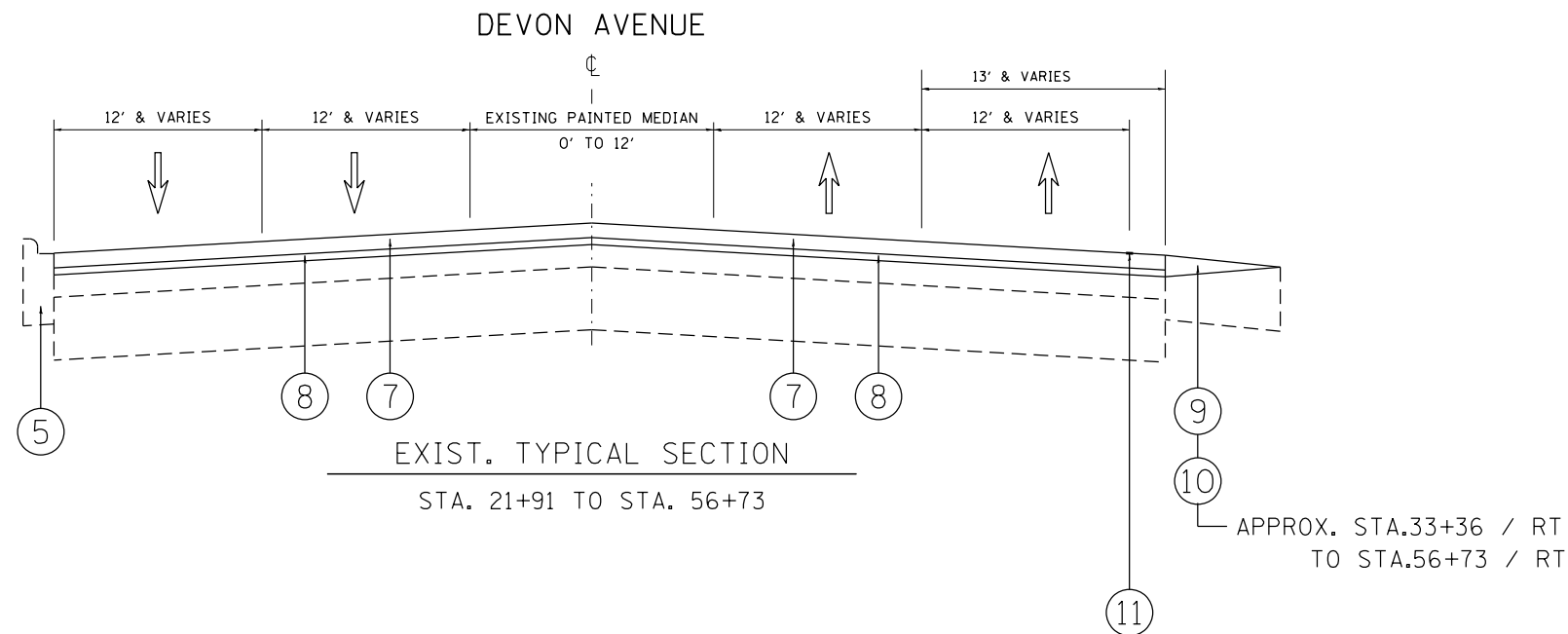
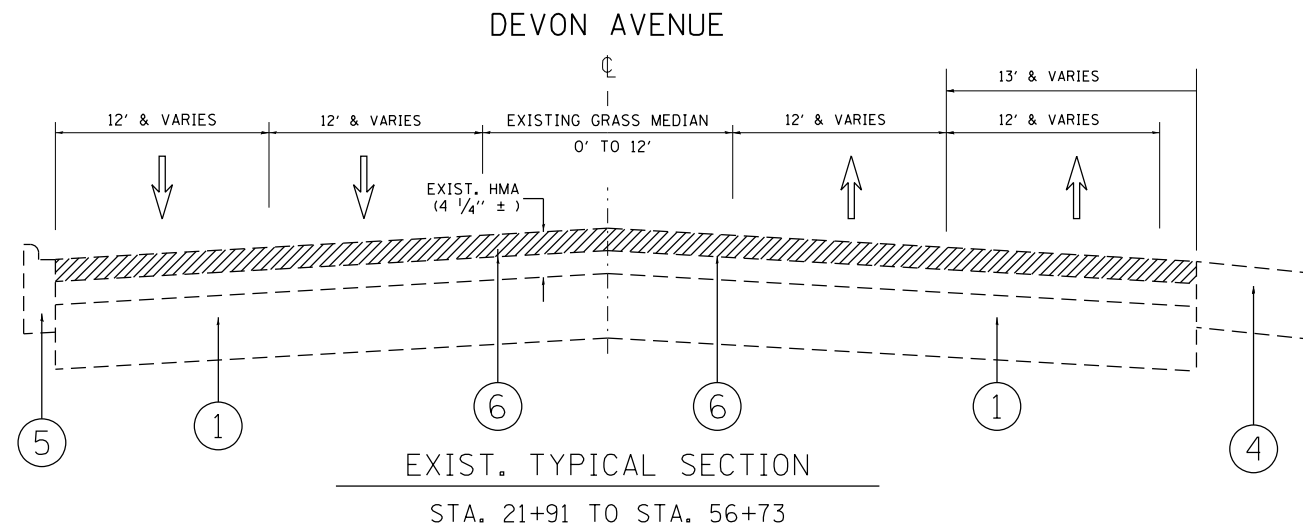


STREET PARKING
STA. 88+00 TO STA. 97+09
NOTE: MILLING FOR SURFACE COURSE ONLY.
BOTH SIDES OF THE ROAD. SEE PLAN
FOR EXACT LOCATIONS.

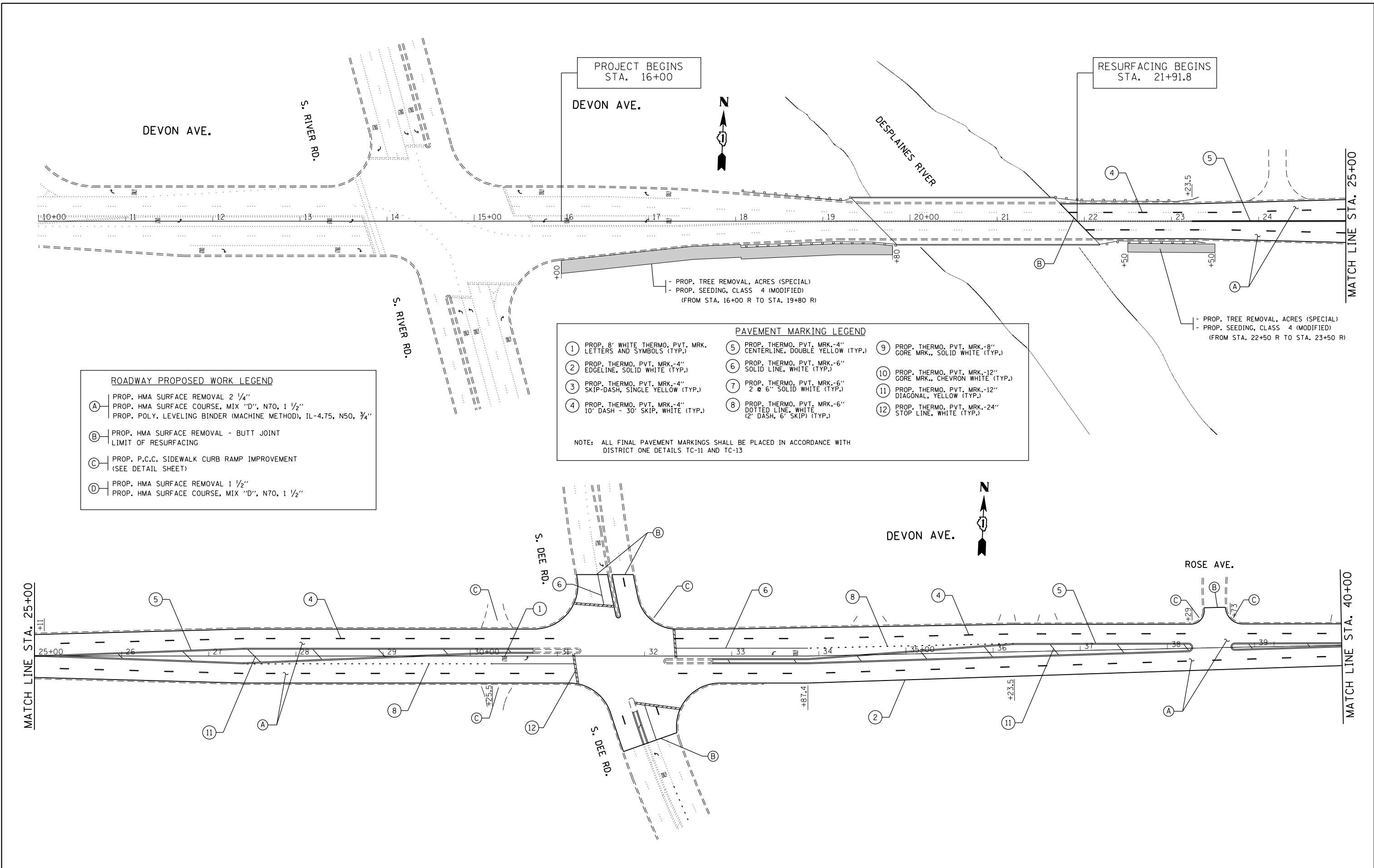
FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\1436\Browns\Design\143616-sh-t-plen.dgn	PLotted SCALE = 100.0000' / in.	CHECKED -	REVISED -			1349	(3197A&B) RS-1	COOK	70	7
PLotted DATE = 2/1/2018	DATE -	REVISED -	REVISED -			CONTRACT NO. 62D18				
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ±9"
- ② EXISTING P.C.C. SIDEWALK, 5" (TYP.)
- ③ EXISTING MEDIAN
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING COMB. CONCRETE CURB AND GUTTER
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 1/4"
- ⑥A PROPOSED HMA SURFACE REMOVAL, 1 1/2" (SEE PLAN FOR EXACT LOCATIONS)
- ⑦ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑧ PROPOSED POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED PAVEMENT MARKING



FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
						1349	(3197A&B) RS-1	COOK	70	8	
						CONTRACT NO. 62D18					
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -	SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.				
	PLOT DATE = 2/1/2018	DATE -	REVISED -								



ROADWAY PROPOSED WORK LEGEND

(A)	PROP. HMA SURFACE REMOVAL 2 1/4" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
(B)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(C)	PROP. P.C.C. SIDEWALK CURB RAMP IMPROVEMENT (SEE DETAIL SHEET)
(D)	PROP. HMA SURFACE REMOVAL 1 1/2" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

PAVEMENT MARKING LEGEND

(1)	PROP. 8" WHITE THERMO. PVT. MKR. LETTERS AND SYMBOLS (TYP.)	(5)	PROP. THERMO. PVT. MKR.-4" CENTERLINE, DOUBLE YELLOW (TYP.)	(9)	PROP. THERMO. PVT. MKR.-8" GORE MKR., SOLID WHITE (TYP.)
(2)	PROP. THERMO. PVT. MKR.-4" EDGELINE, SOLID WHITE (TYP.)	(6)	PROP. THERMO. PVT. MKR.-6" SOLID LINE, WHITE (TYP.)	(10)	PROP. THERMO. PVT. MKR.-12" GORE MKR., CHEVRON WHITE (TYP.)
(3)	PROP. THERMO. PVT. MKR.-4" SKIP-DASH, SINGLE YELLOW (TYP.)	(7)	PROP. THERMO. PVT. MKR.-6" 2 @ 6" SOLID WHITE (TYP.)	(11)	PROP. THERMO. PVT. MKR.-12" DIAGONAL, YELLOW (TYP.)
(4)	PROP. THERMO. PVT. MKR.-4" 10' DASH - 30' SKIP, WHITE (TYP.)	(8)	PROP. THERMO. PVT. MKR.-6" DOTTED LINE, WHITE (2' DASH, 6' SKIP) (TYP.)	(12)	PROP. THERMO. PVT. MKR.-24" STOP LINE, WHITE (TYP.)

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13

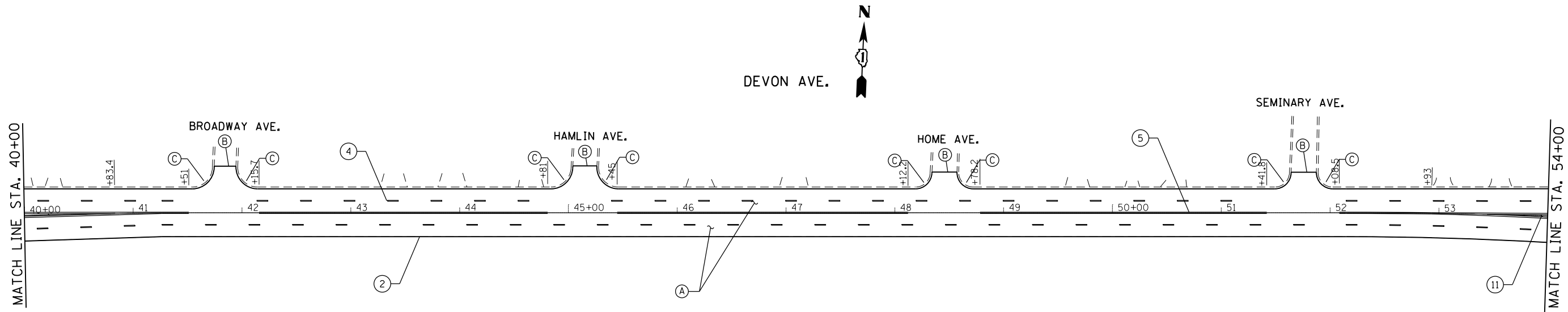
FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\D143616\Drawings\Design\D143616-shr-pln.dgn		REVISED -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	REVISED -
Default	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
--------	-------	----	--------	------	----	------

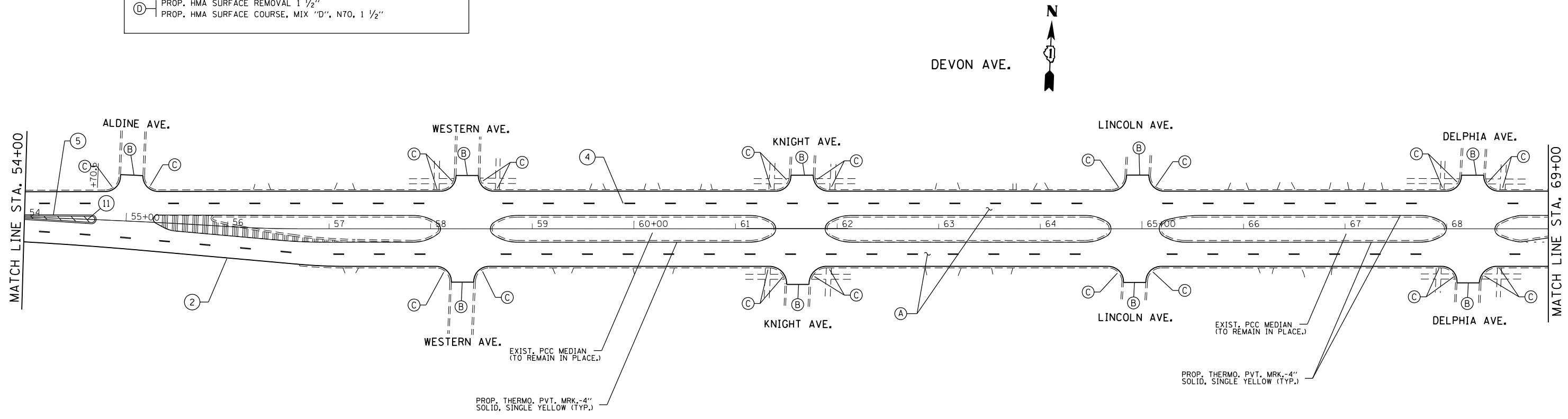
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	9
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



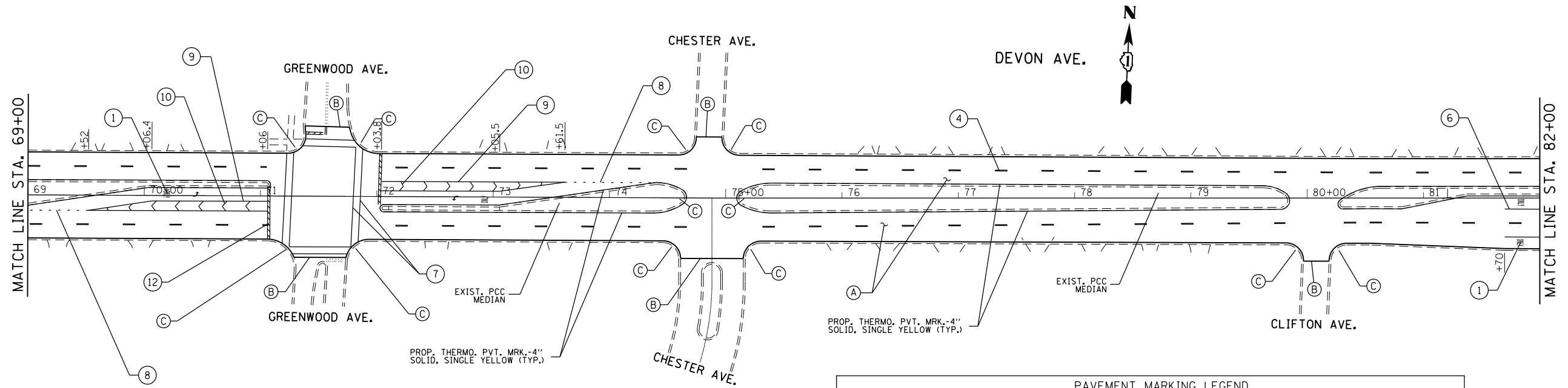
ROADWAY PROPOSED WORK LEGEND	
(A)	PROP. HMA SURFACE REMOVAL 2 1/4" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
(B)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(C)	PROP. P.C.C. SIDEWALK CURB RAMP IMPROVEMENT (SEE DETAIL SHEET)
(D)	PROP. HMA SURFACE REMOVAL 1 1/2" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

PAVEMENT MARKING LEGEND			
(1)	PROP. 8" WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)	(9)	PROP. THERMO. PVT. MRK.-8" GORE MRK., SOLID WHITE (TYP.)
(2)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID WHITE (TYP.)	(10)	PROP. THERMO. PVT. MRK.-12" GORE MRK., CHEVRON WHITE (TYP.)
(3)	PROP. THERMO. PVT. MRK.-4" SKIP-DASH, SINGLE YELLOW (TYP.)	(11)	PROP. THERMO. PVT. MRK.-12" DIAGONAL, YELLOW (TYP.)
(4)	PROP. THERMO. PVT. MRK.-4" 10' DASH - 30' SKIP, WHITE (TYP.)	(12)	PROP. THERMO. PVT. MRK.-24" STOP LINE, WHITE (TYP.)
(5)	PROP. THERMO. PVT. MRK.-4" CENTERLINE, DOUBLE YELLOW (TYP.)		
(6)	PROP. THERMO. PVT. MRK.-6" SOLID LINE, WHITE (TYP.)		
(7)	PROP. THERMO. PVT. MRK.-6" 2 @ 6" SOLID WHITE (TYP.)		
(8)	PROP. THERMO. PVT. MRK.-6" DOTTED LINE, WHITE (2' DASH, 6' SKIP) (TYP.)		

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13



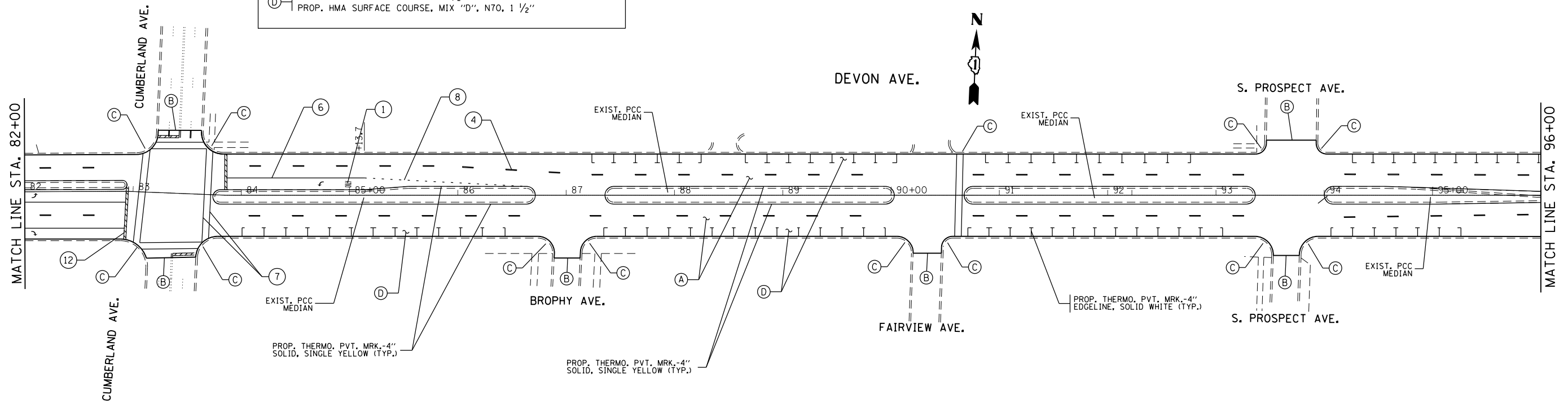
FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.)	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI43616\Drawings\Design\DI43616-sh-t-plen.dgn		REVISIONS	REVISIONS			1349	(3197A&B) RS-1	COOK	70	10	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISIONS			CONTRACT NO. 62D18					
	PLOT DATE = 2/1/2018	DATE -	REVISIONS			ILLINOIS FED. AID PROJECT					



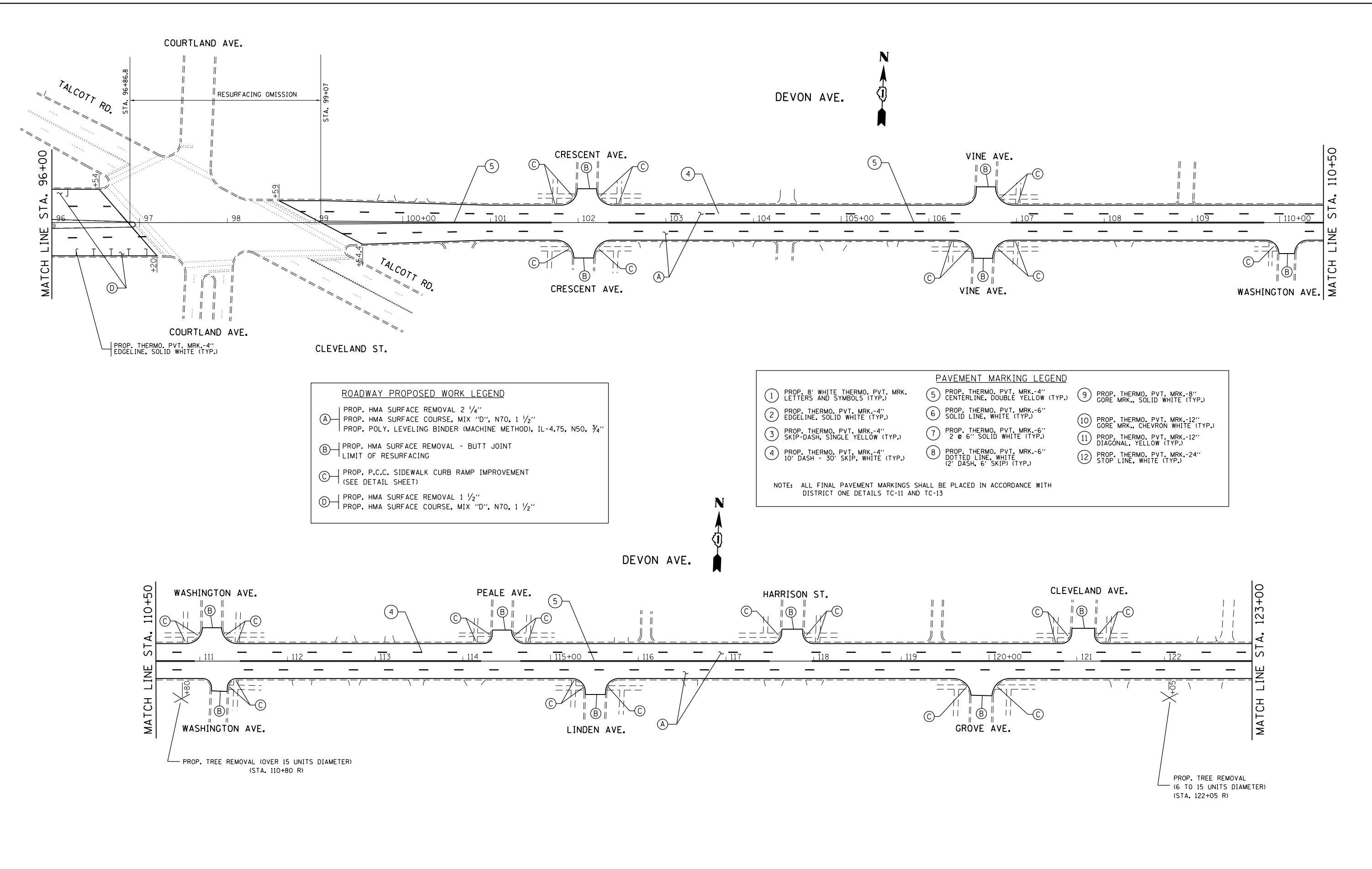
ROADWAY PROPOSED WORK LEGEND	
(A)	PROP. HMA SURFACE REMOVAL 2 1/4" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
(B)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(C)	PROP. P.C.C. SIDEWALK CURB RAMP IMPROVEMENT (SEE DETAIL SHEET)
(D)	PROP. HMA SURFACE REMOVAL 1 1/2" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

PAVEMENT MARKING LEGEND			
(1)	PROP. 8" WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)	(9)	PROP. THERMO. PVT. MRK.-8" GORE MRK., SOLID WHITE (TYP.)
(2)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID WHITE (TYP.)	(10)	PROP. THERMO. PVT. MRK.-12" GORE MRK., CHEVRON WHITE (TYP.)
(3)	PROP. THERMO. PVT. MRK.-4" SKIP-DASH, SINGLE YELLOW (TYP.)	(11)	PROP. THERMO. PVT. MRK.-12" DIAGONAL, YELLOW (TYP.)
(4)	PROP. THERMO. PVT. MRK.-4" 10' DASH - 30' SKIP, WHITE (TYP.)	(12)	PROP. THERMO. PVT. MRK.-24" STOP LINE, WHITE (TYP.)
(5)	PROP. THERMO. PVT. MRK.-4" CENTERLINE, DOUBLE YELLOW (TYP.)		
(6)	PROP. THERMO. PVT. MRK.-6" SOLID LINE, WHITE (TYP.)		
(7)	PROP. THERMO. PVT. MRK.-6" 2 @ 6" SOLID WHITE (TYP.)		
(8)	PROP. THERMO. PVT. MRK.-6" DOTTED LINE, WHITE (2" DASH, 6" SKIP) (TYP.)		

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13



FILE NAME =	USER NAME = parayno1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI436\Drawings\Design\DI43616-sh-t-plan.dgn		CHECKED -	REVISED -			1349	(3197A&B) RS-1	COOK	70	11
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62D18				
	PLOT DATE = 2/1/2018					ILLINOIS FED. AID PROJECT				



ROADWAY PROPOSED WORK LEGEND

(A)	PROP. HMA SURFACE REMOVAL 2 1/4" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
(B)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(C)	PROP. P.C.C. SIDEWALK CURB RAMP IMPROVEMENT (SEE DETAIL SHEET)
(D)	PROP. HMA SURFACE REMOVAL 1 1/2" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

PAVEMENT MARKING LEGEND

(1)	PROP. 8' WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)	(5)	PROP. THERMO. PVT. MRK.-4" CENTERLINE, DOUBLE YELLOW (TYP.)	(9)	PROP. THERMO. PVT. MRK.-8" GORE MRK., SOLID WHITE (TYP.)
(2)	PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID WHITE (TYP.)	(6)	PROP. THERMO. PVT. MRK.-6" SOLID LINE, WHITE (TYP.)	(10)	PROP. THERMO. PVT. MRK.-12" GORE MRK., CHEVRON WHITE (TYP.)
(3)	PROP. THERMO. PVT. MRK.-4" SKIP-DASH, SINGLE YELLOW (TYP.)	(7)	PROP. THERMO. PVT. MRK.-6" 2 @ 6" SOLID WHITE (TYP.)	(11)	PROP. THERMO. PVT. MRK.-12" DIAGONAL, YELLOW (TYP.)
(4)	PROP. THERMO. PVT. MRK.-4" 10' DASH - 30' SKIP, WHITE (TYP.)	(8)	PROP. THERMO. PVT. MRK.-6" DOTTED LINE, WHITE (2' DASH, 6' SKIP) (TYP.)	(12)	PROP. THERMO. PVT. MRK.-24" STOP LINE, WHITE (TYP.)

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13

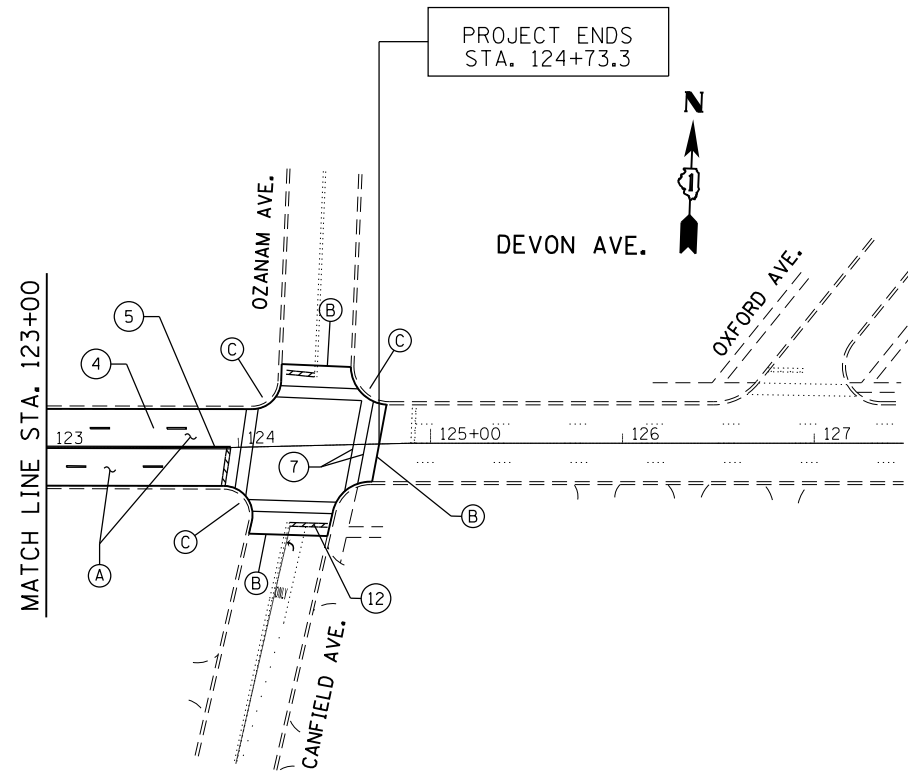
FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
p:\work\11084EBID\INTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\11084EBID\Design\11084EBID-shr-plan.dgn		REVISED -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/1/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	12
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



ROADWAY PROPOSED WORK LEGEND	
(A)	PROP. HMA SURFACE REMOVAL 2 1/4" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
(B)	PROP. HMA SURFACE REMOVAL - BUTT JOINT LIMIT OF RESURFACING
(C)	PROP. P.C.C. SIDEWALK CURB RAMP IMPROVEMENT (SEE DETAIL SHEET)
(D)	PROP. HMA SURFACE REMOVAL 1 1/2" PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"

PAVEMENT MARKING LEGEND		
(1) PROP. 8' WHITE THERMO. PVT. MRK. LETTERS AND SYMBOLS (TYP.)	(5) PROP. THERMO. PVT. MRK.-4" CENTERLINE, DOUBLE YELLOW (TYP.)	(9) PROP. THERMO. PVT. MRK.-8" GORE MRK., SOLID WHITE (TYP.)
(2) PROP. THERMO. PVT. MRK.-4" EDGELINE, SOLID WHITE (TYP.)	(6) PROP. THERMO. PVT. MRK.-6" SOLID LINE, WHITE (TYP.)	(10) PROP. THERMO. PVT. MRK.-12" GORE MRK., CHEVRON WHITE (TYP.)
(3) PROP. THERMO. PVT. MRK.-4" SKIP-DASH, SINGLE YELLOW (TYP.)	(7) PROP. THERMO. PVT. MRK.-6" 2 @ 6" SOLID WHITE (TYP.)	(11) PROP. THERMO. PVT. MRK.-12" DIAGONAL, YELLOW (TYP.)
(4) PROP. THERMO. PVT. MRK.-4" 10' DASH - 30' SKIP, WHITE (TYP.)	(8) PROP. THERMO. PVT. MRK.-6" DOTTED LINE, WHITE (2' DASH, 6" SKIP) (TYP.)	(12) PROP. THERMO. PVT. MRK.-24" STOP LINE, WHITE (TYP.)

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13

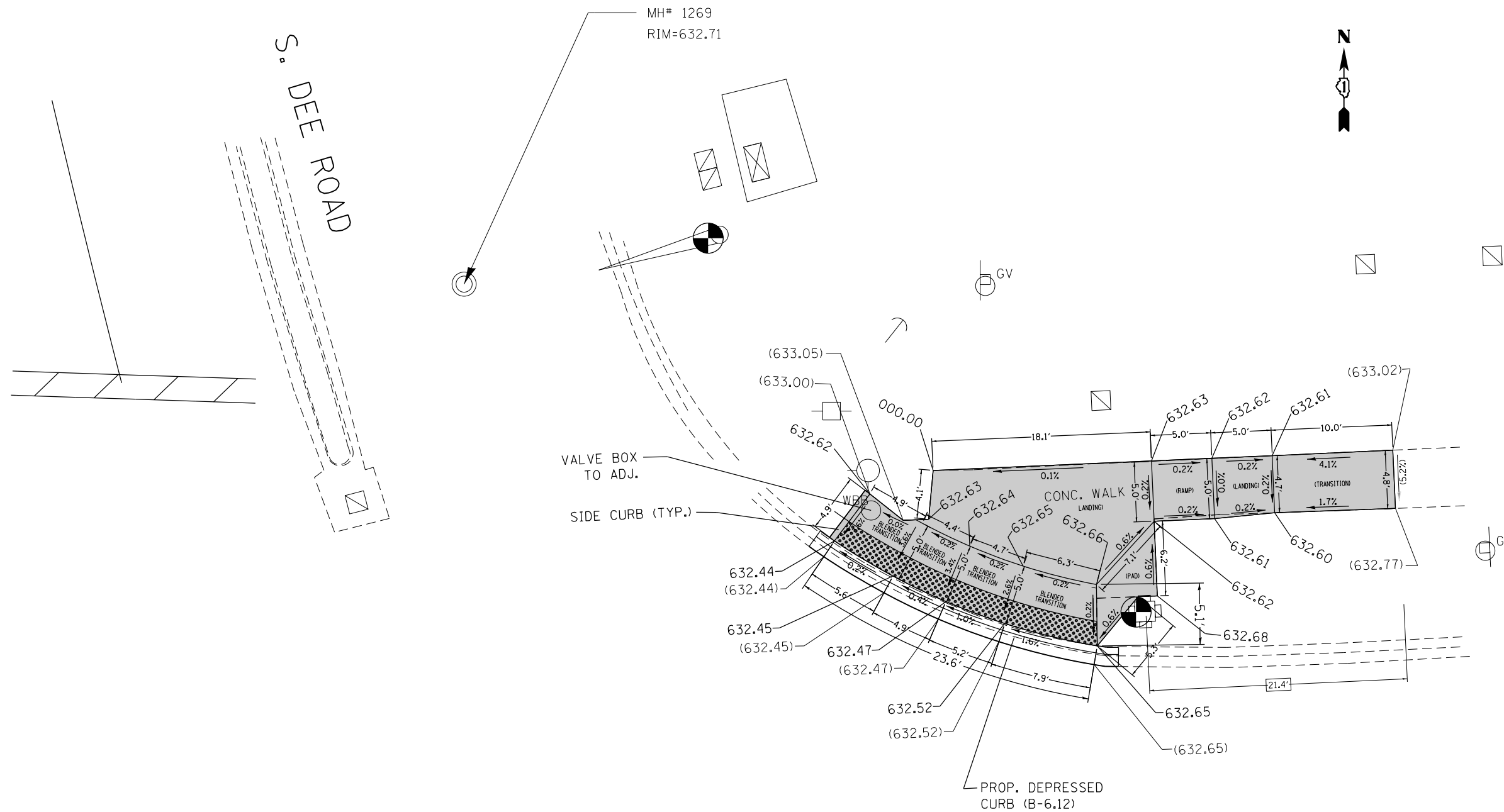
FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI43616\Drawings\Design\DI43616-sh-t-plan.dgn		REVISED -	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/1/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
DEVON AVE. (DES PLAINES RIVER RD. TO CANFIELD AVE.)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	13
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



LEGEND	
xx.xx'	EXISTING LENGTH
====	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK#1 ELEV. 633.067
 REFERENCE BENCHMARK#2 ELEV. 633.098
 BENCHMARK LOCATIONS : NORTHEAST INTERSECTION CORNER,
 "X" IN TRAFFIC SIGNAL BASE (SEE PLAN DETAIL)

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\1436\Drawings\Design\143616-sht-details.dgn		REVISED -	REVISED -
Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/14/2018	DATE -	REVISED -

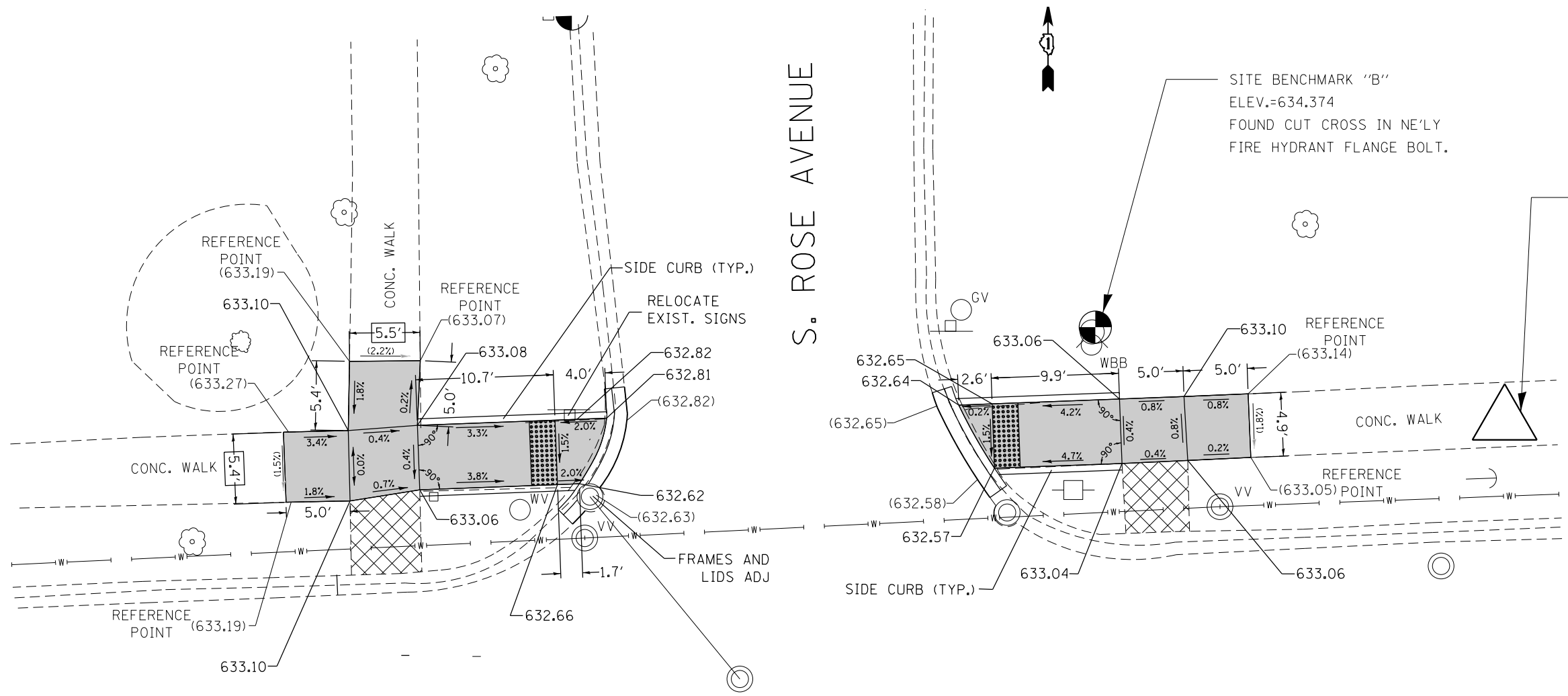
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
 DEVON AVE. AT DEE ROAD, NORTHEAST CORNER (STA. 32+00)**

SCALE: SHEET OF SHEETS STA. TO STA.

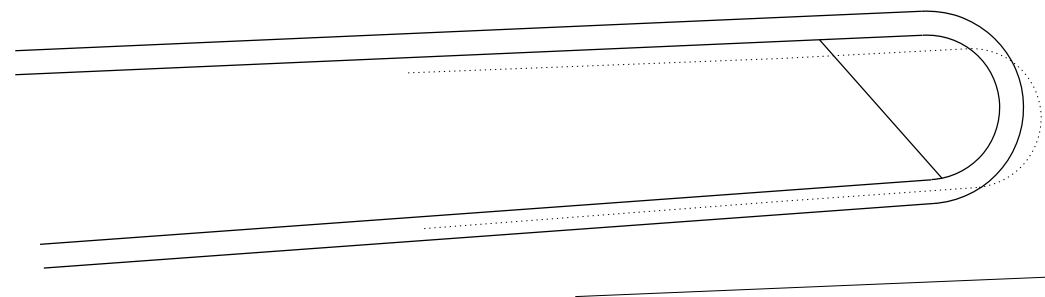
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	14
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

S. ROSE AVENUE



SITE BENCHMARK "B"
ELEV.=634.374
FOUND CUT CROSS IN NE'LY
FIRE HYDRANT FLANGE BOLT.

CWA CP #83
N: 1,941,555.152
E: 1,114,884.791
ELEV.=633.020
SET CUT CROSS
IN CONC. WALK

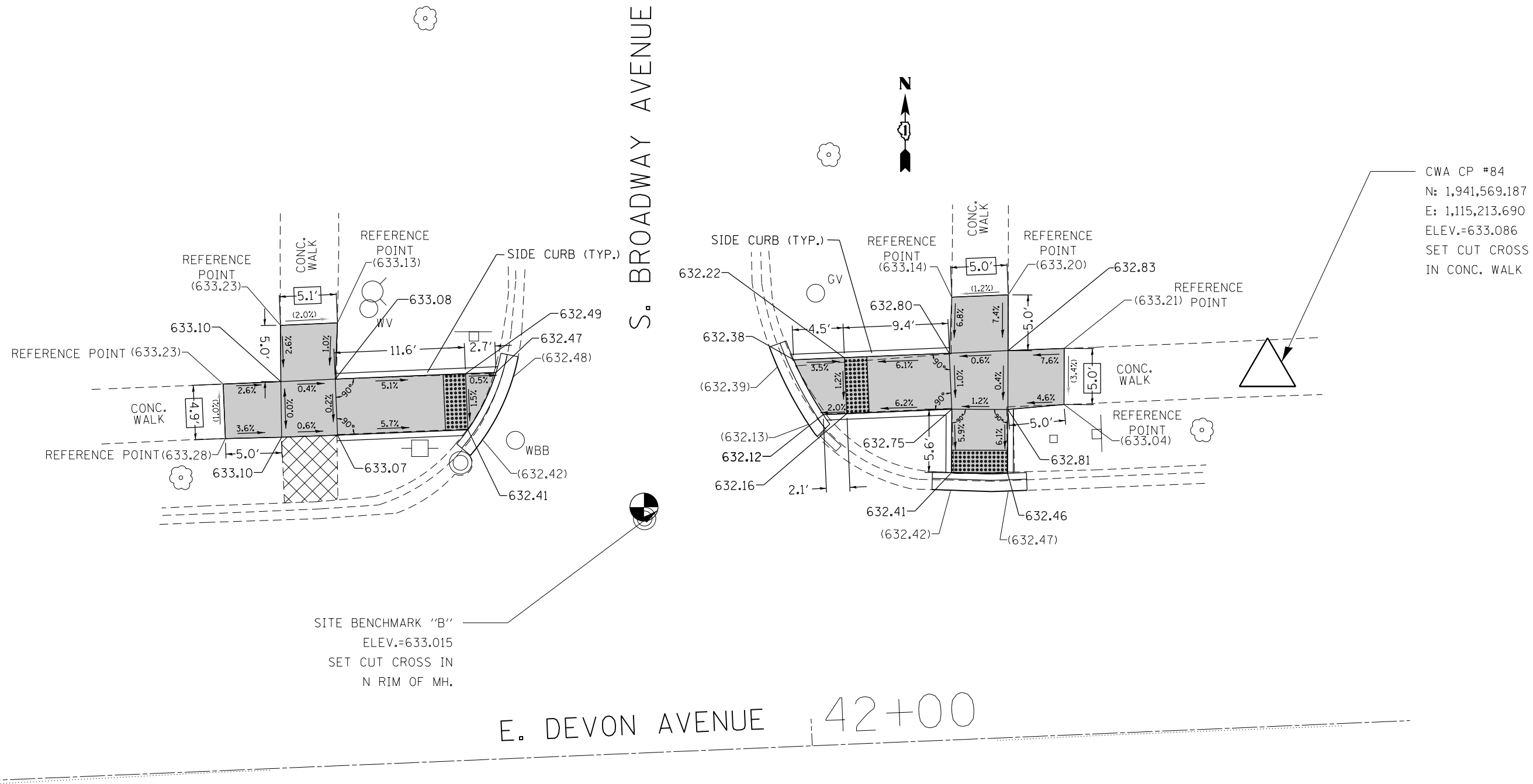


LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV. 634.374
BENCHMARK LOCATION : NORTHEAST INTERSECTION CORNER,
"X" IN NE'LY FIRE HYDRANT FLANGE BOLT. (SEE PLAN DETAIL)

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SIDEWALK DETAIL PLAN DEVON AVE AT ROSE AVE, NW AND NE CORNERS (STA. 38 + 50)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\1436\Drawings\Design\143616-sht-details.dgn		REVISED -	REVISED -			1349	(3197A&B) RS-1	COOK	70	15
Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62D18				
	PLOT DATE = 2/1/2018	DATE -	REVISED -	ILLINOIS FED. AID PROJECT						



REFERENCE BENCHMARK ELEV: 633.015
BENCHMARK LOCATION : PAVEMENT SURFACE,
NORTH RIM OF MANHOLE (SEE PLAN DETAIL)

LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

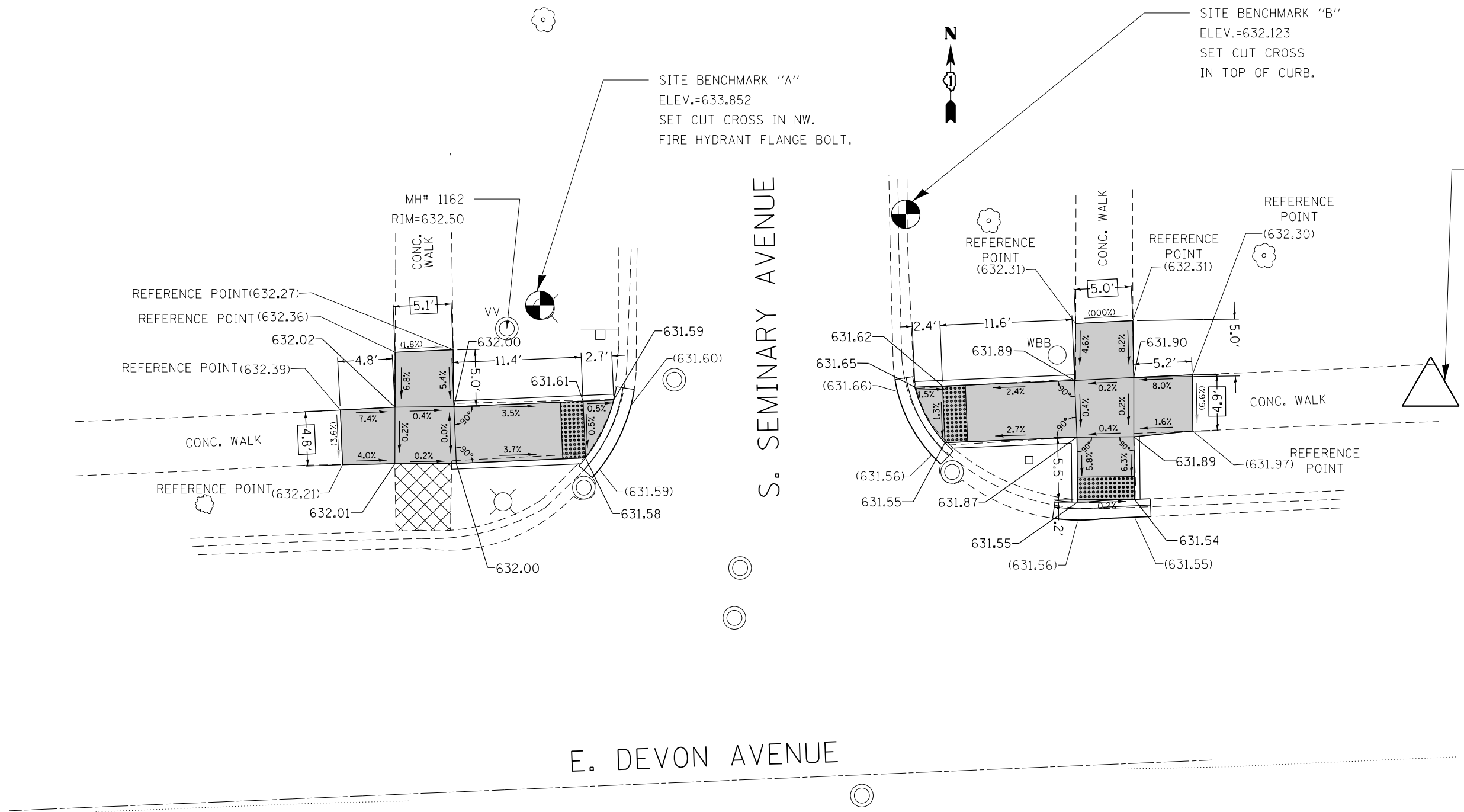
FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI43616\Design\DI43616-shd-details.dgn		REVISED -	REVISED -
Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/1/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
DEVON AVE AT BROADWAY AVE, NW AND NE CORNERS (STA. 41 + 80)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	16
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



CWA CP #87
 N: 1,941,610.982
 E: 1,116,206.148
 ELEV.=632.208
 SET CUT CROSS
 IN CONC. WALK

REFERENCE BENCHMARK "A": ELEV. 633.852

BENCHMARK "A" LOCATION : NORTHWEST INTERSECTION CORNER, "X"
 IN NW. FIRE HYDRANT FLANGE BOLT. (SEE PLAN DETAIL)

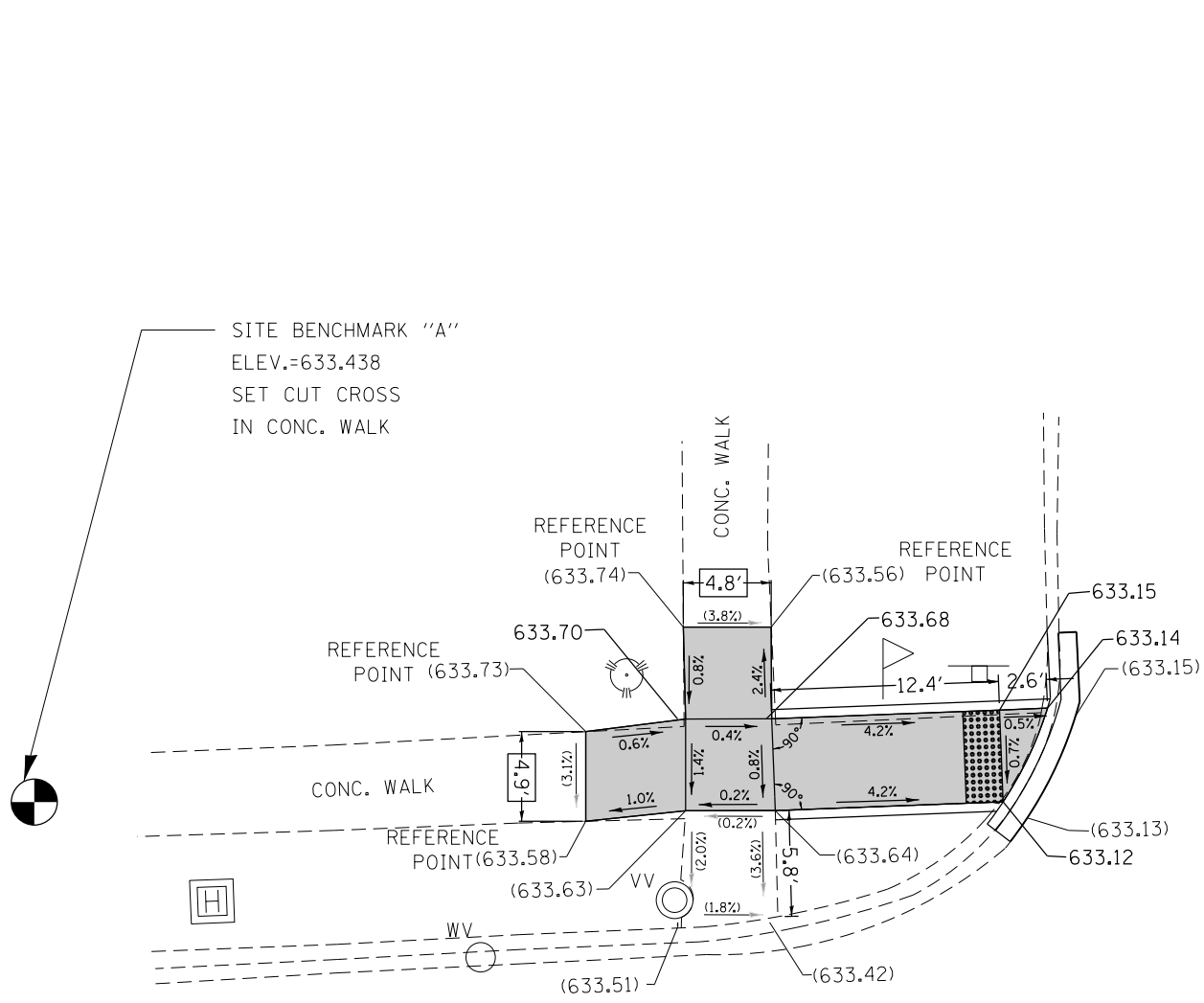
LEGEND

- xx.xx' EXISTING LENGTH
- ===== PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- [Solid Gray Box] PROPOSED SIDEWALK
- [Dotted Box] DETECTABLE WARNINGS
- [Cross-hatched Box] SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

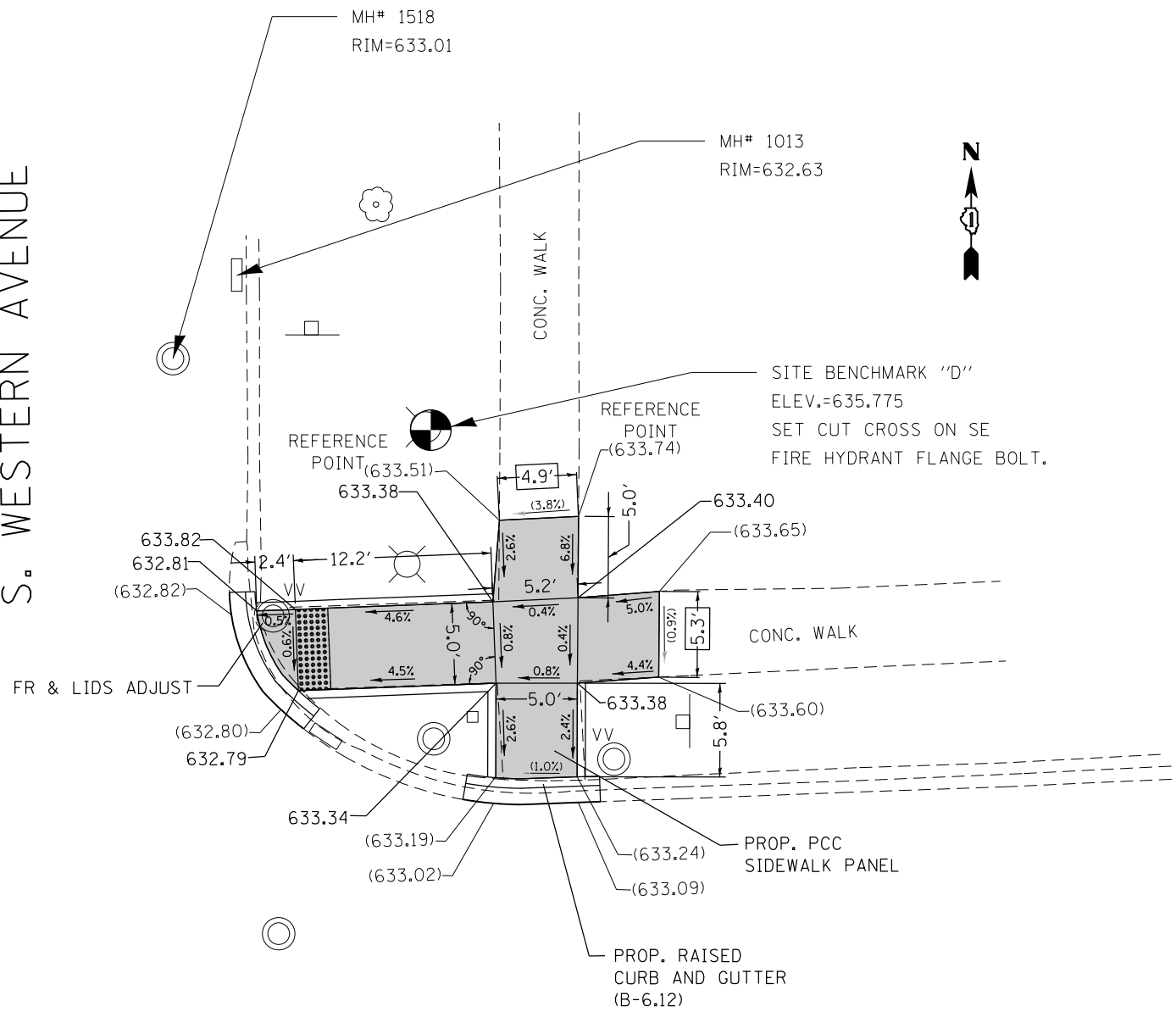
REFERENCE BENCHMARK "B": ELEV. 632.123

BENCHMARK "B" LOCATION : NORTHWEST INTERSECTION CORNER, "X" IN TOP
 OF CURB. (SEE PLAN DETAIL)

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SIDEWALK DETAIL PLAN DEVON AVE AT SEMINARY AVE, NW AND NE CORNERS (STA. 51 + 70)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\D1436\Drawings\Design\D143616-sht-details.dgn		CHECKED -	REVISED -			1349	(3197A&B) RS-1	COOK	70	19	
Default	PLOT SCALE = 10.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62D18					
	PLOT DATE = 2/1/2018					ILLINOIS FED. AID PROJECT					



S. WESTERN AVENUE

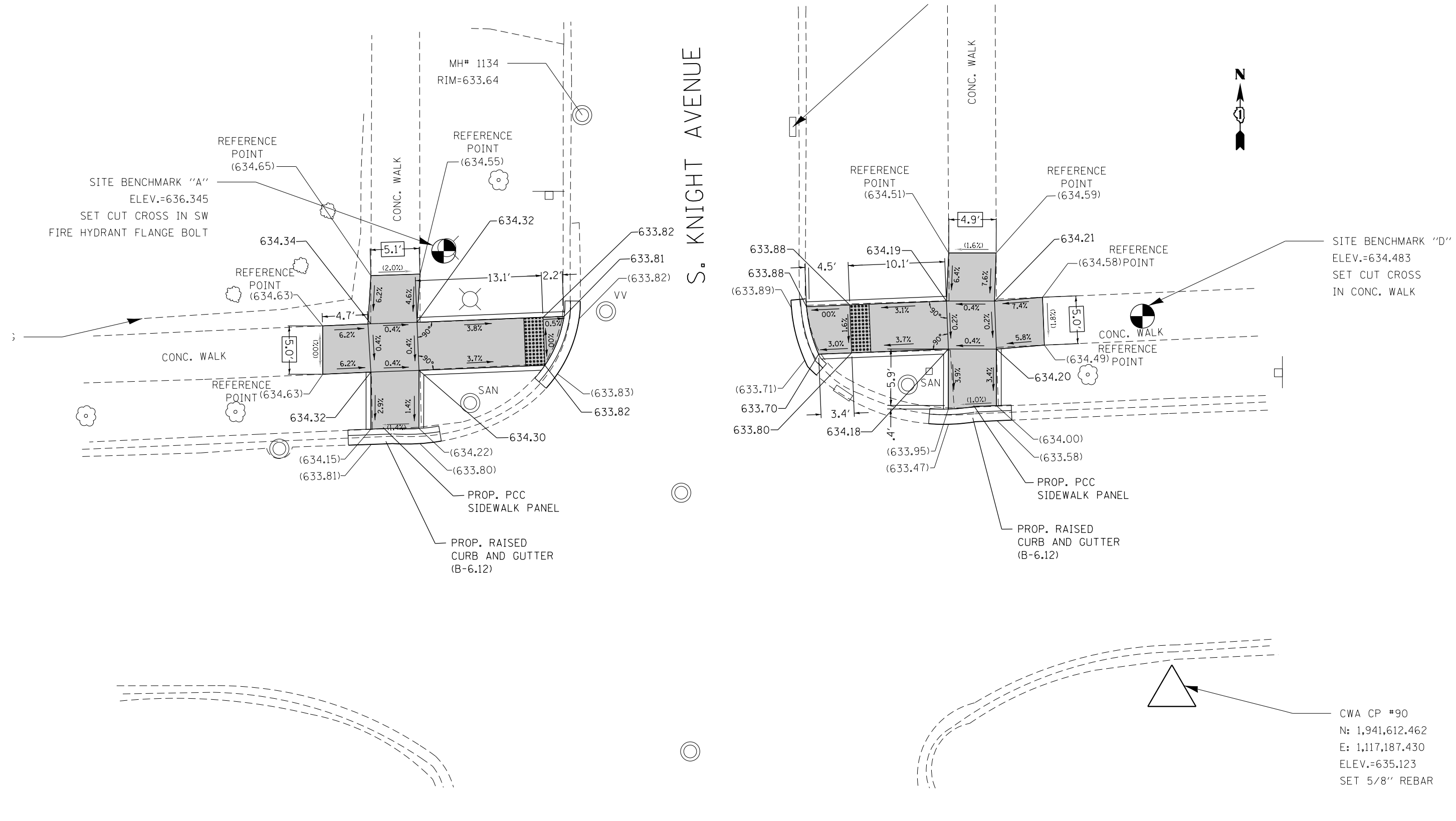


REFERENCE BENCHMARK "A": ELEV. 633.438
 BENCHMARK "A" LOCATION : NORTHWEST INTERSECTION CORNER, "X"
 IN CONC. WALK (SEE PLAN DETAIL)

LEGEND	
xx.xx'	EXISTING LENGTH
==	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
[Solid Grey Box]	PROPOSED SIDEWALK
[Dotted Box]	DETECTABLE WARNINGS
[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK "D": ELEV. 635.775
 BENCHMARK "D" LOCATION : NORTHEAST INTERSECTION CORNER, "X"
 ON SE FIRE HYDRANT FLANGE BOLT. (SEE PLAN DETAIL)

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SIDEWALK DETAIL PLAN DEVON AVE AT WESTERN AVE, NW AND NE CORNERS (STA. 58 + 20)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\1436\Browns\Design\143616-shd-details.dgn	PLotted SCALE = 10.0000' / in.	CHECKED -	REVISED -			1349	(3197A&B) RS-1	COOK	70	21
Default	PLotted DATE = 2/1/2018	DATE -	REVISED -			CONTRACT NO. 62D18				

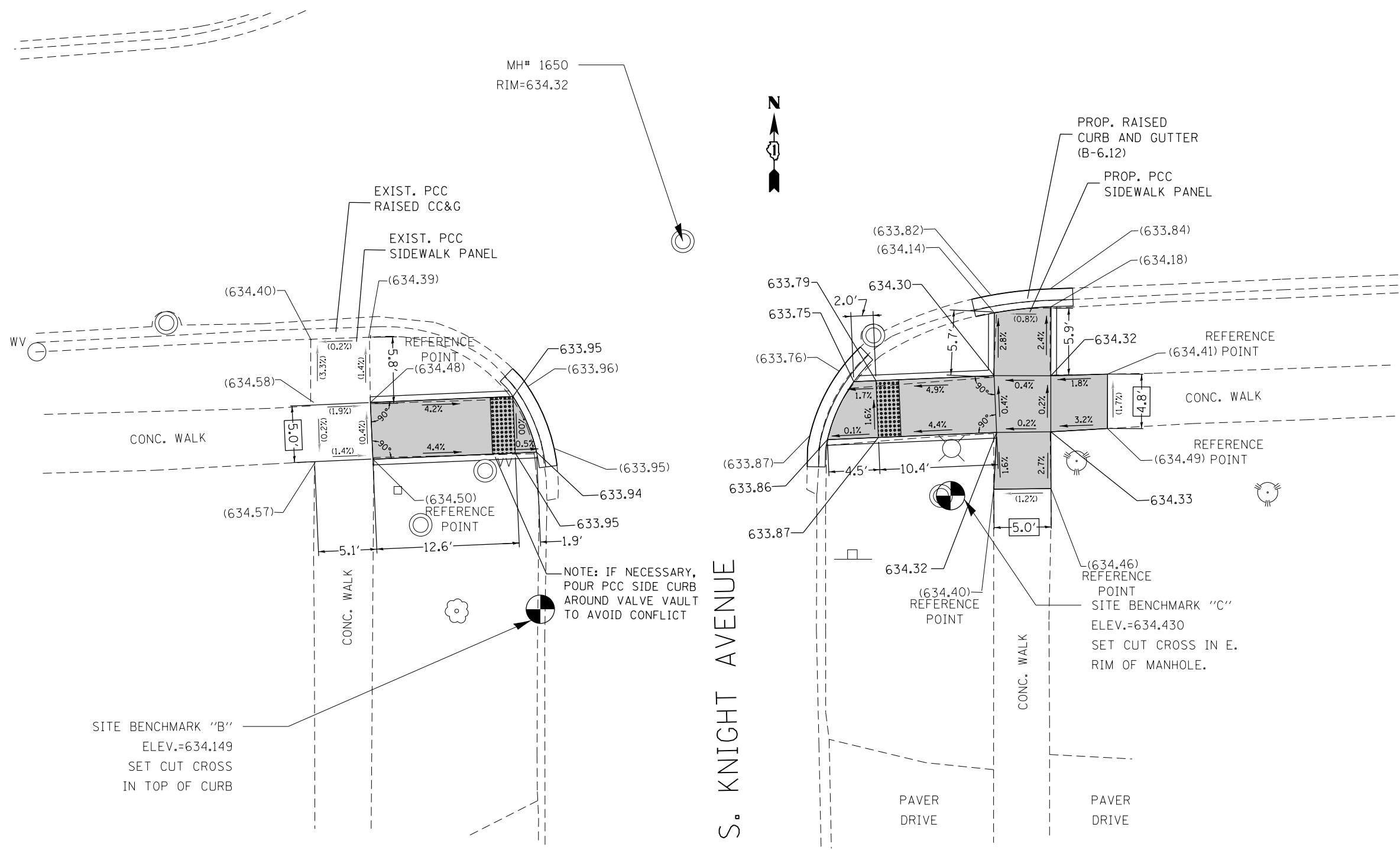


REFERENCE BENCHMARK "A": ELEV. 636.345
 BENCHMARK "A" LOCATION : NORTHWEST INTERSECTION CORNER, "X"
 IN SW FIRE HYDRANT FLANGE BOLT.

LEGEND

xx.xx'	EXISTING LENGTH	[Solid Gray Box]	PROPOSED SIDEWALK
==	PROPOSED SIDE CURB	[Grid Pattern Box]	DETECTABLE WARNINGS
()	EXISTING ELEVATION/SLOPE	[Cross-hatch Box]	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK "D": ELEV. 634.483
 BENCHMARK "D" LOCATION : NORTHEAST INTERSECTION CORNER, "X"
 IN CONC. CARRIAGE WALK



S. KNIGHT AVENUE

REFERENCE BENCHMARK "B": ELEV. 634.149

BENCHMARK "B" LOCATION : SOUTHWEST INTERSECTION CORNER, "X" IN TOP OF CURB.

LEGEND

xx.xx'

EXISTING LENGTH

====

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK "C": ELEV. 634.430

BENCHMARK "C" LOCATION : SOUTHEAST INTERSECTION CORNER, "X" IN E. RIM OF MANHOLE.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
pw\11084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\1436\BROWN\Design\143616-sht-details.dgn		REVISED -	REVISED -
Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/1/2018	DATE -	REVISED -

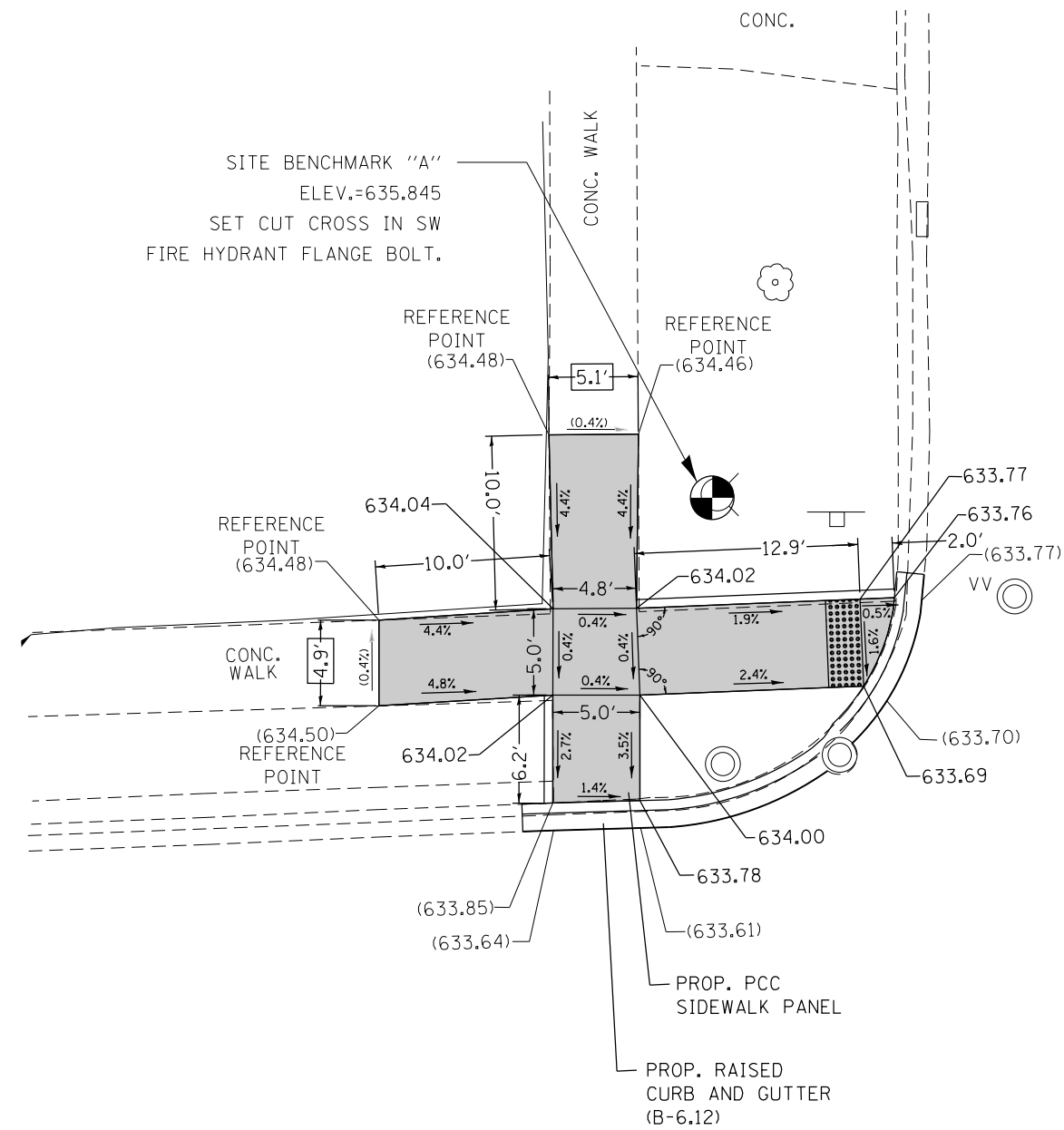
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
DEVON AVE AT KNIGHT AVE, SW AND SE CORNERS (STA. 61 + 50)**

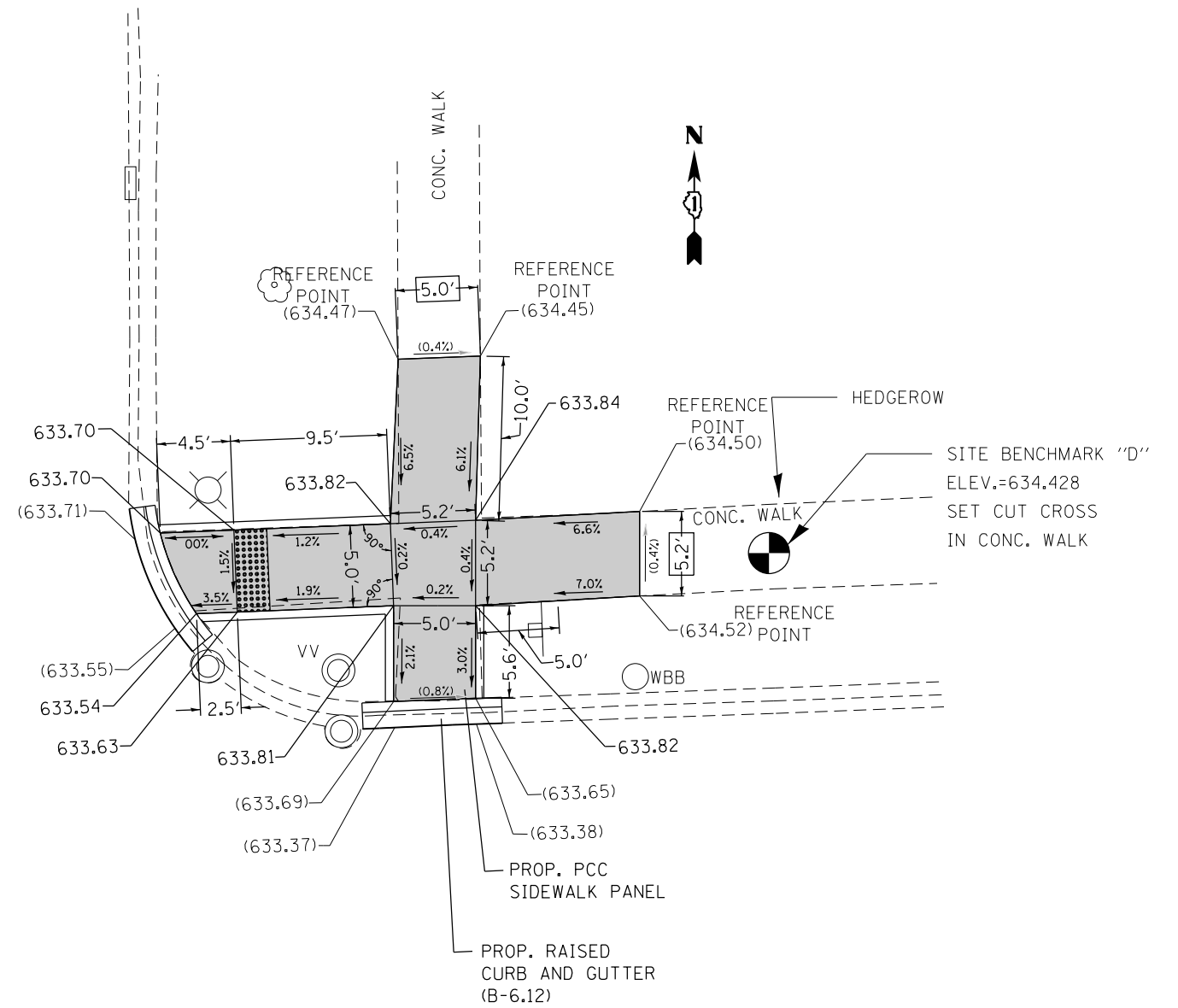
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	24
CONTRACT NO. 62D18				

ILLINOIS FED. AID PROJECT



S. LINCOLN AVENUE



REFERENCE BENCHMARK "A": ELEV. 635.845

BENCHMARK "A" LOCATION : NORTHWEST INTERSECTION CORNER, "X"
IN SW FIRE HYDRANT FLANGE BOLT.

REFERENCE BENCHMARK "D": ELEV. 634.428

BENCHMARK "D" LOCATION : NORTHEAST INTERSECTION CORNER, "X"
IN CONC. WALK.

LEGEND



EXISTING LENGTH



PROPOSED SIDE CURB



EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



PROPOSED SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

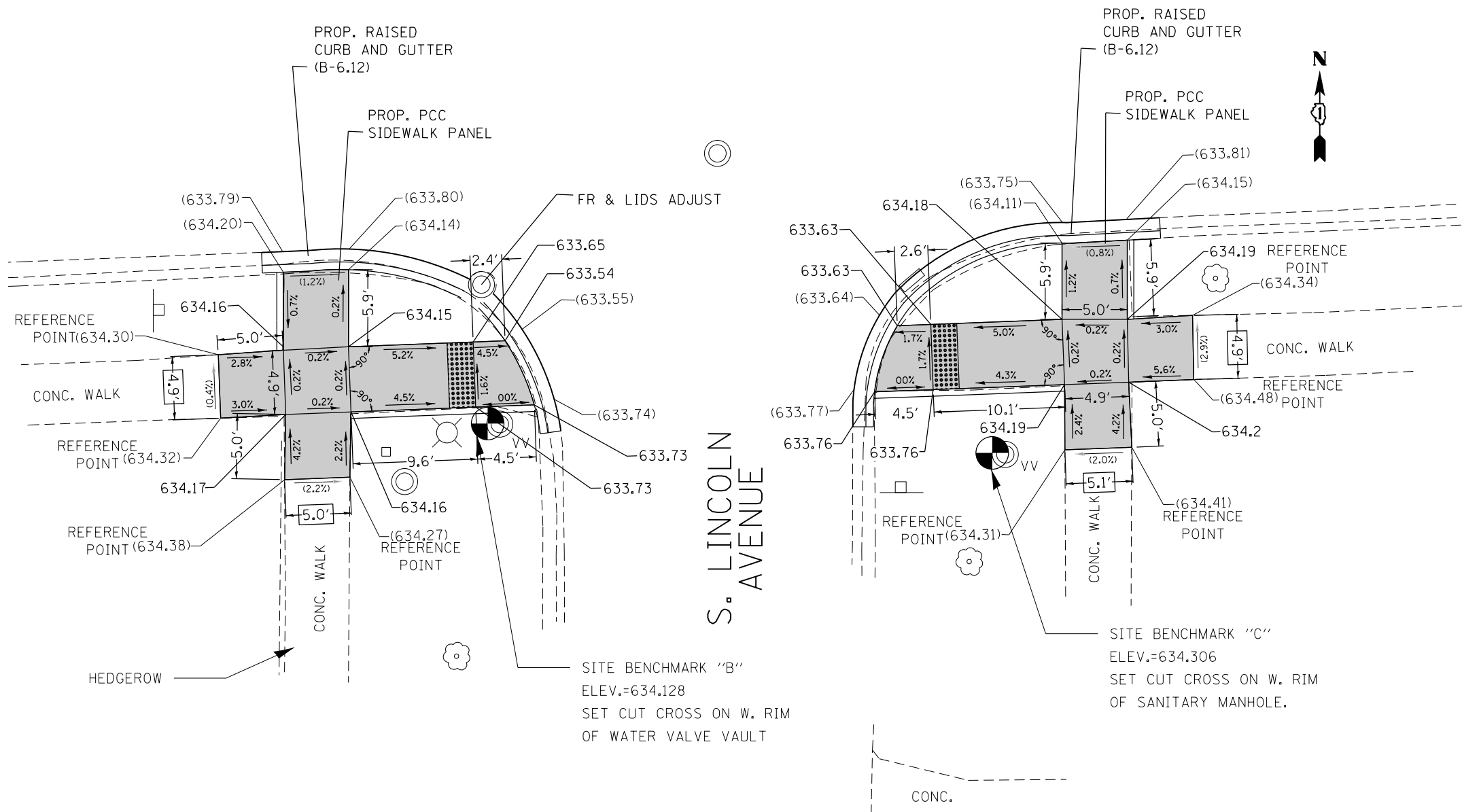
FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
pw\ill\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\D143616\State Design\143616-shd-details.dgn		REVISED -	REVISED -
Default	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/1/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
DEVON AVE AT LINCOLN AVE, NW AND NE CORNERS (STA. 65+00)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
--------	-------	----	--------	------	----	------

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	25
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62D18	



S. LINCOLN AVENUE

REFERENCE BENCHMARK "B": ELEV. 634.128

BENCHMARK "B" LOCATION : SOUTHWEST INTERSECTION CORNER, "X"
ON W. RIM OF WATER VALVE VAULT.

LEGEND

xx.xx'

EXISTING LENGTH

==

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK "C": ELEV. 634.306

BENCHMARK "C" LOCATION : SOUTHEAST INTERSECTION CORNER, "X"
ON W. RIM OF SANITARY MANHOLE.

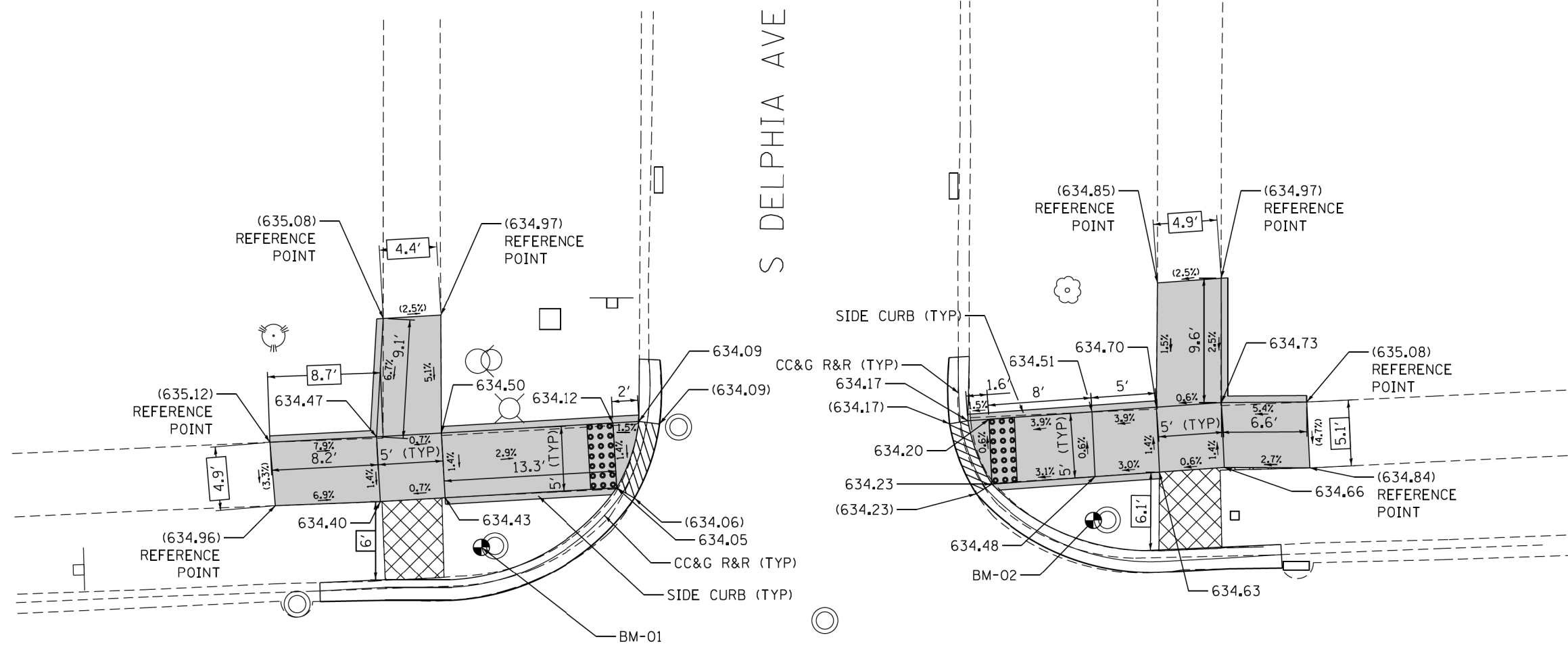
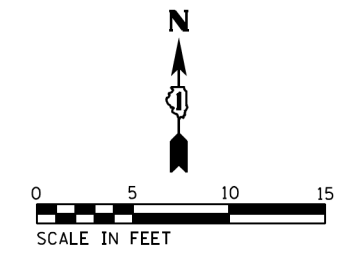
FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
pw\11084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\1436\Drawings\Design\143616-sht-details.dgn		REVISED -	REVISED -
Default	PLOT DATE = 2/1/2018	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
DEVON AVE AT LINCOLN AVE, SW AND SE CORNERS (STA. 65+00)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	26
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



E DEVON AVE

NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 01 EL 634.802
BENCHMARK : "X" CUT ON W RIM OF SAN MH
LOCATION : ON NW COR OF E DEVON AVE AND S DELPHIA AVE

LEGEND

XX.X' EXISTING LENGTH
() EXISTING ELEVATION/SLOPE

	PC CONC SIDEWALK 5 SUB GRAN MAT B 4		DEPRESSED CURB & GUTTER
	DETECTABLE WARNINGS		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 02 EL 634.790
BENCHMARK : "X" CUT ON W RIM OF SAN MH
LOCATION : ON NE COR OF E DEVON AVE AND S DELPHIA AVE

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\DCON\Design\Plan\Plotsheets\062018\sh-ramp-details\sh-ramp-01.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 2/27/2018	DATE - 1/24/2018	REVISED -

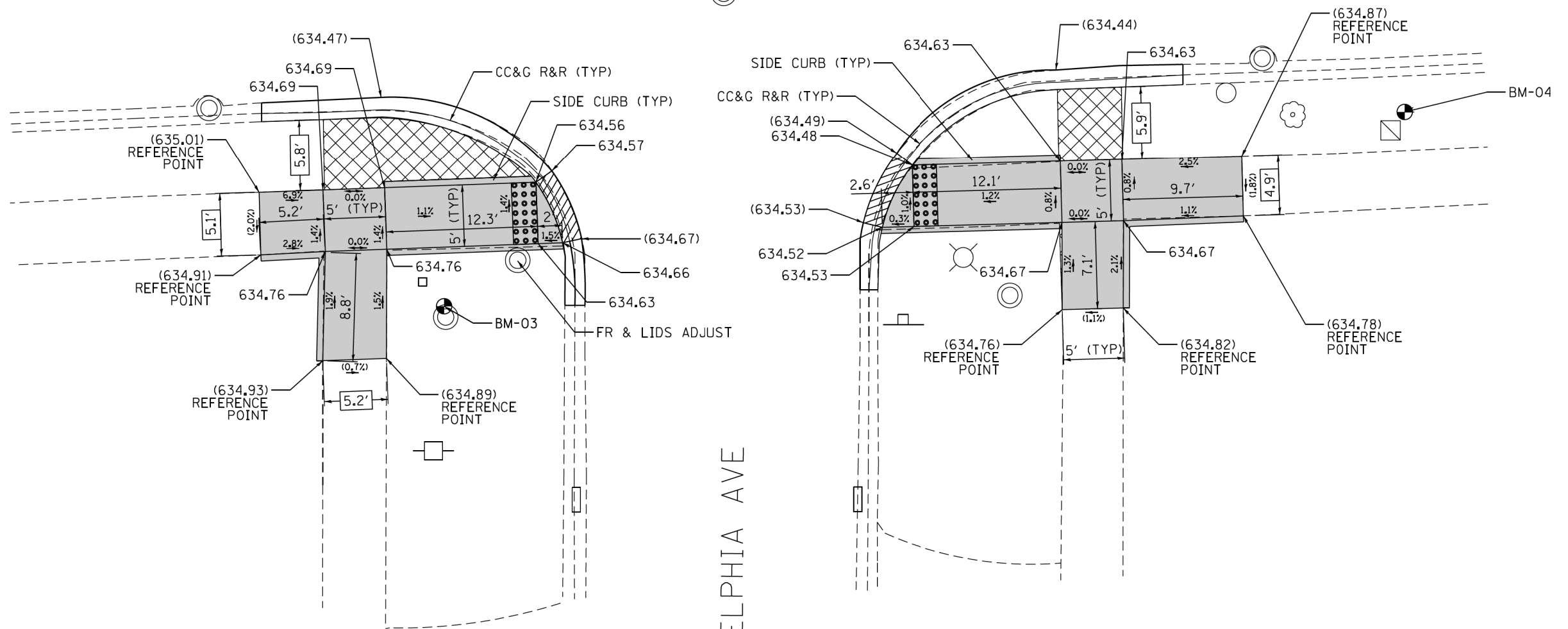
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS E DEVON AVE AT S DELPHIA AVE			
SCALE: 1" = 5'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	27
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



E DEVON AVE



S DELPHIA AVE

NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 03 EL 634.919
BENCHMARK : "X" ON N RIM OF CB
LOCATION : ON SW COR OF E DEVON AVE AND S DELPHIA AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 04 EL 634.806
BENCHMARK : "X" ON NE COR TS HH
LOCATION : ON SE COR OF E DEVON AVE AND S DELPHIA AVE

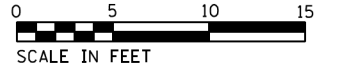
FILE NAME = Z:\17802\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\162018\sh-r-detail1 Ramp-02.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000 ' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 2/27/2018	DATE - 1/24/2018	REVISED -

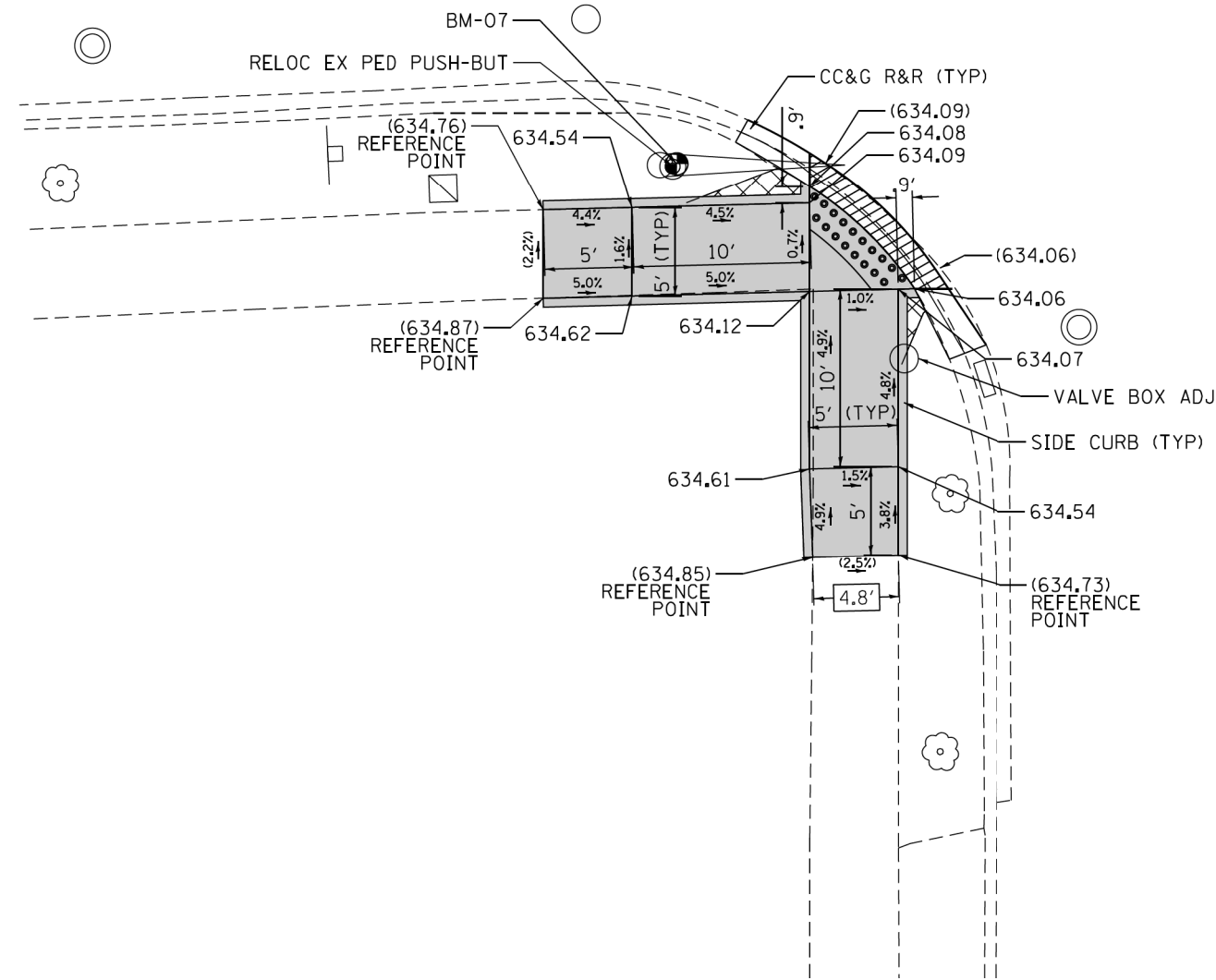
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS E DEVON AVE AT S DELPHIA AVE	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

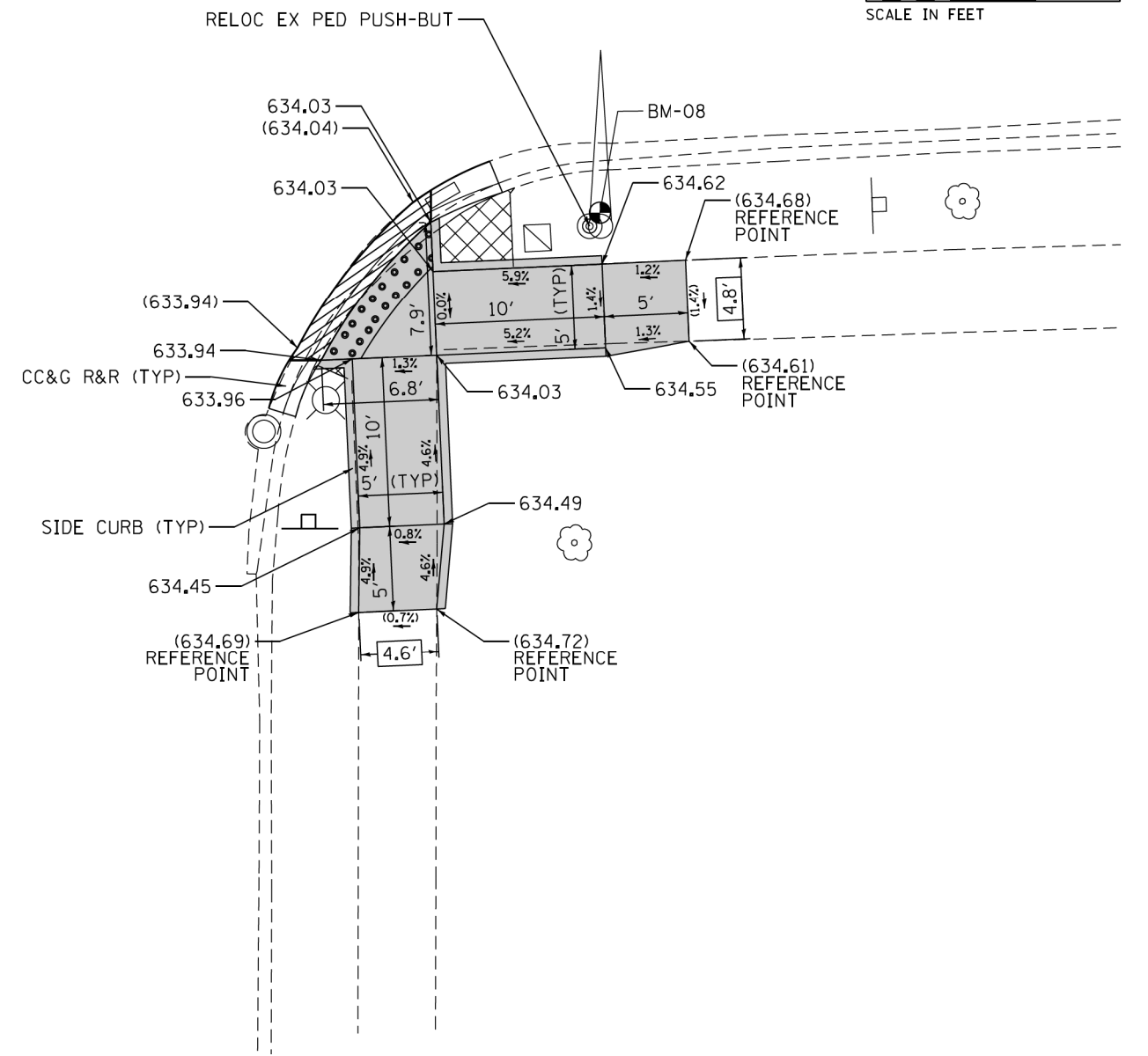
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	28
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



E DEVON AVE



S GREENWOOD AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 07 EL 635.198
BENCHMARK : "X" CUT ON E SIDE OF CONC BASE FOR TS
LOCATION : ON SW COR OF E DEVON AVE AND S GREENWOOD AVE

LEGEND

XX.X' EXISTING LENGTH
() EXISTING ELEVATION/SLOPE

PC CONC SIDEWALK 5
SUB GRAN MAT B 4
DETECTABLE WARNINGS
SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

DEPRESSED CURB
& GUTTER

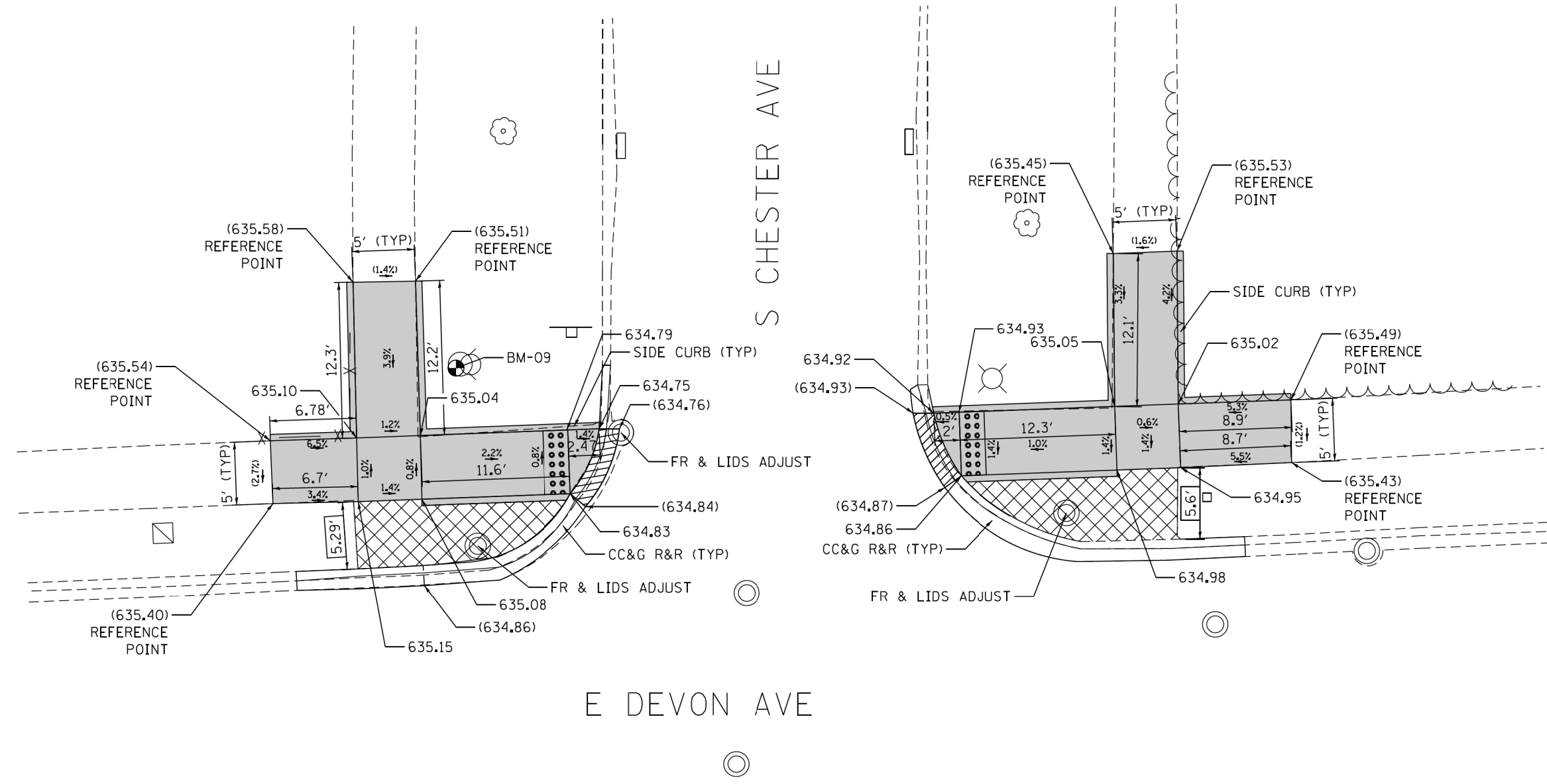
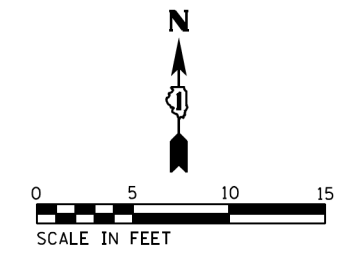
REFERENCE BENCHMARK 08 EL 634.829
BENCHMARK : "X" CUT ON N SIDE OF CONC BASE FOR TS
LOCATION : ON SE COR OF E DEVON AVE AND S GREENWOOD AVE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
E DEVON AVE AT S GREENWOOD AVE

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	30
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 09 EL 636.631
BENCHMARK : "X" CUT IN SW FH FLANGE BOLT
LOCATION : ON NW COR OF E DEVON AVE AND S CHESTER AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10,0000' / 1in	CHECKED - JH	REVISED -
PLOT DATE = 2/27/2018	DATE - 1/24/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
E DEVON AVE AT S CHESTER AVE**

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

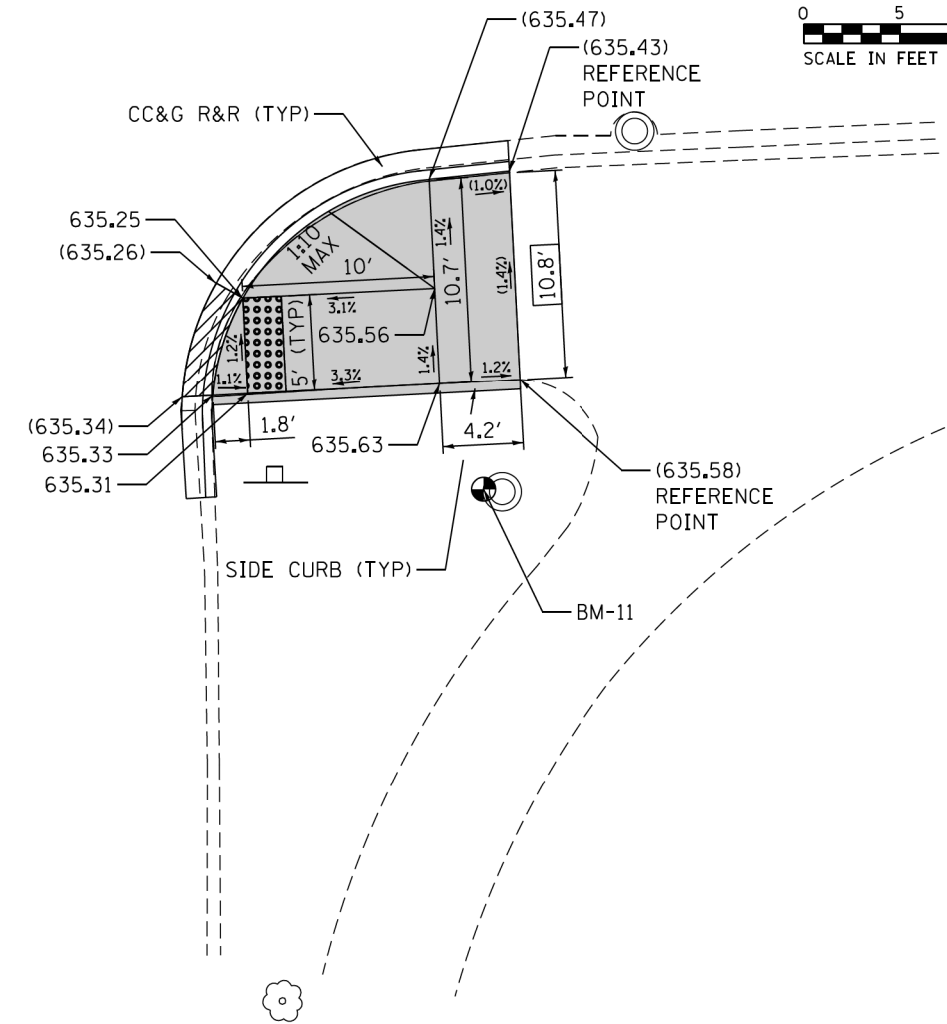
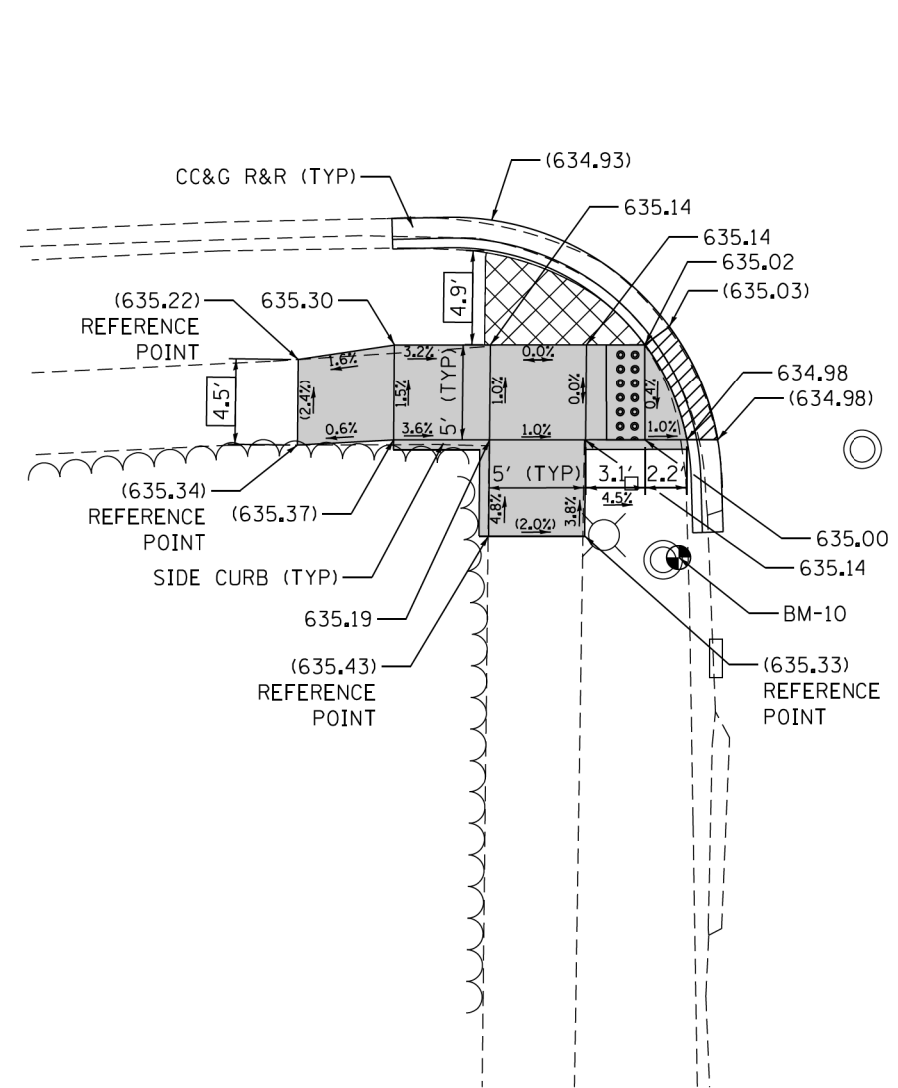
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	31
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\162018\sh-r-detail1 Ramp-05.dgn



E DEVON AVE

S CHESTER AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 10 EL 635.393
BENCHMARK : "X" CUT IN E RIM OF SAN MH
LOCATION : ON SW COR OF E DEVON AVE AND S CHESTER AVE

LEGEND

- XX.X' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 11 EL 635.509
BENCHMARK : "X" CUT IN W RIM OF CB
LOCATION : ON SE COR OF E DEVON AVE AND S CHESTER AVE

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\062018\sh-ramp\details\sh-ramp-06.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 2/27/2018	DATE - 1/24/2018	REVISED -

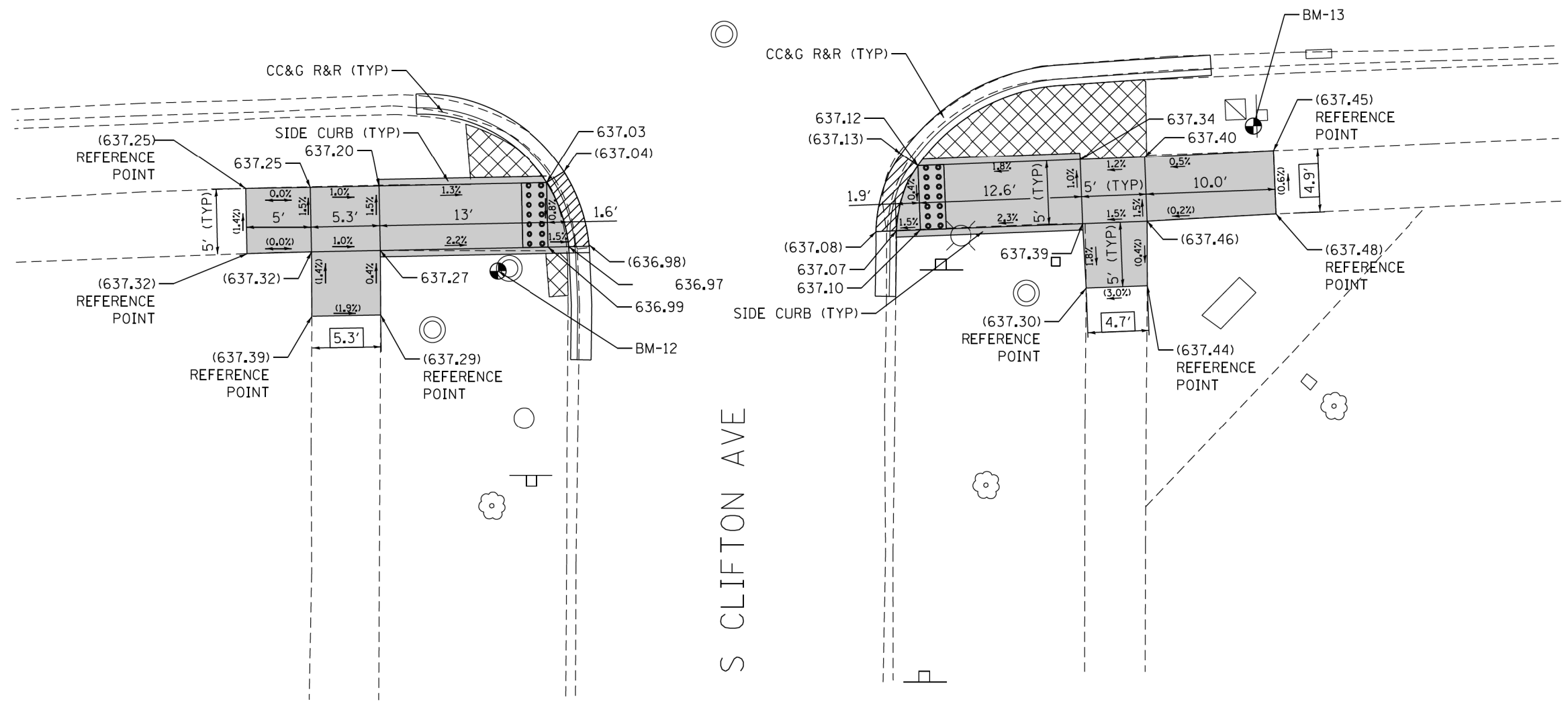
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP DETAILS E DEVON AVE AT S CHESTER AVE			
SCALE: 1" = 5'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	32
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



E DEVON AVE



REFERENCE BENCHMARK 12 EL 637.241
BENCHMARK : "X" CUT IN W RIM OF WV VLT
LOCATION : ON SW COR OF E DEVON AVE AND S CLIFTON AVE

LEGEND

- XX.X' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 13 EL 637.457
BENCHMARK : "X" CUT IN SE COR OF TS HH
LOCATION : ON SE COR OF DEVON AVE AND S CLIFTON AVE

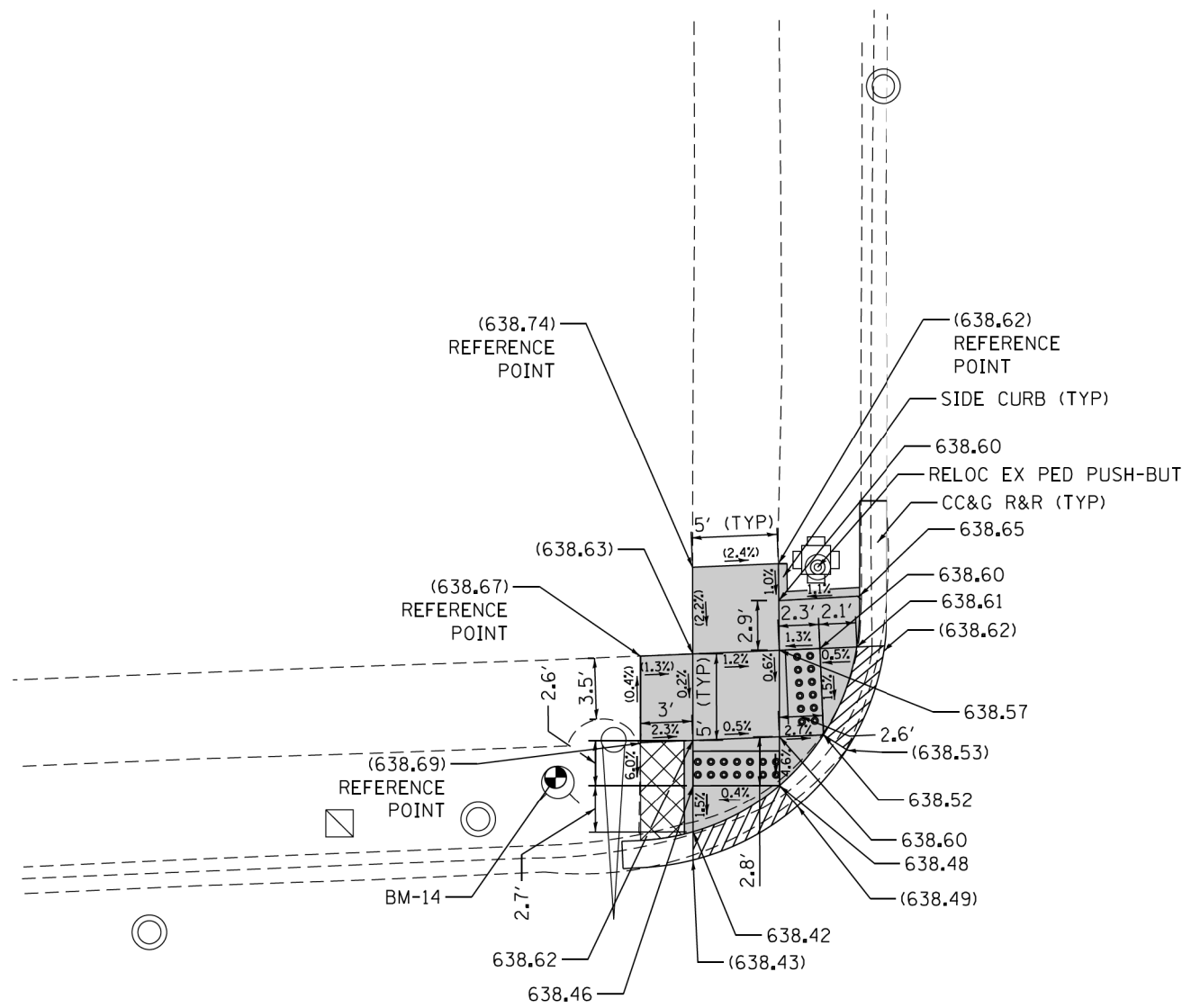
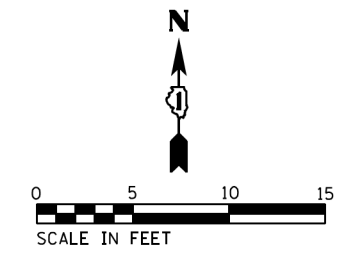
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\062018\sh-r-detail1 Ramp-07.dgn

USER NAME = RGal1	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10,0000' / 1in	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

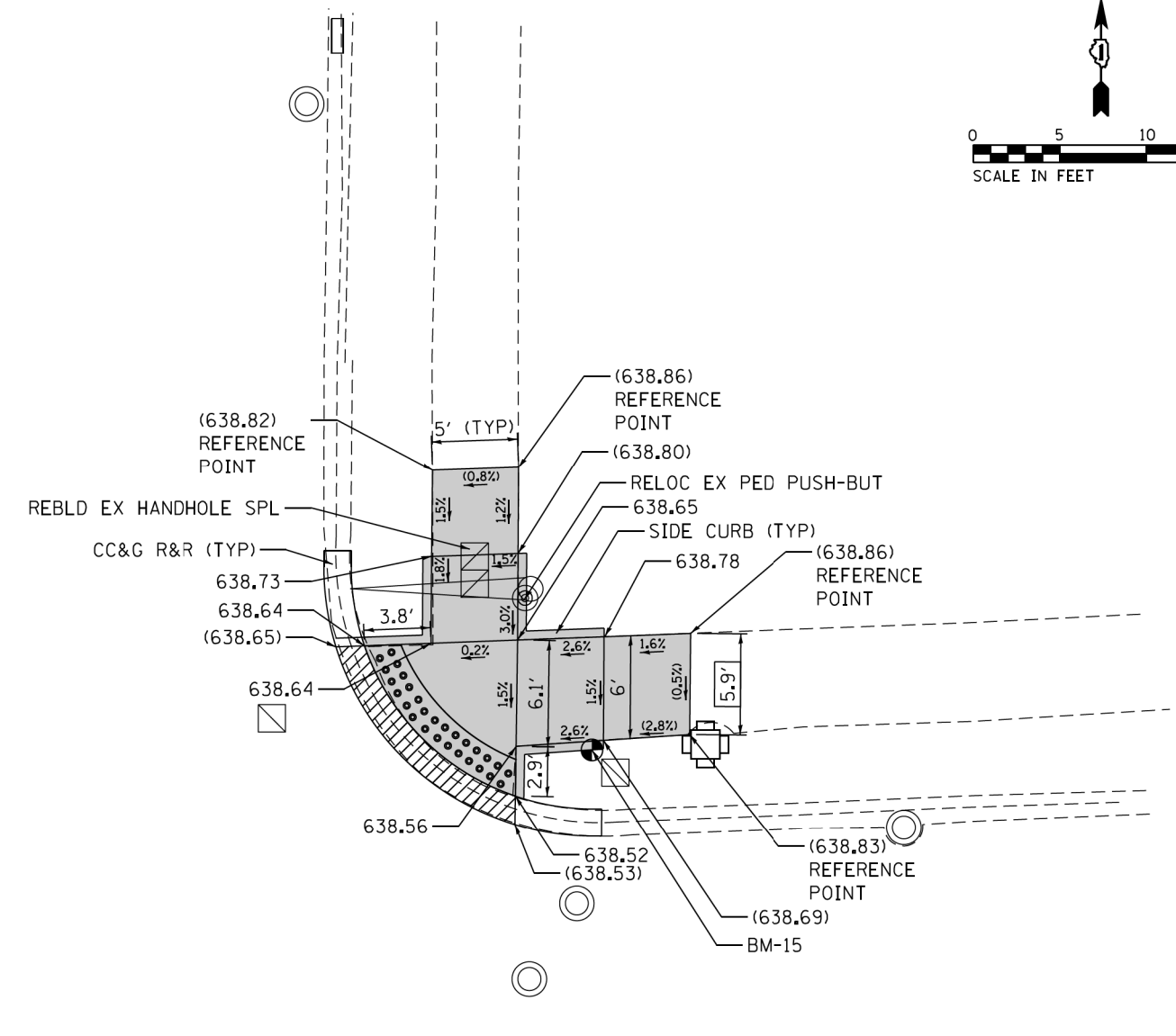
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP DETAILS			
E DEVON AVE AT S CLIFTON AVE			
SCALE: 1" = 5'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	33
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



S CUMBERLAND AVE



E DEVON AVE

NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 14 EL 639.962
BENCHMARK : "X" CUT IN N FH FLANGE BOLT
LOCATION : ON NW COR OF E DEVON AVE AND S CUMBERLAND AVE

LEGEND

PC CONC SIDEWALK 5 SUB GRAN MAT B 4	DEPRESSED CURB & GUTTER
EXISTING LENGTH	DETECTABLE WARNINGS
EXISTING ELEVATION/SLOPE	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 15 EL 639.768
BENCHMARK : "X" CUT IN NE COR OF TS HH
LOCATION : ON NE COR OF E DEVON AVE AND S CUMBERLAND AVE

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave\Central Ave\CON\Design\Plan\Plotsheets\062018\sh-r-detail15Ramp-08.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
DRAWN - RG / MS / KW	CHECKED - JH	REVISED -
PLLOT SCALE = 10.0000 ' / 1"	DATE - 1/24/2018	REVISED -
Default		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

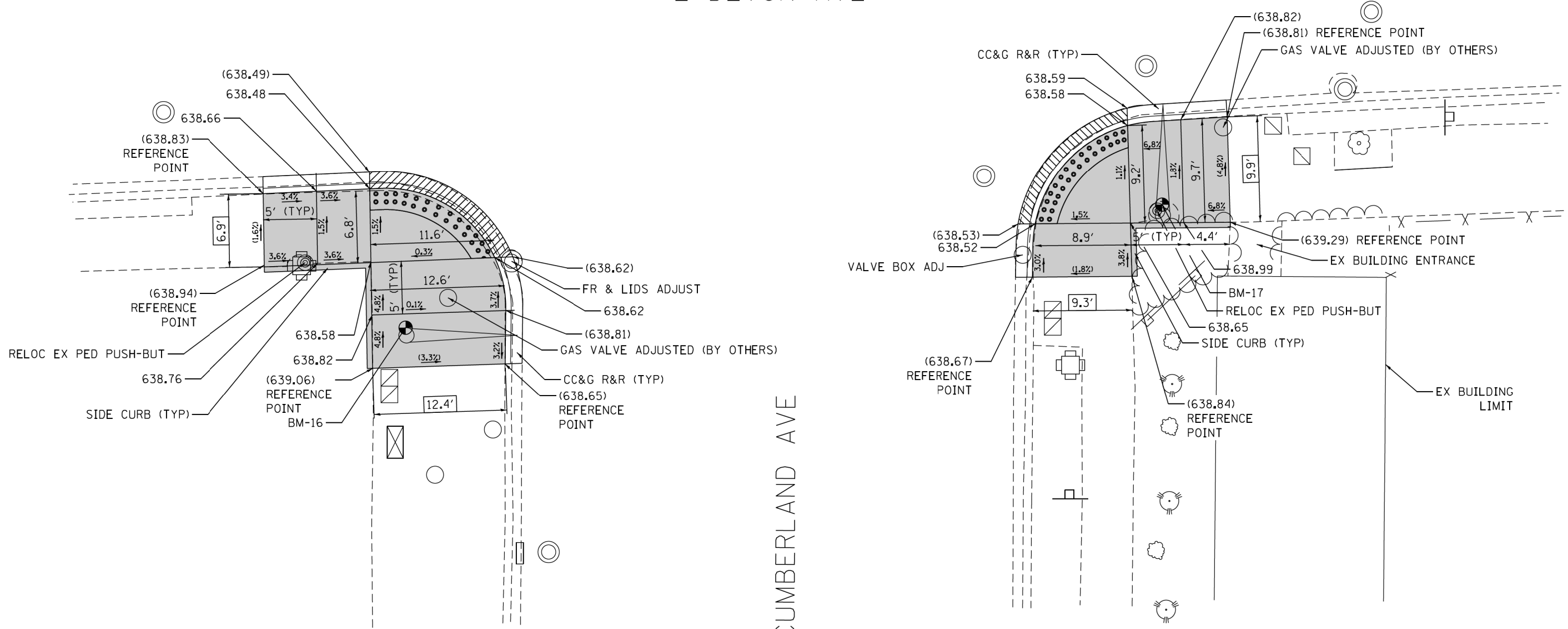
ADA RAMP DETAILS E DEVON AVE AT S CUMBERLAND AVE			
SCALE: 1" = 5'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	34
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



E DEVON AVE

S CUMBERLAND AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 16 EL 639.367
BENCHMARK : "X" CUT IN N SIDE TS BASE
LOCATION : ON SW COR OF E DEVON AVE AND S CUMBERLAND AVE

LEGEND

- [XX.X'] EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE

- [Solid Grey Box] PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- [Dotted Box] DETECTABLE WARNINGS
- [Cross-hatched Box] SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- [Hatched Box] DEPRESSED CURB & GUTTER

REFERENCE BENCHMARK 17 EL 639.274
BENCHMARK : "X" CUT IN N SIDE TS BASE
LOCATION : ON SE COR OF E DEVON AVE AND S CUMBERLAND AVE

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave.Central Ave\CON\Design\Plan\Plotsheets\062018\sh-ramp\sh-ramp-09.dgn

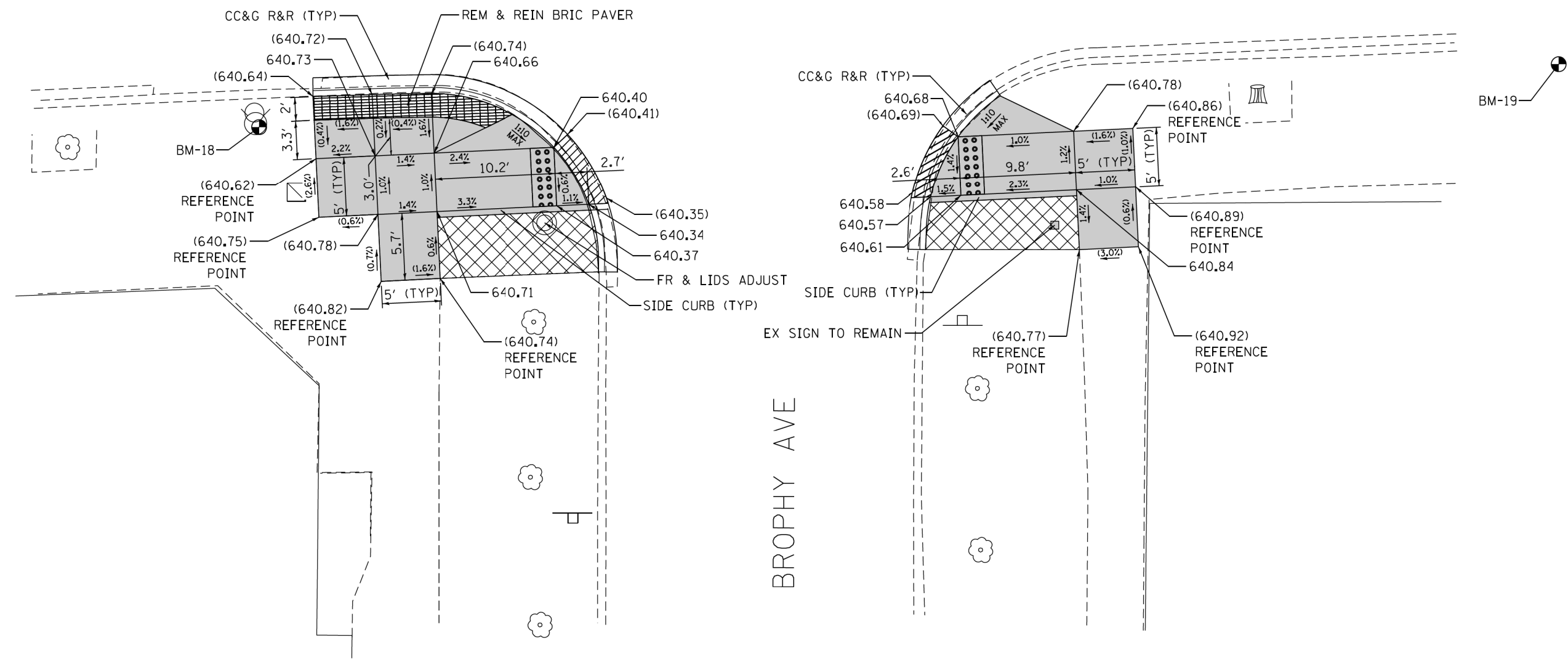
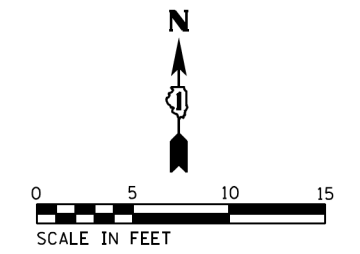
USER NAME = RGal1	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1in	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS	
E DEVON AVE AT S CUMBERLAND AVE	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	35
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 18 EL 642.050
BENCHMARK : "X" CUT IN SE FH FLANGE BOLT
LOCATION : ON SW COR OF E DEVON AVE AND BROPHY AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- XX.X' EXISTING LENGTH
- DETECTABLE WARNINGS
- () EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 19 EL 640.907
BENCHMARK : "X" CUT IN N SIDE OF CONC LP BASE
LOCATION : ON SE COR OF E DEVON AVE AND BROPHY AVE

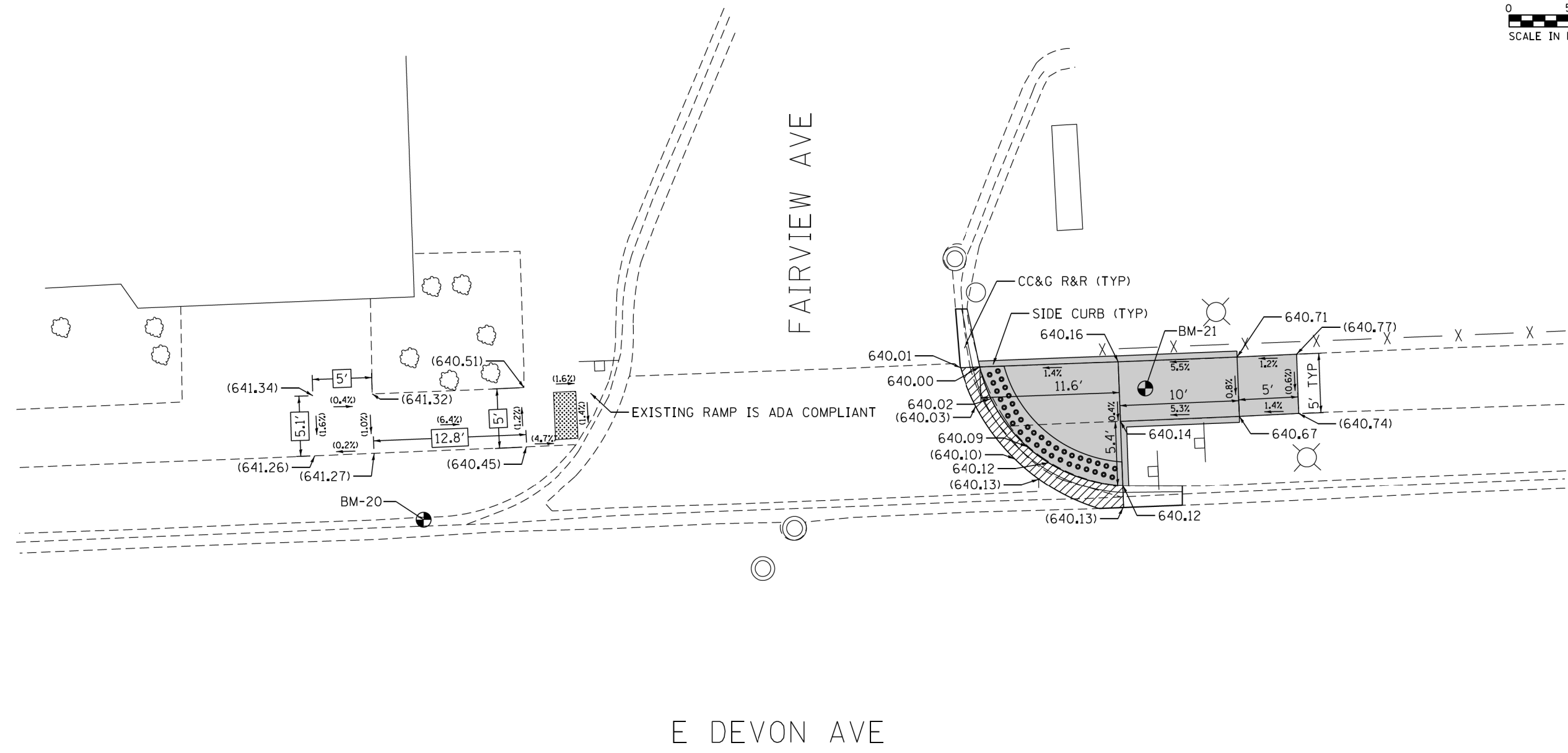
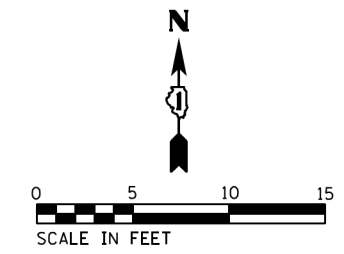
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\062018\sh-ramp-10.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP DETAILS E DEVON AVE AT BROPHY AVE	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	36
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 20 EL 640.165
BENCHMARK : "X" CUT IN TOP OF CURB
LOCATION : ON NW COR OF E DEVON AVE AND FAIRVIEW AVE

LEGEND

- XX.X' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSIBLE CURB & GUTTER
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 21 EL 640.515
BENCHMARK : "X" CUT IN CONC WALK
LOCATION : ON NE COR OF E DEVON AVE AND FAIRVIEW AVE

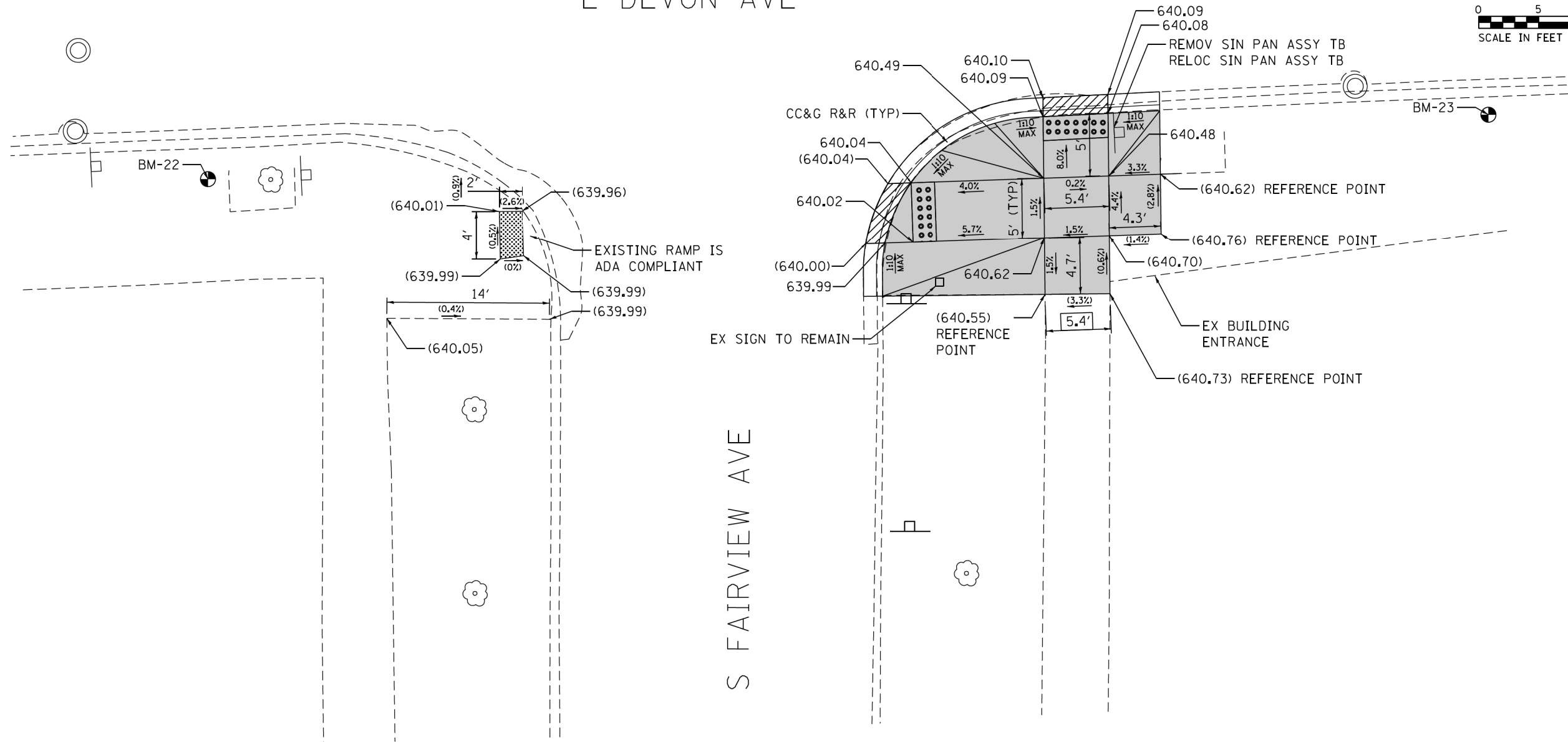
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
E DEVON AVE AT FAIREVIEW AVENUE
SCALE: 1" = 5'
SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	37
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\DCON\Design\Plan\Plotsheets\062018\sh-ramp-details\sh-ramp-1.dgn

E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 22 EL 641.489
BENCHMARK : "X" CUT ON SE FH FLANGE BOLT
LOCATION : ON SW COR OF E DEVON AVE AND S FAIRVIEW AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- XX.X' EXISTING LENGTH
- DETECTABLE WARNINGS
- () EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 23 EL 640.746
BENCHMARK : "X" CUT ON N SIDE OF CONC BASE FOR LP
LOCATION : ON SE COR OF E DEVON AVE AND S FAIRVIEW AVE

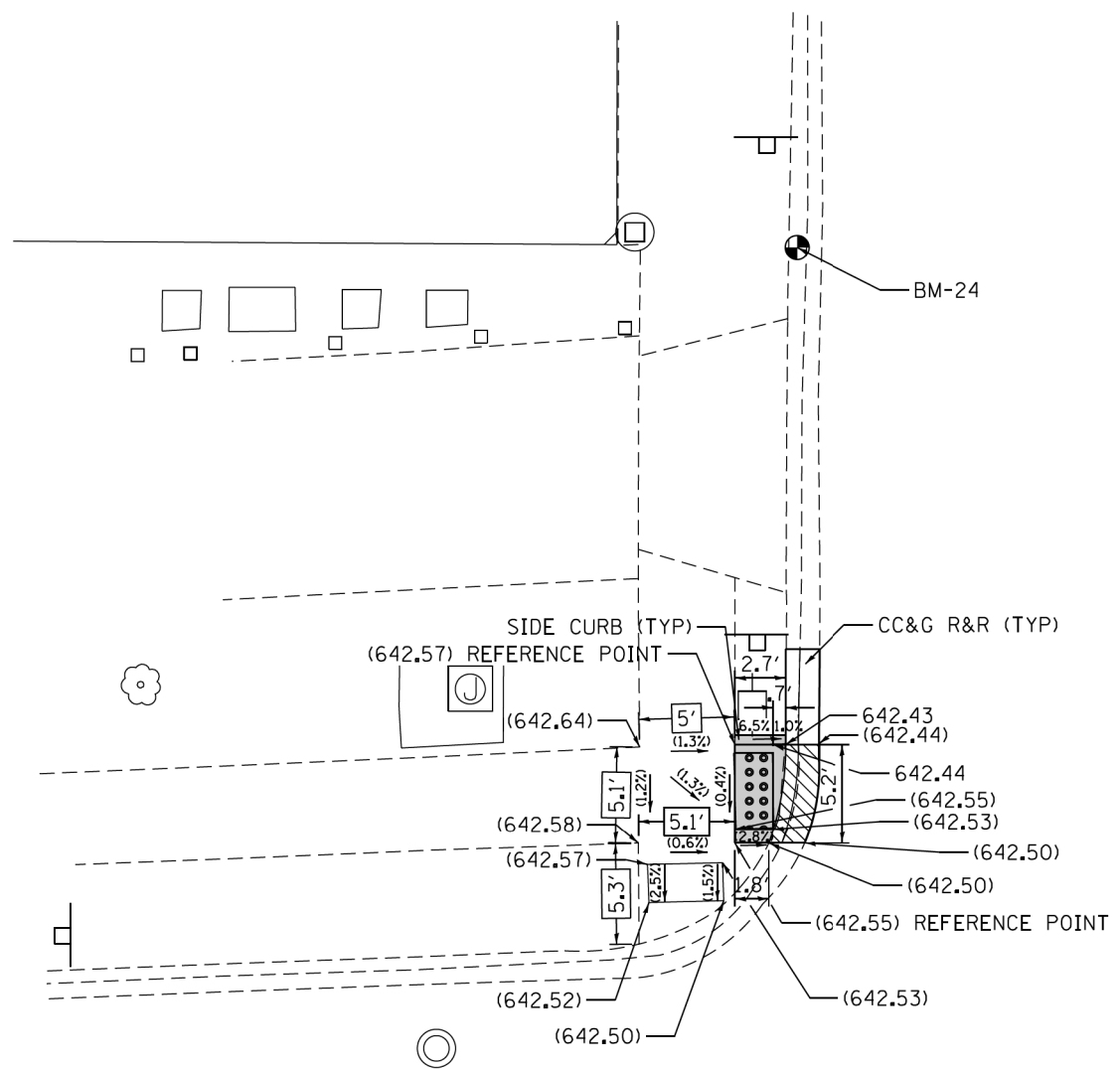
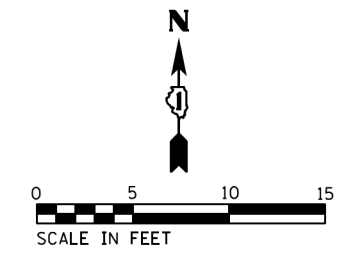
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\062018\sh-r-detail1sRamp-12.dgn

USER NAME = RGal1	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10,0000 ' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

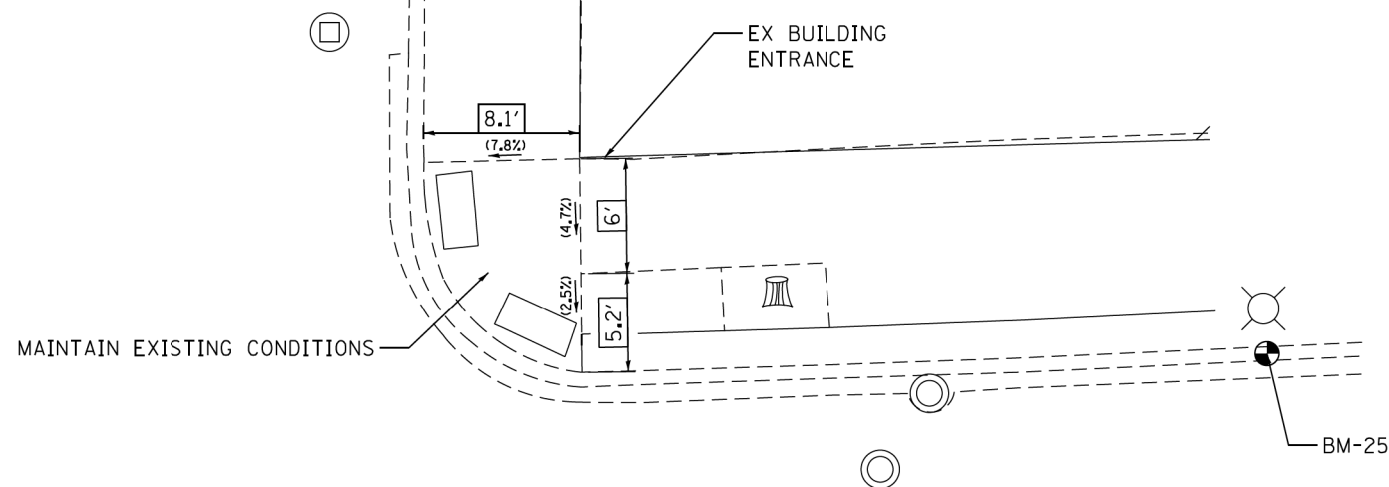
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS	
E DEVON AVE AT S FAIRVIEW AVE	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	38
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



S PROSPECT AVE



E DEVON AVE

NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 24 EL 642.403
BENCHMARK : "X" CUT ON TOP OF CURB
LOCATION : ON NW COR OF E DEVON AVE AND S PROSPECT AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 25 EL 642.961
BENCHMARK : "X" CUT ON TOP OF CURB
LOCATION : ON NE COR OF E DEVON AVE AND S PROSPECT AVE

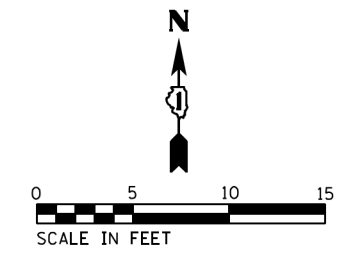
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\DCON\Design\Plan\Plotsheets\062018\sh-r-detail1 Ramp-13.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1in	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

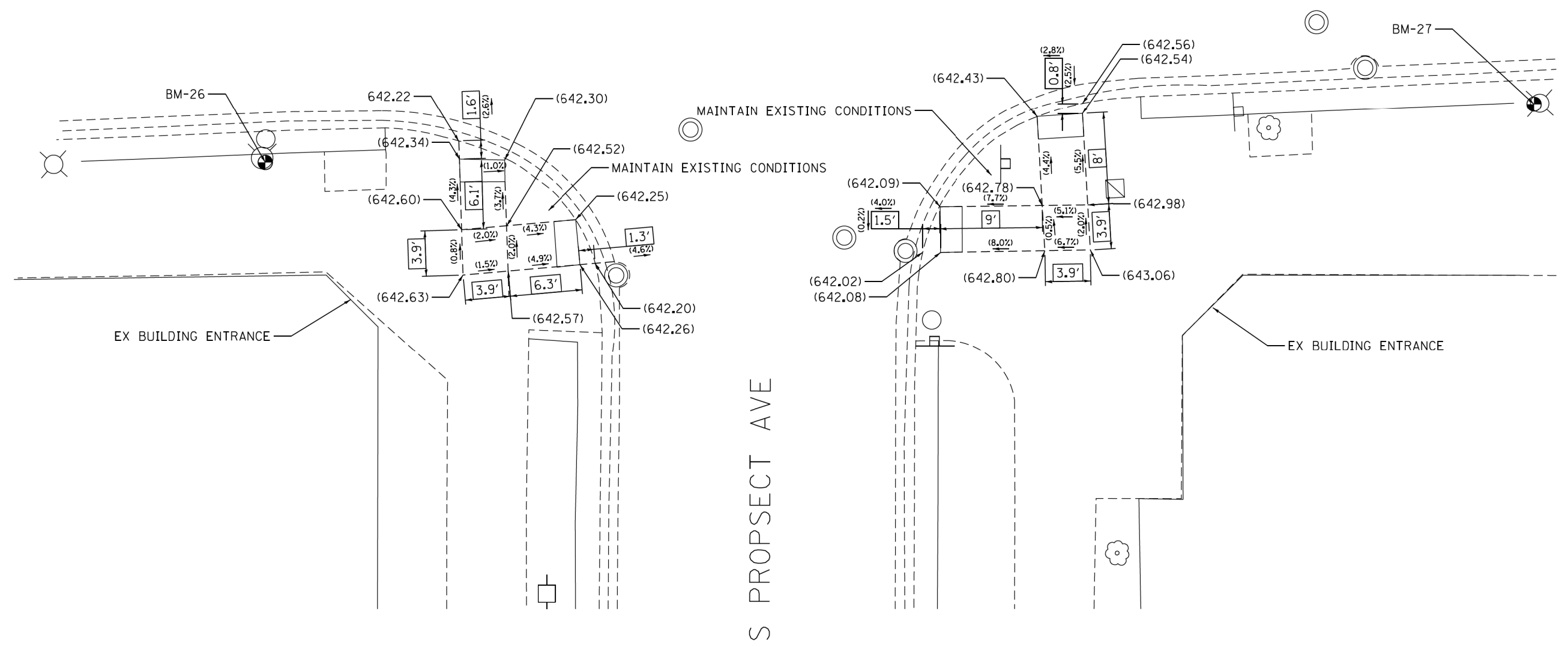
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP DETAILS			
E DEVON AVE AT S PROSPECT AVE			
SCALE: 1" = 5'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	39
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 26 EL 643.593
BENCHMARK : "X" CUT ON SE FH BOLT
LOCATION : ON SW COR OF E DEVON AVE AND S PROSPECT AVE

LEGEND

- XX.X' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- DEPRESSED CURB & GUTTER

REFERENCE BENCHMARK 27 EL 643.234
BENCHMARK : "X" CUT ON W FACE LP BASE
LOCATION : ON SE COR OF E DEVON AVE AND S PROSPECT AVE

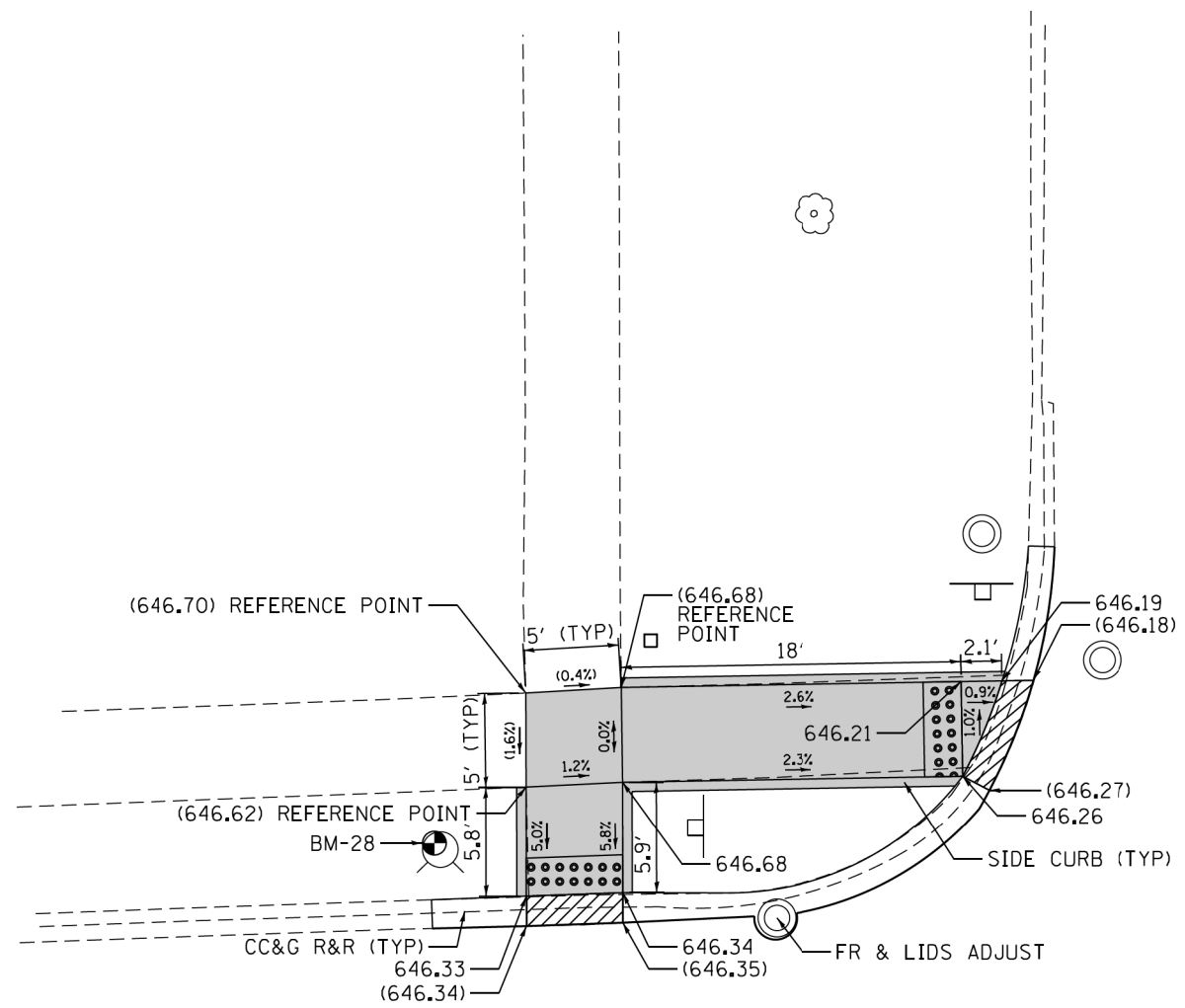
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\062018\sh-ramp-details\sh-ramp-14.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

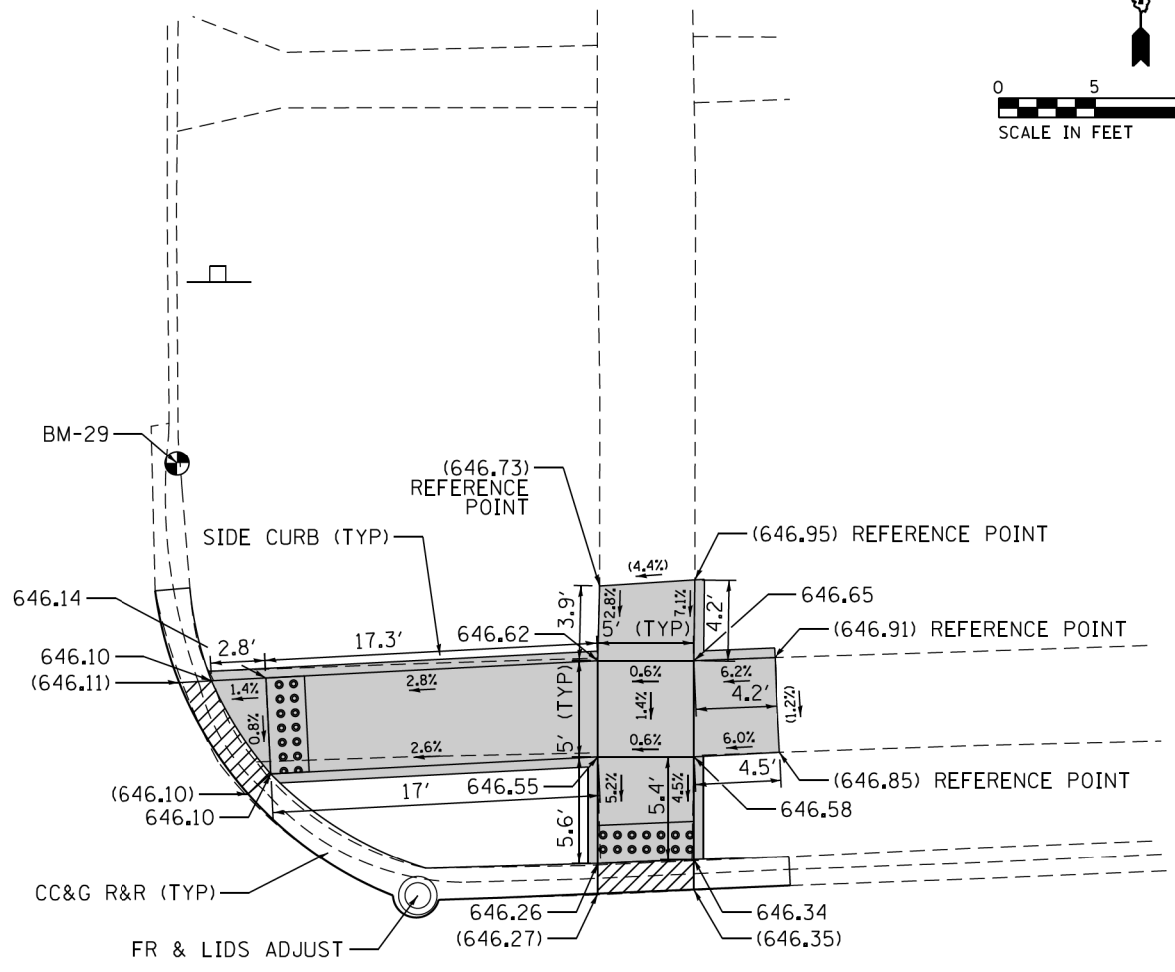
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
E DEVON AVE AT S PROSPECT AVE
SCALE: 1" = 5'
SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	40
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



S CRESCENT AVE



E DEVON AVE

NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 28 EL 648.510
BENCHMARK : "X" CUT IN NW BOLT OF FH
LOCATION : ON NW COR OF E DEVON AVE AND S CRESCENT AVE

LEGEND

XX.X' EXISTING LENGTH
() EXISTING ELEVATION/SLOPE

	PC CONC SIDEWALK 5 SUB GRAN MAT B 4		DEPRESSED CURB & GUTTER
	DETECTABLE WARNINGS		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 29 EL 646.137
BENCHMARK : "X" CUT IN TOP OF CURB
LOCATION : ON NE COR OF E DEVON AVE AND S CRESCENT AVE

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\062018\sh-r-detail15Ramp-15.dgn

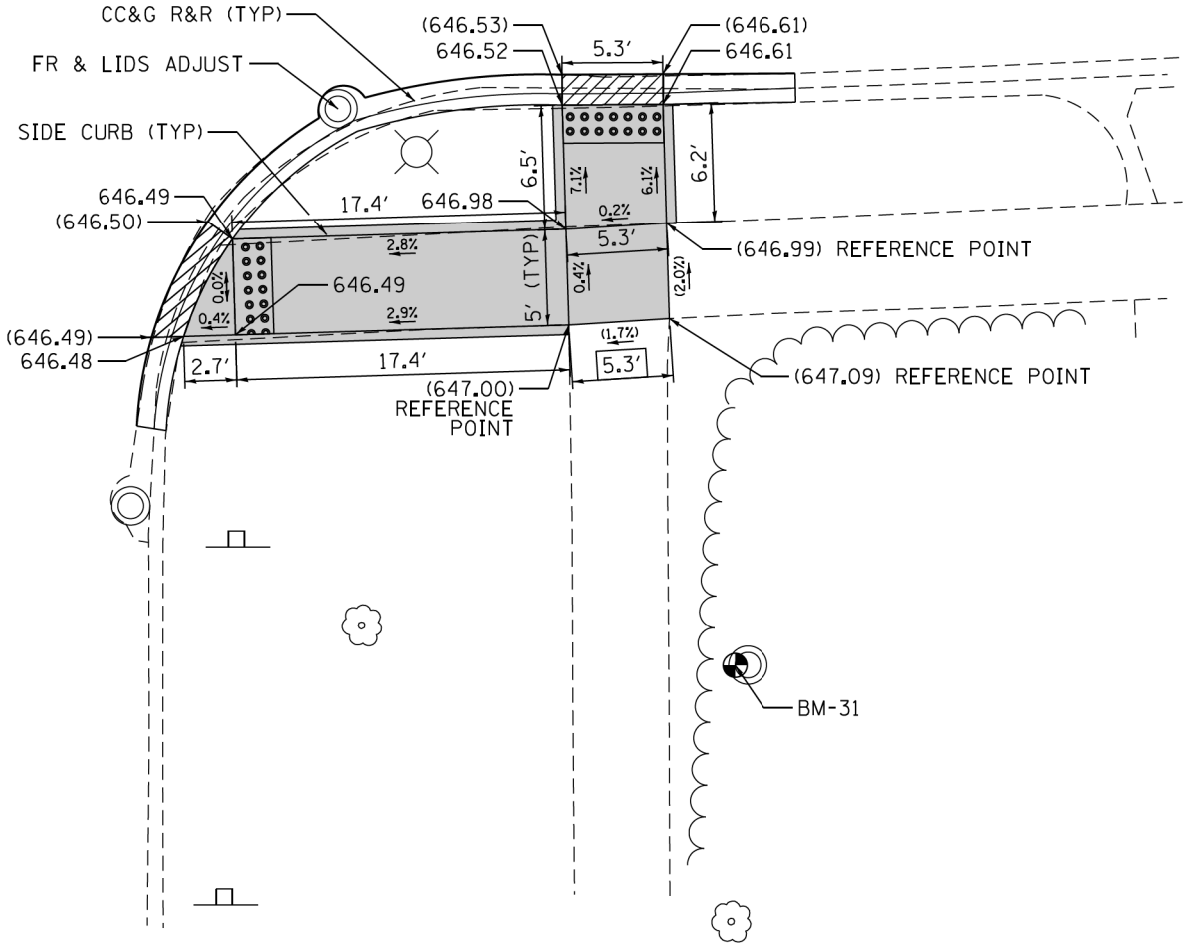
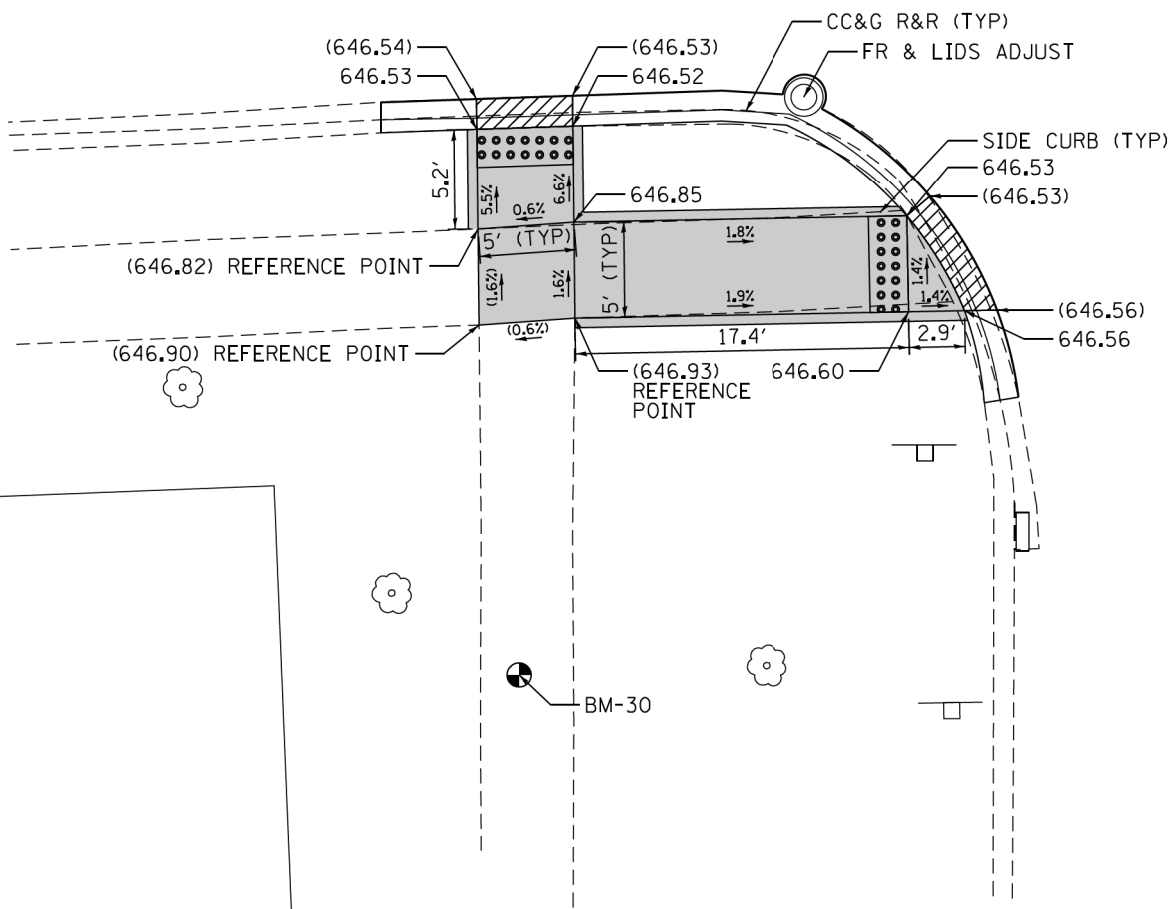
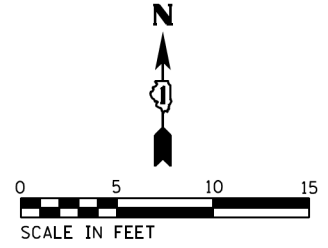
USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
DRAWN - RG / MS / KW	CHECKED - JH	REVISED -
PLLOT SCALE = 10.0000' / 1in	DATE - 1/24/2018	REVISED -
Default		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS E DEVON AVE AT S CRESCENT AVE			
SCALE: 1" = 5'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	41
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

E DEVON AVE



S CRESCENT AVE

NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 30 EL 646.938
BENCHMARK : "X" CUT IN WALK
LOCATION : ON SW COR OF E DEVON AVE AND S CRESCENT AVE

LEGEND

XX.X' EXISTING LENGTH
() EXISTING ELEVATION/SLOPE

PC CONC SIDEWALK 5 SUB GRAN MAT B 4
DETECTABLE WARNINGS
SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

DEPRESSED CURB & GUTTER

REFERENCE BENCHMARK 31 EL 648.414
BENCHMARK : "X" CUT IN W RIM OF MH
LOCATION : ON SE COR OF E DEVON AVE AND S CRESCENT AVE

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CDGN\Design\Plan\Plotsheets\062018\sh-ramp-details\sh-ramp-16.dgn

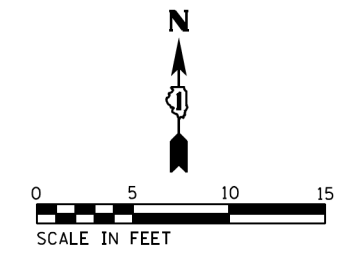
USER NAME = RGal1	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10,0000' / 1in	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: 1" = 5'		SHEET	OF	SHEETS	STA.	TO	STA.
----------------	--	-------	----	--------	------	----	------

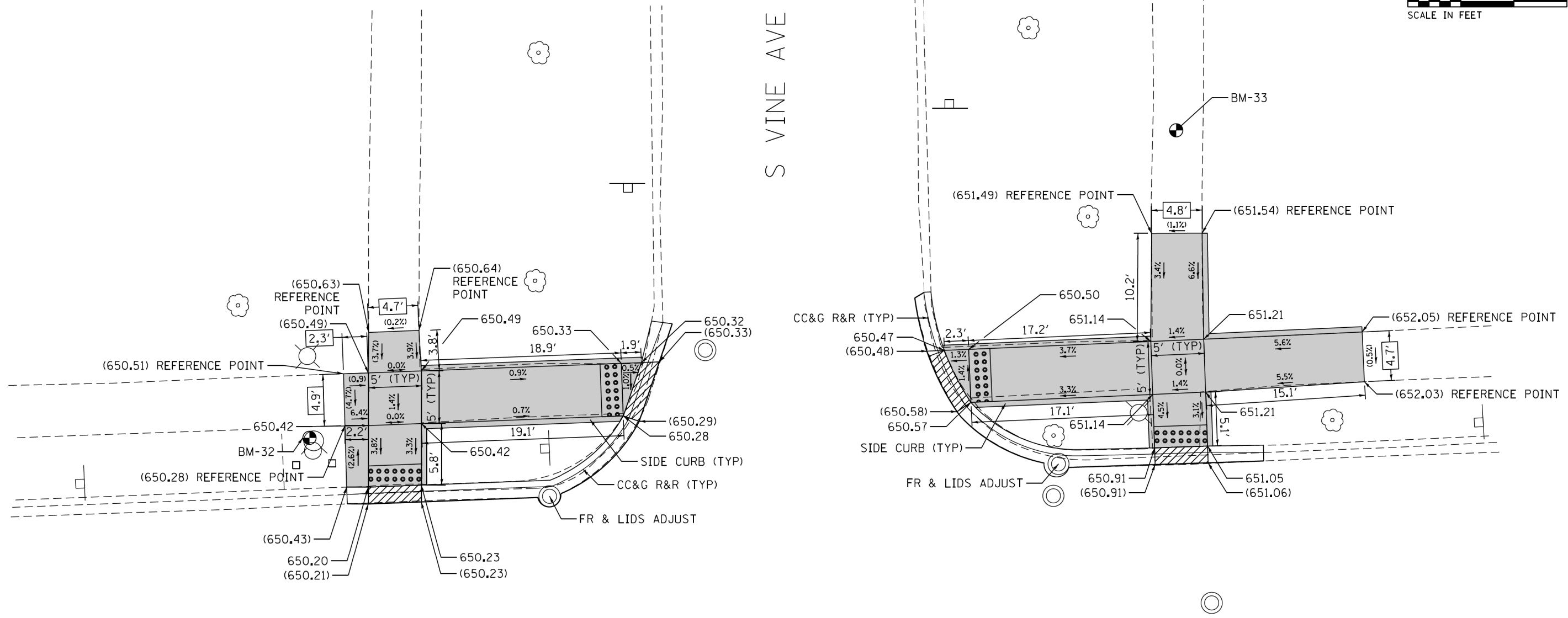
ADA RAMP DETAILS
E DEVON AVE AT S CRESCENT AVE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	42
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



S VINE AVE

E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 32 EL 651.567
BENCHMARK : "X" CUT IN NW FH BOLT
LOCATION : ON NW COR OF E DEVON AVE AND S VINE AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 33 EL 651.280
BENCHMARK : "X" CUT IN WALK
LOCATION : ON NE COR OF E DEVON AVE AND S VINE AVE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

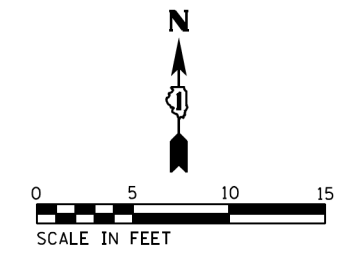
ADA RAMP DETAILS
E DEVON AVE AT S VINE AVE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	43
CONTRACT NO. 62D18				

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

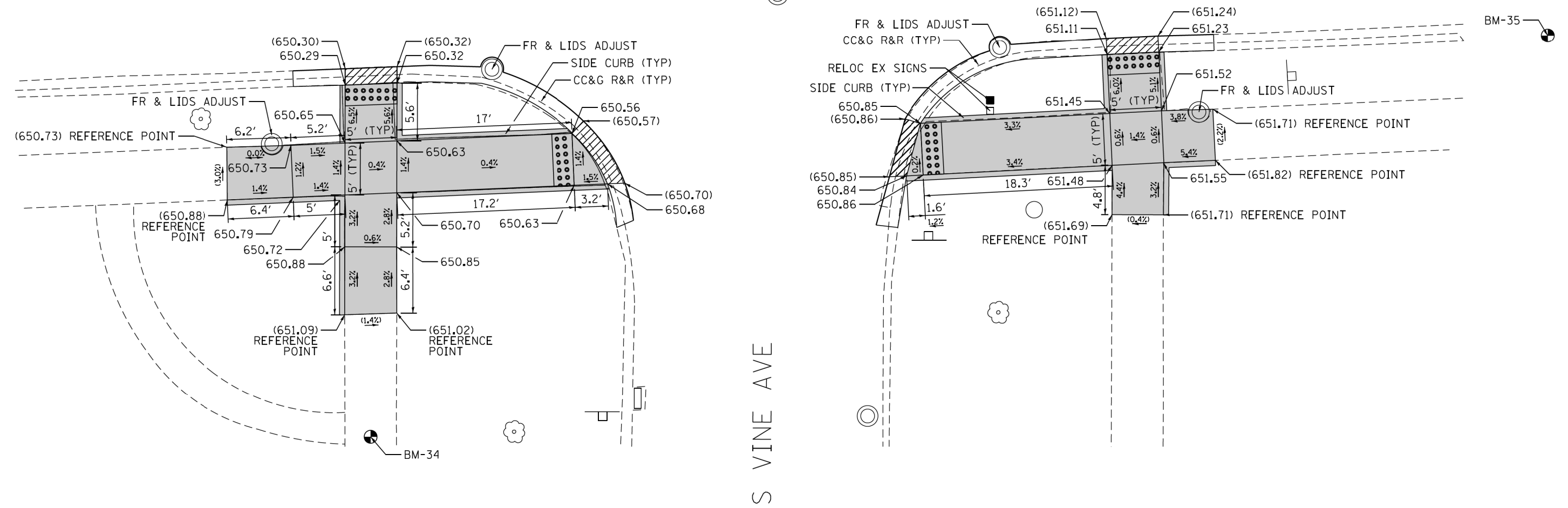
ILLINOIS FED. AID PROJECT

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\DCON\Design\Plan\Plotsheets\062018\sh-r-detail1Ramp-17.dgn



E DEVON AVE

S VINE AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 35 EL 651.107
BENCHMARK : "X" CUT IN WALK
LOCATION : ON SW COR OF E DEVON AVE AND S VINE AVE

LEGEND

XX.X' EXISTING LENGTH
() EXISTING ELEVATION/SLOPE

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 35 EL 652.028
BENCHMARK : "X" CUT IN TOP OF CURB
LOCATION : ON SE COR OF E DEVON AVE AND S VINE AVE

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave.Central Ave\DCON\Design\Plan\Plotsheets\062018\sh-r-detail18.rmp-18.dgn

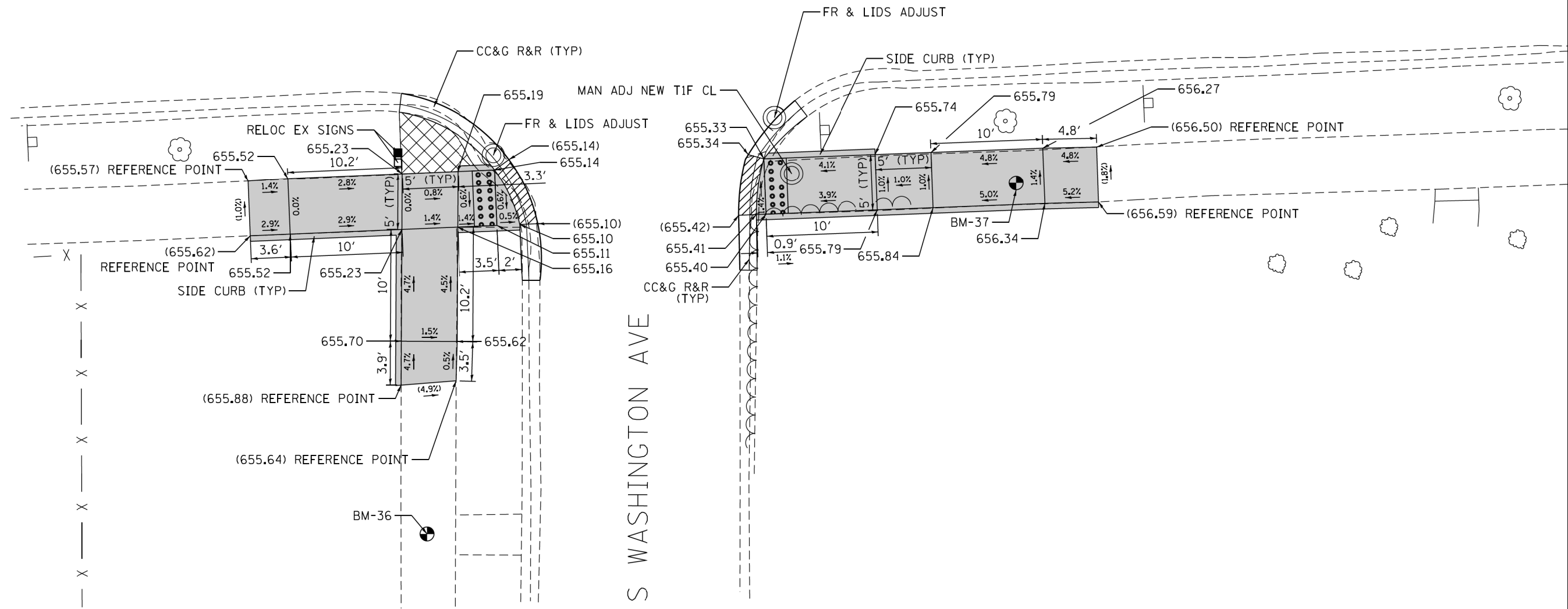
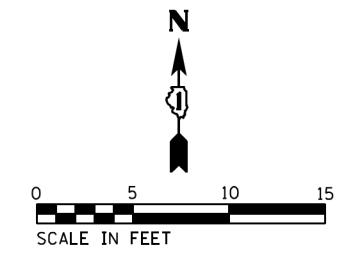
USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1in.	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS	
E DEVON AVE AT S VINE AVE	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	44
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 36 EL 655.942
BENCHMARK : "X" CUT IN WALK
LOCATION : ON SW COR OF E DEVON AVE AND SOUTHBOUND
S WASHINGTON AVE

LEGEND

- PC CONC SIDEWALK 5
SUB GRAN MAT B 4
- DEPRESSED CURB
& GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING
ELEVATION/SLOPE
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 37 EL 656.450
BENCHMARK : "X" CUT IN WALK
LOCATION : ON SE COR OF E DEVON AVE AND SOUTHBOUND
S WASHINGTON AVE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
E DEVON AVE AT S WASHINGTON AVE

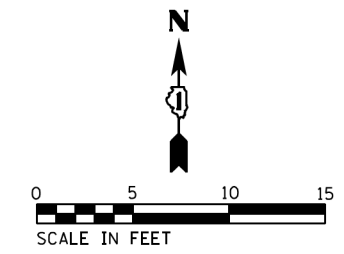
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	45
CONTRACT NO. 62D18				

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

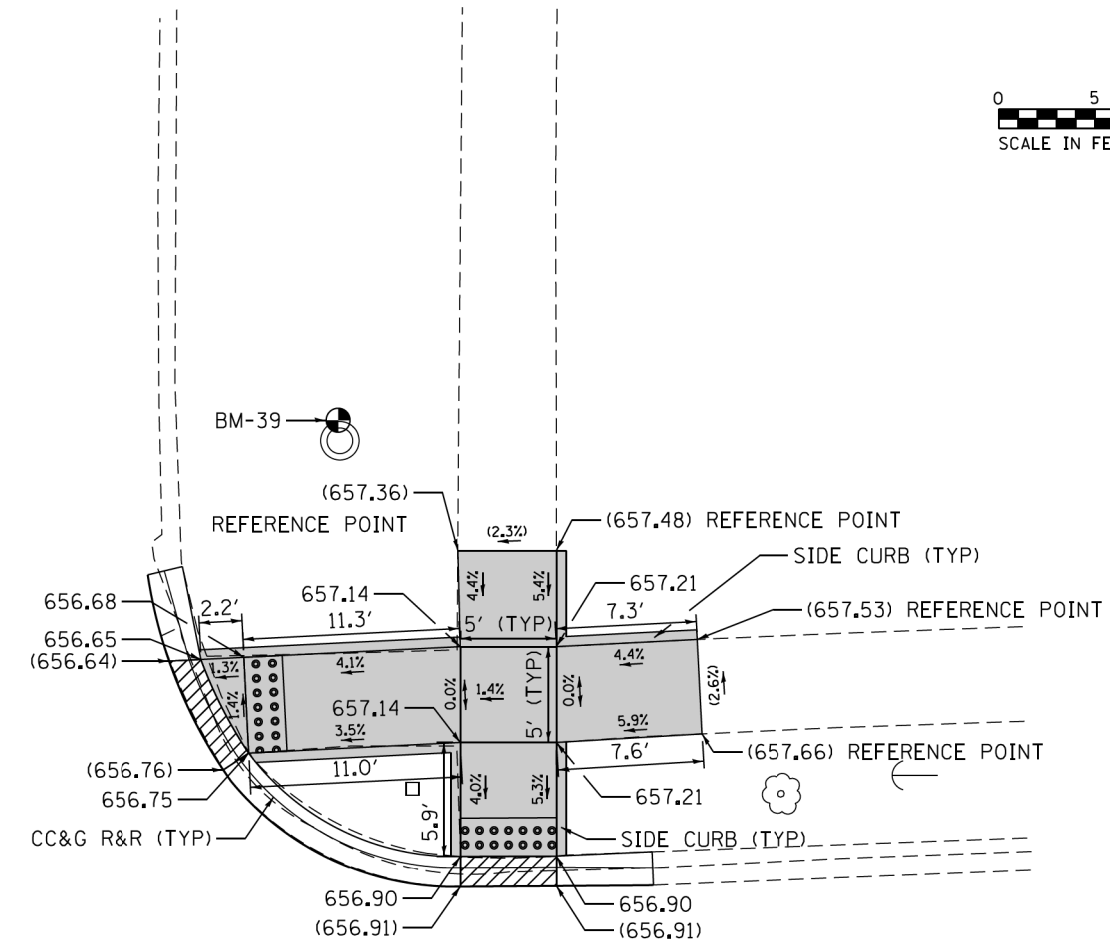
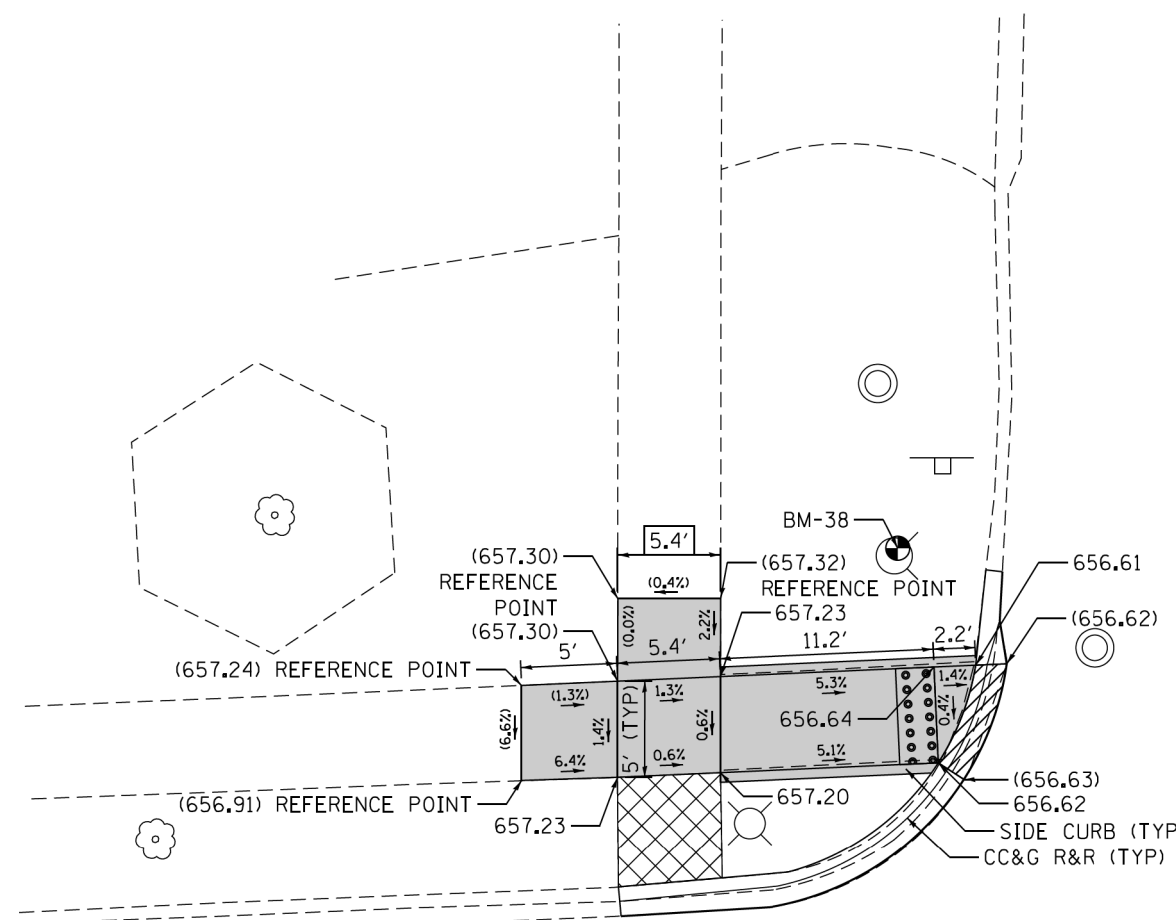
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\DCON\Design\Plan\Plotsheets\062018\sh-r-detail1aRamp-19.dgn

Default



S WASHINGTON AVE

E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 38 EL 659.159
 BENCHMARK : "X" CUT IN ARROW BOLT FH
 LOCATION : ON THE NW COR OF E DEVON AVE AND S WASHINGTON AVE

LEGEND

- XX.X' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 39 EL 657.300
 BENCHMARK : "X" CUT ON N RIM OF MH
 LOCATION : ON NE COR OF E DEVON AVE AND S WASHINGTON AVE

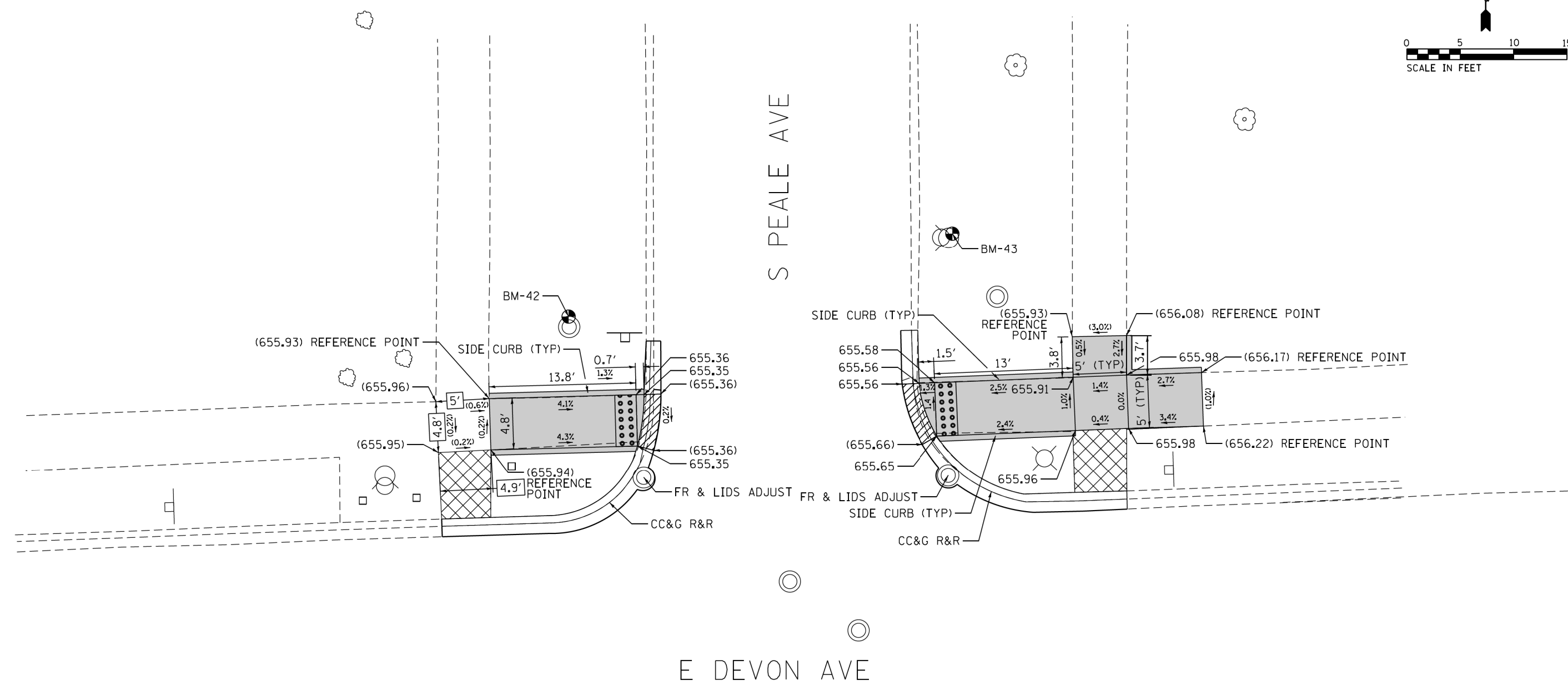
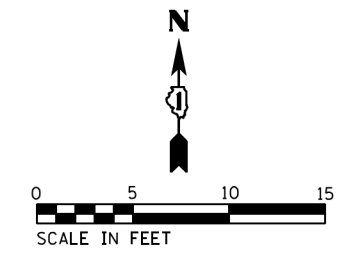
USER NAME = RGall	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS	
E DEVON AVE AT S WASHINGTON AVE	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	46
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

FILE NAME = Z:\17002\05 IDOT D1 W05 ADA Ramps Devon Ave.Central Ave\DCON\Design\Plan\Plotsheets\162018\sh-r-detail1sRamp-20.dgn



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 42 EL 655.593
BENCHMARK : "X" CUT IN N FACE MH RIM
LOCATION : ON THE NW COR OF E DEVON AVE AND S PEALE AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 43 ELEV 657.497
BENCHMARK : "X" CUT ON NE FH BOLT
LOCATION : ON NE COR OF E DEVON AVE AND S PEALE AVE

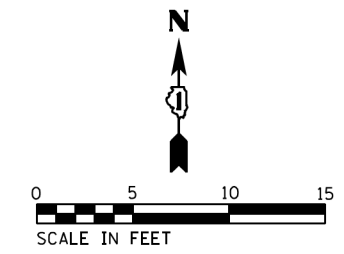
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\062018\sh-r-detail1sRamp-22.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1in	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

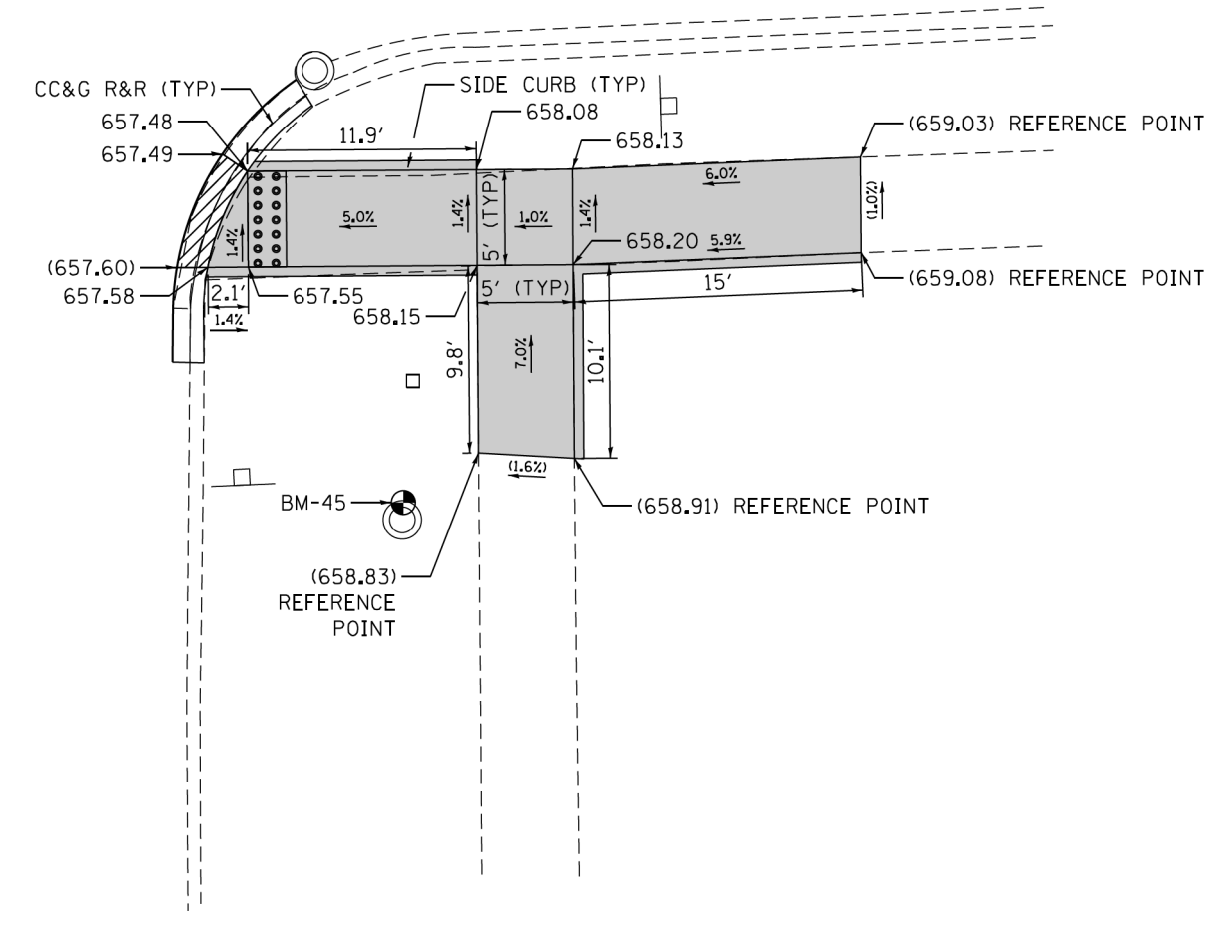
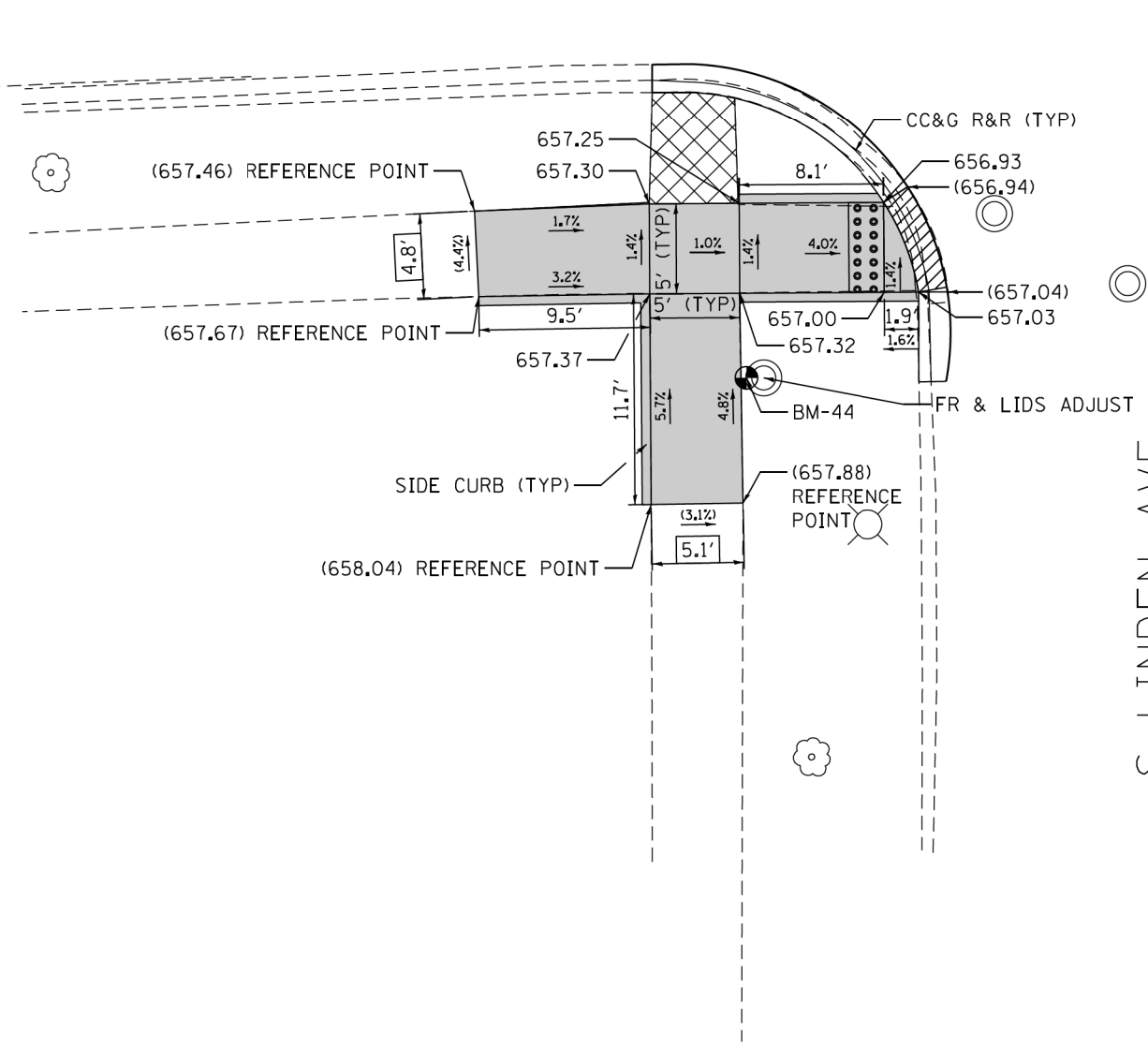
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP DETAILS E DEVON AVE AT S PEALE AVE			
SCALE: 1" = 5'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	48
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 44 EL 658.058
BENCHMARK : "X" CUT ON W FACE WV VLT
LOCATION : ON THE SW COR OF E DEVON AVE AND S LINDEN AVE

LEGEND

- XX.X' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- DEPRESSED CURB & GUTTER

REFERENCE BENCHMARK 45 EL 658.682
BENCHMARK : "X" CUT ON N FACE CB
LOCATION : ON SE COR OF E DEVON AVE AND S LINDEN AVE

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

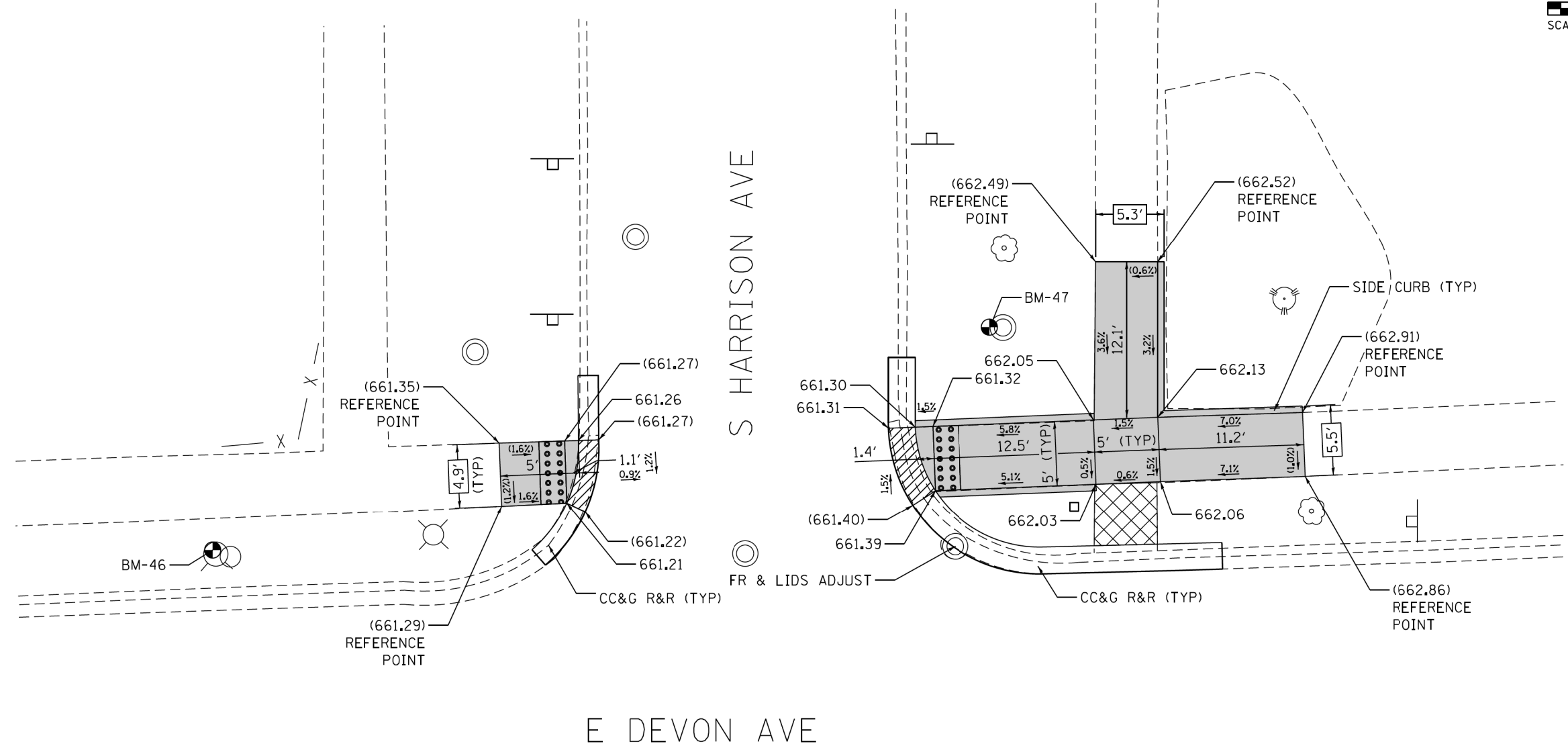
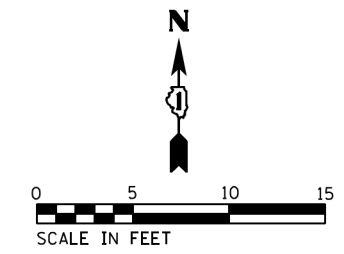
**ADA RAMP DETAILS
E DEVON AVE AT S LINDEN AVE**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	49
CONTRACT NO. 62D18				

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\DCON\Design\Plan\Plotsheets\062018\sh-ramp-detail1sRamp-23.dgn



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 46 EL 663.181
BENCHMARK : "X" CUT IN NW BOLT FH
LOCATION : ON NW COR OF E DEVON AVE AND S HARRISON AVE

LEGEND

- XX.X' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 47 EL 662.050
BENCHMARK : "X" CUT IN W RIM CB
LOCATION : ON NE COR OF E DEVON AVE AND S HARRISON AVE

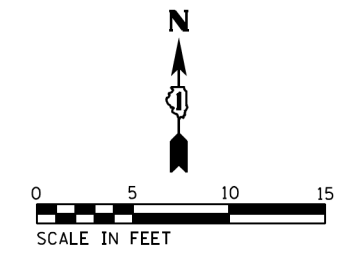
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
E DEVON AVE AT S HARRISON AVE

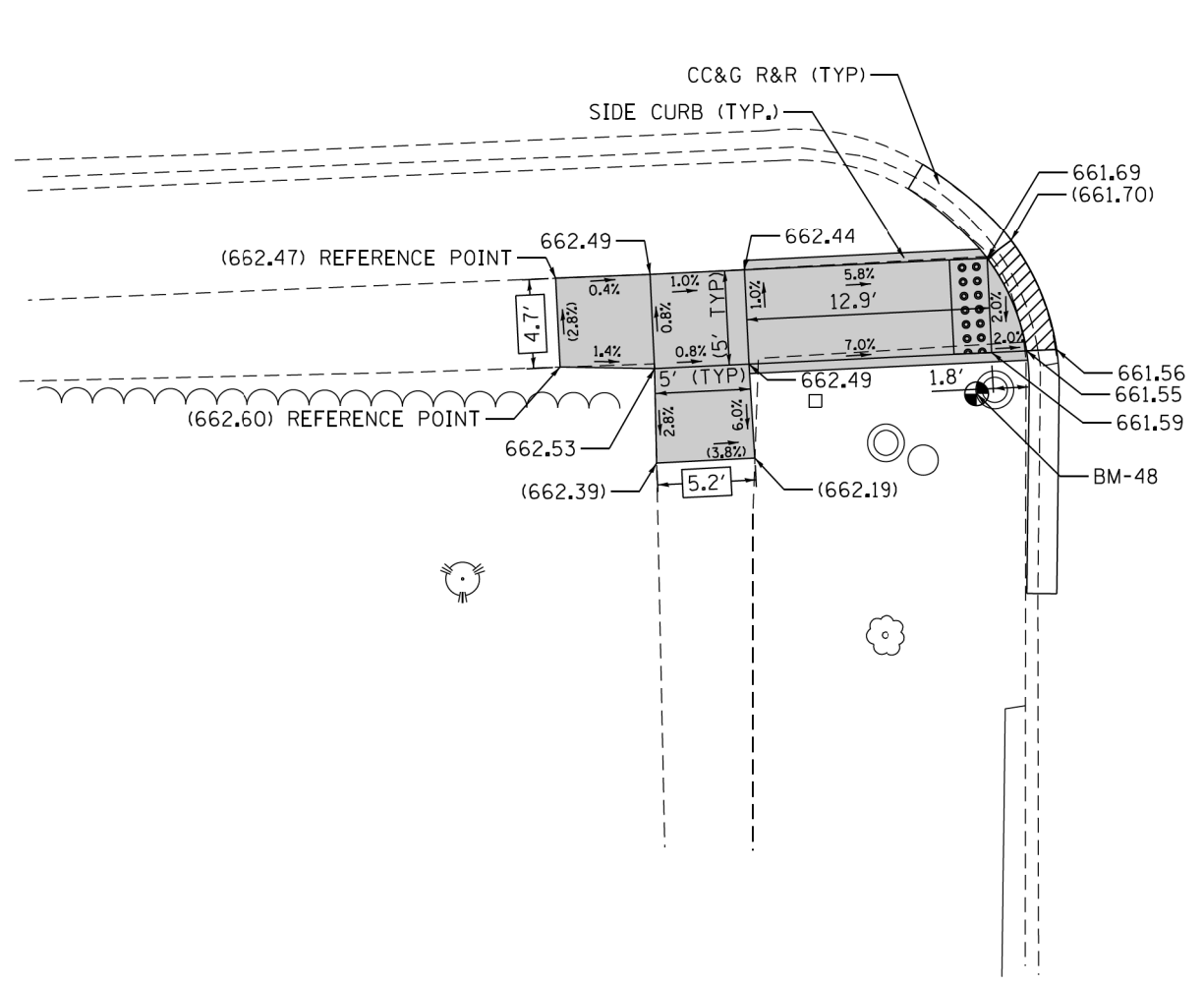
SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	50
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

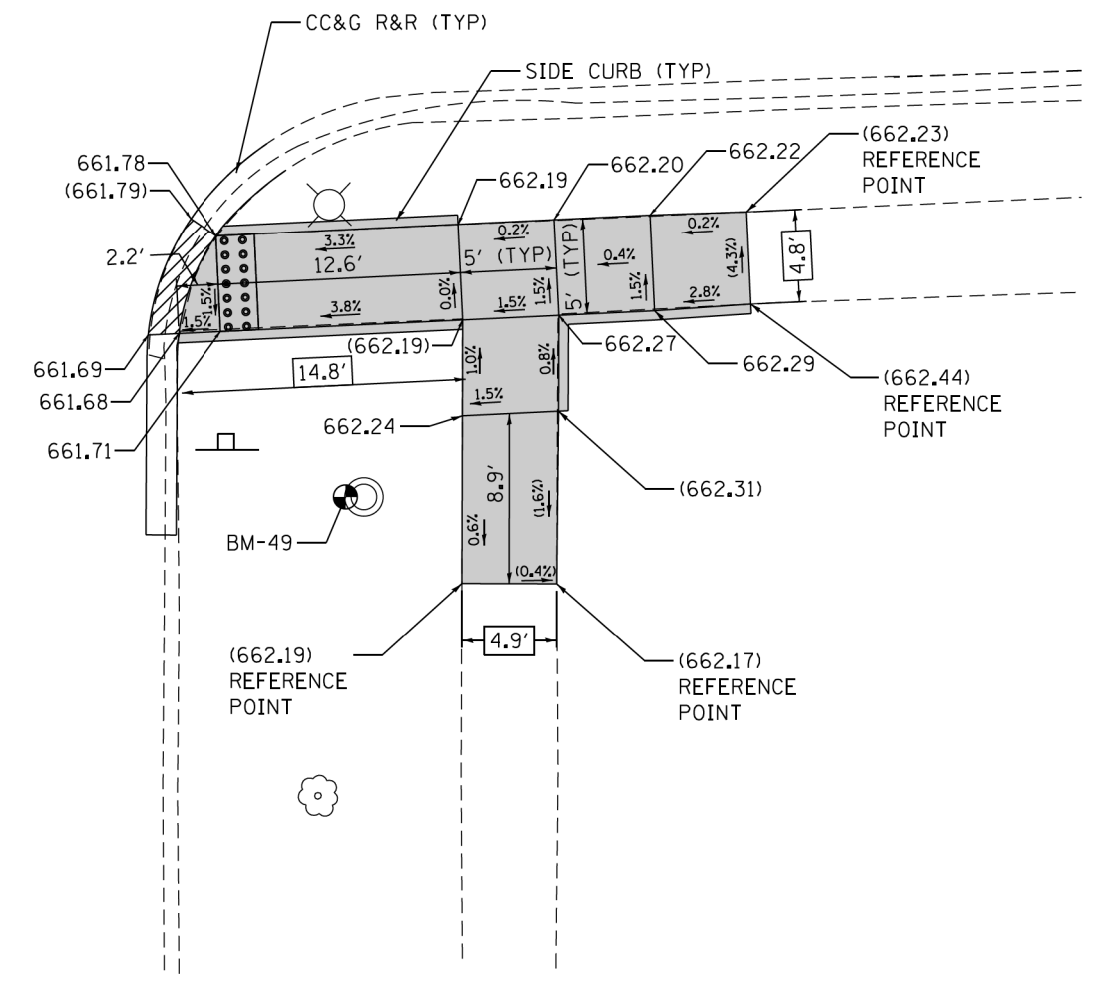
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave.Central Ave\Design\Plan\Plotsheets\062018\sh-ramp-24.dgn



E DEVON AVE



S GROVE AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 48 EL 661.934
BENCHMARK : "X" CUT IN W RIM OF VV
LOCATION : ON THE SW COR OF E DEVON AVE AND S GROVE AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 49 EL 661.847
BENCHMARK : "X" CUT IN W RIM CB
LOCATION : ON SE COR OF E DEVON AVE AND S GROVE AVE

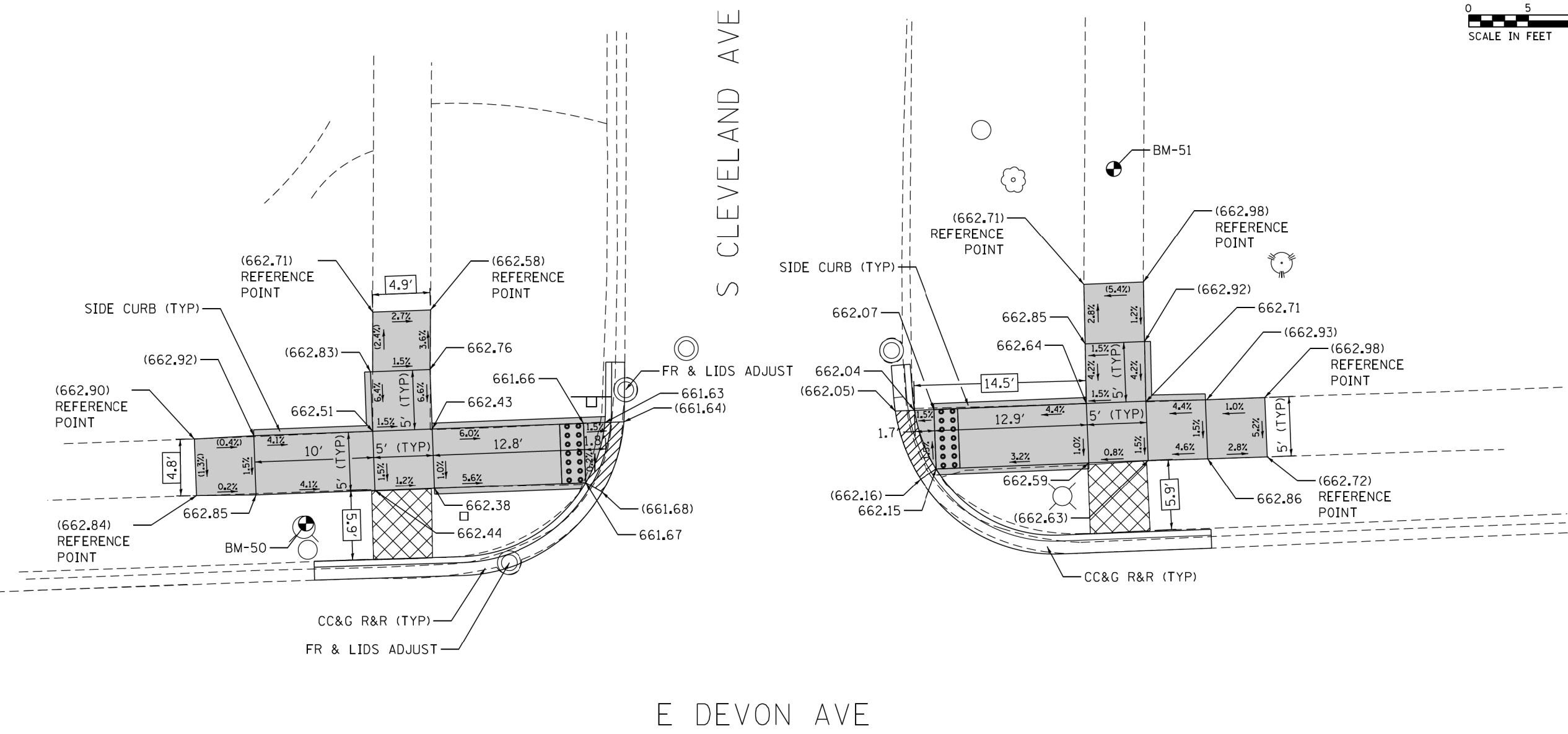
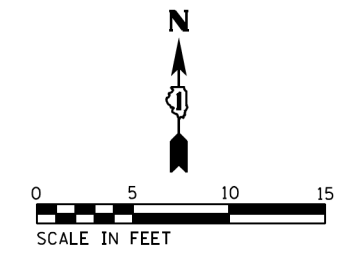
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\162018\sh-ramp-details\162018\sh-ramp-25.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10,0000 ' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP DETAILS	
E DEVON AVE AT S GROVE AVE	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	51
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 50 EL 663.346
BENCHMARK : "X" CUT IN NE FH BOLT
LOCATION : ON NW COR OF E DEVON AVE AND S CLEVELAND AVE

LEGEND

- PC CONC SIDEWALK 5' SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 51 EL 662.645
BENCHMARK : "X" CUT ϕ WALK
LOCATION : ON NE CORNER OF E DEVON AVE AND S CLEVELAND AVE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

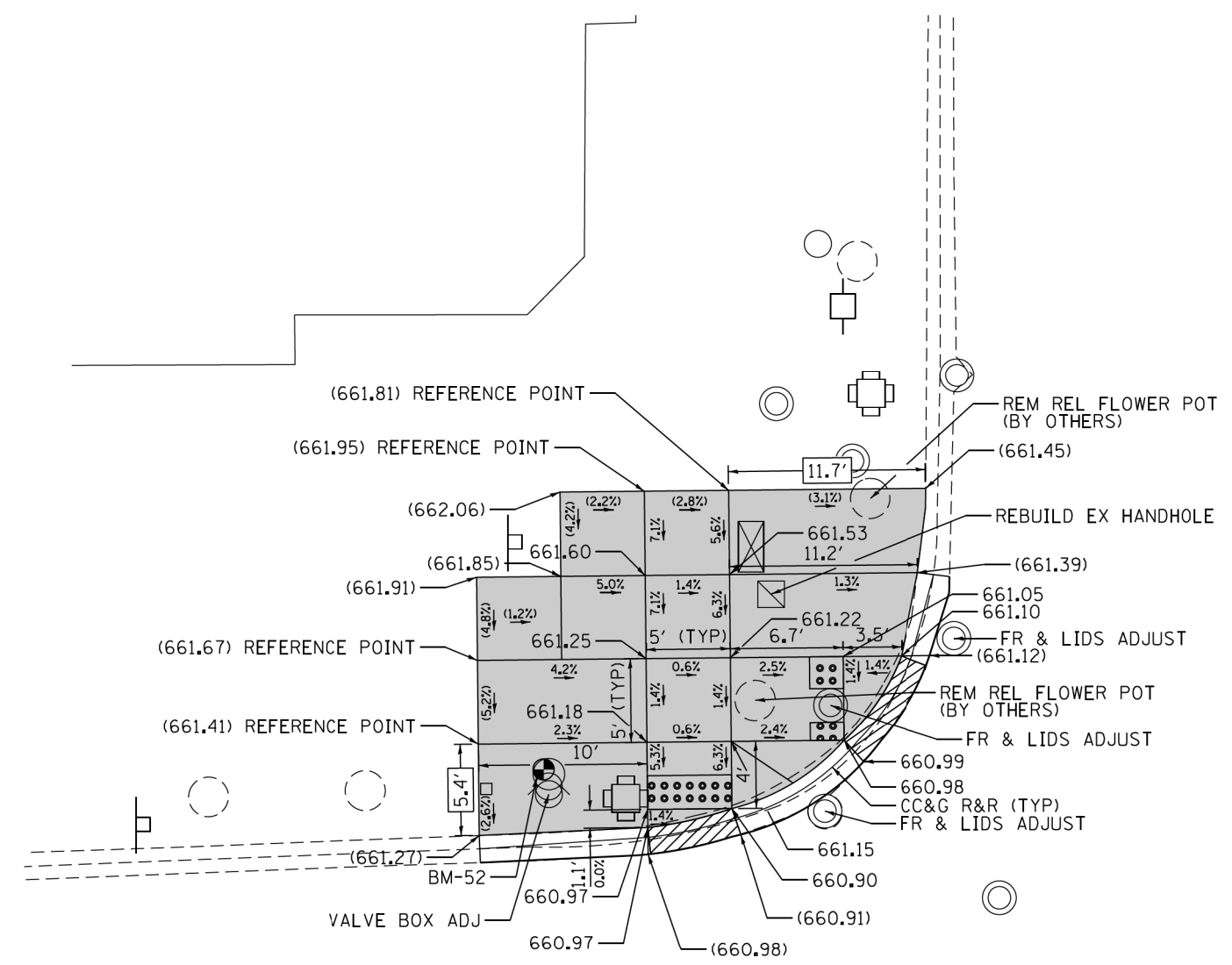
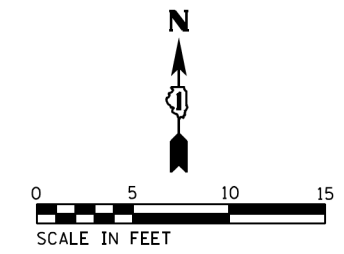
ADA RAMP DETAILS
E DEVON AVE AT S CLEVELAND AVE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	52
CONTRACT NO. 62D18				

SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.

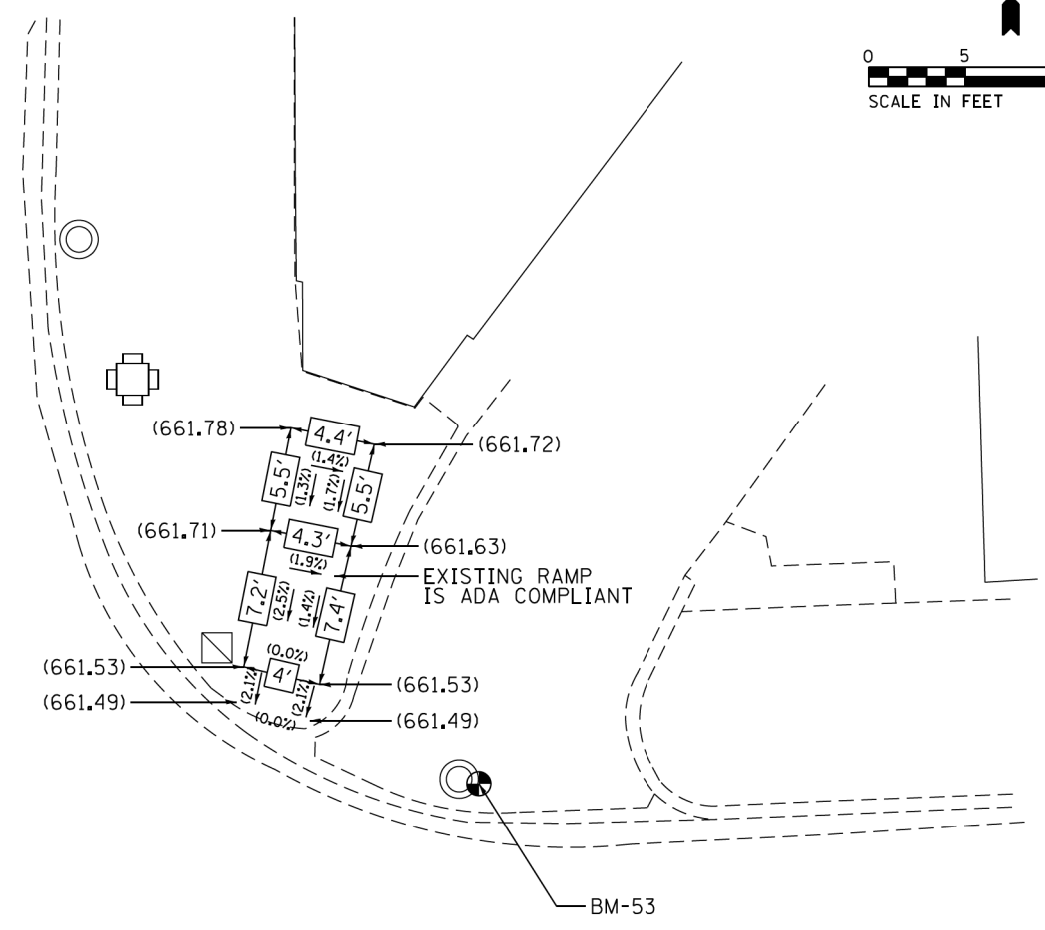
ILLINOIS FED. AID PROJECT

FILE NAME = Z:\17002\05 IDOT D1 W05 ADA Ramps Devon Ave Central Ave\CON\Design\Plan\Plotsheets\062018\sh-r-detail1sRamp-26.dgn



N OZANAM AVE

E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING
PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 52 EL 662.890
BENCHMARK : "X" CUT IN NW FH BOLT
LOCATION : ON THE NW COR OF E DEVON AVE AND N OZANAM AVE

LEGEND

- PC CONC SIDEWALK 5 SUB GRAN MAT B 4
- DEPRESSED CURB & GUTTER
- EXISTING LENGTH
- DETECTABLE WARNINGS
- EXISTING ELEVATION/SLOPE
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK 53 EL 661.448
BENCHMARK : "X" CUT IN E RIM MH
LOCATION : ON NE COR OF E DEVON AVE AND N OZANAM AVE

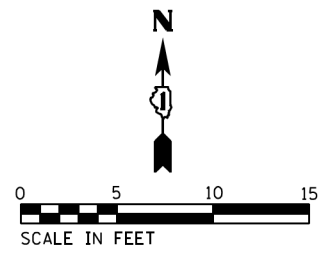
FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\DCON\Design\Plan\Plotsheets\062018\sh-r-detailRamp-27.dgn

USER NAME = RGe11	DESIGNED - RG / MS / KW	REVISED -
	DRAWN - RG / MS / KW	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - JH	REVISED -
PLOT DATE = 1/24/2018	DATE - 1/24/2018	REVISED -

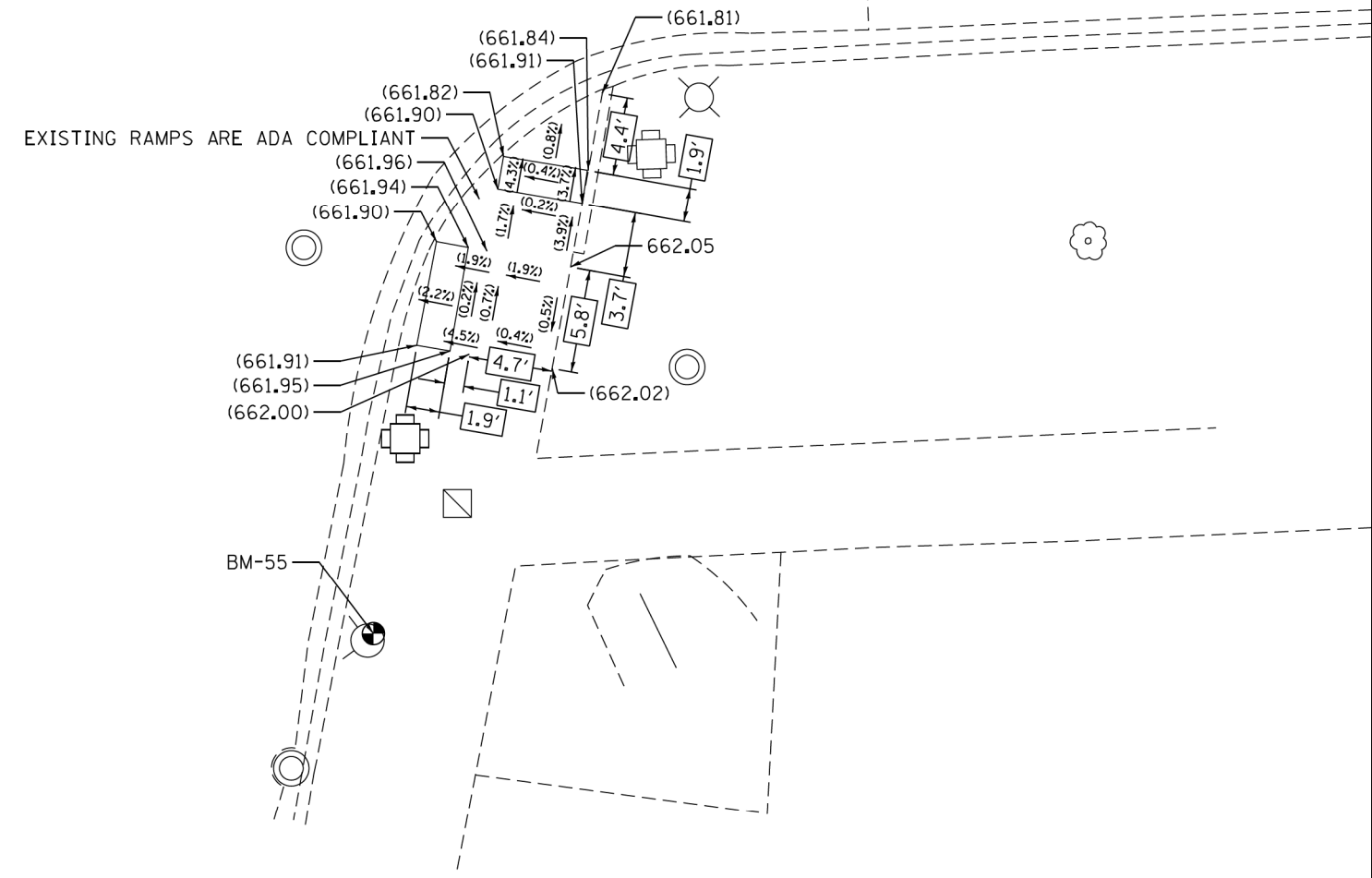
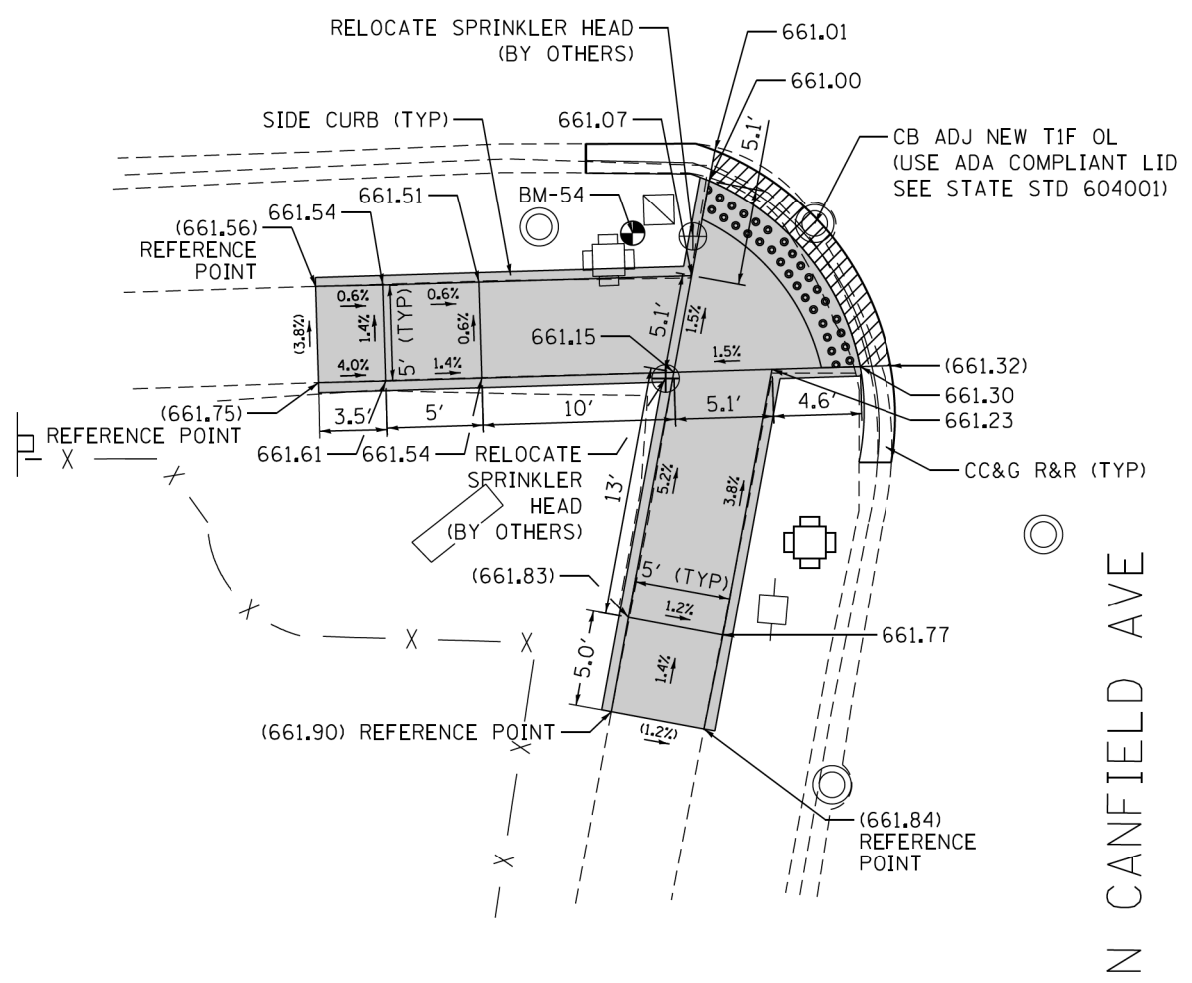
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP DETAILS E DEVON AVE AT N OZANAM AVE			
SCALE: 1" = 5'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	53
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				



E DEVON AVE



NOTE:
REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 54 EL 661.350
 BENCHMARK : "X" CUT IN SW COR TS HH
 LOCATION : ON SW COR OF E DEVON AVE AND N CANFIELD AVE

LEGEND

- | | | | |
|---------------------|---|------------------|-------------------------------------|
| [XX.X'] | EXISTING LENGTH | [Hatched Box] | DEPRESSIBLE CURB & GUTTER |
| () | EXISTING ELEVATION/SLOPE | [Dotted Box] | DETECTABLE WARNINGS |
| [Cross-hatched Box] | SIDWALK REMOVAL REPLACE W/TOPSOIL & SOD | [Solid Grey Box] | PC CONC SIDEWALK 5 SUB GRAN MAT B 4 |

REFERENCE BENCHMARK 55 EL 663.265
 BENCHMARK : "X" CUT CROSS IN N BOLT FH
 LOCATION : ON SE COR OF E DEVON AVE AND N CANFIELD AVE

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
 E DEVON AVE AT N CANFIELD AVE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	3197A&B RS-1	COOK	70	54
CONTRACT NO.				62D18

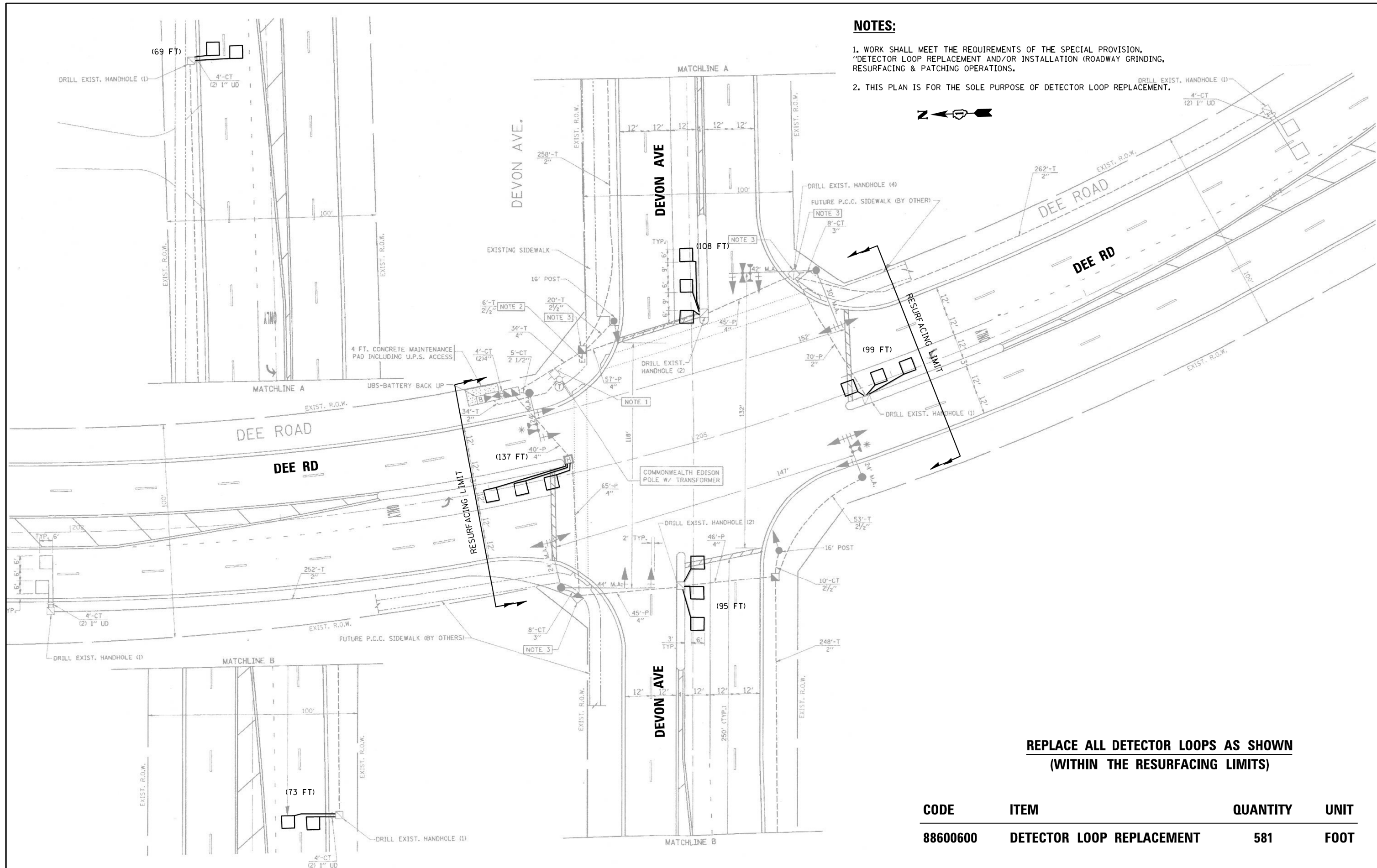
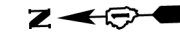
SCALE: 1" = 5'
 SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

FILE NAME = Z:\17002\05 IDOT 01 W05 ADA Ramps Devon Ave Central Ave\CDGN\Design\Plan\Plotsheets\062018\sh-r-detail-ramp-28.dgn

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	581	FOOT

FILE NAME =	USER NAME = SNOWBA	DESIGNED - BAS	REVISED -
TSS040 DEVON AVE AT DEE RD_06262017.dgn		DRAWN - BAS	REVISED -
Default	PLOT SCALE = 40,000 / 1 in.	CHECKED - LP	REVISED -
	PLOT DATE = 6/26/2017	DATE - 06/26/2017	REVISED -

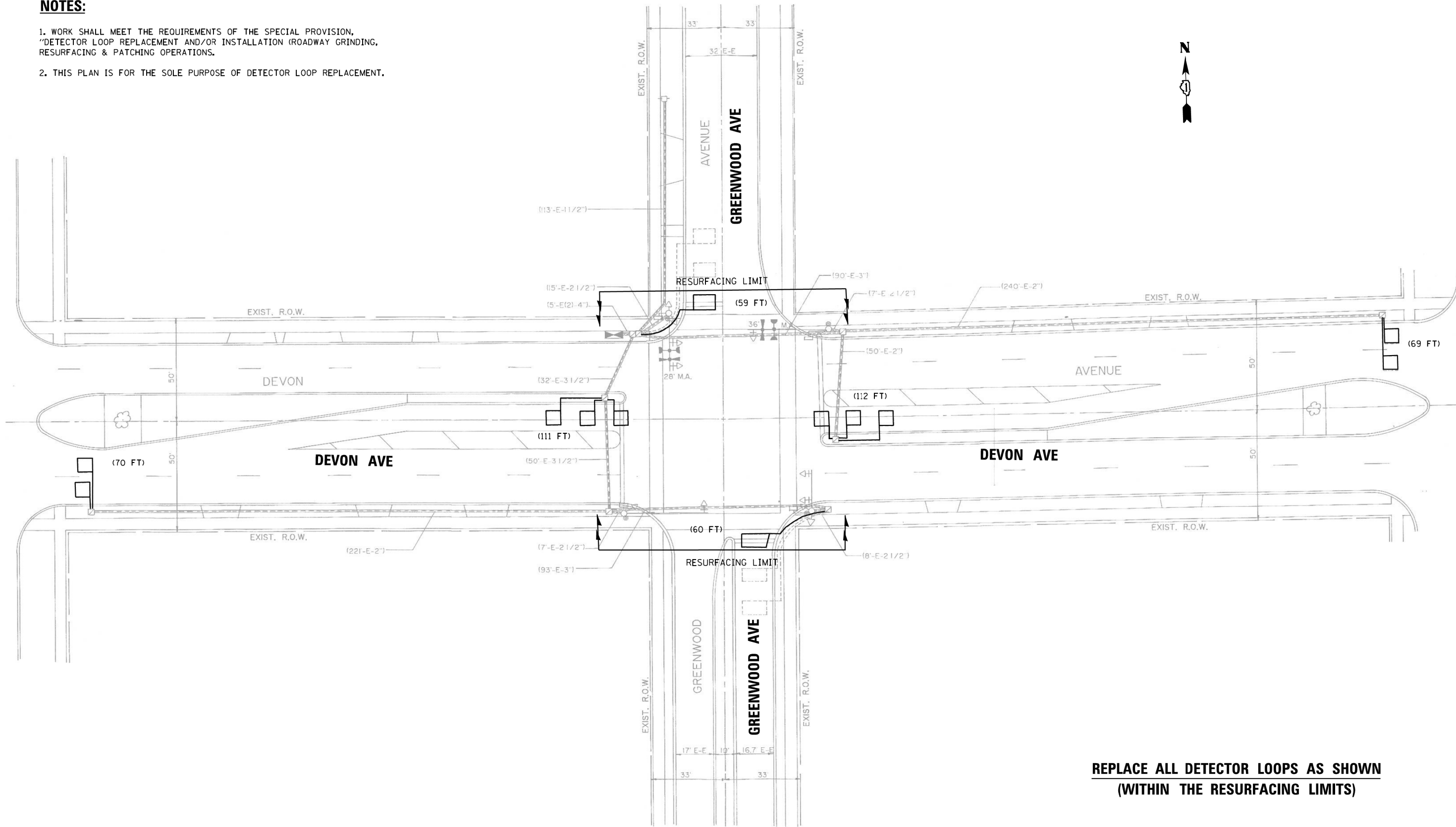
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETECTOR LOOP REPLACEMENT PLAN DEVON AVE AT S. DEE RD				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197 A&B) RS-1	COOK	70	55
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	481	FOOT

FILE NAME =	USER NAME = SNOWBA	DESIGNED - BAS	REVISED -
TS22150 DEVON AVE AT GREENWOOD AVE.06/2017.dgn		DRAWN - BAS	REVISED -
Default	PLOT SCALE = 48.0185' / 1in.	CHECKED - LP	REVISED -
	PLOT DATE = 6/26/2017	DATE - 06/26/2017	REVISED -

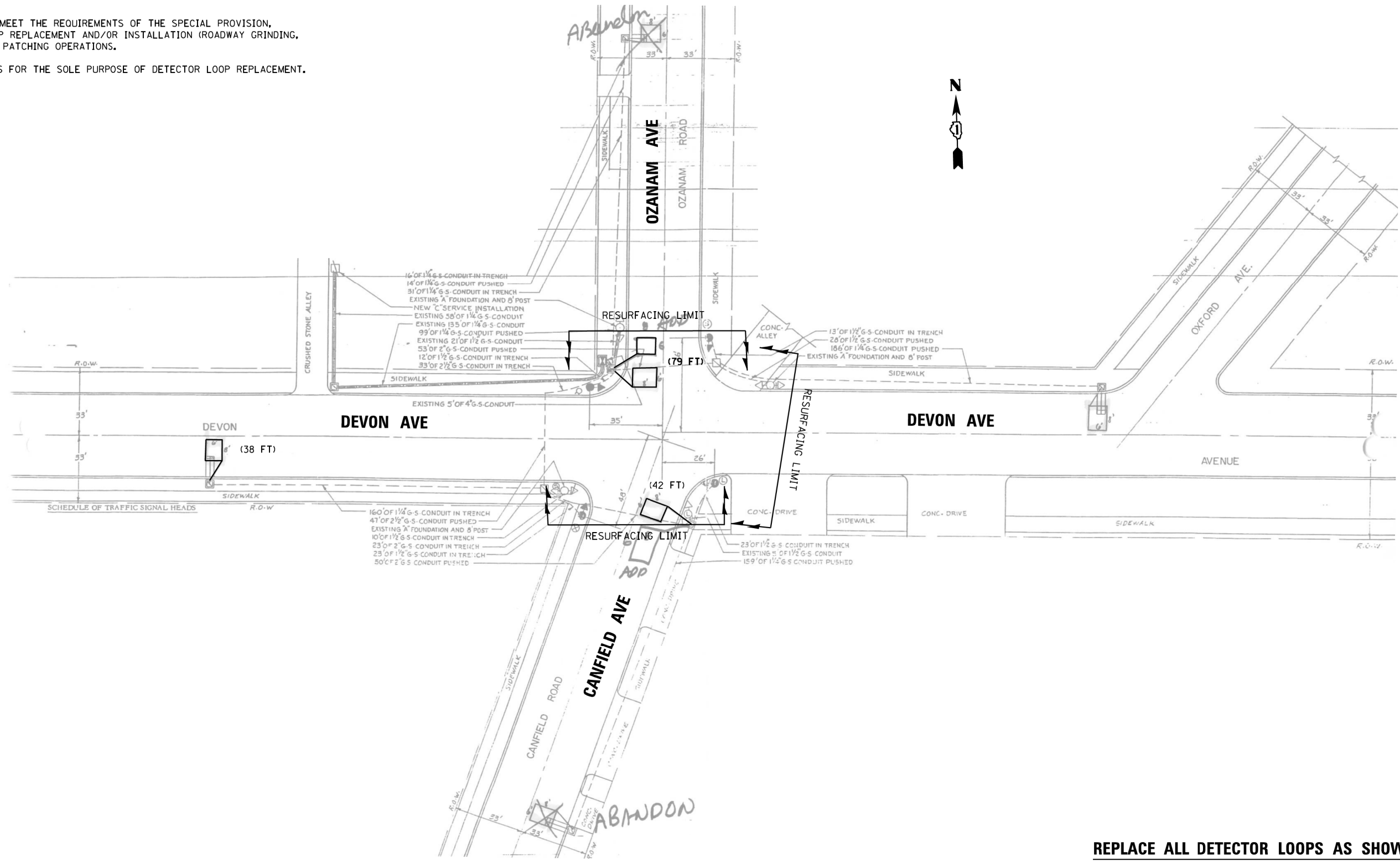
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETECTOR LOOP REPLACEMENT PLAN DEVON AVE AT GREENWOOD AVE			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197 A&B) R5-1	COOK	70	56
CONTRACT NO. 62D18				
ILLINOIS FED. AID PROJECT				

NOTES:

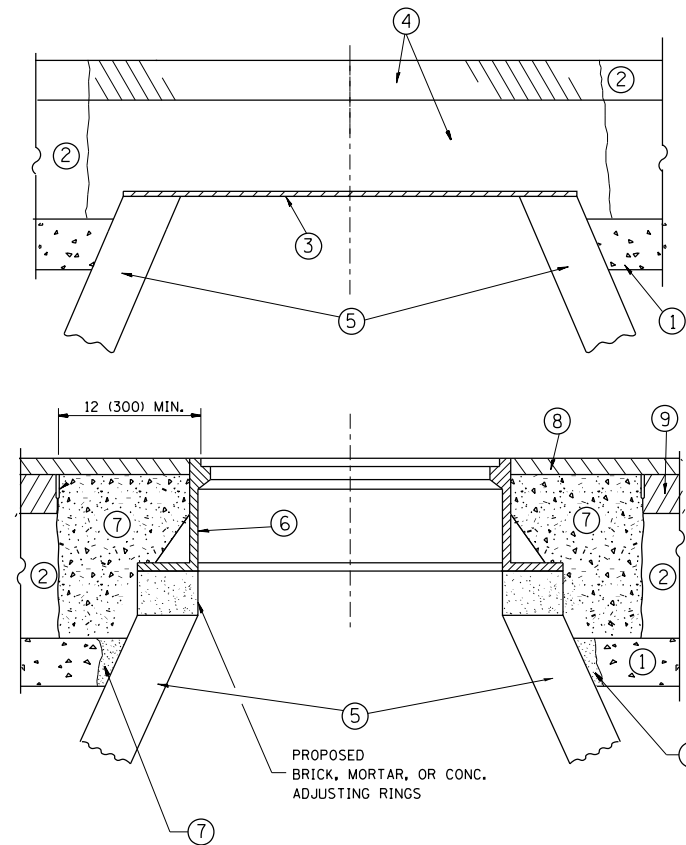
1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	159	FOOT

FILE NAME = TS4255 DEVON AVE AT CANFIELD AVE_06262017.dgn	USER NAME = SNOWBA	DESIGNED - BAS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN DEVON AVE AT CANFIELD AVE	F.A.P. RTE. 1349	SECTION (3197 A&B) RS-1	COUNTY COOK	TOTAL SHEETS 70	SHEET NO. 58		
Default	PLOT SCALE = 48,0140' / 1" =	CHECKED - LP	REVISED -			CONTRACT NO. 62D18						
	PLOT DATE = 6/26/2017	DATE - 06/26/2017	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
ILLINOIS FED. AID PROJECT												



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

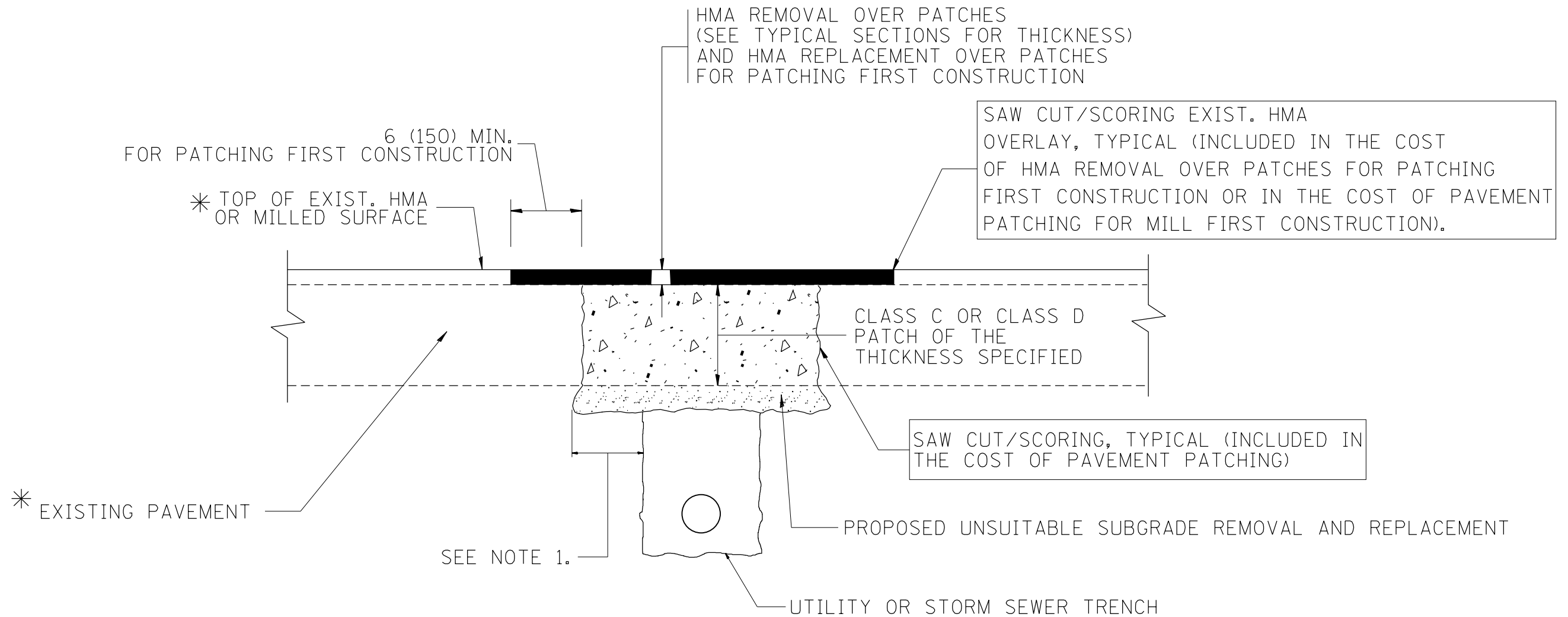
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = paraynoal	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\1436\Drawings\Design\Diststd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 2/1/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	59
BD600-03 (BD-8)		CONTRACT NO. 62D18		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = paraynoal	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\1436\Drawings\Design\Diststd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					1349	(3197A&B) RS-1	COOK	70	60
	PLOT DATE = 2/1/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62D18				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

18" (450) MAX.

T/2 *

3" (75) MIN.

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

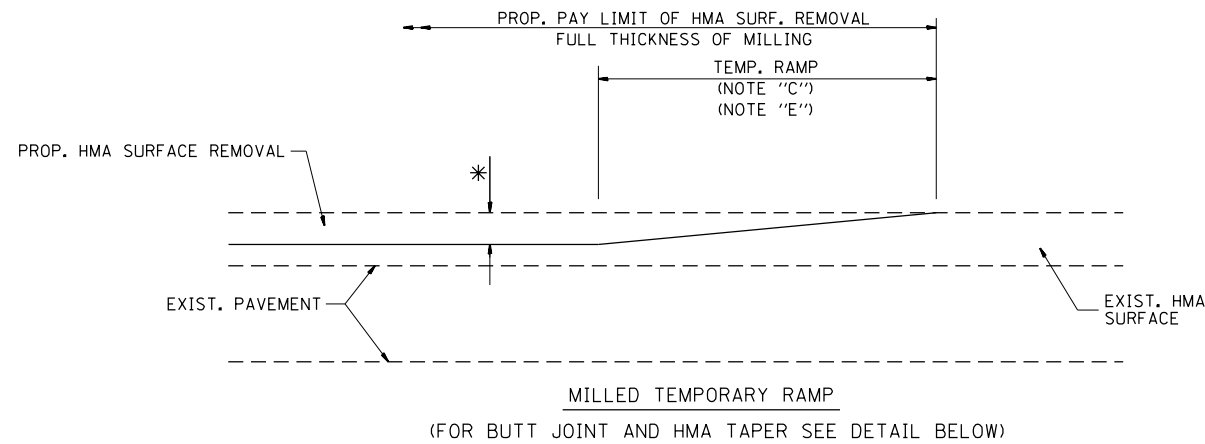
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

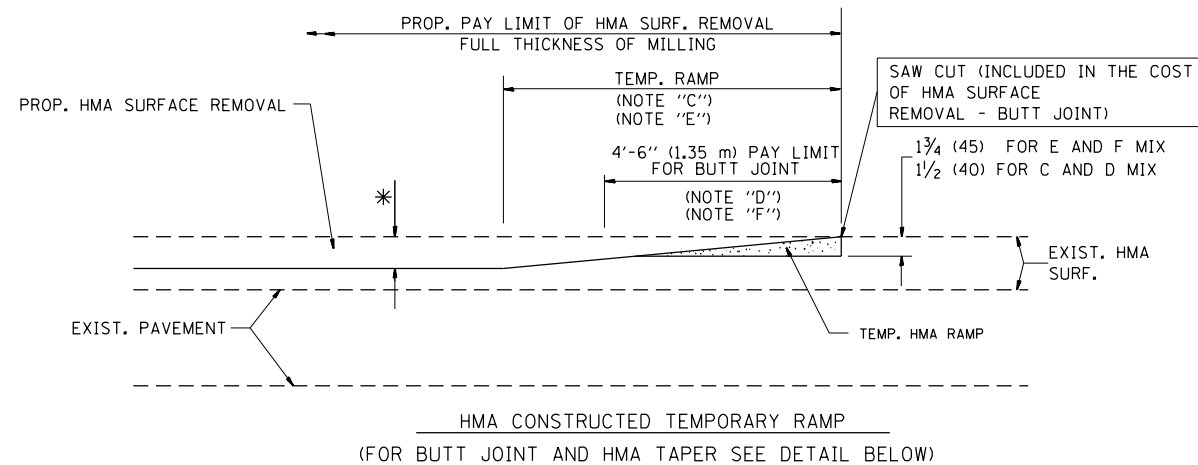
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = paraynoal	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01436\BROWN\to\Design\Diststd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - A. ABBAS 03-21-97					1349	(3197A&B) RS-1	COOK	70	61
PLOT DATE = 2/1/2018	DATE - 03-11-94	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 12-15-09		BD600-06 (BD-24)			CONTRACT NO. 62D18				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

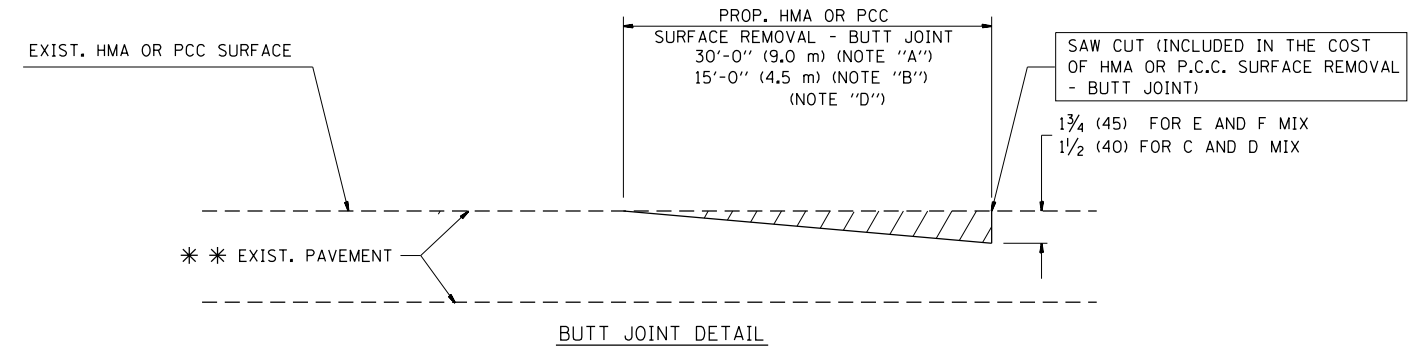


OPTION 1

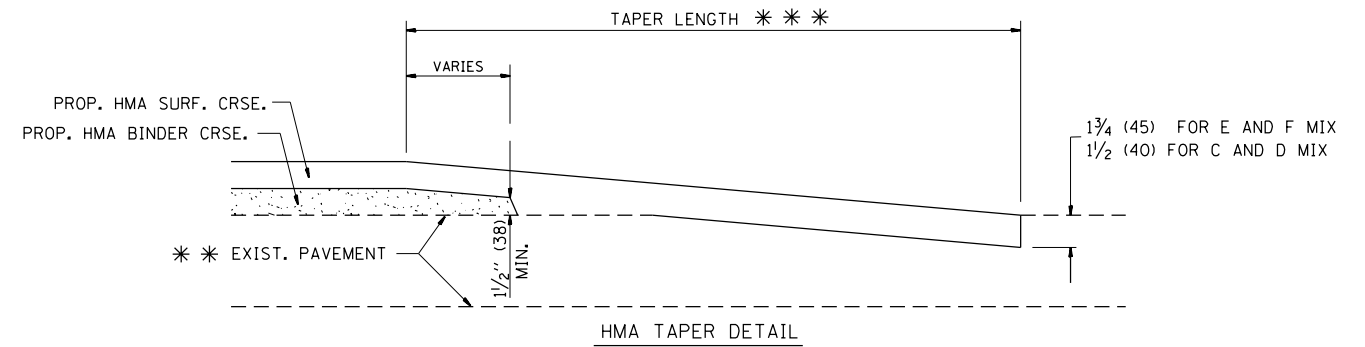


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

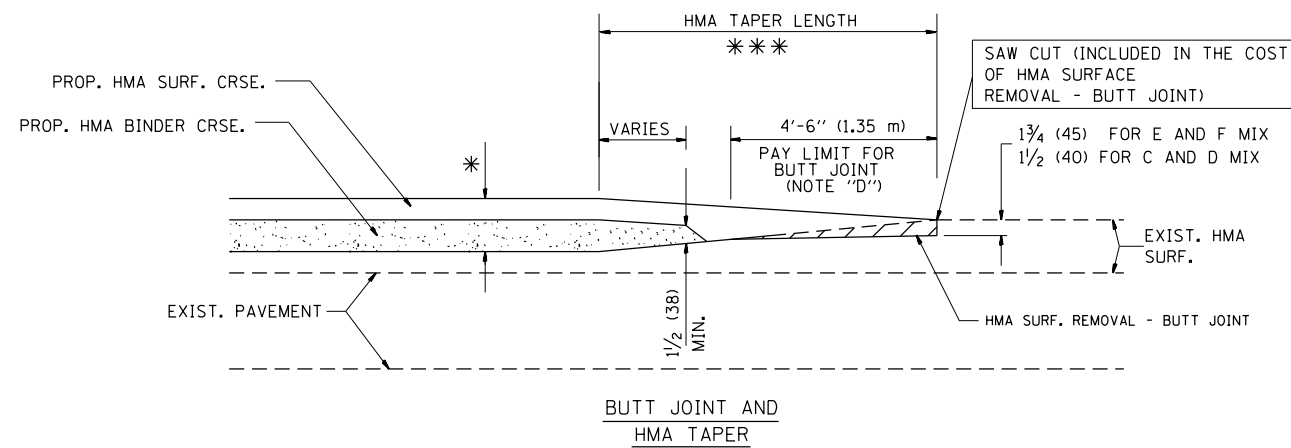
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



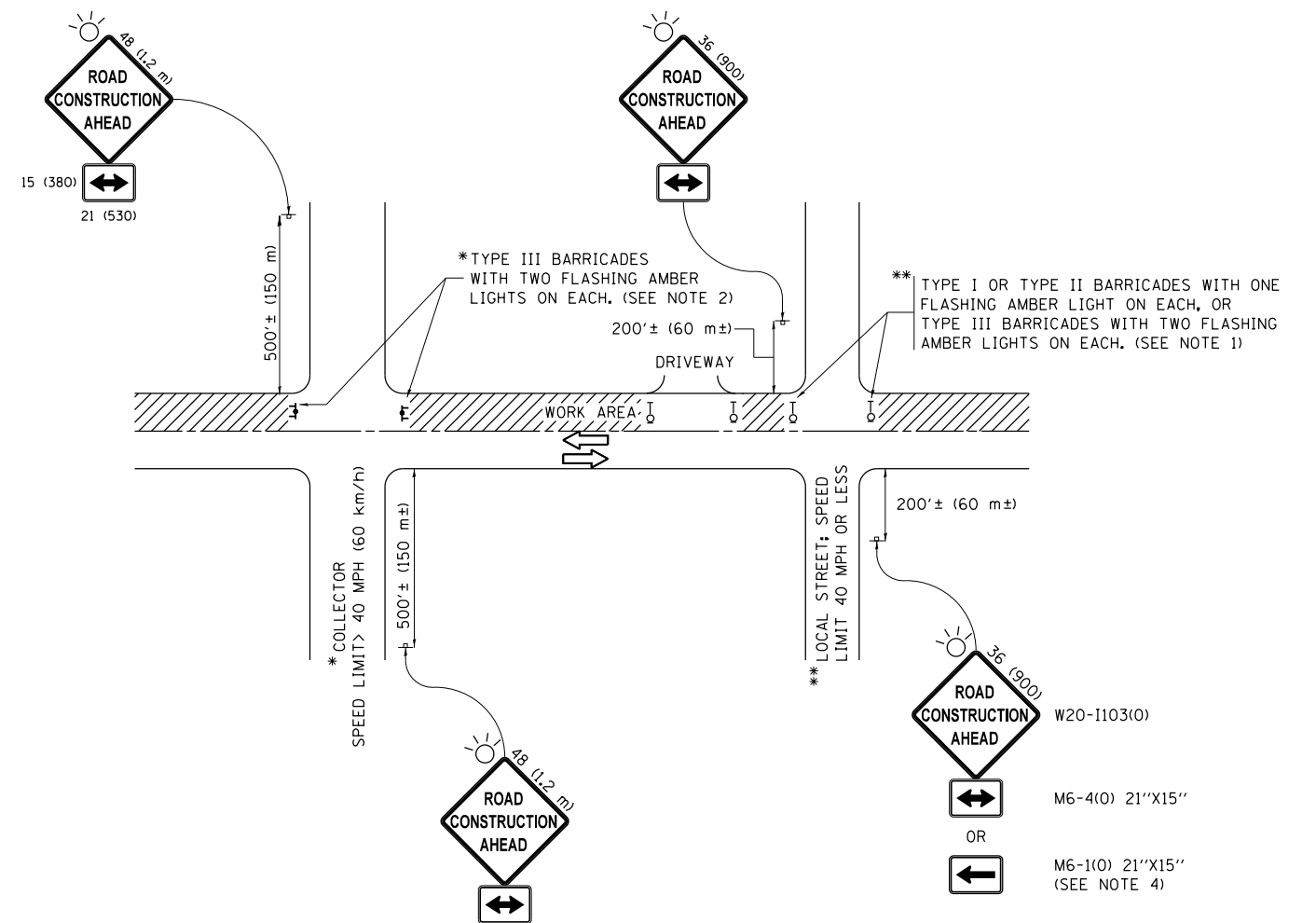
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = paraynoal	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\01436\Drawings\Design\Diststd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 2/1/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	62
BD400-05 BD32		CONTRACT NO. 62D18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

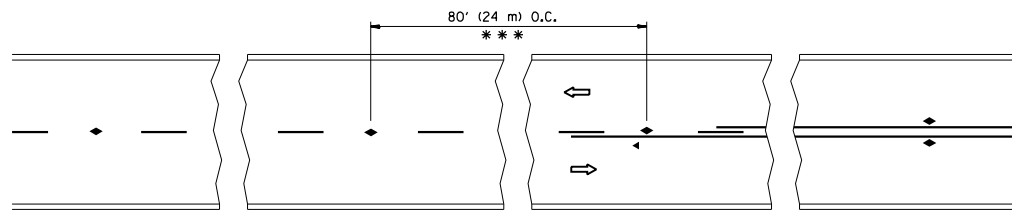
FILE NAME =	USER NAME = paraynoal	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\11084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\1436\Drawings\Design\Diststd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 2/1/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

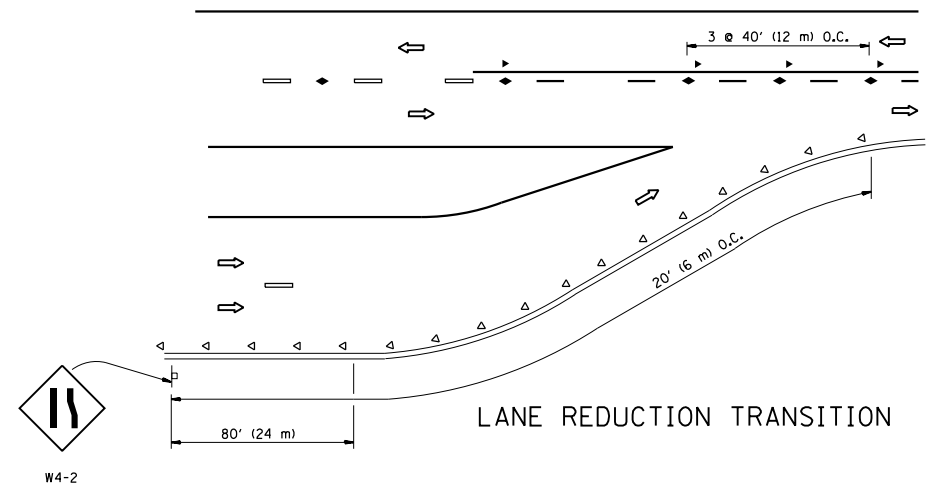
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	63
TC-10			CONTRACT NO. 62D18	
ILLINOIS FED. AID PROJECT				

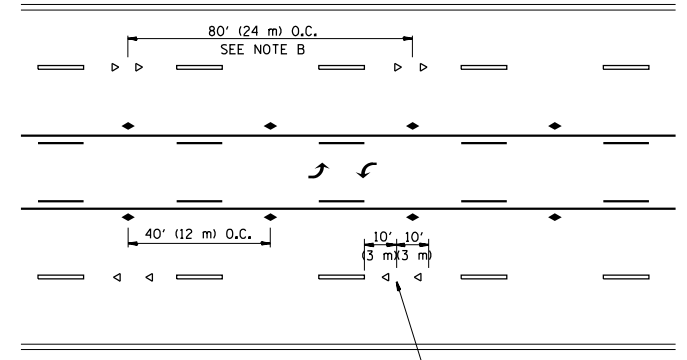


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

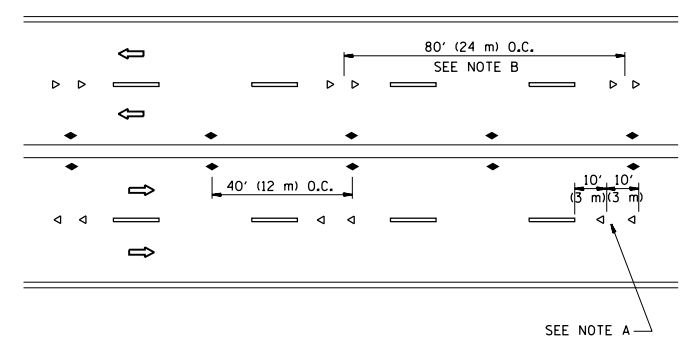
TWO-LANE/TWO-WAY



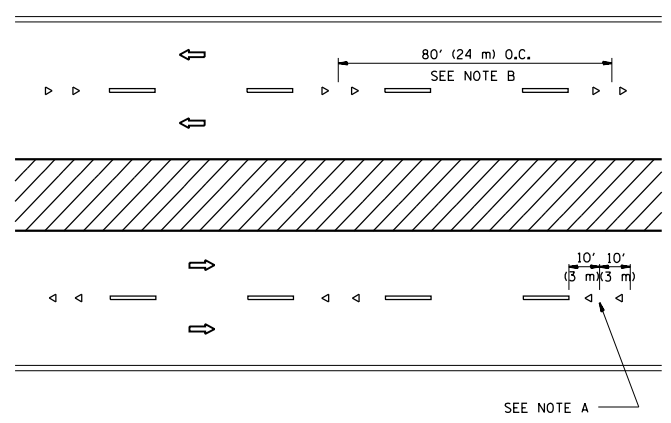
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

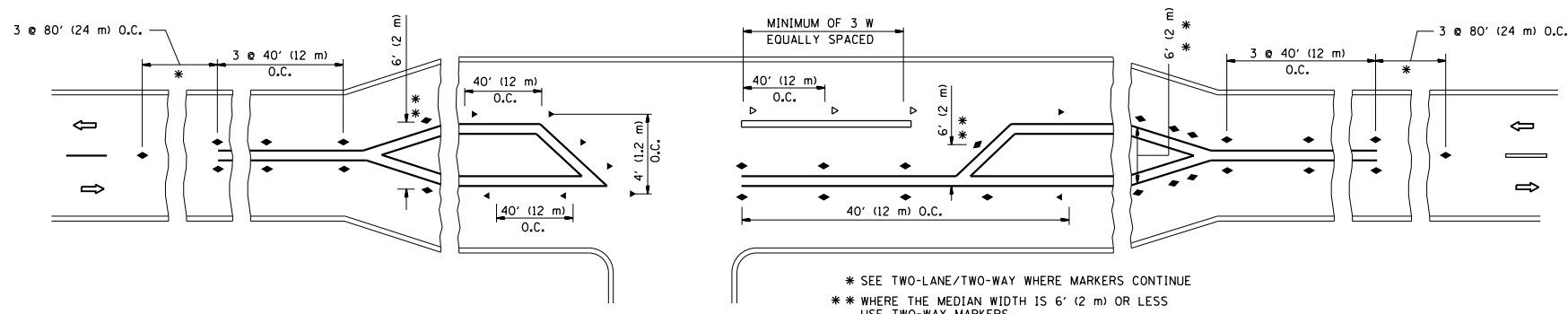
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

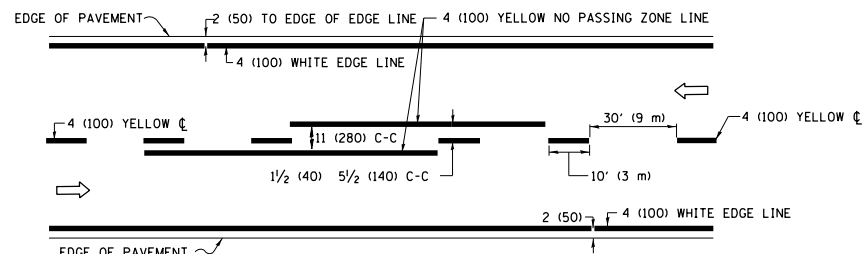


LEFT TURN

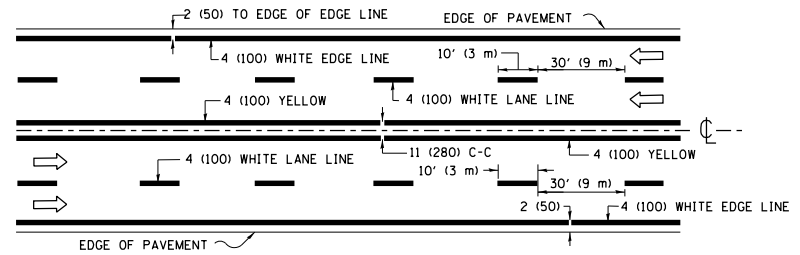
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

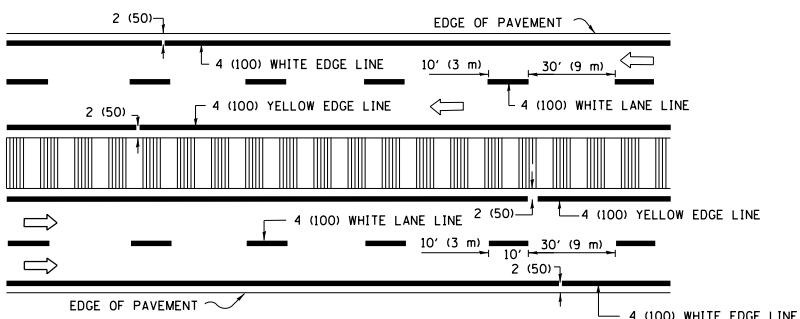
FILE NAME =	USER NAME = parayno1	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\DI436\BROW\Info\Design\Diststd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99					1349	(3197A&B) RS-1	COOK	70	64	
PLOT DATE = 2/1/2018	DATE -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCIUS 09-09-09		TC-11			CONTRACT NO. 62D18					
					SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



2-LANE ROADWAY

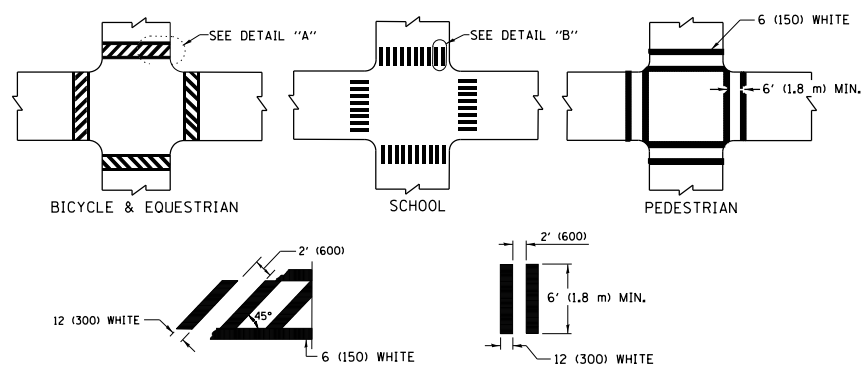


MULTI-LANE UNDIVIDED



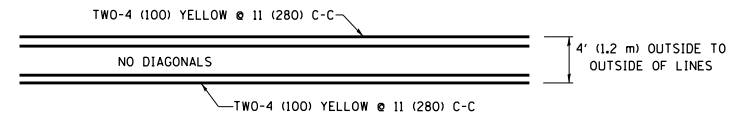
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

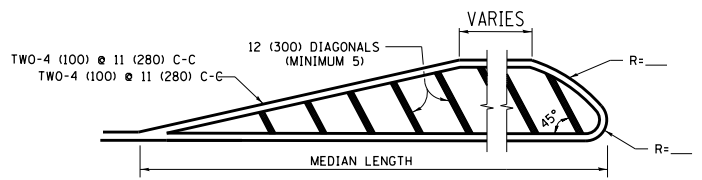


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

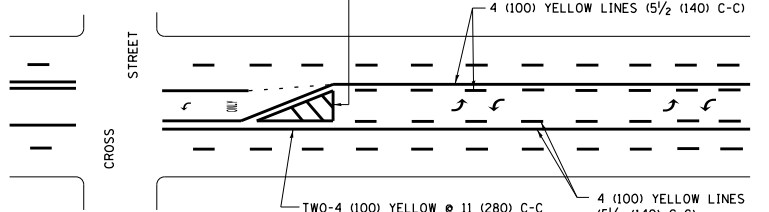


4' (1.2 m) WIDE MEDIANS ONLY



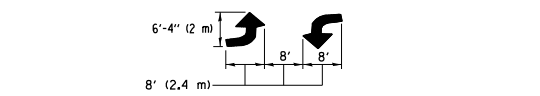
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

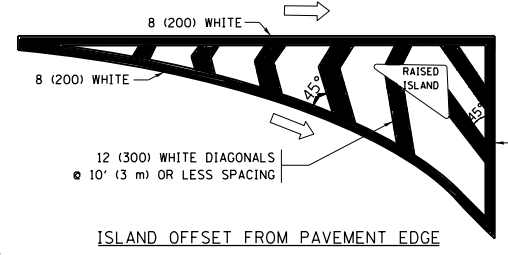
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



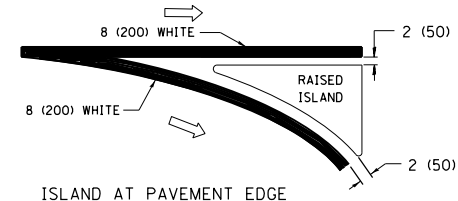
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

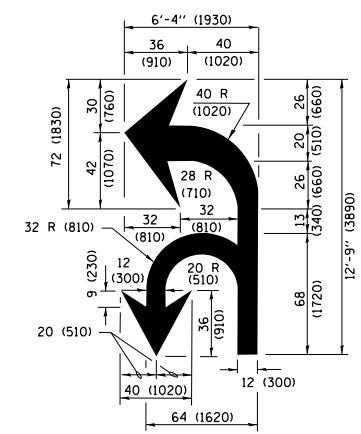


ISLAND OFFSET FROM PAVEMENT EDGE

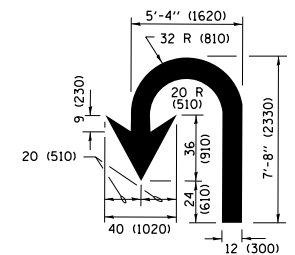


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = paraynoal	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01436\Browns\Design\Diststd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 2/1/2018		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	65
TC-13		CONTRACT NO.	62D18	
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

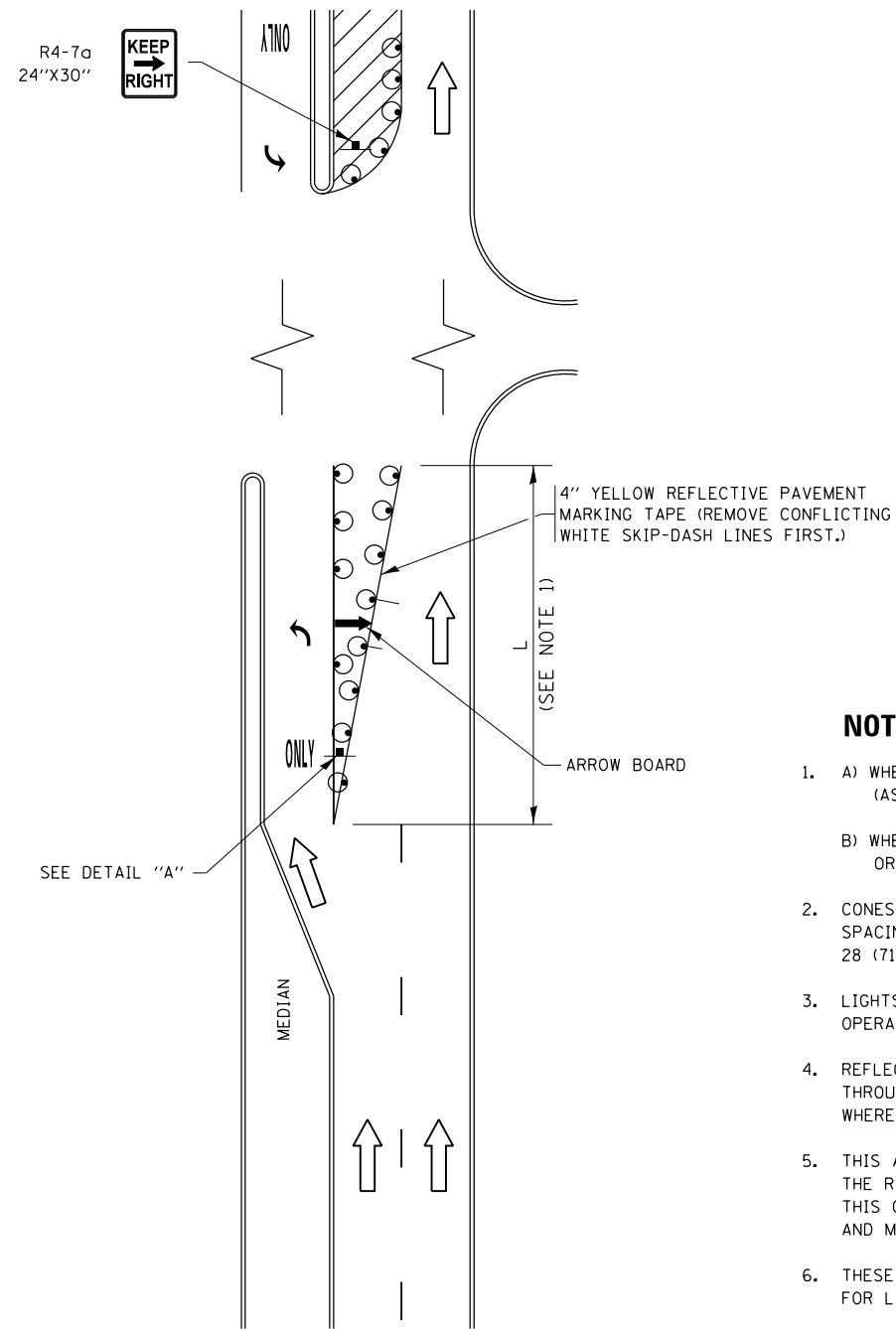


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

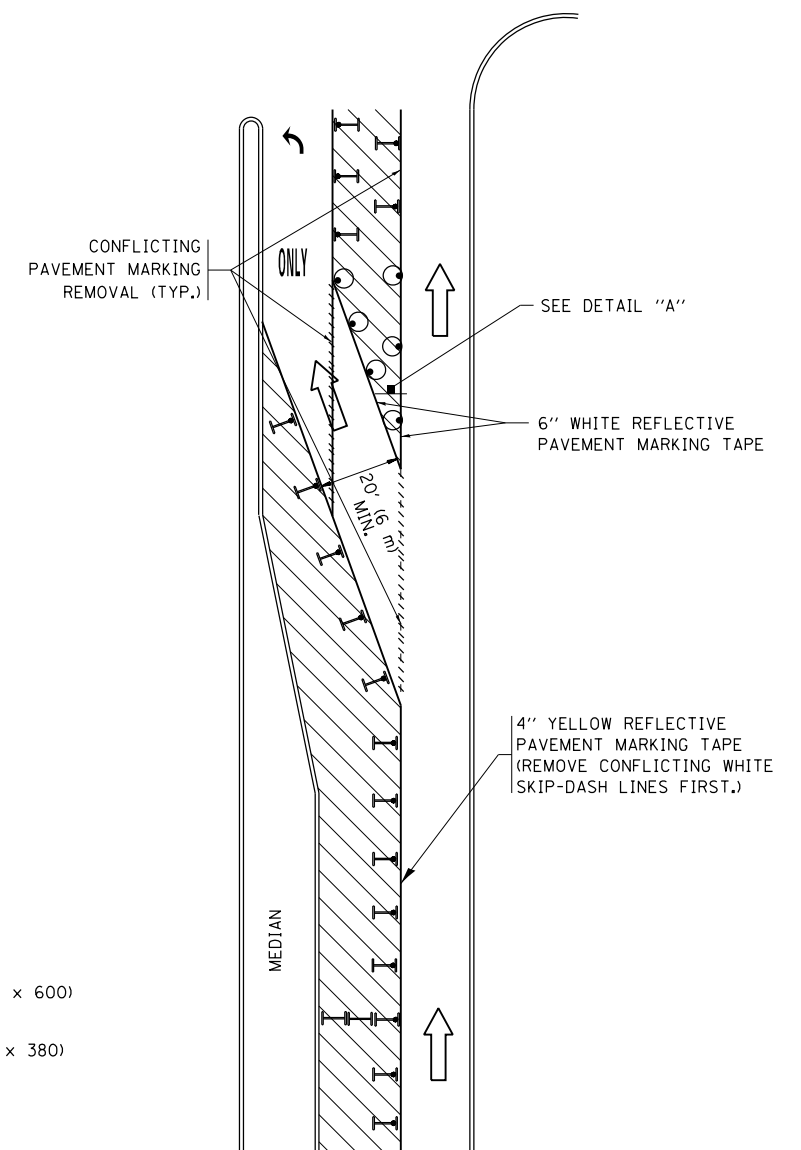
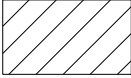








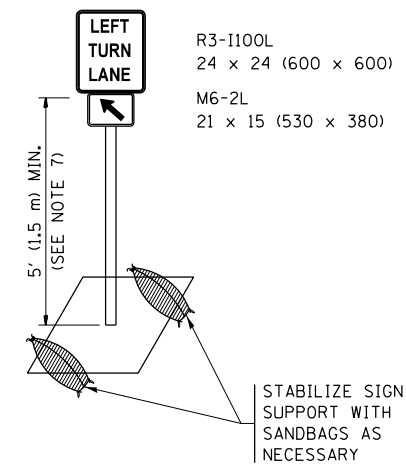
FIGURE 2

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  ARROW BOARD
-  TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  SIGN ASSEMBLY
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

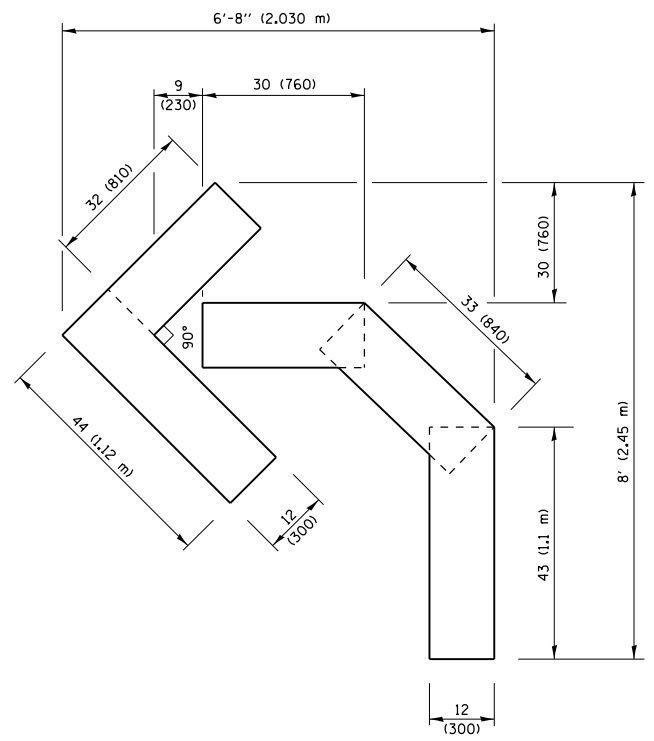


DETAIL A

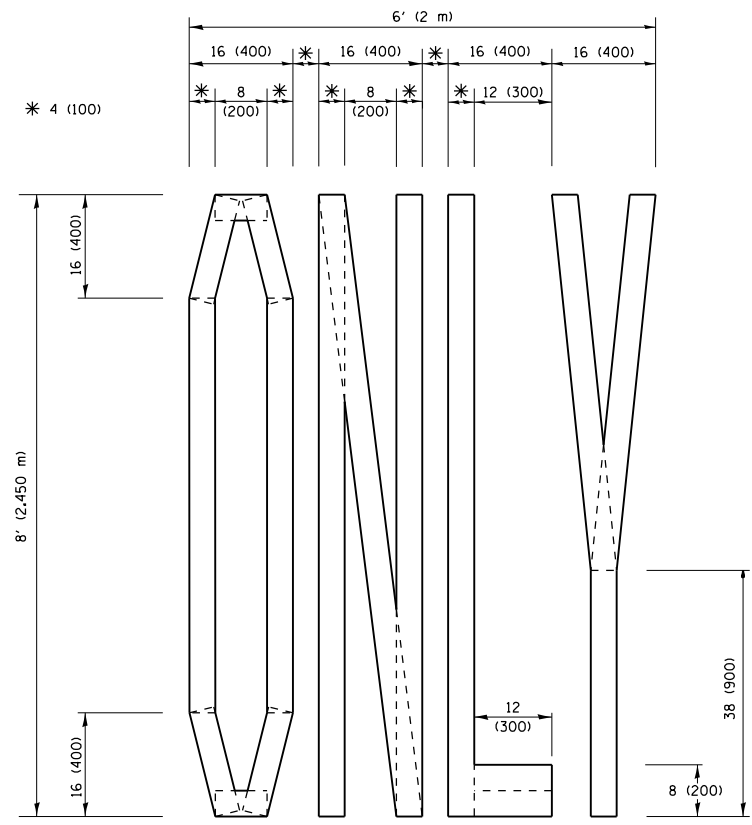
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = parayno1	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\01436\REVISED Design\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			1349	(3197A&B) RS-1	COOK	70	66
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62D18		
	PLOT DATE = 2/1/2018	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.

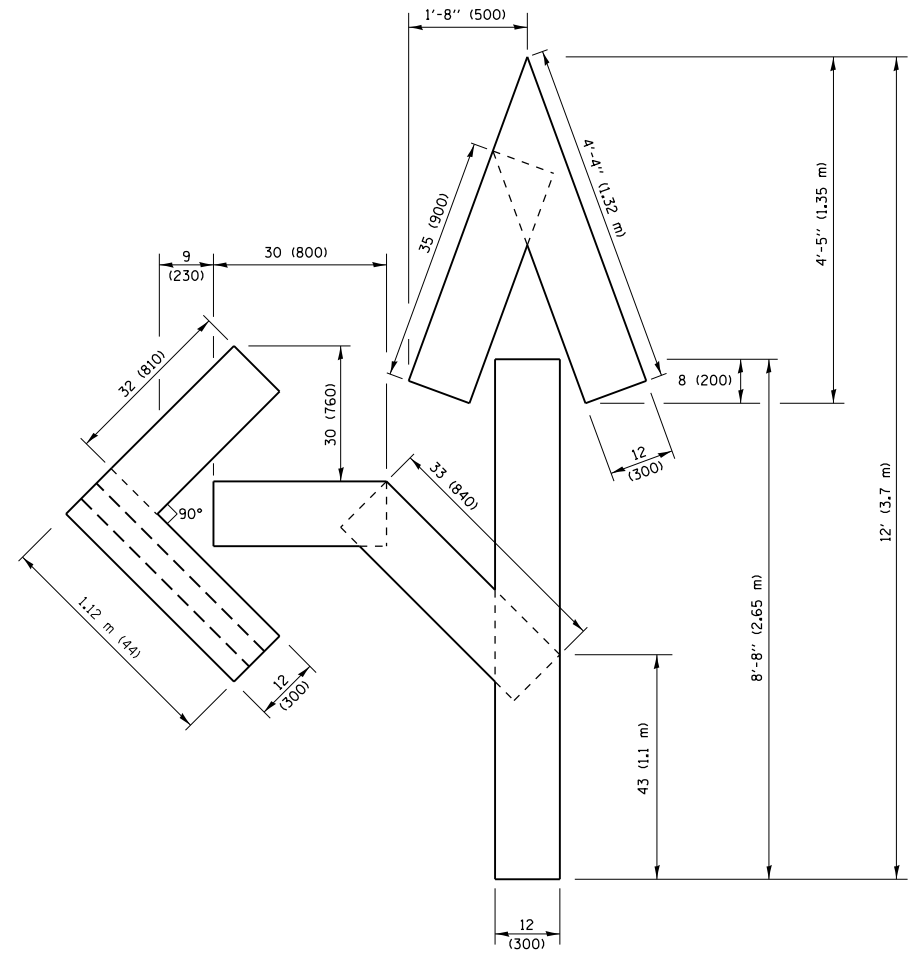
ILLINOIS FED. AID PROJECT



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

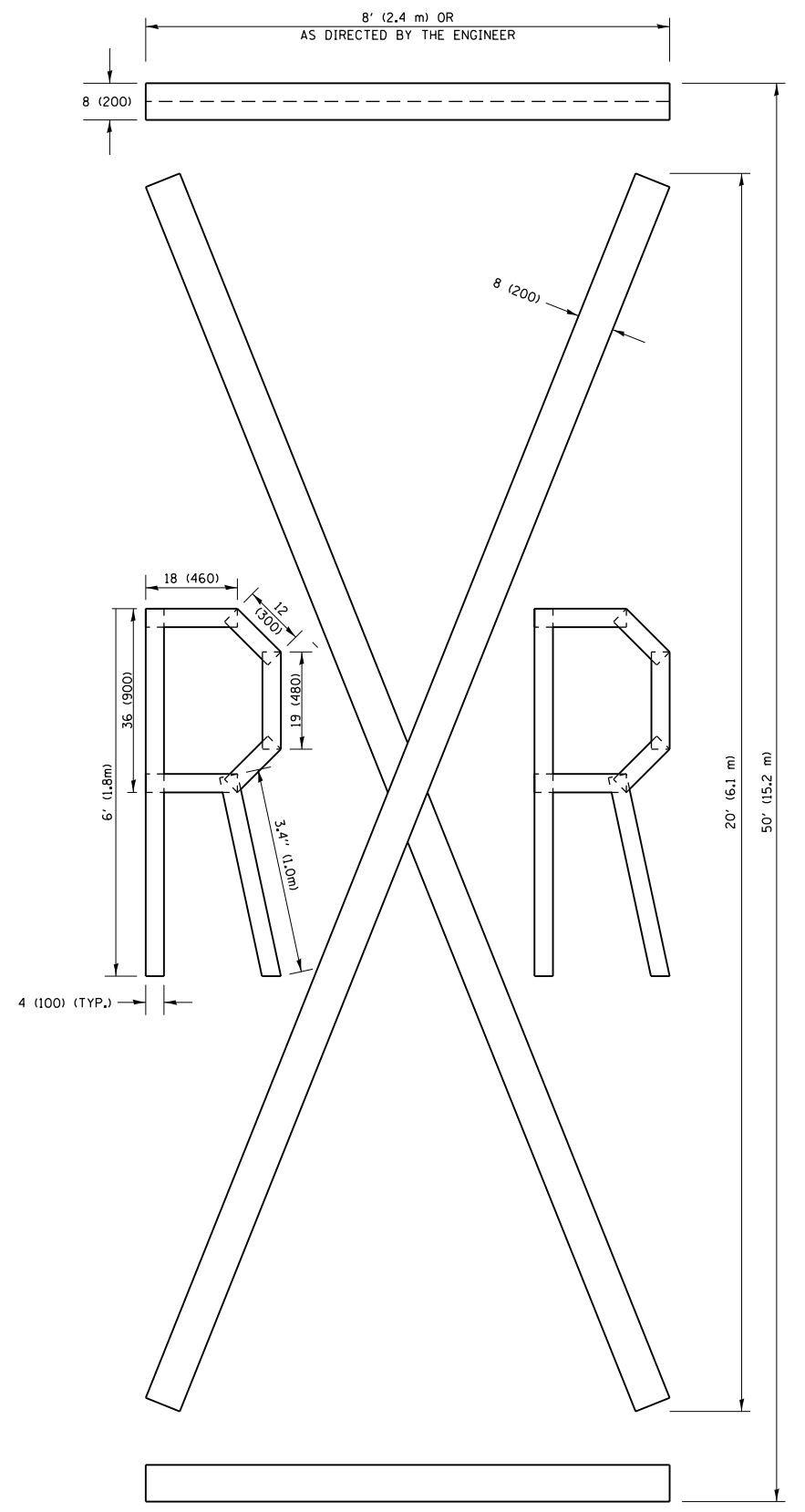


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

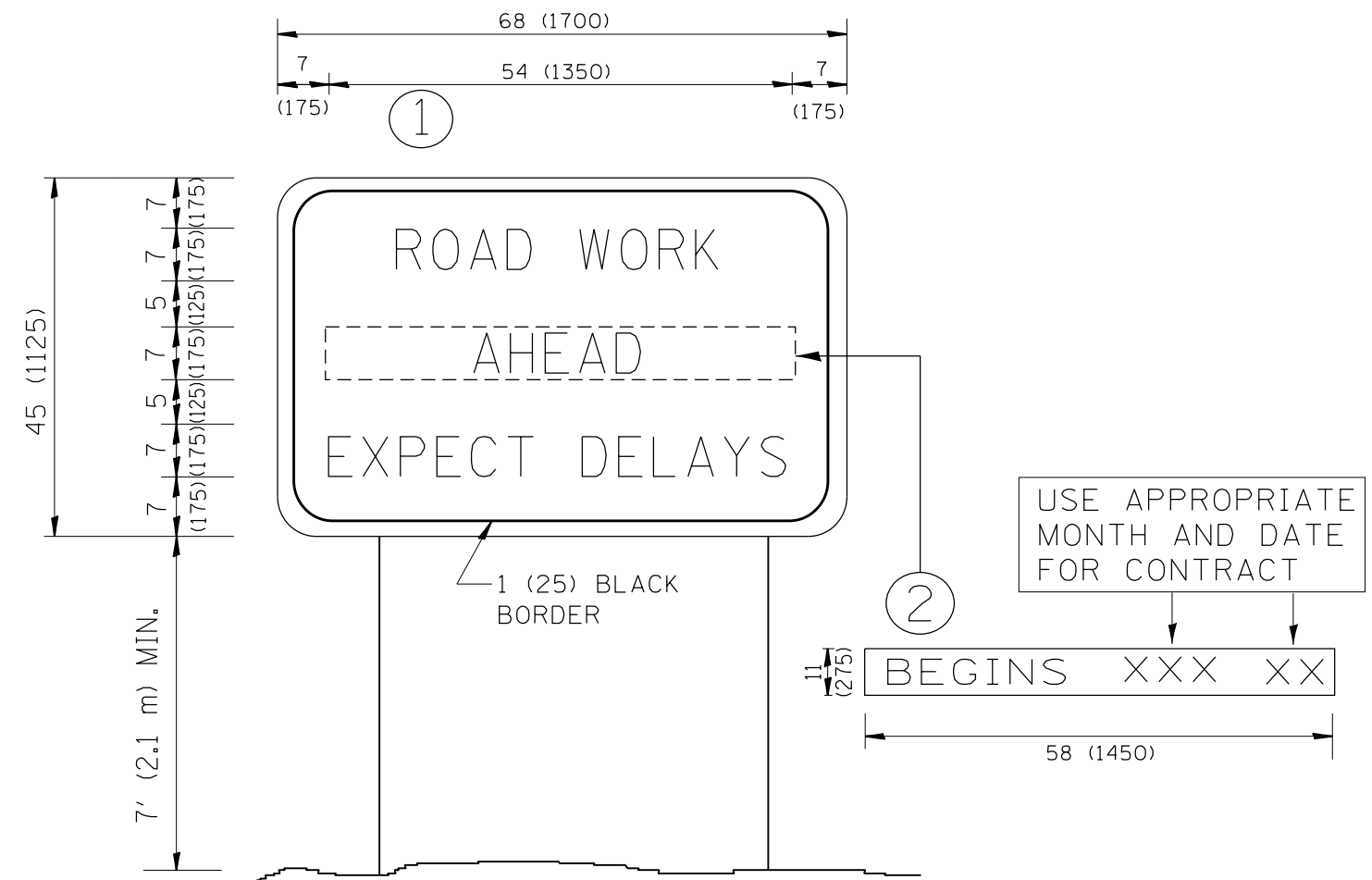
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01436\Drawings\Design\Diststd.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE -	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	67
TC-16		CONTRACT NO. 62D18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED - R. MIRS 09-15-97
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\1436\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - R. MIRS 12-11-97
		DATE -	REVISED - T. RAMMACHER 02-02-99
			REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

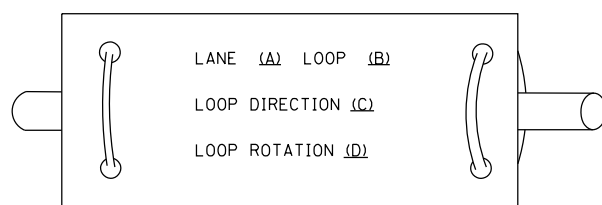
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	68
TC-22			CONTRACT NO. 62D18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

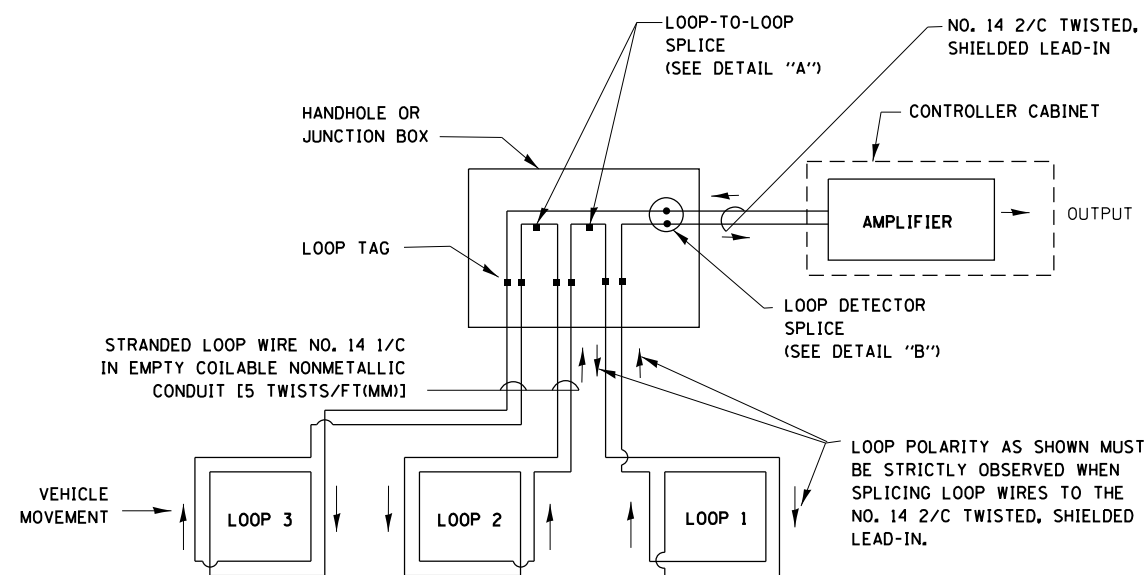
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

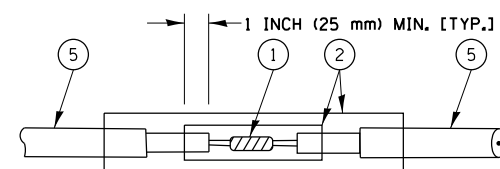


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

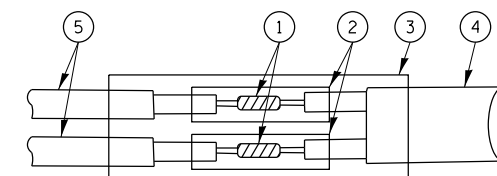


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

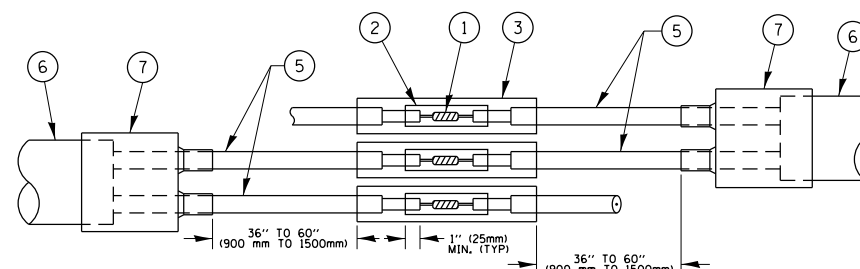


DETAIL "A"
LOOP-TO-LOOP SPLICE

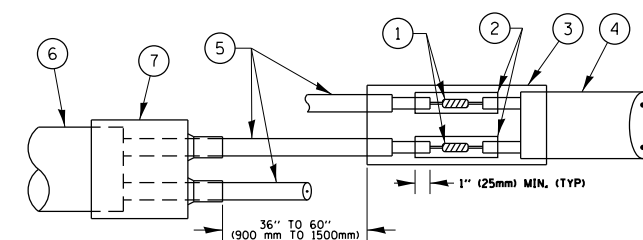


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -
pw:\IL\084EBID\INTEG.\Illinois.gov\PIWID\Documents\DOT Offices\District 1\Projects\01436\Drawings\Design\Diststd.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 2/1/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

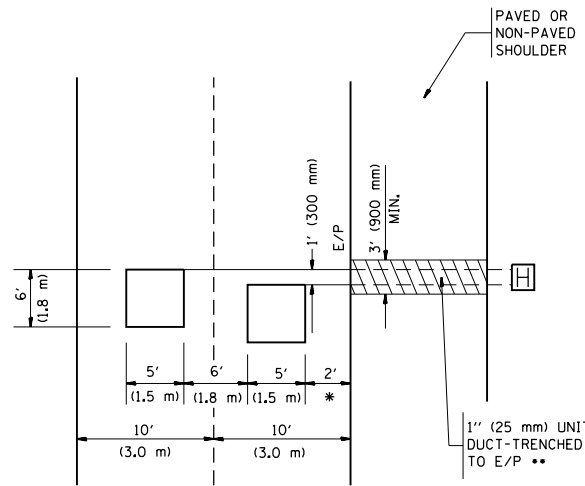
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1349	(3197A&B) RS-1	COOK	70	69
TS-05			CONTRACT NO. 62D18	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



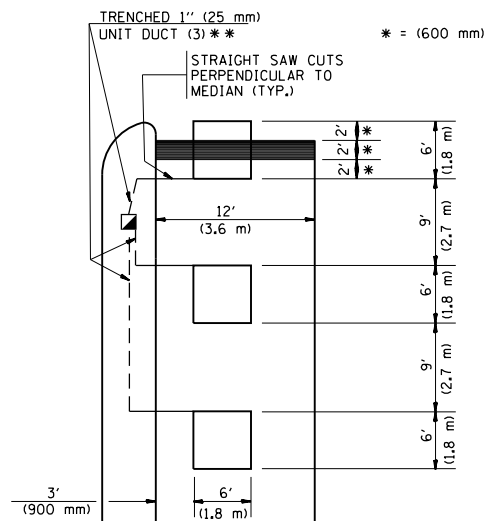
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

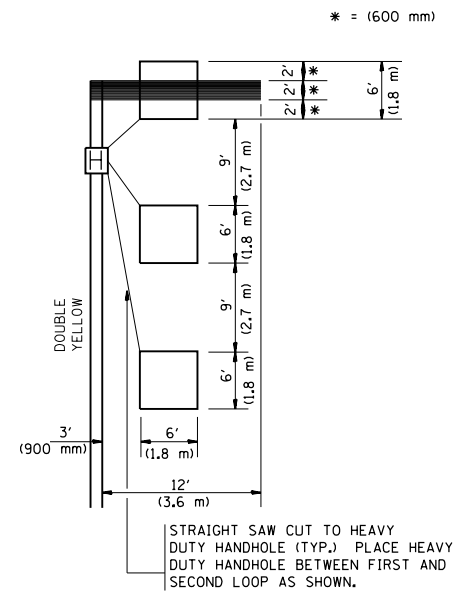


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

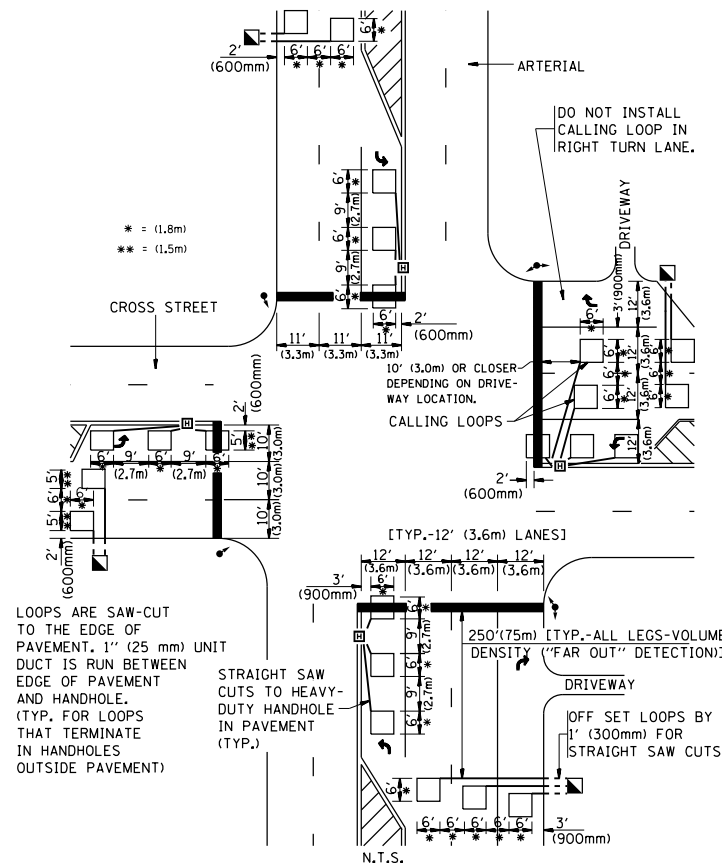
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



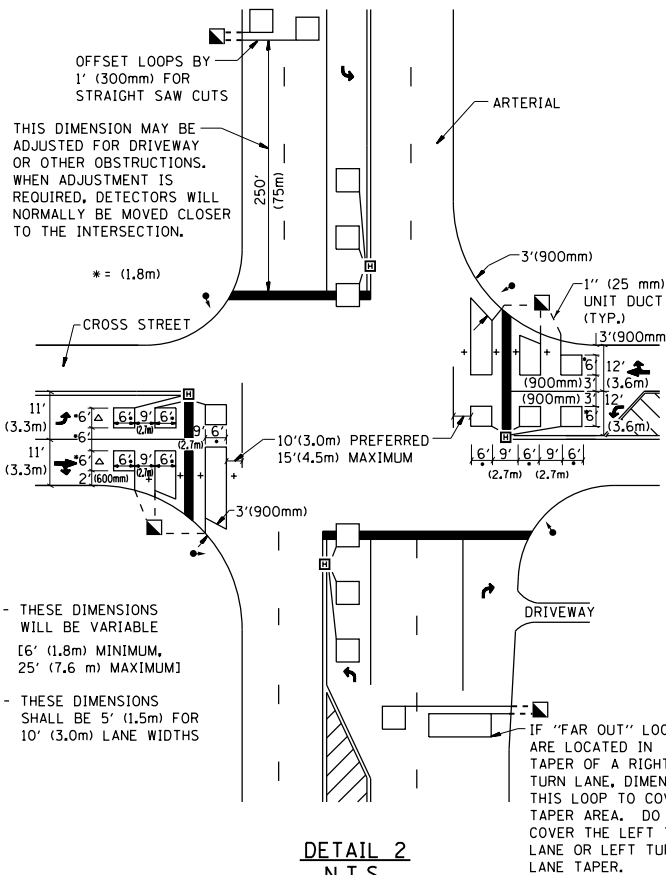
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = paraynoal	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\01436\Drawings\Design\Diststd.dgn		CHECKED - R.K.F.	REVISED -		1349	(3197A&B) RS-1	COOK	70	70			
PLOT SCALE = 100.0000' / 1".		DATE -	REVISED -		TS-07			CONTRACT NO. 62D18				
PLOT DATE = 2/1/2018			REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			