

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2690	17-00091-00-RS	COOK	20	1
		ILLINOIS	CONTRACT NO. 61E63	

04-27-2018 LETTING ITEM 166

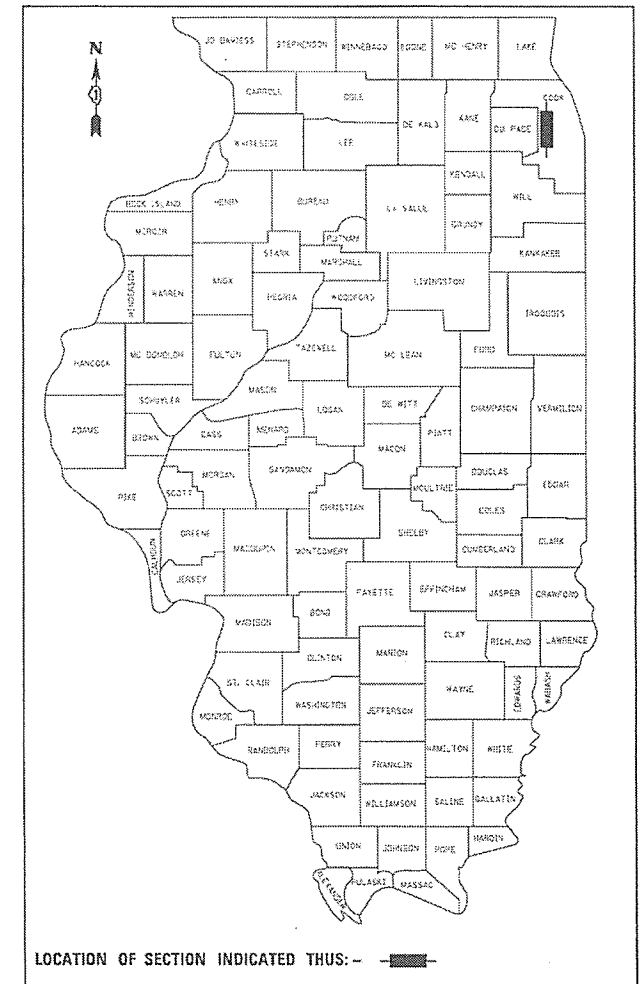
STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

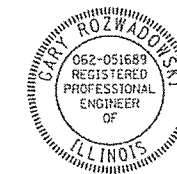
PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY

F.A.U. 2690 (WOLF ROAD)  
NORTH AVENUE (F.A.U. 307) TO WINTERS DRIVE  
ROADWAY RESURFACING  
Section No.: 17-00091-00-RS  
Project No. HUUT(332)  
CITY OF NORTHLAKE  
COOK COUNTY  
JOB NO.: C-91-052-18

FOR INDEX OF SHEETS, SEE SHEET NO. 2



LOCATION OF SECTION INDICATED THIS: -



*Gary Rozwadowski*  
GARY ROZWADOWSKI  
ILLINOIS REGISTRATION NO. 062-051689  
EXPIRATION DATE: 8/30/18

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

APPROVED: *[Signature]*  
Jeffrey T. Sherwin  
CITY OF NORTHLAKE, MAYOR

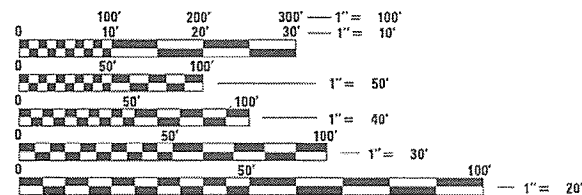
PASSED: *[Signature]*  
February 21, 2018  
Christopher Holt  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID  
BASED ON LIMITED  
REVIEW: *[Signature]*  
February 21, 2018  
Anthony D. Dugley  
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

DESIGN DESIGNATION ROUTE : MAJOR COLLECTOR  
DESIGN SPEED = 30 M.P.H.  
POSTED SPEED = 30 M.P.H.

TRAFFIC DATA: 2014  
ADT = 10,750



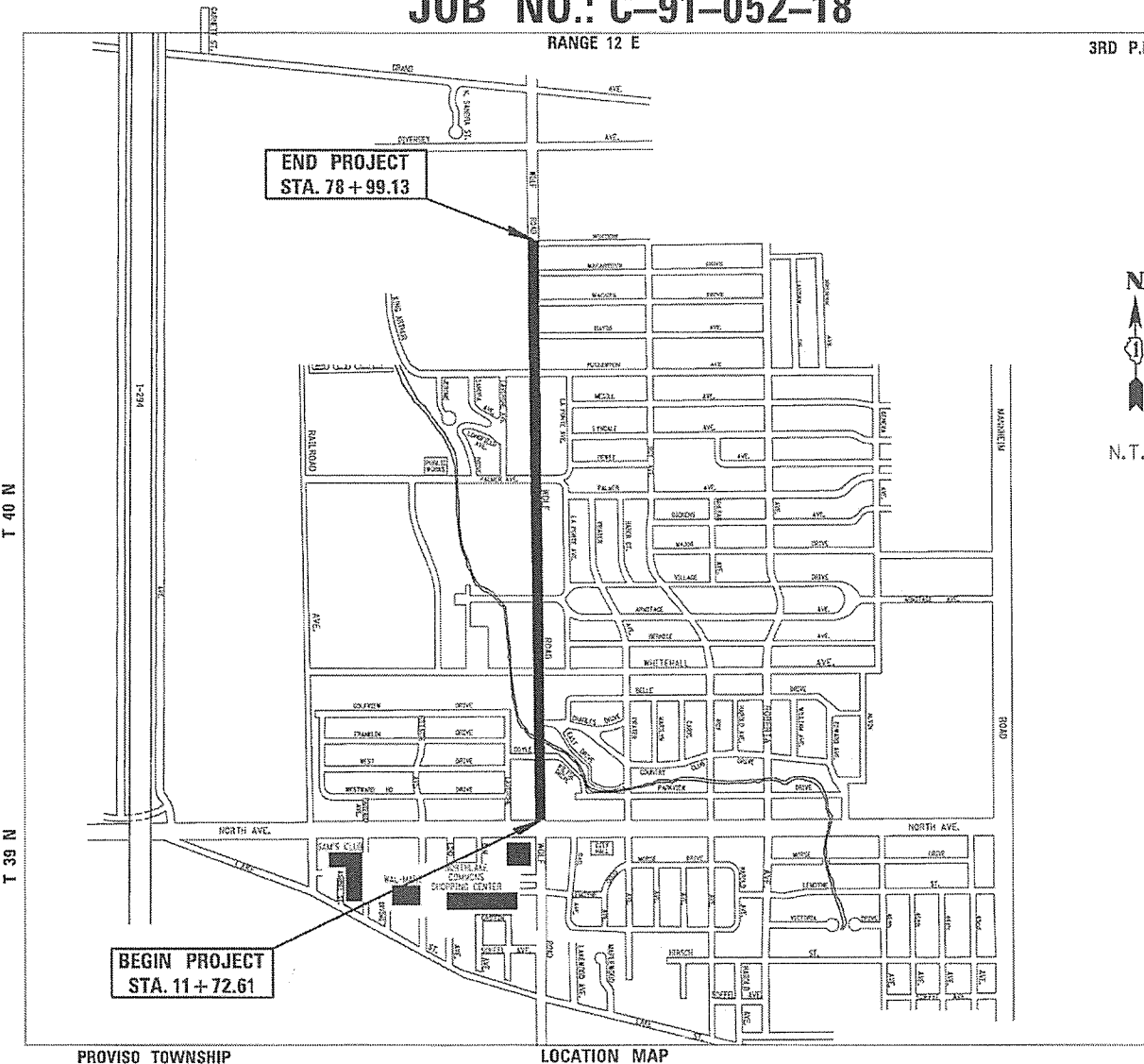
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

**CB** CHRISTOPHER B. BURKE ENGINEERING, LTD.  
9575 W. Higgins Road, Suite 600  
Rosemont, Illinois 60018  
(847) 823-0500

PROFESSIONAL DESIGN FIRM NO.: 184-00875  
EXPIRATION DATE: APRIL 30, 2018

CONTRACT NO. 61E63



T 40 N  
T 39 N

PROVISO TOWNSHIP  
LOCATION MAP  
N.T.S.  
GROSS LENGTH OF PROJECT = 6,850 LINEAL FEET (1.29 MI.)  
NET LENGTH OF PROJECT = 6,850 LINEAL FEET (1.29 MI.)

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, PE, (847) 705-4406 SCHAUMBURG, IL

## GENERAL NOTES

### INDEX OF SHEETS

<u>SHEET NO.</u>	<u>DESCRIPTION</u>
1	TITLE SHEET
2	GENERAL NOTES AND HIGHWAY STANDARDS
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS
5-10	ROADWAY PLANS
11	CONSTRUCTION DETAILS
12	FRAME AND LID ADJUSTMENT WITH MILLING
13	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
14	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
15	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
16	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
17	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
18	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
19	ARTERIAL ROAD INFORMATION SIGN (TC-22)
20	DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING (TS-07)

### HIGHWAY STANDARDS

000001-06 - STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-10 - PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-03 - DIAGONAL CURB RAMPS FOR SIDEWALKS
424021-04 - DEPRESSED CORNER FOR SIDEWALKS
442101-08 - CLASS B PATCHES
604001-04 - FRAME AND LIDS TYPE 1
606001-07 - CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05 - OFF ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701301-04 - LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03 - LANE CLOSURE, 2L, 2W, MOVING OPERATIONS – DAY ONLY
701427-05 – LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701601-09 – URBAN LANE CLOSURE, MULTI LANE, 2W, WITH NONTRAVERSABLE MEDIAN
701602-09 - URBAN LANE CLOSURE, MULTI LANE, 2W, BIDIRECT LEFT TURN LANE
701701-10 - URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06 - SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-07 - TRAFFIC CONTROL DEVICES

### SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2018; THE LATEST EDITIONS OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD) AND "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS"; THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT STANDARD.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

### UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE CITY OF NORTHLAKE IN UNDERGROUND UTILITY CONSTRUCTION WHICH THE CITY MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITY EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE CITY AND ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATIONS IS REQUIRED). THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

### WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE COMPLETED PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS.

ALL EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT THE CITY OF NORTHLAKE WATER DEPARTMENT (TEL. NO. 708-343-8700) FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

### MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT. EXCEPT FOR PERIODS OF SHORT DURATION.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL.

PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS DIRECTED BY THE ENGINEER (SEE IDOT STD. 424001-09).

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

ALL UNDERGROUND, DRIVEWAY, CONCRETE, AND LANDSCAPE RESTORATION WORK IS TO BE COMPLETED BEFORE THE SURFACE COURSE CAN BE INSTALLED.

FOR HOT-MIX ASPHALT SURFACE REMOVAL, THE EXISTING ASPHALT SURFACE SHALL BE REMOVED TO THE DEPTH SPECIFIED. THE GRINDINGS SHALL BE REMOVED FROM THE SITE AND THE SURFACE MECHANICALLY BROOMED UNTIL THE SURFACE IS

COMPLETELY FREE OF ANY LOOSE MATERIAL AND DEBRIS. GRINDING OF THE EXISTING CONCRETE BASE MAY BE REQUIRED TO ESTABLISH THE SPECIFIED DEPTH.

CLASS B PATCHES SHALL CONSIST OF REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT AT LOCATIONS DIRECTED BY THE ENGINEER. AFTER MILLING IS COMPLETE, THE EXISTING PAVEMENT INCLUDING THE BASE AND HMA SURFACE SHALL BE REMOVED TO A DEPTH OF TEN (10) INCHES AND REPLACED WITH TEN (10) INCHES OF SI CONCRETE. THE SURFACE OF THE PATCH SHALL MEET THE SURFACE OF THE HOT-MIX ASPHALT SURFACE REMOVAL. ALL HOLES, SOFT PLACES AND OTHER DEFECTS IN THE SUBBASE OR SUBGRADE SHALL BE CORRECTED BY THE CONTRACTOR BY REMOVING THE UNSUITABLE MATERIAL, ADDING MORE CONCRETE AS SPECIFIED THE STANDARD SPECIFICATIONS.

FRESH OIL SIGNS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER. CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT ALL SIDE STREETS AND BOTH ENDS OF THE ROADWAY WHILE CONSTRUCTION IS IN PROGRESS.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AS WELL AS SUPERVISION/DIRECTION AND MEANS/METHODS OF CONSTRUCTION.

BEFORE BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE LINE AND GRADES SHOWN ON THE CONTRACT DRAWINGS. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONTRACT DRAWINGS, THE CONTRACTOR SHALL IMMEDIATELY REPORT SAME TO THE ENGINEER PRIOR TO PERFORMING WORK. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK AS REQUIRED.

CERTAIN INFORMATION SHOWN ON THESE DRAWINGS HAS BEEN OBTAINED FROM DRAWINGS OF RECORD. CONTRACTOR SHALL VERIFY SUCH INFORMATION. WHERE DISCREPANCIES ARE DISCOVERED THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER. FAILURE BY THE CONTRACTOR TO IMMEDIATELY NOTIFY THE ENGINEER OF SUCH DISCREPANCIES SHALL RESULT IN THE CONTRACTOR BEARING THE FULL BURDEN OF ALL RISKS/COSTS ATTRIBUTED TO THE DISCOVERED DISCREPANCY.

SOIL EROSION PROTECTION SHALL BE IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL. ALL DISTURBED AREAS (NOT IMPERVIOUS IN NATURE) SHALL BE FINE GRADED, TOP SOIL RESTORED (MIN 4 INCHES) AND SEED/MULCH APPLIED UNLESS OTHERWISE SPECIFIED ON THE PLANS.

ANY DEFACED WORK SHALL BE CORRECTED OR REPLACED BY THE CONTRACTOR. THE CITY WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE PER CONDITIONS OF ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS.

FILE NAME =	USER NAME = wbioftus	DESIGNED - WBL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES AND HIGHWAY STANDARDS</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\NORTHLAKE\940032HR - Municipal Review	Projects\940032 HR 200\940032HR281\Civil	DRAWN 032HR128.sh	REVISED -			2690	17-00091-00-RS	COOK	20	2	
	PLOT SCALE = 2'	CHECKED - MW	REVISED -			<b>CONTRACT NO. 61E63</b>					
	PLOT DATE = 3/1/2018	DATE - 8/17/17	REVISED -			SCALE:	SHEET NO.	OF	SHEETS	STA.	TO
						ILLINOIS FED. AID PROJECT					

**SUMMARY OF QUANTITIES**

SUMMARY OF QUANTITIES				STP - LAFO 80% FED. 20% LOCAL WOLF ROAD
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0005 TOTAL QUANTITY
CODE NO.	ITEM	UNIT		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD		800
28000510	INLET FILTERS	EACH		98
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND		35,650
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON		5,940
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON		4,435
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT		11,000
42400400	PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH	SQ FT		3,000
42400800	DETECTABLE WARNINGS	SQ FT		750
44000162	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/2"	SQ YD		48,000
44000600	SIDEWALK REMOVAL	SQ FT		13,650
44200966	CLASS B PATCHES, TYPE I, 10 INCH	SQ YD		800
44200970	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD		4,000
44200974	CLASS B PATCHES, TYPE III, 10 INCH	SQ YD		2,800
44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD		2,000
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD		48,000
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM		1
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM		1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM		1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM		1
70300100	SHORT TERM PAVEMENT MARKING	FOOT		33,210
70300100	SHORT TERM PAVEMENT MARKING REMOVAL	FOOT		33,210
** 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT		1,075
** 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT		28,660
** 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT		1,640
** 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT		535
** 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT		2,375
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH		6
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT		1,200
* X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH		93
* Z0013798	CONSTRUCTION LAYOUT	L SUM		1
* Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT		2,730
* XX002258	STRUCTURES TO BE ADJUSTED	EACH		70
* XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD		200
* XX004786	ADJUSTMENTS, SPECIAL	EACH		28
* XX006947	HOT MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD		150

Δ

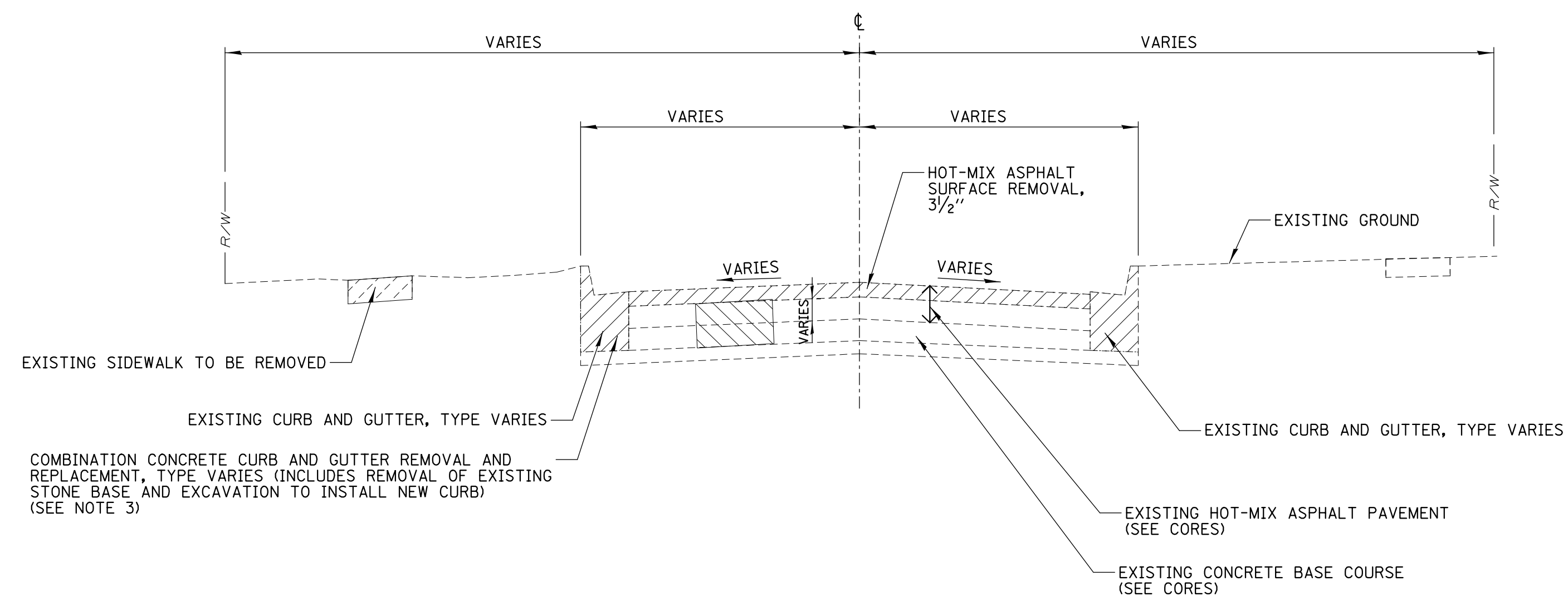
Δ

\* INDICATES SPECIAL PROVISION  
\*\* INDICATES SPECIALTY ITEM

Δ 0042

FILE NAME =	USER NAME = wbleftus	DESIGNED - WBL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\NDRTHLAKE\940032HR - Municipal Review	Projects\940032 HR 200's\940032HR201\Civil\DRAWING\032HR201.WBL	CHECKED - MW	REVISED -			2690	17-00091-00-RS	COOK	20	3
PLOT SCALE = 1"	DATE - 8/17/17	REVISED -				CONTRACT NO. 61E63				
PLOT DATE = 2/16/2018						[ILLINOIS] FED. AID PROJECT				

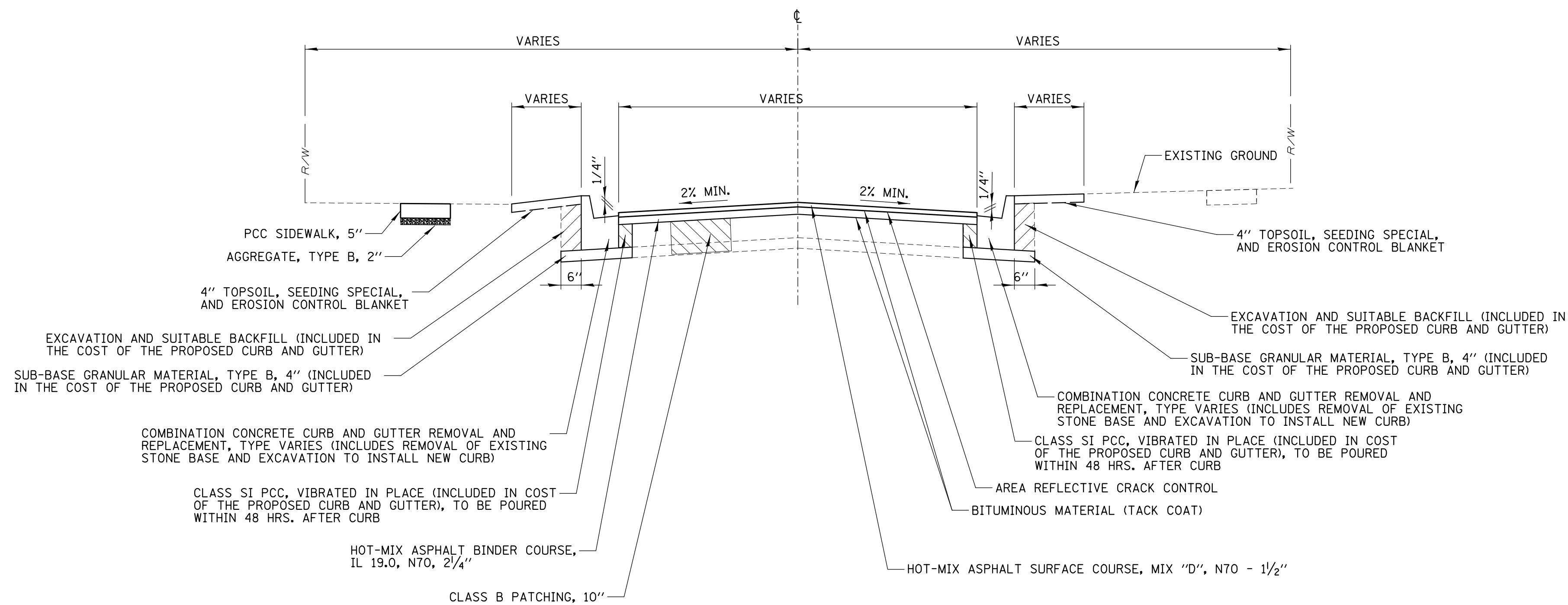
SCALE: SHEET NO. OF SHEETS STA. TO STA.



**EXISTING TYPICAL SECTION**  
**STA. 11 + 72 TO STA. 78 + 99, WOLF ROAD**

**NOTES:**

1. ADDITIONAL STONE BACK FILLING SHALL BE INCLUDED IN THE COST FOR COMBINATION CONCRETE CURB AND GUTTER.
2. CONTRACTOR SHALL SAWCUT PAVEMENT PRIOR TO REMOVING THE CURB.
3. PAVEMENT REMOVAL ADJACENT TO PROPOSED CURB AND GUTTER (INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER).
4. ALL WORK INCLUDING LANDSCAPE RESTORATION MUST BE COMPLETED AND APPROVED BY ENGINEER PRIOR TO FINAL PAYOUT.
5. CONTRACTOR SHALL MILL PRIOR TO PATCHING.
6. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR PETROMAT (FABRIC) ENCOUNTER DURING GRINDING OPERATIONS. REMOVAL OF PETROMAT SHALL BE CONSIDERED INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL.



**PROPOSED TYPICAL SECTION**  
**GRIND AND OVERLAY**  
**STA. 11 + 72 TO STA. 78 + 99, WOLF ROAD**

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
ROADWAY ITEM	AC-TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", (IL-9.5mm) N70	PG58-22/ PG58-28*	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0 N70	PG58-22/ PG58-28*	4% @ 70 GYR
TEMPORARY HOT-MIX ASPHALT SURFACE COURSE, MIX "D", (IL-9.5mm) N50	PG58-22/ PG58-28*	4% @ 50 GYR

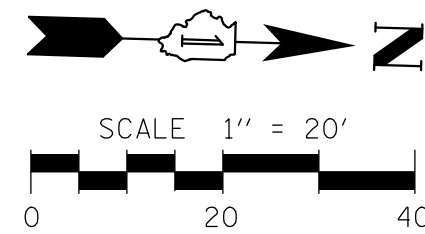
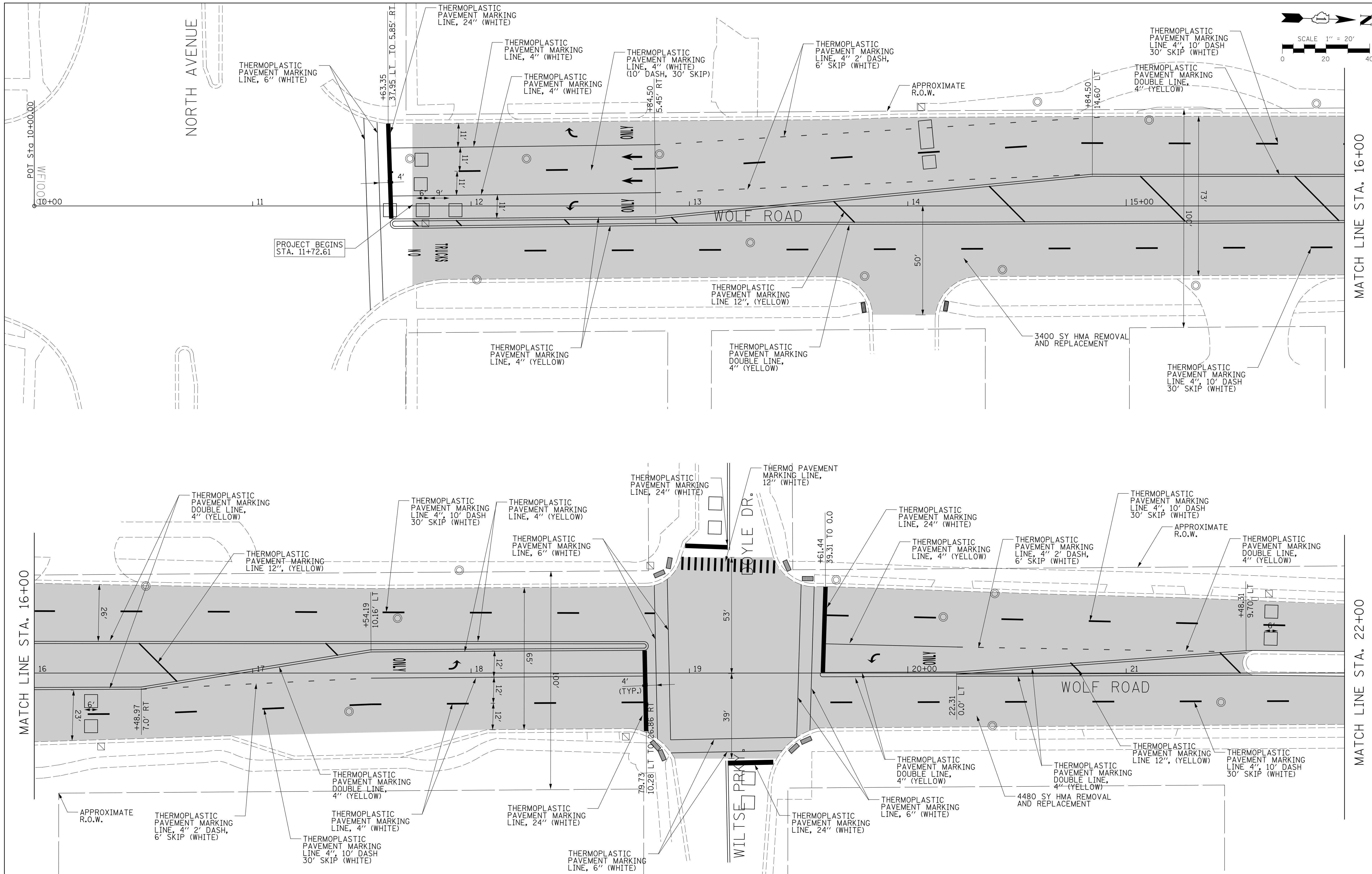
**NOTES:**

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- \*2. WHEN ASPHALT BINDER REPLACEMENT (ABR) EXCEEDS 15%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG58-28. NO MORE THAN 2% RECLAIMED ASPHALT SHINGLES SHALL BE ALLOWED IN THE ASPHALT.

**LOCATIONS:**

TOTAL HOT-MIX ASPHALT TO BE REMOVED (3 1/2") & REPLACED (3 3/4")

- |   |
|---|
| 1. WOLF ROAD<br>NORTH AVENUE TO WINTERS DRIVE |
|---|



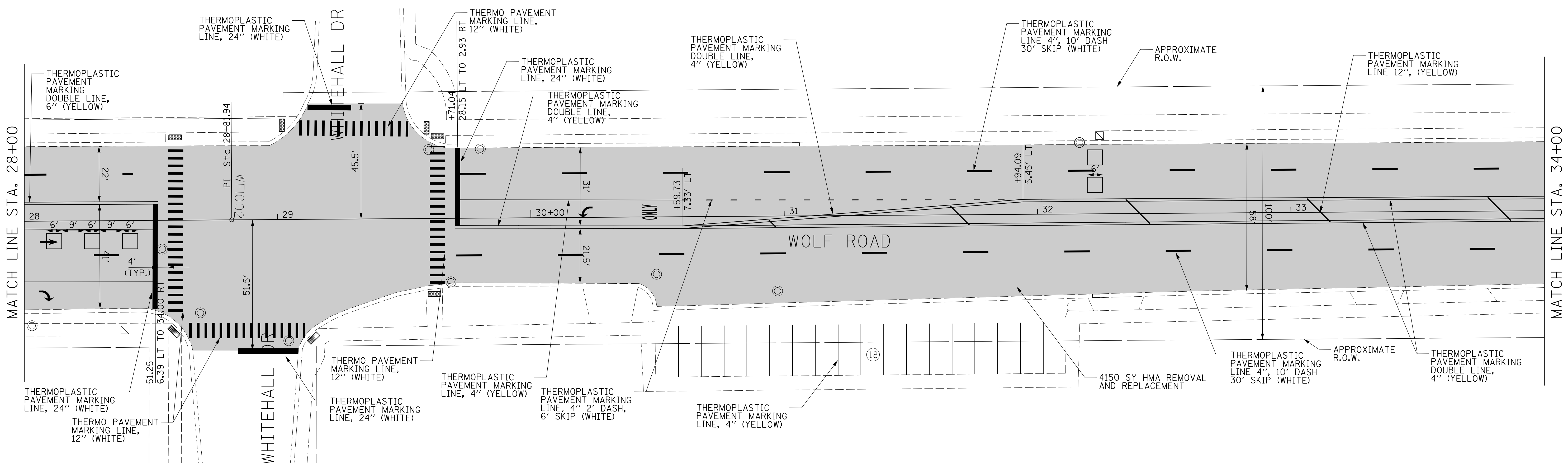
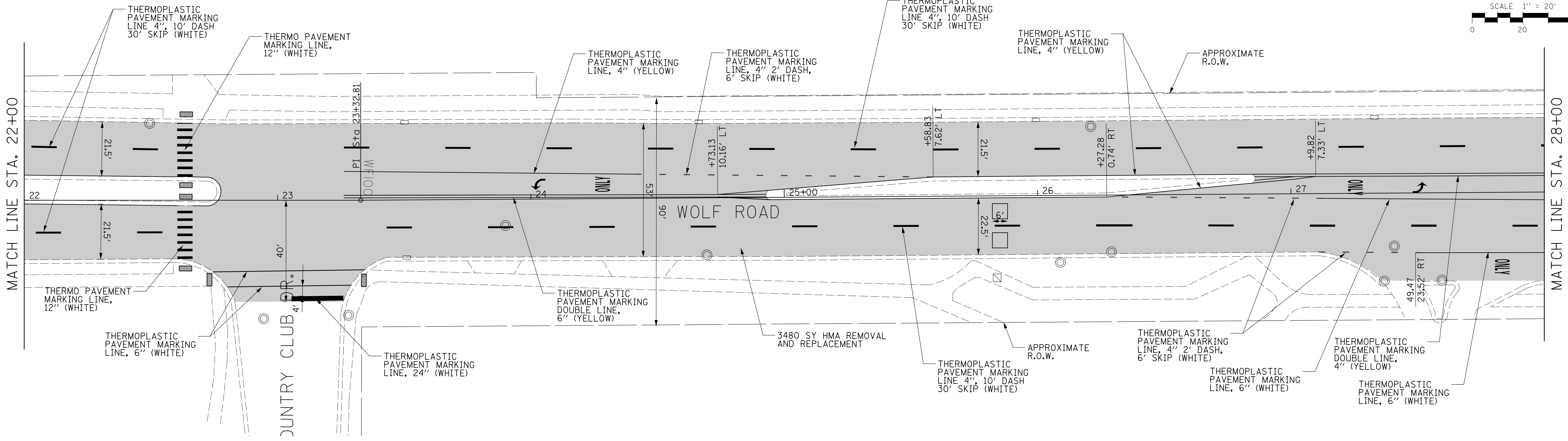
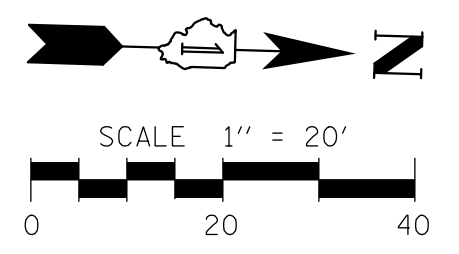
FILE NAME = N:\NORTHLAKE\940032HR - Municipal Review	USER NAME = wbioftus	DESIGNED - WBL	REVISED -
Projects\940032 HR 200\940032HR281\Civil1	PROJECT NO. = 17-00091-00-RS	CHECKED - MW	REVISED -
PLOT SCALE = 20'	DATE = 8/17/17		
PLOT DATE = 2/16/2018			

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
WOLF ROAD**

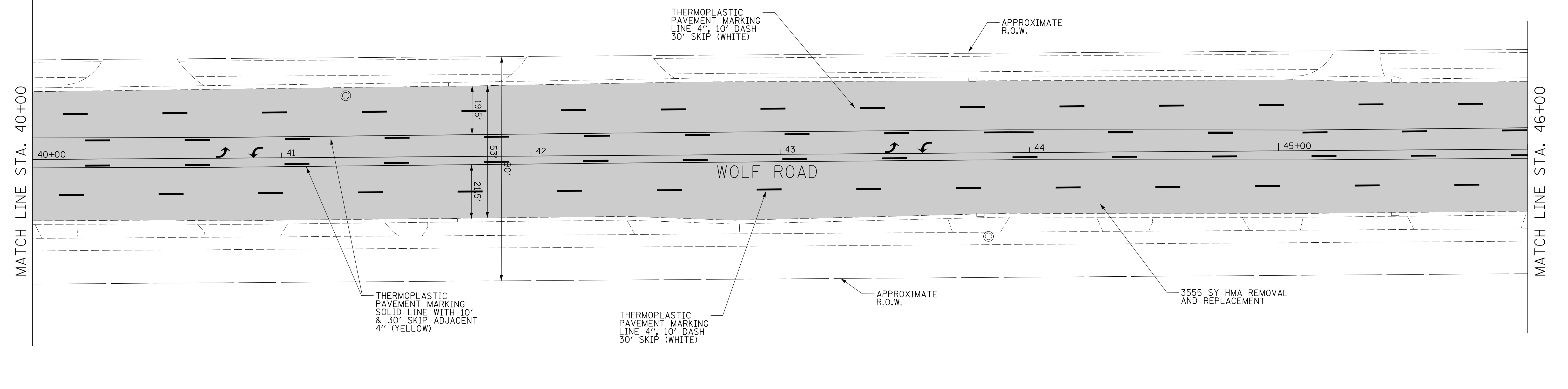
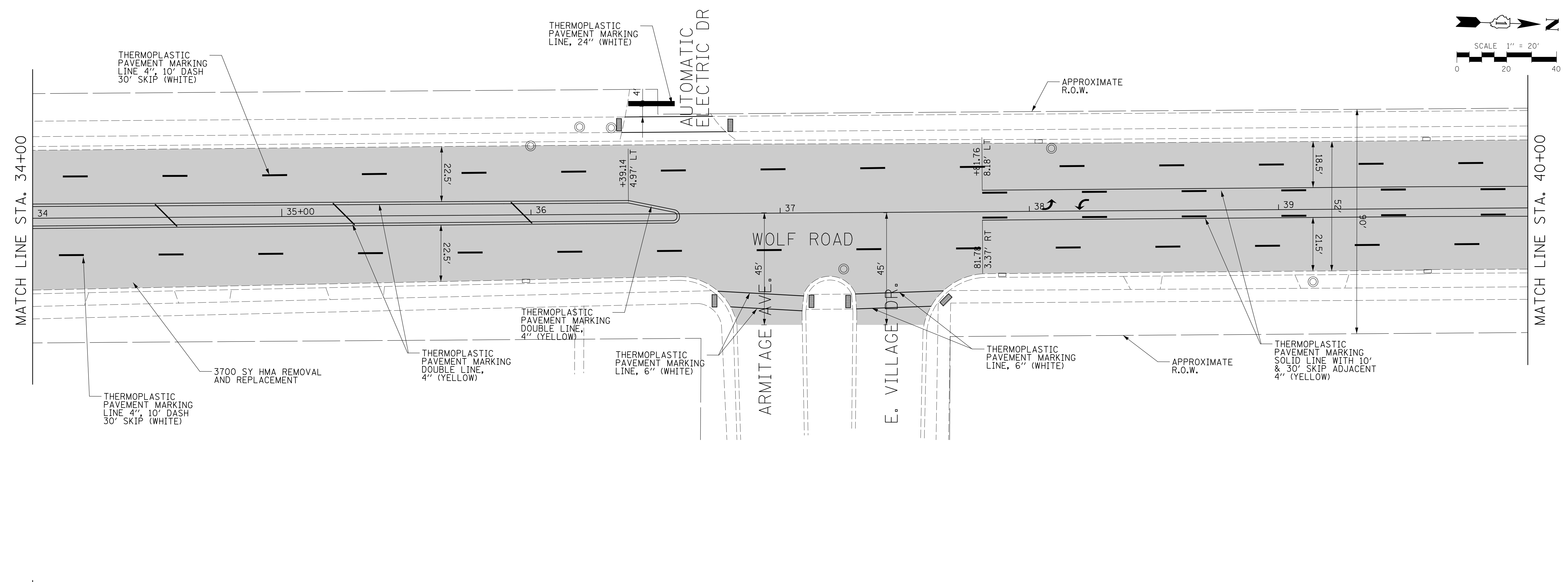
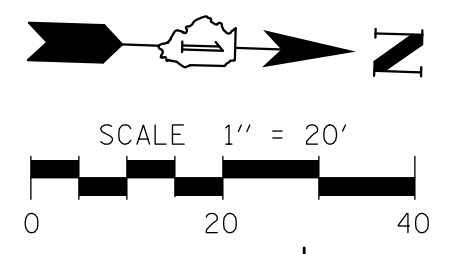
SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_\_ SHEETS STA. 10+00 TO STA. 22+00

F.A.U. RTE. 2690	SECTION 17-00091-00-RS	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 5
CONTRACT NO. 61E63			ILLINOIS FED. AID PROJECT	

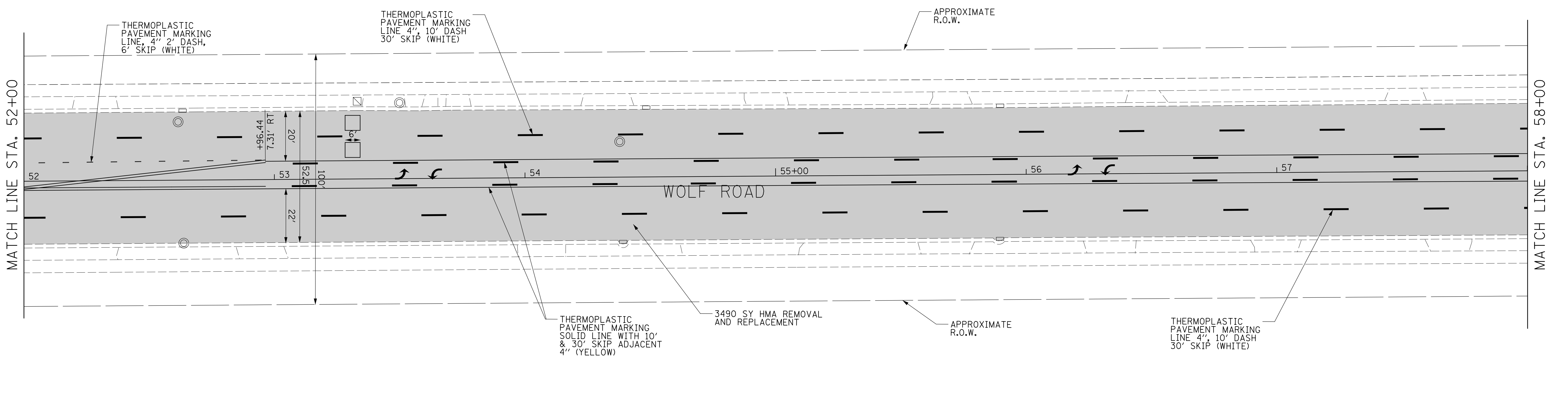
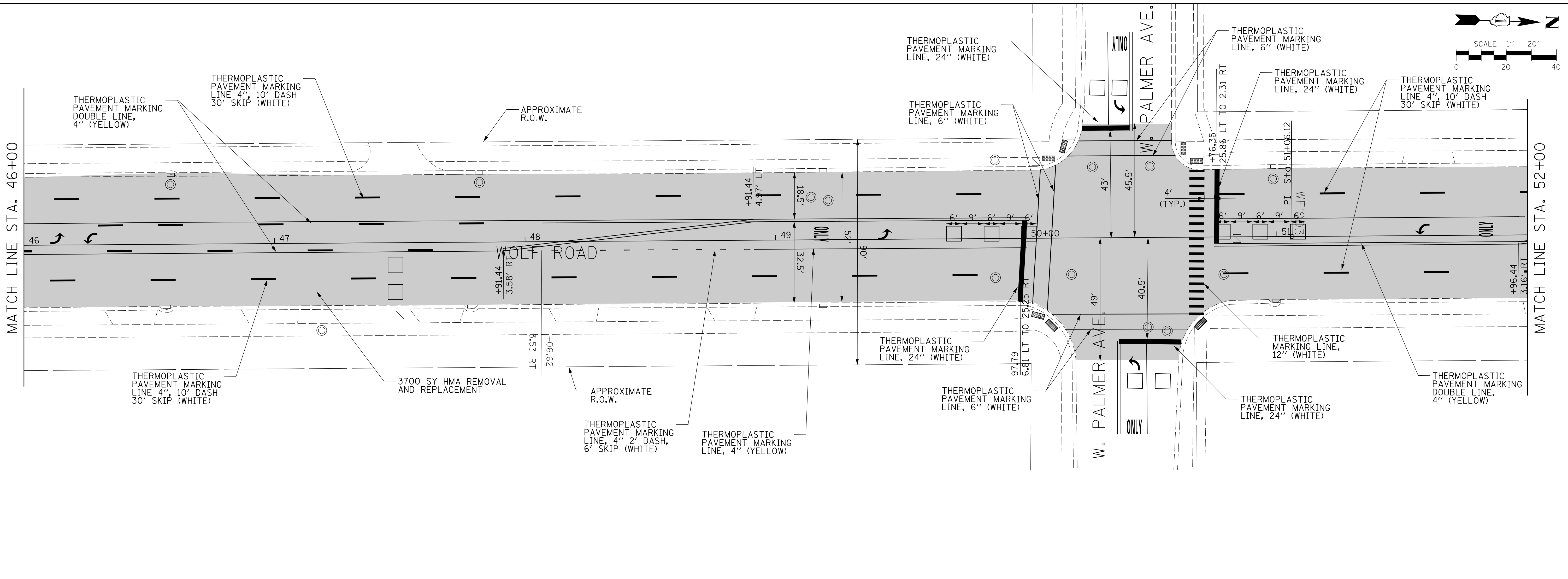
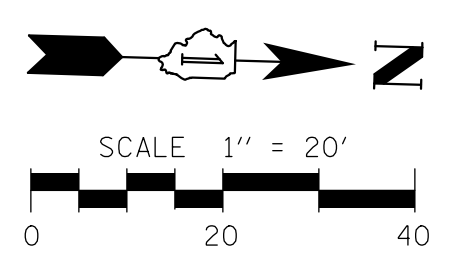


FILE NAME =	USER NAME = wblotus	DESIGNED - WBL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN WOLF ROAD</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\NORTHLAKE\940032HR - Municipal Review	Projects\940032 HR 200\940032HR281\Civil1	PROJECT NO. 940032HR281-01	REVISED -			2690	17-00091-00-RS	COOK	20	6	
PLOT SCALE = 20'		CHECKED - MW	REVISED -			CONTRACT NO. 61E63					
PLOT DATE = 2/16/2018		DATE - 8/17/17	REVISED -			ILLINOIS FED. AID PROJECT					

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. 22+00 TO STA. 34+00



FILE NAME = N:\NORTHLAKE\940032HR - Municipal Review	USER NAME = wbioftus	DESIGNED - WBL	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN WOLF ROAD</b>	F.A.U. RTE. = 2690	SECTION = 17-00091-00-RS	COUNTY = COOK	TOTAL SHEETS = 20	SHEET NO. = 7		
PROJECTS \940032 HR 200\940032HR281\Civil1	PROJECTS \940032 HR 200\940032HR281\Civil1	CHECKED - MW	REVISED -			SCALE: _____	SHEET NO. _____ OF _____ SHEETS	CONTRACT NO. 61E63	ILLINOIS FED. AID PROJECT			
PLOT SCALE = 20'	DATE = 8/17/17	DATE = 8/17/17	REVISED -			STA. 34+00 TO STA. 46+00						
PLOT DATE = 2/16/2018			REVISED -									



FILE NAME = N:\NORTHLAKE\940032HR - Municipal Review	USER NAME = wbioftus	DESIGNED - WBL	REVISED -
Projects\940032 HR 200's\940032HR281\Civil	PROJECT NO. = 17-00091-00-RS	CHECKED - MW	REVISED -
PLOT SCALE = 20'	DATE = 8/17/17		
PLOT DATE = 2/16/2018			

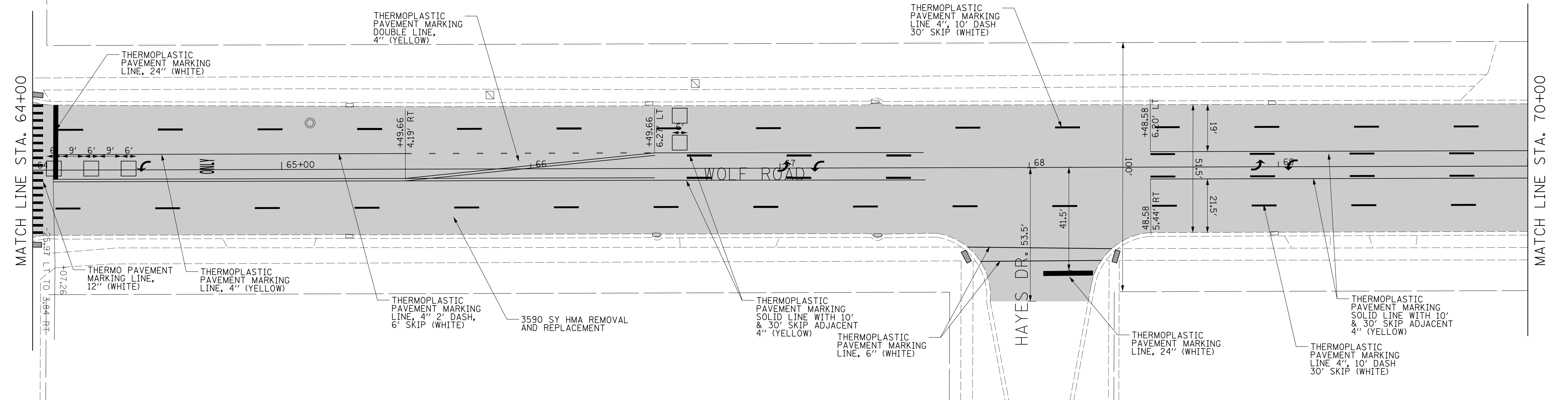
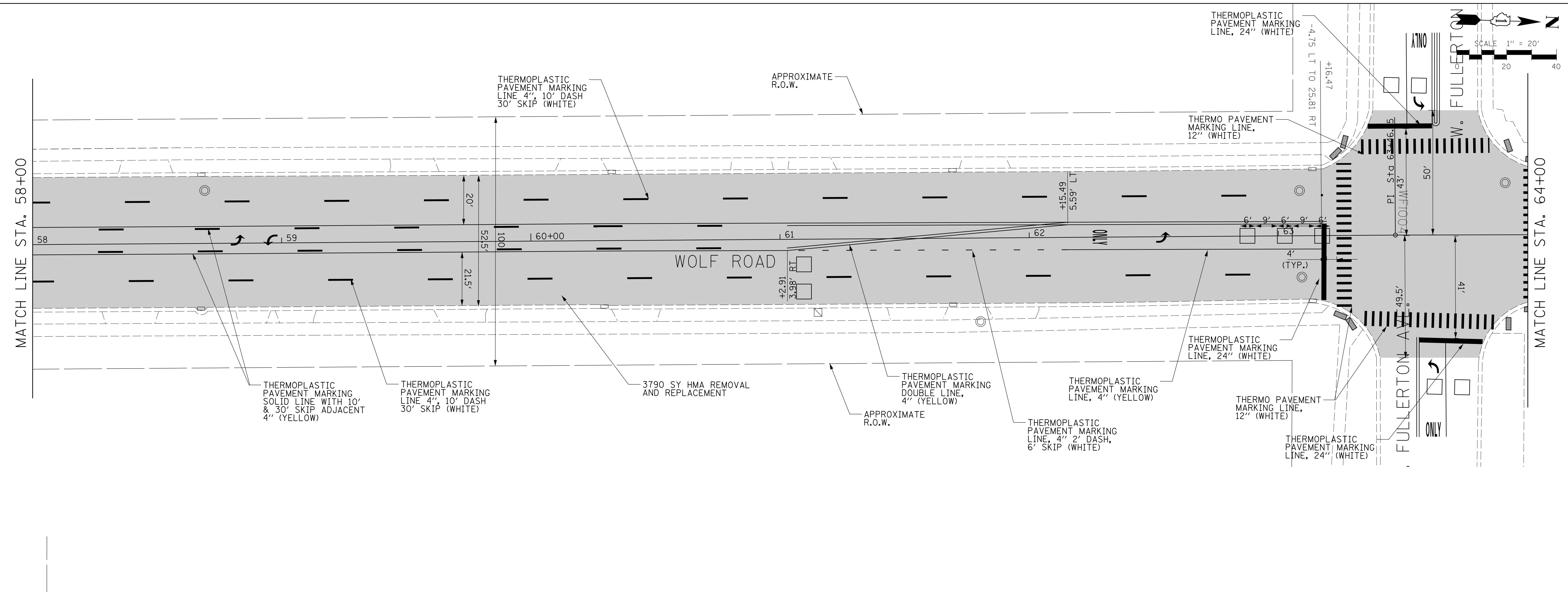
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
WOLF ROAD**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. 46+00 TO STA. 58+00

F.A.U. RTE. = 2690	SECTION = 17-00091-00-RS	COUNTY = COOK	TOTAL SHEETS = 20	SHEET NO. = 8
				CONTRACT NO. 61E63
ILLINOIS FED. AID PROJECT				





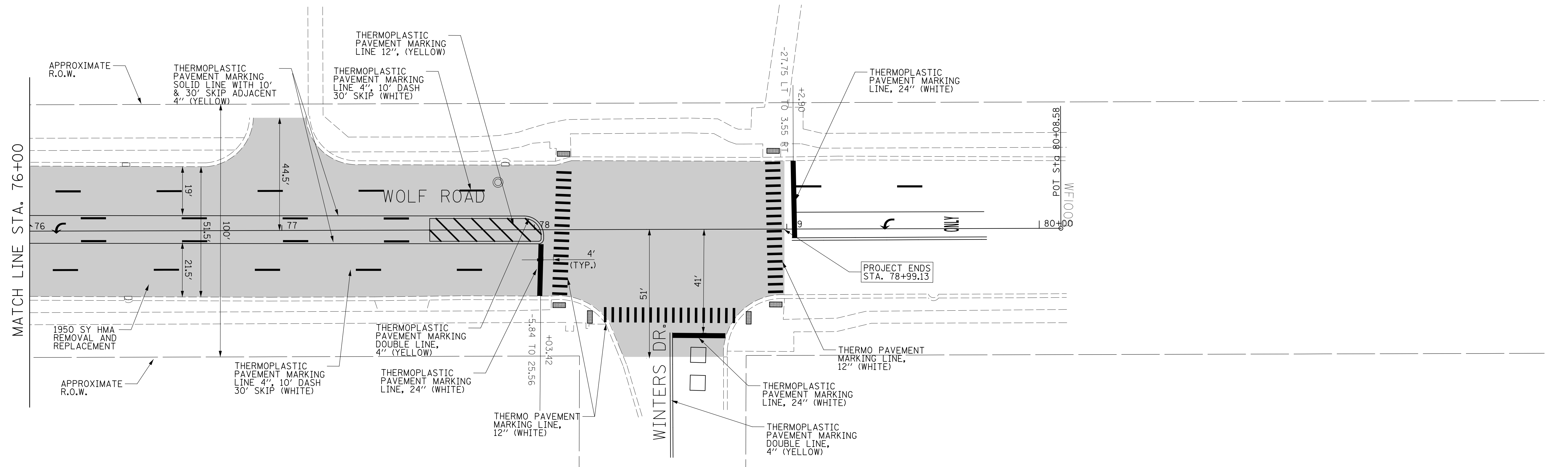
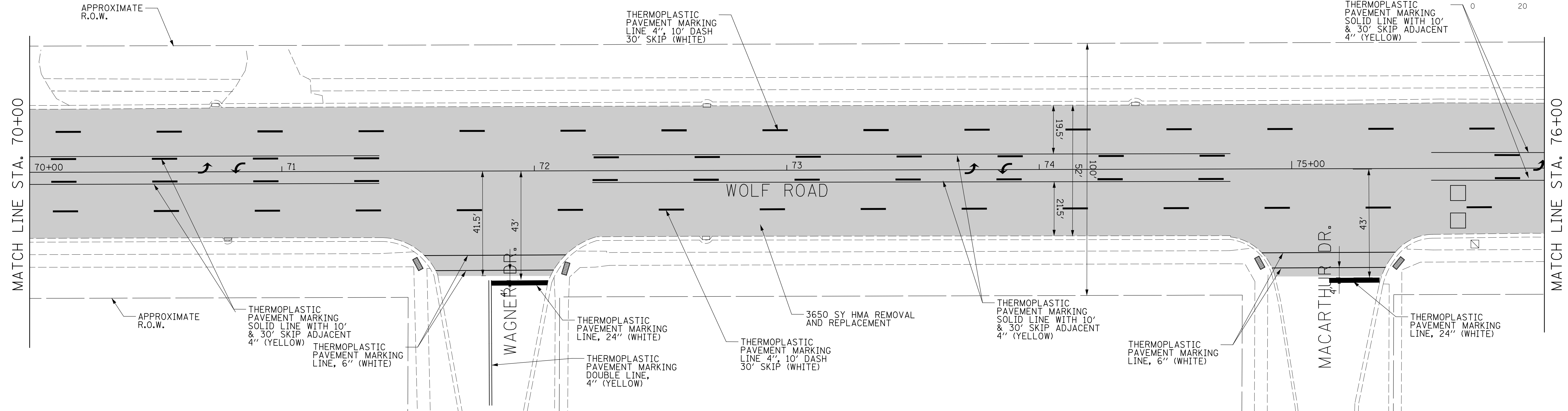
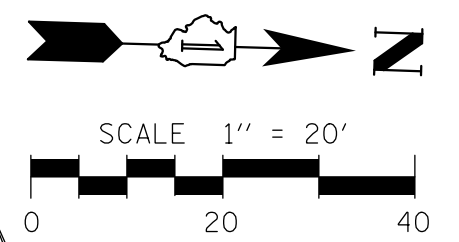
FILE NAME =	USER NAME = wbioftus	DESIGNED - WBL	REVISED -
N:\NORTHLAKE\940032HR - Municipal Review	Projects\940032 HR 200\940032HR281\Civil1	PROJECT\940032HR281-03	REVISED -
	PLOT SCALE = 20'	CHECKED - MW	REVISED -
	PLOT DATE = 2/16/2018	DATE - 8/17/17	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
WOLF ROAD**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. 58+00 TO STA. 70+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2690	17-00091-00-RS	COOK	20	9
CONTRACT NO. 61E63			ILLINOIS FED. AID PROJECT	



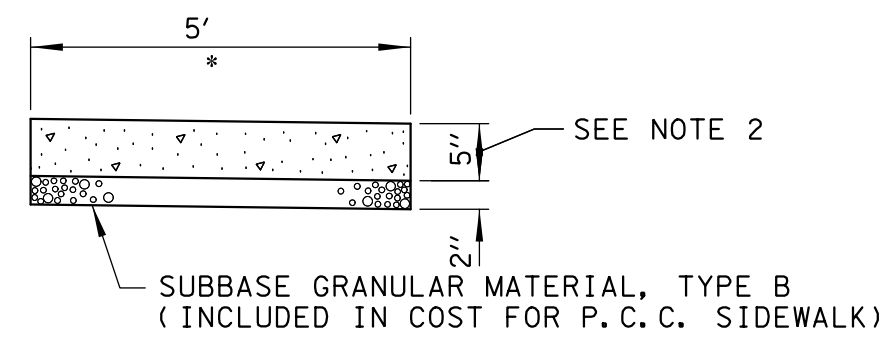
FILE NAME = N:\NORTHLAKE\940032HR - Municipal Review	USER NAME = wbioftus	DESIGNED - WBL	REVISED -
Projects\940032 HR 200\940032HR281\Civil1	PROJECT = 17-00091-00-RS	CHECKED - MW	REVISED -
PLOT SCALE = 20'	DATE = 8/17/17		
PLOT DATE = 2/16/2018			

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
WOLF ROAD**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. 70+00 TO STA. 80+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2690	17-00091-00-RS	COOK	20	10
CONTRACT NO. 61E63				
ILLINOIS FED. AID PROJECT				

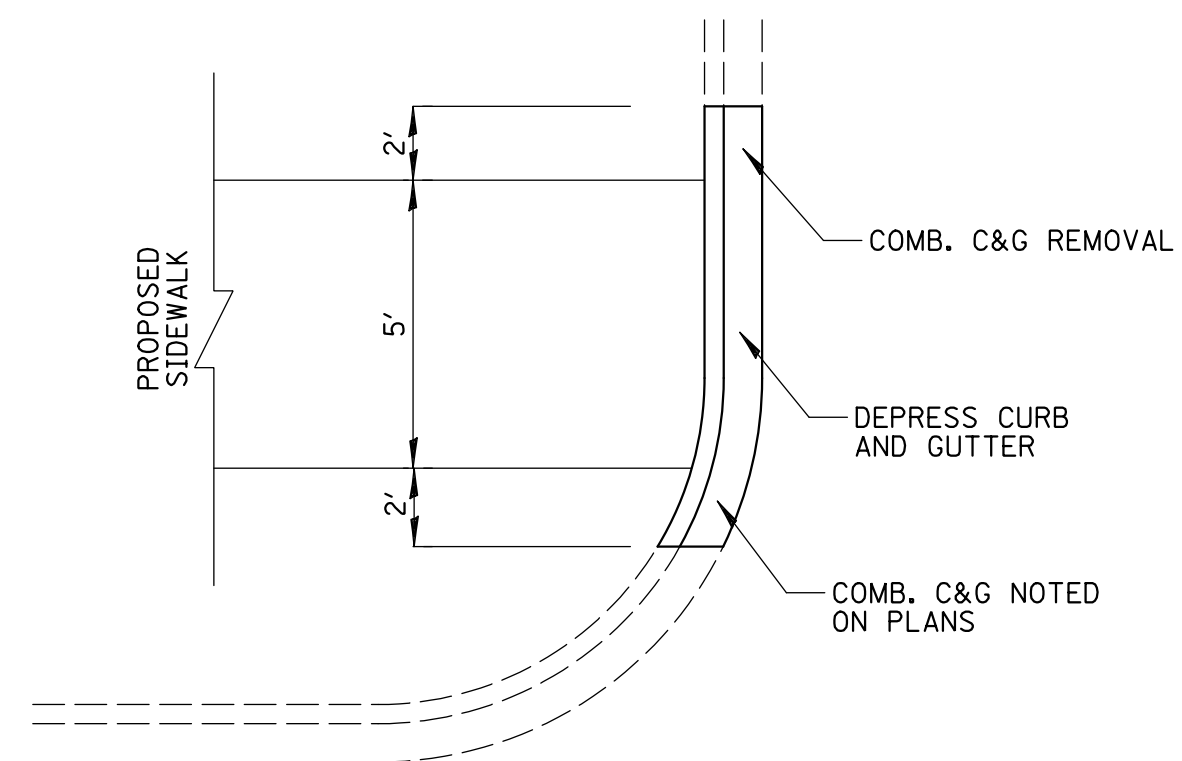


• CROSS SLOPE 2%

**NOTES:**

1. ALL REQUIRED EARTH EXCAVATION AND SUBBASE GRANULAR MATERIAL, TYPE B TO CONSTRUCT P.C.C. SIDEWALK TO MEET CURRENT ADA REQUIREMENTS SHALL BE INCLUDED IN THE COST FOR P.C.C. SIDEWALK.
2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
3. ALL LANDSCAPE RESTORATION (TOPSOIL, SEEDING, CLASS I, MULCH METHOD 3) SHALL BE INCLUDED IN COST FOR P.C.C. SIDEWALK.

**P.C.C. SIDEWALK DETAIL**



**PROPOSED DEPRESSED CURB & GUTTER  
EXISTING CURB & GUTTER NOT DEPRESSED  
NOT TO SCALE**

**P.C.C. SIDEWALK DETAIL**

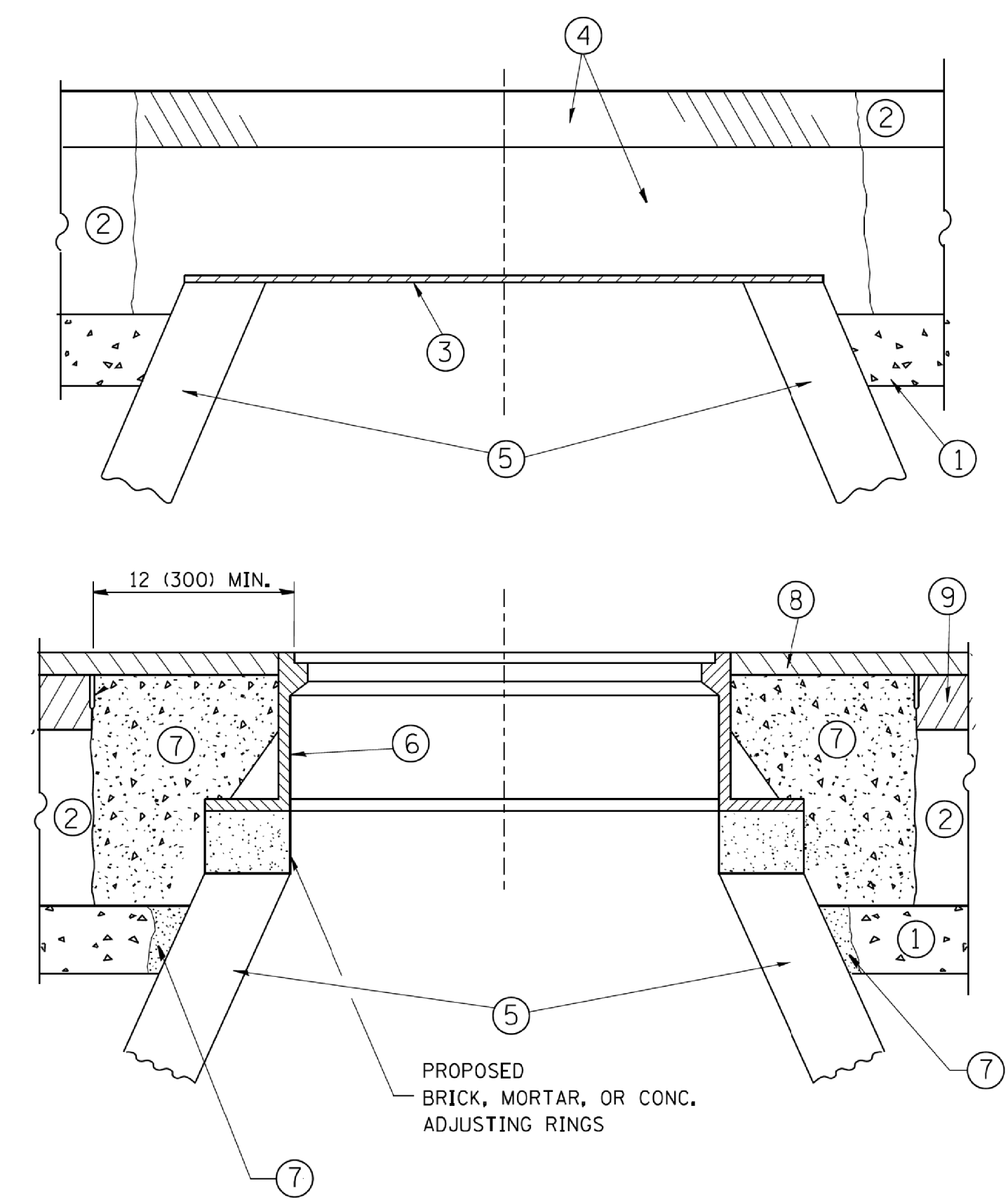
FILE NAME =	USER NAME = wbioftus	DESIGNED - WBL	REVISED -
N:\NORTHLAKE\940032HR - Municipal Review	Projects\940032 HR 200\940032HR281\Civil1	DRAWN 032HR281.sh WBL	REVISED -
	PLOT SCALE = 20'	CHECKED - MW	REVISED -
	PLOT DATE = 2/16/2018	DATE - 8/17/17	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAILS**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2690	17-00091-00-RS	COOK	20	11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E63	



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

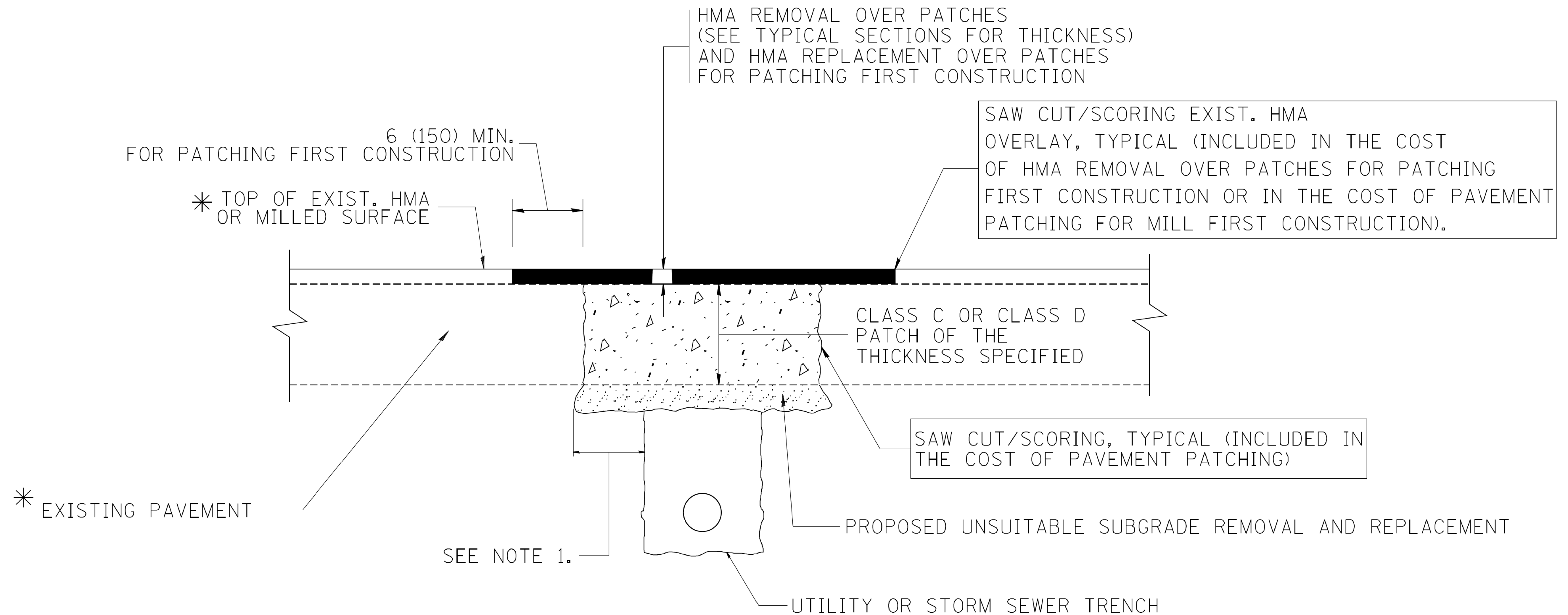
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
et:\p\work\p1\dot\bauerdl\d0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1/648.5000' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2690	17-00091-00-RS	COOK	20	12
<b>BD600-03 (BD-8)</b>		CONTRACT NO.	61E63	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\ds\std22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	2690	17-00091-00-RS	COOK	20	13
		PLOT SCALE = 50.000' / IN.	CHECKED -		REVISED - R. BORO 09-04-07				<b>BD400-04 (BD-22)</b>		CONTRACT NO. 61E63			
		PLOT DATE = 10/27/2008	DATE - 10-25-94		REVISED - K. ENG 10-27-08	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT								

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

② SEEDING, MULCH, AND TOPSOIL, FURNISH AND PLACE 4" WILL NOT BE PAID FOR SEPARATELY.  
~~② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED~~

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

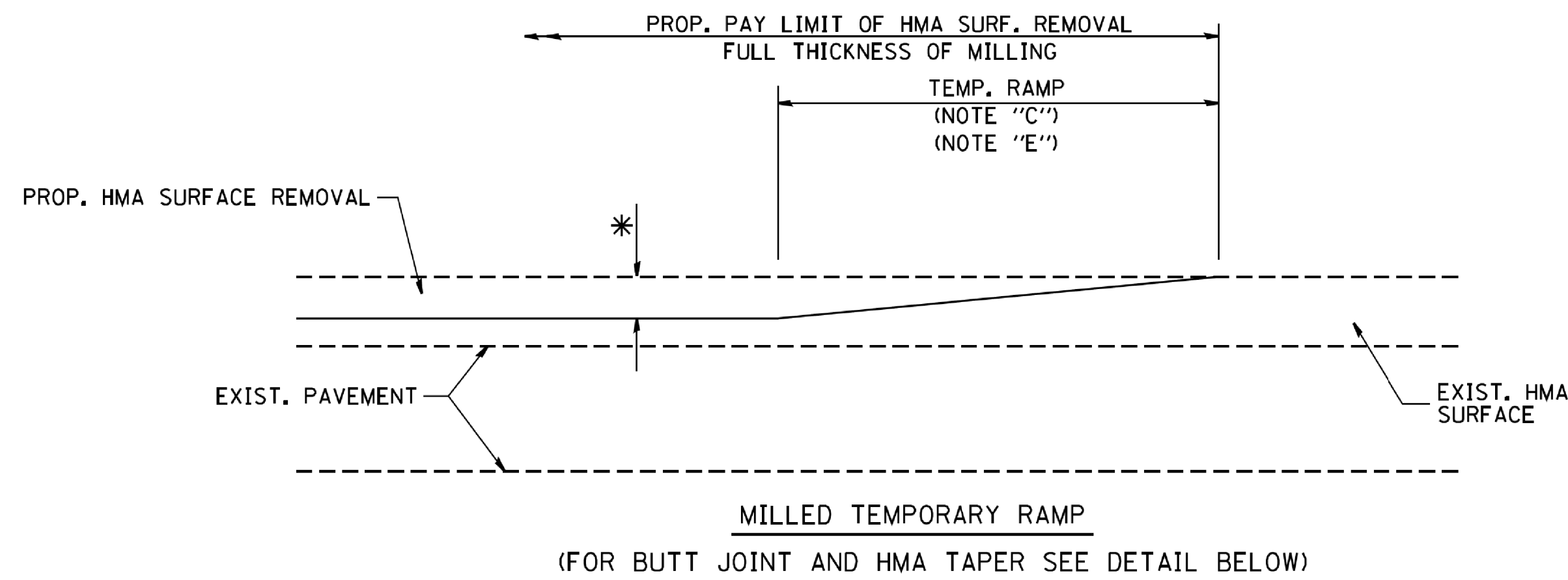
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

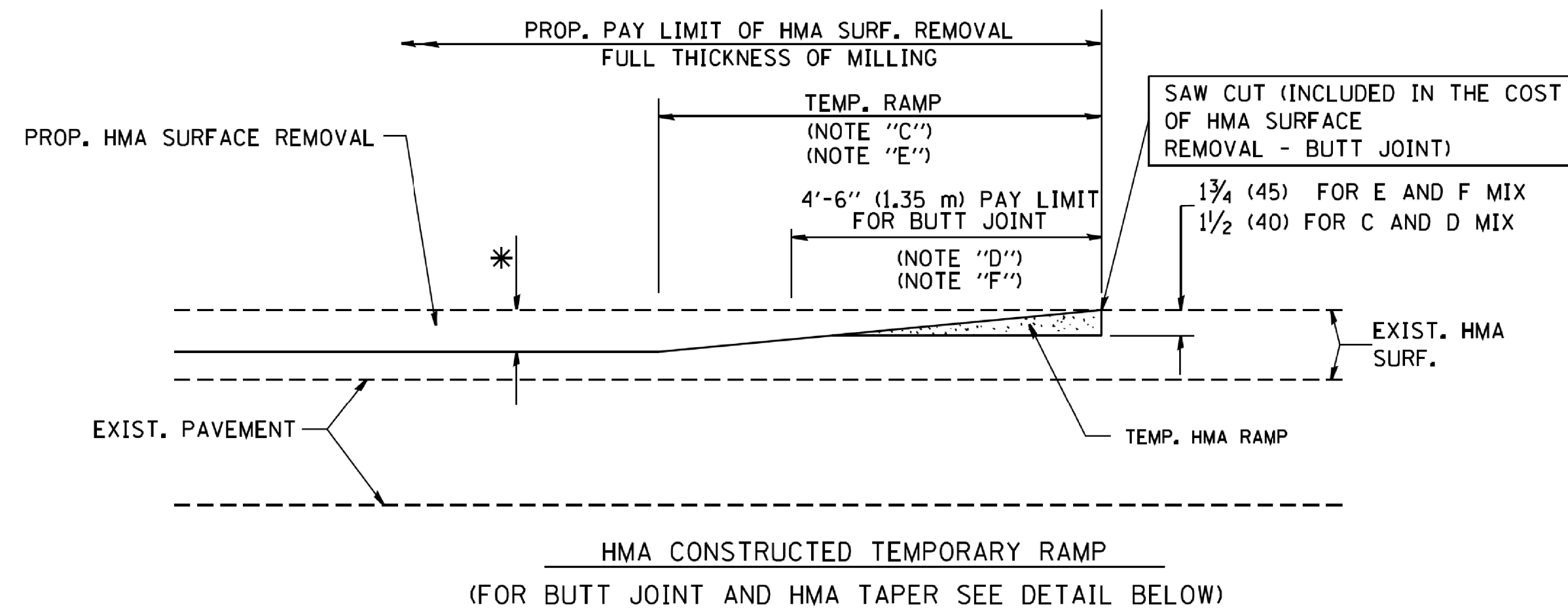
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
e:\pwwork\pwwork\drvakosgn\0108315\bc24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	2690	17-00091-00-RS	COOK	20	14
		PLOT SCALE = 50.000' / IN.	REVISED - M. GOMEZ 01-22-01						<b>BD600-06 (BD-24)</b>		CONTRACT NO.	61E63	
		PLOT DATE = 12/15/2009	DATE - 03-11-94		REVISED - R. BORO 12-15-09				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

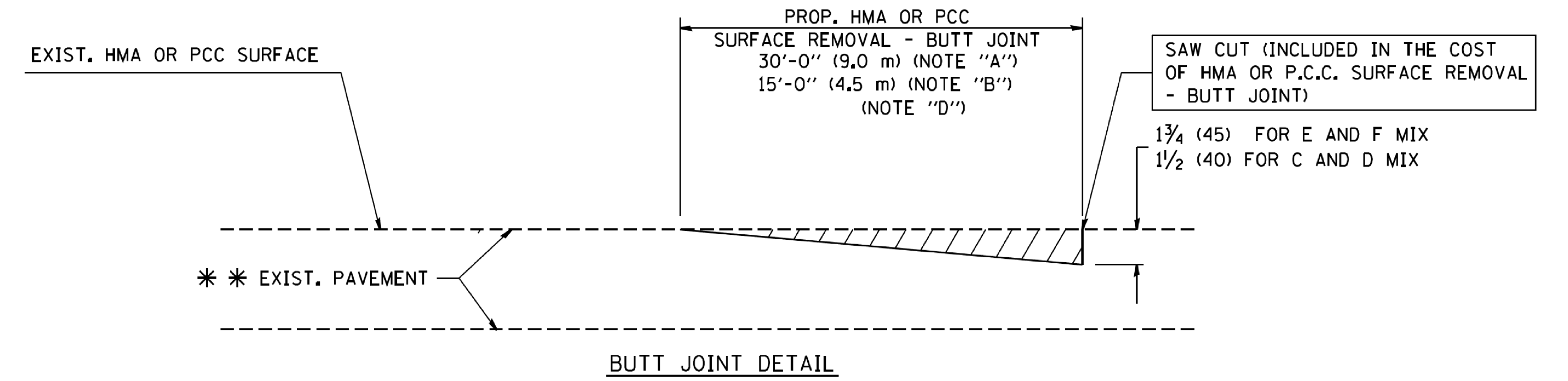


**OPTION 1**

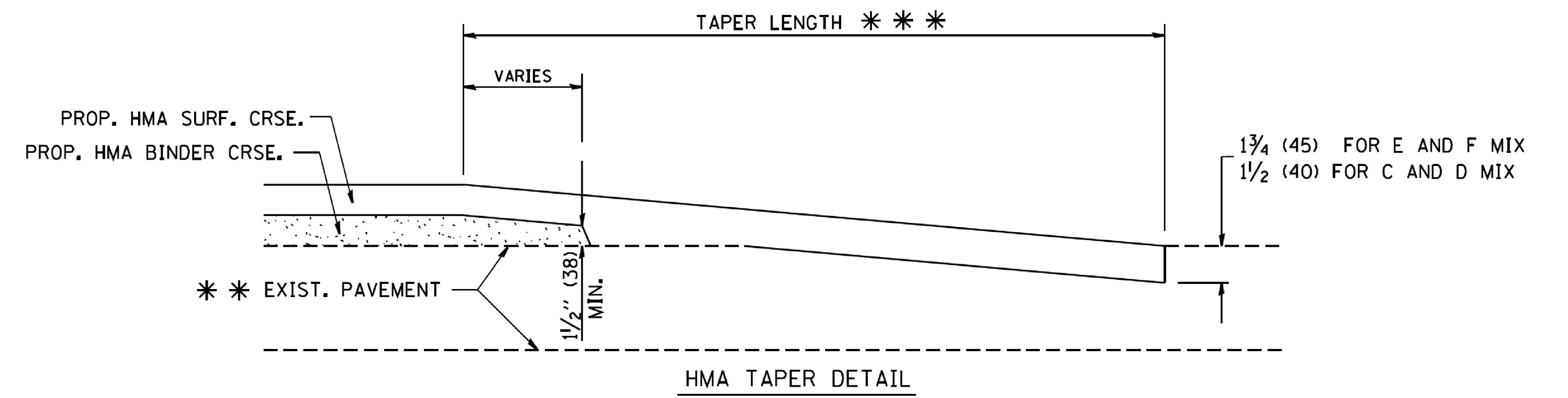


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

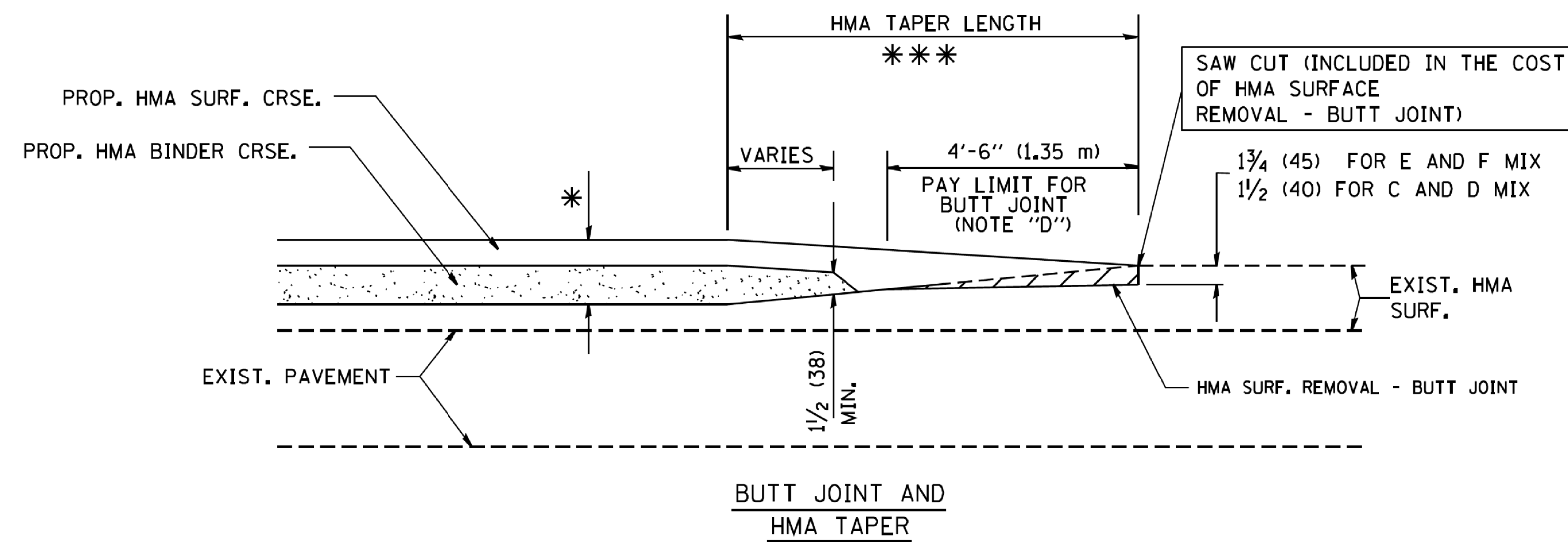
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



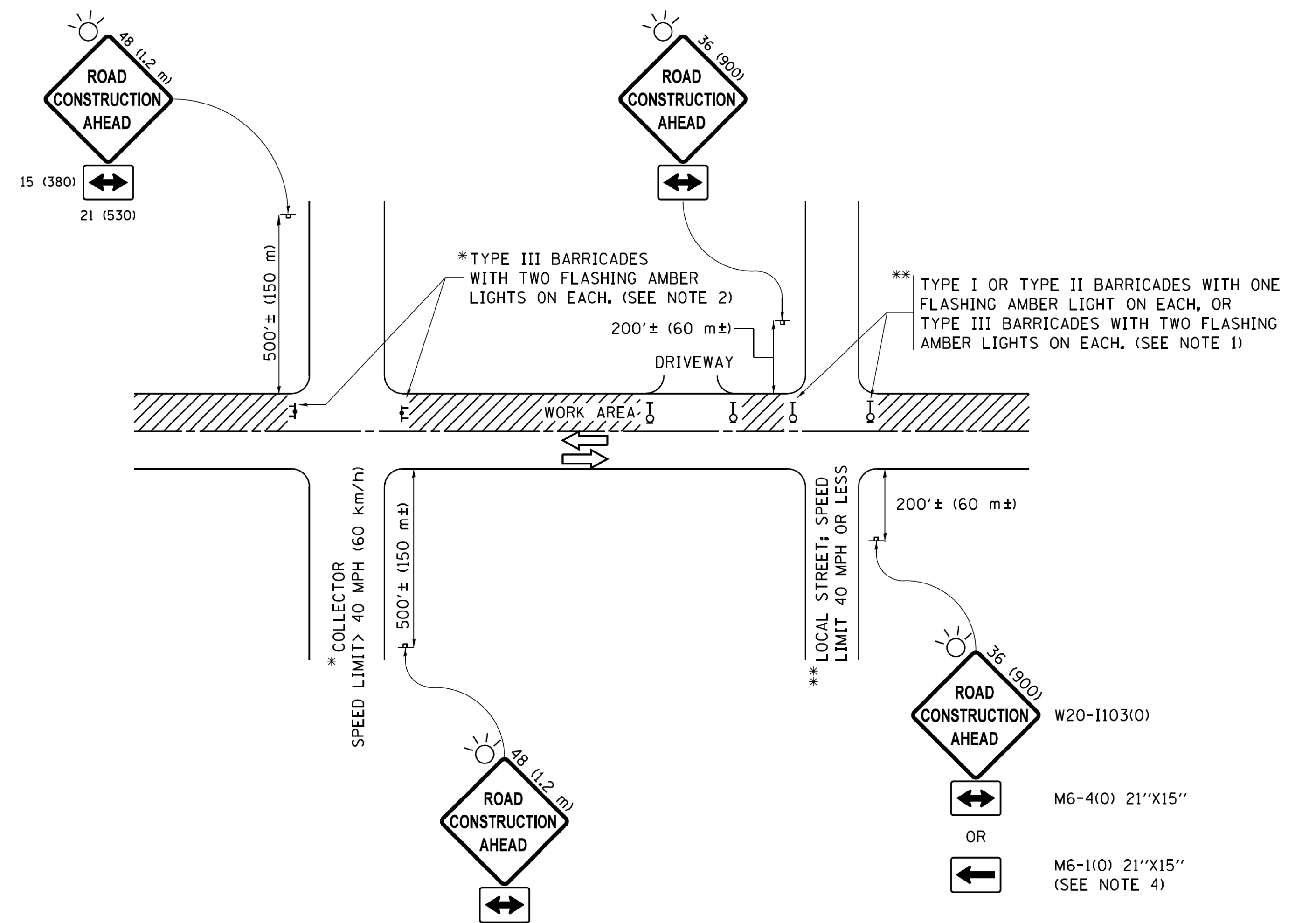
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME = W:\dststd\22x34\bd32.dgn	USER NAME = geglonebt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2690	17-00091-00-RS	COOK	20	15
<b>BD400-05 BD32</b>		CONTRACT NO.	61E63	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footenj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\1\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\CAD\to\CADsheets\to10.dgn		DRAWN	REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

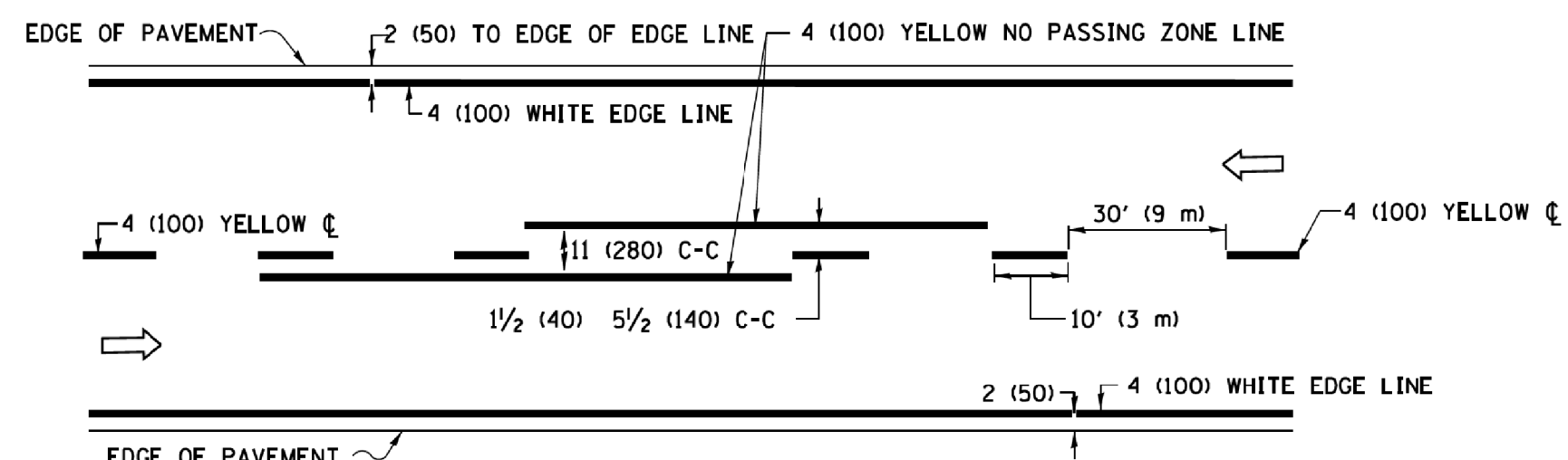
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

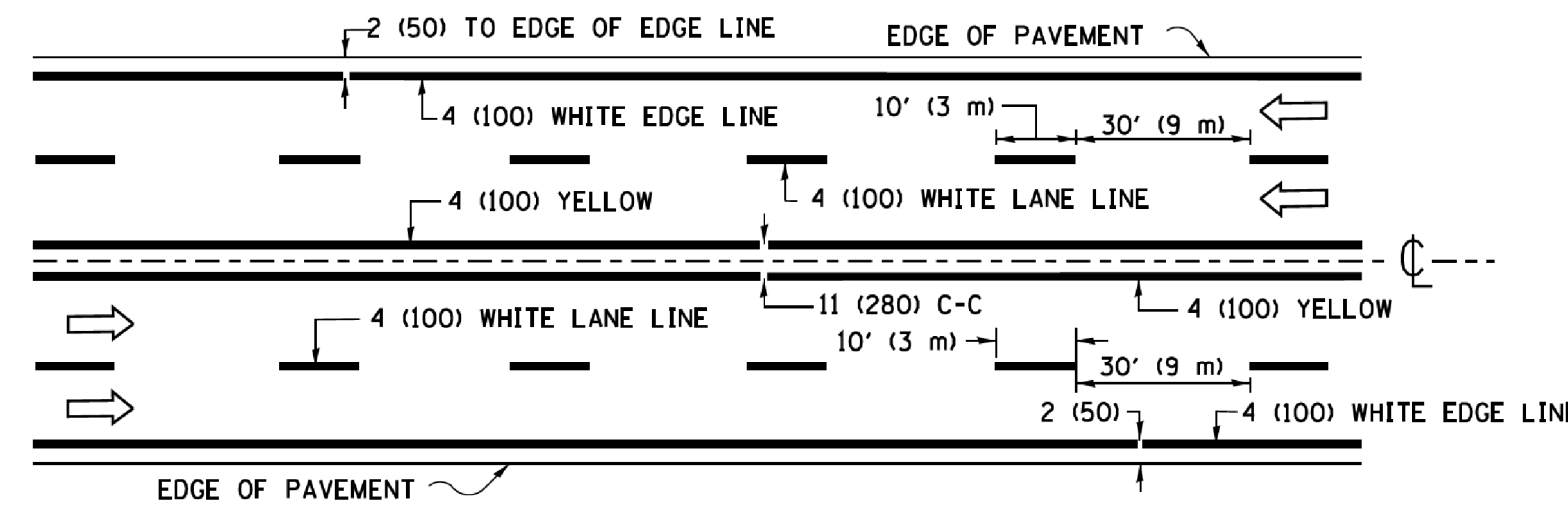
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2690	17-00091-00-RS	COOK	20	16
<b>TC-10</b>			<b>CONTRACT NO. 61E63</b>	
ILLINOIS FED. AID PROJECT				

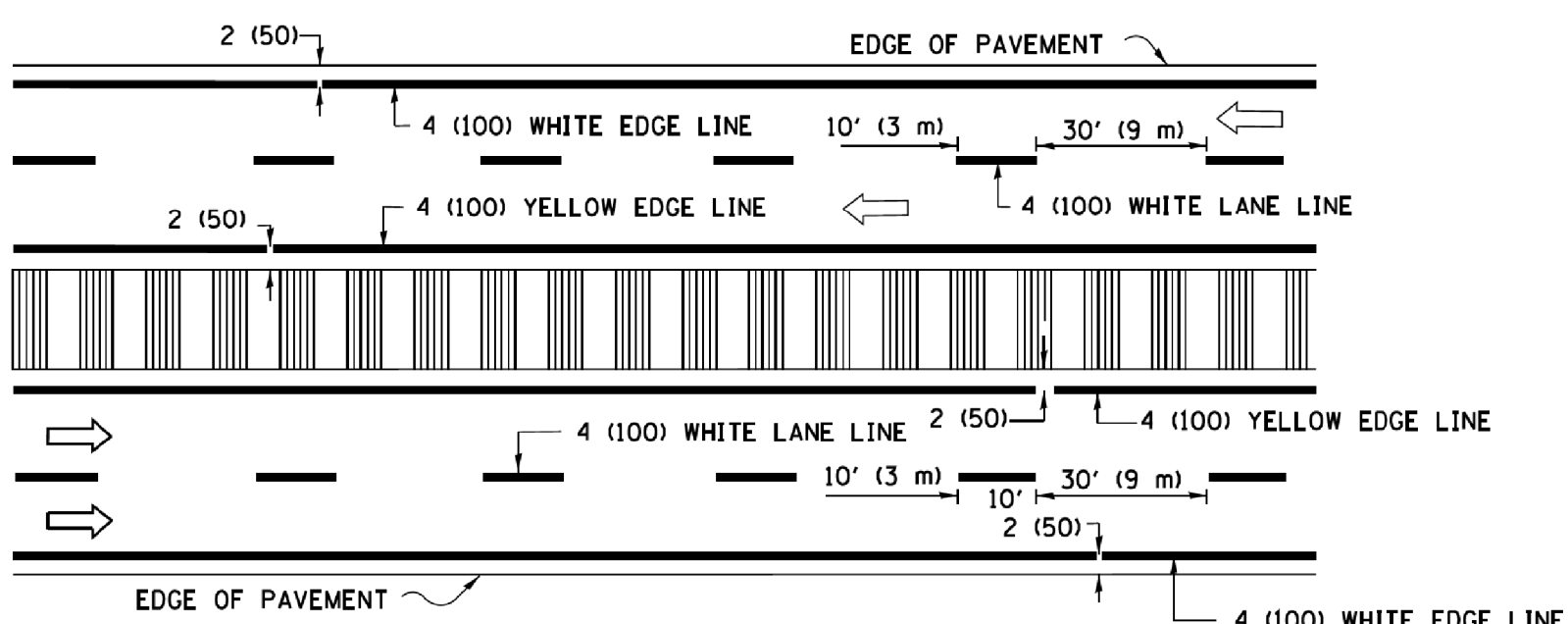




**2-LANE ROADWAY**

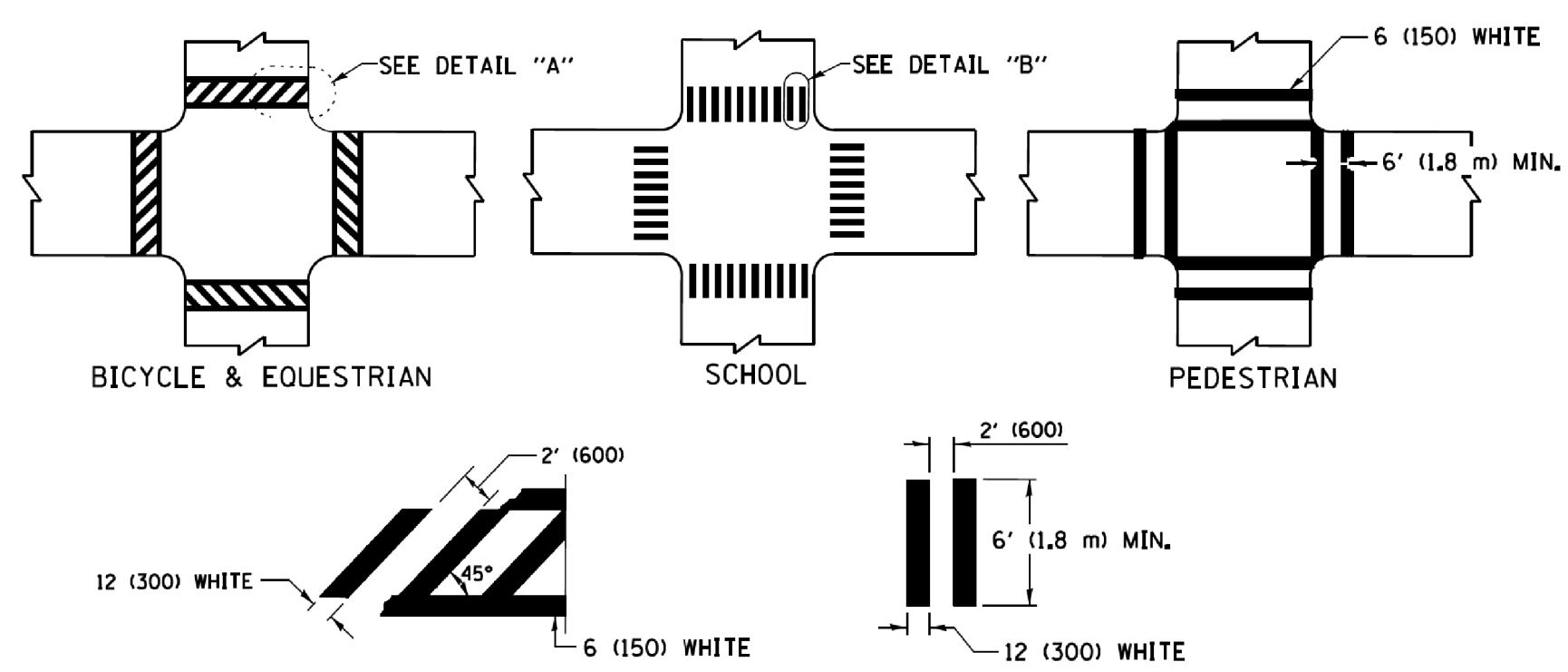


**MULTI-LANE UNDIVIDED**



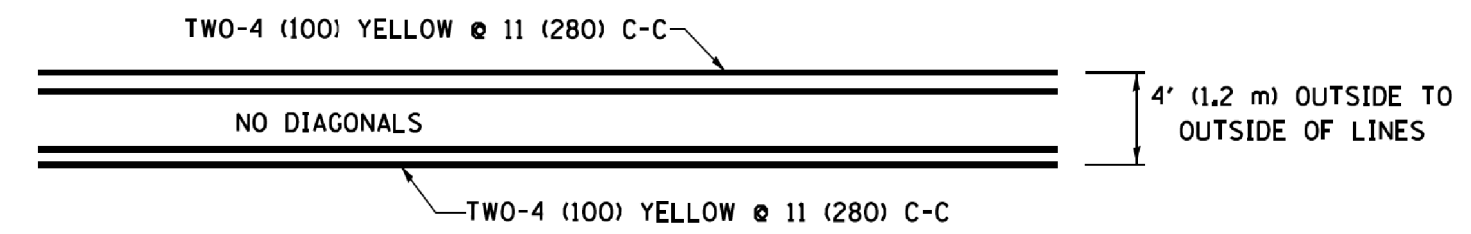
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

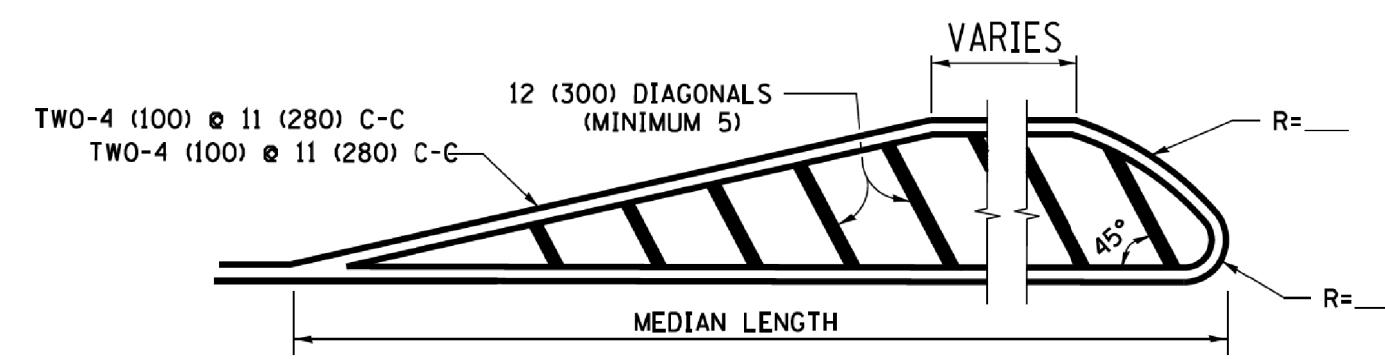


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



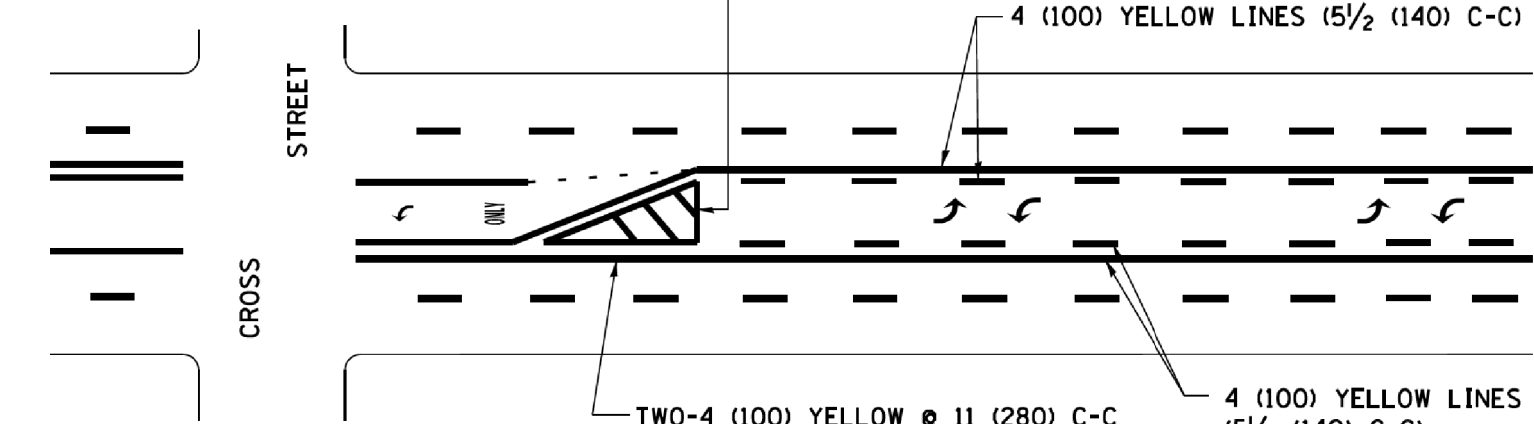
**4' (1.2 m) WIDE MEDIANS ONLY**



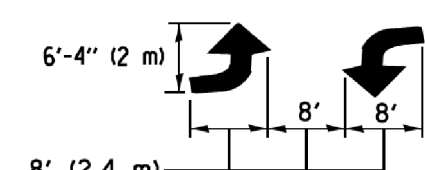
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**

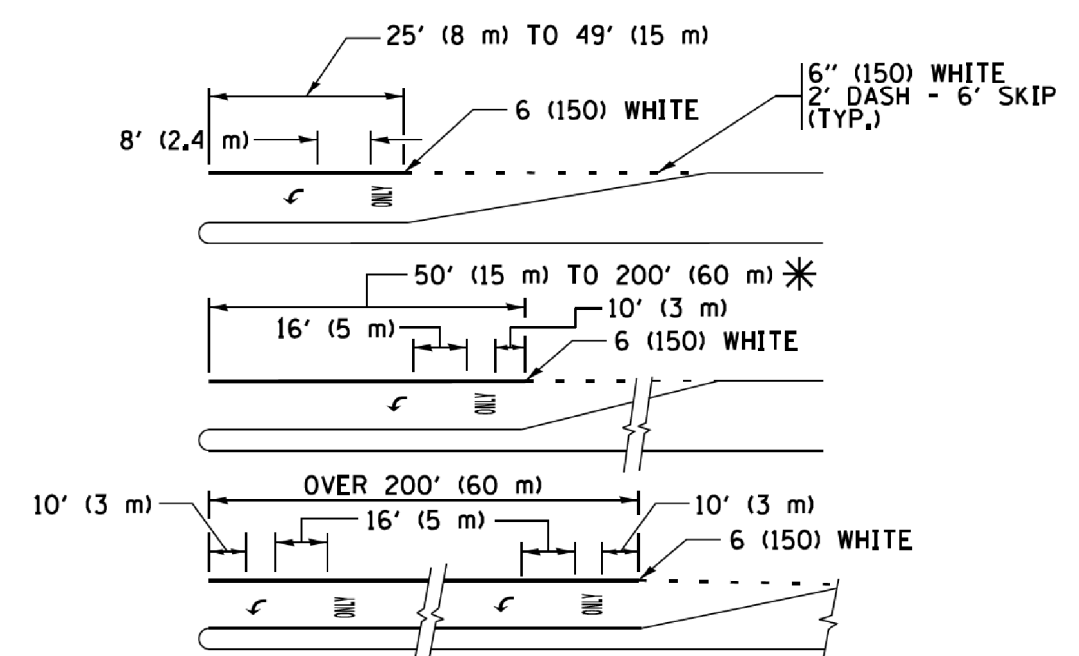


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

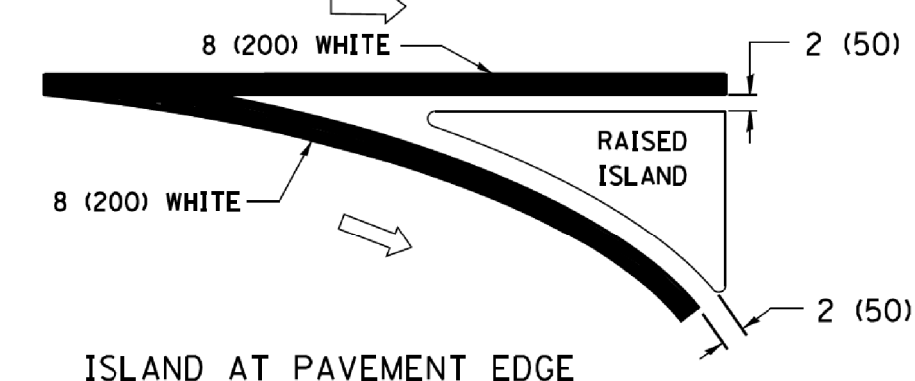
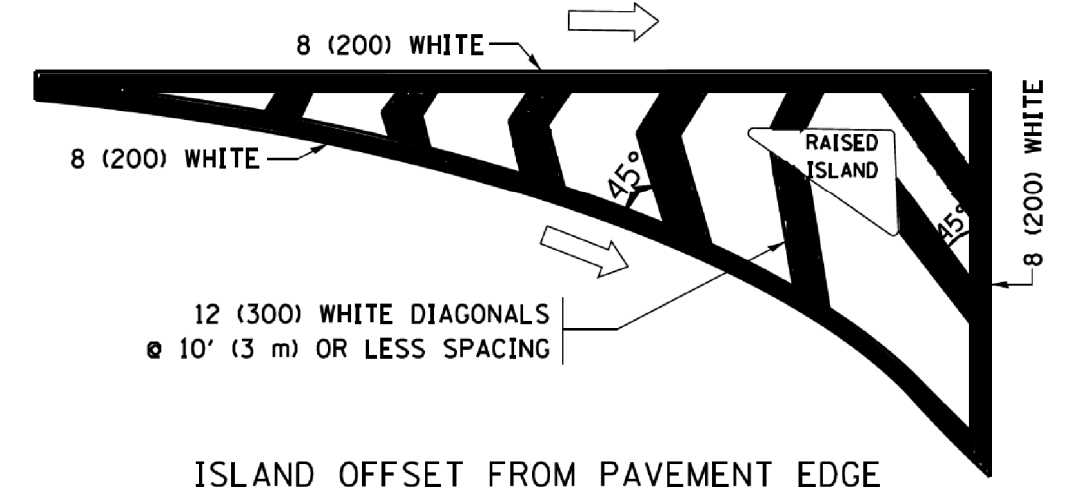


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

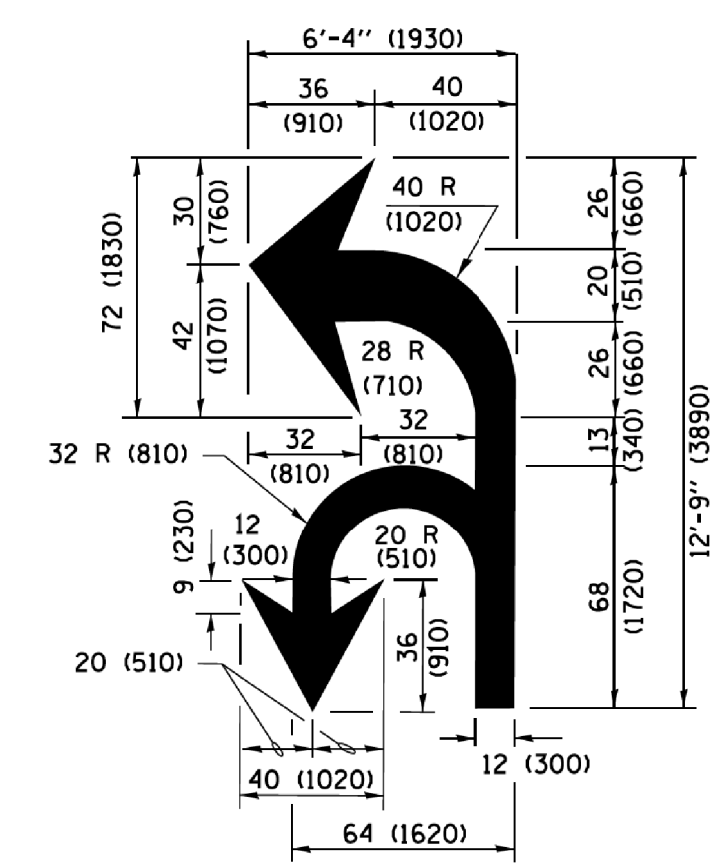
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

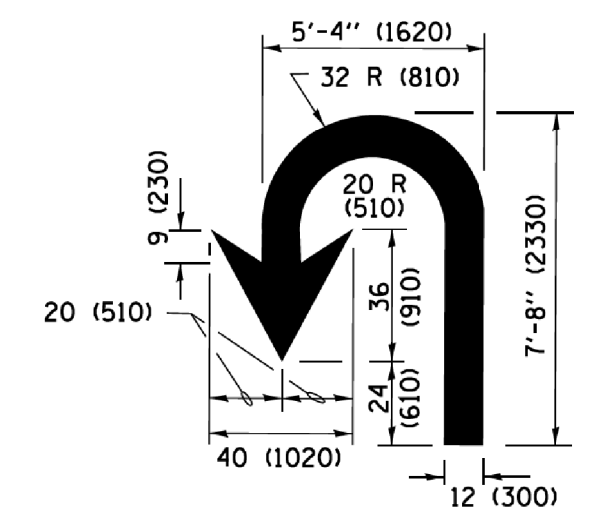
**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

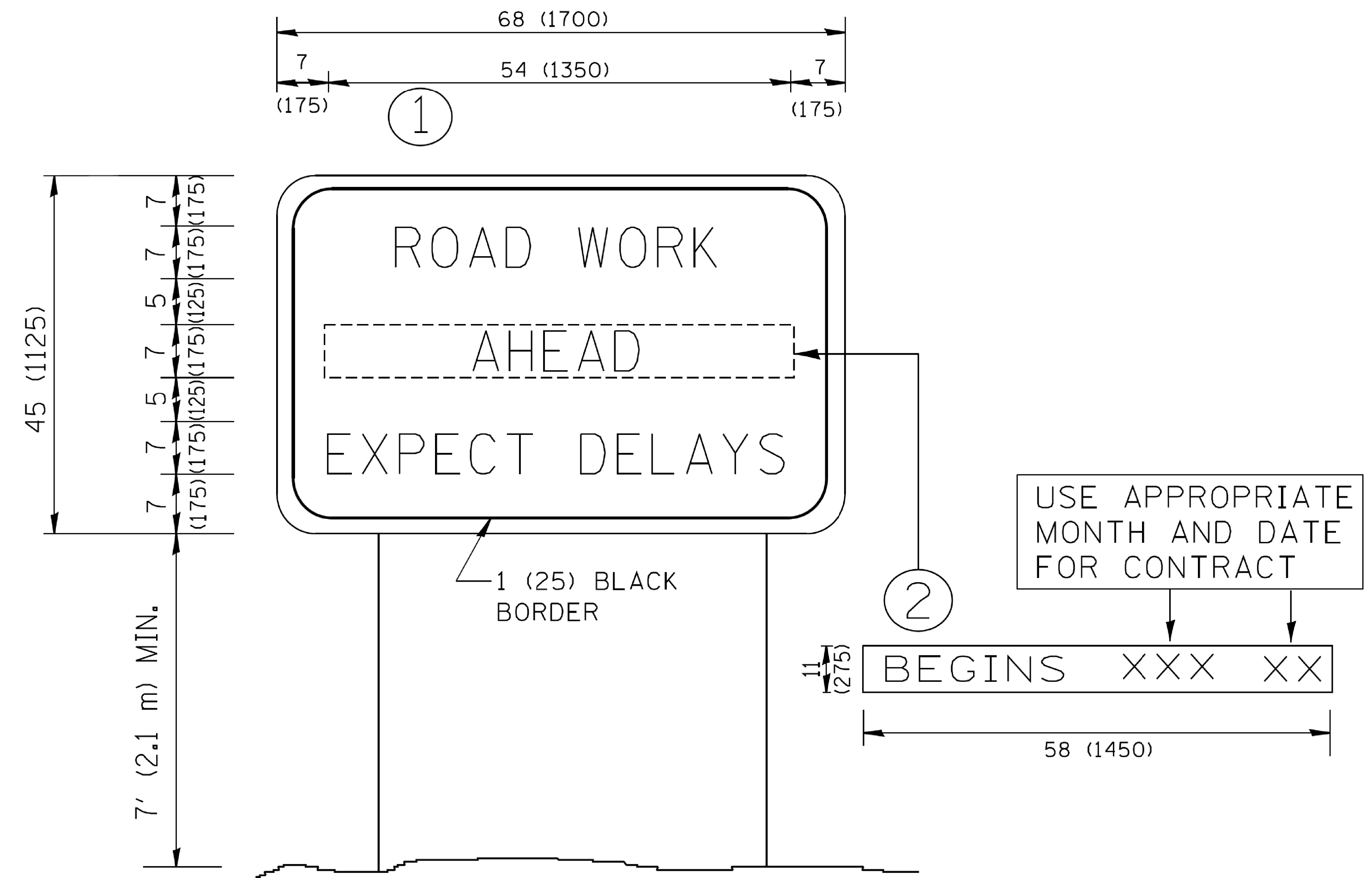
D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.





**NOTES:**

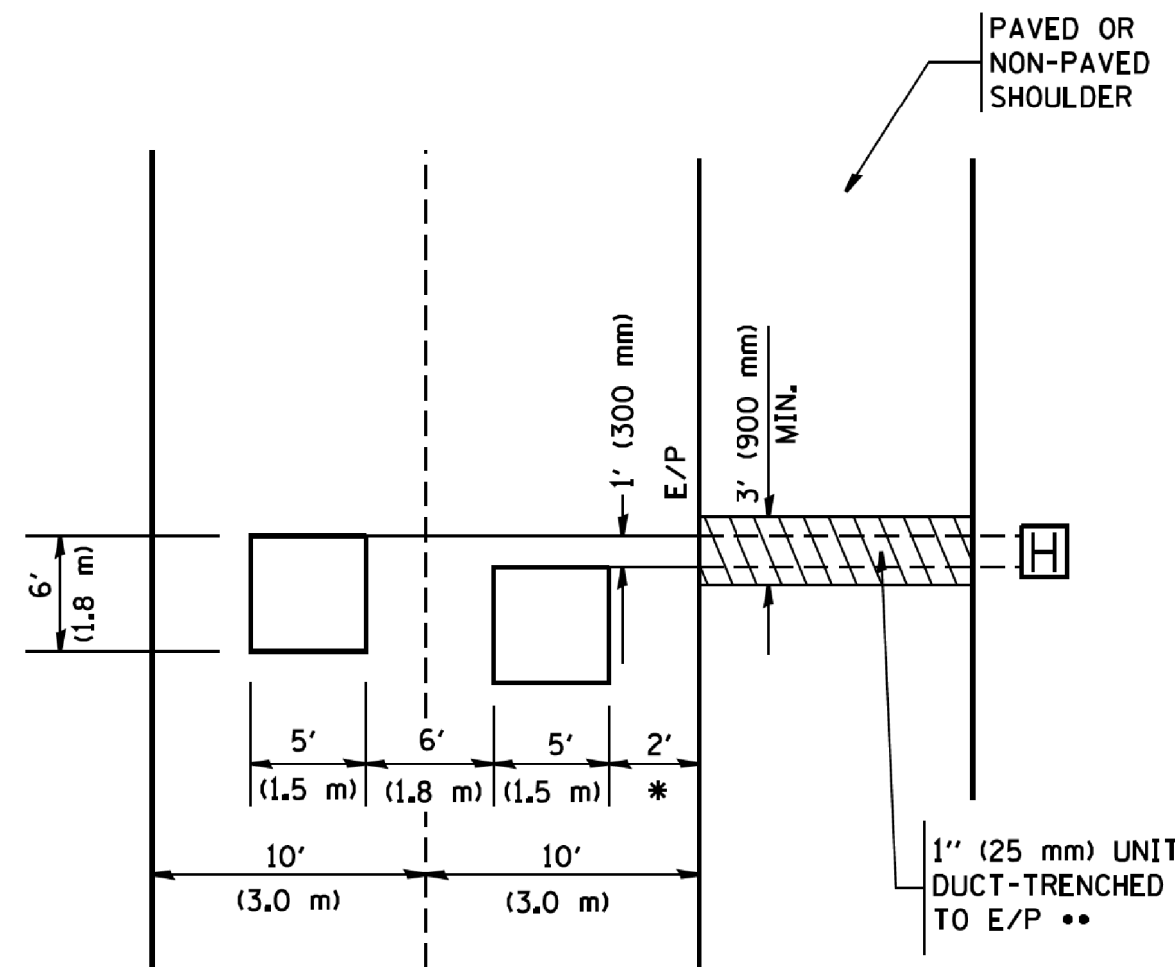
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distsd\22x34\to22.dgn	USER NAME = gaglianobt	DESIGNED - DRAWN -	REVISED - REVISED -	R. MIRS 09-15-97 R. MIRS 12-11-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>			F.A.U. RTE. 2690	SECTION 17-00091-00-RS	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 19
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -	T. RAMMACHER 02-02-99		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT	
	PLOT DATE = 1/4/2008	DATE -	REVISED -	C. JUCIUS 01-31-07				<b>TC-22</b>		<b>CONTRACT NO.</b>		61E63	

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



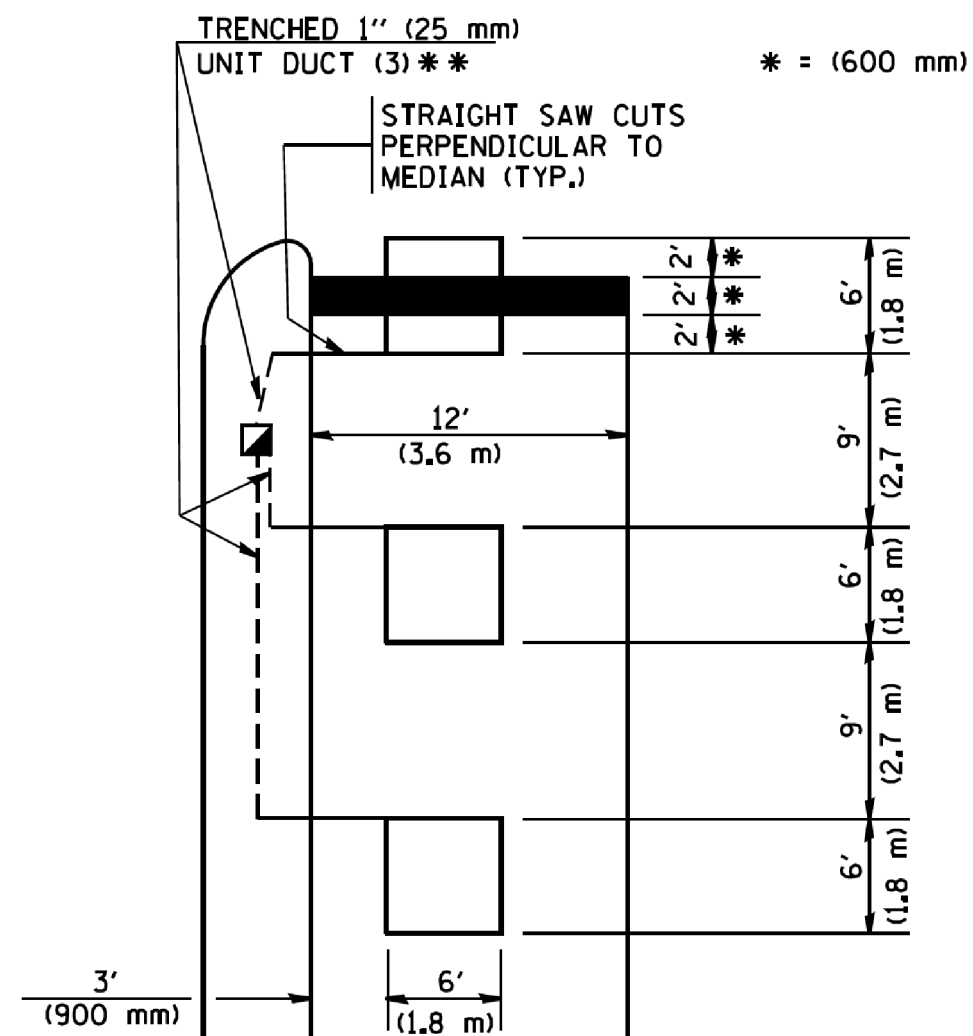
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

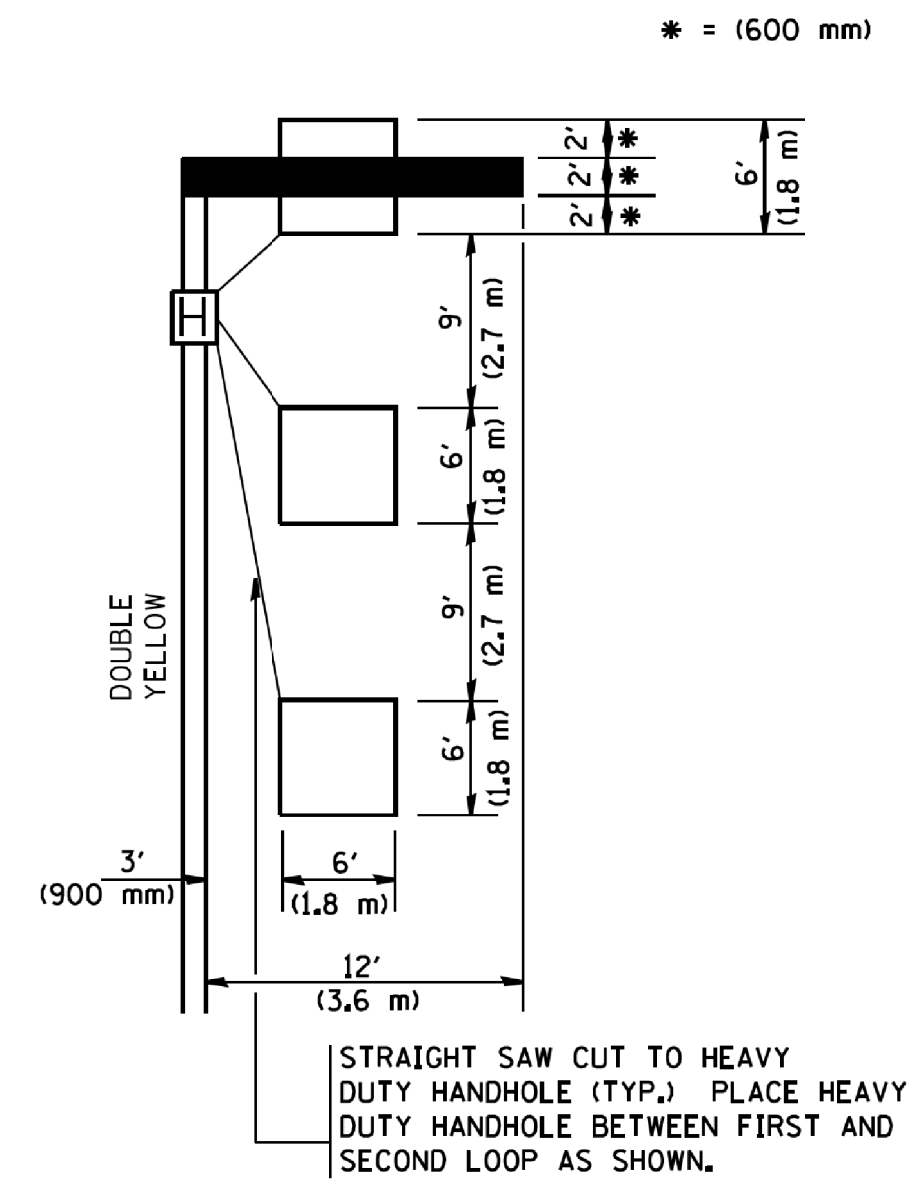


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



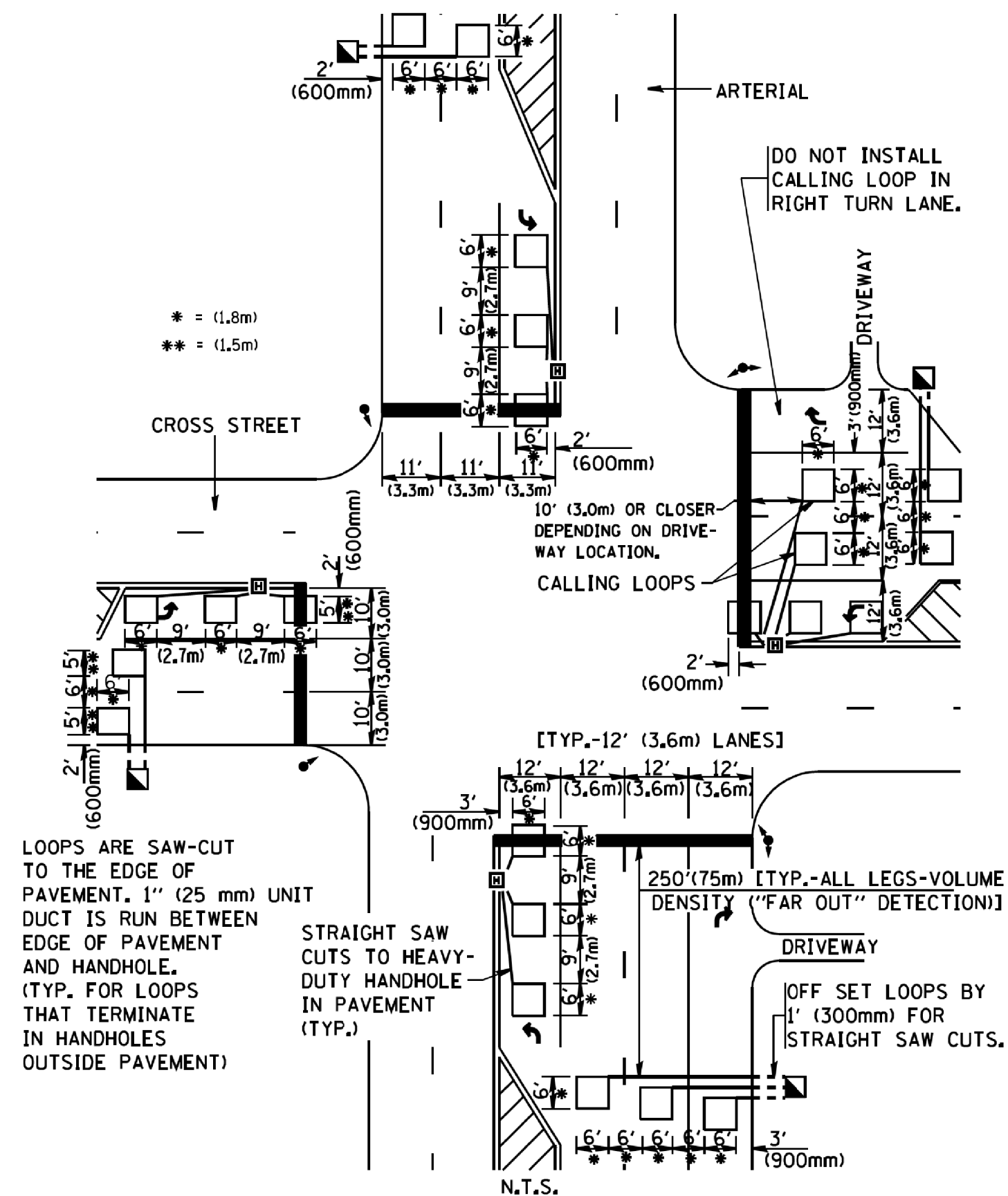
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

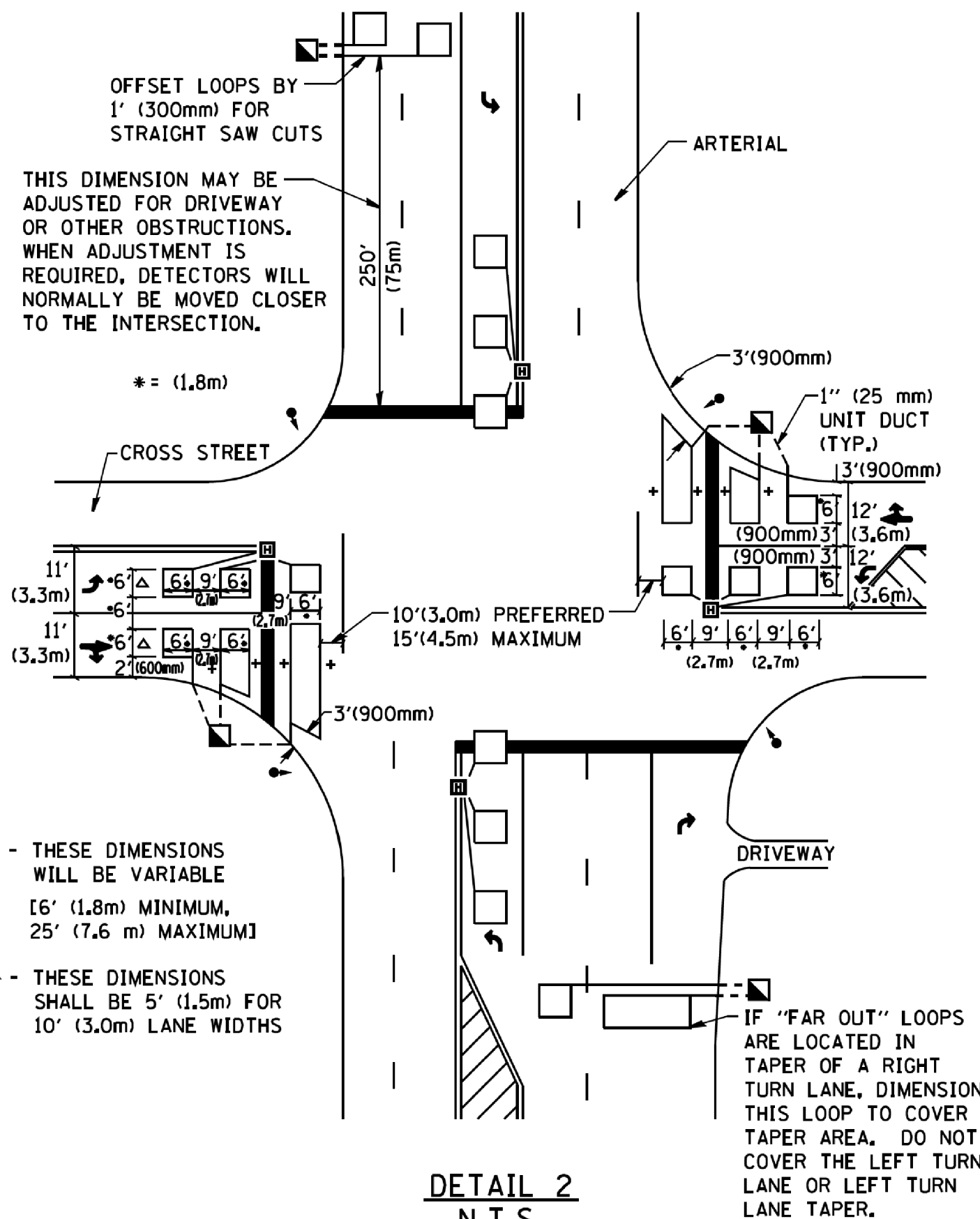
- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\dststd\22x34\ts07.dgn	USER NAME = geglant	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2690	17-00091-00-RS	COOK	20	20
<b>TS-07</b>		<b>CONTRACT NO.</b>	61E63	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				